

# **South Anthony Boulevard Railroad Grade Separation Study Report to Community**

**November 30, 2011**

**Andorfer Commons Theatre –  
Indiana Tech University**



## Goals for Tonight's Meeting

- Recap of the meeting from last November
- Provide an overview of the options that were developed for the feasibility study
- Present recommended option
- Present opportunities for enhancement of the project
- Solicit input from attendees
- Respond to questions/comments





**Project Location**



## Statistics

- **History of congestion**
- **South Anthony Boulevard– over 13,000 vehicles per day**
- **Wayne Trace – over 7,000 vehicles per day**
- **Norfolk Southern Railroad - 50 trains per day per Norfolk Southern**
- **Trucks and large emergency vehicles unable to use corridor**
- **Barrier to investment and development**

## Goals

- **Develop a long-term solution**
- **Assure public input on the solution**
- **Meet the long-term needs of the community**
- **Enhance investment opportunities on Anthony Boulevard**
- **Maximize connectivity for Southeast Fort Wayne**
- **Give this vicinity a sense of place and placemaking**
- **Integrate historic aspects and enhance user experience**
- **Provide structures with a 75 to 100 year life expectancy**



## **Public Involvement**

- **Public Introduction – November 18, 2010**
- **Focus Group Sessions – Early 2011**
  - **Urban Enterprise Association Board**
  - **Corridor and Area Businesses**
  - **City Council Members**
  - **Local Government Leaders**
  - **Community Leaders**
  - **Neighborhood Residents**
  - **Transportation and Emergency services**
- **Majority of Focus Group participants noted a grade separation was needed**



***“Respect this area, its history, its commerce, and its people.”***

***“Do something great, make this a better place, or don’t do anything at all.”***

## **Train Blockages and Area Street Usage**

- Anthony Boulevard is vital to area travel
- Frequent train blockages have stifled business and housing development
- School Bus Crossings
- Complaints from businesses and delivery companies
- Significant support for keeping Winter Street open
- Concerns raised about trains parking for long periods
- Neighborhood residents report frequent detours to Hanna Street underpass or east to the aging Edsall Street overpass
- Public safety officials try to avoid the area due to potential delays



## Projected Construction Impacts

- Concerns regarding impact on businesses if Anthony Boulevard is closed during construction.
- Current train avoidance strategies would work for most during potential construction closures.
- There was support for keeping Anthony Boulevard open as much as possible during construction.

## Sensitive Features to Be Protected

- Protect existing Anthony Boulevard businesses to the maximum extent possible.
- Protect Shepherd of the City Church and keep it from harm.
- Limit the impacts to remaining viable houses.





## Recommended Area Improvements

- **Lighting**
- **Pedestrian and bicycle connectivity**
- **Bus stops**
- **Neighborhood placemaking**
- **Greenspacing and landscaping**
- **Extend improvements beyond rail crossing**
- **Include private property**

## **Recommended Theme When Improvements are Completed**

- **Urban not suburban**
- **Celebrate history**
- **Use historic materials and decorative elements**
- **Preserve Tokheim Tower**
- **Public art**

## Recommended Land Use Improvements

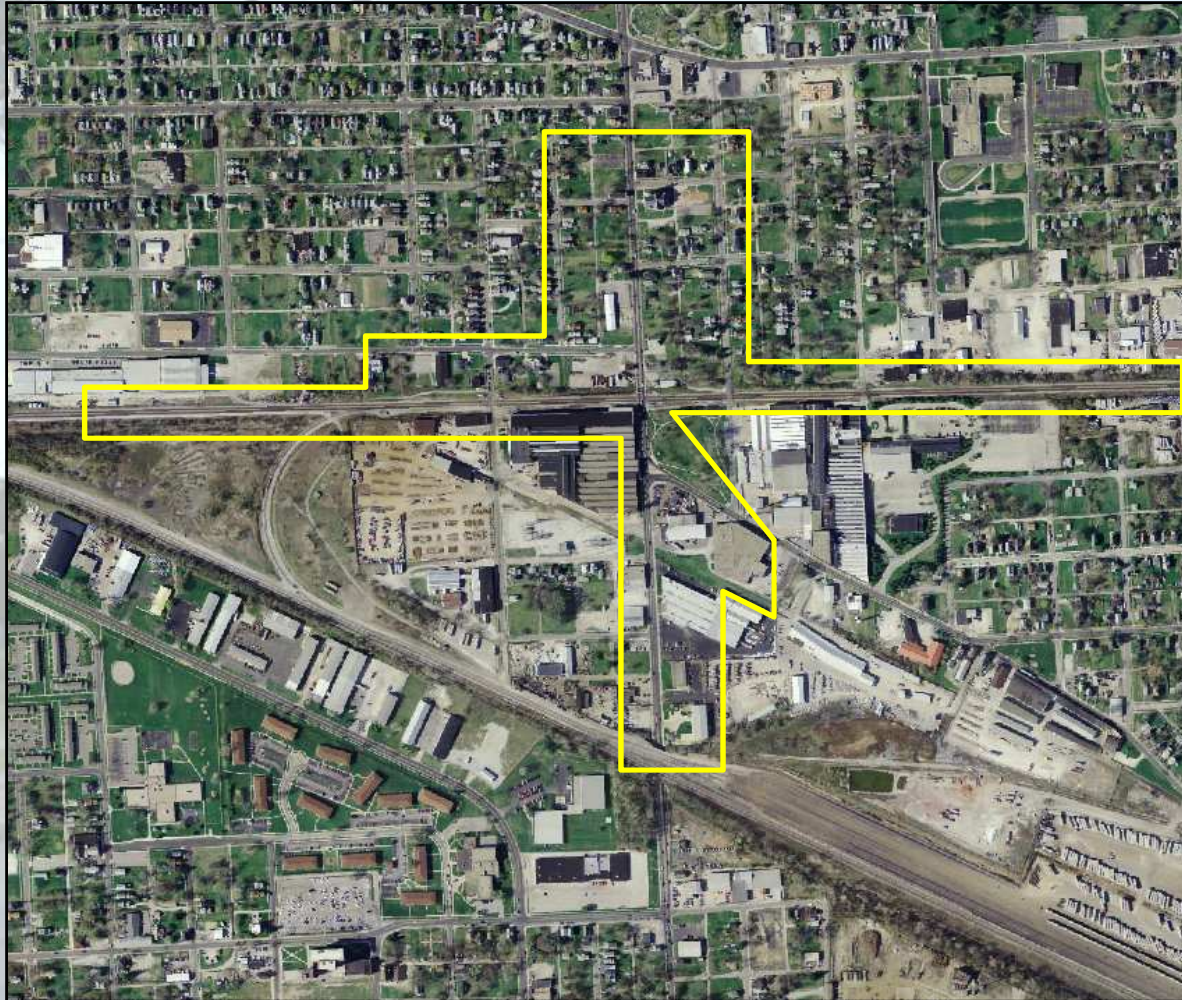
- Industrial corridor
- Quality restaurants and shops
- Quality housing

## Value of Proceeding With a Grade Separation

- 80% ranked 4 or 5
- 100% for certain groups

## What We Have Studied

- Defining the impacted area



## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries – Fort Wayne GIS Database



## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
  - Simons Street
  - Lanternier Street
  - Hayden Street
  - Luther Street
  - Lillie Street
  - Fletcher Street

## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking



## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
  - 50 trains/day
  - 60 MPH
  - No traffic disruption
  - Future 3<sup>rd</sup> track
  - 23' clearance above track
  - 16'-6" clearance below structure

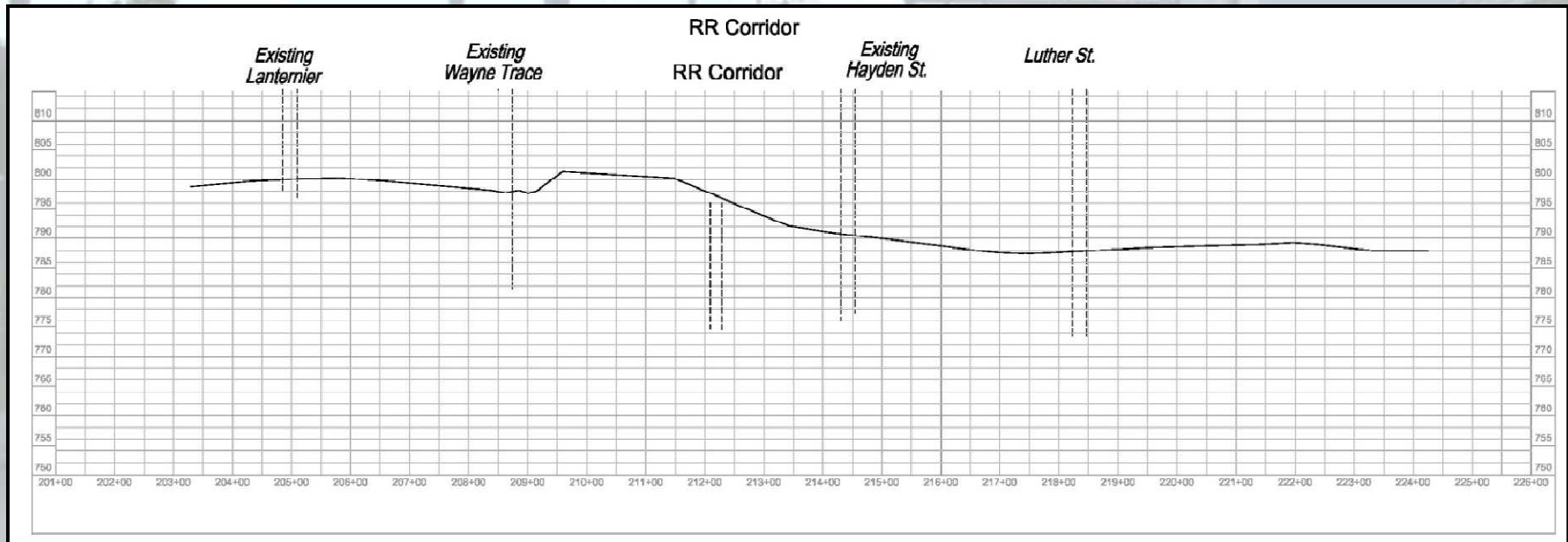


## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
- Anthony Boulevard traffic volume and patterns
  - Traffic study to identify number of lanes required

## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
- Anthony Boulevard traffic volume and patterns
- Topography through the corridor



## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
- Anthony Boulevard traffic volume and patterns
- Topography through the corridor
- Utilities and potential realignments
  - I&M
  - NIPSCO
  - Comcast
  - Frontier
  - Municipal Utilities



## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
- Anthony Boulevard traffic volume and patterns
- Topography through the corridor
- Utilities and potential realignments
- Roadway alignment options
  - Multiple options evaluated
    - Shift to east
    - Maintain in the existing roadway
    - Shift to west

## What We Have Studied

- Defining the impacted area
- Property ownership and parcel boundaries
- Impacts to side streets and accesses
- Community impacts—connectivity, area quality and placemaking
- Norfolk-Southern policies and train traffic volume
- Anthony Boulevard traffic volume and patterns
- Topography through the corridor
- Utilities and potential realignments
- Roadway alignment options
- Underpass and overpass impacts to nearby businesses and accesses
- Underpass and overpass impacts to nearby homes and neighborhoods
  - Shift to minimize impacts
  - Maintain traffic through corridor and on Wayne Trace
  - Maintain or re-establish access



## What We Have Studied

- Pedestrian and bicycle access



## What We Have Studied

- Pedestrian and bicycle access
- Safety issues
- Historic preservation issues
- Stormwater and drainage needs
  - 72" sewer to facilitate draining underpass

## What We Have Studied

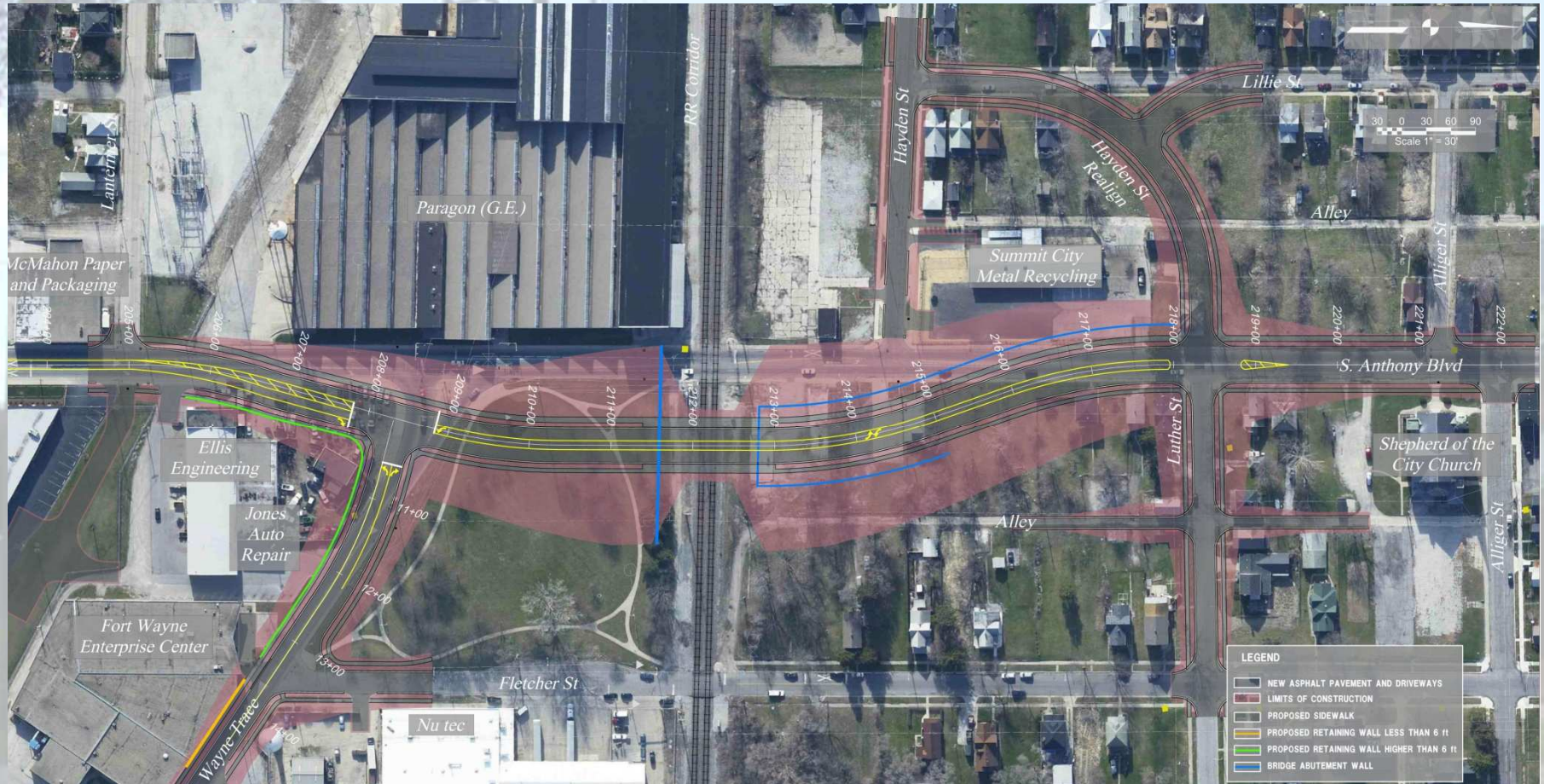
- Pedestrian and bicycle access
- Safety issues
- Historic preservation issues
- Stormwater and drainage needs
- Past, present, and potential future land uses
- Environmental issues
- Impacts of the railroad underpass to the south, near Creighton Avenue



## Options Evaluated

- Everything was on the table
- Narrowed down to 3 options
- Three Options Evaluated
  - Overpass with “T” intersection at Wayne Trace
  - Underpass with roundabout at Wayne Trace
  - Underpass with “T” intersection at Wayne Trace

# Overpass with "T" Intersection at Wayne Trace

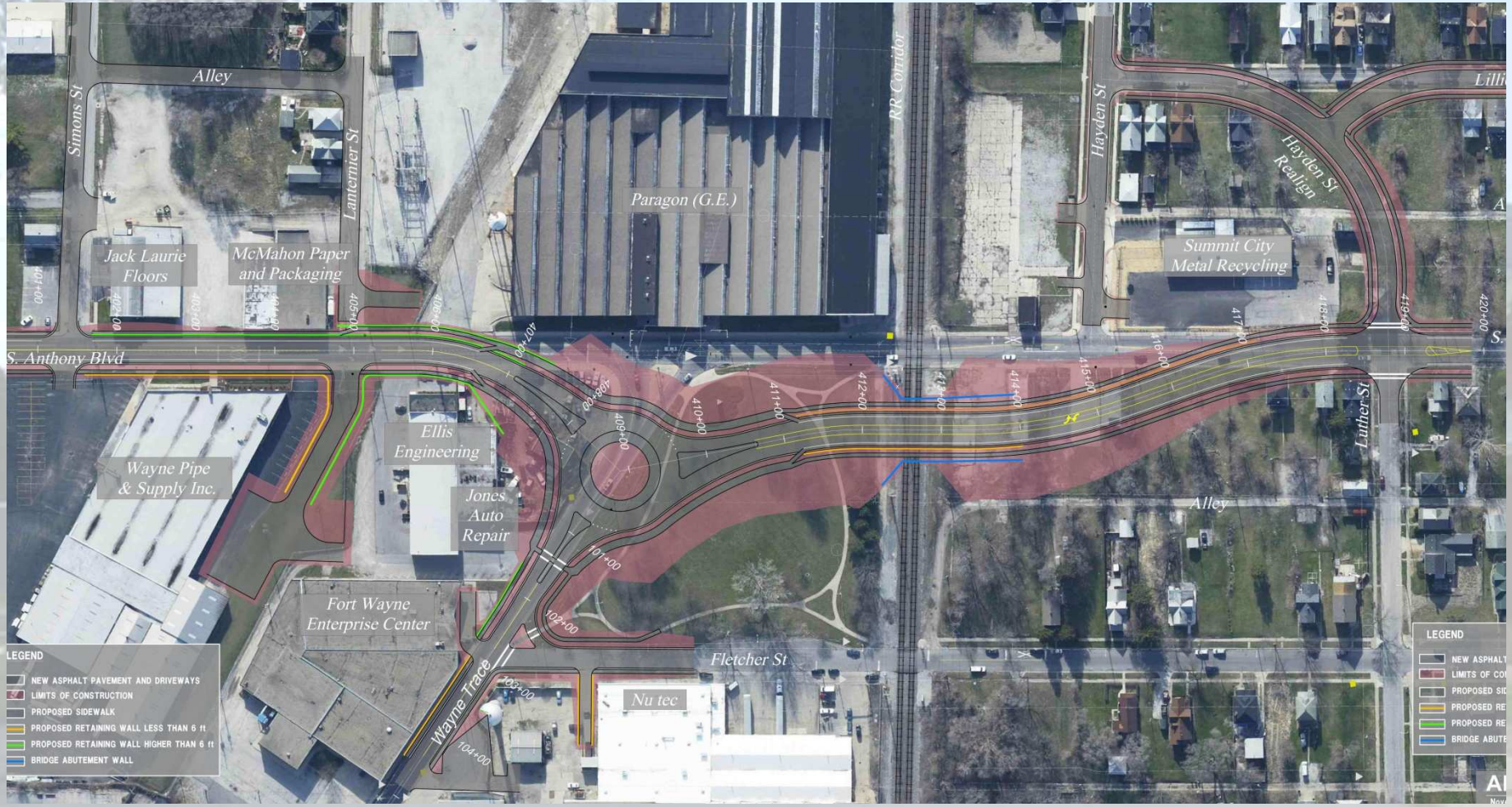


# Overpass with “T” Intersection at Wayne Trace

- **Attributes**
  - Meets project goals
  - Least impact to business access along South Anthony Boulevard
  - Least involvement by the railroad
- **Drawbacks**
  - Largest footprint
  - Impacts the largest number of parcels
  - Requires large retaining walls
  - Extends project north to Alliger Street
  - Greater impact to residential area to the north



# Underpass with Roundabout at Wayne Trace

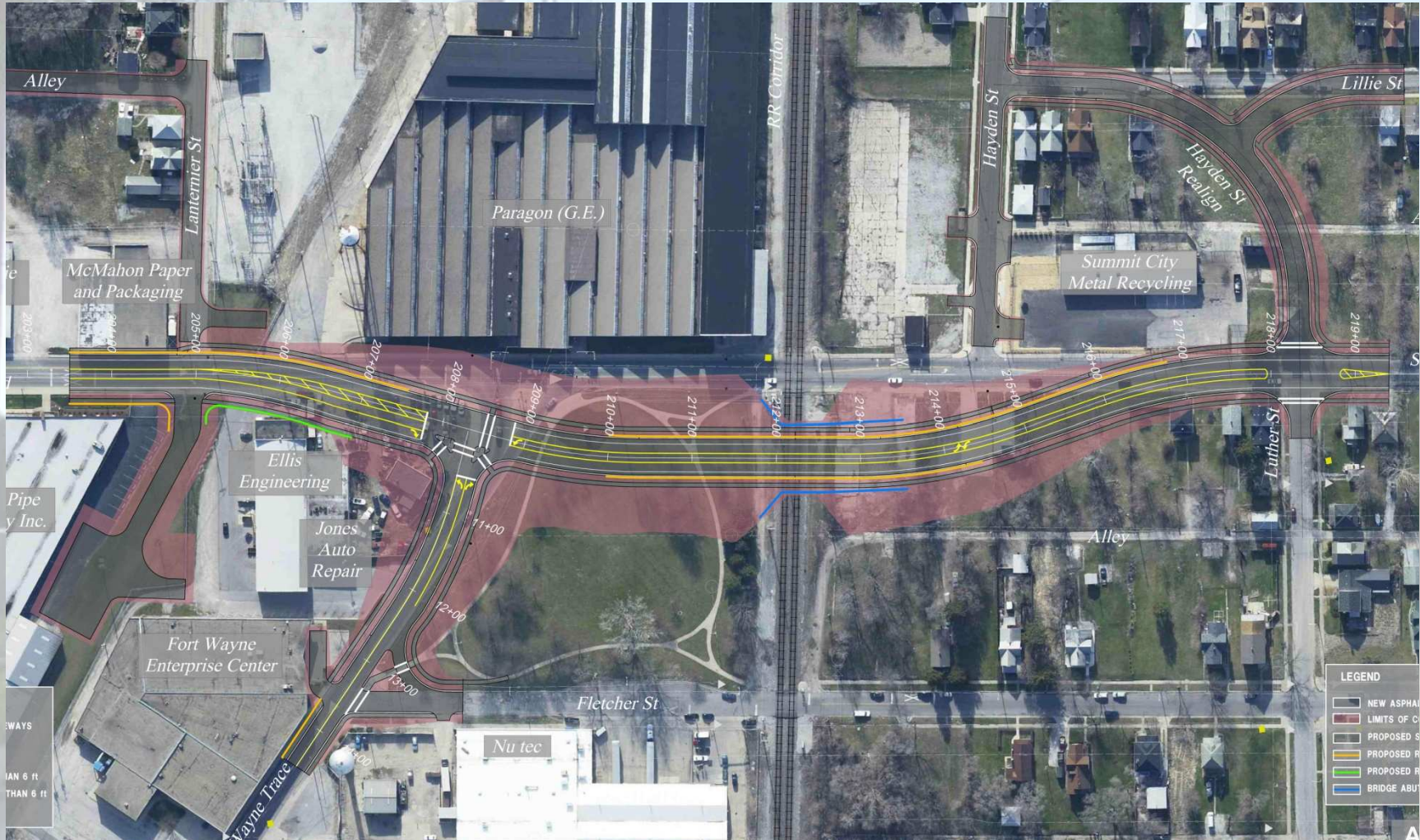


# Underpass with Roundabout at Wayne Trace

- **Attributes**
  - Meets project goals
  - Provides opportunities for enhancements
  - Eliminates signalized intersection
  - Minimizes impacts to residential area to the north
- **Drawbacks**
  - Most disruptive to business access
  - Impacts a large number of parcels
  - Requires additional retaining walls
  - Extends project south of Simons Street
  - Extensive coordination with railroad



# Underpass with "T" Intersection at Wayne Trace



# Underpass with “T” Intersection at Wayne Trace

- **Attributes**
  - Meets project goals
  - Provides opportunities for enhancements
  - Impacts least number of parcels
  - Smallest project footprint
  - Shortest project length
- **Drawbacks**
  - Somewhat disruptive to business access
  - Requires additional smaller scale retaining walls
  - Extensive coordination with railroad



## Recommended Option - Underpass with “T” Intersection at Wayne Trace

- Opportunities to enhance user experiences
  - Separation between pedestrian and vehicular facilities
  - Landscaping opportunities
  - Aesthetic treatments to proposed walls
  - Opportunities for placemaking







**Possible View North on Anthony  
Toward Overpass**



**Existing Condition**





**Possible View South on Anthony  
Toward Overpass**



**Existing Condition**





**Rail Bridge Design Opportunity**



# Where Do We Go From Here?

- Review and incorporate feedback from this public meeting
- Prepare the final feasibility study report
- Make final feasibility study report available to the public
- Identify and apply for funding from all potential sources

