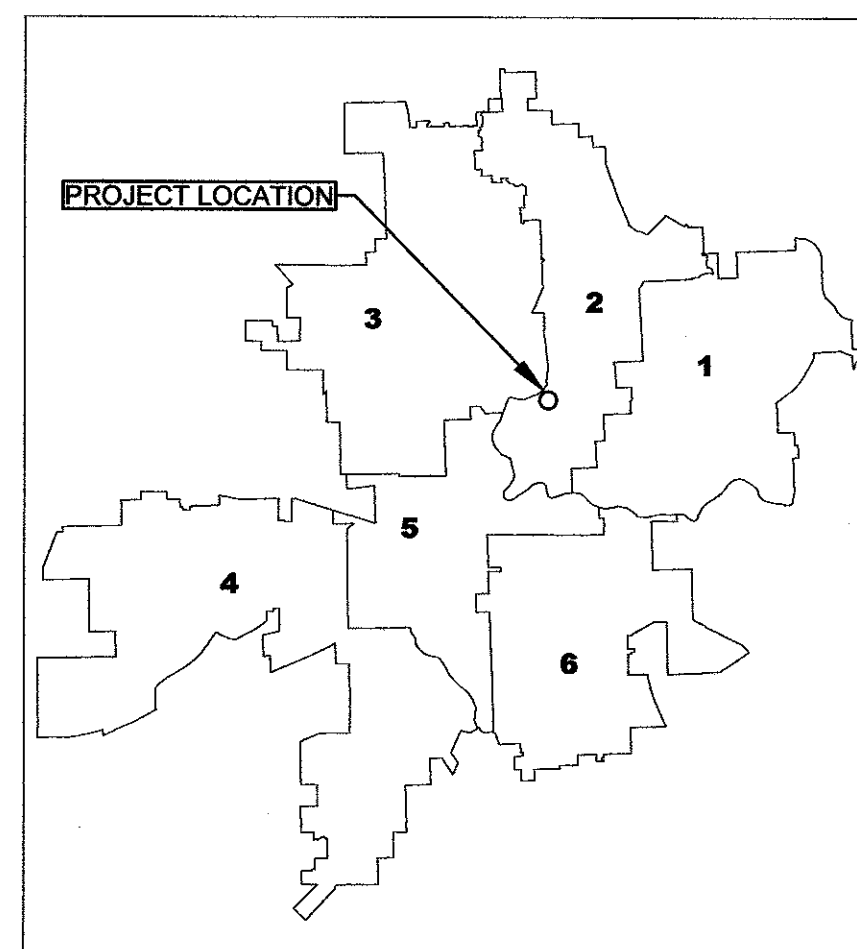
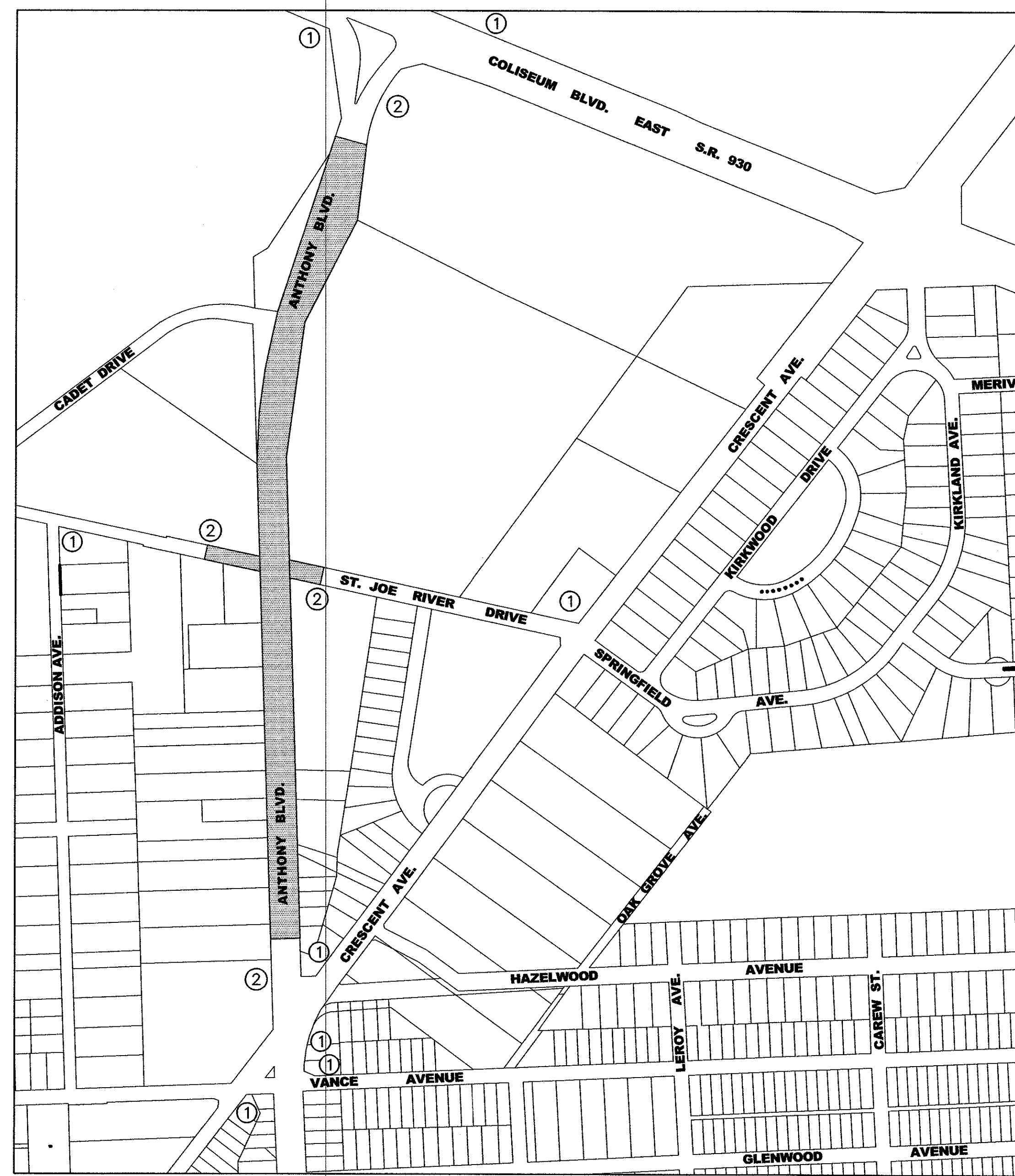
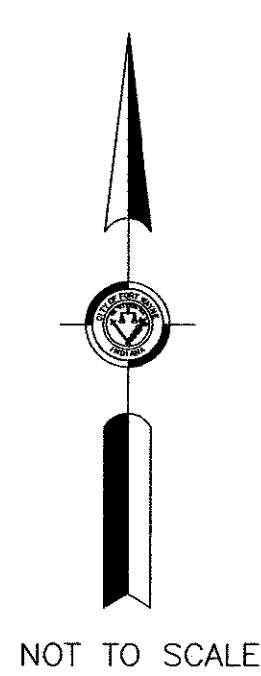


NORTH ANTHONY BLVD. REHABILITATION CRESCENT AVE. TO COLISEUM BLVD.



CITY OF FORT WAYNE
COUNCIL DISTRICTS



LOCATION MAP

- ① ROAD CONSTRUCTION AHEAD 8 EA.
- ② END CONSTRUCTION 4 EA.

MAYOR
HONORABLE THOMAS C. HENRY
THE BOARD OF PUBLIC WORKS
 ROBERT P. KENNEDY
 CHAIR
 MIKE AVILA
 MEMBER
 KUMAR MENON
 MEMBER
COMMON COUNCIL REPRESENTATIVE
 RUSS JEHL
 DISTRICT 2

DRAFTING SCHEDULE			
NO.	DATE	DESCRIPTION	BY
1	08/10/16	PRELIMINARY	RLR
2	03/15/17	DESIGN	RLR
3	04/27/17	FINAL PLANS	RLR
4			
5			
6			
7			
8			

PLANS CERTIFIED BY:

 SHAN GUNAWARDENA, P.E., D.T.O.E.
 CITY ENGINEER
 This 7th Day of November 2017



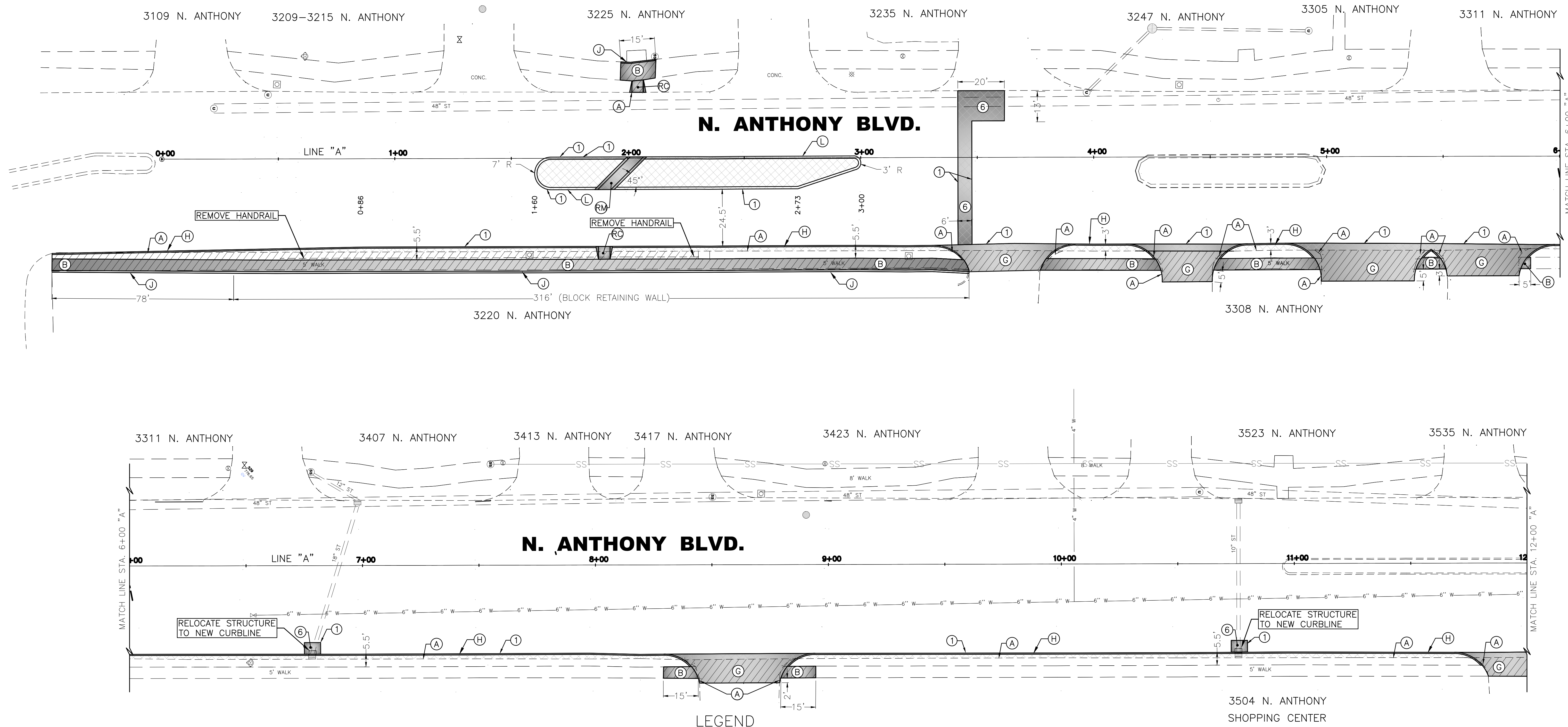
TRANSPORTATION ENGINEERING SERVICES				CITIZENS SQUARE, 200 EAST BERRY STREET 46802			
- REVISIONS -							
NO.	DATE	DESCRIPTION	BY				

**NORTH ANTHONY BOULEVARD
 CRESENT AVE. TO COLISEUM BLVD.
 REHABILITATION**

Designed by: M. GRAY RES. W.O. S.T.
 Drawn by: R. REYNOLDS
 Checked by: M. TRIVINO 0143C Sheet 1 of 25



SCALE 1"=20'



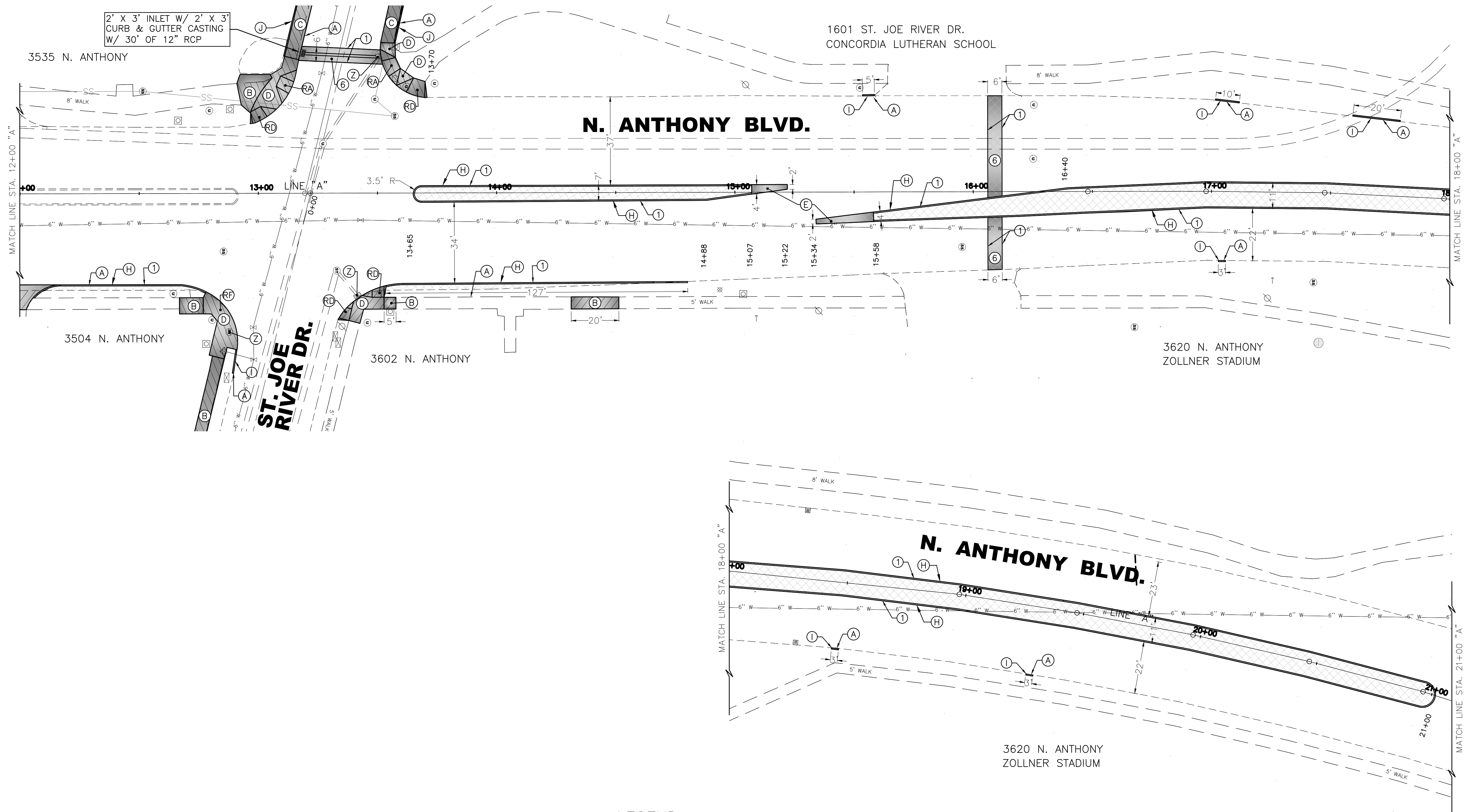
LEGEND

- NEW CONSTRUCTION
- REMOVAL OF CONC. (INCLUDES MEDIAN)
- REMOVAL OF CONCRETE PAVEMENT
- (A) CURB REMOVAL
- (B) CONCRETE SIDEWALK 4"
- (C) CONCRETE CURB FACE WALK
- (D) CONCRETE WINGWALK & CURB RAMPS
- (E) CONCRETE CENTER CURB, TYPE "C"
- (G) CONCRETE FOR COMMERCIAL DRIVES 8"
- (H) CONCRETE CURB, TYPE III
- (I) CONCRETE CURB, TYPE II-A
- (J) CURB ON BACK OF WALK 6"
- (L) CONCRETE CURB, TYPE III (MODIFIED)
- (P) 2' X 3' CURB & GUTTER CASTING
- (RA) CURB RAMP TYPE "A"
- (RF) CURB RAMP TYPE "F"
- (RC) CURB RAMP TYPE "C"
- (RD) CURB RAMP TYPE "D"
- (RM) CURB RAMP TYPE "M"
- (Z) ADJUST CASTING TO GRADE
- (1) FULL DEPTH SAWING
- (6) HMA FOR PATCHING
8.5" 25 mm BASE, TYPE C OVER
6" COMPACTED AGGREGATE BASE, No. 53

NOTE:
THE LOCATIONS OF UNDERGROUND UTILITIES AS SHOWN ARE APPROXIMATE. THEIR LOCATIONS WERE DETERMINED FROM INFORMATION PROVIDED BY THE SURVEY OR AVAILABLE MAPPING. ALL LOCATIONS SHALL BE FIELD VERIFIED BY CONTRACTOR BEFORE EXCAVATING.

NOTE:
ALL RIGHT-OF-WAY & PROPERTY LINES DEPICTED ON DRAWINGS ARE APPARENT RIGHT-OF-WAY & PROPERTY LINES.

NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. CONCRETE CONSTRUCTION DETAILS		
Designed by: M. Gray	RES. W.O.	S.T.
Drawn by: R. Reynolds	0143C	
Checked by: M. TREVINO	Sheet 2 of 25	



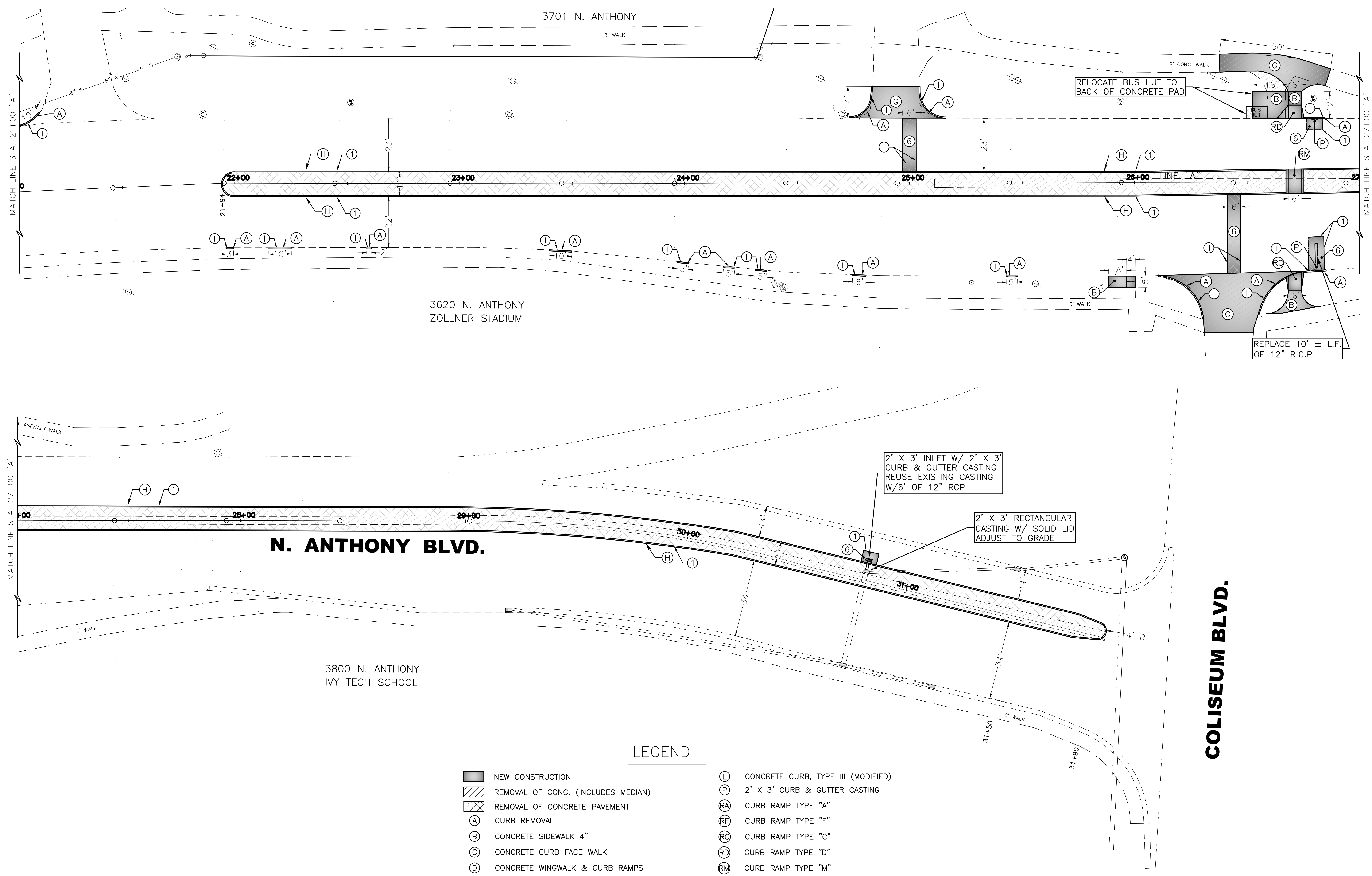
LEGEND

- NEW CONSTRUCTION
- REMOVAL OF CONC. (INCLUDES MEDIAN)
- REMOVAL OF CONCRETE PAVEMENT
- (A) CURB REMOVAL
- (B) CONCRETE SIDEWALK 4"
- (C) CONCRETE CURB FACE WALK
- (D) CONCRETE WINGWALK & CURB RAMPS
- (E) CONCRETE CENTER CURB, TYPE "C"
- (G) CONCRETE FOR COMMERCIAL DRIVES 8"
- (H) CONCRETE CURB, TYPE III
- (I) CONCRETE CURB, TYPE II-A
- (J) CURB ON BACK OF WALK 6"
- (L) CONCRETE CURB, TYPE III (MODIFIED)
- (P) 2' X 3' CURB & GUTTER CASTING
- (RA) CURB RAMP TYPE "A"
- (RF) CURB RAMP TYPE "F"
- (RC) CURB RAMP TYPE "C"
- (RD) CURB RAMP TYPE "D"
- (RM) CURB RAMP TYPE "M"
- (Z) ADJUST CASTING TO GRADE
- (1) FULL DEPTH SAWING
- (6) HMA FOR PATCHING
8.5" 25 mm BASE, TYPE C OVER
6" COMPACTED AGGREGATE BASE, No. 53

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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. CONCRETE CONSTRUCTION DETAILS		
Designed by: M. Gray	RES. W.O.	S.T.
Drawn by: R. Reynolds	0143C	
Checked by: M. TREVINO	Sheet 3 of 25	



N. ANTHONY BLVD.

COLISEUM BLVD.

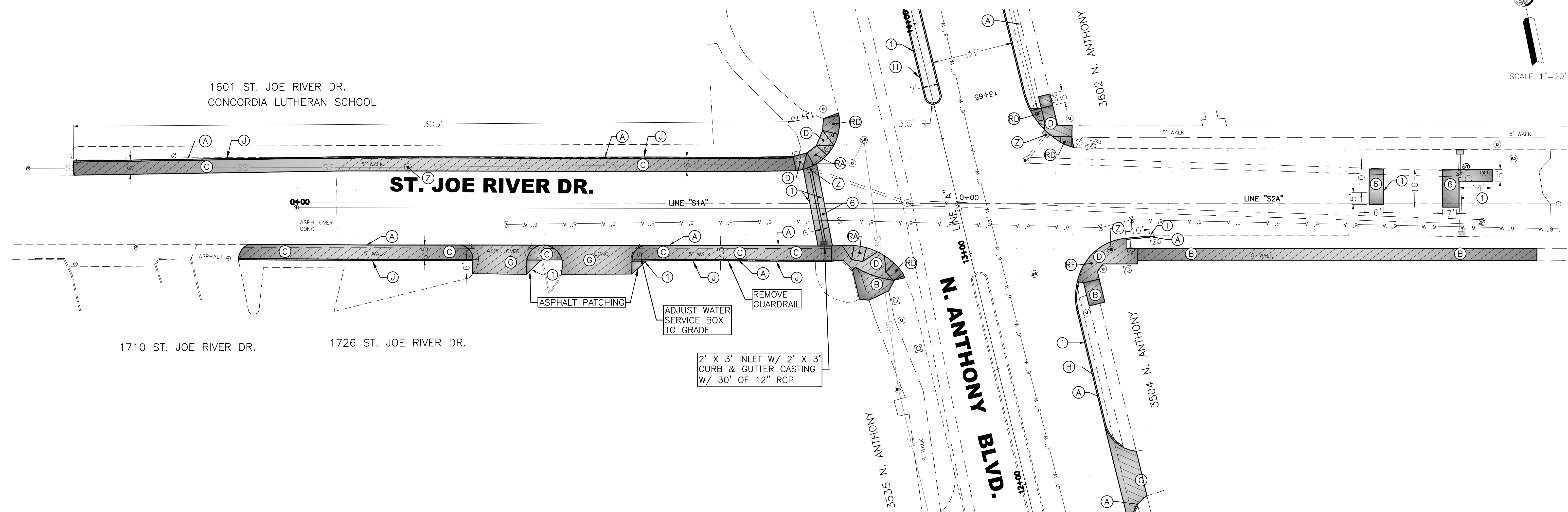
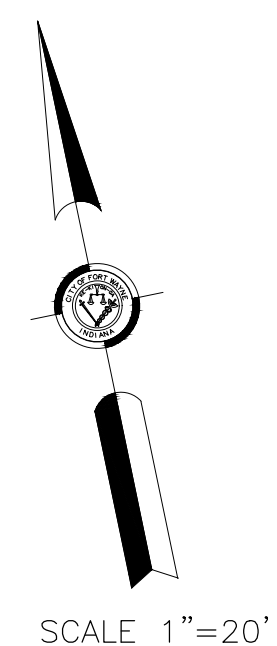
LEGEND

- | | |
|--------------------------------------|--------------------------------------|
| ■ NEW CONSTRUCTION | Ⓛ CONCRETE CURB, TYPE III (MODIFIED) |
| ▨ REMOVAL OF CONC. (INCLUDES MEDIAN) | Ⓟ 2' X 3' CURB & GUTTER CASTING |
| ▩ REMOVAL OF CONCRETE PAVEMENT | ⓇA CURB RAMP TYPE "A" |
| Ⓐ CURB REMOVAL | ⓇF CURB RAMP TYPE "F" |
| Ⓑ CONCRETE SIDEWALK 4" | ⓇC CURB RAMP TYPE "C" |
| Ⓒ CONCRETE CURB FACE WALK | ⓇD CURB RAMP TYPE "D" |
| Ⓓ CONCRETE WINGWALK & CURB RAMPS | ⓇM CURB RAMP TYPE "M" |
| Ⓔ CONCRETE CENTER CURB, TYPE "C" | Ⓢ ADJUST CASTING TO GRADE |
| Ⓞ CONCRETE FOR COMMERCIAL DRIVES 8" | Ⓣ FULL DEPTH SAWING |
| Ⓡ CONCRETE CURB, TYPE III | Ⓤ HMA FOR PATCHING |
| Ⓢ CONCRETE CURB, TYPE II-A | 8.5" 25 mm BASE, TYPE C OVER |
| Ⓣ CURB ON BACK OF WALK 6" | 6" COMPACTED AGGREGATE BASE, No. 53 |

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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. CONCRETE CONSTRUCTION DETAILS	
Designed by: M. Gray	RES. W.O.
Drawn by: R. Reynolds	0143C
Checked by: M. TREVINO	Sheet 4 of 25



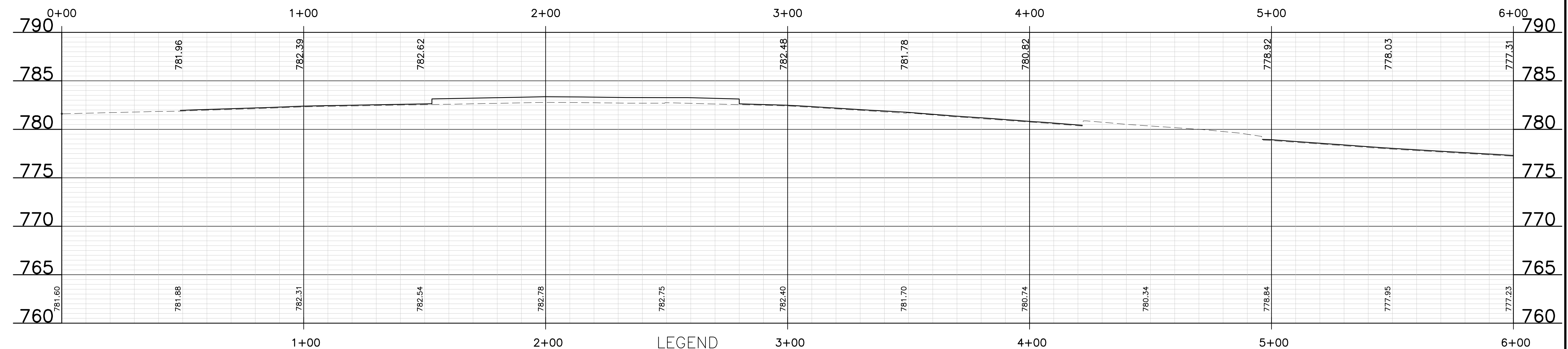
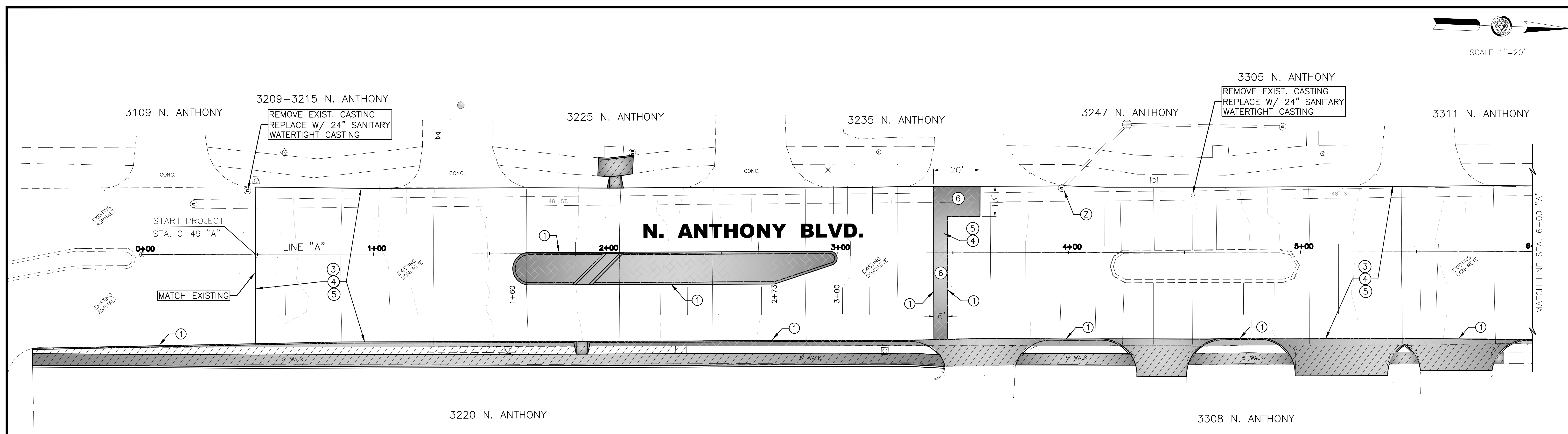
LEGEND

- | | |
|--|--|
| <ul style="list-style-type: none"> NEW CONSTRUCTION REMOVAL OF CONC. (INCLUDES MEDIAN) REMOVAL OF CONCRETE PAVEMENT CURB REMOVAL CONCRETE SIDEWALK 4" CONCRETE CURB FACE WALK CONCRETE WINGWALK & CURB RAMPS CONCRETE CENTER CURB, TYPE "C" CONCRETE FOR COMMERCIAL DRIVES 8" CONCRETE CURB, TYPE III CONCRETE CURB, TYPE II-A CURB ON BACK OF WALK 6" | <ul style="list-style-type: none"> CONCRETE CURB, TYPE III (MODIFIED) 2' X 3' CURB & GUTTER CASTING CURB RAMP TYPE "A" CURB RAMP TYPE "F" CURB RAMP TYPE "C" CURB RAMP TYPE "D" CURB RAMP TYPE "M" ADJUST CASTING TO GRADE FULL DEPTH SAWING HMA FOR PATCHING
8.5" 25 mm BASE, TYPE C OVER
6" COMPACTED AGGREGATE BASE, No. 53 |
|--|--|

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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. CONCRETE CONSTRUCTION DETAILS		
Designed by: M. Gray	W.O.	S.T.
Drawn by: R. Reynolds	0143C	
Checked by: M. TREVINO	Sheet 5 of 25	



LEGEND

- ① FULL DEPTH SAWING
- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
- ⑤ FIBERMAT TYPE B
- ⑥ HMA FOR PATCHING
8.5", 25 mm BASE, TYPE C OVER
6" COMPACTED AGGREGATE FOR BASE, NO. 53
- ⑦ 2.5" PCCP/ASPHALT MILLING
- ⑧ ADJUST CASTING TO GRADE

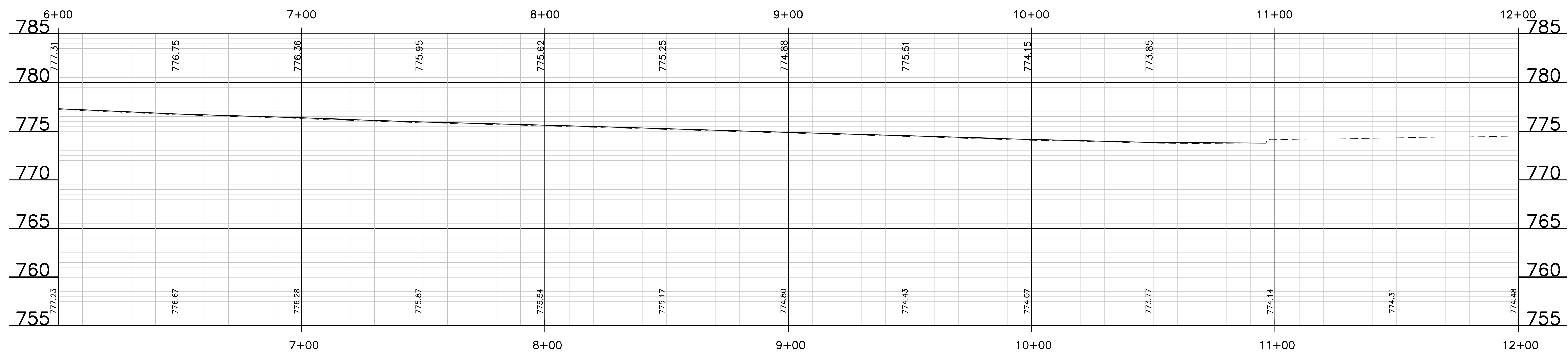
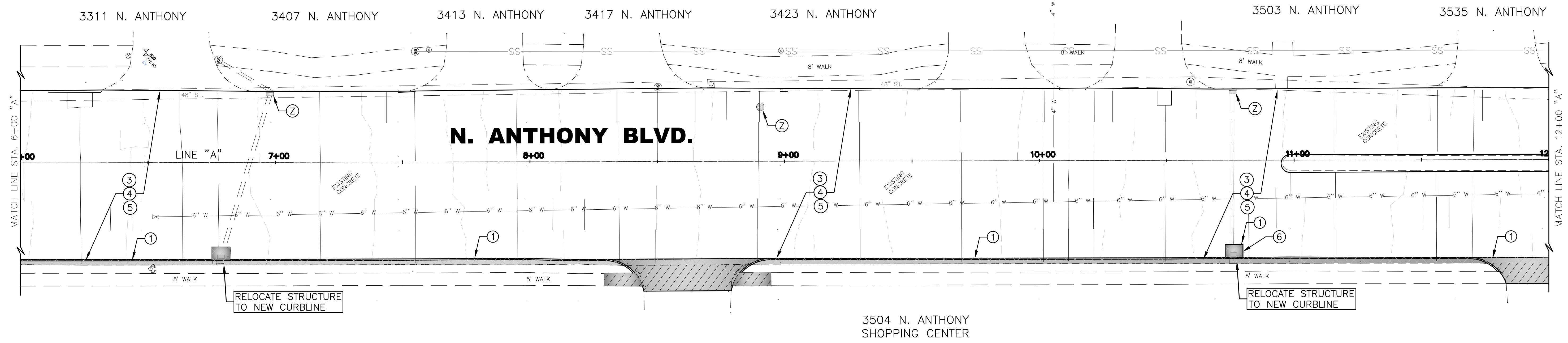
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN		
Designed by: M. Gray	RES. W.O. S.T.	
Drawn by: R. Reynolds	0143C	Sheet 6 of 25
Checked by: M. TREVINO		



SCALE 1"=20'



LEGEND

- ① FULL DEPTH SAWING
- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
- ⑤ FIBERMAT TYPE B
- ⑥ HMA FOR PATCHING
8.5", 25 mm BASE, TYPE C OVER
6" COMPACTED AGGREGATE FOR BASE, NO. 53
- ⑦ 2.5" PCCP/ASPHALT MILLING
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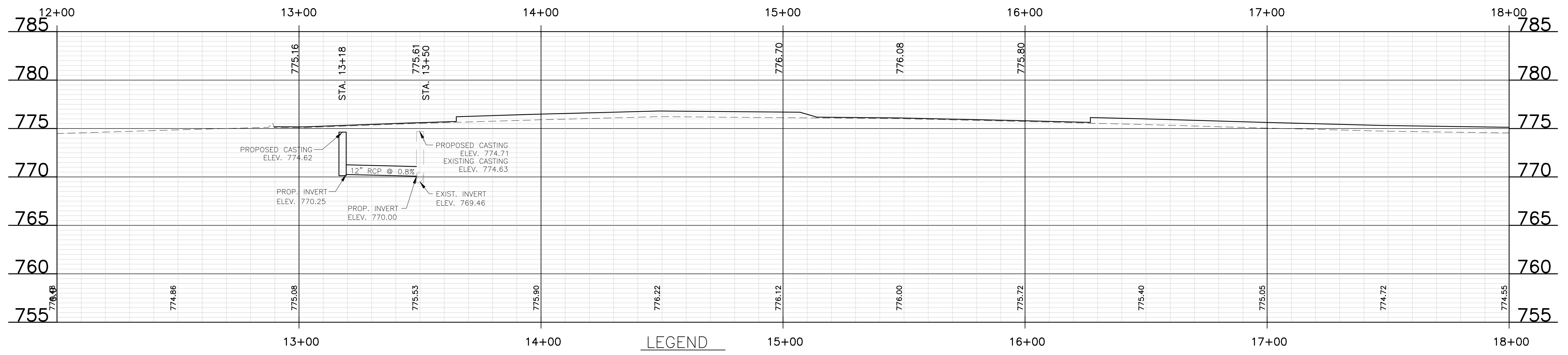
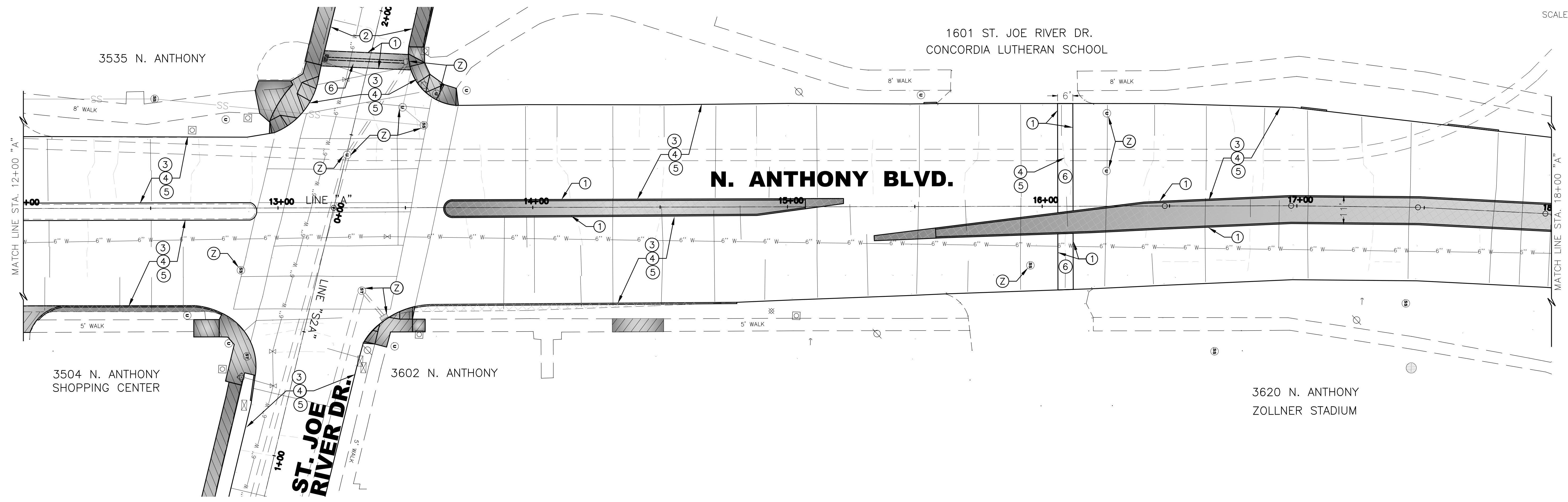
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds	0143C	S.T.
Checked by: M. TREVINO		

Sheet 7 of 25



SCALE 1"=20'



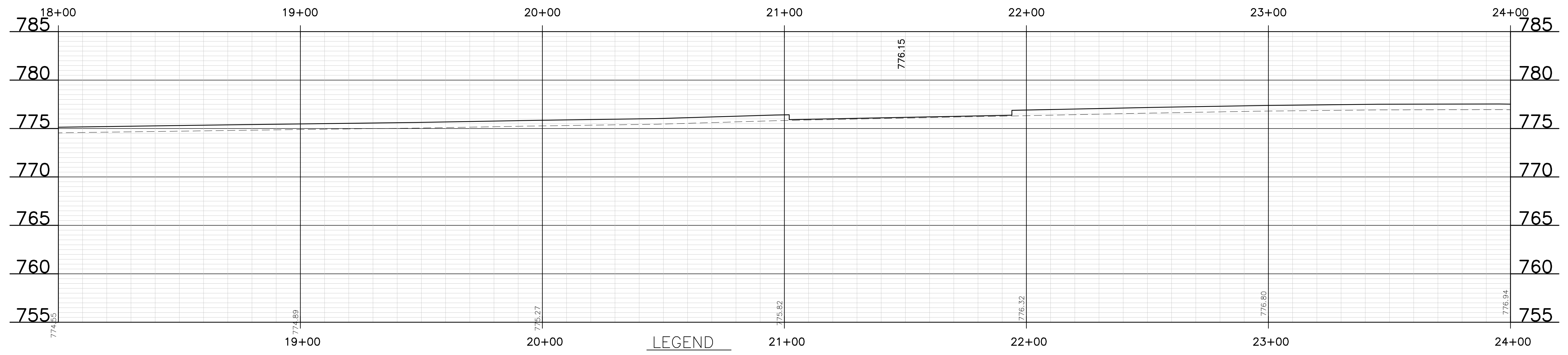
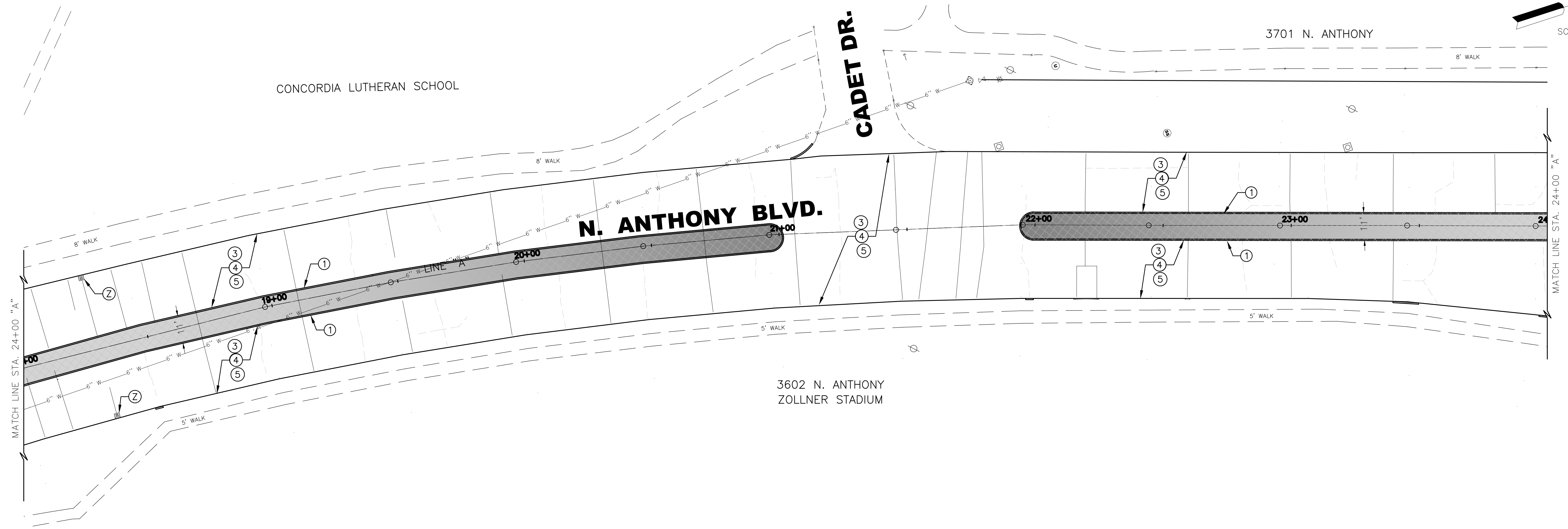
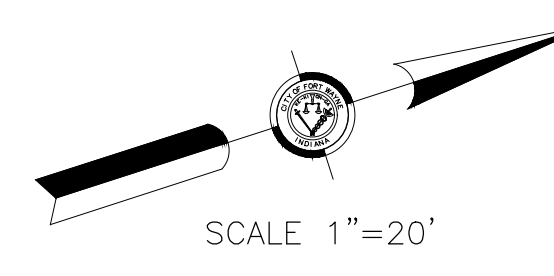
LEGEND

- ① FULL DEPTH SAWING
- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
- ⑤ FIBERMAT TYPE B
- ⑥ HMA FOR PATCHING
8.5", 25 mm BASE, TYPE C OVER
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN		
Designed by: M. Gray	RES. W.O.	S.T.
Drawn by: R. Reynolds	0143C	
Checked by: M. Trevino	Sheet 8 of 25	

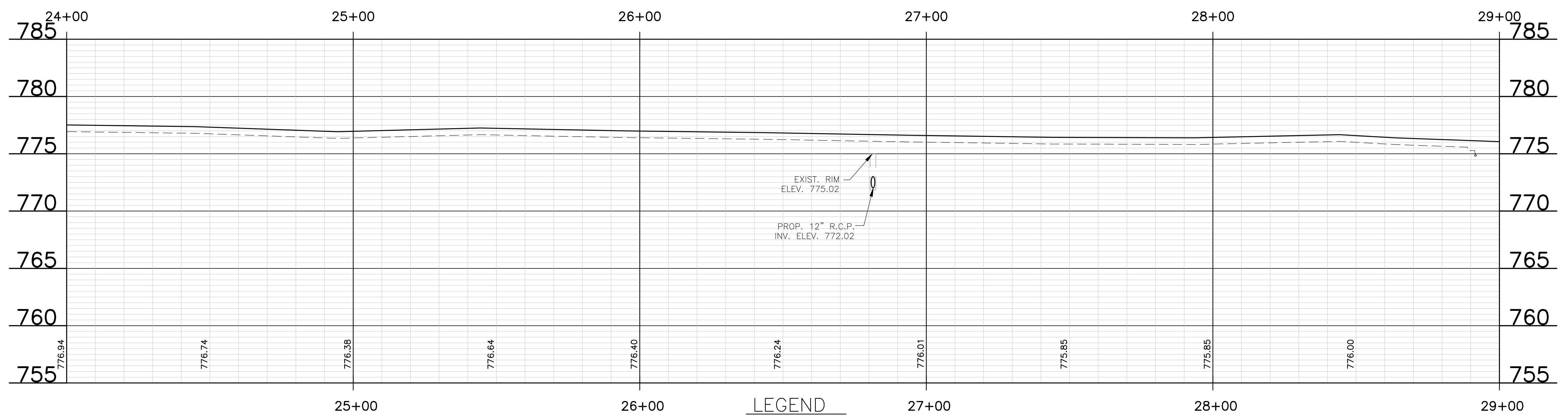
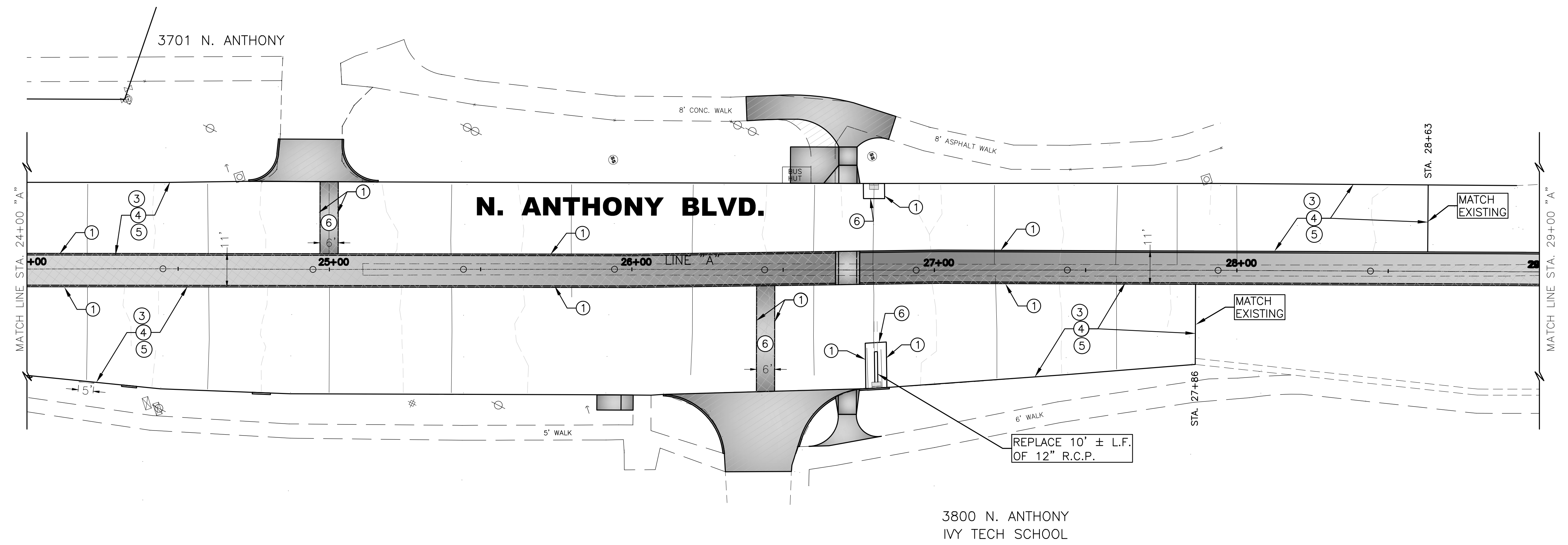
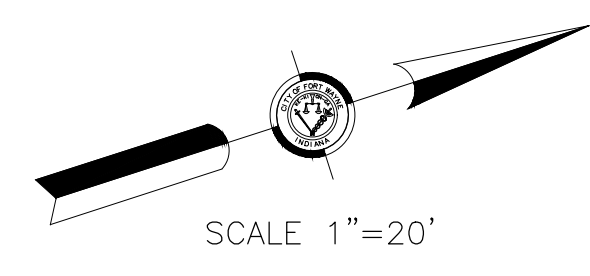


- LEGEND**
- ① FULL DEPTH SAWING
 - ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
 - ③ 2.5" PCCP MILLING
 - ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
 - ⑤ FIBERMAT TYPE B
 - ⑥ HMA FOR PATCHING
8.5", 25 mm BASE, TYPE C OVER
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds	S.T.	0143C
Checked by: M. TREVINO	Sheet <u>9</u> of <u>25</u>	



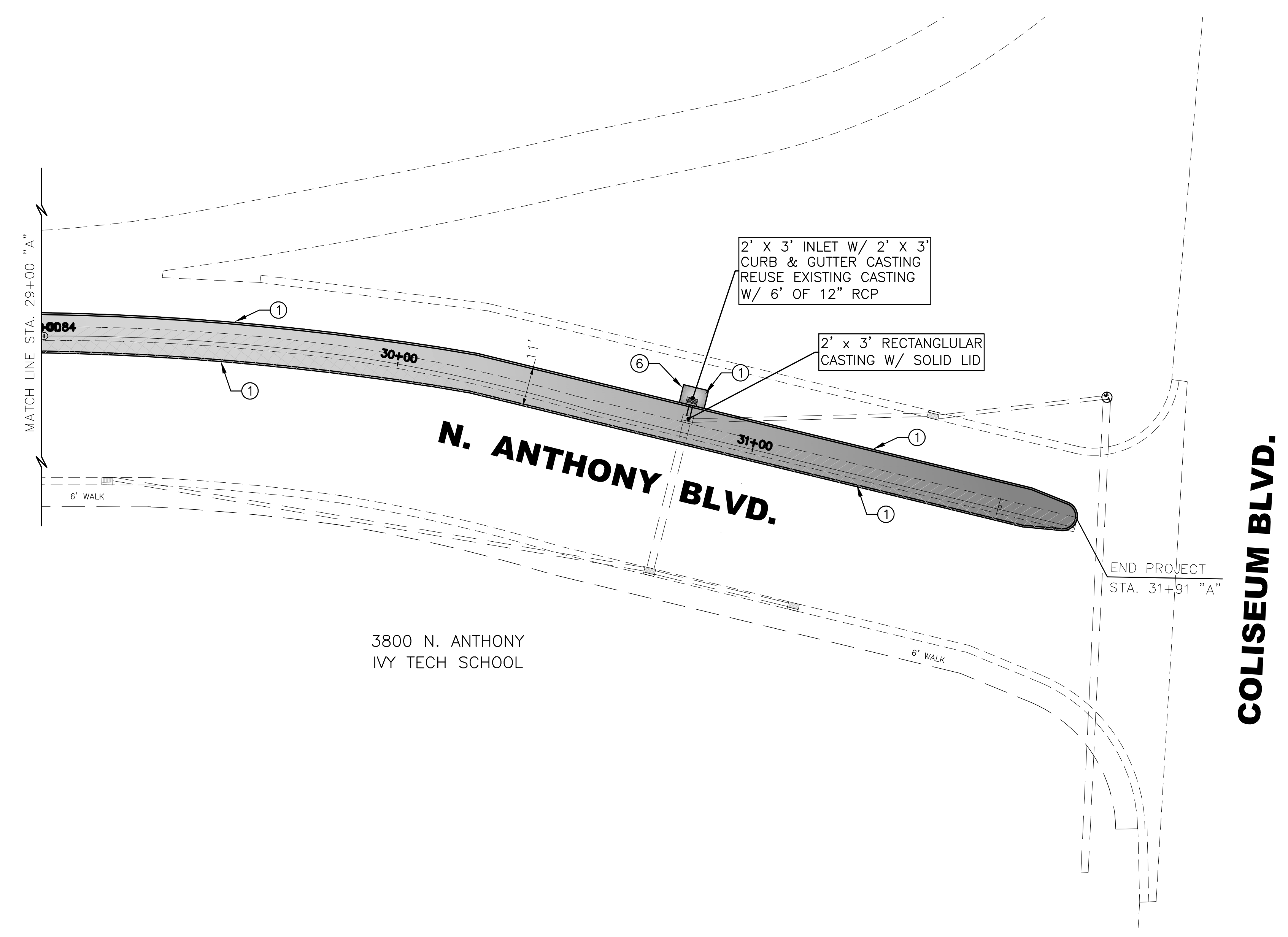
LEGEND

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- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
- ⑤ FIBERMAT TYPE B
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8.5", 25 mm BASE, TYPE C OVER
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN		
Designed by: M. Gray	RES. W.O. S.T.	
Drawn by: R. Reynolds	0143C	Sheet 10 of 25
Checked by: M. TREVINO		



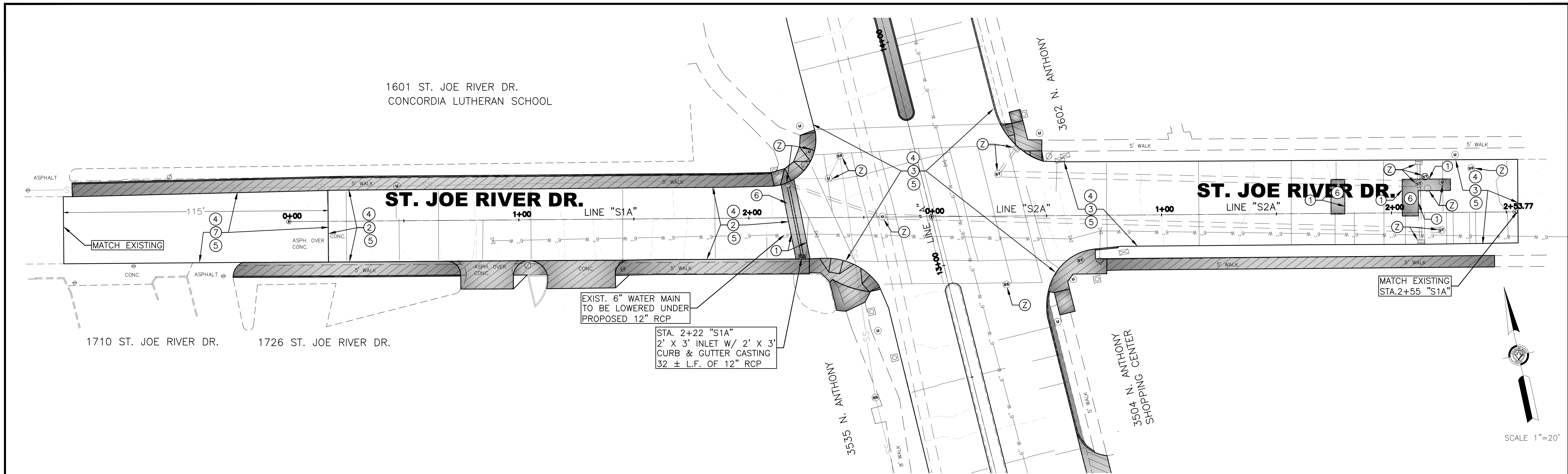
LEGEND

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- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
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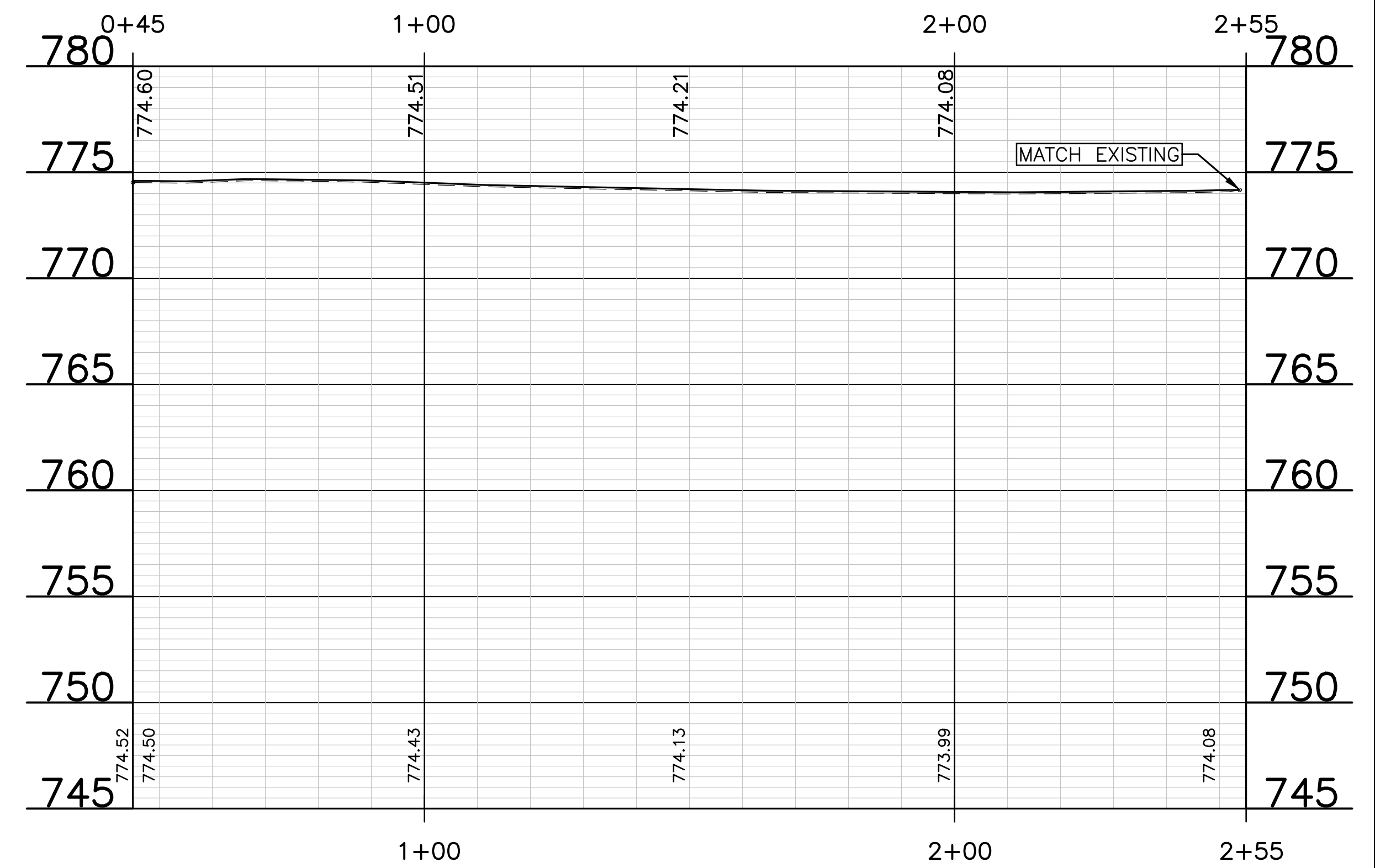
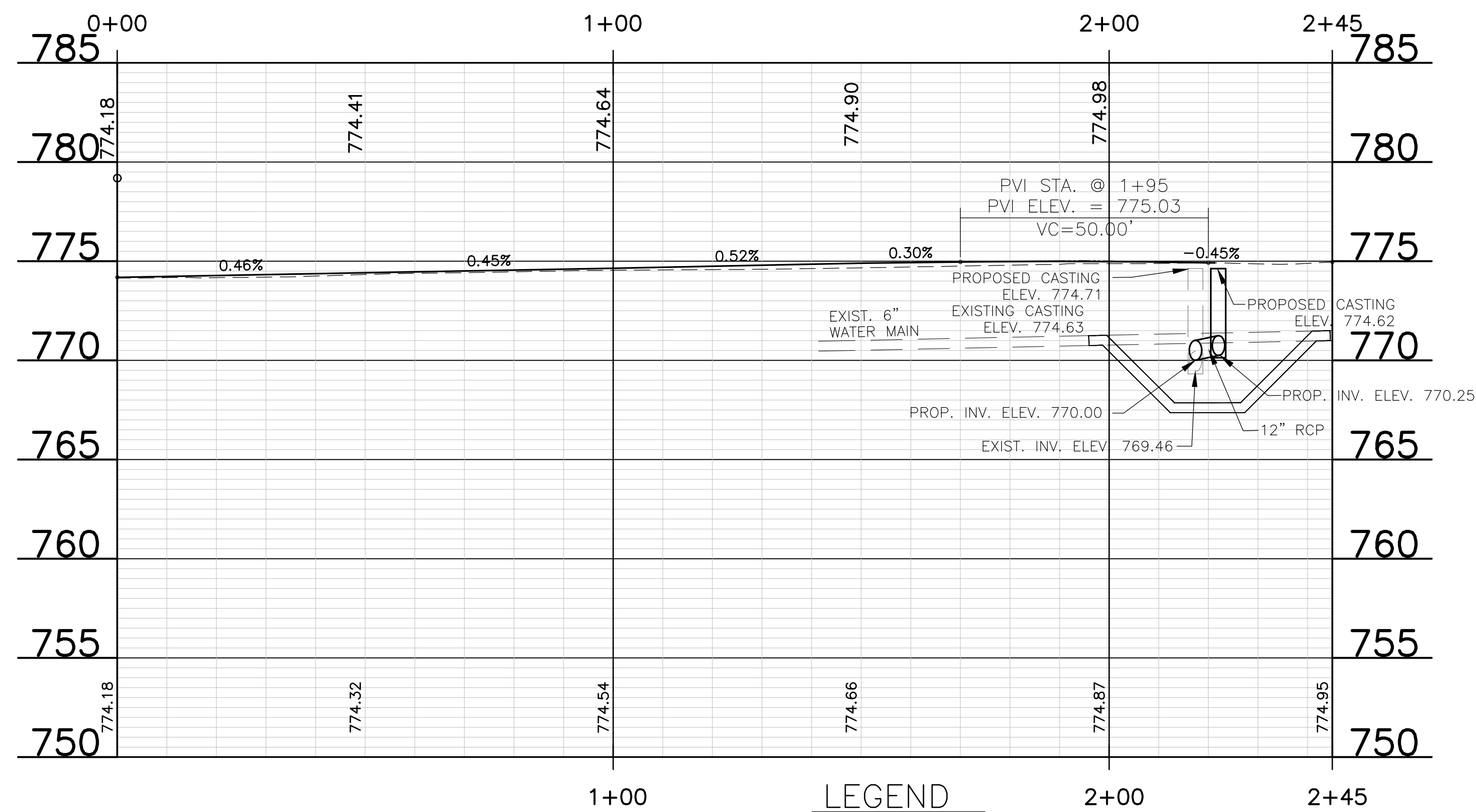
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. MILLING & ASPHALT PLAN			
Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		Sheet 11 of 25
Checked by: M. TREVINO			



SCALE 1"=20'



- LEGEND**
- ① FULL DEPTH SAWING
 - ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
 - ③ 2.5" PCCP MILLING
 - ④ 1.5", 9.5 mm SURFACE, TYPE C OVER 2", 19 mm INTERMEDIATE, TYPE C
 - ⑤ FIBERMAT TYPE B
 - ⑥ HMA FOR PATCHING 8.5", 25 mm BASE, TYPE C OVER 6" COMPACTED AGGREGATE FOR BASE, NO. 53
 - ⑦ 2.5" PCCP/ASPHALT MILLING
 - ⑧ ADJUST CASTING TO GRADE

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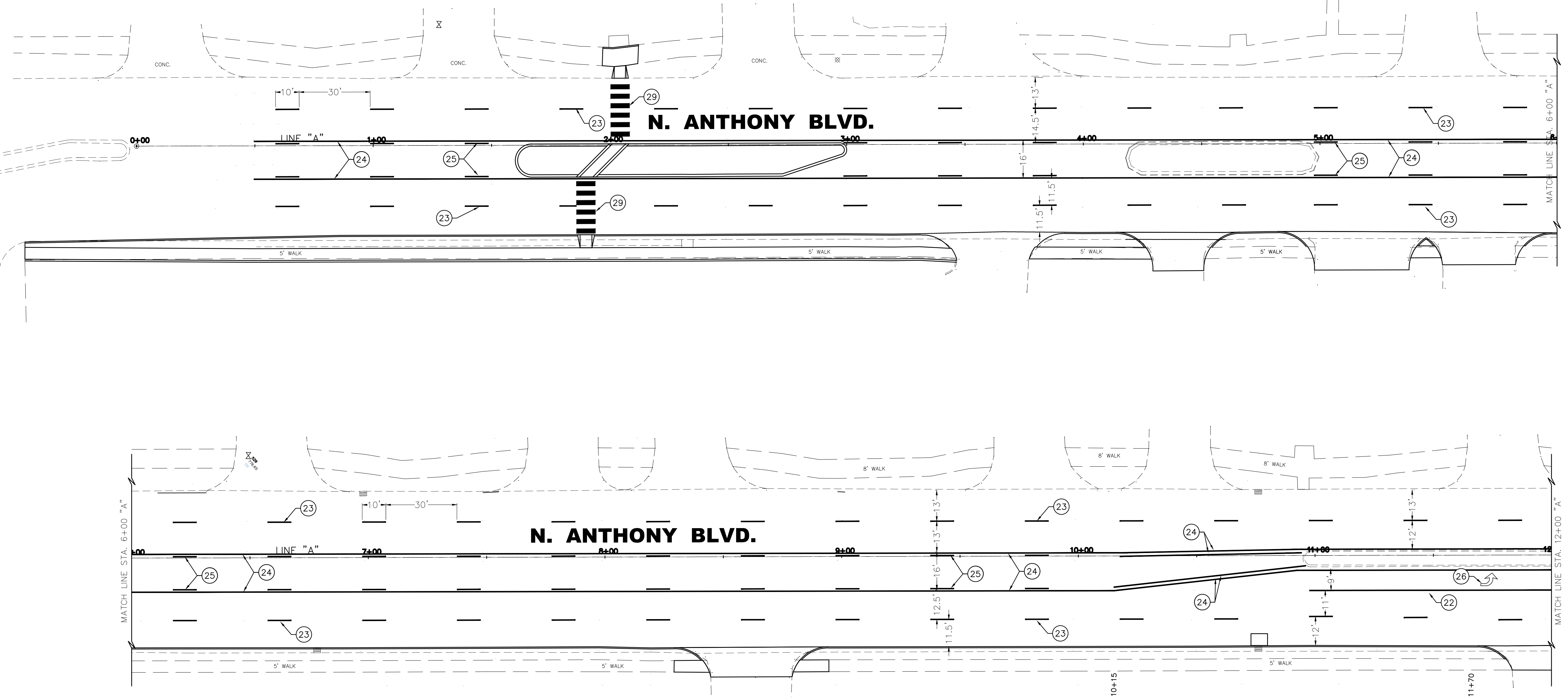
NOTE:
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**NORTH ANTHONY BOULEVARD
CRESENT AVE. TO COLISEUM BLVD.
MILLING & ASPHALT PLAN**

Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		
Checked by: M. Trevino	Sheet 12 of 25		



SCALE 1"=20'



LEGEND

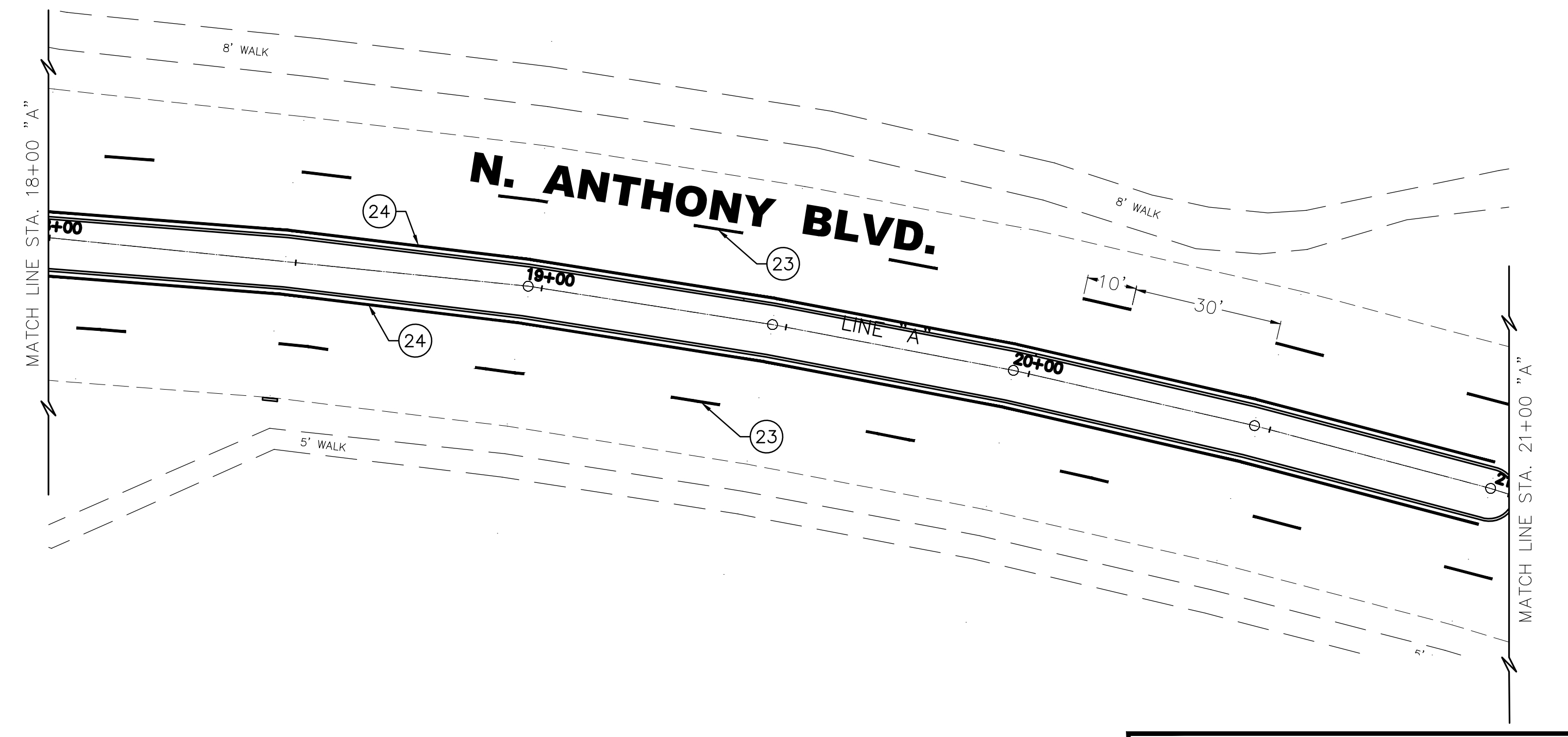
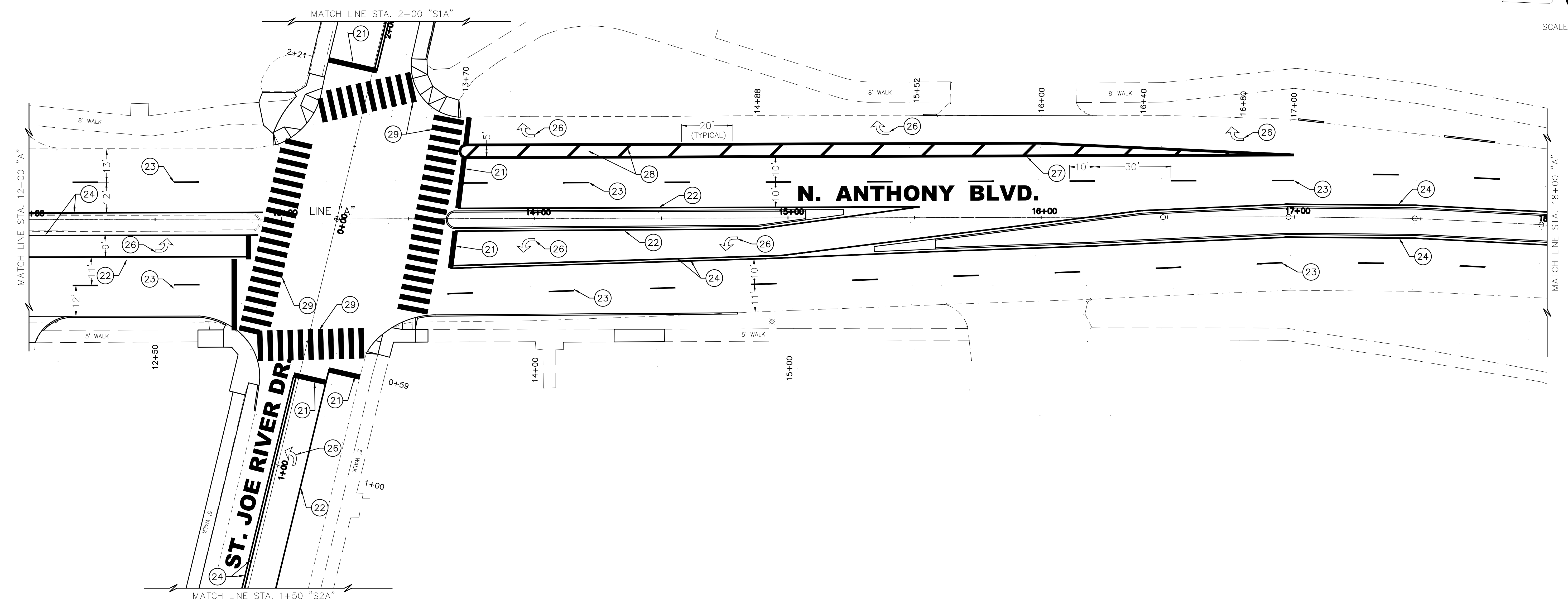
- ①⑨ GRIND EXISTING STRIPING
- ②① TRANSVERSE MARKING, THERMOPLASTIC, STOP BAR, 24"
- ②② LINE, EPOXY PAINT, SOLID WHITE, 4"
- ②③ LINE, EPOXY PAINT, BROKEN WHITE, 4"
- ②④ LINE, EPOXY PAINT, SOLID YELLOW, 4"
- ②⑤ LINE, EPOXY PAINT, BROKEN YELLOW, 4"
- ②⑥ PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW, SINGLE OR DOUBLE
- ②⑦ LINE, THERMOPLASTIC, SOLID WHITE, 8"
- ②⑧ TRANSVERSE MARKING, THERMOPLASTIC, SOLID WHITE, 12"
- ②⑨ TRANSVERSE MARKING, THERMOPLASTIC, PIANO KEY, 24"

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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. PAVEMENT MARKING PLANS		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds		S.T.
Checked by: M. TREVINO	0143C	Sheet 13 of 25



SCALE 1"=20'

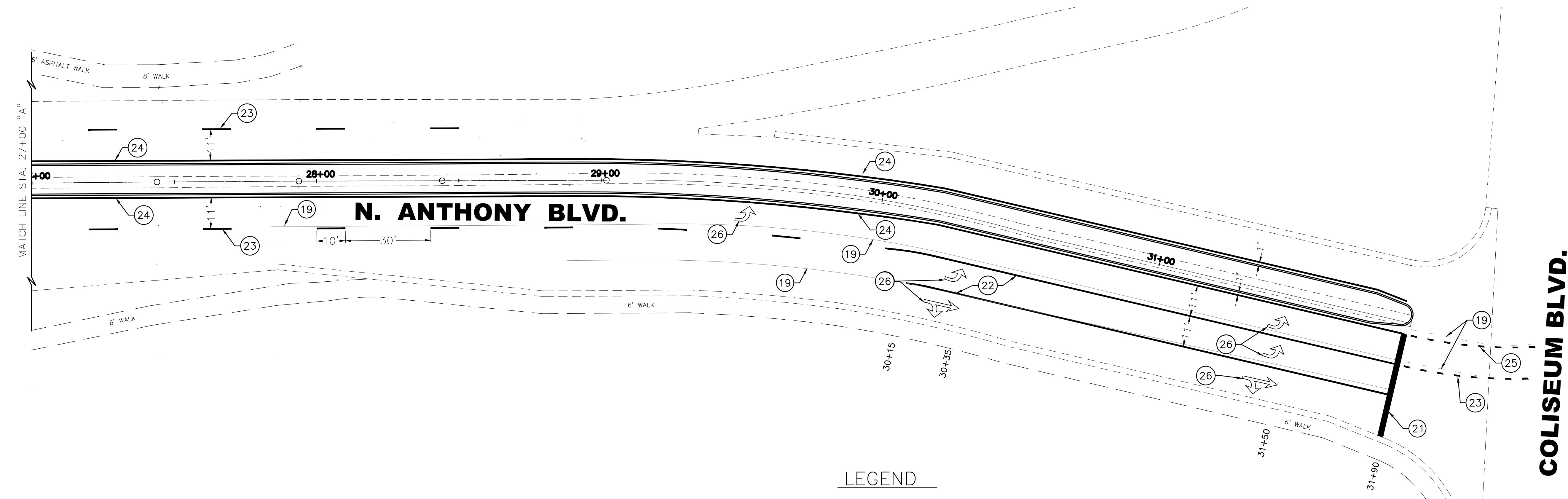
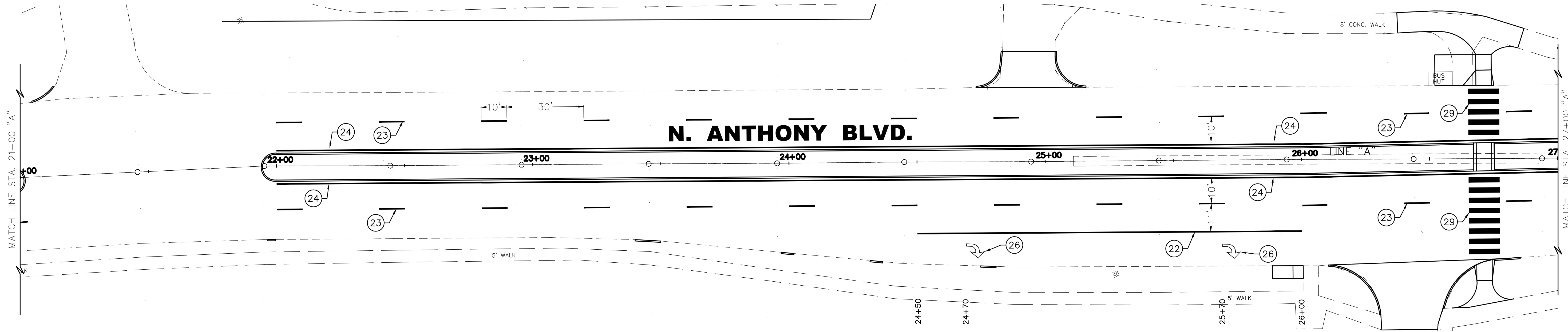


LEGEND

- ⑲ GRIND EXISTING STRIPING
- ⑳ TRANSVERSE MARKING, THERMOPLASTIC, STOP BAR, 24"
- ㉑ LINE, EPOXY PAINT, SOLID WHITE, 4"
- ㉒ LINE, EPOXY PAINT, BROKEN WHITE, 4"
- ㉓ LINE, EPOXY PAINT, SOLID YELLOW, 4"
- ㉔ LINE, EPOXY PAINT, BROKEN YELLOW, 4"
- ㉕ PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW, SINGLE OR DOUBLE
- ㉖ LINE, THERMOPLASTIC, SOLID WHITE, 8"
- ㉗ TRANSVERSE MARKING, THERMOPLASTIC, SOLID WHITE, 12"
- ㉘ TRANSVERSE MARKING, THERMOPLASTIC, PIANO KEY, 24"

NOTE:
 THE LOCATIONS OF UNDERGROUND UTILITIES AS SHOWN ARE APPROXIMATE. THEIR LOCATIONS WERE DETERMINED FROM INFORMATION PROVIDED BY THE SURVEY OR AVAILABLE MAPPING. ALL LOCATIONS SHALL BE FIELD VERIFIED BY CONTRACTOR BEFORE EXCAVATING.

NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. PAVEMENT MARKING PLANS		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds		S.T.
Checked by: M. Trevino	0143C	
		Sheet 14 of 25

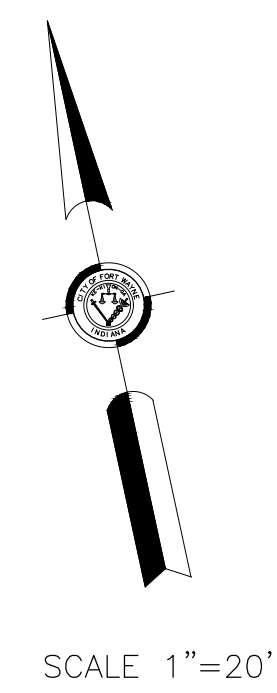
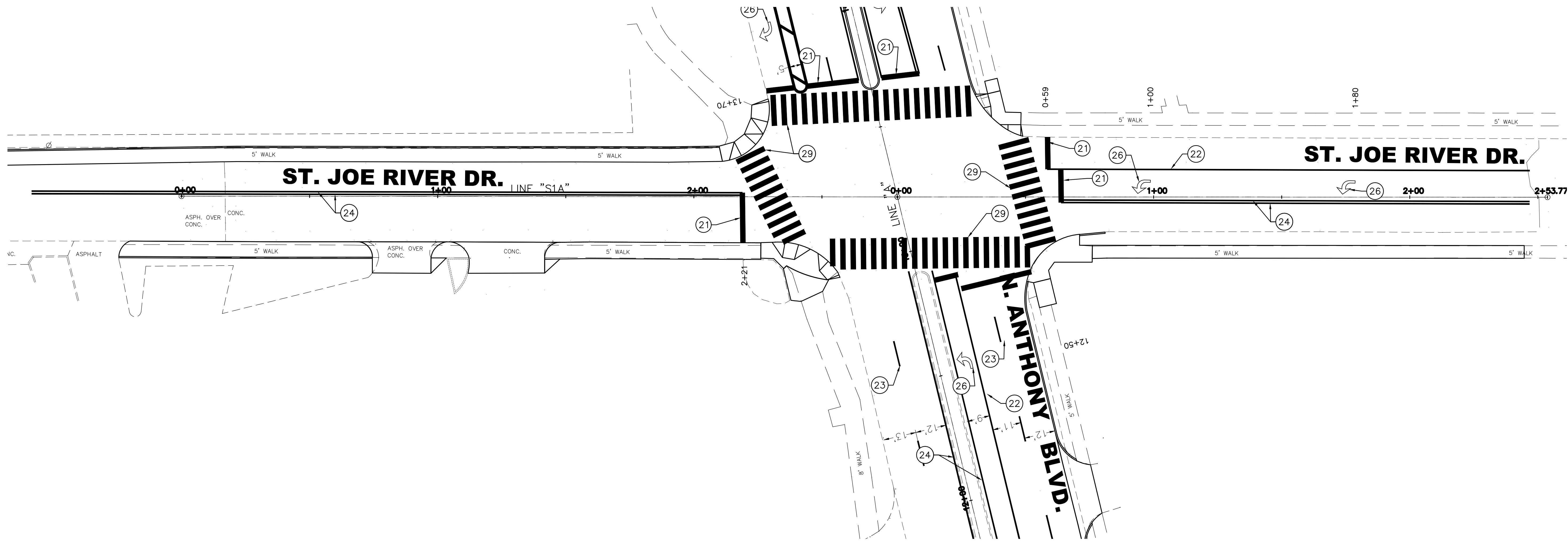


LEGEND

- 19 GRIND EXISTING STRIPING
- 21 TRANSVERSE MARKING, THERMOPLASTIC, STOP BAR, 24"
- 22 LINE, EPOXY PAINT, SOLID WHITE, 4"
- 23 LINE, EPOXY PAINT, BROKEN WHITE, 4"
- 24 LINE, EPOXY PAINT, SOLID YELLOW, 4"
- 25 LINE, EPOXY PAINT, BROKEN YELLOW, 4"
- 26 PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW, SINGLE OR DOUBLE
- 27 LINE, THERMOPLASTIC, SOLID WHITE, 8"
- 28 TRANSVERSE MARKING, THERMOPLASTIC, SOLID WHITE, 12"
- 29 TRANSVERSE MARKING, THERMOPLASTIC, PIANO KEY, 24"

NOTE:
THE LOCATIONS OF UNDERGROUND UTILITIES AS SHOWN ARE APPROXIMATE. THEIR LOCATIONS WERE DETERMINED FROM INFORMATION PROVIDED BY THE SURVEY OR AVAILABLE MAPPING. ALL LOCATIONS SHALL BE FIELD VERIFIED BY CONTRACTOR BEFORE EXCAVATING.

NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. PAVEMENT MARKING PLANS		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds		S.T.
Checked by: M. TREVINO	0143C	Sheet 15 of 25



LEGEND

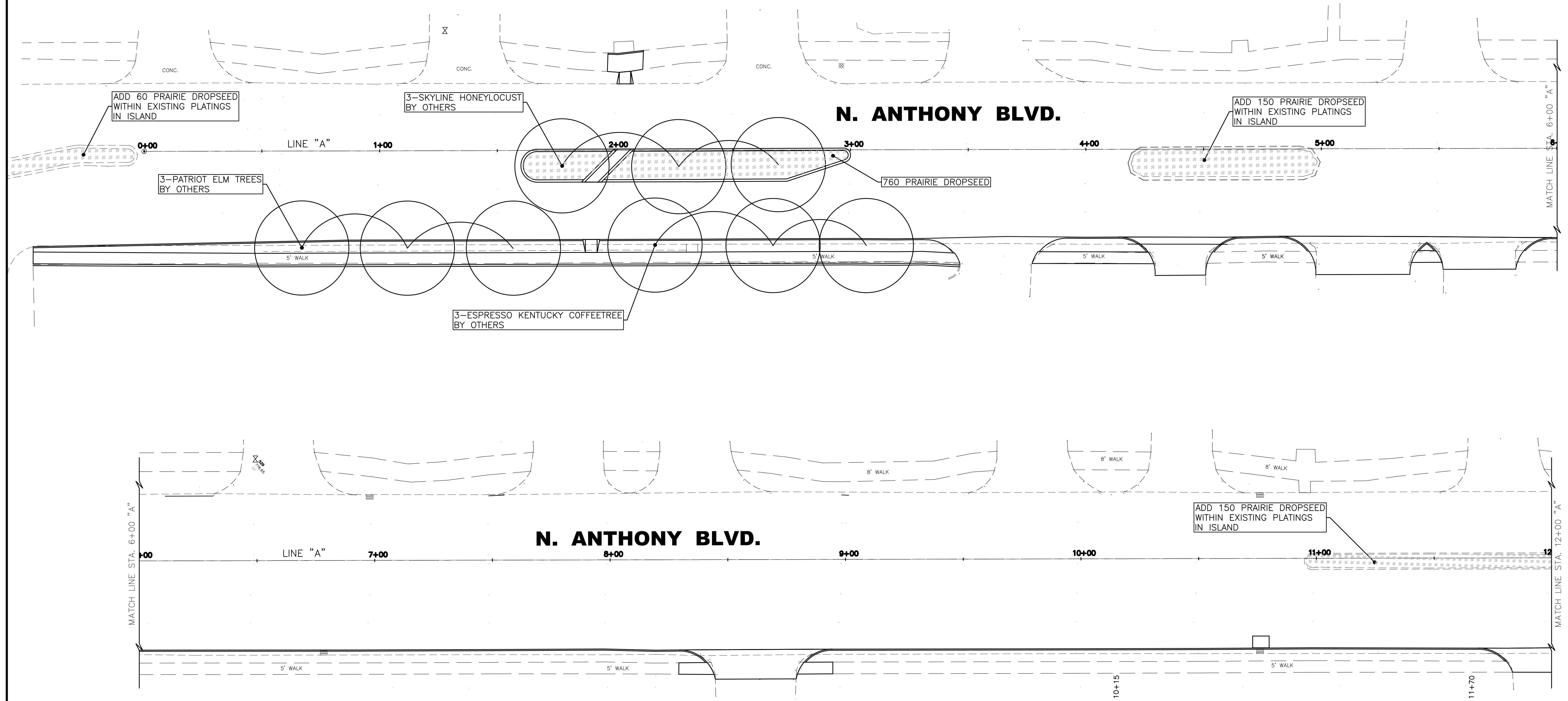
- 19 GRIND EXISTING STRIPING
- 21 TRANSVERSE MARKING, THERMOPLASTIC, STOP BAR, 24"
- 22 LINE, EPOXY PAINT, SOLID WHITE, 4"
- 23 LINE, EPOXY PAINT, BROKEN WHITE, 4"
- 24 LINE, EPOXY PAINT, SOLID YELLOW, 4"
- 25 LINE, EPOXY PAINT, BROKEN YELLOW, 4"
- 26 PAVEMENT MESSAGE MARKING, THERMOPLASTIC, LANE INDICATION ARROW, SINGLE OR DOUBLE
- 27 LINE, THERMOPLASTIC, SOLID WHITE, 8"
- 28 TRANSVERSE MARKING, THERMOPLASTIC, SOLID WHITE, 2"
- 29 TRANSVERSE MARKING, THERMOPLASTIC, PIANO KEY, 24"

NOTE:
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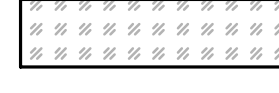
NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. PAVEMENT MARKING PLANS		
Designed by: M. Gray	RES.	W.O.
Drawn by: R. Reynolds		S.T.
Checked by: M. TREVINO	0143C	Sheet 16 of 25



SCALE 1"=20'



LEGEND

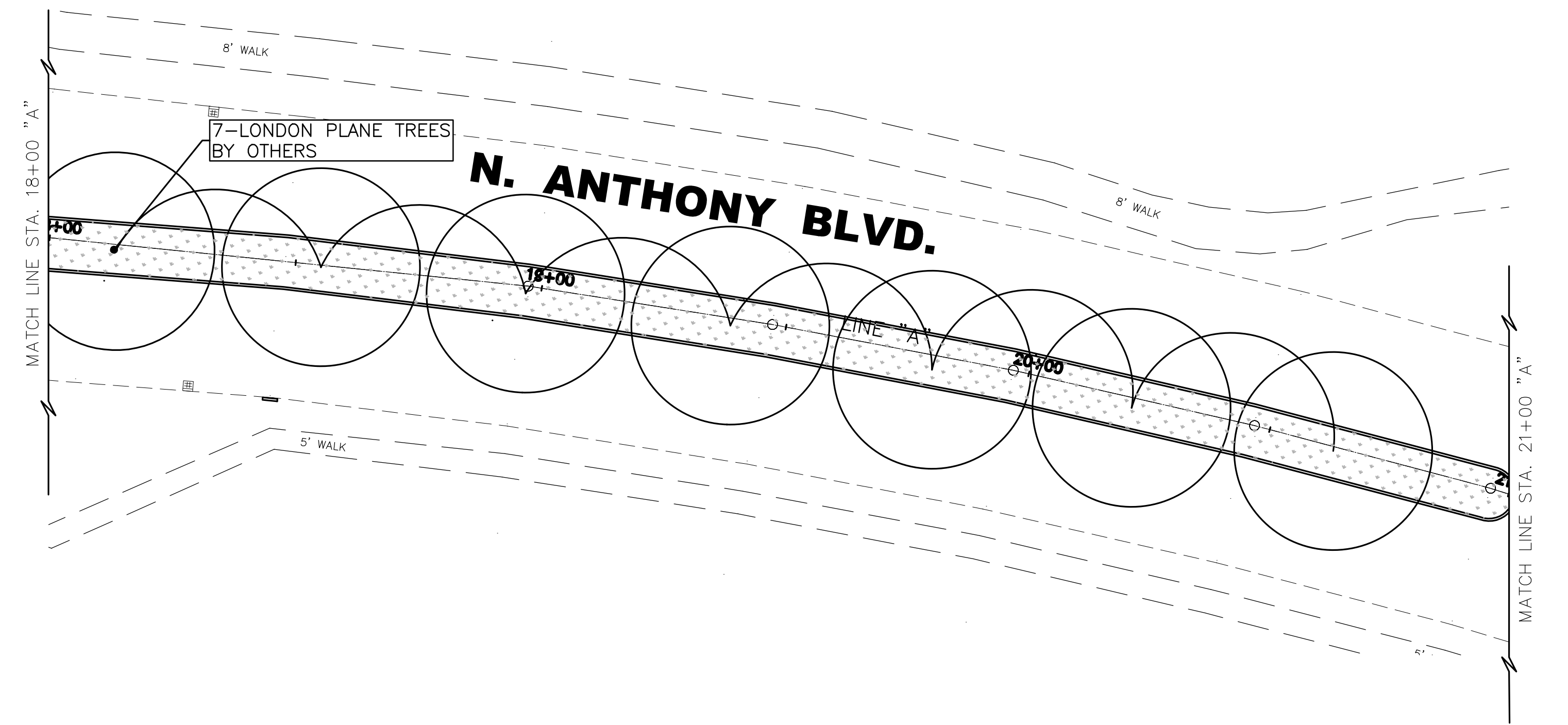
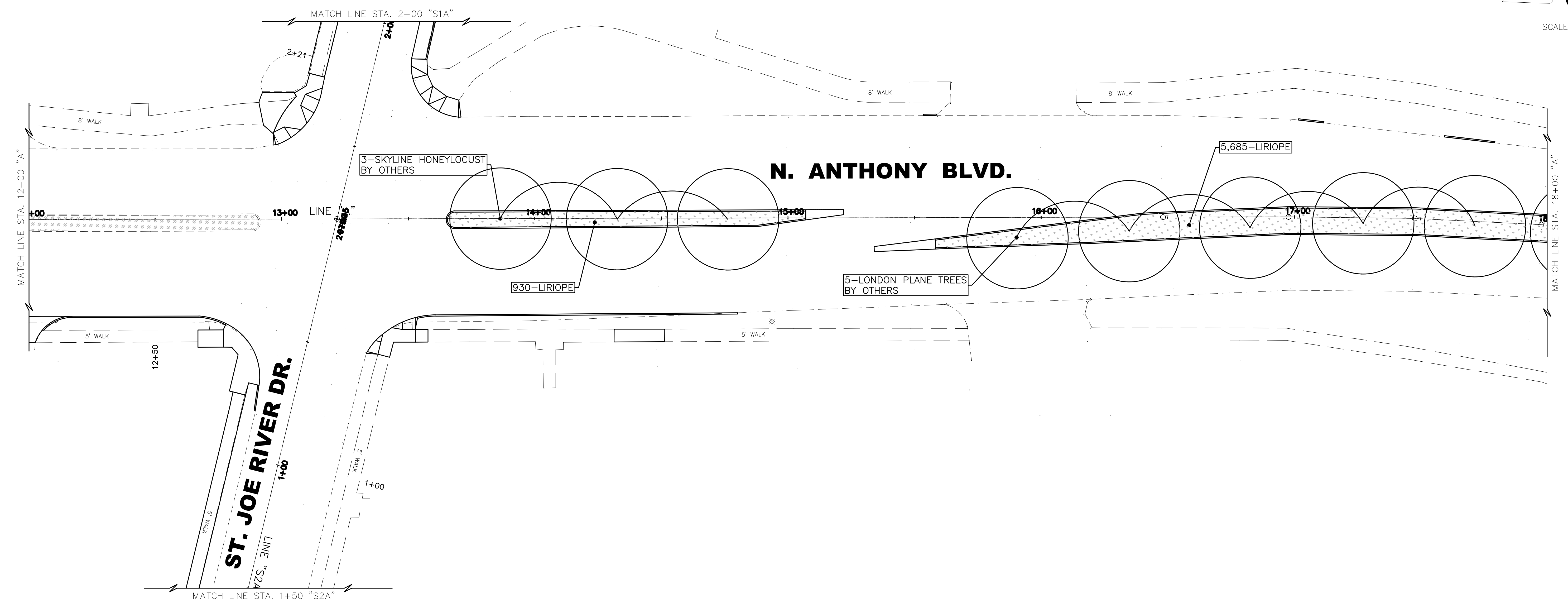
-  LIRIOPE
-  PRAIRIE DROPSEED

NOTE:
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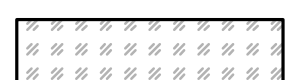
NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. LANDSCAPING PLAN			
Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		Sheet 17 of 25
Checked by: M. TREVINO			



SCALE 1"=20'

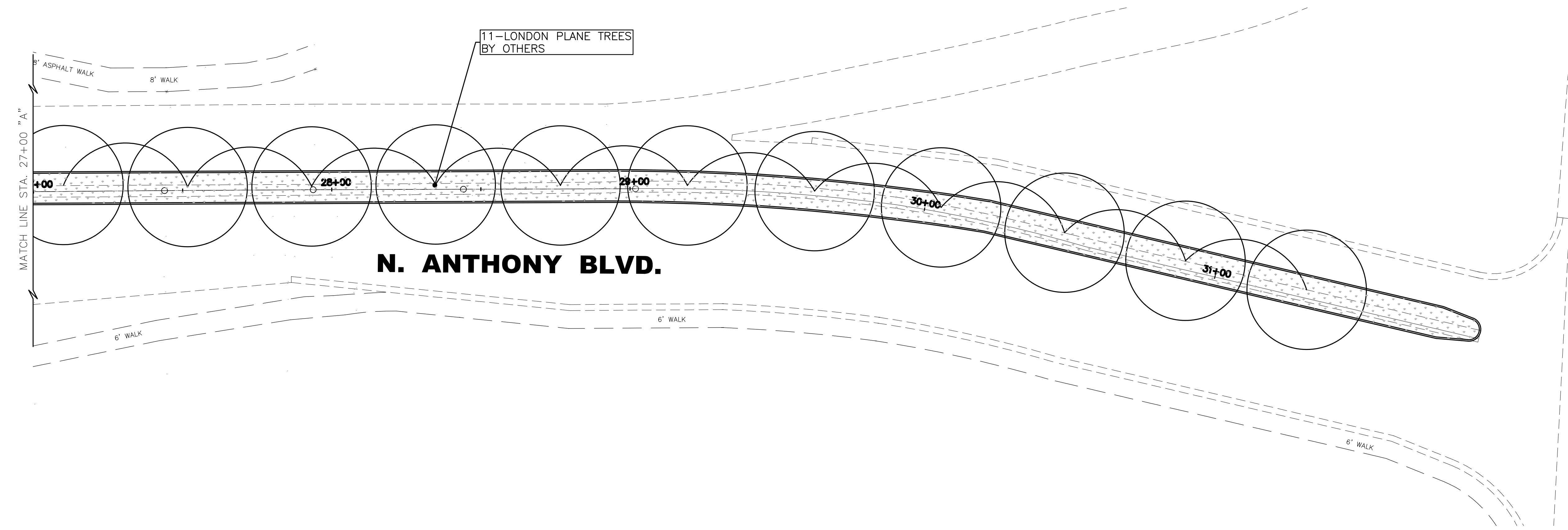
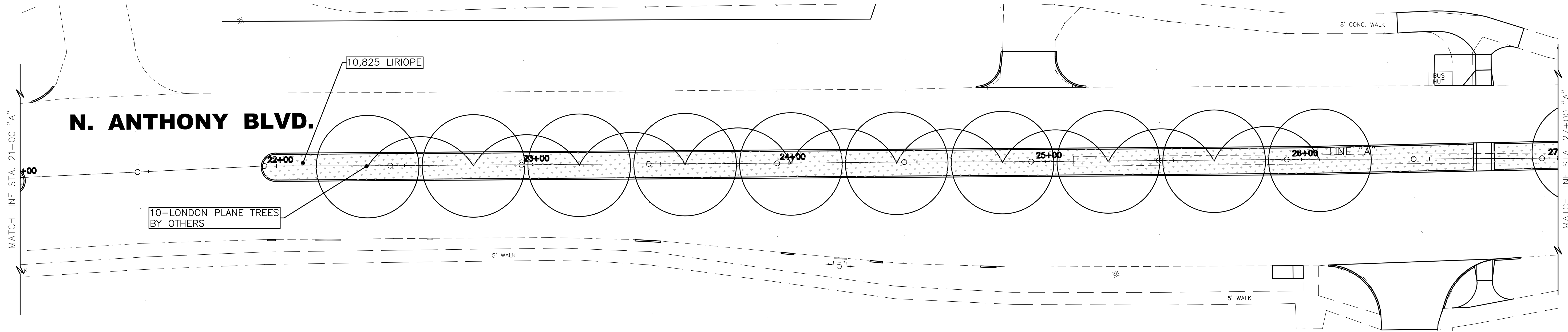


LEGEND



-  LIRIOPE
-  PRAIRIE DROPSEED

NOTE:
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. LANDSCAPING PLAN			
Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		Sheet 18 of 25
Checked by: M. TREVINO			

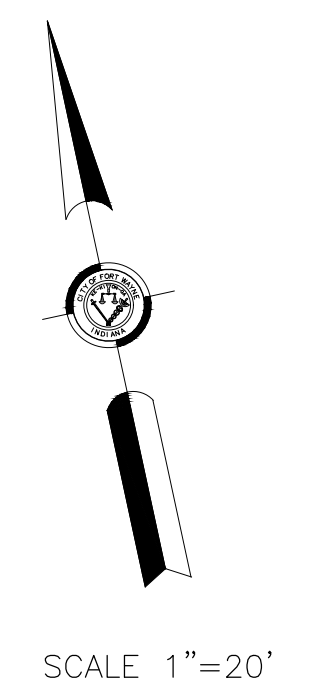
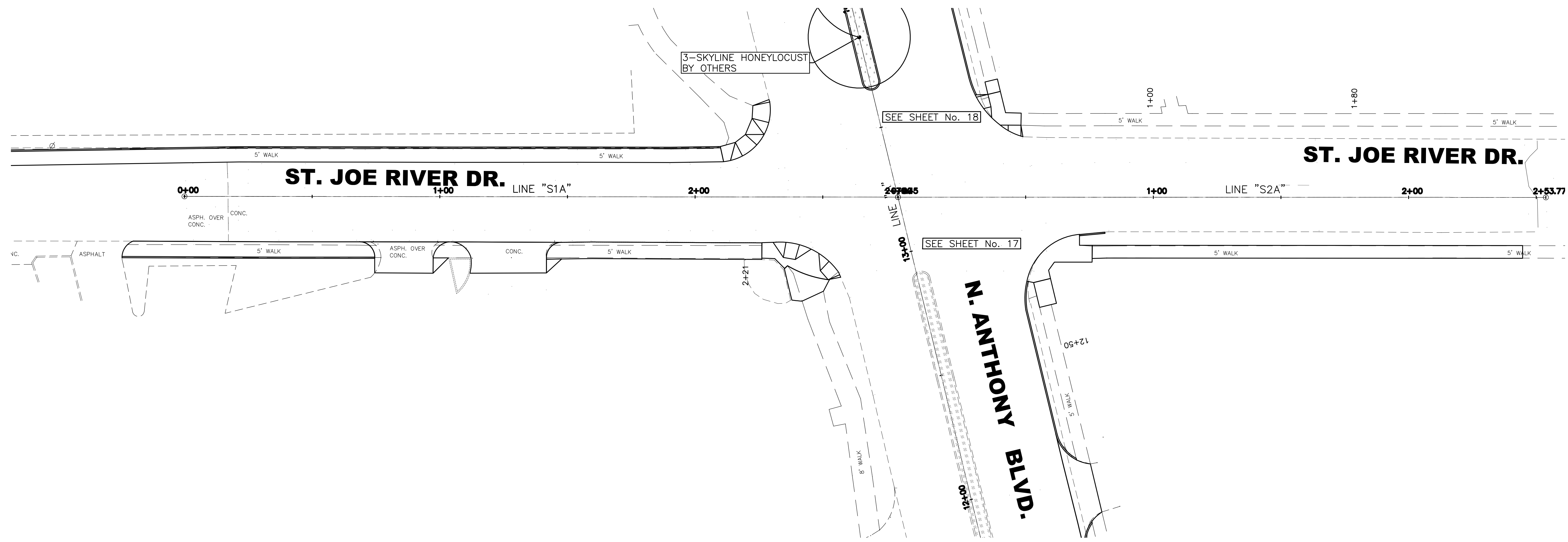


LEGEND

-  LIRIOPE
-  PRAIRIE DROPSEED



NOTE:
 THE LOCATIONS OF UNDERGROUND UTILITIES AS SHOWN ARE APPROXIMATE. THEIR LOCATIONS WERE DETERMINED FROM INFORMATION PROVIDED BY THE SURVEY OR AVAILABLE MAPPING. ALL LOCATIONS SHALL BE FIELD VERIFIED BY CONTRACTOR BEFORE EXCAVATING.

NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. LANDSCAPING PLAN			
Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		Sheet 19 of 24
Checked by: M. TREVINO			



QTY.	COMMON NAME	SCIENTIFIC NAME	SIZE	REMARKS
1,190	PRAIRIE DROPSEED	SPOROBOLUS HETEROLEPIS	QUART CONTAINER	18" O.C.
17,440	LIRIOPE	LIRIOPE SPICATA	QUART CONTINER	12" O.C.
6	SKYLINE HONEYLOCUST	GLEDITSIA TRIACANTHOS VAR. INERMIS "SKYCOLE"	2" CALIPAR B&B	40 O.C.
33	LONDON PLANE TREE	PLATANUS X ACERFOLIA	2" CALIPAR B&B	40' O.C.
3	ESPRESSO KENTUCKY COFFEETREE	GYMNOCLADUS DIOICUS "ESPRESSO"	2" CALIPAR B&B	40' O.C.
3	PATRIOT ELM TREE	ULMUS "PATRIOT"	2" CALIPAR B&B	40' O.C.

LEGEND

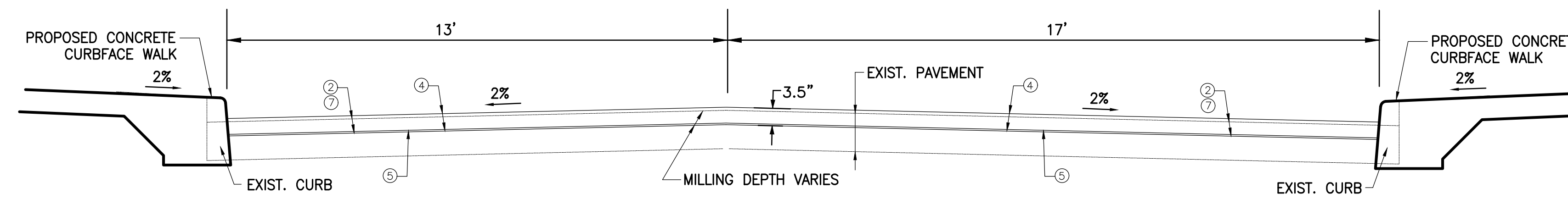
-  LIRIOPE
-  PRAIRIE DROPSEED

NOTE:
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NORTH ANTHONY BOULEVARD CRESENT AVE. TO COLISEUM BLVD. LANDSCAPING PLAN			
Designed by: M. Gray	RES.	W.O.	S.T.
Drawn by: R. Reynolds	0143C		Sheet 20 of 25
Checked by: M. TREVINO			

Typical Section #3

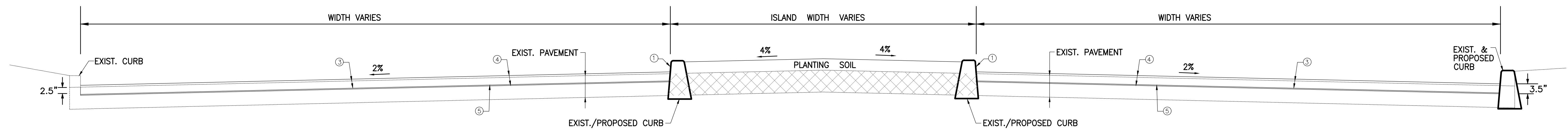
NOT TO SCALE



TYPICAL STREET SECTION
(LINE "S1A" ONLY)

Typical Section #2

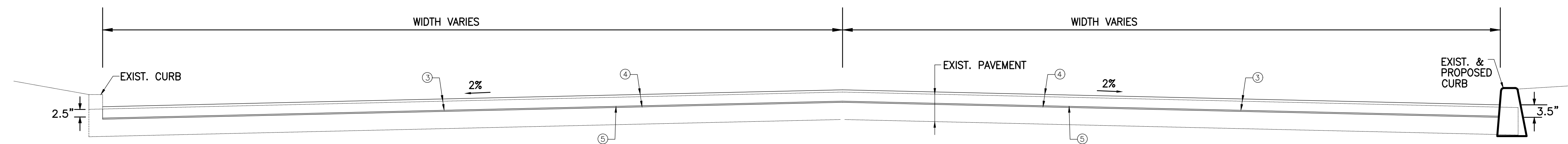
NOT TO SCALE



TYPICAL STREET SECTION
W/ CENTER ISLAND
(LINE "A" ONLY)

Typical Section #1

NOT TO SCALE



TYPICAL STREET SECTION
(LINE "A" & LINE "S2A")

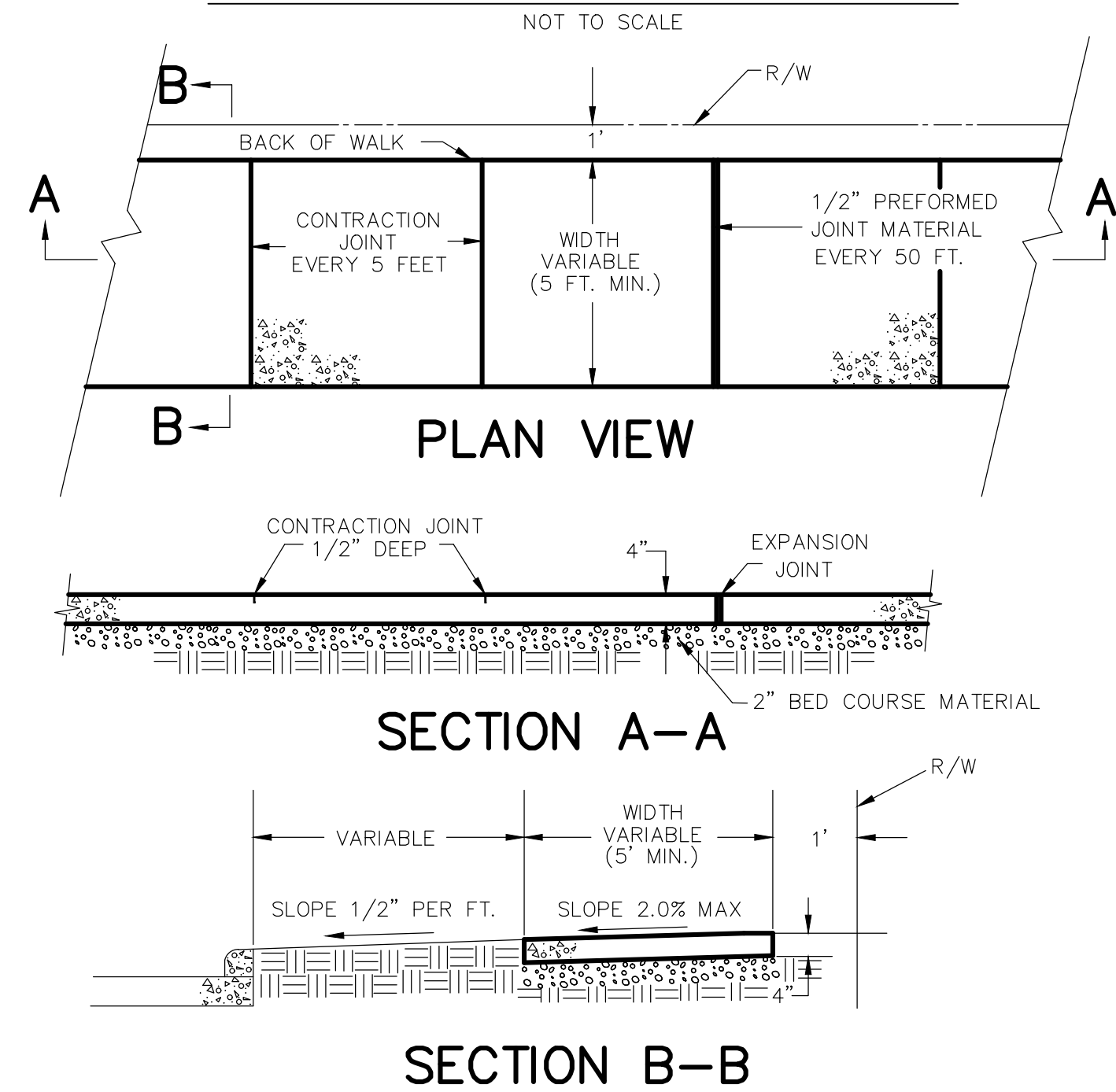
LEGEND

- ① FULL DEPTH SAWING
- ② TRANSITION PCCP MILLING (VARIABLE DEPTH)
- ③ 2.5" PCCP MILLING
- ④ 1.5", 9.5 mm SURFACE, TYPE C OVER
2", 19 mm INTERMEDIATE, TYPE C
- ⑤ FIBERMAT TYPE B
- ⑦ 2.5" PCCP/ASPHALT MILLING

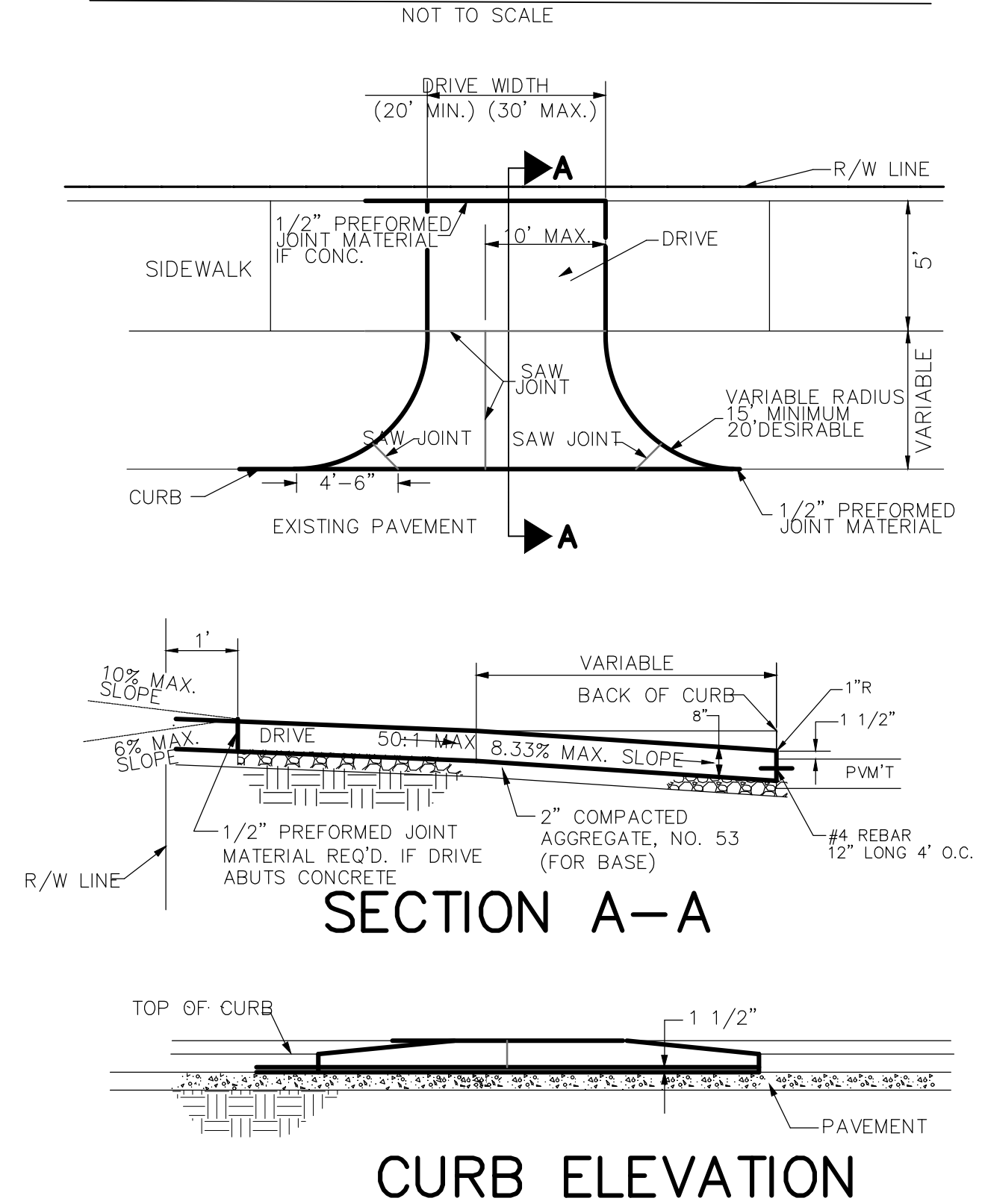
NORTH ANTHONY BOULEVARD
CRESENT AVE. TO COLISEUM BLVD.
TYPICAL CROSS SECTIONS

Designed by: M. GRAY	RES.	W.O.	S.T.
Drawn by: R. REYNOLDS	0143C		
Checked by: M. TREVINO			

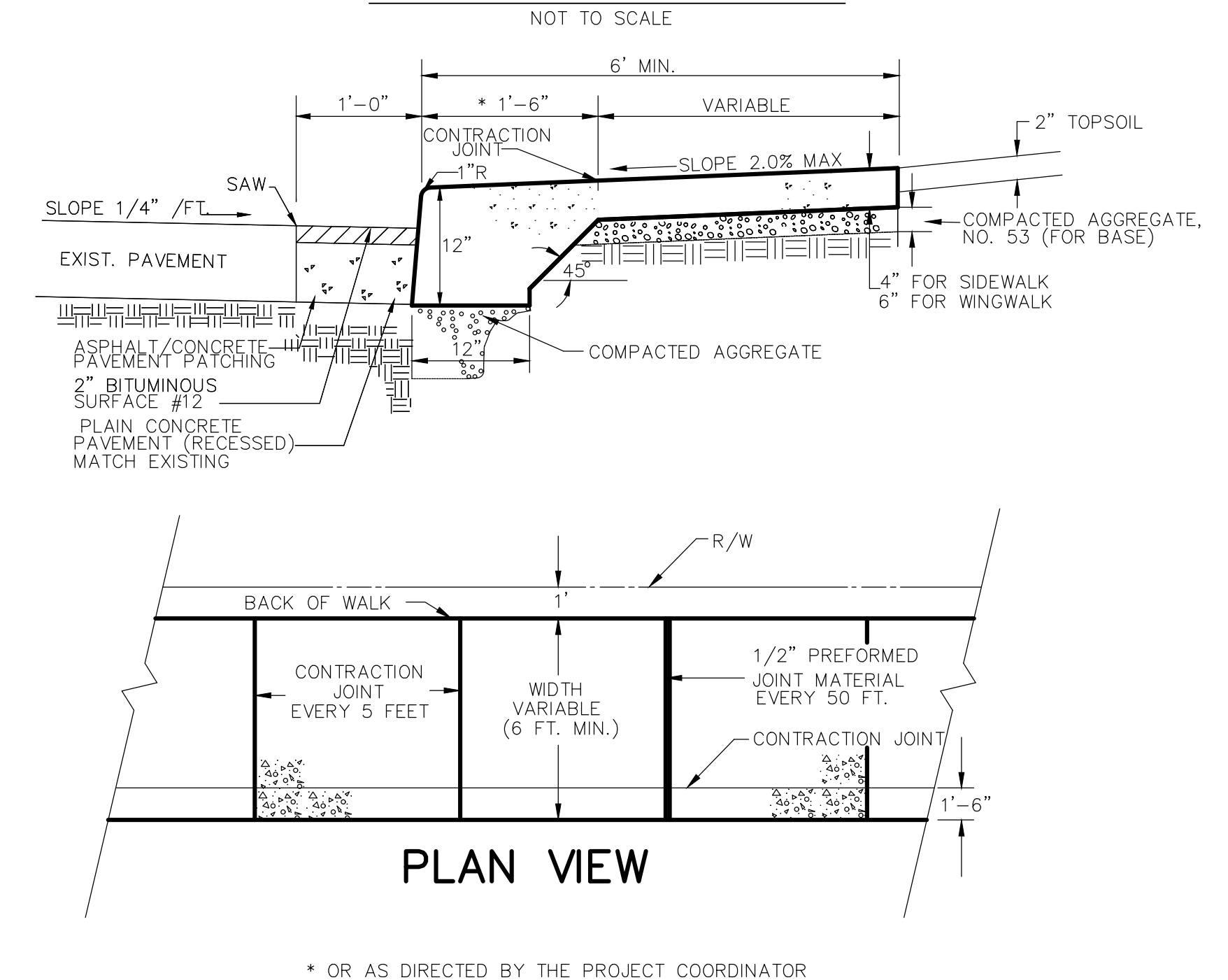
TYPICAL CONCRETE SIDEWALK



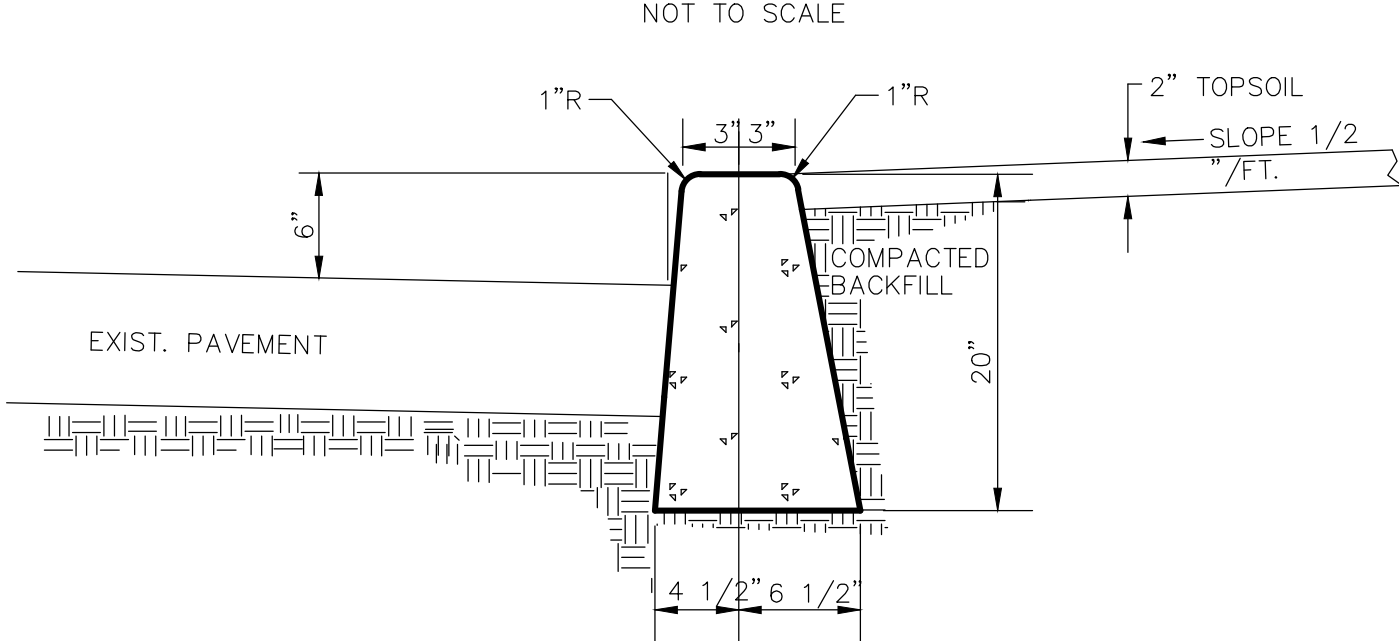
COMMERCIAL DRIVE APPROACH



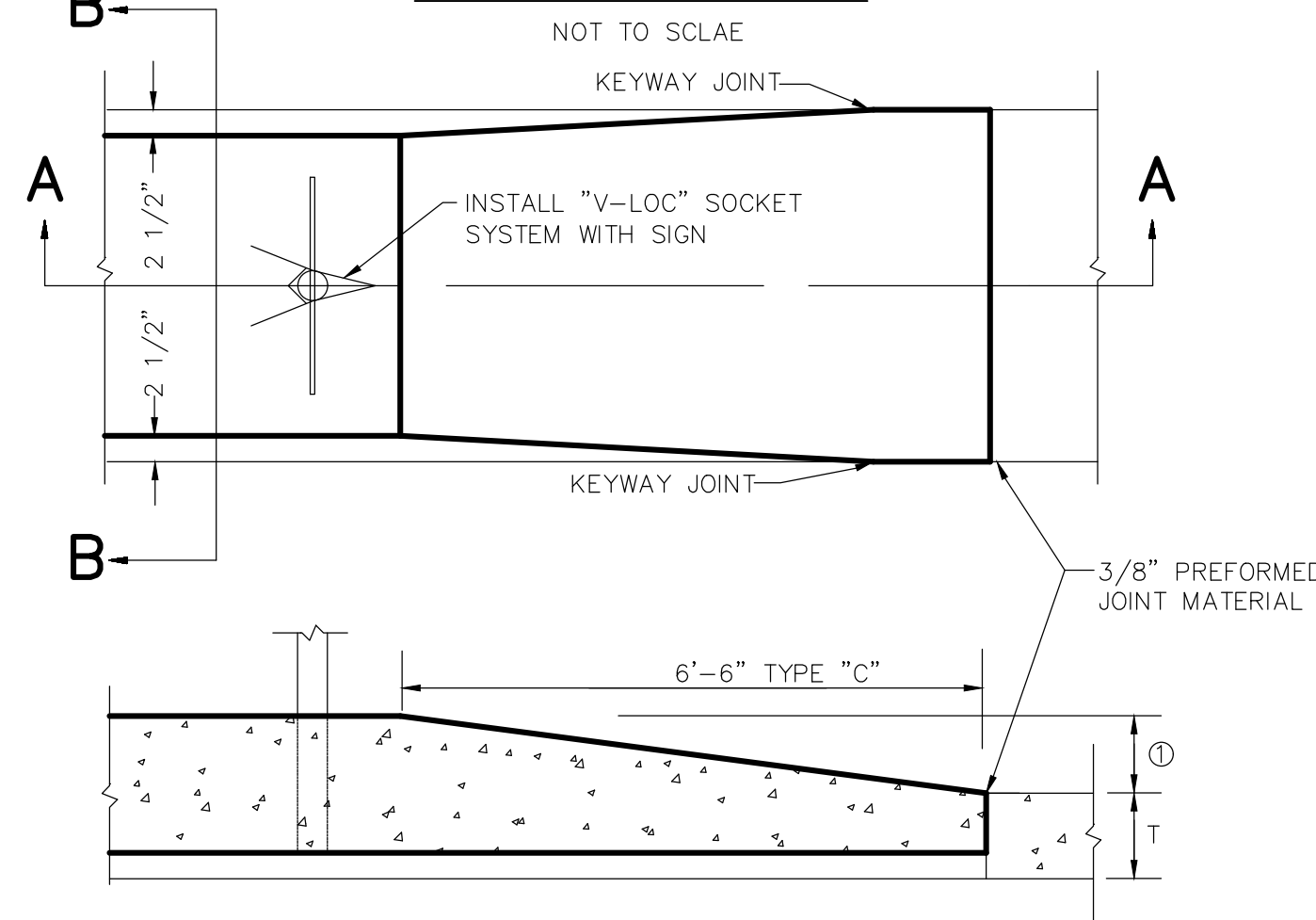
CURBSIDE SIDEWALK



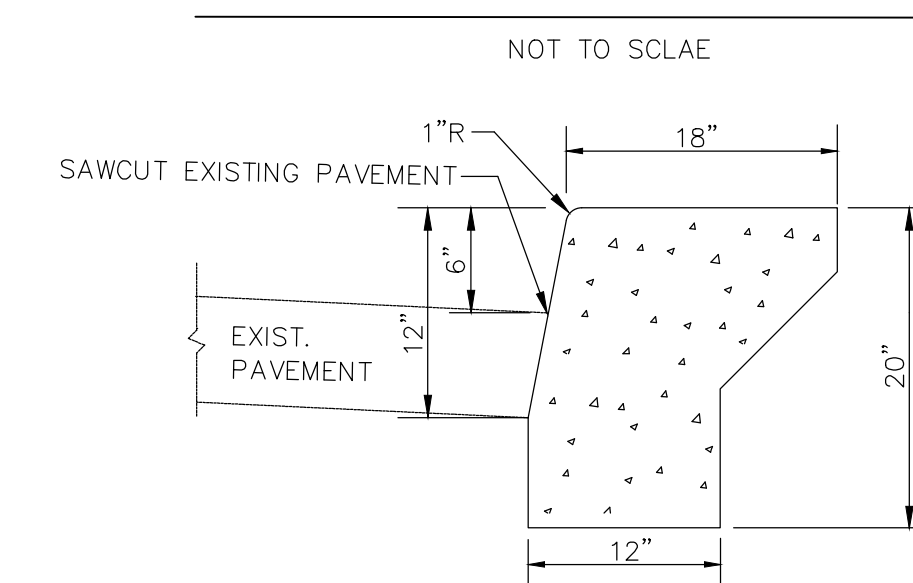
CONCRETE CURB TYPE III



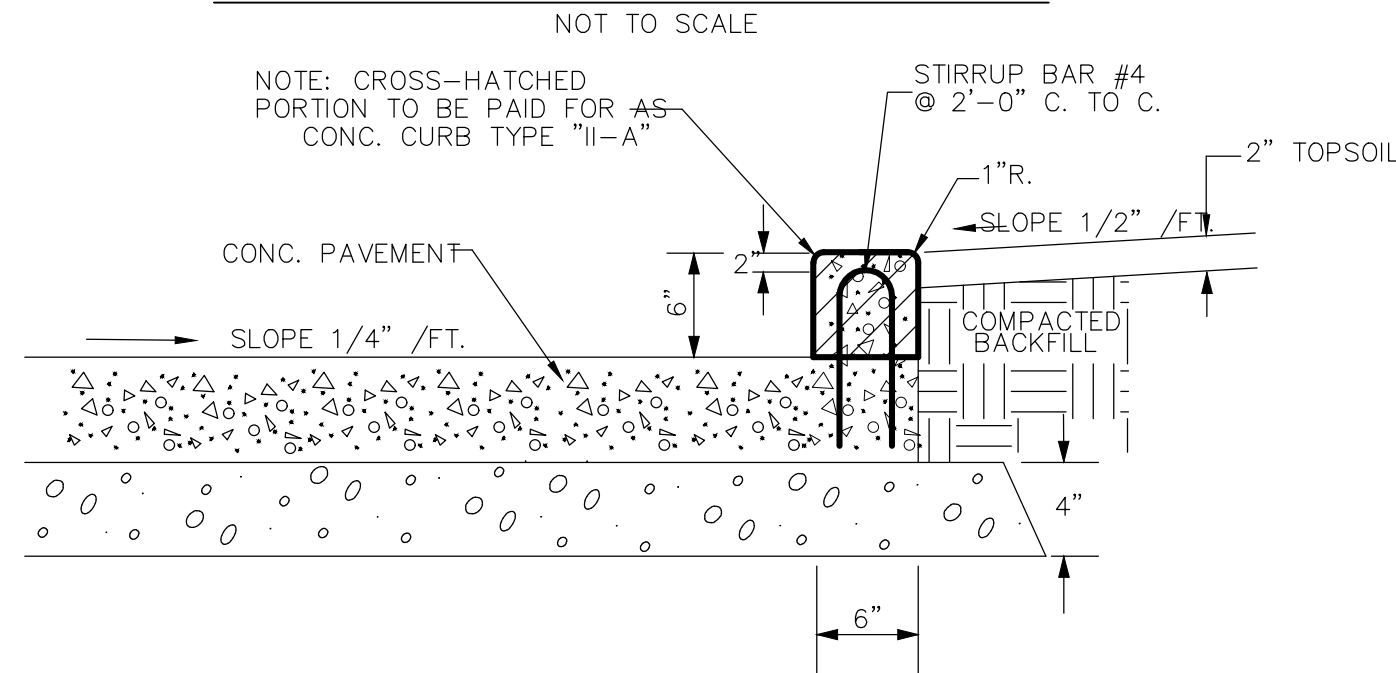
CONCRETE CENTER CURB TYPE "C"



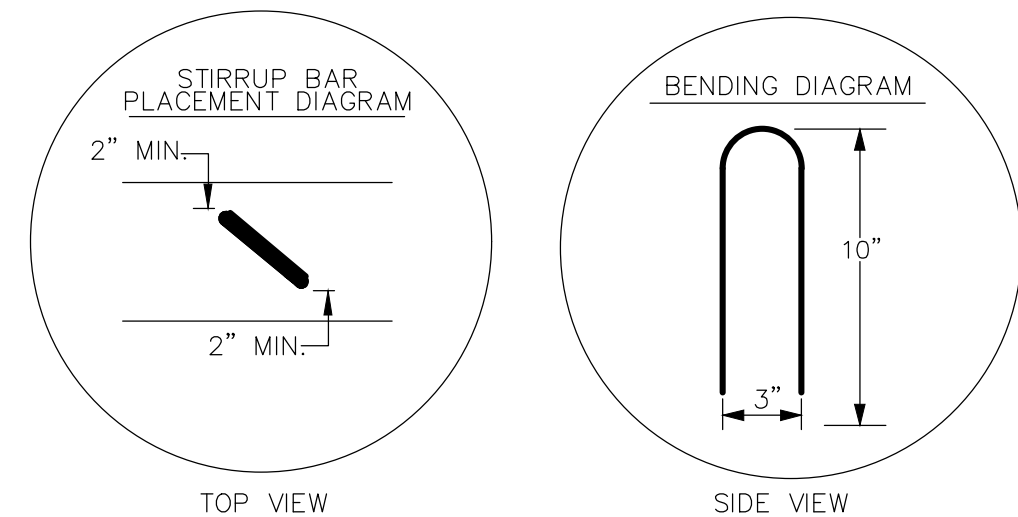
SPECIAL CONCRETE CURB TYPE III



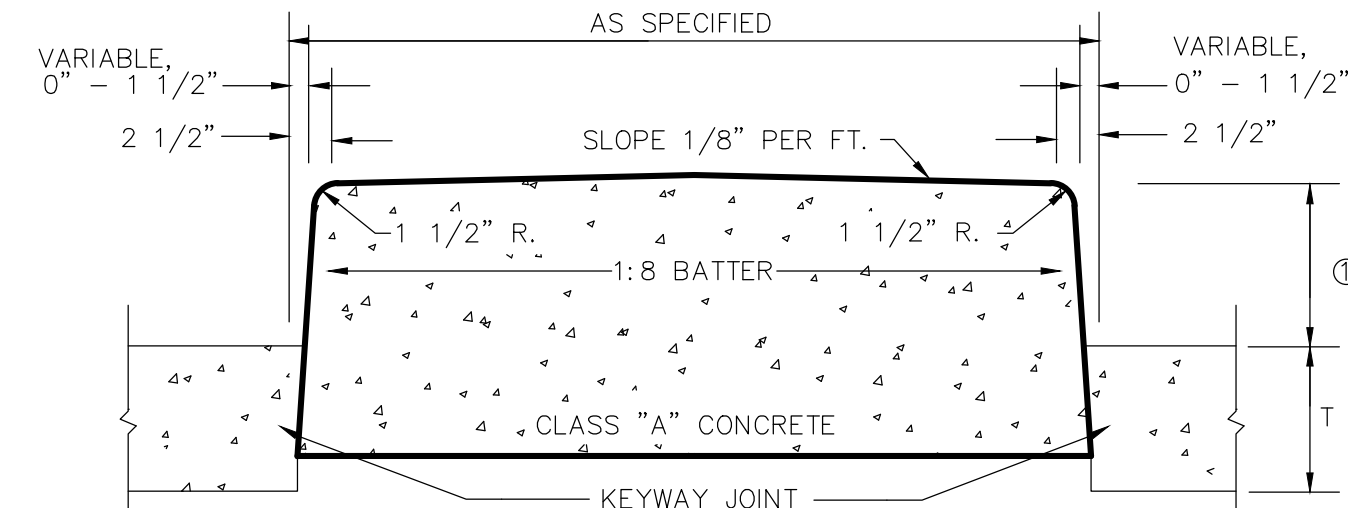
CONCRETE CURB TYPE II-A



NOTE: STIRRUP BARS TO BE INCLUDED IN THE COST OF CONC. CURB TYPE II-A



SECTION A - A



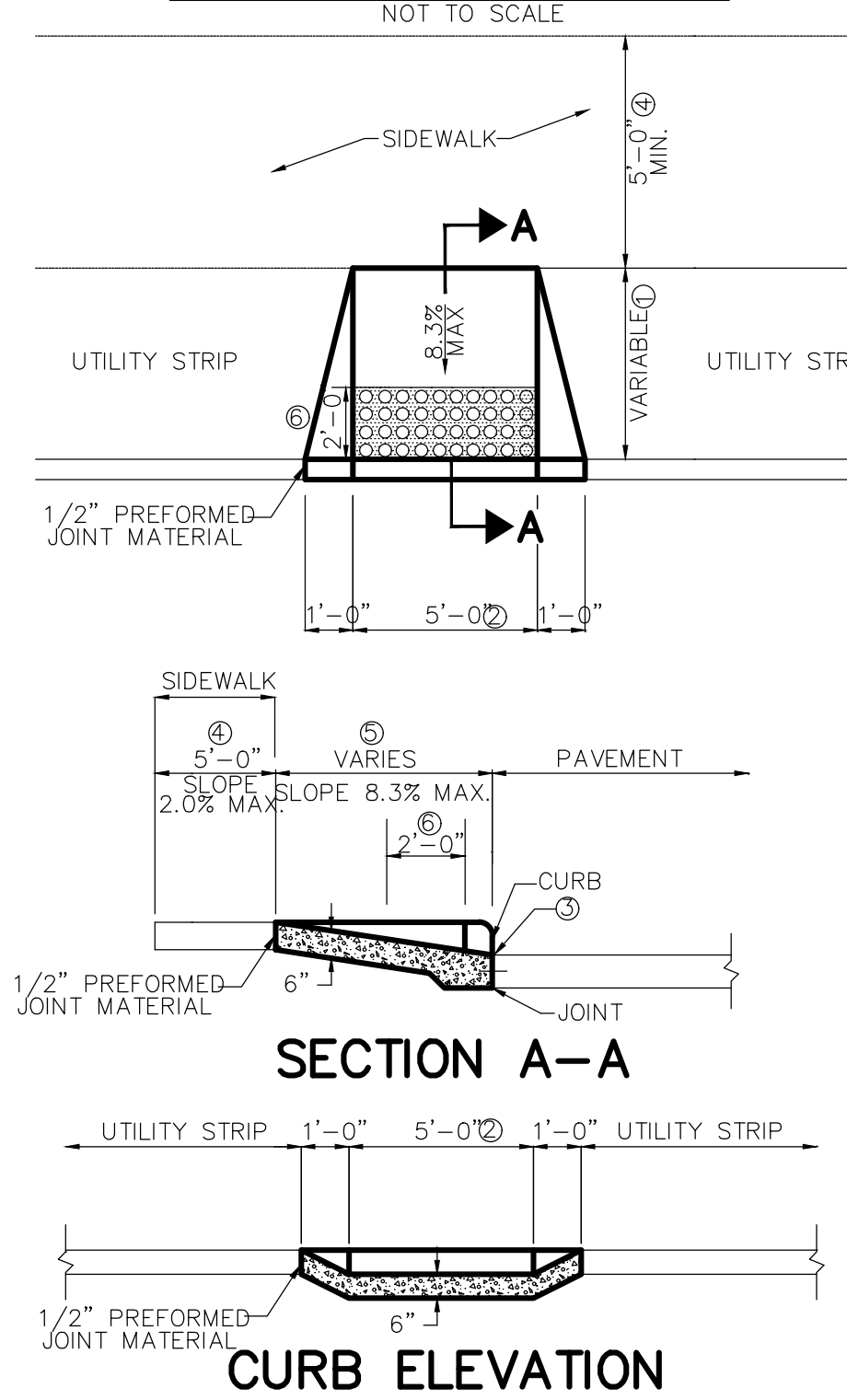
SECTION B - B

T = NORMAL PAVEMENT DEPTH
 Ø = 8" FOR CONCRETE CENTER CURB TYPE "C"

**NORTH ANTHONY BOULEVARD
 CRESENT AVE. TO COLISEUM BLVD.
 DETAILS**

Designed by: M. GRAY	RES.	W.O.	S.T.
Drawn by: R. REYNOLDS	0143C		
Checked by: M. TREVINO	Sheet 22 of 25		

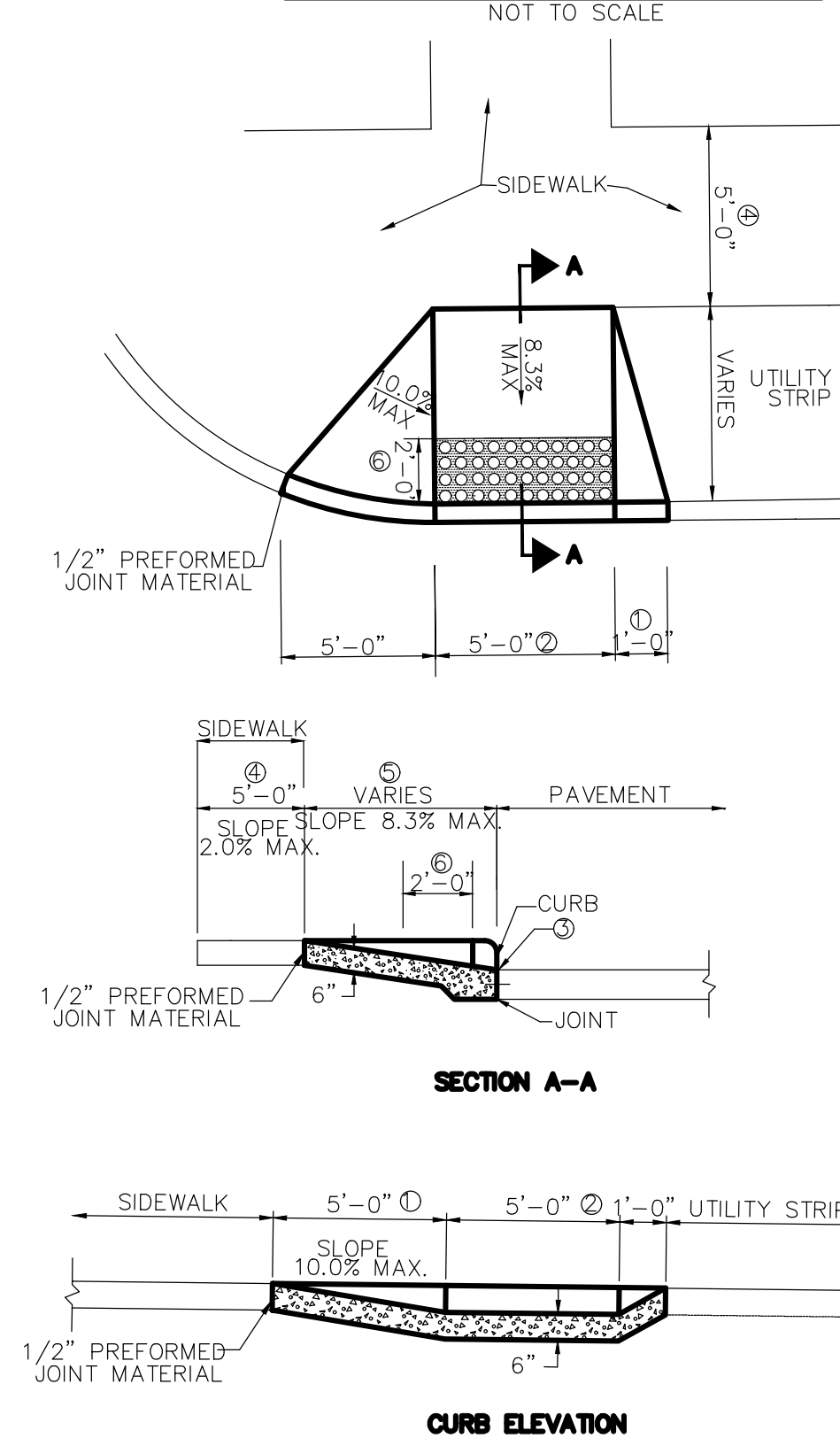
CURB RAMP TYPE C



GENERAL NOTES

- ① THESE DIMENSIONS ARE BASED ON A 6" CURB HEIGHT, AND SHALL BE PROPORTIONALLY ADJUSTED FOR OTHER CURB HEIGHTS.
- ② A 4'-0" MINIMUM WIDTH RAMP MAY BE USED WHEN EXISTING SPACE PROHIBITS THE CONSTRUCTION OF THE 5'-0" WIDE RAMP.
- ③ THE BOTTOM EDGE OF THE CURB RAMP SHALL BE FLUSH WITH THE EDGE OF THE ADJACENT PAVEMENT AND GUTTER LINE.
- ④ TURNING SPACE AT THE TOP OF CURB RAMP SHALL HAVE MAXIMUM CROSS SLOPE OF 2.0% IN ANY DIRECTION. WHERE SITE INFEASIBILITY PRECLUDES A TURNING SPACE SLOPE OF 2.0% IN ANY DIRECTION, THE SLOPE PERPENDICULAR TO THE CURB FACE SHALL NOT EXCEED 2.0%. A TURNING SPACE (4.0 FT) MINIMUM BY (4.0 FT) MINIMUM SHALL BE PROVIDED AT THE TOP OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK, THE TURNING SPACE SHALL BE (4.0 FT) MINIMUM BY (5.0 FT) MINIMUM. THE (5.0 FT) DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- ⑤ THE RUNNING SLOPE OF THE CURB RAMP SHALL CUT THROUGH OR SHALL BE BUILT UP TO THE CURB AT RIGHT ANGLES OR SHALL MEET THE GUTTER GRADE BREAK AT RIGHT ANGLES WHERE THE CURB IS CURVED. THE RUNNING SLOPE OF THE CURB SHALL BE 5.0% MINIMUM AND 8.3% MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED (15.0 FT). THE RUNNING SLOPE OF THE TURNING SPACE SHALL BE 2.0% MAXIMUM.
- ⑥ DETECTABLE WARNING SURFACE SHALL BE BRICK RED IN COLOR. SEE SPECIAL PROVISIONS FOR ACCEPTED TACTILE WARNING UNITS.

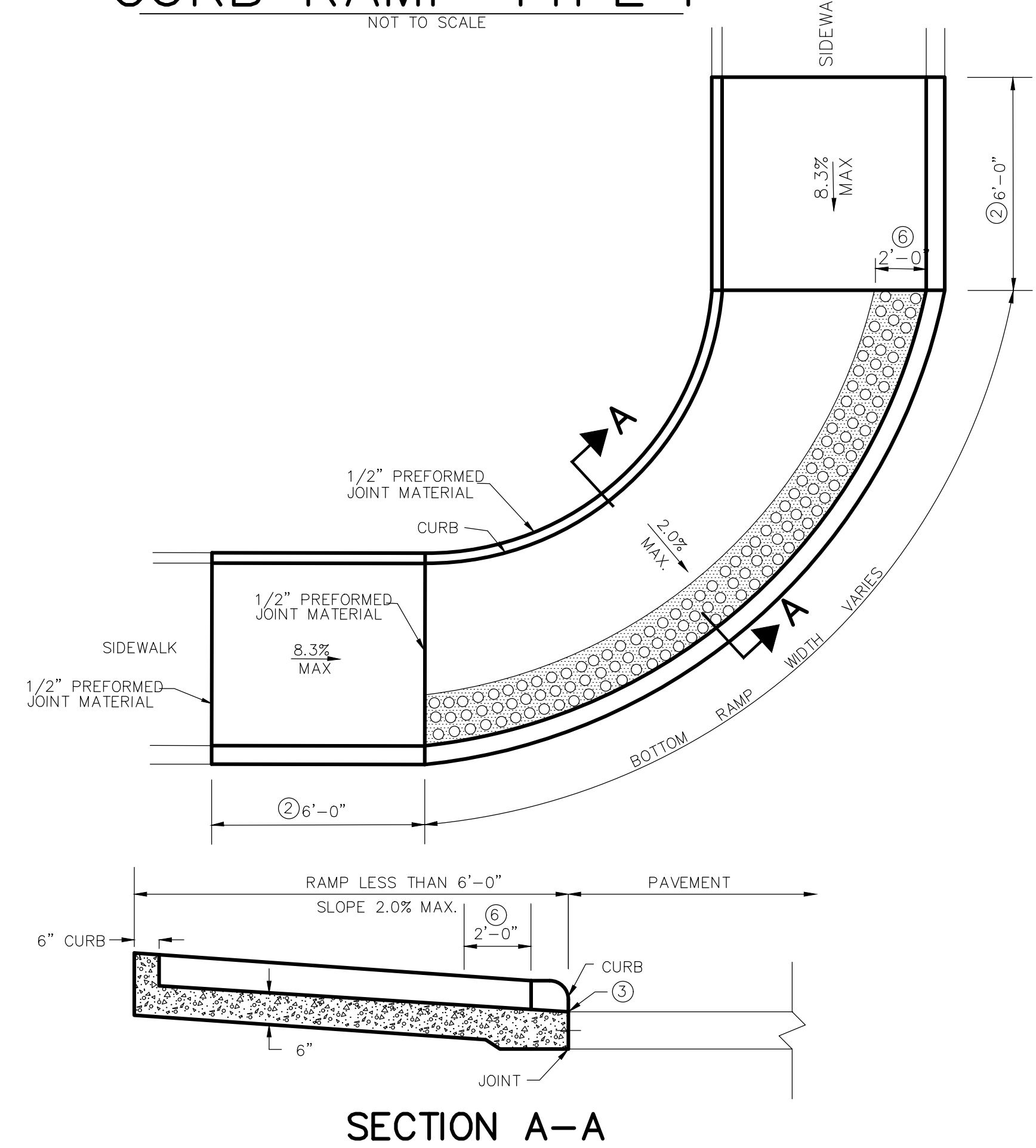
CURB RAMP TYPE D



GENERAL NOTES

- ① THESE DIMENSIONS ARE BASED ON A 6" CURB HEIGHT, AND SHALL BE PROPORTIONALLY ADJUSTED FOR OTHER CURB HEIGHTS.
- ② A 4'-0" MINIMUM WIDTH RAMP MAY BE USED WHEN EXISTING SPACE PROHIBITS THE CONSTRUCTION OF THE 5'-0" WIDE RAMP.
- ③ THE BOTTOM EDGE OF THE CURB RAMP SHALL BE FLUSH WITH THE EDGE OF THE ADJACENT PAVEMENT AND GUTTER LINE.
- ④ TURNING SPACE AT THE TOP OF CURB RAMP SHALL HAVE MAXIMUM CROSS SLOPE OF 2.0% IN ANY DIRECTION. WHERE SITE INFEASIBILITY PRECLUDES A TURNING SPACE SLOPE OF 2.0% IN ANY DIRECTION, THE SLOPE PERPENDICULAR TO THE CURB FACE SHALL NOT EXCEED 2.0%. A TURNING SPACE (4.0 FT) MINIMUM BY (4.0 FT) MINIMUM SHALL BE PROVIDED AT THE TOP OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK, THE TURNING SPACE SHALL BE (4.0 FT) MINIMUM BY (5.0 FT) MINIMUM. THE (5.0 FT) DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- ⑤ THE RUNNING SLOPE OF THE CURB RAMP SHALL CUT THROUGH OR SHALL BE BUILT UP TO THE CURB AT RIGHT ANGLES OR SHALL MEET THE GUTTER GRADE BREAK AT RIGHT ANGLES WHERE THE CURB IS CURVED. THE RUNNING SLOPE OF THE CURB SHALL BE 5.0% MINIMUM AND 8.3% MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED (15.0 FT). THE RUNNING SLOPE OF THE TURNING SPACE SHALL BE 2.0% MAXIMUM.
- ⑥ DETECTABLE WARNING SURFACE SHALL BE BRICK RED IN COLOR. SEE SPECIAL PROVISIONS FOR ACCEPTED TACTILE WARNING UNITS.

CURB RAMP TYPE F

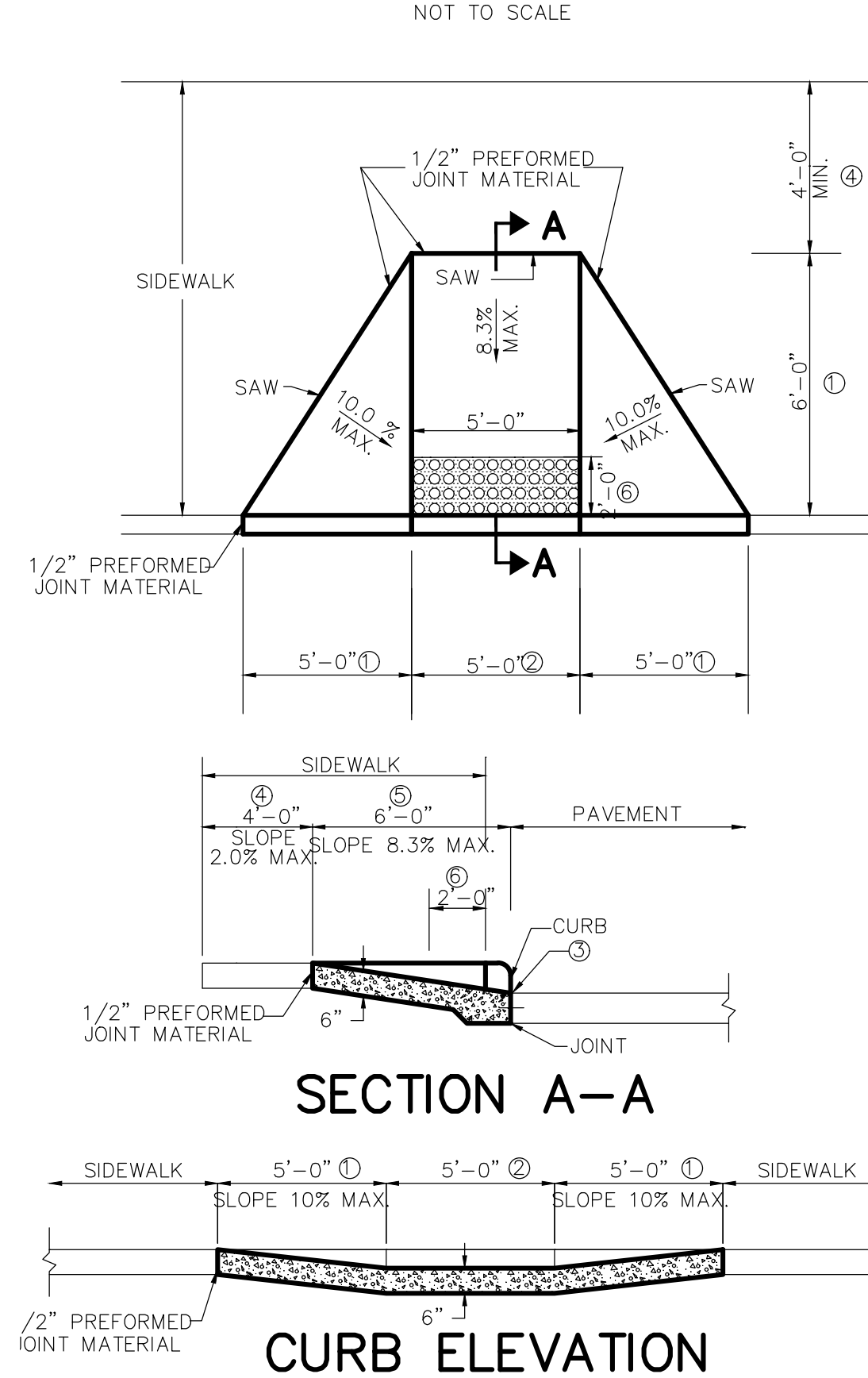


GENERAL NOTES

NOTE:
CURB RAMP SHALL NOT BE USED ON NEW CONSTRUCTION, BUT ONLY WHEN FIELD CONDITIONS WARRANT FOR EXISTING SIDEWALK ALTERATIONS. THIS RAMP REQUIRES CITY ENGINEER'S APPROVAL.

- ① THESE DIMENSIONS ARE BASED ON A 6" CURB HEIGHT, AND SHALL BE PROPORTIONALLY ADJUSTED FOR OTHER CURB HEIGHTS.
- ② A 4'-0" MINIMUM WIDTH RAMP MAY BE USED WITH ENGINEERS APPROVAL WHEN EXISTING SPACE PROHIBITS THE CONSTRUCTION OF THE 6'-0" WIDE RAMP.
- ③ THE BOTTOM EDGE OF THE CURB RAMP SHALL BE FLUSH WITH THE EDGE OF THE ADJACENT PAVEMENT AND GUTTER LINE.
- ⑥ DETECTABLE WARNING SURFACE SHALL BE BRICK RED IN COLOR. SEE SPECIAL PROVISIONS FOR ACCEPTED TACTILE WARNING UNITS.

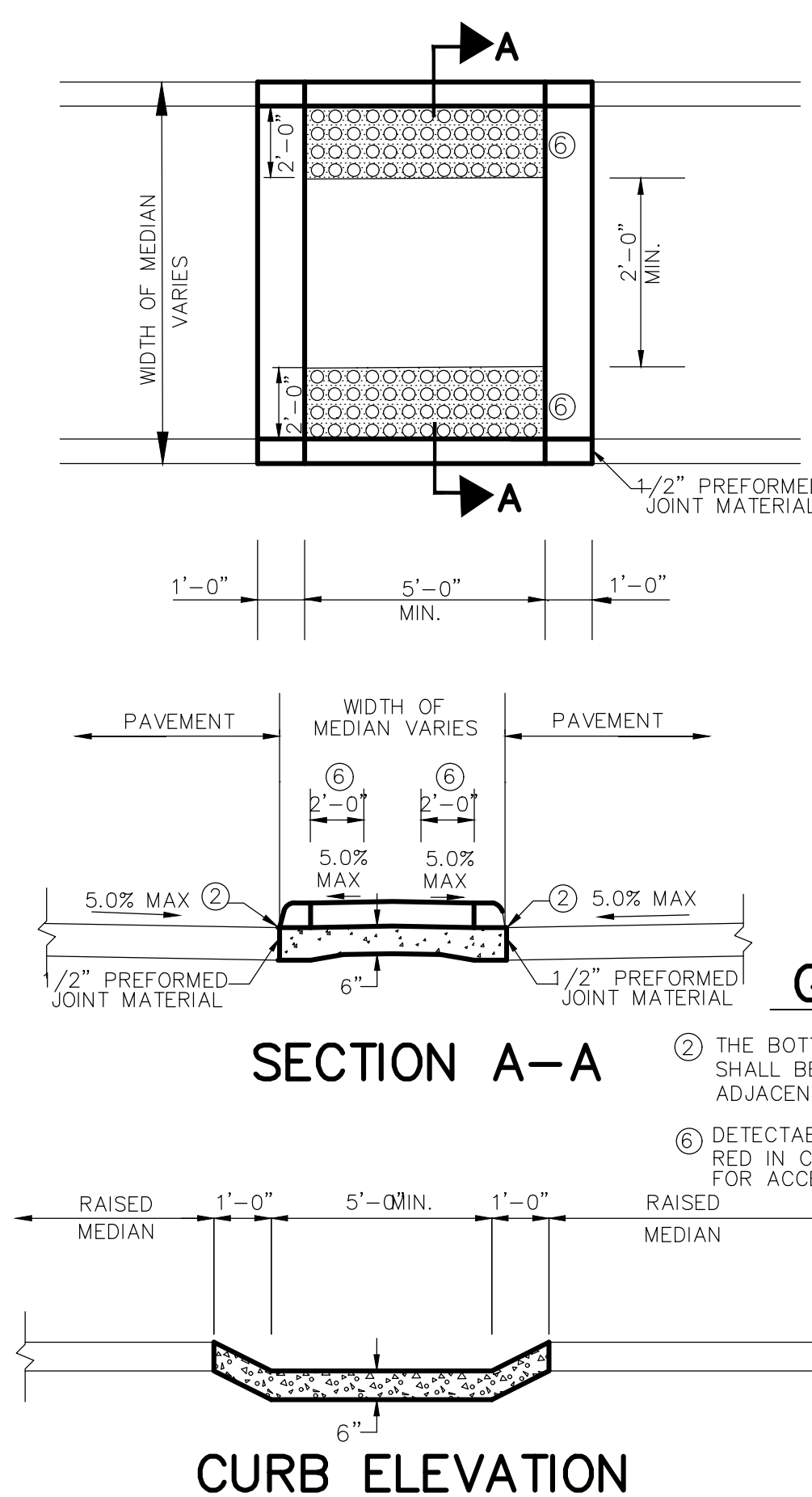
CURB RAMP TYPE A



GENERAL NOTES

- ① THESE DIMENSIONS ARE BASED ON A 6" CURB HEIGHT, AND SHALL BE PROPORTIONALLY ADJUSTED FOR OTHER CURB HEIGHTS.
- ② A 4'-0" MINIMUM WIDTH RAMP MAY BE USED WHEN EXISTING SPACE PROHIBITS THE CONSTRUCTION OF THE 5'-0" WIDE RAMP.
- ③ THE BOTTOM EDGE OF THE CURB RAMP SHALL BE FLUSH WITH THE EDGE OF THE ADJACENT PAVEMENT AND GUTTER LINE.
- ④ TURNING SPACE AT THE TOP OF CURB RAMP SHALL HAVE MAXIMUM CROSS SLOPE OF 2.0% IN ANY DIRECTION. WHERE SITE INFEASIBILITY PRECLUDES A TURNING SPACE SLOPE OF 2.0% IN ANY DIRECTION, THE SLOPE PERPENDICULAR TO THE CURB FACE SHALL NOT EXCEED 2.0%. A TURNING SPACE (4.0 FT) MINIMUM BY (4.0 FT) MINIMUM SHALL BE PROVIDED AT THE TOP OF THE CURB RAMP AND SHALL BE PERMITTED TO OVERLAP OTHER TURNING SPACES AND CLEAR SPACES. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK, THE TURNING SPACE SHALL BE (4.0 FT) MINIMUM BY (5.0 FT) MINIMUM. THE (5.0 FT) DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
- ⑤ THE RUNNING SLOPE OF THE CURB RAMP SHALL CUT THROUGH OR SHALL BE BUILT UP TO THE CURB AT RIGHT ANGLES OR SHALL MEET THE GUTTER GRADE BREAK AT RIGHT ANGLES WHERE THE CURB IS CURVED. THE RUNNING SLOPE OF THE CURB SHALL BE 5.0% MINIMUM AND 8.3% MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED (15.0 FT). THE RUNNING SLOPE OF THE TURNING SPACE SHALL BE 2.0% MAXIMUM.
- ⑥ DETECTABLE WARNING SURFACE SHALL BE BRICK RED IN COLOR. SEE SPECIAL PROVISIONS FOR ACCEPTED TACTILE WARNING UNITS.

CURB RAMP TYPE M

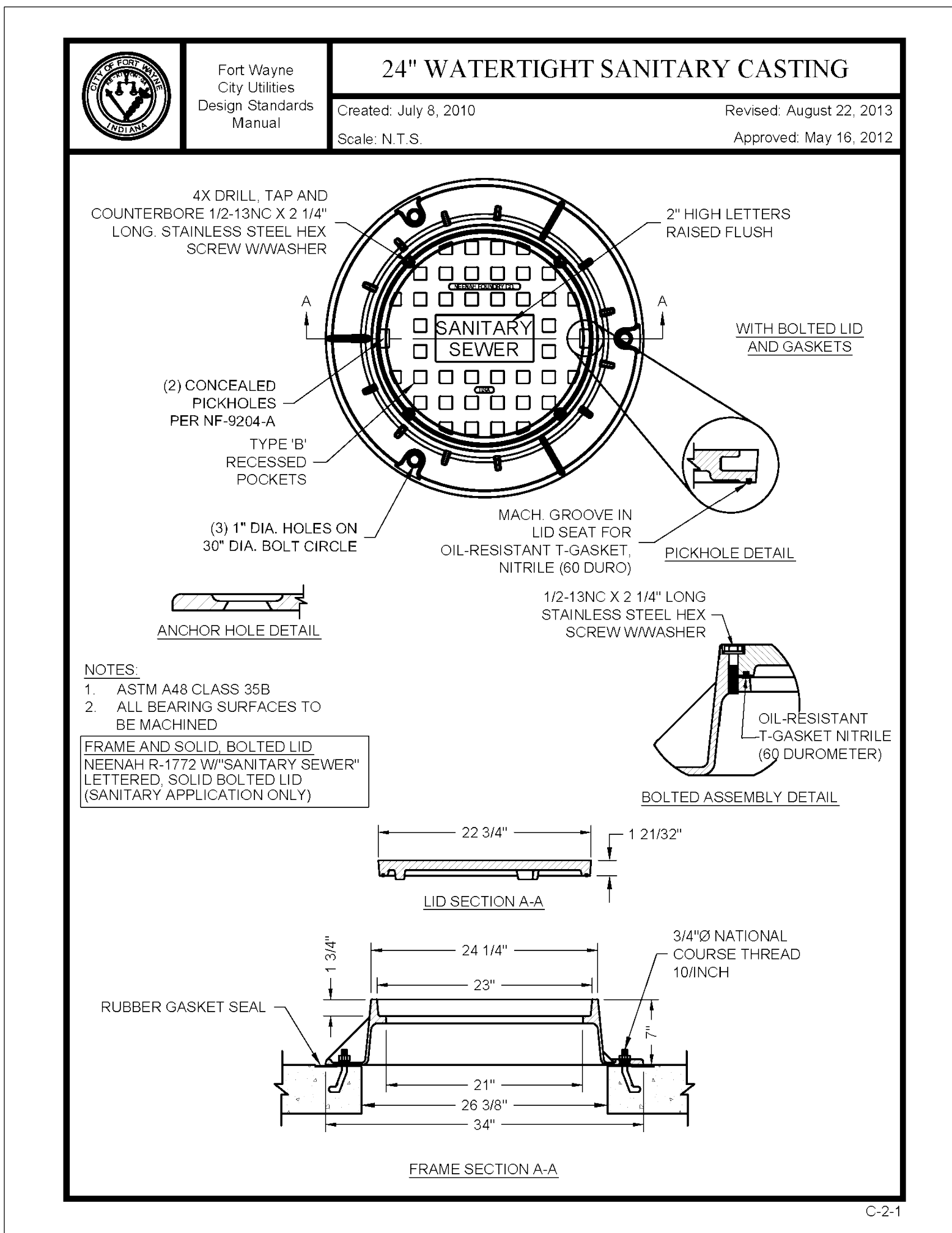
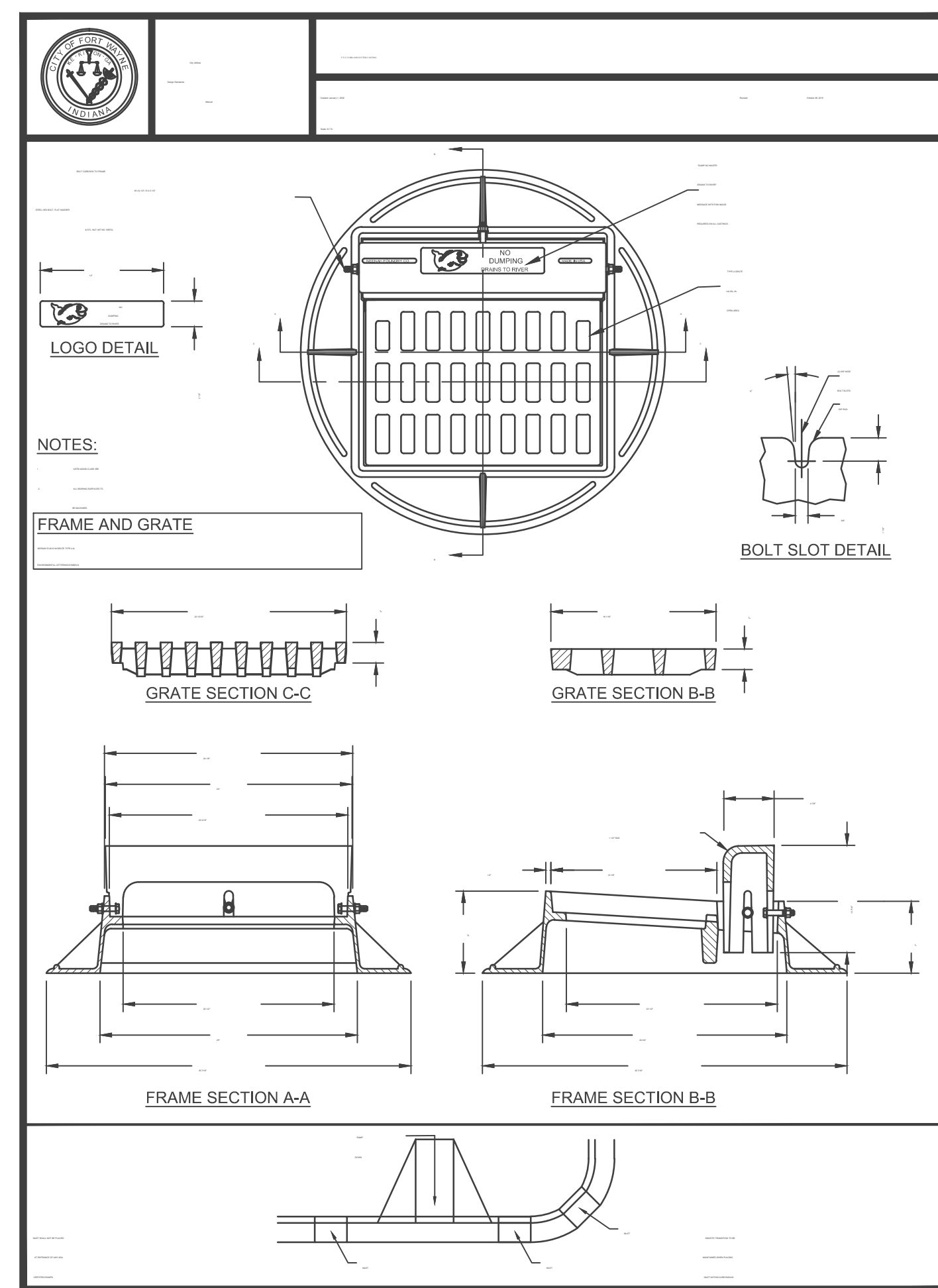
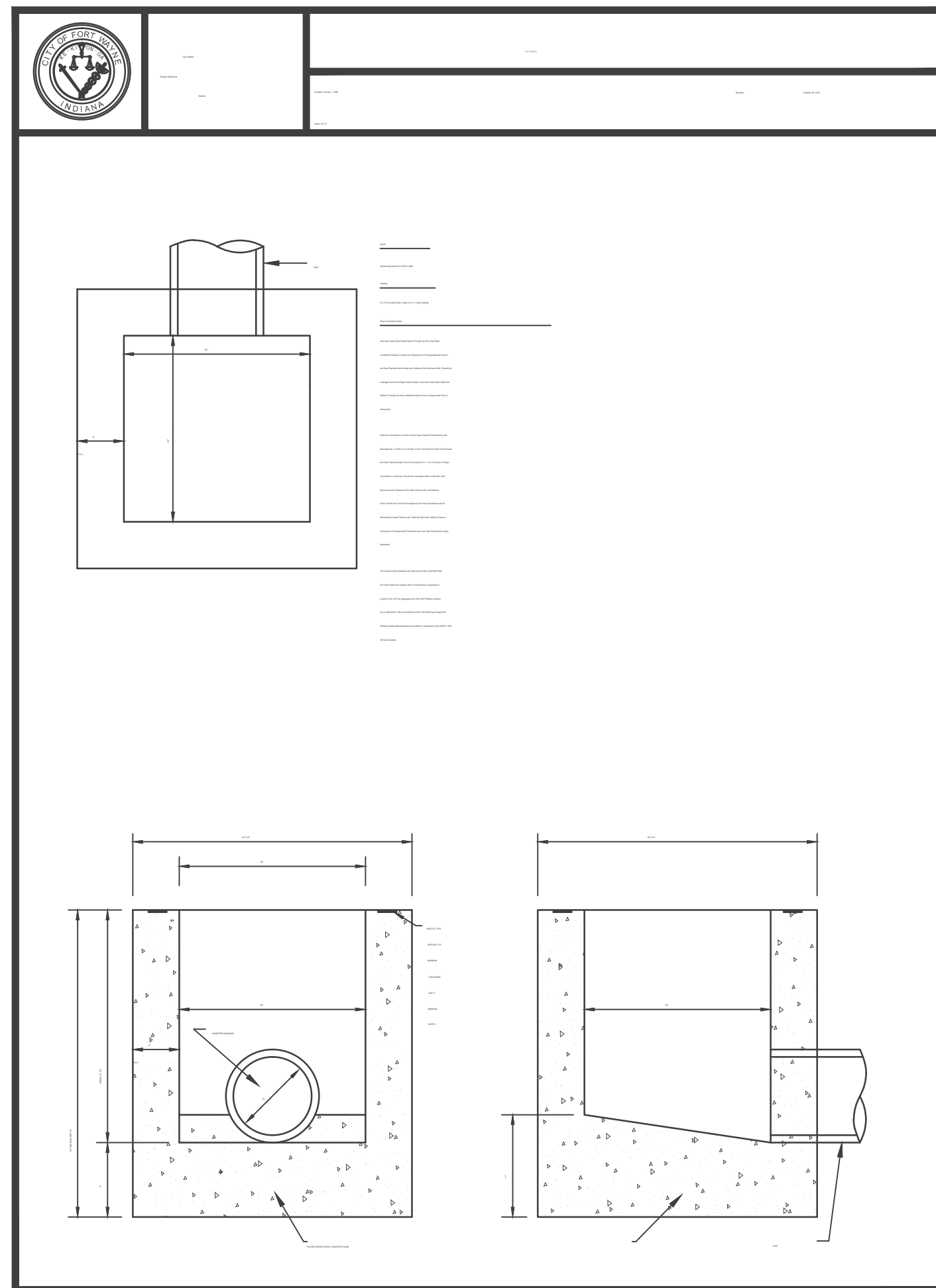
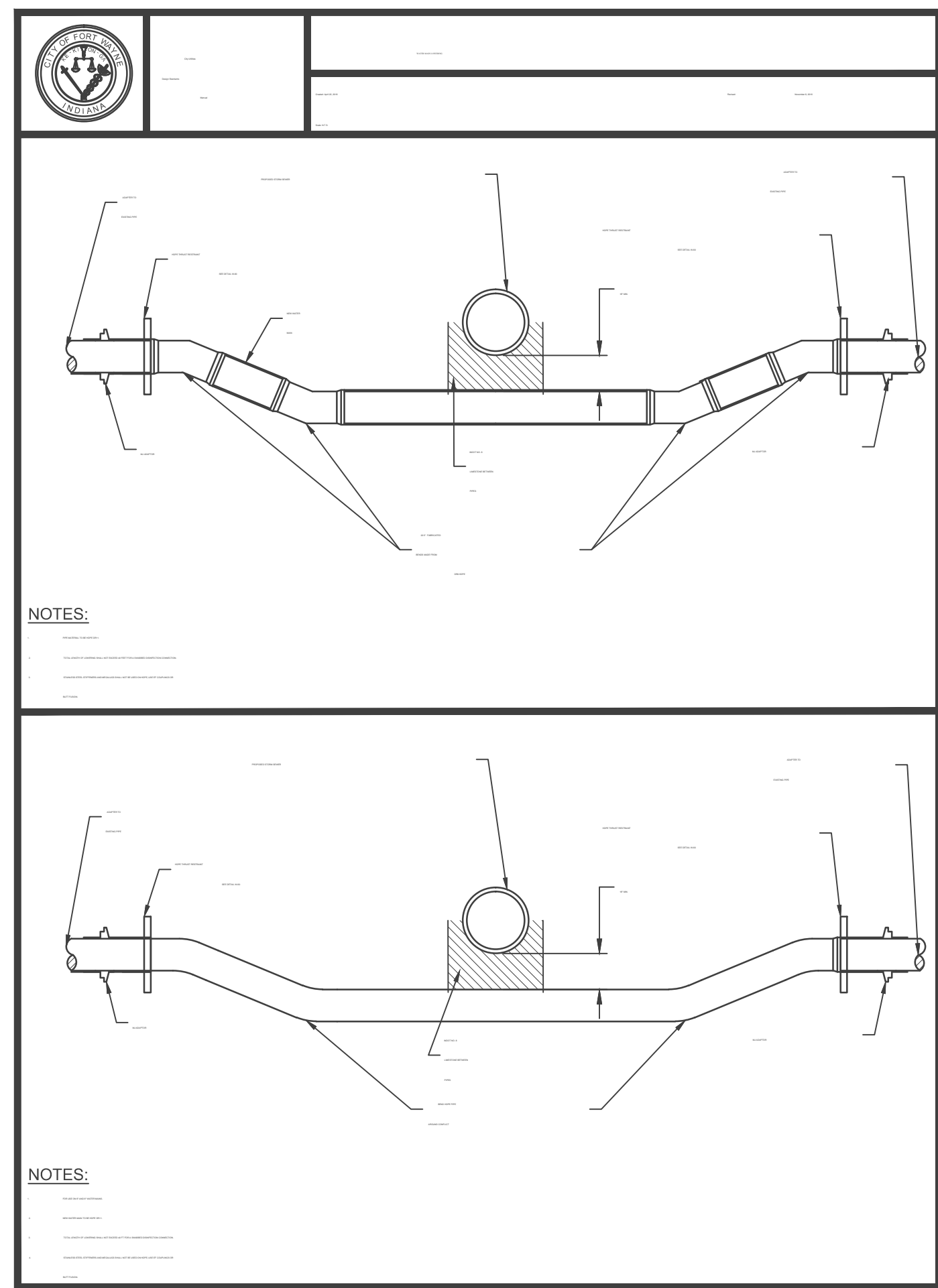


GENERAL NOTES

- ② THE BOTTOM EDGE OF THE CURB RAMP SHALL BE FLUSH WITH THE EDGE OF THE ADJACENT PAVEMENT AND GUTTER LINE.
- ⑥ DETECTABLE WARNING SURFACE SHALL BE BRICK RED IN COLOR. SEE SPECIAL PROVISIONS FOR ACCEPTED TACTILE WARNING UNITS.

**NORTH ANTHONY BOULEVARD
CRESENT AVE. TO COLISEUM BLVD.
DETAILS**

Designed by: M. GRAY	RES. W.O.	S.T.
Drawn by: R. REYNOLDS	0143C	
Checked by: M. TREVINO		Sheet 23 of 25



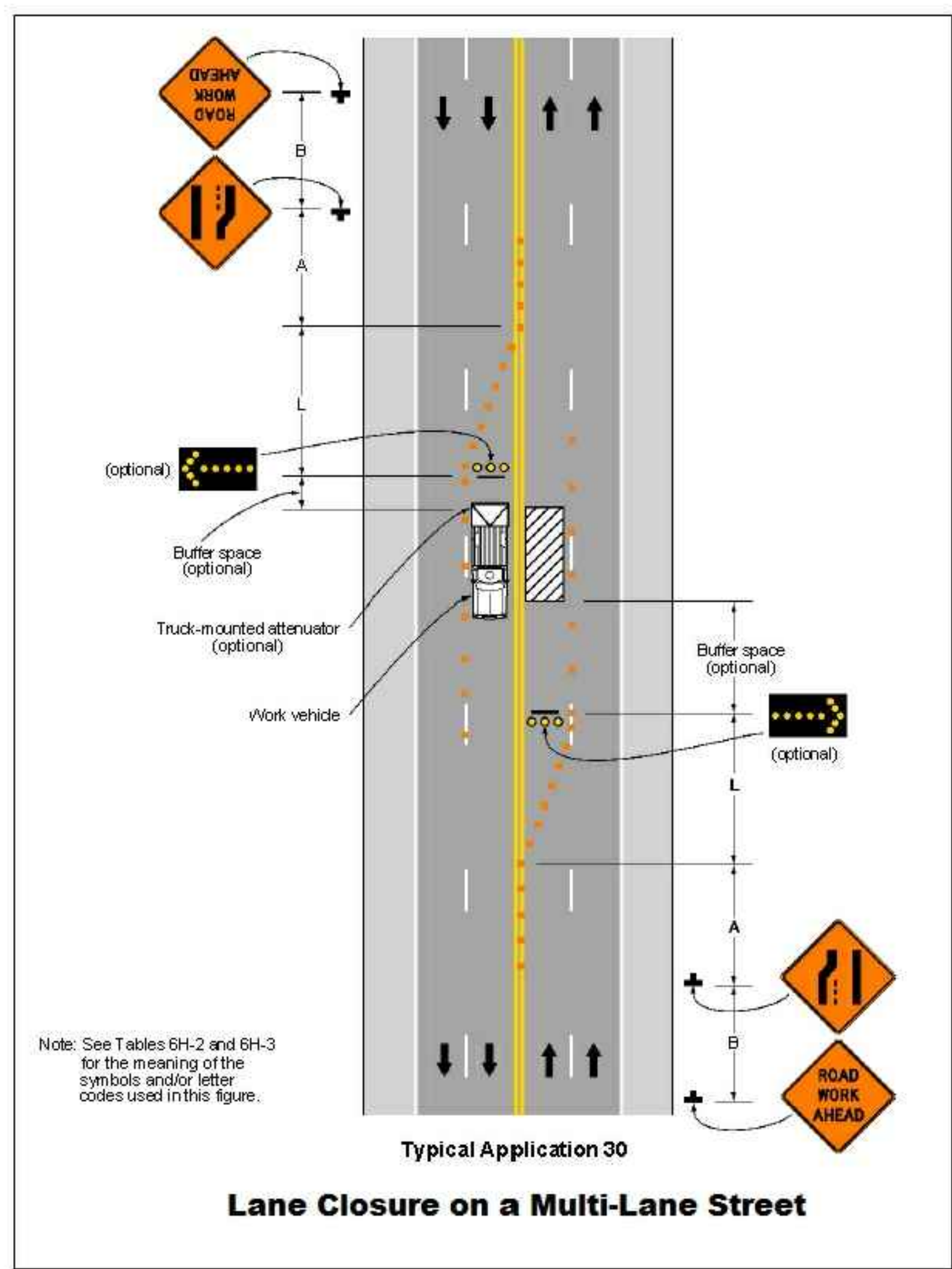
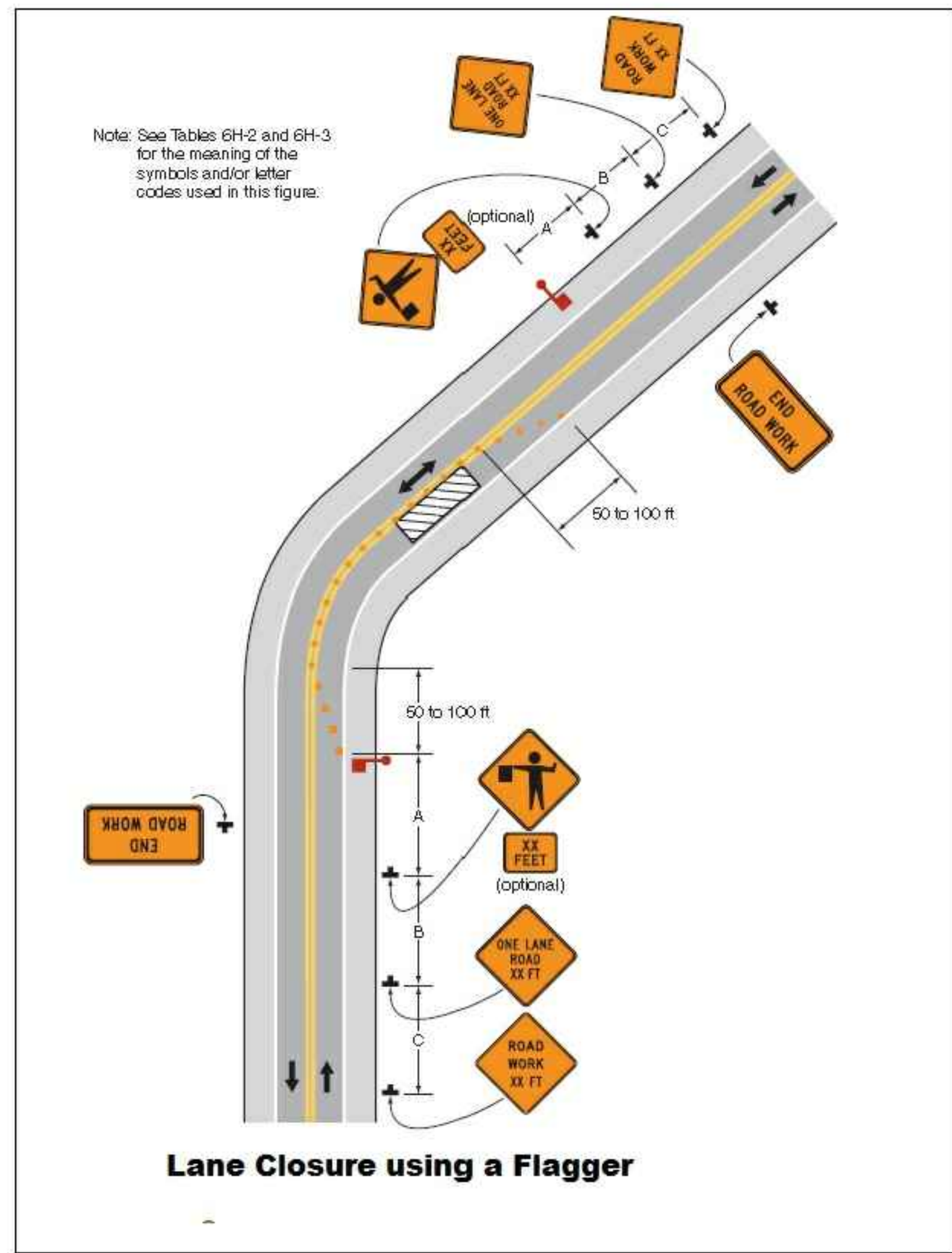


Table 6C-3. Taper Length Criteria for Temporary Traffic Control Zones

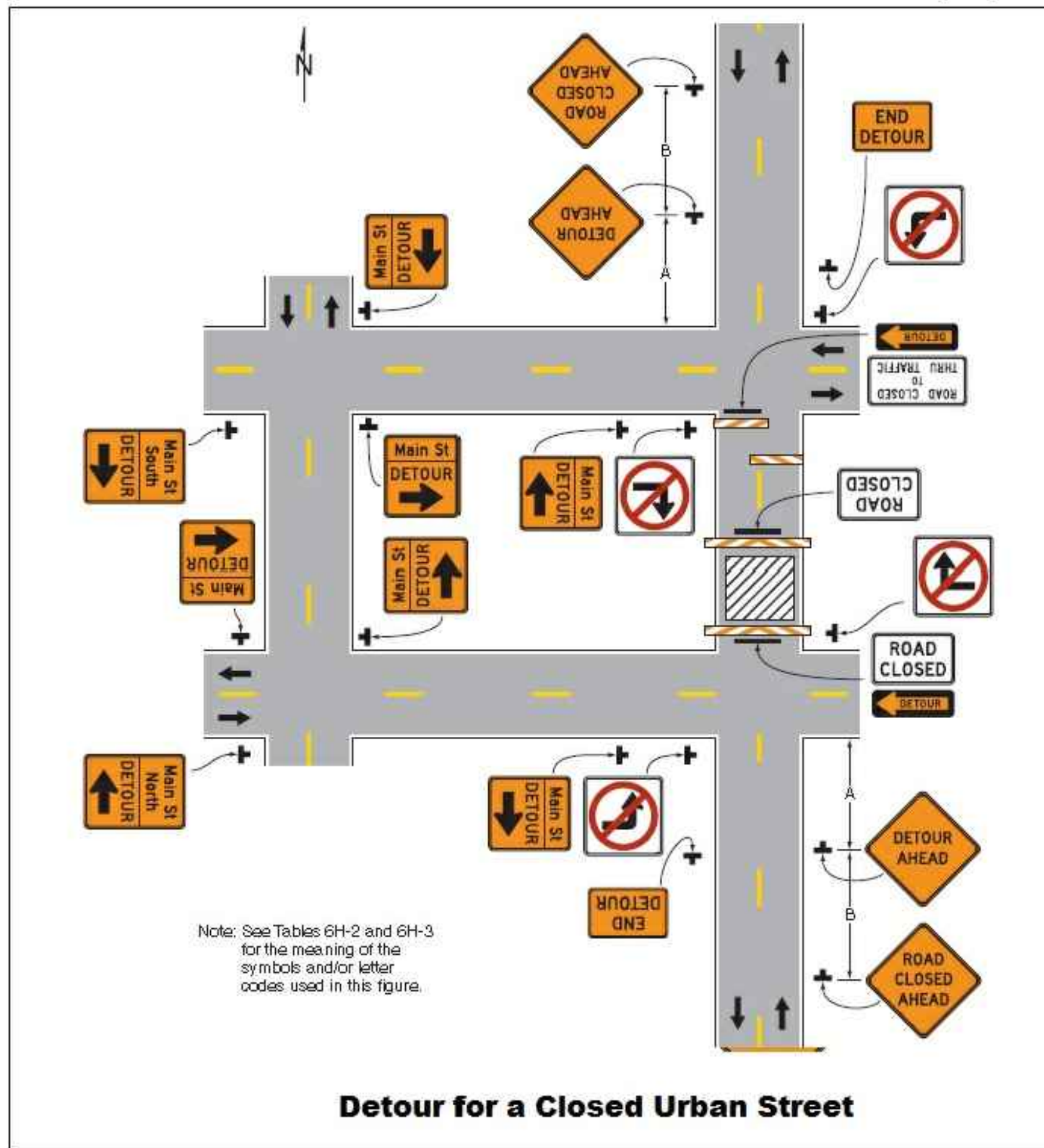
Type of Taper	Taper Length
Merging Taper	at least L
Shifting Taper	at least 0.5 L
Shoulder Taper	at least 0.33 L
One-Lane, Two-Way Traffic Taper	50 feet minimum, 100 feet maximum
Downstream Taper	50 feet minimum, 100 feet maximum

Note: Use Table 6C-4 to calculate L.

Table 6C-4. Formulas for Determining Taper Length

Speed (S)	Taper Length (L) in feet
40 mph or less	$L = \frac{WS}{60}$
45 mph or more	$L = WS$

Where: L = taper length in feet
 W = width of offset in feet
 S = posted speed limit, or off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph



Section 6C.10 One-Lane, Two-Way Traffic Control

Standard:

01 Except as provided in Paragraph 5, when traffic in both directions must use a single lane for a limited distance, movements from each end shall be coordinated.

Guidance:

02 Provisions should be made for alternate one-way movement through the constricted section via methods such as flagger control, a flag transfer, a pilot car, traffic control signals, or stop or yield control.

03 Control points at each end should be chosen to permit easy passing of opposing lanes of vehicles.

04 If traffic on the affected one-lane roadway is not visible from one end to the other, then flagging procedures, a pilot car with a flagger used as described in Section 6C.13, or a traffic control signal should be used to control opposing traffic flows.

Option:

05 If the work space on a low-volume street or road is short and road users from both directions are able to see the traffic approaching from the opposite direction through and beyond the worksite, the movement of traffic through a one-lane, two-way restriction may be self-regulating.

Considerations for Subdivision Streets

Project Engineer may direct the contractor to provide a "passing blister" zone within a 1-lane section of re-constructed roadway where 2-way traffic is being maintained. These areas may be within subdivisions or public streets with low traffic volumes as determined by the Project Engineer.

The following information on this sheet has been provided for contractor convenience and is taken from the Manual for Uniform Traffic Control Devices, Part 6.