

FEDERAL HIGHWAY ADMINISTRATION

FINDING OF NO SIGNIFICANT IMPACT

For

Indiana Project
Des. No. 0400587

State Boulevard, between Spy Run Avenue and Cass Street,
including the bridge over Spy Run Creek
City of Fort Wayne
Allen County, Indiana

An environmental assessment (EA) was approved by the Federal Highway Administration (FHWA) on May 14, 2014. This project provides for the construction of added travel lanes for State Boulevard in the City of Fort Wayne, Allen County, Indiana.

A public hearing was held on June 18, 2014 on the EA and the Draft Section 4(f). No revisions or modifications to the design of this project, which would alter the scope or intent of this project or would increase any impacts to either the natural or human environments, have been made.

The FHWA has determined that this project will have no significant impact on the natural and human environment. The Final Section 4(f) has been found legally sufficient. This Finding of No Significant Impact (FONSI) is based on the environmental assessment that has been independently evaluated by the FHWA and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project. It provides sufficient evidence and analysis for determining that an environmental impact statement is not required. The FHWA takes full responsibility for the accuracy, scope and content of the enclosed Environmental Assessment.

Date

3/2/15

Janice Osadczuk
Team Leader, FHWA



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

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Michael R. Pence, Governor
Karl B. Browning, Commissioner

February 26, 2015

Mr. Rick Marquis
Division Administrator
FHWA, Indiana Division
575 N. Pennsylvania St., Room 254
Indianapolis, Indiana 46204

Re: FONSI request packet prepared for State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587

Dear Mr. Marquis:

We would request the timely review of the attached information packet necessary for the preparation of the Finding of No Significant Impact (FONSI) regarding the aforementioned project pursuant to 40 CFR, Part 1500.4q and paragraph 5 of the DOT Order 5610.1C implementing the National Environmental Policy Act of 1969. This information packet includes the following documents:

1. Approved Environmental Assessment (Text Only)
2. Approved Additional Information Document (Text Only)
3. Final Section 106 Documentation
4. Final Section 4(f) Documentation
5. Official Public Hearing Transcript and the disposition of the comments received
6. Project Commitments

On May 14, 2014, the Environmental Assessment for this project was approved by the Federal Highway Administration and released for public involvement (Attachment 1 – page 2). A public hearing was held on June 18, 2014. Certification of Public Involvement was received on September 12, 2014 (Attachment 1 – page 2). An Additional Information (AI) document was prepared to evaluate the Consulting Parties Proposed Alternative (CPPA), as presented at the public hearing, and to provide information regarding properties acquired through the Fort Wayne Voluntary Floodplain Relocation Fund. The AI was approved on December 17, 2014 (Attachment 2 – page 5).

The Section 106 Area of Potential Effect (APE) Determination (36 CFR 800.4(a)(1)), and the Finding of Adverse Effect (36 CFR 800.6(a)(3)), was approved by Federal Highway on February 27, 2013 (Attachment 3 – page 3) and concurred with by the State Historic Preservation Officer (SHPO) on April 1, 2013 (Attachment 3 – pages 466-467). On December 12, 2014 an Addendum was prepared to the State Boulevard Reconstruction Project 800.11(e) Documentation to incorporate the CPPA (Attachment 3 – pages 503-518). The AI, Addendum and

Version 12/15/2014 of the Memorandum of Agreement (MOA) was transmitted to the SHPO and consulting parties in a letter dated December 22, 2014 (Attachment 3 – pages 500-502). The MOA (Version 12/15/2014) was executed on December 29, 2014, outlining the proposed mitigation for the adverse impacts the project will have on the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District (Attachment 3 – pages 520-530). The Bridge over Spy Run Creek falls within the scope of the Historic Bridge Programmatic Agreement (HBPA); and therefore, does not require an MOA for the adverse effect the project will have on the resource. The executed MOA was provided to the Advisory Council on Historic Preservation (ACHP) January 5, 2015 fulfilling FHWA's requirements of 36 CFR 800.11(f) (Attachment 3 – page 531).

The proposed undertaking will convert property from the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek, to a transportation use. The FHWA has determined the appropriate Section 106 finding is Adverse Effect for both districts and therefore, an Individual Section 4(f) evaluation was undertaken (Attachment 4 – pages 1-55). However, proposed impacts to the Bridge over Spy Run falls within the stipulations for the HBPA Section 4(f). In compliance with Section 4(f), pursuant to 23 CFR Part 774.5, the draft Section 4(f) documentation was provided to the US Department of Interior (DOI) for review and comment on December 30, 2014 (Attachment 4 – pages 56-57). In a letter dated February 12, 2015, the DOI concurred with the FHWA and INDOT on a determination of no feasible or prudent alternatives to the preferred alternative, if built as proposed, which would result in impacts to eligible properties (Attachment 4 – pages 58-59). The Individual Section 4(f) document was reviewed by FHWA and determined to be legally sufficient on February 18, 2015 (Attachment 4 – page 61).

In addition, one property, Vesey Park was also noted in the project limits as a Section 4(f) resource. The City of Fort Wayne Parks Department provided a letter in support of this project on January 23, 2013 (Attachment 4 – page 68). The project would most likely have a *de minimis* effect on Vesey Park, a Section 4(f) property, as it will not adversely affect the activities, features, and attributes that qualify it for protection under Section 4(f). There are no Section 6(f) resources associated with this project.

No revisions or modifications to the design of this project which would alter the scope or intent of this project or would increase any impacts to either the natural or human environments have been made. All commitments made in the Environmental Assessment and AI will be satisfied.

Upon the satisfactory completion of your review of the FONSI request information packet, we would request preparation of the necessary FONSI for this project in order to complete the NEPA process.

Sincerely,



Laura Hilden
Environmental Services Director
INDOT, Environmental Services Division

LEH/REB
Attachments

Attachment 1

Approved Environmental Assessment (Text Only) – Pages 1-37



AMERICAN
STRUCTUREPOINT
INC.

ENVIRONMENTAL ASSESSMENT
Des. No. 0400587

State Boulevard Reconstruction Project
State Boulevard between Spy Run Avenue and Cass Street
including the bridge over Spy Run Creek

City of Fort Wayne
Allen County, Indiana

Prepared for

City of Fort Wayne, Board of Public Works
Citizen Square
200 East Berry Street, Suite 240
Fort Wayne, Indiana 46802

Prepared by

American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

May 2, 2014

Indiana Department of Transportation

County Allen Route State Boulevard Des. No. 0400587 Project No. _____

FHWA-Indiana Environmental Document

CATEGORICAL EXCLUSION/ENVIRONMENTAL ASSESSMENT FORM GENERAL PROJECT INFORMATION

Road No./County:	State Boulevard/Allen County
Designation Number:	0400587 (Phase 1, Terrace Road to Spy Run Avenue -1005151, Phase 2, Cass Street to Terrace Road -1005154, Pedestrian Bridge-1005155, and Spy Run Creek Bridge-1005152)
Project Description/Termini:	State Boulevard Reconstruction Project- State Boulevard between Spy Run Avenue and Cass Street including the bridge over Spy Run Creek

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

<input type="checkbox"/>	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
<input type="checkbox"/>	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services).
<input type="checkbox"/>	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA.
<input checked="" type="checkbox"/>	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA.

Note: For documents prepared by or for Environmental Services, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

ESM Signature _____	Date _____	ES Signature <u><i>[Signature]</i></u>	Date <u>8 MAY 14</u>
<u><i>[Signature]</i></u>	Date <u>5-14-14</u>		
FHWA Signature _____	Date _____		

Release for Public Involvement

ESM Initials _____	Date _____	ES Initials _____	Date _____
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Certification of Public Involvement

<u><i>Mary Wright</i></u>	<u>9/12/14</u>
Examiner, Public Hearings Signature	Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

Name and organization of CE/EA Preparer: Briana M. Hope – American Structurepoint, Inc.

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Discuss what public involvement activities (legal notices, letters to affected property owners and residents, meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Survey notice letters were sent to adjacent property owners on March 18, 2009, informing them of the proposed project. Copies of the survey notice letters are included in Appendix F pages F-2 to F-3.

Public Information Meetings (5 Meetings)

A total of five public meetings have been held throughout the development of this project. Public information meetings were initiated by the City in 2008 and 2009 (September 11, 2008; October 27, 2008; November 17, 2008; May 28, 2009; and September 30, 2009) to solicit input from the public during the early design stages of the proposed project. The public meetings consisted of formal presentations and opportunities for public questions and comments. In addition, a design charrette was utilized as part of the September 30, 2009, public meeting to further aid the City of Fort Wayne in collecting information regarding how the connecting streets should intersect with the new State Boulevard. Information gathered from the charrette was evaluated and taken into consideration and incorporated into the proposed design. The public meetings were held in varying locations to allow the public to attend meetings that would be most convenient and easily accessible to them. Meetings were held at the City Building, Northside High School, and the Allen County Public Library.

Neighborhood Association Meetings (13 Meetings)

The City of Fort Wayne attended multiple neighborhood meetings to present project information and address project questions and concerns. In most cases, the City attended a regularly scheduled meeting held in the neighborhoods, but also met with individual representatives of associations when requested. As the Brookview Civic Neighborhood is located within the proposed project limits, the majority of the meetings involved this neighborhood association or individual representatives from the association. In an effort to help adjacent property owners better understand the proposed project, a representative from the project team met twice in the field to walk the proposed project with interested individuals from the Brookview neighborhood. In addition to the Brookview Civic Neighborhood, the City also met with neighborhood associations outside the limits of the project. The purpose of these meetings was to answer questions and concerns expressed about the project and discuss how they would be affected as they travel through the area whether by motorized vehicles or other modes of transportation. The additional neighborhood associates consisted of Northside Neighborhood Association, Historic Oakwood Neighborhood Association, West Central Neighborhood Association, Bloomingdale Neighborhood Association, and Forest Park Neighborhood Association.

Open House Events (3 Events)

The City of Fort Wayne conducted a series of three open house events to present preliminary renderings of the preferred alignment to the public. These meetings were held on February 25, 2013, from 5:00 PM to 7:00 PM at the Franke Pond Pavilion located at 3411 Sherman Boulevard, Franke Parke, Fort Wayne; on March 1, 2013, from 11:00 AM to 1:00 PM at the Main Branch Allen County Public Library, Meeting Room A, 900 Library Plaza, Fort Wayne; and on March 7, 2013, from 5:00 PM to 7:00 PM at the Psi Ote Barn - Lower Level, Bob Arnold Northside Park, located at East State Boulevard and Parnell Avenue, Fort Wayne. Renderings were also available for comment on the City of Fort Wayne website. Comments were accepted at the open house, on-line, via email, and US Postal Service. For reference to renderings presented, see Appendix F pages F-25 to F-32.

Other Group and Individual Meetings (27 Meetings)

When requested, the City of Fort Wayne met with individuals, including representatives of interested groups, business owners, and adjacent property owners. The City met with these individuals to help explain the project, provide project updates, and address comments and concerns. Meeting with these individuals and representatives further helped the City ensure information regarding the project was reaching the public. Representatives from the varying groups brought comments and concerns to the City and distributed project information to their groups.

See Appendix F page F-4 for a list of all meetings, dates, and locations.

Section 106 (3 Consulting Party Meetings)

The Section 106 Area of Potential Effect (APE) determination (36 CFR 800.4(a)(1)) and the Adverse Effect determination (36 CFR 800.11(e)) were approved by FHWA on February 27, 2013, and distributed to the State Historic Preservation Officer (SHPO) on March 1, 2013. Upon release for public involvement for this document, copies of both

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County Allen Route State Boulevard Des. No. 0400587 Project No. _____

this document and the approved Adverse Effect determination will be submitted to Consulting Parties for review. A public notice describing the project and the Section 106 finding of "Adverse Effect" will be published in local media in conjunction with the Legal Notice of Public Hearing.

The bridge over Spy Run Creek was advertised for reuse, per the Historic Bridges Programmatic Agreement (HBPA). A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-490 to C-496. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

In addition, three consulting party meetings were also held to discuss the findings of Historical Properties Report, effect findings, and options to avoid, minimize and mitigate adverse effects to the surrounding cultural resources. Meetings were held on December 15, 2009; September 1, 2011; and September 19, 2012. A total of 35 individuals, representing the FHWA, State, City, neighborhood associations, historic preservation groups, and adjacent property owners were invited to participate in the consulting party meetings.

For reference to consulting party consultation see Appendix C pages C-2 to D-476.

Public Hearing

The Indiana Department of Transportation (INDOT) Public Involvement Procedures Policy requires a public hearing be scheduled and held for projects classified as EAs. A Legal Notice of Public Hearing will be published twice in local media, and may be mailed via First Class US Mail to adjacent property owners and local or state officials whom may have an interest in the proposed project, and may be posted on the City of Fort Wayne website. The EA will be made available for public review. Comments will be accepted for 30 days following the hearing. The public hearing will include an informal open house, formal presentation, and comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

A public notice describing the project and the Section 4(f) *de minimis* finding associated with Vesey Park will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes

☒

No

☐

Remarks:

During the preliminary project development, multiple citizens and consulting parties have expressed their opposition to the proposed project and the proposed impacts associated with the identified cultural resources and the overall footprint of the project.

As part of the Section 106 process, multiple consulting parties have expressed their concern associated with the project purpose and need as well as the magnitude of potential impact the preferred alternative would have on the identified historic resources within the project area.

Opportunity for a Public Hearing Required

Yes

☒

No

☐

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Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: City of Fort Wayne INDOT District: Fort Wayne
Local Name of the Facility: State Boulevard

Funding Source: ☒ Federal ☐ State ☒ Local ☐ Private

PURPOSE AND NEED:

Describe the problem that the project will address.

The purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity, and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (1-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run Avenue and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one westbound lane. East of Clinton Street, State Boulevard is a 2-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stellhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

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Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E
Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E
Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements, which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 feet. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards. For further reference to the IDM see http://www.in.gov/indot/design_manual/design_manual_2013.htm.

Curve Radius Table:

Station Line "A"	Existing Curve Radius	Required Radius (30 mph)
18+66.60	175 feet	300 feet
24+64.47	243 feet	300 feet
27+23.73	210 feet	300 feet

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway's safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 feet along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 feet along State Boulevard to safely make the right turn maneuver. As noted in the "Intersection Sight Distance Table" below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approximate Existing Sight Distance (feet)	Required Sight Distance (feet)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Drive (South)	LT	150	420
Westbrook Drive (North)	LT	210	420
Eastbrook Drive (South)	LT	270	420
Eastbrook Drive (South)	RT	210	375
Eastbrook Drive (North)	LT	250	420
Terrace Road (North)	RT	160	375

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Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top 20 high crash locations in Allen County for the time period 2007-2011. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0, which indicates that a safety problem exists.

Crash Location	2 0 0 7				2 0 0 8				2 0 0 9				2 0 1 0				2 0 1 1			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard and Eastbrook Drive	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11	9	1	0	1.26	12	3	0	1.69
State Boulevard and Clinton Street	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38	30	3	0	2.04	36	8	0	2.45
State Boulevard And Spy Run Avenue	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48	27	7	0	1.63	43	11	0	2.60
State Boulevard and Westbrook Drive	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16	9	1	0	1.26	12	3	0	1.69

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System, which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9, which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is five years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Creek Flood Control Study (Christopher B. Burke, 2005), "this flooding is caused primarily by backwater from the St. Mary's River, which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Seven out of the 17 years (1978, 1982, 2003, 2004, 2005, 2008, and 2009), State Boulevard was closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

Indiana Department of Transportation

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PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Allen
Municipality: Fort Wayne
Limits of Proposed Work: State Boulevard between Spy Run Avenue and Cass Street in Fort Wayne
Total Work Length / Area: 0.45 Miles

Is an Interchange Modification Study/Interchange Justification Study (IMS/IJS) required?
If yes, when did the FHWA grant a conditional approval for this project?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the Remarks box below, describe in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The current preferred alternative is Alternative 3A. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. The overall alternative length is 2,370 feet. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary. New curb inlets and storm sewer would be constructed throughout the project limits. A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

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OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Alternative 1: Butler Road – Vance Road Corridor: This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts to the Franke Parke Elementary School and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project; however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

Alternative 1 results in the use of the Brookview-Irvington Historic District (northern extents), Vesey Park, and Franke Park, all Section 4(f) resources.

Alternative 1 is not reasonable as it does not address any of the Project's purpose and need. Alternative 1 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential and commercial relocations for construction and approximately 2.0 miles of new roadway through existing forested land. For these reasons, Alternative 1 has been eliminated from further consideration.

Alternative 2: Spring Street – Tennessee Avenue: This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places (NRHP).

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central museum, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project; however, the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative would result in the use of 4(f) resources including Lakeside Park, Lawton Park, and the NRHP eligible bridge over the St. Joseph River.

The alternative is not reasonable as it does not address any part of the Project's purpose and need. Alternative 2 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential, commercial, and recreational property impacts/relocations for construction. For these reasons, Alternative 2 has been eliminated from further consideration.

Alternative 3B: Widen State Boulevard on Existing Alignment: This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four lanes. This alternative would require a new bridge with additional travel

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lanes over Spy Run Creek. The overall alternative length is 2,700 feet.

This alternative would require approximately 18 residential relocations (contributing properties) from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. Furthermore, this alternative requires a higher number of residential and historic property relocations for construction as compared to other alternatives.

Alternative 3C: Shift State Boulevard Alignment South: This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4 lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. The existing intersection of State Boulevard with Eastbrook Drive would be eliminated and converted to a cul-de-sac. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Terrace Road and State Boulevard. The Terrace Road extension would be required to provide access to the neighborhood north of existing State Boulevard as a result of access restrictions due to Clinton Street being a one-way south roadway. This alternative would also require a new bridge over Spy Run Creek at an elevation seven feet above the existing bridge elevation.

Similar to Alternative 3A, the realignment of State Boulevard and change in elevation would result in the bifurcation of the Brookview-Irvington Park Historic District. Contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the existing bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of Eastbrook Drive (north of State Boulevard) to a cul-de-sac, the replacement of the bridge over Spy Run Creek, and the removal of five contributing properties, the landscape of the area would be modified altering the character and setting of the district. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad will also change the character of the district along State Boulevard. Furthermore, the realignment of State Boulevard would require the acquisition of right-of-way from the Fort Wayne Park and Boulevard System Historic District, again altering the historic location of State Boulevard. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). The prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over the contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing new visual element to the Fort Wayne Park and Boulevard System Historic District.

While this alternative would reduce the number of contributing property relocations on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Boulevard would cross Spy Run Creek; impacts to the creek would be increased by approximately 330 linear feet for the purposes of re-grading. The new bridge length would be approximately 250 feet longer than the bridge design included in Alternatives 3A or 3D. This alternative would also require construction of a new intersection of State Boulevard with Clinton Street. The new intersection would be built in close proximity to the new Terrace Road intersection which would significantly impede traffic operations and efficiency as well as increase project costs due to additional traffic signal work. The increased length of the proposed bridge combined with relocating the roadway south would also require the intersection of State Boulevard and Clinton Street to be raised two to three feet, thus causing additional reconstruction along Clinton Street (approximately 500 feet) and further increasing project costs. In addition to the nine residential relocations that are also considered contributing resources, this alternative would result in the relocation of four commercial businesses, including the gas station at the southwest corner of Clinton Street and State Boulevard, a plumbing business on the southeast corner, a dog grooming business located just south of the gas station, and a storage unit business located on the southwest corner of Spy Run Avenue and State Boulevard.

Alternative 3C addresses the project’s congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street which would create new operational and safety issues due to its close proximity to the new Terrace Road intersection. Project costs associated with Alternative 3C are an estimated five million dollars more than any other alternative due to increased impacts to commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street.

Alternative 3D: Substandard Horizontal Curve Correction with a 3-Lane Typical Section: This alternative is similar to Alternative 3A but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes and correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have two ten foot travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes would be separated by a twelve-foot wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes would be separated by a twelve foot two way left turn lane. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four eleven foot travel lanes, two in each direction, separated by a twelve foot two way left turn lane. As appropriate, left turn lanes would be

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installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway. As a part of this project, the new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

By reducing the typical section from 4-lanes (Alternative 3A) to 3-lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this alternative would continue to result in the same 4(f) use as Alternative 3A to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run Creek.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3D does not fully address corridor connectivity or traffic congestion concerns along the corridor. This alternative would not address the congestion concerns at the intersection of State Boulevard and Clinton Street. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within an urban area. This intersection currently functions at a low Level of Service. Alternative 3D would not address the poor Level of Service (E/F) at State Boulevard and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion along the corridor, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain. Furthermore, this alternative would result in the same use of 4(f) resources as compared to Alternative 3A.

Alternative 4: No Build: With the No Build Alternative, there would be no use of resources subject to Section 4(f) provisions. This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate. The existing roadway would continue to flood causing continued problems with accessibility and pavement deterioration. Traffic accidents would most likely continue to increase as the current congestion issues would not be addressed. The existing bridge over Spy Run Creek is currently rated structurally deficient and the estimated remaining life of the superstructure is five years. This structure is in immediate need of replacement due to the condition. East-west connectivity would continue to be a problem for the overall transportation network. The no build alternative would likely result in the complete failure of the structure over Spy Run Creek.

The No Build Alternative would not meet any of the needs of the project; therefore, is not considered a feasible and prudent alternative.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems, or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe)

X
X
X
X

ROADWAY CHARACTER:

Functional Classification:	<u>Minor Arterial</u>			
Current ADT:	<u>20,650</u>	<u>VPD 2009</u>	Design Year ADT:	<u>26,200</u> <u>VPD 2030</u>
Current Year DHV	<u>1,730</u> <u>VPH</u>	Trucks (%) <u>2</u>	Design Year DHV	<u>2,620</u> <u>VPH</u> Trucks (%) <u>2</u>
Designed Speed (mph):	<u>35</u>	Legal Speed (mph):	<u>30</u>	

<p>Existing</p> <p>Number of Lanes: <u>2</u></p> <p>Type of Lanes: <u>Through Travel Lanes</u></p> <p>Pavement Width: <u>10</u> ft.</p> <p>Shoulder Width: <u>NA</u> ft.</p> <p>Median Width: <u>NA</u> ft.</p> <p>Sidewalk Width: <u>5</u> ft.</p>	<p>Proposed</p> <p><u>5</u></p> <p><u>4 through travel lanes and 1 left turn lane when required</u></p> <p><u>10-11</u> ft.</p> <p><u>NA</u> ft.</p> <p><u>8</u> ft.</p> <p><u>6 - 10</u> ft.</p>
--	--

Setting: <input checked="" type="checkbox"/> Urban	Setting: <input type="checkbox"/> Suburban	Setting: <input type="checkbox"/> Rural
Topography: <input checked="" type="checkbox"/> Level	Topography: <input type="checkbox"/> Rolling	Topography: <input type="checkbox"/> Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES:

Structure Number(s): Allen County Bridge No. 00546 Sufficiency Rating: 27.9 (2006 Allen County Structure Inventory and Appraisal Report)

	Existing	Proposed
Bridge Type:	Concrete Girder	Continuous Composite Prestressed Concrete Box Beam
Number of Spans:	<u>1</u>	<u>3</u>
Weight Restrictions:	<u>NA</u> ton	<u>NA</u> ton
Height Restrictions:	<u>NA</u> ft.	<u>NA</u> ft.
Curb to Curb Width:	<u>24</u> ft.	<u>56</u> ft.
Outside to Outside Width:	<u>26</u> ft.	<u>85.83</u> ft.
Shoulder Width:	<u>1</u> ft.	<u>2</u> ft.
Length of Channel Work:	<u>NA</u> ft.	<u>270</u> ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks: The existing bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010). The Bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

The proposed bridge over Spy Run Creek would be a three span, continuous, composite, prestressed concrete box beam structure. The proposed span lengths are 28 feet, 58 feet, and 28 feet. The structure would have a total bridge width of 85 feet and 10 inches, and would be comprised of four 10-foot travel lanes, a 12-foot left turn lane, with 2-foot shoulders. In addition, a 16-foot, 2-inch wide sidewalk on the north side and a 12-foot, 8-inch wide sidewalk on the south side are also proposed. The clear roadway width is 56 feet and the proposed structure would be skewed 30-degrees to the left.

Will the structure be rehabilitated or replaced as part of the project? Yes ☒ No ☐ N/A ☐

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One (1) travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.

From Clinton Street to Spy Run Avenue, 2-way traffic will be maintained on the existing westbound lanes of existing State Boulevard while the proposed east bound lanes are being constructed. Once the eastbound lanes are built, 2-way traffic will be maintained on the newly constructed eastbound lanes until the proposed west bound lanes are constructed.

From Westbrook Drive to Clinton Street, 2-way traffic will be maintained on the existing roadway and bridge structure while the new alignment portions of the eastbound State Boulevard lanes and bridge structure are constructed to the south of the existing alignment. Once the eastbound portion of proposed State Boulevard is constructed, 2-way traffic will be maintained on the proposed eastbound lanes while the westbound lanes and remaining bridge structure are constructed.

From Cass Street to Westbrook Drive, 2-way traffic will be maintained on the westbound lanes of existing State Boulevard while the eastbound lanes are being constructed. Temporary asphalt pavement widening may be required on the northern side of State Boulevard between Cass Street and Westbrook Drive to accommodate 2-way traffic. Once the proposed eastbound lanes are constructed, 2-way traffic will be maintained on the eastbound lanes while the westbound lanes are being constructed.

MOT plans were included as part of the plan sets made available for public review at the three open house events hosted by the City (February 25, 2013, March 1, 2013, and March 7, 2013). No comments or concerns have been received regarding the MOT plan.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 1,062,295 Right-of-Way: \$ 2,300,000 Construction: \$ 1,500,000/6,572,000
(FY 2015) (FY 2017/2018)

Anticipated Start Date of Construction: April 1, 2015

Date project incorporated into STIP July 11, 2013

If in an MPO area, location of project in TIP on pages 42, 43, and 51* which was incorporated by reference into The STIP on July 11, 2013

*Administrative modification processed for project to account for the change in Year of Expenditure for Right-of-Way and Construction costs.

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Agricultural	0.00	0.00
Commercial	1.06	0.57
Forest	0.00	0.00
Industrial	0.00	0.00
Other	0.00	0.00
Other: Park	0.55	0.12
Residential	2.19	1.81
Wetlands	0.00	0.00
TOTAL	3.80	2.50

Remarks: Approximately 3.80 acres of additional permanent right-of-way will be acquired for the construction of the proposed project. Existing right-of-way currently extends approximately 25 feet from the centerline on both sides of State Boulevard. The right-of-way to be acquired will be primarily residential; however, some right-of-way will also be acquired from commercial areas. Acquisition of 15 whole parcels is anticipated as part of the proposed project. Acquisition of 15 residential structures is anticipated.

Approximately 2.50 acres of temporary right-of-way will be acquired for grading, driveway construction, and tie-ins. Project plans, including existing and proposed right-of-way limits, are included in Appendix A pages A-11 to A-129 of

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this document.

All right-of-way will be acquired in accordance with applicable federal and state procedures. Those procedures include specific requirements for appraisals, review appraisals, negotiations, and relocation benefits. Compliance with these procedures will assure the fair and equitable treatment of affected residents and businesses. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 as amended.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Streams, Rivers, Watercourses & Jurisdictional Ditches	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
State Wild, Scenic or Recreational River	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

There is one stream located within the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. One stream, Spy Run Creek, was identified as potential "waters of the US". Defined bed and bank were observed to be associated with Spy Run Creek. An ordinary high water mark (OHWM) was estimated at a depth of 1.5 feet. Spy Run Creek flows south through the project area under existing State Boulevard eventually outletting into the Saint Mary's River.

This stream is not a state natural, scenic, or recreational river. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E pages E-2 to E-12.

Based on the preliminary project design, avoidance of all waterways is not possible. The bridge carrying State Boulevard over Spy Run Creek will completely span the ordinary high water mark; however, impacts as a result of storm water outfalls, existing bridge removal, and channel grading are unavoidable. The total permanent impacts to waterways associated with the project are 292 linear feet and include a temporary crossing for construction, storm water outfalls, and stream bank stabilization for erosion control purposes.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Other Surface Waters		<input checked="" type="checkbox"/>		
Reservoirs		<input checked="" type="checkbox"/>		
Lakes		<input checked="" type="checkbox"/>		
Farm Ponds		<input checked="" type="checkbox"/>		
Detention Basins		<input checked="" type="checkbox"/>		
Storm Water Management Facilities		<input checked="" type="checkbox"/>		
Other: _____		<input checked="" type="checkbox"/>		

Remarks:

There are no other surface waters located in the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E, pages E-2 to E-12.

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Presence

Yes

No

Impacts

Yes

No

Wetlands

☐
☒
☐
☐

Total wetland area: 0 acre(s) Total wetland area impacted: 0 acre(s)
(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Impacted Acres – Permanent	Impacted Acres - Temporary	Total Impacted Acres	Comments
Totals:					

Wetlands

Wetland Determination

Wetland Delineation Report

USACE Isolated Waters Determination

Mitigation Plan

Documentation

Yes

No

☒
☒
☐
☐
☐
☐
☒
☒

ES Approval Dates

LPA Project/Red Flag

LPA Project

Jurisdiction for all waterways will be given to the USACE

Individual

Wetland

Finding

Yes

No

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

☐
☐
☐
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☐
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☐

Measures to avoid, minimize and mitigate wetland impacts need to be discussed in the remarks section

Remarks:

There are no wetlands located in the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E pages, E-2 to E-12.

Presence

Yes

No

Impacts

Yes

No

Terrestrial Habitat

☒
☐
☒
☐

Use the remarks table to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Terrestrial habitat within the project corridor includes residential yard and grassed passive park along Spy Run Creek. Approximately 2.19 acres of residential property and 0.55 acre of grassed passive park are located within the project study area and will be impacted by the proposed project. None of these areas are considered significant or sensitive habitat.

The US Fish and Wildlife Service (USFWS), in their April 20, 2009, early coordination response letter provided comments relative to impacts to wetlands, streams, and forested areas. USFWS indicated they felt shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore, trees lost to the project should be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail. The USFWS letter also indicated there is no known habitat for any endangered species within the project area and stated the project is not likely to adversely affect endangered species. For reference to this coordination see Appendix B, page B-15 TO B-16.

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Appropriate stormwater best management practices will be implemented as part of the project and stormwater collection system. In addition, a landscaping plan is proposed as part of this project. The landscaping plan will help address the replacement of trees removed from residential yards and along the Spy Run Creek corridor. Trees will be planted along the proposed roadway and remaining green spaces in an effort to mitigate for the anticipated loss of trees as well as to help preserve the park like appearance currently associated with this segment of State Boulevard.

Coordination with the IDNR on November 18, 2009, recommended appropriate sediment and erosion control measures and restrictions to minimize impacts to fish, wildlife, and botanical resources. IDNR stated the Natural Heritage Program's data indicated no plant or animal species listed as state or federally threatened, endangered, or rare have been reported in the project vicinity. For reference to this coordination see Appendix B, page B-19.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?

Are karst features located within or adjacent to the footprint of the proposed project?

If yes, will the project impact any of these karst features?

Yes No

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Use the remarks table to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

The project is located outside of the designated karst area of the state as identified in the October 13, 1993, Memorandum of Understanding (MOU). No karst features were observed or are known to exist within or adjacent to the proposed project area. The 1993 Karst MOU is not applicable to this project, and a karst assessment is not required. Project location mapping is included in Appendix A, page A-2. No karst features were noted on the Red Flag Investigation Mapping included in Appendix D pages D-2 to D-14.

Threatened or Endangered Species

Within the known range of any federal species?

Any critical habitat identified within project area?

Federal species found in project area (based upon informal consultation)?

State species found in project area (based upon consultation with IDNR)?

Is Section 7 formal consultation required for this action?

Presence
Yes No

Impacts
Yes No

<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

Coordination with the IDNR on November 18, 2009, confirmed the Natural Heritage Database has been checked and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project's vicinity. See Appendix B, page B-19 for reference to the IDNR coordination letter.

Coordination with the USFWS on April 20, 2009, indicated the proposed project area is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the candidate eastern massasauga rattlesnake (*Sistrurus catenatus*). Re-coordination with USFWS on March 19, 2014, indicated that the endangered species in All County, Indiana had been revised. In addition to the previously identified species, Allen County is now within the range of the Federally endangered rayed bean mussel (*Villosa fabalis*) and the proposed endangered northern long-eared bat (*Myotis septentrionalis*). There is no known habitat for any of these species within the proposed project area; therefore, the proposed project is not likely to adversely affect these endangered, proposed endangered, and candidate species. In addition, both the April 20, 2009 and March 19, 2014 USFWS coordination stated "this precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation." See Appendix B, page B-15 to B-16 and B-25 to B-26 for reference to the USFWS coordination letters.

SECTION B – OTHER RESOURCES

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County Allen Route State Boulevard Des. No. 0400587 Project No. _____

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Drinking Water Resources				
Sole Source Aquifer (SSA)		X		
Is the Project in the St. Joseph Aquifer System?		X		
Is the FHWA/EPA SSA MOU Applicable?		X		
Initial Groundwater Assessment Required?		X		
Detailed Groundwater Assessment Required?		X		
Source Water Protection Area(s)		X		
Public Water System(s)	X			X
Residential Well(s)		X		
Wellhead Protection Area		X		

Remarks:

The proposed project is located in Allen County; therefore, the project is not located within the area of the St. Joseph Sole Source Aquifer the only legally designated sole source aquifer in Indiana. The FHWA/EPA Sole Source Aquifer MOA is not applicable to this project, and a groundwater assessment is not required.

Review of the Wellhead Proximity Locator (<http://idemmaps.idem.in.gov/whpa/>) on March 15, 2013, indicated the proposed project area is not located in a wellhead protection area.

Drinking water is provided by the City of Fort Wayne within the project area. Existing water mains will be replaced as necessary throughout the project corridor.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Flood Plains				
Longitudinal Encroachment		X		
Transverse Encroachment	X			X
Is the project located in a FEMA designated floodplain?	X			X
Homes located in floodplain within 1000' up/downstream from project.	X			X

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

Per the INDOT Categorical Exclusion manual, the proposed project includes a new bridge on new alignment, and is therefore considered a Category 5 project.

A hydraulic design study has been performed by American Structurepoint, and concluded that the project will meet all requirements of the Indiana Design Manual, and may therefore be considered to have no adverse impact on the floodplain. This hydraulic study was approved on May 13, 2010, by INDOT Hydraulics Section. A summary of this study is included in Appendix E, pages E-13 to E-17.

There will be no substantial impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evaluation routes; therefore it has been determined that this encroachment is not substantial. A map depicting the mapped DFIRM flood plain boundaries is included in Appendix E, pages E-11 to E-12.

Formal permit approval of the IDNR under the Flood Control Act (IC 14-28) will be obtained for this project.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Farmland				
Agricultural Lands		X		
Prime Farmland (per NRCS)		X		
NRCS Form AD-1006/CPA-106 scored ≥ 160 ?	Yes	No		
		X		

Provide the NRCS Form AD-1006/CPA-106 score and state whether there is a significant loss of farmland as a result of the project in the remarks section. See CE Manual for guidance to determine which NRCS form is appropriate for your project.

This is page 16 of 34 Project name: State Boulevard Reconstruction Date: May 2, 2014

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Remarks:

As is required by the Farmland Protection Policy Act (FPPA), the NRCS has been coordinated with (March 10, 2009). The NRCS indicated that the project will not cause a conversion of prime farmland, Appendix B, page B-8. Since there will not be a conversion of prime farmland, the requirements of the FPPA are not applicable and the completion of the CPA-106 is not required. No other alternatives other than those already discussed in this document will be considered without a reevaluation of the project's potential impacts upon farmland. This project will not have a significant impact to farmland.

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates
Minor Projects PA Clearance			

Eligible and/or Listed Resource Present

Results of Research

	Yes	No
Archaeology		X
History/Architecture	X	
NRHP Buildings/Site(s)	X	
NRHP District(s)	X	
NRHP Bridge(s)	X	

Project Effect

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
No Historic Properties Affected		X	
No Adverse Effect		X	
Adverse Effect	X		FHWA: 02/27/2013 SHPO: 04/01/2013

Documentation Prepared

Documentation

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
Historic Properties Short Report		X	
Historic Property Report	X		ES: 07/16/2012 SHPO 08/13/2012
Archaeological Records Check/ Review	X		
Archaeological Phase Ia Survey Report	X		ES: 07/16/2012 SHPO 08/13/2012
Archaeological Phase Ic Survey Report		X	
Archaeological Phase II Investigation Report		X	
Archaeological Phase III Data Recovery		X	
APE, Eligibility and Effect Determination	X		FHWA: 02/27/2013 SHPO 04/01/2013
800.11 Documentation	X		FHWA: 02/27/2013 SHPO 04/01/2013
Memorandum of Agreement	X		Approval date to be documented in FONSI request to FHWA

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work, which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effect (APE): The APE is centered on State Boulevard in Fort Wayne, Wayne Township, Allen County, Indiana. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway. Maps depicting the APE are included in Appendix C, pages C-134 to C-137.

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The archaeological APE is defined as the project footprint.

Coordination with Consulting Parties: An invitation to consulting parties and a request for participation in the Section 106 process was provided to federal, state, and local agencies initially on March 23, 2009. Additional requests (multiple dates) for participation in the process was provided as individuals or groups expressed interest. Those agencies were invited to be consulting parties and participate in the development of the project in accordance with provisions of Section 106 of the National Historic Preservation Act.

The following is a list of organizations and individuals that were invited or requested to be consulting parties. If no response was received to the consulting party invitation after 30 days, it was assumed the parties involved did not wish to act as consulting parties. FHWA, INDOT, and SHPO are considered automatic consulting parties.

Organization/Name	Response
Indiana Historical Society	No response
Fort Wayne City Council	Added 03/23/2009
ARCH, Inc.	Participant 03/26/2009
Allen County Historian	Participate 03/27/2009
Fort Wayne Historic Preservation Review Board	Participant 04/02/2009
Indiana Landmarks (formerly known as Historic Landmarks Foundation), Northern Regional Office	Participate 04/13/2009
Brookview Neighborhood Association	Participate 05/01/2009
Indiana Historic Spans Task Force	Participate 05/01/2009
Friends of the Parks of Allen County	Participate 05/22/2009
City of Fort Wayne	Participate 06/01/2009
Allen County Historical Society	No response
Irvington Park Neighborhood Association	Participate 07/09/2009
Historic Bridge Expert, James L. Cooper	No response
Adjacent Property Owner, Susan Haneline	Added 12/01/2009
Northside Galleries	Added 11/07/2009
Adjacent Property Owner, Karl Dietsch	Added 12/01/2009
Northeastern Indiana Regional Coordinating Council	Added 12/01/2009
Adjacent Property Owner, Annette "Jan" Dailey	Added 12/01/2009
Westbrook 5, LLC	Added 12/06/2009
Barrett & McNagney, LLP	Added 12/06/2009
Martin Riley Architects and Engineers	Added 12/06/2009
Earth Source, Inc.	Added 12/15/2009
Spy Run Neighborhood Association	No response
Five Points Neighborhood Association	No response
Bloomingtondale Neighborhood Association	No response
Advisory Council on Historic Preservation	Declined 07/31/2012

Archaeology: Archaeological Consultants of Ossian completed an Archaeological Field Reconnaissance of the proposed State Boulevard Reconstruction Project on April 2, 2009. No archaeological sites were located during the field reconnaissance. The Archaeological Field Reconnaissance Report concluded no properties on or eligible for listing on the NRHP will be affected by the proposed project. In reviewing the area previously surveyed by Archaeological Consultants of Ossian it was determined that there were areas within the limits of the preferred alternative for the proposed State Boulevard Improvements Project that had not been surveyed. On July 11, 2012, Archaeological Consultants of Ossian completed the Indiana Archaeological Short Report, for the additional area required for the State Boulevard Improvements project. The short report was reviewed and approved by the Indiana Department of Transportation, Cultural Resources (INDOT-CR) on July 16, 2012, and the State Historic Preservation Officer (SHPO) on August 13, 2012.

Historic Properties: A Historic Properties Report (HPR) was prepared by The Westerly Group, Inc. in September 2009, for the proposed State Boulevard Reconstruction Project. Historic properties were identified and evaluated in accordance with current Section 106 federal regulations. Four properties were recommended as eligible for listing on the NRHP including 315 East State Boulevard, the proposed Brookview-Irvington Park Historic District, the bridge carrying East State Boulevard over Spy Run Creek, and State Boulevard (within the historic district). In February 2012, Weintraut & Associates, Inc. prepared an Additional Information Report (AI) to append the HPR. The AI was prepared to supplement the HPR following the inclusion of two new NRHP-listed resources within the APE. As part of the AI investigation two districts were identified that were listed in the NRHP after the HPR (2009) was prepared. Portions of both the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010) and Brookview-Irvington Park Historic District

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(NRHP, 2011) are contained within the project APE. The AI further recommended that the portion of State Boulevard within the Brookview-Irvington Park Historic District is best represented as a contributing component of the NRHP historic districts and would not be recommended eligible as an individual resource, and that 315 East State Boulevard does not meet the criteria to be eligible for listing in the NRHP. The AI was reviewed and approved by INDOT-CR on May 10, 2012, and SHPO on June 22, 2012.

Documentation, Findings: Two historic properties are listed in the NRHP: Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District. One historic property has previously been determined eligible for the NRHP: Bridge over Spy Run Creek.

- Fort Wayne Park and Boulevard System Historic District (NRHP, 2010)—Adverse Effect
- Brookview-Irvington Park Historic District (NRHP, 2011)—Adverse Effect
- Bridge over Spy Run Creek (NBI No. 0200273)—Adverse Effect

The Section 106 APE Determination (36 CFR 800.4(a)(1)), and the Finding of Adverse Effect (36 CFR 800.6(a)(3), was approved by Federal Highway on February 27, 2013 and concurred with by the SHPO on April 1, 2013. The Section 800 Determination and Finding Documentation, signed by FHWA will be sent to all consulting parties at the same time the Environmental Assessment is released for public involvement. A Draft Memorandum of Agreement (MOA) was prepared to outline the proposed 'Adverse Effect' the project will have on the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District and the proposed mitigation for those adverse impacts. The Bridge over Spy Run Creek falls within the scope of the HBPA; and therefore, does not require an MOA for the adverse effect the project will have on the resource. The Draft MOA will be distributed to the IDNR-DHPA and consulting parties at the same time the Environmental Assessment is released for public involvement. Once the MOA is finalized and signed it will be forwarded to the ACHP for their information and record.

Public Involvement: Three consulting party meetings were held to discuss the findings of Historical Properties Report, effect findings, and options to avoid, minimize and mitigate adverse effects to the surrounding cultural resources. A total of 35 individuals, representing the FHWA, State, City, neighborhood associations, historic preservation groups, and adjacent property owners were invited to participate in the consulting party meetings. Meetings were held on December 15, 2009, September 1, 2011, and September 19, 2012. Meeting minutes can be found in the Section 106 Documentation in Appendix C, pages C-222 to C-224, C-340 to C-348, and C-427 to C-434.

A multitude of comments were received from consulting parties during the Section 106 process. Most comments received were to express concern with the scope and magnitude of the project and the significant impact it will have on the Brookview-Irvington Parks Historical District. Copies of all Section 106 consulting party comments can be found in Appendix C, pages C-189 to C-485.

The bridge over Spy Run Creek was advertised for reuse, per the HBPA. A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-491 to C-493. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

A public notice describing the project and the Section 106 finding of "Adverse Effect" will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. This will also be the final chance for a responsible party to come forward to fund perseveration of the bridge. Should no party come forward within 30 days and the draft MOA be approved the 106 process will be concluded. A summary of any comments received and the disposition of those comments will be included in the FONSI request packet to be reviewed by FHWA prior to their issuance of a FONSI.

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SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES Approval/dates</u>
	Yes	No	Yes	No	
Parks & Other Recreational Land					
Publicly owned park	X		X		
Publicly owned recreation area		X			
Other (school, state/national forest, bikeway, etc.)		X			
Programmatic Section 4(f)		X			
Individual Section 4(f) Evaluation		X			
“De minimis” Impact	X				Pending FONSI

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES Approval/dates</u>
	Yes	No	Yes	No	
Wildlife & Waterfowl Refuges					
National Wildlife Refuge		X			
State Fish and Wildlife Area – recreation or refuge areas only		X			
Programmatic Section 4(f)		X			
Individual Section 4(f) Evaluation		X			
“De minimis” Impact		X			

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES approval/dates</u>
	Yes	No	Yes	No	
Historic Properties					
Sites eligible and/or listed on the NRHP	X		X		
Programmatic Section 4(f)	X				Historic Bridge PA Pending FONSI
Individual Section 4(f) Evaluation	X				Pending FONSI
“De minimis” Impact		X			

Discuss Programmatic Section 4 (f) and De minimis Section 4(f) impacts in the remarks section below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, De minimis and Individual Section 4(f) documents please refer to the “Procedural Manual for the Preparation of Environmental Studies.” Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:	<p><u>Parks and other Recreational Land</u></p> <p>“De minimis” Impact – Vesey Park: One property, Vesey Park was noted in the project limits as a Section 4(f) resource. This park is operated by the City of Fort Wayne Parks Department and includes the green space along Spy Run Creek between Eastbrook Drive and Westbrook Drive connecting the larger portion of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary’s River. The park features open space among the trees with areas for picnicking and views to Spy Run Creek. This undertaking would convert approximately 0.55-acre of permanent right-of-way to a transportation use for the installation of a new bridge over Spy Run Creek and State Boulevard. Avoidance of this resource is not feasible as the existing roadway crosses Spy Run Creek and Vesey Park and one purpose of the project is to replace the existing bridge. Coordination with the City of Fort Wayne Parks Department regarding the proposed project was undertaken. The City of Fort Wayne Parks Department provided a letter in support of this project on January 23, 2013. The project will have a <i>de minimis</i> effect on Vesey Park, a Section 4(f) property, as it will not adversely affect the activities, features, and attributes that qualify Vesey Park for protection under Section 4(f). For reference to the communication see Appendix J page J-2 to J-8.</p> <p>A public notice describing the project and the Section 4(f) <i>de minimis</i> finding associated with Vesey Park will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FNOSI) request document submitted to the FHWA.</p> <p><u>Historical Properties</u></p> <p>It has been determined two historic districts and a historic bridge eligible for listing in the NRHP exist within the APE of this project. The undertaking will affect the Fort Wayne Park and Boulevard System Historic District, the Brookview-</p>
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Irvington Park Historic District, and the Bridge over Spy Run Creek.

Programmatic Section 4(f) – Bridge over Spy Run Creek: The Bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010) and is thus considered a Section 4(f) resource based upon 23 CFR 774.11(e). The Bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. As part of the project, the bridge will be removed and replaced on new alignment.

The project falls within the stipulations for the Historic Bridges Programmatic Section 4(f). Per the Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges, three specific alternatives must be evaluated prior to the use of a historic bridge. The following are these alternatives, along with findings that are supported through consultation with consulting parties:

1. Do Nothing. The do nothing alternative has been studied. The do nothing alternative ignores the basic transportation need. For the following reasons this alternative is not feasible and prudent:
 - a. Maintenance - The do nothing alternative does not correct existing deficiencies that cause the bridge to be considered structurally deficient or deteriorated. These deficiencies can lead to sudden collapse and potential injury or loss of life. Normal maintenance is not considered adequate to cope with the situation.
 - b. Safety - The do nothing alternative does not correct the situation that causes the bridge to be considered deficient.

Because of these deficiencies the bridge poses serious and unacceptable safety hazards to the traveling public and places intolerable restriction on transport and travel.

2. Build on New Location Without Using the Old Bridge. Investigations have been conducted to construct a new bridge on a new location or parallel to the old bridge (allowing for a 1-way couplet).
 - a. Preservation of Old Bridge - It is not feasible and prudent to preserve the existing bridge, even if a new bridge were to be built at a new location. The existing bridge carrying State Boulevard over Spy Run Creek provides an insufficient waterway opening and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. *State Boulevard Reconstruction From Spy Run Creek to Cass Street, Version February 20, 2013, Fort Wayne, Allen County, Indiana Des. No.: 0400587 Federal Project Number: IN20071404* 17. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River frequently, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Creek Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River, which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure. This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides an insufficient waterway opening and is quickly deteriorating.

3. Rehabilitation without Affecting the Historic Integrity of the Bridge. Studies have been conducted of rehabilitation measures, but, for the following reason, this alternative is not feasible and prudent:
 - a. The bridge is so structurally deficient that it cannot be rehabilitated to meet minimum acceptable load requirements without affecting the historic integrity of the bridge.

The project's alternatives were developed using the July 17, 2006, Programmatic Agreement (PA) on Indiana's Historic Bridges, as well as guidance provided on this PA by INDOT subsequent to its enactment. According to the Indiana Historic Bridge Inventory report dated December 2010, the Bridge over Spy Run Creek (NBI No. 0200273) is considered

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a non-select candidate for inclusion on the NRHP. As such, the project was evaluated utilizing guidance from this PA for non-select bridges.

Initial Section 4(f) alternatives were sent out with the HPR to consulting parties and SHPO on August 15, 2011, along with the invitation to the September 1, 2011 Consulting Party Meeting. The initial alternatives were discussed and further developed as a result of input received during Consulting Party meetings which were held throughout the development of the project. The final Section 4(f) alternatives were included in the Section 800 documentation presented by INDOT to FHWA for their review and comment and approved on February 27, 2013. The Section 800 documentation was then submitted to SHPO for review on March 1, 2013 and concurred with on April 1, 2013.

The Alternatives Analysis resulted in the identification of a preferred alternative (described previously in this document in the Project Description Section) that includes replacement of the existing bridge, thus resulting in an "Adverse Effect". FHWA signed the finding of "Adverse Effect" on February 27, 2013. By signature of this document, the FHWA has concluded that there is no feasible and prudent alternative to the Section 4(f) use of the Bridge over Spy Run Creek (NBI No. 0200273) through replacement.

The Bridge over Spy Run Creek (NBI No. 0200273) was advertised for reuse, per the HBPA. A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-490 to C-496. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

A public notice describing the project and the Programmatic Section 4(f) will be advertised concurrently with the EA release for public involvement in local media. The public notice will solicit comments regarding the project for a 30-day comment period. This will also be the final chance for a responsible party to come forward to fund preservation of the bridge. If a responsible party does not take ownership of the bridge it will be demolished.

Individual Section 4(f) Evaluation – Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Historic District: The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of 11 parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and Bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NRHP in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized." Approximately 0.60 acres of permanent right-of-way will be acquired from this district as part of the proposed project.

The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total of 424 contributing resources including houses, garages, and the combined plats of the district, as well as the previously determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965, represents the construction dates of most buildings within the historic district, and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination. Approximately 2.60 acres of permanent right-of-way and 15 residential relocations will be required from this district as part of the proposed project.

This undertaking will convert property from two historic districts and an historic bridge, all NRHP eligible properties, to a transportation use. The FHWA has determined the appropriate Section 106 finding is Adverse Effect for both districts and therefore, an Individual Section 4(f) evaluation was undertaken. An Individual 4(f) Document has been prepared, which discusses project use of the Section 4(f) resources. The Individual 4(f) Document evaluated and summarized the proposed project's purpose and need, reasonable alternatives, Section 4(f) resources, and all possible planning to minimize harm to those resources. The report identified Alternative 3A as the alternative which would cause the least over all harm in light of the statute's preservation purpose. This alternative includes widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4 lanes while correcting the substandard horizontal curve. For reference to the Section 4(f) evaluation, see Appendix J pages J-9 to J-51.

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Mitigation measures have been detailed in a Memorandum of Agreement (MOA) to be executed by consulting parties. As mitigation for unavoidable impacts to each of the NRHP listed historic districts, the City of Fort Wayne shall implement context sensitive design solutions for this undertaking, salvage architectural details from homes to be demolished, explore funding opportunities for neighborhood improvements, and convene an Advisory Team to ensure the project is developed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. The Bridge over Spy Run Creek falls within the scope of the HBPA; and therefore, does not require an MOA for the adverse effect the project will have on the resource.

In compliance with Section 4(f), pursuant to 23 CFR Part 774.5, the draft Section 4(f) documentation was provided to the US Department of Interior (DOI) for review and comment on May 24, 2013. The DOI provided comments on July 8, 2013 (Appendix J, pages J-52 to J-53). The DOI indicated they would tend to concur with the FHWA and INDOT that there are no feasible and prudent alternatives to the preferred alternative, if built as proposed, which would result in impacts to Section 4(f) properties. Constrained linear features such as State Boulevard offer few good alternatives when 4(f) resources have grown up on either side of the corridor and the functionality of the feature becomes compromised by growing populations. DOI also states that as recently as this last December, there was still considerable disagreement over the project and its mitigation. The Department cannot concur with the INDOT and FHWA because there is no evidence that all parties, including the SHPO, have agreed to the mitigation measures, or is there evidence in the evaluation that the MOA has been signed. DOI reserves their concurrence with the hope that the final 4(f) will present the necessary agreements. For reference to the Section 4(f) documentation see Appendix J, pages J-9 to J-51.

A public notice describing the project and the Individual Section 4(f) Evaluation for impacts to the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Historic District will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. After the conclusion of the comment period efforts will be made to finalize the MOA and obtain concurrence from all necessary signatories. Once the MOA has been signed and the Section 4(f) has been finalized it will be submitted to DOI for final concurrence. The Individual Section 4(f) document will then be reviewed by FHWA for legal sufficiency. Comments or concerns brought forth during this process will be addressed in the FONSI request document submitted to the FHWA.

Section 6(f) Involvement

Section 6(f) Property

<u>Presence</u>		<u>Use</u>	
Yes	No	Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The project will not involve any properties acquired by or improved with the Land and Water Conservation Fund (LWCF). The US Department of the Interior, National Park Service LWCF Detailed Listing of Grants Grouped by County was reviewed for Allen County. Twenty-four sites were noted in Allen County, all of which are outside of the project area. Therefore, there is no Section 6(f) involvement and there will be no taking of LWCF property. DNR's Division of Outdoor Recreation early coordination response (April 7, 2009) also confirmed that no LWCF properties are within the project area. See Appendix B, pages B-27 to B-28 for a copy of the Allen County 6(f) property listings.

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SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

☒

☐

If YES, then:

Is the project in the most current MPO TIP?

☒

☐

Is the project exempt from conformity?

☐

☒

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

☒

☐

Is a hot spot analysis required (CO/PM)?

☐

☒

Is an MSAT level 1a Analysis required?

☐

☒

Is an MSAT level 1b Analysis required?

☒

☐

Is an MSAT level 2 Analysis required?

☐

☒

Is an MSAT level 3 Analysis required?

☐

☒

Is an MSAT level 4 Analysis required?

☐

☒

Is an MSAT level 5 Analysis required?

☐

☒

Remarks:

The project area is located within the air quality maintenance area of ozone and attainment for particulate matter. Copies of the air quality maps are included in Appendix G pages G-16 to G-18. The FY 2014 to 2017 Transportation Improvement Program for the Northeastern Indiana Regional Coordinating Council was found to conform to air quality regulations and incorporated by reference into the FY 2014 to 2017 State Transportation Improvement Program on July 11, 2013. The proposed project is regionally significant and non-exempt. For reference to the planning documents see Appendix G, pages G-8 to G-15.

The purpose of this project is to improve vehicular and pedestrian/bicycle safety along State Boulevard. This project has been determined to generate minimal air quality impacts for CAAA criteria pollutants and has not been linked with any special MSAT concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause an increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES model forecasts a combined reduction of over 80 percent in the total annual emission rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 100 percent. This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

SECTION F - NOISE

Noise

Is a noise analysis required in accordance with FHWA regulations and INDOT's noise policy?

Yes

No

☒

☐

No

Yes/ Date

ES Approval of Noise Analysis

☐

10/18/2011 (Technical Sufficiency)

Remarks:

The proposed State Boulevard Reconstruction Project proposes road improvements on new alignment utilizing federal funds. Under the provisions of 23 CFR, part 772, the project is considered a "Type I" noise project requiring an analysis of potential noise impacts and, if so, whether there are feasible and reasonable ways to mitigate those impacts.

A noise analysis was prepared by the Corradino Group following the guidance in the Federal Highway Administration's (FHWA's) *Highway Traffic Noise: Analysis and Abatement Guidance* (July 2010) and the Indiana Department of Transportation's (INDOT's) *Procedural Manual for Preparing Environmental Documents* and its *Traffic Noise Policy* (July 2011).

Noise measurements were made in conformance with Federal Highway Administration (FHWA) guidance at six locations that represent 63 residential receivers present within 500 feet of the proposed improvement (the analysis distance criterion set in INDOT's *Traffic Noise Policy*). The noise measurement locations represent worst case

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locations for all homes in what are considered noise sensitive areas. An additional measurement was made at another noise sensitive receiver, North Side High School, beyond the east construction limit of the proposed project. Land use at the west project end is commercial, as it is in the east, with the exception of the school. The residential receivers fall into land use category B in terms of FHWA's Noise Abatement Criteria (NAC) (Table 1). The applicable noise criterion for this land use is 67 dBA in terms of the one-hour equivalent noise level, expressed as L_{eq} (1h). Because Part 772 defines potential impacts in terms of noise levels approaching or exceeding the NAC and INDOT's *Noise Policy* defines approaching as one decibel, the effective value for impact analysis in Indiana for land use category B is 66 dBA, rather than 67 dBA. The school falls into NAC land use category C, which is subject to the same NAC dBA criterion.

Existing measured noise levels did not approach or exceed the NAC at any receiver, with the exception noted below. Analysis using the Traffic Noise Model (TNM2.5) validated the noise measurements obtained in the field. TNM2.5 modeling also finds no receivers will experience future project noise levels that approach or exceed the NAC, with the same exception. And, no modeled receiver will experience predicted noise levels that substantially exceed existing noise levels (INDOT's *Noise Policy* defines this as 15 dBA). So, except for measurement site 2S, there are no noise impacts and no mitigation is needed.

Measurement site 2S represents a home on the south side of State Boulevard, where the new alignment joins the existing alignment west of Clinton Street, plus the home across State Boulevard on the east side of Terrace Street. These homes are 22 feet and 16 feet, respectively, from existing State Boulevard. The home on the south side of State Boulevard was a measurement site because early engineering did not call for its acquisition. More detailed design found it was necessary to acquire this home for the project. The house on the north side will remain and will be approximately 50 feet from the future roadway edge. It will experience noise levels exceeding the NAC. However, there is no feasible or reasonable mitigation that could protect this home.

Based upon preliminary design costs and design criteria, no locations have been identified where noise abatement is likely. Noise abatement has not been found to be feasible because effective noise barriers require long, uninterrupted segments of barrier to be feasible. As such, because of the existing cross streets, access points, alleyways and driveways located throughout the project area, it is not feasible to construct effective noise barriers for the roadway. Noise walls would not be reasonable because the cost of providing a wall for an individual home would exceed INDOT cost-effectiveness guidelines. Therefore, there is no feasible or reasonable noise mitigation proposed.

A reevaluation of the noise analysis will occur during final design. If during final design it has been determined that conditions have changed such that noise abatement is feasible and reasonable, the abatement measurements might be provided. The final decision on the installation of any abatement measure(s) will be made upon the completion of the project's final design and the public involvement processes.

For reference, the complete Noise Study Report is provided in Appendix I, pages I-2 to I-53. A copy of the approval of the technical sufficiency of the Noise Analysis (from INDOT Environmental Services) was received on October 18, 2011, and is included in Appendix I, page I-54.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
 Will the proposed action result in substantial impacts to community cohesion?
 Will the proposed action result in substantial impacts to local tax base or property values?
 Will construction activities impact community events (festivals, fairs, etc.)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The proposed project will improve public safety, improve roadway capacity at intersections, improve traffic flow along the project corridor, and improve the infrastructure along State Boulevard. No substantial adverse community impacts are anticipated to result from this project. The project will require a total of 15 residential relocations. The project will not affect community cohesion because it will not substantially change access or travel patterns within the community.

Currently, the State Boulevard project corridor does not provide an adequate and safe link between the two Greenway Trail Systems located in the project area. The proposed project will provide this link between the Pufferbelly Trail and the St. Joseph Pathway. New sidewalks, varying in width from five feet to ten feet, will be constructed on both sides of the roadway.

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The preferred alternative is anticipated to improve neighborhood/community cohesion, as one of the identified needs addressed by the proposed project is pedestrian safety. The proposed project will address the limited north/south pedestrian connectivity caused by traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street. As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail.

The project is not anticipated to affect any public facilities during construction. Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes

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No

☒

Remarks:

This project will improve public safety, traffic flow, and infrastructure along State Boulevard. The project will improve existing conditions and will not result in any substantial indirect or cumulative impacts. The project will reconstruct an existing road in an already fully developed area.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public utilities, fire, police, emergency services, religious institutions, public transportation or pedestrian and bicycle facilities? Discuss the maintenance of traffic, and how that will affect public facilities and services.

Yes

☐

No

☒

Remarks:

Based on the Maintenance-of-Traffic Plan, traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. Access to residential, commercial, and public properties will be maintained throughout construction.

Early coordination describing the project was sent to public agencies, including the highway department, sheriff's department, fire department, public schools, and other local public agencies. No other responses were received from local agencies. See Appendix B, page B-7 for reference to the early coordination list.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes

☒

No

☐

Are any EJ populations located within the project area?

☒☐

Will the project result in adversely high or disproportionate impacts to the EJ population?

☐☒

Remarks:

An EJ concern is considered any impact that would have a disproportionately high and adverse effect on an environmental justice population. For EJ analysis, the reference community is typically a county, city, or town that contains the project and is called the community of comparison (COC). The community that overlaps the project limits is called the affected community (AC). Affected communities, which are more than 50 percent minority or low-income are automatically EJ populations. For all other affected communities, an EJ population exists if the low-income population or minority population is 25 percent higher than the population in the COC. A low-income population is a population with a median income that is below the federal poverty guidelines. A minority population consists of individuals who belong to one or more minority groups.

The project area is comprised of two Census Tracts, as determined by a review of the 2010 US Census data. These Census Tracts are considered to be the ACs. For this analysis, Allen County was analyzed as the COC. Within Allen County, 16.3 percent of the population was considered low-income and 28.8 percent were considered minority populations. An EJ population would exist if the population exceeds 20.4 percent low income or 36.0 percent minority respectively.

Within the project limits, Census Tract 00500 includes the eastern portion of the proposed project. According to the 2010 US Census, 33.3 percent of this population is low income and 34.8 percent is minority. Census Tract 00701 includes the western portion of the proposed project. According to the 2010 US Census, 27.9 percent of this population is low income and 25.4 percent is minority. As such, a potential environmental justice low income population exists within the Affected Community as compared to Allen County. For reference see the table below and Appendix H pages H-2 to H-10.

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Analysis of Potential EJ Populations	COC	AC	
	Allen County, Indiana	Census Tract 00500	Census Tract 00701
LOW-INCOME			
Total Population for whom poverty status is determined (estimated)	248,772	2,766	3,342
Total Population Below Poverty Level (estimated)	40,534	922	931
Percent Low-income	16.3%	33.3%	27.9%
125 Percent of COC	20.4%	AC>125% COC	AC>125% COC
Potential Low-income EJ Impact?		Yes	Yes
MINORITY			
Total population (all races)	254,228	2,939	3,343
White alone or in combination	181,101	1,915	2,493
Number Non-white/Minority	73,127	1,024	850
Percent Non-white/Minority	28.8%	34.8%	25.4%
125 Percent of COC	36.0%	AC>125% COC	AC>125% COC
Potential Minority EJ Impact?		No	No

The 15 residential properties are anticipated to be acquired as part of the proposed project. Avoidance of these acquisitions is not possible due the proximity of the existing structures to the roadway and due to re-alignment of the proposed roadway. Impacts have been minimized to the greatest extent possible. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Relocation resources are available to all residential and business relocates without discrimination. No person displaced by this project will be required to move from a displaced dwelling unless comparable replacement housing is available to that person.

The project is intended to improve safety along State Boulevard by widening and realigning the roadway. The widening and realignment is required to help correct sight distance issues and substandard intersections, as well as provide turn lanes as appropriate. The project will also increase pedestrian safety by the addition of sidewalks varying in width from five feet to ten feet along both sides of the roadway.

The proposed project is expected to benefit the immediate project area including those Census Tracts with environmental justice concerns, through addition of pedestrian facilities, correction of drainage issues associated with the roadway, and improvement of the existing roadway. The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River with frequently, therefore affecting roadway safety by flooding State Boulevard and requiring the closure of the roadway. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often. The proposed project will address this issue by raising the vertical alignment of the roadway approximately seven feet at the proposed bridge over Spy Run Creek. This will significantly reduce the amount of road closures due to flooding events and allow emergency vehicles and local residents access during times when they may not have in the past. Noted negative effects include up to 15 residential relocations and the impact those will have on the existing neighborhood.

Significant efforts were made to engage and involve the public in the project planning process. Early coordination was initiated with representatives of the community. On multiple occasions the City of Fort Wayne met with neighborhood associations, business owners, adjacent property owners, and interested groups. The City met with these individuals to help explain the project, provide project updates, and address comments and concerns. Meeting with these groups, individuals, and representatives further helped the City ensure the public was involved in the planning process. In addition five public information meetings and three open-house style public information meetings were conducted to further attempt to engage the public. Significant efforts were made to encourage participation in the meetings, including public notices and press releases published in the *Fort Wayne Journal Gazette*. For additional information see the public involvement documents associated with this project see Appendix F pages F-12 to F-24.

The positive effects of the project outweigh the noted negative effects; the project would be a benefit to those in the area. The State Boulevard Reconstruction Project would not cause a disproportionate impact on the known EJ community. Significant efforts were made to encourage full and fair participation by all potentially affected communities in the project planning process, and suggestions and comments received from community participants are being considered in the final project design. As a result of this analysis and public involvement process, the requirements of Executive Order 12898 and the policy principles of the US DOT have been addressed, and no further evaluation is warranted.

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Relocation of People, Businesses or Farms:

Will the proposed action result in the relocation people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Yes	No
X	
	X
	X

Number of relocations: Residences: 15 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the Remarks section.

Remarks:

Relocations have been minimized to the extent practical. Existing structures to be relocated are generally within zero to thirty feet of the proposed edge of pavement. Significant property acquisition cannot be avoided due to the roadway alignment and profile. For reference to the parcels anticipated to be relocated see plans included in Appendix A pages A-11 to A-129.

There are no other relocations anticipated from this project; however, during property acquisition, it is possible additional structures may be acquired. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Relocation resources are available to all residential and business relocatees without discrimination. No person displaced by this project will be required to move from a displaced dwelling unless comparable replacement housing is available to that person.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

	Documentation	
	Yes	No
Red Flag Investigation	X	
Hazardous Materials Site Assessment Form	X	
Phase I Initial Site Assessment (ISA)	X	
Phase II Preliminary Site Investigation (PSI)		X
Design/Specifications for Remediation required?		X

	No	Yes/ Date
ES Review of Investigations	X	

Include a summary of findings for each investigation.

Remarks:

A Red Flag Investigation (RFI) was initiated by American Structurepoint, Inc., in 2007. The investigation included a search of nationwide and local database resources provided by IndianaMap and FirstSearch. A total of 46 hazardous material concern records were identified within a 0.5-mile of the project radius. Results of the 2007 preliminary investigation recommended a Phase I Initial Site Assessment (ISA). Prior to completion of the RFI on April 26, 2013 a search of nationwide and local databases was again performed to review updated information. No additional hazardous material concern records were identified in the 2013 search.

A Hazardous Material Site Visit Form was also completed for the project area. The Hazardous Materials Site Visit Form did not identify any additional hazardous materials concerns.

An ISA was prepared by American Structurepoint, Inc on November 11, 2011. A total of five sites were assessed, with no sites identified as having a Recognized Environmental Condition (REC). Because no RECs were identified, no additional investigations are necessary. The following are those properties addressed as part of the ISA.

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Site ID	Address	Site Name	REC	Additional Investigation Recommended
1	215 West State Boulevard	Ink Spot Printing	None	No
2	324 East State Boulevard	Kroger	None	No
3	310 West State Boulevard	Townsend and Pratt Auto Sales	None	No
4	2230 North Clinton Street	Lassus Brothers Oil Handy Dandy	None	No
5	2522 Cass Street	Superior Collision	None	No

A copy of the RFI, Hazardous Material Site Visit Form, and ISA Executive Summary are included in Appendix D, pages D-2 to D-14.

SECTION I – PERMITS CHECKLIST

	<u>Required</u>	<u>Not Required</u>
Army Corps of Engineers (404/Section 10 Permit)		
Individual Permit (IP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDEM		
Section 401 WQC	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDNR		
Construction in a Floodway	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mitigation Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
US Coast Guard Section 9 Bridge Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Others (Please discuss in the Remarks section below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The project will require a Section 404 from the USACE and a Section 401 Water Quality Certification from IDEM for impacts to regulated wetlands or waterways. The project will require a Construction in a Floodway permit from IDNR for the crossings of Spy Run Creek. The project will require a Rule 5 Erosion Control Permit from IDEM if at least one acre of land is disturbed.

The local project sponsor is responsible for obtaining all required permits.

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SECTION J- ENVIRONMENTAL COMMITMENTS

Information below must be included on Commitments Summary Form. List all commitments, indicating which are firm and which are optional.

Remarks:

Firm Commitments

USFWS

1. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries.
2. Restrict below-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.
3. Restrict channel work and vegetation clearing to within the width of the normal approach road right-of-way.
4. Minimize the extent of artificial bank stabilization.
5. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
6. Implement temporary erosion and siltation control devices such as placement of straw bales in drainage ways and ditches, covering exposed areas with burlap, jute matting or straw, and grading slopes to retain runoff in basins.
7. Revegetate all disturbed soil areas immediately upon project completion.
8. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season (as applicable).

IDNR

9. Minimize and contain within the project limits in channel disturbance and the clearing of trees and brush.
10. Do not work in the waterway from April 1 through June 30 without prior written approval of the Division of Fish and Wildlife
11. Post "Do Not Mow or Spray" signs along the right-of-way.
12. Seed and protect all disturbed streambanks and slopes that are 3: 1 or steeper with erosion control blankets (follow manufacturer's recommendations for installation); seed and apply mulch on all other disturbed areas.
13. Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion; low endophyte tall fescue may be used in "high maintenance" areas only.[Alternate wording – check your letter - Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes as soon as possible upon completion; low endophyte tall fescue may be used in ditch bottom and side slopes only.]
14. Do not cut any trees suitable for Indiana bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark) from April 1 through September 30.
15. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
16. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
17. Do not work in salmonid waterways from March 15 through June 15 and from July 15 through November 30 without the prior written approval of the Department of Natural Resources, Division of Fish and Wildlife.
18. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
19. The project must not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. This includes maintaining land under the bridge unarmored with riprap to allow for wildlife passage.
20. If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317)232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.
21. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
22. Place all excavated material landward of the floodway.
23. Do not leave felled trees, brush, or other debris in the floodway. Remove all construction debris from the floodway.
24. Keep the bridge waterway opening free of debris and sediment at all times.
25. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree, which is removed that is ten inches or greater in diameter-at-breast height within the regulatory floodway or as required by permit conditions.

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IDEM

26. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project.
27. IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff.
28. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. Dirt tracked onto paved roads from unpaved areas should be minimized.
29. All facilities slated for renovation or demolition must be inspected by an Indiana-licensed asbestos inspector prior to renovation or demolition activities. If regulated asbestos- containing material (RACM) that may become airborne is found, demolition, renovation, or asbestos removal activities must be performed in accordance with notification and emission control requirements.
30. In all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition.
31. IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust.
32. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7 percent) oil distillate, is prohibited during the months of April through October.
33. Stabilize all disturbed areas upon completion of land disturbing activities.
34. Sediment-laden water, which otherwise would flow from the project site shall be treated by erosion and sediment control measures appropriate to minimize sedimentation.
35. Wastes and unused building materials shall be managed and disposed of in accordance with all applicable statutes and regulations.
36. A stable construction site access shall be provided at all points of construction traffic ingress and egress to the project site.
37. Public or private roadways shall be kept cleared of accumulated sediment that is a result of run-off or tracking.

MOA (commitments are considered firm pending the MOA approval)

38. The City of Fort Wayne shall consider and, where feasible, shall implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.
39. The City of Fort Wayne shall consider and, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences.
40. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.
41. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:
 - a. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
 - b. Context sensitive solutions, such as protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; maintaining pedestrian connections along the former Eastbrook and Westbrook drives; the rights-of-way, shall be included among the measures considered.
 - c. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
 - d. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic

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- preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.
- e. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail Des. No. 0710990 or US 27 Nos. 0101527 and 0200914) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.
 - f. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
 - g. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before schedule meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.
 - h. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
 - i. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.
42. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."
43. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
- a. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - b. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
44. Photo documentation will include black and white prints of digital photographs and a digital video disc ("DVD") containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" that are in effect at the time.
- a. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 - b. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
 - c. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic

Indiana Department of Transportation

County Allen Route State Boulevard Des. No. 0400587 Project No. _____

District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and

- d. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.
45. The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler's landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps or other WPA era programs in public projects.
46. The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within thirty (30) days, acceptance will be assumed. If the Indiana SHPO responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

For Consideration

USFWS

1. Shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore we request that trees lost to the project be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail.

INDOT-Fort Wayne District

2. This project will be taking place within the NRHP Eligible Brookview/Irvington Park Historic District. This neighborhood is eligible due to the layout of the streets following Spy Run Creek. Taking of right-of-way in the area will most likely constitute a Section 4(f) impact. Due to these situations, minimization of impacts in this area should be considered by multiple alternatives to show the proposed plan is the most feasible and prudent. Context sensitive design to fit the historic setting of the neighborhood should also be investigated.

Indiana Department of Transportation

County Allen Route State Boulevard Des. No. 0400587 Project No. _____

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Remarks:

Agency	Date Mailed	Response Received	Appendix Location
US Fish and Wildlife Service	March 16, 2009 March 19, 2014	April 20, 2009 March 19, 2014	B-15 to B-16 B-25 to B-26
US Natural Resources Conservation Service	March 16, 2009	March 20, 2009	B-8
US Army Corps of Engineers	March 16, 2009	May 11, 2009	B-17 to B-18
Indiana Geological Survey	March 16, 2009	April 06, 2009	B-13
Aeronautics Section – INDOT	March 16, 2009	No Response	
Indiana Department of Environmental Management	April 24, 2013 (electronic submittal)	April 24, 2013	B-21 to B-24
IDNR, Division of Fish and Wildlife	March 16, 2009	November 18, 2009	B-19
Ninth Coast Guard Unit	March 16, 2009	March 30, 2009	B-9
Fort Wayne District – INDOT	March 16, 2009	March 30, 2009	B-10
Allen County Sheriff's Department	March 16, 2009	No Response	
City of Fort Wayne	March 16, 2009	April 3, 2009	B-11 to B-12
Allen County Surveyor	March 16, 2009	No Response	
Allen County Highway Department	March 16, 2009	No Response	
City of Fort Wayne Office of Mayor	March 16, 2009	No Response	
Fort Wayne Community School Board	March 16, 2009	No Response	
Allen County Executive Board of Health	March 16, 2009	No Response	
Northside High School	March 16, 2009	No Response	
Allen County Parks and Recreation	March 16, 2009	No Response	
Imagine Master Academy	March 16, 2009	No Response	
Forest Park Elementary School	March 16, 2009	No Response	
Department of Planning Services	March 16, 2009	No Response	
IDNR – Division of Outdoor Recreation	March 16, 2009	April 07, 2009	B-14
Allen County Engineer	March 16, 2009	No Response	
Allen County Board of Commissioners	March 16, 2009	No Response	

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• US Army Corps of Engineers – May 11, 2009	B-17 to B-18
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Attachment 2

Approved Additional Information study (Text Only) – Pages 1-5



AMERICAN
STRUCTUREPOINT
INC.

December 15, 2014

Mr. Jason Kaiser
Environmental Scoping Manager
Indiana Department of Transportation
Fort Wayne District Office
5333 Hartfield Road
Fort Wayne, Indiana 46808

Re: Additional Information Document
Des. No. 0400587
State Boulevard Reconstruction Project
between Spy Run Avenue and Cass Street
Fort Wayne, Allen County, Indiana

Dear Mr. Kaiser:

This letter documents the results of an Additional Information (AI) study of the State Boulevard Reconstruction Project between Spy Run Avenue and Cass Street in Fort Wayne, Allen County, Indiana. An Environmental Assessment (EA) Document was prepared for this project, and was approved on May 14, 2014 under Des. No. 0400587.

On June 18, 2014 a Public Hearing was held for the proposed project. At the June 18, 2014 Public Hearing and in a letter dated July 18, 2014 (attachment pages 1 to 3) ARCH, Inc. proposed an alternative prepared by Storrow Kinsella Associates and Transportation Solutions, LLC. Storrow Kinsella Associates and Transportation Solutions, LLC were commissioned by ARCH, Inc, Indiana Landmarks, Friends of the Parks, and the Brookview-Irvington Park Neighborhood Association to examine the background research developed for the proposed project to determine if there was an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible, and avoided, minimized or mitigated the adverse effect to the neighborhood. As such, it was determined through coordination with the Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT) that the submitted alternative should be evaluated as part of the environmental process.

In addition, it has been requested by FHWA and INDOT that the residential properties acquired through the Voluntary Floodplain Relocation Fund of Fort Wayne be discussed as part of the environmental process. Therefore, the purpose of this AI is to evaluate the Consulting Parties Proposed Alternative (CPPA) and to provide information about properties acquired through the Voluntary Floodplain Relocation Fund.

CPPA

The CPPA, as presented by Storrow Kinsella Associates in collaboration with Transportation Solutions, LLC (attachment pages 3 to 14), consists of a two-lane parkway alignment shifted south of existing State Boulevard between Clinton Street and the Westbrook/Edgehill Drive intersection.

The transition from existing State Boulevard to the CPPA includes a single lane roundabout at the Westbrook/Edgehill Drive intersection and a two-lane signalized hybrid roundabout at the North Clinton Street intersection. The CPPA includes a new crossing of Spy Run Creek raised above the 100-year flood elevation and a multi-use path separated from the roadway. The multi-use path would utilize the existing Spy Run Creek Bridge. If the deteriorated bridge condition or flooding issues dictate removal, a new multi-use path bridge would be constructed. Eastbrook Drive would be converted to a cul-de-sac just north of the realigned State Boulevard. Access to existing State Boulevard would be obtained by utilizing the proposed roundabout at Clinton Street. No direct access to the realigned State Boulevard would be provided at Eastbrook Drive, Oakridge Road, or Terrace Road. See attachment page 6 for graphic representation of the CPPA.

The CPPA would require the relocation of at least two businesses and one residential property to construct Clinton Street roundabout. The CPPA is estimated to cost \$1.6 million more than Alternative 3A (preferred). The cost of the CPPA is elevated due to the increased construction cost associated with a larger footprint and increased infrastructure associated with the two proposed roundabouts, the addition of a second pedestrian bridge, the potential for mechanically stabilized earth (MSE) retaining walls needed to keep fill slopes from extending into Spy Run Creek and associated with the realigned State Boulevard near the proposed Eastbrook Drive cul-de-sac.

Potential Historic Property Impacts

Similar to Alternative 3A (preferred) and 3C, the CPPA proposed realignment of State Boulevard and change in elevation associated with the CPPA would result in the bifurcation of the Brookview-Irvington Park Historic District. Contributing features located within the project area would be removed from their historical locations: State Boulevard relocation and the removal of the existing bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of Eastbrook Drive (south of State Boulevard) to a cul-de-sac, and the replacement of the bridge over Spy Run Creek, the landscape of the area would be modified altering the character and setting of the district. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard. Furthermore, the realignment of State Boulevard would require the acquisition of right-of-way from the Fort Wayne Park and Boulevard System Historic District, again altering the historic location of State Boulevard. The realigned State Boulevard profile would have a significant increase in vertical elevation as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). The prefabricated trail bridge, pedestrian access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over the contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the Fort Wayne Park and Boulevard System Historic District.

Unlike the required removal of 15 residential structures contributing to the Brookview-Irvington Historic District associated with Alternative 3A (preferred), the CPPA would not require any contributing residential structure removals; however, the realignment of State Boulevard and change in elevation would still result in a bifurcation of the district and the removal of contributing features from their historical location. The alteration and removal of contributing features from their historical location as proposed in the CPPA would also result in similar impacts to the Fort Wayne Park and Boulevard System Historic District. The CPPA also requires the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park. In addition, the contributing residential structures avoided by the CPPA and removed by Alternative 3A (preferred), as described in the May 14, 2014 approved EA, do not possess historically unique features when compared to the remaining residential structures in the Brookview-Irvington Park Historic District, which would make them individually eligible for the National Register of Historic Places. A significant portion of the contributing structures to be removed by Alternative 3A (preferred) are also located in areas that flood multiple times a year and thus continue to deteriorate at a relatively rapid rate.

Capacity Analysis (attachment pages 15 to 20)

American Structurepoint conducted a capacity analysis to evaluate the State Boulevard and Clinton Street intersection improvements as proposed in the CPPA as well as document the 2009 and 2030 traffic operations for all other alternatives considered in the environmental document (Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4).

CPPA Capacity Analysis

The CPPA includes a two-lane roundabout with a southbound left turn bypass lane at the Clinton Street intersection (attachment page 6). The accommodation of the bypass lane into the design of the roundabout would require that the entering and exiting flow on the east leg (State Boulevard) of the intersection be signalized. The proposed left turn bypass lane would also require all other roundabout circulating traffic wishing to exit eastbound onto State Boulevard to stop or yield to the southbound left turning traffic resulting in excessive congestion in the roundabout. The CPPA also proposes approach metering in order to provide gaps for certain approaches when heavy traffic flows dominate upstream approaches. However, because a left turn bypass at a roundabout is an unprecedented treatment in the United States and potentially unsafe due to lack of driver familiarity, the decision was made by FHWA and INDOT that such treatment was not desirable from a drivers' expectancy standpoint and therefore not considered in the capacity analysis conducted by American Structurepoint. Instead, three different roundabout scenarios were analyzed to determine if a multi-lane roundabout could feasibly operate at the intersection. These scenarios are defined in the attachment on page 15. As presented in the December 9, 2013 letter, 2005 traffic counts were utilized in the conceptual design of the CPPA. The 2005 traffic data was originally used for the scoping of the State Boulevard project. In 2009, the Northern Indiana Regional Coordinating Council (NIRCC) provided updated turning movement counts. Both 2005 and 2009 traffic data were analyzed for the purpose of evaluating the effectiveness of the CPPA State Boulevard/Clinton Street intersection improvement.

NIRCC has established a LOS "D" as the minimum acceptable peak hour service level for intersections and corridors within the urban area. A LOS of A thru D is considered acceptable and is an indicator of acceptable delay and level of intersection congestion. For the CPPA, the overall intersection LOS is E or F during the AM and PM peak hours in all three scenarios analyzed, with the exception of the AM peak hour of Scenario 1 in which a LOS B is expected. However, during the PM peak hour in Scenario 1, a LOS E is expected. Therefore, the capacity analysis prepared by American Structurepoint concluded that the proposed CPPA roundabout at State Boulevard and Clinton Street would not provide an acceptable level of service (LOS) in the design year, as established by NIRCC, and therefore does not adequately address the congestion mitigation component of the purpose and need of the proposed project.

EA Alternatives Capacity Analysis

To be consistent in the comparisons of alternatives evaluated as part of the overall environmental process and because design year traffic operations were not documented in the approved EA, a capacity analysis was performed for the alternatives evaluated in the approved EA (Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4). The capacity analysis was performed for each alternative and evaluated the intersections of Clinton Street/State Boulevard and Spy Run Avenue/State Boulevard. The analysis grouped together Alternatives 1, 2, 3D, and 4 because the intersection lane configurations are the same for each alternative. Likewise, Alternatives 3A, 3B, and 3C were grouped together for the same reason. In addition, Alternatives 1, 2, 3D, and 4 are the same as the existing intersection lane configurations. For this reason, the capacity analysis was run with existing signal timings. When multiple movements displayed LOS E or F in the design year 2030, the capacity analysis for these alternatives was rerun with optimized signal timings. Optimization of signal timing slightly improved operations; however, several movements resulting in LOS E and F were still documented.

The year 2009 and 2030 analysis for Alternatives 1, 2, 3D, and 4 document that the current lane configurations do not operate at an acceptable LOS even with optimized traffic signal timings in 2030. On the contrary, Alternatives 3A, 3B, and 3C would operate acceptably in the year 2030. For these alternatives, all movements operated at an acceptable LOS (LOS D or above). Therefore, the preferred alternative as recommended in the approved EA (Alternative 3A) meets the purpose and need of the State Boulevard project with regard to traffic operations.

CPPA Alternative Evaluation Conclusion

The CPPA is not reasonable as it does not satisfy the Project's purpose and need. Based on a capacity analysis prepared for the CPPA, this alternative would not address the traffic congestion issues established by the Project's primary purpose and need. The intersections of State Boulevard with Spy Run and Clinton Street would not function at an acceptable level of service in the design year. For the CPPA, the overall intersection LOS is E or F during either the AM or PM peak hours in

all scenarios analyzed. The CPPA would also likely require a level one design exception¹ with regards to roadway geometrics as it appears the CPPA utilizes substandard curvature in the proposed relocated segment of State Boulevard resulting in substandard sight distance conditions. Therefore, the CPPA does not appear to address the safety components associated with the sight distance, geometrics, and congestion. However, while not as significant as the need to address congestion and the safety components associated with sight distance, geometrics, and congestion, the CPPA does address the flooding and Greenways Trail System connectivity components of the purpose and need by proposing to elevate the roadway above the 100-year elevation and provide a separated multi-use path. Furthermore, this alternative would require an estimated \$9.6 million project cost, approximately \$1.6 million (20% increase) more than the preferred alternative (3A) presented in the May 14, 2014, approved EA (attachment pages 76-110). For these reasons, the CPPA is not considered reasonable and has been eliminated from further consideration.

An Addendum to the State Boulevard Reconstruction Project 800.11(e) Documentation has been prepared to incorporate the evaluation of the CPPA as presented by Storrow Kinsella Associates in collaboration with Transportation Solutions, LLC (attachment pages 23-24). In addition, the Draft Section 4(f) Evaluation for Impacts to Historic Properties prepared for the State Boulevard Reconstruction Project was also updated to include the CPPA as alternative considered and screened (attachment pages 25-62).

VOLUNTARY FLOODPLAIN RELOCATION FUND OF FORT WAYNE

As previously stated, FHWA and INDOT requested that the residential properties acquired through the Voluntary Floodplain Relocation Fund of Fort Wayne be discussed as part of the environmental process.

As a result of being built on the banks of the St. Mary's River, the St. Joseph River, and the Maumee River, the City of Fort Wayne is routinely impacted by flood events. As such, the City of Fort Wayne conducted several studies in conjunction with FEMA, the USACE, and the Maumee River Basin Commission to develop a flood protection plan. These studies were conducted by Rust Engineering (1996) and Christopher Burke Engineering (2005). The results and recommendations of the studies was a mix of strategies including construction of flood walls, earthen berms, flood proofing of properties, and voluntary buy outs. The recommendations were implemented using a mix of federal and local funding sources, identifying the worst areas and addressing them with a multitude of strategies, including the construction of levees, flood walls, detention basins and voluntary buyouts.

The Spy Run Creek area which passes through the State Boulevard project area is one of several tributaries that is prone to quick and extreme flooding. The recommendations in the 2005 study included the acquisition and removal of 23 homes along Eastbrook and Westbrook Drives located on the banks of Spy Run Creek between Clinton Street and State Boulevard. After the removal of the homes, a riparian green space along with an earthen berm would be erected to protect adjacent properties. The flood protection activities along Westbrook Drive were completed in 2008.

Similar flood protection activities were also initiated in 2008 along Eastbrook Drive. At the same time the preliminary engineering design of the State Boulevard widening project was initiated as a federal aid project. Prior to 2008, State Boulevard was only intended to be a widening project as identified in the NIRCC's Transportation Improvement Program (TIP). As the proposed widening project developed it was determined that roadway realignment would be required to correct the sub-standard horizontal curve in the vicinity of Spy Run Creek. It was then determined that the realigned roadway would likely pass through the Eastbrook Drive area where the voluntary flood buy-outs were occurring. The City then requested a meeting with INDOT to discuss the potential overlap of the on-going flood buy-out program and the proposed State Boulevard project. A meeting was held on June 26, 2008 at the Fort Wayne District offices of INDOT. As directed by INDOT and FHWA during the June 26th meeting, the City stopped further land acquisition associated with the flood buy-out program within the Eastbrook Drive area. It was determined by INDOT and FHWA that the previous purchases by the voluntary floodplain relocation fund of Fort Wayne would be considered previously owned properties, purchased and cleared under a separate program of local funds, and were in no way an attempt to circumvent federal regulations. Therefore, INDOT and FHWA determined the acquisitions were not an avoidance of federal regulations. See

¹ A design exception is a request for an exception to specific design criteria, required when an element of a proposed design does not meet the standard design criteria as set forth in the Indiana Design Manual. A design exception is submitted to and approved by INDOT. Level one design exceptions are those exceptions related to highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability.

Des. No. 0400587
Page 5 of 5

attachment pages 63-75 for a time line and further documentation detailing the City's flood protection activities in the Westbrook and Eastbrook Drive vicinity as it relates to the State Boulevard project.

Unless specifically discussed in this AI document, all impacts remain the same as in the approved EA. If you have any questions, please feel free to contact me at (317) 547-5580 or by e-mail at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:slg

Attachments:

1. July 18, 2014 Letter from ARCH, Inc. -- Pages 1-3
2. The *Consulting Parties Proposed Alternative* (CPPA) as presented by Storrow Kinsella Associates, in collaboration with Transportation Solutions, LLC -- Pages 4-14
3. Capacity Analysis of CPPA Alternative for State Boulevard and Clinton Street Intersection -- Pages 15-22
4. Addendum to the State Boulevard Reconstruction Project 800.11(e) Documentation (text only) -- Pages 23-24
5. Draft Section 4(f) Evaluation for Impacts to Historic Properties prepared for the State Boulevard Reconstruction Project (text only) -- Pages 25-62
6. Voluntary Floodplain Relocation Fund of Fort Wayne Documentation -- Pages 63-75
7. Approved EA Document (text only), May 14, 2014 - Pages 76-110

AI Document Approval



Jason Kaiser
Environmental Scoping Manager
INDOT -- Fort Wayne District

12-17-14

Approval Date



Laura Hilden
Director of Environmental Services
INDOT

12/17/2014

Approval Date



Joyce Newland
Planning/Environmental Specialist
FHWA

12.17.14

Approval Date

Attachment 3

Final Section 106 Documentation – Pages 1-531

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECTS FINDING
STATE BOULEVARD RECONSTRUCTION
FROM SPY RUN TO CASS STREET
FORT WAYNE, ALLEN COUNTY, INDIANA
DES. NO. 0400587
FEDERAL PROJECT NUMBER: IN20071404**

AREA OF POTENTIAL EFFECTS

(Pursuant to 36 CFR Section 800.4(a)(1))

The Area of Potential Effects (APE) is centered on State Boulevard in Fort Wayne, Wayne Township, Allen County, Indiana. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway.

The archaeological APE is defined as the project footprint.

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

Two historic properties are listed in the National Register of Historic Places (NR): Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District. One historic property has previously been determined eligible for the NR: Bridge over Spy Run Creek.

Fort Wayne Park and Boulevard System Historic District (NR, 2010). The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. It encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler. The district includes nearly 2,000 acres of parks, boulevards, and sites. Eight resources (seven of which are contributing) identified as part of the Fort Wayne Park and Boulevard System Historic District are located within the APE for this project. The FWPB is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909 to 1955.

Brookview-Irvington Park Historic District (NR, 2011). The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906 to 1965.

Bridge over Spy Run (NBI No. 0200273). The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927. The Bridge over Spy Run is also identified as a Contributing resource in the Fort Wayne Park and Boulevard System Historic District and the Brookview-Irvington Park Historic District.

EFFECT FINDING

Fort Wayne Park and Boulevard System Historic District (NR, 2010)—Adverse Effect
Brookview-Irvington Park Historic District (NR, 2011)—Adverse Effect
Bridge over Spy Run (NBI No. 0200273)—Adverse Effect

Federal Highway Administration (FHWA) has determined a finding of Adverse Effect is appropriate for this undertaking.

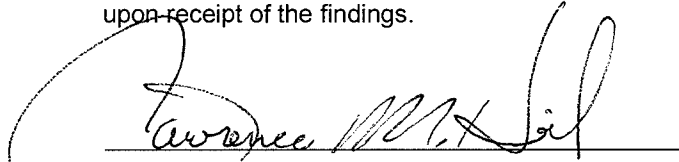
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Fort Wayne Park and Boulevard System Historic District – This undertaking will convert property from the Fort Wayne Park and Boulevard System Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Fort Wayne Park and Boulevard System Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Brookview-Irvington Park Historic District – This undertaking will convert property from the Brookview-Irvington Park Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Brookview-Irvington Park Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Bridge over Spy Run (NBI No. 0200273) – This resource is used for transportation purposes. This undertaking will have an "Adverse Effect" on the Bridge over Spy Run, a Section 4(f) property; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Bridge over Spy Run. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Consulting parties will be provided a copy of the findings and determinations of FHWA, in accordance with FHWA's Section 106 procedures. Comments will be accepted for thirty (30) days upon receipt of the findings.

A handwritten signature in black ink, appearing to read "Lawrence M. Hil", is written over a horizontal line.

for Karen Bobo
Acting Division Administrator
Federal Highway Administration

2-27-2013

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR SECTION 800.6(a)(3)
STATE BOULEVARD RECONSTRUCTION
FROM SPY RUN TO CASS STREET
FORT WAYNE, ALLEN COUNTY, INDIANA
DES. NO.: 0400587
FEDERAL PROJECT NUMBER: IN20071404**

1. DESCRIPTION OF THE UNDERTAKING

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The Preferred Alternative for this project is Alternative 3A, Access Alternate 2. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four ten-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an eight-foot wide raised median and a two-way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment will be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four eleven-foot travel lanes, two in each direction, separated by a twelve-foot two-way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway. Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. The project also includes a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive, which will connect the Pufferbelly Trail.

36 CFR § 800.16(d) defines the Area of Potential Effects (APE) as the “geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.”

The APE is centered on State Boulevard. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway. (See Appendix B, Maps and Site Plans for a map of the APE.) The archaeological APE is defined as the project footprint.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Pursuant to 36 CFR Section 800.4(b), project consultants conducted efforts to identify historic properties—including consultation—as part of this Section 106 undertaking.

The Westerly Group, Inc. (WGI) initiated efforts to identify historic properties. According to WGI, historians investigated the National Register of Historic Places (NR), Indiana Register of Historic Sites and Structures (SR), *Indiana Historic Bridge Inventory*, *Fort Wayne Interim Report*, Sanborn Fire Insurance maps, as well as other primary and secondary sources. Online resources were also accessed to complete the research. Research included a review of the property listing: “The Civilizing of a Midwestern City: The Park and Boulevard System of Fort Wayne, Indiana--A Plan for the Ideal Development of Transportation, Parks and Residential Subdivision,” the Historic Property Report (HPR) for the US 27 Southbound Bridge Replacement Over Spy Run Creek (Ross Nelson), and a preliminary NR nomination for Wildwood Park Historic District.¹

On March 18, 2009, archaeologists for Archaeological Consultants of Ossian conducted a field reconnaissance for a project area of approximately 6.43 acres. The reconnaissance included shovel testing, pedestrian walkover, and auger testing.

On March 23, 2009, American Structurepoint, Inc. (Structurepoint) sent a Section 106 Early Coordination Letter, describing the proposed project and inviting the following parties to join Section 106 consultation: Indiana State Historic Preservation Officer (SHPO); Indiana Department of Transportation (INDOT); INDOT—Fort Wayne District, City of Fort Wayne Engineer; Historic Landmarks Foundation of Indiana (now Indiana Landmarks); Allen County Historian; Allen County—Fort Wayne Historical Society; ARCH, Inc.; Fort Wayne Historic Preservation Review Board; and John Shoaff, Fort Wayne city council member. The Allen County historian declined to participate. The City of Fort Wayne, ARCH, Inc., Fort Wayne Historic Preservation Commission, and Indiana Landmarks Northern Regional Office, accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

Archaeological Consultants of Ossian completed an Archeological Field Reconnaissance Report on April 2, 2009. In the report, archaeologists stated, “It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places.” The archaeologist recommended no further work and project clearance. (See Appendix E: Report Summaries.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Both organizations accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

In a letter dated April 23, 2009, SHPO concurred with ARCH, Inc.’s request that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Also, due to potential eligibility of the Bridge over Spy Run Creek, SHPO requested that bridge historian Dr. James Cooper and Historic Spans Taskforce representative Paul Brandenburg be invited to join consultation. Historic Spans Taskforce accepted the invitation to join consultation. The SHPO also requested that the APE be “carefully delineate[d]” to take into account impacts. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On July 2, 2009, Structurepoint transmitted the Archaeological Field Reconnaissance report to the SHPO. (See Appendix F: Correspondence and Meeting Minutes.)

On July 9, 2009, the Irvington Park Neighborhood Association joined consultation. (See Appendix B: Consulting Parties.)

¹ These are the sources that the Westerly Group lists in the Historic Property Report (2009), page 8.

On November 9, 2009, Structurepoint sent consulting parties and SHPO a copy of the HPR for review and comment. The HPR recommended the Brookview-Irvington Park National Register Historic District, Bridge over Spy Run (NBI No. 0200273), and the House at 315 East State Boulevard eligible for listing in the NR. The HPR also recommended the portion of State Boulevard within the Brookview-Irvington Park district as individually NR eligible and contributing to the district. (See Appendix E: Report Summaries and Appendix F: Correspondence and Meeting Minutes.)

On December 1, 2009, Structurepoint sent an invitation to consulting parties to attend a meeting on December 15, 2009, to discuss the identification of historic properties. On December 8, 2009, Structurepoint provided a revised agenda. On December 15, 2009, a consulting party meeting was held at the City-County Building in Fort Wayne to discuss the identification and evaluation of historic resources per the HPR and future steps in the Section 106 process. Consulting parties requested that the APE be enlarged but offered no additional information regarding the identification and evaluation of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On July 6, 2011, Structurepoint sent an email to consulting parties conveying digital copies of its letters to SHPO. Structurepoint stated, "Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15 day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters." The letter also stated, "if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a 'cc' during this Section 106 process." (See Appendix F: Correspondence and Meeting Minutes.)

On July 13, 2011, an agency meeting was held with FHWA, INDOT, and SHPO to discuss SHPO's comments on the recent Purpose and Need submission; how to address the comments and concerns of consulting parties; Section 4(f); and ACHP involvement. (See Appendix F: Correspondence and Meeting Minutes.)

On August 15, 2011, Structurepoint sent a letter by post to consulting parties informing them of a consulting party meeting scheduled for September 1, 2011. Enclosures included a meeting agenda, agency coordination meeting minutes, a copy of a letter to SHPO, a copy of a letter to FHWA, Historic Bridge Programmatic Agreement Alternatives Analysis, Individual Section 4(f) Alternatives Analysis, Corridor Alternatives Map, and Consulting Party Questions/Comments and Responses. Structurepoint conveyed the same data electronically in an email dated August 16, 2011. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated August 29, 2011, FHWA wrote to the Advisory Council on Historic Preservation (ACHP) asking if they would be involved in consultation for this project. According to FHWA's letter, "FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A - Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), " *... adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*" and (c)(3) for " *... cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council's involvement could help resolve ...* " (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 1, 2011, at Citizens Square in the City of Fort Wayne to discuss project updates; purpose and need update; consulting party comments and responses document; alternatives review; and future steps. Following the meeting, Structurepoint agreed to prepare a three-lane design alternative for review. It was also decided that a another

consulting party meeting would take place after the issuance of the 800.11 documentation and the finding of “Adverse Effect” and at that time the group would discuss mitigation and forming an advisory committee to consult on mitigation measures. (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, an agency meeting with FHWA, INDOT, and Structurepoint was held to follow-up on the consulting party meeting. (See Appendix F: Correspondence and Meeting Minutes.)

On September 29, 2011, Structurepoint transmitted the following material to consulting parties: meeting minutes from the September 1, 2011 consulting party meeting; agency coordination meeting summary from September 2, 2011; revised individual Section 4(f) analysis including an additional alternative; traffic data from NIRCC; and ACHP correspondence. (See Appendix F: Correspondence and Meeting Minutes.)

In February 2012, Structurepoint contracted with Weintraut & Associates, Inc. (W&A) to update the HPR through an Additional Information Report (AI). The purpose of the AI Report was to supplement the HPR following the inclusion of two new NR-listed resources within the APE.

On March 6, 2012, historians from W&A walked and drove the APE, viewed all the resources within the APE, and photographed and recorded survey notes about resources greater than fifty years of age considered or rated Contributing or higher. Historians also field verified the APE at that time.

Historians for W&A published an AI report in April 2012. The report identified two districts that were listed in the NR after publication of the HPR: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). Portions of both districts are within the APE. The Bridge over Spy Run (NBI No. 0200273) was previously determined eligible for listing in the NR. Historians expressed the opinion that proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts and recommended a finding of “Historic Properties Affected—Adverse Effect” for the project. (See Appendix E: Report Summaries.)

On May 22, 2012, Structurepoint conveyed the AI Report to consulting parties, superseding their letter of May 17, 2012. (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an agency meeting was held with FHWA, INDOT, and SHPO to discuss the State Boulevard Project and the eligibility recommendations and findings.

On July 3, 2012, Archaeological Consultants of Ossian conducted an additional records check at the DHPA and on July 5 and 6, 2012 conducted a field reconnaissance for additional project areas. An Archaeological Short Report, completed July 11, 2012, concluded the Phase Ia reconnaissance located no archaeological resources and recommended project clearance. (See Appendix E: Report Summaries.)

On July 16, 2012, Structurepoint conveyed an archaeological short report for the additional area required for the State Boulevard Improvements project. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2012, Structurepoint sent a letter to consulting parties conveying FHWA's Findings and Determinations for the project. Structurepoint invited consulting parties to comment on the letter and to attend a consulting party meeting on September 19, 2012, to discuss the resolution of adverse effects. No comments were received regarding the identification of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On September 18, 2012, Structurepoint sent an email and attachment to consulting parties regarding project impacts to houses at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard—within the Brookview-Irvington Historic District. The letter stated: “It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions.” (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 19, 2012, to discuss the resolution and mitigation of adverse effects. (See Appendix F: Correspondence and Meeting Minutes.)

No further efforts to identify historic properties, including consultation, took place as part of this Section 106 undertaking.

Timeline of Formal Consultation

(See Appendix F: Correspondence and Meeting Minutes for referenced correspondence.)

March 23, 2009:	Structurepoint initiates consultation by sending a Section 106 Early Coordination Letter
April 23, 2009:	SHPO comments on the Early Coordination Letter
July 2, 2009:	Structurepoint sends archaeological report to SHPO
November 9, 2009:	Structurepoint transmits a copy of the HPR to SHPO and consulting parties
December 1, 2009:	Structurepoint sends invitation to a consulting party meeting
December 14, 2009:	SHPO comments on the HPR and Archaeology Report
December 15, 2009:	Consulting party meeting held in Fort Wayne
December 28, 2009:	Structurepoint sends minutes from consulting party meeting
January 27, 2010:	SHPO comments on minutes of December 15, 2009, consulting party meeting and asks questions regarding Purpose and Need
February 4, 2010:	Structurepoint sends a CD to SHPO containing the City of Fort Wayne 2005 Flood Control Study, traffic data, and revised meeting minutes
March 10, 2010:	SHPO comments on revised minutes from the consulting party meeting and the other informational items sent on February 4, 2010
May 19, 2011:	Structurepoint responds to questions raised in SHPO correspondence comments of January 27, 2010 and March 10, 2010
June 17, 2011:	Structurepoint sends documents missing from May 19, 2011 transmittal and extends comment period for another fifteen days
July 5, 2011:	SHPO responds to Structurepoint’s letters of January 27, 2010 and March 10, 2010

July 6, 2011:	Structurepoint emails consulting parties digital copies of letters to SHPO
July 13, 2011:	Agency meeting with FHWA, INDOT, and SHPO discussing SHPO's comments on recent Purpose and Need submission; how to address consulting parties comments; Section 4(f); and Inviting ACHP involvement
August 15, 2011:	Structurepoint sends an invitation to consulting party meeting scheduled for September 1, 2011
August 15, 2011:	Structurepoint sends a letter to FHWA requesting the agency invite the ACHP to participate
August 16, 2011:	Structurepoint conveys August 15, 2011, letter to FHWA electronically to consulting parties
August 29, 2011:	FHWA asks the ACHP to participate in consultation
September 1, 2011:	Consulting party meeting held in Fort Wayne
September 2, 2011:	Agency meeting with FHWA, INDOT, and Structurepoint to follow-up on the consulting party meeting held the previous day
September 22, 2011:	ACHP requests additional information regarding the project in order to determine if its participation is warranted
September 29, 2011:	Structurepoint transmits meeting minutes and additional information that consulting parties had requested to consulting parties
November 7, 2011:	SHPO responds to the Structurepoint's letters of August 15, 2011, and September 29, 2011
May 22, 2012:	Structurepoint sends a letter conveying the AI Report to consulting parties and replacing a letter sent May 17, 2012
June 20, 2012:	Agency meeting discusses the State Boulevard Project and the eligibility recommendations and findings
June 22, 2012:	SHPO responds to AI Report
July 2, 2012:	Structurepoint sends SHPO a letter answering questions raised in its letter dated November 7, 2011
July 11, 2012:	Archaeological Consultants of Ossian prepares an Archaeological Short Report for additional areas of the project. In the report, archaeologists conclude the Phase Ia reconnaissance located no archaeologists and recommended project clearance
July 16, 2012:	Structurepoint sends an archaeological short report for the additional area required for the State Boulevard Improvements project to SHPO
July 31, 2012:	ACHP declines to participate in consultation

August 1, 2012:	SHPO responds to the June 20, 2012, agency meeting
August 13, 2012:	SHPO concurs with the archaeological short report
August 29, 2012:	Structurepoint sends letter conveying FHWA's Findings and Determination of Adverse Effect and the draft 800.11(e) documentation. The letter includes an invitation to a consulting party meeting
September 18, 2012:	Structurepoint transmits letter regarding project impacts to three parcels on East State Boulevard
September 19, 2012:	Consulting parties meeting
October 5, 2012:	INDOT informs consulting parties of they will have an opportunity to comment on Section 106 materials during the Environmental Assessment review period
November 15, 2012:	SHPO responds to draft MOA
December 18, 2012:	Agency meeting to discuss mitigation

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Three historic properties will be affected by the undertaking: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek.

Fort Wayne Park and Boulevard System Historic District (NR, 2010). The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and Bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NR in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized."

Brookview-Irvington Park Historic District (NR, 2011). The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street and Jacobs Avenue. The district contains a total of 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965 and represents the construction dates of most buildings within the

historic district and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination.

Bridge over Spy Run (NBI No. 0200273). The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

The proposed project will include the widening, realignment, and elevation of State Boulevard approximately 190 feet south and the construction of a new access road between the existing and proposed State Boulevard alignment (and the conversion of some existing intersections to cul-de-sacs). The project also proposes modified alignments at Westbrook Drive and Clinton Street. Additionally, the proposed project requires approximately fifteen residential relocations, and the removal of trees and older street amenities. Construction of concrete curb and gutter, raised median, left turn lanes, decorative lighting, modified traffic lights, landscaped utility strip, curb inlets, and storm sewers. A new bridge structure will replace the existing bridge over Spy Run Creek. The project also includes a prefabricated pedestrian trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive. Sidewalk ramps will be extended from the proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. Additionally, removal and relocation has been selected as the most prudent and feasible alternative for the Bridge over Spy Run. The elevation of State Boulevard and the removal of the properties that contribute to the district will result in the creation of new public spaces in a residential setting and the bifurcation of the Brookview-Irvington Historic District. These elements will result in an adverse effect on the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT--INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." Because construction of the Pufferbelly Trail is reasonably foreseeable and because environmental assessment of the Pufferbelly Trail undertaking is currently taking place, effects from that project have been included in this application of the criteria of adverse effects.

Fort Wayne Park and Boulevard System Historic District. The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Fort Wayne Park and Boulevard System Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause "[p]hysical destruction of or damage to all or part of the property," particularly through the realignment and elevation of State Boulevard. Further, land from identified Contributing features within the district, including the Bridge over Spy Run and "Sloping Hills and Natural Features" (SR-10), will be taken.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause "[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines."

Per 36 CFR 800.5(a)(2)(iii), the undertaking will cause “[r]emoval of the property from its historic location,” at the location of the State Boulevard realignment.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The Contributing Bridge over Spy Run Creek will be replaced, a prefabricated trail bridge, retaining walls, and ramps (associated with the Pufferbelly Trail) will be constructed over State Boulevard at the abandoned New York Central railroad right-of-way, the Contributing State Boulevard will be realigned, widened, and elevated. The Contributing Eastbrook Drive will be converted to a cul-de-sac.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, widening and elevation of the Contributing State Boulevard, the construction of a cul-de-sac at the Contributing Eastbrook drive, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps and retaining walls (associated with the Pufferbelly Trail) over the Contributing State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the Contributing Bridge over Spy Run Creek.

Per 36 CFR 800.5(a)(2)(vi), the project will not result in “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), there will be no “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

Brookview-Irvington Park Historic District. The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Brookview-Irvington Park Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” The proposed undertaking will result in the relocation of Contributing residential resources, the removal of private space, and change the orientation of the Brookview neighborhood plat, a Contributing resource. The elevation of the re-aligned State Boulevard will result in the bifurcation of the district.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the undertaking will result in the “[r]emoval of the property from its historic location.” State Boulevard will be widened, elevated, and realigned south from its historic location; residential relocations will take place within the Brookview-Irvington Park Historic District. The Bridge over Spy Run, a Contributing resource, will be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance” through the realignment of some roadways, conversion of some intersections to cul-de-sacs, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. The landscape of the area will be modified by the realigned and elevated State Boulevard and by the realignment of the original Brookview-Irvington Park plat, both of which

were identified as Contributing to the district. The removal of the Contributing homes will change the character of the plat since the open, public spaces will be much larger than they are presently.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, elevation, and widening of State Boulevard, the realignment of Westbrook Drive and Clinton Street, the construction of cul-de-sacs at some locations, the removal of some streetscape elements and the introduction of other streetscape elements, construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. All of these elements will change the character of the district along State Boulevard.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

Bridge over Spy Run (NBI No. 0200273). The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the previously determined NR-eligible Bridge over Spy Run.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” Bridge over Spy Run will either be removed from its present location or demolished as part of this undertaking. The removal or demolition will be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The pending removal or demolition of the bridge is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.” Bridge over Spy Run will be removed or demolished as part of the undertaking, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iii), the property will be removed from its historic location either by demolition or removal, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” Bridge over Spy Run will either be removed from its present location or demolished. The removal or demolition is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The bridge’s setting will change as a result of: the realignment and widening of State Boulevard, the construction of cul-de-sacs near the bridge, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps (associated with the Pufferbelly Trail) over State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the bridge over Spy Run Creek, and the approximately fifteen residential relocations.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .” though the bridge will be removed and demolished.

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

Consultation has occurred regarding Purpose and Need, Alternatives, and Traffic Data in order to reduce impacts on historic properties. Consulting parties offered comments regarding the project purpose and need, selection of project alternatives, and traffic data during the Section 106 process for this undertaking and requested changes to the alternatives analysis as a result of that information. At the request of consulting parties, a third alternative—Alternative 3D—was considered. (Consulting party comments on purpose and need, alternatives selection, and traffic data—which will be included as part of the Environmental documentation—are included in Appendix F: Correspondence and Meeting Minutes.)

Consultants have met with consulting parties and with the Indiana SHPO to discuss options to avoid, minimize and mitigate adverse effects on December 15, 2009 (consulting parties and SHPO), July 13, 2011 (SHPO), September 1, 2011 (consulting parties and SHPO), September 2, 2011 (SHPO), June 20, 2012 (SHPO), September 19, 2012 (consulting parties and SHPO), and December 18, 2012 (SHPO). The Indiana SHPO agreed to enter into a Memorandum of Agreement (MOA) at a meeting held on June 20, 2012. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of the Bridge over Spy Run; Restore character of State Boulevard within the district; and Educational mitigation.

A consulting party meeting was held on September 19, 2012, to discuss stipulations for the MOA and to solicit other mitigation ideas. At that meeting, Structurepoint shared the following efforts to minimize impacts: 1. A three-lane road with center turn lane was discarded because it did not provide the required level of service. 2. Reduction of right-of-way impacts to allow three properties to remain in place. (After agency consultation, this was discarded.) 3. Maintaining existing curvature between Eastbrook and Terrace Road. 4. Maintaining existing curb lines of Eastbrook Drive where possible.

The following alternatives have been evaluated for the Bridge over Spy Run:

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structures Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge structure is five years from the date of the inspection of the report (2011). The existing bridge is currently below the flood elevation of the St. Mary’s River which causes the bridge to be overtopped with backwater from the St. Mary’s River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) “This flooding is caused primarily by backwater from the [St.] Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

According to the recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010. Six out of the seventeen years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed

due to the flooding events. Road closure due flooding events appears to be happening more consistently in recent years, restricting emergency traffic more often.

1. Rehabilitation for Continued Vehicular Use (two-way option)

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

2. Rehabilitation for Continued Vehicular Use (one-way option)

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven

feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

3. Bypass (Non-vehicular use)

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the *Fort Wayne Journal-Gazette* and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546.

The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

4. Replacement

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

5. Relocate and Replacement

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately seven feet to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure was marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge was listed as available for re-use in the *Fort Wayne Journal-Gazette* on February 2, 2010. The bridge was also listed as available on the INDOT Website and signs were posted on the bridge offering it for re-use. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use.

Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for

vehicular use. While this alternative does meet the project's purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

The following alternatives have been evaluated for the State Boulevard Reconstruction project:

Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) terminus of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the

corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately seventy-five residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility. This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

Access Alternate 1

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Access Alternate 2 (Preferred Access Alternative)

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State

Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.

Access Alternate 3

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately twenty residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety

concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to four-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately four to five-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project’s purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

Alternative 3D: Preferred Alignment with 3-Lane Typical Section

This alternative is similar to Alternative 3A (Preferred Alternative) but features a three-lane typical section rather than a four-lane typical section. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to three-lanes while correcting the substandard horizontal curve.

By reducing the typical section from four-lanes (Alternative 3A/Preferred Alternative) to three-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four-lanes to three-lanes is only ten-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton

Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.

The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

In response to requests for additional review time for the 800.11(e) and findings and determinations distributed on August 29, 2012 INDOT wrote an email on October 5, 2012, to consulting parties regarding comments on the project. INDOT informed consulting parties that the 800.11 documentation would be updated and the finding, draft Memorandum of Agreement [MOA], draft 4(f) evaluation, "will be released with the Draft Environmental Assessment for an additional comment for both consulting parties and the public." Therefore, the comment period for the 800.11 documentation, draft MOA, and draft 4(f) evaluation would not be extended. (See Appendix F: Correspondence.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet with it and its consultants to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain, landscape architect, made the presentation. Cain's plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A: Plans.)

After discussion, the City and its consultants agreed that in addition to the mitigation landscape plan, they would look for other ways to mitigate the adverse effect, such as grants to rehabilitate the facades of existing houses (if practical and legally viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

During the course of consultation, the following organizations have responded affirmatively to the invitation to join consultation: City of Fort Wayne; Friends of the Parks of Allen County; Allen County Historian; Indiana Landmarks—Northern Regional Office; Fort Wayne Historic Preservation Commission; ARCH, Inc.; Brookview Neighborhood Association; Indiana Historic Spans Taskforce; Irvington Park Neighborhood Association. Additionally, the following individuals or organizations participated in or requested to join consultation: Charley Shirmeyer, Northside Galleries; Albert Cohan, Westbrook 5, LLC; Thomas Niezer, Barret & McNagny, LLP; Ronald Ross, Martin Riley Architects and Engineers; Dan Ernst, Earth Source, Inc.; Jan Dailey, State Boulevard Resident. (See Appendix B: Consulting Parties.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks of Allen County and Brookview Neighborhood Association be invited to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On April 23, 2009, SHPO wrote in response to the notification concerning the reconstruction of State Boulevard and requested a literature review, historic context, research methodology, property descriptions, and NR eligibility evaluations and recommendations to aid analysis of the project. SHPO recommended the Friends of the Parks and Boulevard Neighborhood Association, Indiana Historic Spans Task Force, and bridge historian Dr. James L. Cooper be invited to participate as consulting parties. (See Appendix F: Correspondence and Meeting Minutes.)

On December 7, 2009, Jan Dailey wrote in response to the HPR: "I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effects." In regard to the project, she stated, "While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling

that residents of this neighborhood share.” She also requested that “a separate study be conducted in possible land use of the former Kroger Fuel Center.” (See Appendix F: Correspondence and Meeting Minutes.)

On December 8, 2009, Indiana Landmarks—Northern Regional Office wrote in response to the HPR. Landmarks agreed that Brookview-Irvington Park Historic District is eligible for the NR and suggested modifications to the HPR recommendations in light of NR nominations being composed by ARCH, Inc. Indiana Landmarks also requested more information on the proposed design in order to comment on a preliminary effect finding. Indiana Landmarks disagreed with the APE, asked some preliminary questions regarding the purpose and need in relation to historic properties, questioned the appropriateness of including a “trail bridge” in this Section 106 investigation, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, and expressed the need for a “broad range of alternatives” to be included as part of the project options, and expressed concerns about the impacts of a different project on this Section 106 undertaking. (See Appendix F: Correspondence and Meeting Minutes.)

On December 9, 2009, ARCH, Inc. wrote in response to the HPR. Arch, Inc. agreed with the recommendation of eligibility for the Brookview-Irvington Park Historic District, noting that an NR nomination was being prepared. ARCH, Inc. requested the inclusion of proposed design maps, requested more detailed data regarding the project purpose and need, questioned the inclusion of the “trail bridge” in this Section 106 study, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, disagreed with the APE, stated the importance of consulting “early in the undertaking’s planning,” expressed concerns about the impacts of a different project on this Section 106 undertaking and specifically stated “we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate.” Finally, ARCH, Inc. agreed with statements regarding flooding in the area, but stated they “contend that this is an issue which is recent.”

In a letter dated December 10, 2009, Julie Donnell, president of the Friends of the Parks of Allen County, Inc. wrote in response to the meeting agenda and HPR. Donnell expressed concern over the project’s Section 106 process, including the concern “that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and that after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.” The letter also commented on the Brookview Neighborhood, concurring with other consulting party comments on the resource and posing questions regarding the project’s effects on the landscape, and expressed the integral importance of the landscape in the Brookview neighborhood’s integrity. The letter requested considering the inclusion of the Cultural Landscape Foundation in the Section 106 process. (See Appendix F: Correspondence and Meeting Minutes.)

On December 14, 2009, SHPO wrote in response to the Draft HPR. Regarding the APE, SHPO wrote that “we are not yet prepared to comment on the adequacy of the APE.” SHPO commented on the HPR in the same letter, stating, “[o]ur initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.” SHPO also wrote in response to the archaeological report that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [NR] within the area which was surveyed for this project by Archaeological Consultants of Ossian,” but noted that the final alignment was not yet determined and that further archaeological investigations may be necessary. SHPO asked for more

information on the project alignment and the purpose and need. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held December 15, 2009 in Fort Wayne, consulting parties expressed concern with the APE used in the HPR, noted the importance of the “park-like setting” to the Brookview neighborhood, and questioned the selection of alternatives. (See Appendix F: Correspondence and Meeting Minutes.)

On January 27, 2010, SHPO responded to minutes of the consulting party meeting held December 15, 2009. SHPO requested more information regarding the purpose and need but stated that perhaps their questions would be answered in the forthcoming information packet for consulting parties. SHPO expressed concern about the Purpose and Need of the project. SHPO also asked for “clarification” on “the substandard nature of the roadway curvature on State Boulevard,” especially in light of statements from consulting parties “that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard.” SHPO also stated “[w]e believe it is important for FHWA to evaluate this project’s purpose and need carefully before the Section 106 consultation proceeds much further. . . . Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.” Regarding the APE, SHPO asked some questions given the list of the alternatives provided at the December 15, 2009, consulting party meeting as well as in light of statements from consulting parties. “If . . . diversion of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.” SHPO also stated that “we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.” (See Appendix F: Correspondence and Meeting Minutes.)

SHPO wrote on March 10, 2010, in response to the revised meeting minutes from the December 15, 2009, meeting. In the letter, SHPO stated that the Spy Run Bridge had been finalized as a Non-Select, NR-eligible bridge per the Indiana Statewide Historic Bridge Inventory. SHPO restated the understanding that Arthur Shurcliff intended “that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses. . . that contribute to the Brookview-Irvington Park Historic District are demolished.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 15, 2011, Jill D. Downs, chairperson of the Preservation Committee of ARCH, Inc., wrote to the Deputy SHPO regarding Structurepoint’s May 19, 2011, letter. Downs questioned whether the revised Purpose and Need would “trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.” (See Appendix F: Correspondence.)

On June 16, 2011, John H. Shoaff wrote that as a member of the city council, they “face an unpleasant two-fold task of fighting for a properly democratic, participatory process...” (See Appendix F: Correspondence and Meeting Minutes.)

On June 16, 2012, Todd Zeiger, Indiana Landmarks sent an email asking for clarification of whether consulting parties were to comment on the May 19, 2012, letter and requesting a thirty day extension to the review period. (See Appendix F: Correspondence and Meeting Minutes.)

On June 17, 2011, Julie Donnell of the Friends of the Parks of Allen County sent an email to American Structurepoint conveying her letter dated June 14, 2011, in which she requested an additional thirty days of review. She expressed surprise that changes were made to Purpose and Need without "communicating this." In the text of the email, Donnell wrote: "In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond." The email also said, "We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project." (See Appendix F: Correspondence and Meeting Minutes.)

On July 1, 2011, John H. Shoaff wrote to point out discrepancies in traffic numbers presented. (See Appendix F: Correspondence and Meeting Minutes.)

On July 5, 2011, SHPO responded to Structurepoint's letter of May 19, 2011. In their letter, SHPO wrote that it appeared appropriate to expand the APE "if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project" and stated foreseeable "areas where the character of use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well." SHPO also requested Structurepoint review previous correspondence and meeting minutes and "make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 10 letter." SHPO also suggested that Structurepoint share comments "that have been or shortly will be received in response to your May 19 and June 17 letters." The letter re-stated comments from December 14, 2009, regarding the archaeology report. (See Appendix F: Correspondence and Meeting Minutes.)

Suzanne Slick, of the Irvington Park Neighborhood Association, sent an email on July 6, 2011, expressing disappointment with the project's evaluation of impacts to neighborhood residents. The letter also stated, "There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability." (See Appendix F: Correspondence and Meeting Minutes.)

On July 7, 2011, Michelle Briggs Wedaman of the Brookview Neighborhood Association emailed Structurepoint and asked that her email address be updated in the project record and that she would provide comments on behalf of the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

At an Agency Coordination meeting held July 13, 2011, SHPO suggested that Structurepoint coordinate to evaluate if the project would result in a need to change the NR district boundaries. SHPO also suggested that American Structurepoint more specifically address the consulting party issues and comments in coordination. It was also agreed upon that the ACHP should be invited to participate in the State Boulevard project at this stage in the Section 106 process, rather than later. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2011, Suzanne Slick wrote regarding the consulting party comment and response form. Slick wrote regarding the consultation process, "People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive." Slick expressed concern with the proposed project and provided links to websites associated with

various aspects encountered in this project. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held September 1, 2011, consulting parties questioned the response process and whether all comments had been shared. Consulting parties were encouraged to respond to any Section 106 correspondence, even if the thirty day time period had passed. An effort would be made to post all Section 106 documentation on the City of Fort Wayne's website. Consulting parties suggested that the project include consultation with a professional landscape architect. It was also noted that the State Boulevard curve is included in the Fort Wayne Park and Boulevard System Historic District which is different from the Brookview-Irvington Historic District. SHPO requested the consultant "look at the implications of reduction the width of a new alignment. . .[and]. . . evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements." (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, at the Agency Meeting with FHWA and INDOT, FHWA stated it would follow-up on its invitation to the ACHP, noting that the ACHP's involvement in the process would be beneficial. During the meeting it was agreed that Structurepoint would provide consulting parties with a more elaborate alternatives analysis, would look into developing a Section 106 page for this project on the City of Fort Wayne's website, and that an addendum to the HPR would be prepared. (See Appendix F: Correspondence and Meeting Minutes.)

The ACHP responded to FHWA's invitation to join consultation on September 22, 2011. ACHP requested additional documentation in order to "determine whether our participation in the consultation to resolve adverse effects is warranted." (See Appendix F: Correspondence and Meeting Minutes.)

On November 7, 2011, SHPO responded to the material conveyed August 15, 2011, and September 29, 2011. Regarding the Brookview-Irvington Park Historic District, SHPO stated, "Having considered the marked aerial photograph shown at the last consulting party meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative." However, SHPO added, the proposed realignment of State Boulevard within the district "is not an ideal situation from a [NR] boundary delineation standpoint." Further, SHPO stated, "We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished." SHPO also offered additional comments from the September consulting party meeting that had not been recorded in the meeting minutes regarding the alternatives analysis. SHPO also questioned the feasibility of converting the existing Spy Run Bridge into a pedestrian bridge. SHPO stated they would also recommend, "where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden." SHPO also suggested shifting the proposed alignment somewhat to the east to better reflect Kessler's original plan for connecting State Boulevard. SHPO noted that this change may "result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along State Boulevard. Even if the project costs were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan." (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an Agency meeting was held to discuss the State Boulevard Project. At the meeting, Structurepoint reviewed the responses to the SHPO letter of November 7, 2011, and

agreed to send them in writing. It was decided to hold a meeting with consulting parties in early September to discuss the Additional Information HPR, to present the preferred alternative and to discuss the MOA. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of bridge over Spy Run; Restore character of State Blvd within the district; and Educational mitigation.

On June 22, 2012, SHPO provided comment on the AI Report. In the letter, SHPO stated, "we agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the [APE], for inclusion in the [NR]." SHPO agreed that the house at 315 East State Boulevard "does not appear to possess sufficient historical or architectural significance or integrity to be eligible of inclusion in the [NR]." SHPO also commented on the explanatory note contained in the Fort Wayne Park and Boulevard NR nomination form which stated the portion of State Boulevard within the Brookview-Irvington Historic District was individually eligible for the NR. SHPO stated, "we do not consider that comment . . . to confer individual eligibility on State Boulevard or any part of it." SHPO further stated, "we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the [APE] is individually eligible" for the NR, but added "[w]e do not disagree, however, with the Fort Wayne Park and Boulevard system nomination identification of the portion of State Boulevard in question as a contributing resource to that historic district." (See Appendix F: Correspondence and Meeting Minutes.)

Regarding archaeology, SHPO stated, "Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources." SHPO noted that one example of areas that may need archaeological survey included "a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report." If the entire lot would need to be acquired as part of the project, "then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated July 31, 2012, the ACHP wrote that "[b]ased upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time." However, the Council did request to be notified in the event of an Adverse Effect finding and at that time the Council would "re-evaluate the undertaking . . . and advise you whether or not we have changed our decision regarding participation in consultation." (See Appendix F: Correspondence and Meeting Minutes.)

On August 13, 2012, the Indiana SHPO concurred with the archaeology short report (Stilwell, 7/11/12) that "no further investigations appear necessary at these additional portions of the project area" and that the office had not identified any archaeological resources listed or eligible for listing in the NR. (See Appendix F: Correspondence and Meeting Minutes.)

At the consulting party meeting held on September 19, 2012, consulting parties were asked to provide input into mitigation for the proposed undertaking. Most comments focused on purpose and need for the project; some spoke about traffic issues. Michelle Briggs Wedaman (Brookview Neighborhood Association) asked for context sensitive solutions at the beginning of the project rather than the end. Susan Haneline (property owner) asked why the owners of the three residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes. Todd Zeiger (Indiana Landmarks) encouraged the involvement of the ACHP because he feels that there was anticipatory demolition as part of a flood control project. He asked that it be noted in this documentation that there is a bifurcation of the district. Tom Cain (City of Fort Wayne) pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in

those landscape elements needs discussion in the documentation. The visual and special components of the larger landscape need to be understood so they can be addressed in a mitigation discussion. Michael Galbraith (ARCH, Inc.) encouraged ACHP involvement, objected to the change in historic consultant, asserted that the APE is inappropriate, and raised the question of cumulative impacts. Edward Welling (Friends of the Parks of Allen County) said that mitigation is premature since the APE is not appropriate; the MOA should be postponed until Environmental Assessment is complete. Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. Dr. James Glass (Deputy SHPO) expressed reservations that consensus can be developed for this project; he stated that this meeting was the time for consulting parties to put forth mitigation ideas. John Carr (SHPO staff) requested any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion. Mr. Galbraith objected to the timing of the consulting party meeting; Patrick Carpenter, manager of the INDOT-CRO, said that the timing was established so that consulting parties could discuss mitigation and formulate new ideas. Ms. Wedamen said that she did not believe that the public process has been followed. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 14, 2012, Karl Dietsch wrote regarding a safety issue in the proposed project area. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 17, 2012, eleven residents of the Brookview Neighborhood jointly submitted a letter regarding the State Boulevard project. The letter expressed support of the project. The residents stated, "We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated to the south of its current location" and went on to conclude, "We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issue unresolved." (See Appendix F: Correspondence and Meeting Minutes.)

Sara Kruger Geyman, a member of the public, wrote in response to the meeting held September 19, 2012. (Note that the letter conveying responses to the consulting party meetings was dated August 21, 2012, and is likely a typo.) Geyman expressed concern "that residents are not and have not been consulted in this matter" and expressed dissatisfaction with meeting's facilitation. Geyman offered comments to the project in general, objecting to its necessity and, regarding Section 106, stating: "Mitigation is premature in a plan and a process that has been faulty from the beginning. It is a proverbial lollipop stuck in the hands of resident to quiet them down and distract them from the truth." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 1, 2012, Susan R. Haneline, a Brookview neighborhood home owner, expressed support for the project, noting that the current problems with flooding and bridge deterioration "do nothing to showcase what IS historical about the neighborhood." Haneline added, "We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood." Haneline's letter also included photographs showing recent flooding in the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

Susan Haneline submitted an additional letter dated October 2, 2012. Haneline stated the current proposed design, "seems . . . to actually enhance historic vision, not cause it to be destroyed." Haneline offered suggestions to "respect the historic vision," including: 1.) "Installing historically correct lighting in the area"; 2.) "Plantings and green space that gives the area a park like feel, such as period style benches, grouping of trees and flowers, perhaps even brick style side walks"; 3.) "stone or brick entrance pillars for the neighborhood"; 4.) adding trees and flower beds to the bifurcated State Boulevard; 5.) "small monuments" conveying the history of the neighborhood and

Arthur Shurcliff; 6.) “find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 3, 2012, John Shoaff wrote regarding the project, consulting party meeting, and 800.11 material. Shoaff wrote, “I cannot support the current State Boulevard widening plan in anything like its present form. . .” In particular, Shoaff objected to plans to elevate the road as a “perversion of the proper use of the ‘By-pass and Arterial concept’ . . .” Shoaff identified “two legitimate needs” in the Brookview neighborhood: the repair or replacement of the Bridge over Spy Run Creek and the elimination of a “blind spot at the foot of State Boulevard, near the intersection with Westbrook.” Shoaff stated that project plans should address these needs but be “minimally harmful to the historic district.” Shoaff added that discussion of project planning and mitigation discussion “should await the outcome of the Environmental Assessment.” (See Appendix F: Correspondence and Meeting Minutes.)

Shoaff also included comments on the September 19, 2012, consulting party meeting. Shoaff responded to comments received by Michelle Briggs Wedaman from FHWA’s representative. Shoaff objected to the facilitation of the meeting stating “the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.”

Shoaff enclosed letter “signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen.” The letter objected to the State Boulevard project. (See Appendix F: Correspondence and Meeting Minutes.)

Also on October 3, 2012, Suzanne Slick wrote regarding the project and the consulting party meeting of September 19, 2012. Slick stated that not building the project is preferable to mitigation and objected to the facilitation of the consulting party meeting. The letter re-stated some comments offered previously by consulting parties regarding the Purpose and Need and design. Slick objected to the traffic data previously supplied by Structurepoint and offered two examples in which she found low-volume traffic while utilizing the State Boulevard. Slick stated the APE was inappropriate. (See Appendix F: Correspondence and Meeting Minutes.)

Julie Downs, Friends of the Parks of Allen County, submitted comments via a letter dated October 3, 2012. Downs stated the Friends of the Parks of Allen County agreed with the finding of adverse effect for the project but added “any discussion of mitigation is, at best, premature; at worst, the proposed [MOA] is a bad faith attempt to confuse an already complicated and unfair process.” Downs also stated the “APE is not comprehensive enough and should include historic districts along State Boulevard” and “it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.” Finally, on behalf of members of the Friends of the Parks of Allen County who attended the September 19, 2012, consulting party meeting, Downs objected to the facilitation of the meeting and concluded, “Under these circumstances, the public is not being served properly at all.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Jill Downs wrote regarding the 800.11(e) and draft MOA. Downs agreed with the project’s adverse effect finding but noted “the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration” and pointed to the September 19, 2012, consulting party meeting as proof of this breakdown. She stated it was premature to discuss mitigation because the Environmental Assessment had not been completed; the bifurcation of the district, elevation of State Boulevard, and the Pufferbelly Trail project should be added to the list of adverse effects; the Pufferbelly Trail project should be incorporated into the effects discussion; and the project has not fully accounted for the previous removal of several homes by the City of Fort Wayne which creates the impression of less impact

as a result of the project. Downs concluded by stating she did not see the need to reconstruct State Boulevard. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michael Galbraith of ARCH, Inc., wrote formally requesting an extension of the thirty-day comment period for the proposed MOA and mitigation measures. Galbraith stated, "We do not in any form, fashion, or manner concur with the proposed mitigation as present either in the draft supplied with the FHWA 4(f) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintraut." Galbraith also stated that "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact." (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michelle Briggs Wedaman of the Brookview Neighborhood Association, wrote requesting a thirty-day extension of the consulting party comment period to incorporate the material provided on September 18, 2012, into their comments. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Wedaman stated that previous questions from the December 2009 and September 2011 consulting party meetings "have remained unanswered," particularly those dealing "Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the area of impact of this project." Wedaman questioned how an appropriate discussion of mitigation could take place prior to the completion of the environmental assessment. (See Appendix F: Correspondence and Meeting Minutes.)

The SHPO wrote in response to the project in a letter dated October 4, 2012. SHPO concurred with the opinion of the archaeological short report, the Section 106 finding of effect and that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and Bridge on State Boulevard over Spy Run would all be adversely affected as part of this undertaking. SHPO expressed concern "about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District and suggested some minimization measures. In particular, SHPO wondered if "it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive." SHPO expressed sympathy for the preference of some property owners along the south side of State Boulevard who preferred to have their entire property, rather than a smaller portion, purchased, "However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect."

SHPO also offered suggestions for design for minimizing impacts and suggestions for mitigation, including an advisory team, use of context-sensitive designs, photographic documentation of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Todd Zeiger of Indiana Landmarks—Northern Regional Office wrote formally requesting a thirty-day extension on the comment period in light of the material conveyed September 18, 2012. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Zeiger stated "We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(f) compliance document." Zeiger added "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that

information and assess it within the context of the other informant provided in the 4(F) document.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Tom Cain, Fort Wayne urban designer and Creager Smith, Fort Wayne historic preservation planner, wrote regarding the project. Both agreed with the project's adverse effect finding. The letter listed twenty-one specific adverse effects of the project on the landscape to serve as the “potential basis of mitigation measures.” Cain and Smith also stated “we are available to assist in the development of mitigation design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on a team.” (See Appendix F: Correspondence and Meeting Minutes.)

On October 15, 2012, Tom Cain, City of Fort Wayne, called W&A to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but wanted to make suggestions within the context of SHPO's assessment of project impacts so that the City may address all adverse effects. Cain also stated that impacts to the Brookview neighborhood should be enumerated. (See Appendix F: Correspondence and Meeting Minutes.)

On October 16, 2012, W&A contacted Tom Cain in response to his phone call the previous day. W&A explained that Structurepoint was very glad to have his input on this project and, at a minimum, would consult with him prior to the agency meeting. Cain spoke about the landscape changes that would take place as a result of the undertaking, particularly the changes from private to public space around the undertaking. He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s. Cain stated he would like for mitigation to deal with changes in scale that will occur; tree planting should occur within three feet of the roadway (and not the standard ten feet required on highways.) Cain stated this would change the scale of the undertaking for the residents. Cain also stated he would convey additional mitigation suggestions via email and stated the importance of achieving the “right feel” for the space. (See Appendix F: Correspondence and Meeting Minutes.)

On November 15, 2012, SHPO wrote in response to Structurepoint's offer to draft specific language for the MOA. (See Appendix F: Correspondence and Meeting Minutes.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain (landscape architect/City of Fort Wayne) made the presentation. Cain's plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A, Plans.) Dr. James Glass (SHPO) expressed appreciation for the effort Mr. Cain had put forth for a thoughtful landscape plan. Dr. Glass said that his office needed time to digest but that he understood Mr. Cain's point that in a Section 106 sense, there was a need to mitigate for the houses and for the loss of historic character. He also understood that there are larger issues of flood control and engineering that make this project difficult. There was discussion of other resources that may be preserved as far as compensation for the lost historic resources (houses and landscaping). It was agreed that SHPO would be given time to digest the landscape design presented at the meeting and that the City and its consultants would look for additional ways to mitigate, such as grants to rehabilitate the facades of existing houses (if practical and legally

viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. Mary Ann Naber (FHWA preservation officer) suggested that the attendees look at the mitigation provided in Tampa. (See Appendix F: Correspondence and Meeting Minutes.)

No other comments were received.

A public notice of Adverse Effect will be posted in a local newspaper and the public afforded thirty (30) days to comment. If appropriate, this document will be revised to reflect those comments.

APPENDIX A. Plans

INDEX TO SHEETS

[illegible][illegible]

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE
DESIGNED:	ASJ	DRAWN:	OAK	
CHECKED:	SWC	CHECKED:	SMC	

INDIANA DEPARTMENT OF TRANSPORTATION			HORIZONTAL SCALE	BRIDGE FILE
			VERTICAL SCALE	DESIGNATION NO. 1005151
			SURVEY BOOK	SHEETS 2
			CONTRACT	PROJECT NO. 1005151

INDEX & GENERAL NOTES	
141	89

130

125

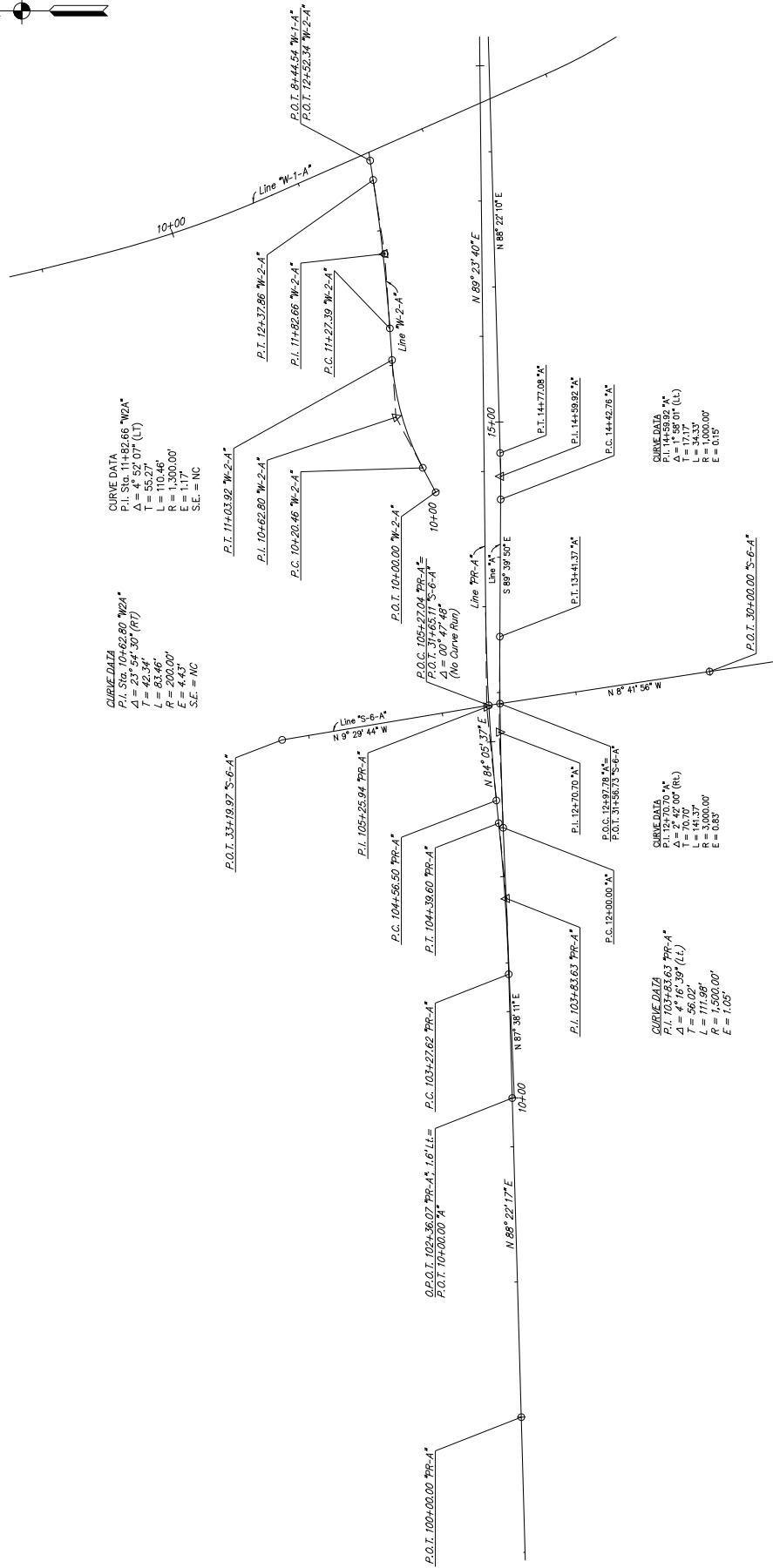
120



SEC. 35, T. 31 N., R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC	
CHECKED: SMC		PROJECT NO.		10051	
HORIZONTAL SCALE		BRIDGE FILE		DESIGNATION NO.	
VERTICAL SCALE		SURVEY BOOK		SHEETS	
INDIANA		DEPARTMENT OF TRANSPORTATION		4 of 89	
PLAT NO. 1		CONTRACT		PROJECT NO.	
				10051	



HORIZONTAL SCALE	BRIDGE FILE	
1" = 40'		
VERTICAL SCALE	DESIGNATION NO.	
	1005151	
SURVEY BOOK	SHEETS	
	5	of 89
CONTRACT	PROJECT NO.	
	1005151	

INDIANA
DEPARTMENT OF TRANSPORTATION

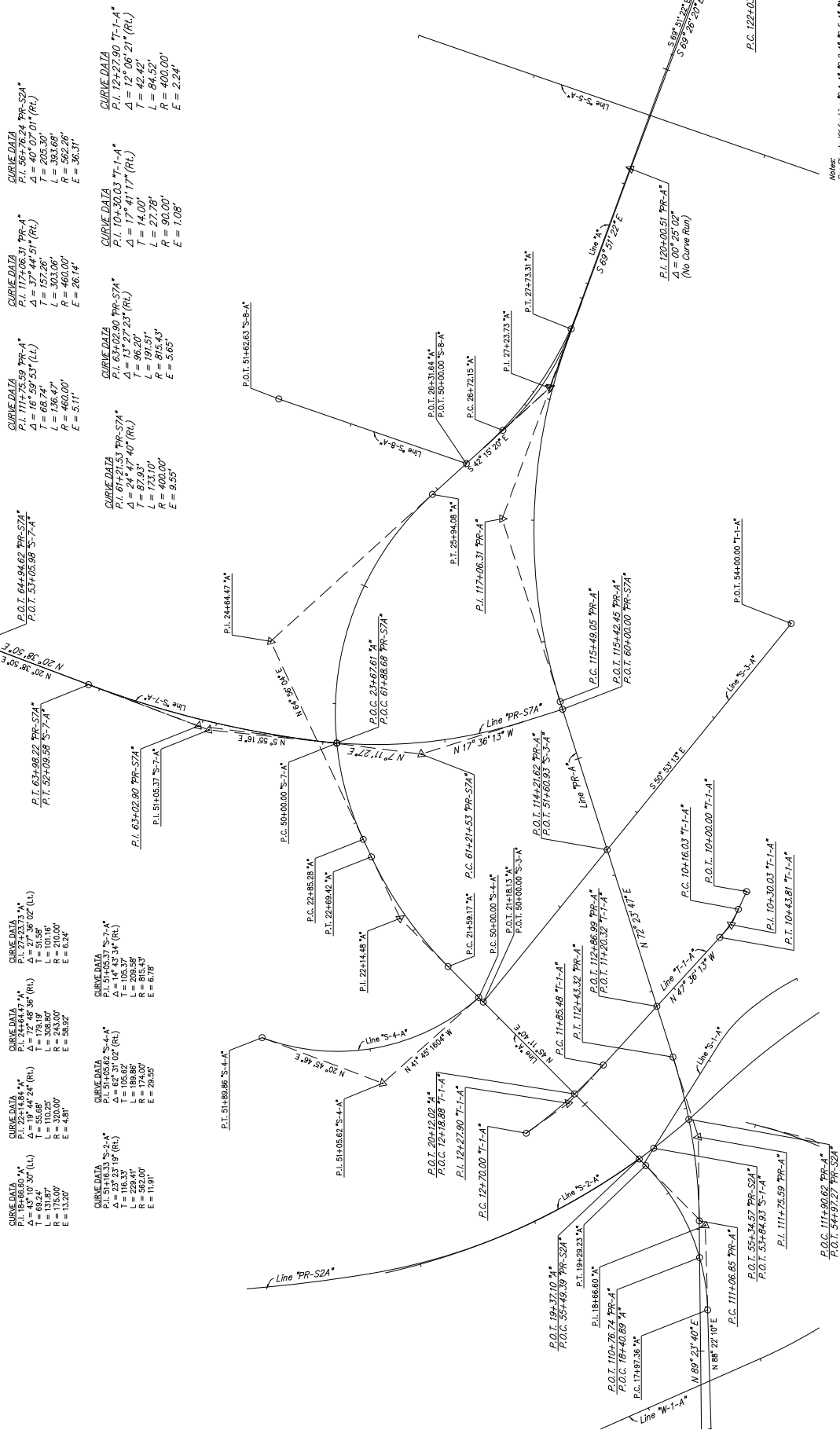
GEOMETRIC TIE-UP

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: _____	ASU _____	DRAWN: _____	CAK _____
CHECKED: _____	SMC _____	CHECKED: _____	SMC _____

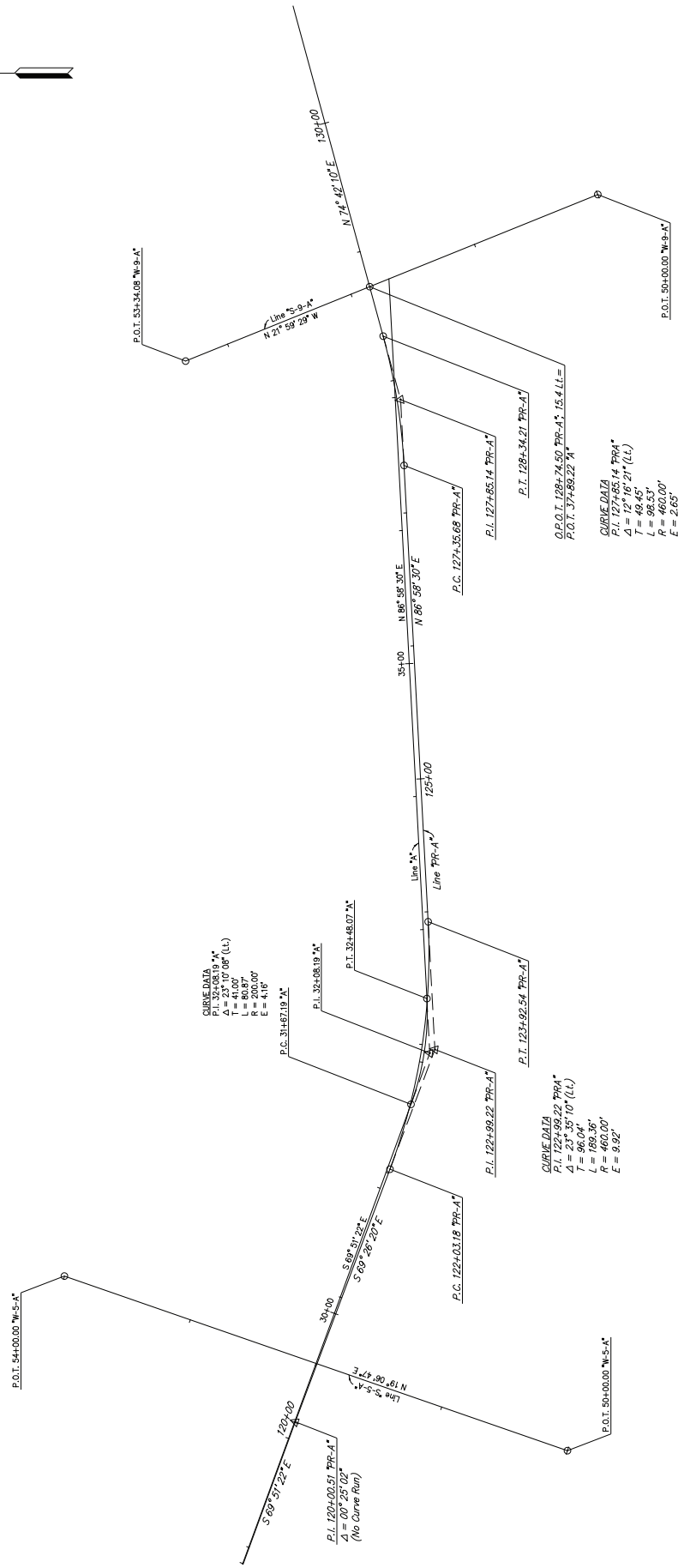
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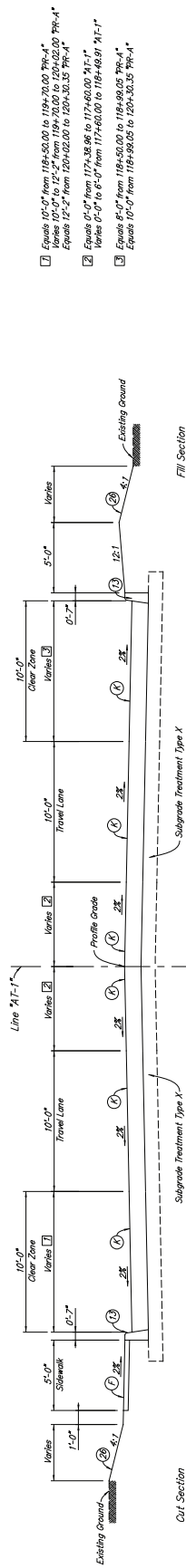
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120



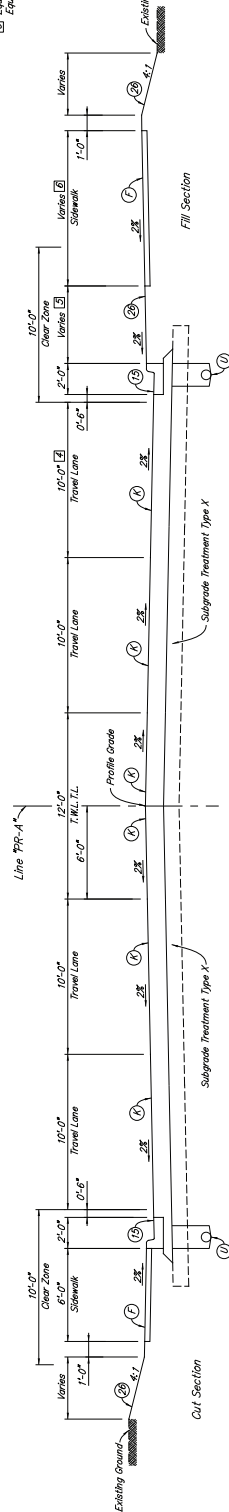
INDIANA		DEPARTMENT OF TRANSPORTATION		GEOMETRIC TIE-UP	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED:		ASU		CAK	
CHECKED:		SMC		SMC	
HORIZONTAL SCALE		BRIDGE FILE		DESIGNATION NO.	
VERTICAL SCALE		SURVEY BOOK		SHEETS	
CONTRACT		PROJECT NO.		SHEETS	
				6 of 89	
				PROJECT NO.	
				10051	

[illegible]



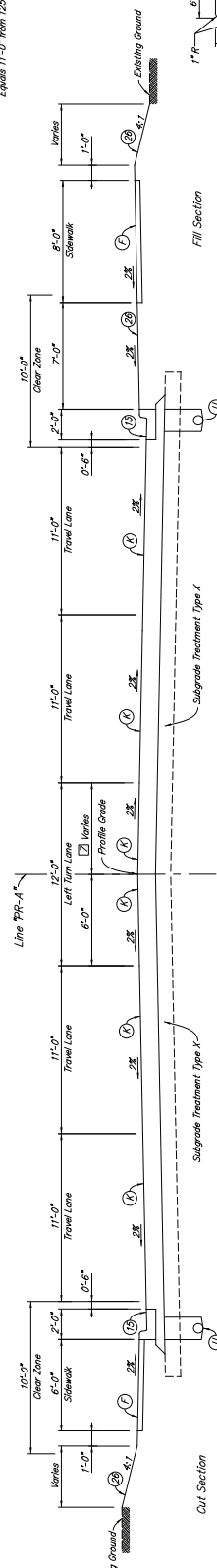
Typical Section - Incident Construction
117+38.96 to 118+48.97 "T-1"

- ① Equals 10'-0" from 118+50.00 to 119+70.00 "PR-A"
- ② Equals 10'-0" to 12'-2" from 119+70.00 to 120+02.00 "PR-A"
- ③ Equals 12'-2" from 120+02.00 to 120+30.35 "PR-A"
- ④ Equals 7'-0" from 118+50.00 to 118+98.05 "PR-A"
- ⑤ Equals 0'-0" from 118+98.05 to 120+30.35 "PR-A"
- ⑥ Equals 8'-0" from 118+50.00 to 118+98.05 "PR-A"
- ⑦ Equals 10'-0" from 118+98.05 to 120+30.35 "PR-A"



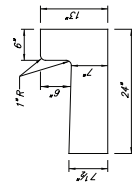
Typical Section - State Boulevard
118+50.00 to 120+30.35 "PR-A"

- ① Equals 6'-0" from 120+63.35 to 123+75.00 "PR-A"
- ② Equals 6'-0" to 11'-0" from 123+75.00 to 125+10.00 "PR-A"
- ③ Equals 11'-0" from 125+10.00 to 125+30.00 "PR-A"

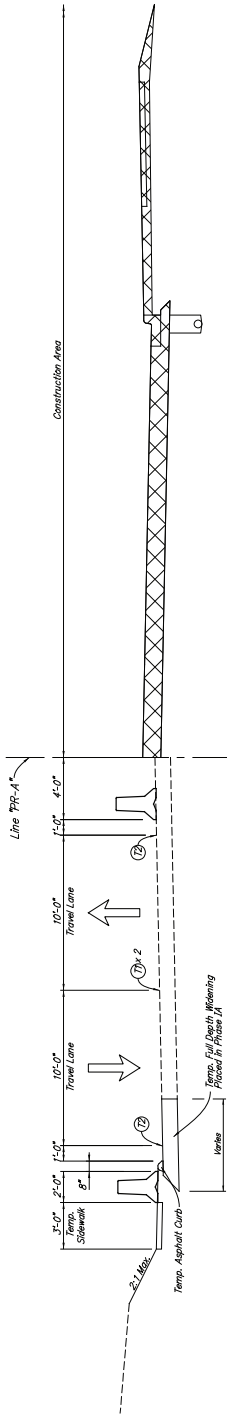


Typical Section - State Boulevard
120+63.35 to 125+30.00 "PR-A"

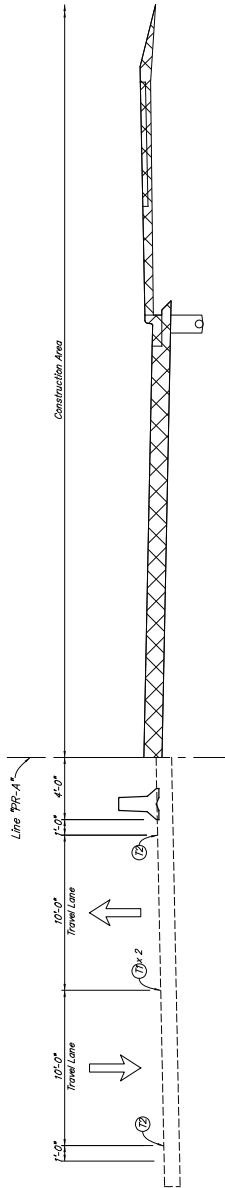
Combined Concrete Curb & Gutter, Modified Detail
Not to Scale



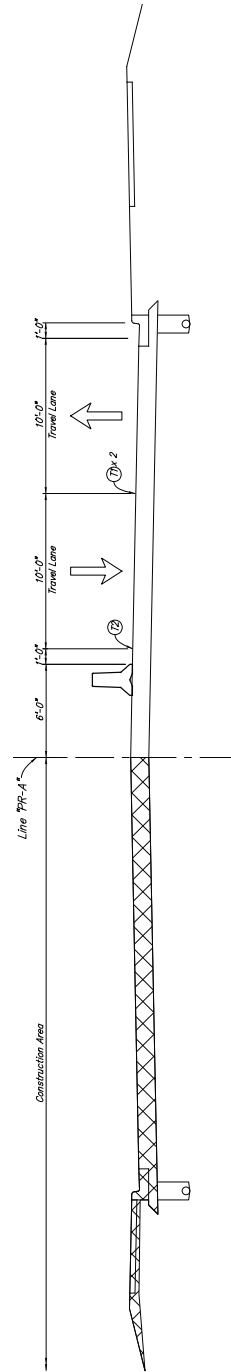
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK		SHEET NO. 9	
CHECKED: SMC		CHECKED: SMC		PROJECT NO. 105151	
LEGEND		INDIANA		BRIDGE FILE	
① 165 #3yd. HMA Surface, Type "A", on 27" #3yd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 53, Base		DEPARTMENT OF TRANSPORTATION		VERTICAL SCALE	
② Concrete Curb and Gutter, Modified		TYPICAL CROSS SECTIONS		SURVEY BOOK	
③ Concrete Curb		LINE "PR-A"		CONTRACT	
④ Concrete Slab, 4"				SHEETS	
				PROJECT NO.	
				105151	



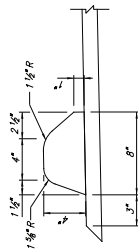
PHASE I



PHASE I



PHASE II



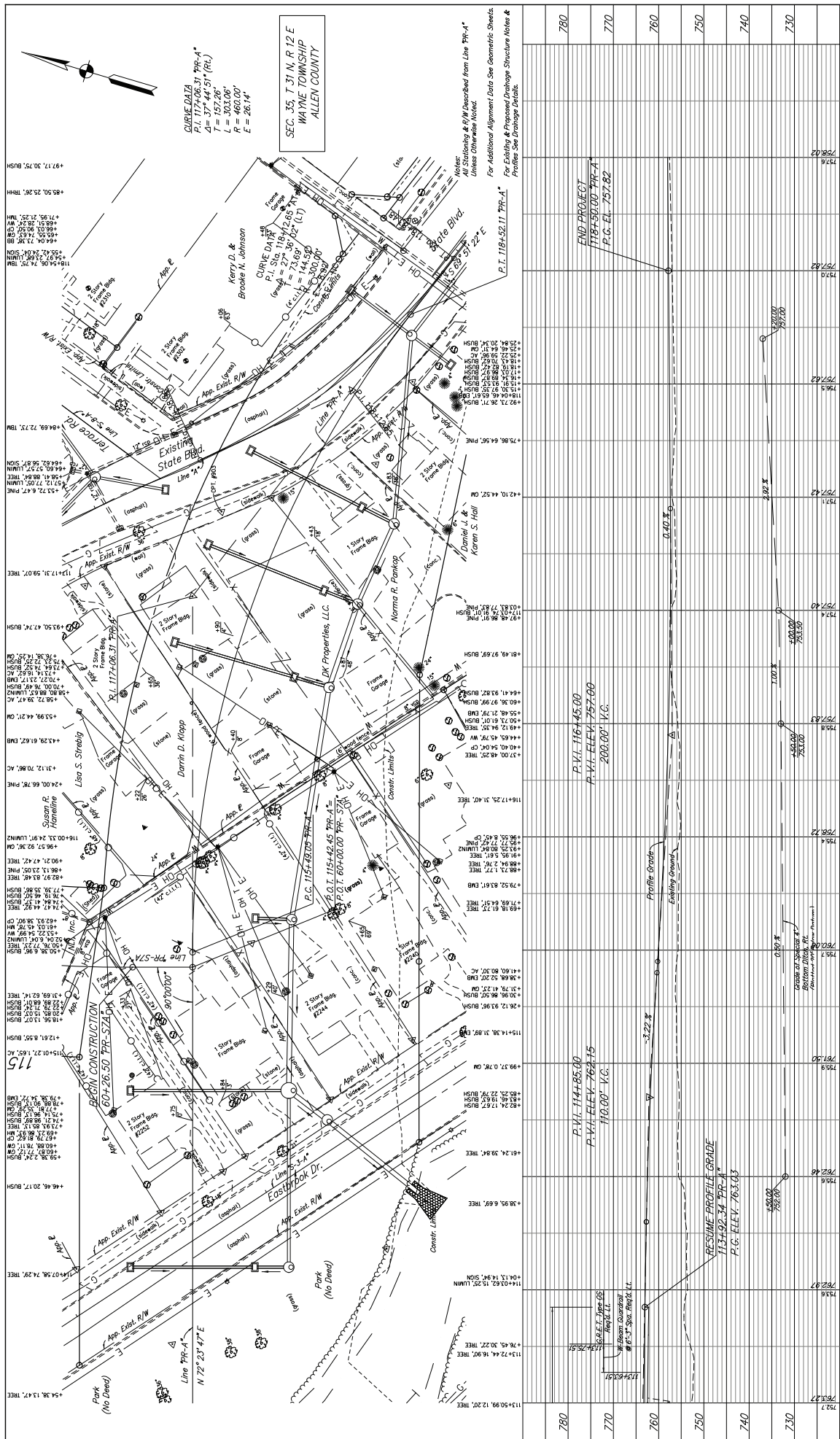
ASPHALT CURB (MOUNTABLE) DETAIL

INDIANA DEPARTMENT OF TRANSPORTATION				BRIDGE FILE	
RECOMMENDED FOR APPROVAL				DESIGNATION NO.	
DESIGNED: ASU				SHEETS	
DRAWN: CAK				12 of 89	
CHECKED: SMC				PROJECT NO.	
				10051	
				CONTRACT	
				10051	

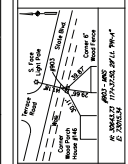
Notes: NMM Surface to be Placed Following Phase II Under Live Traffic

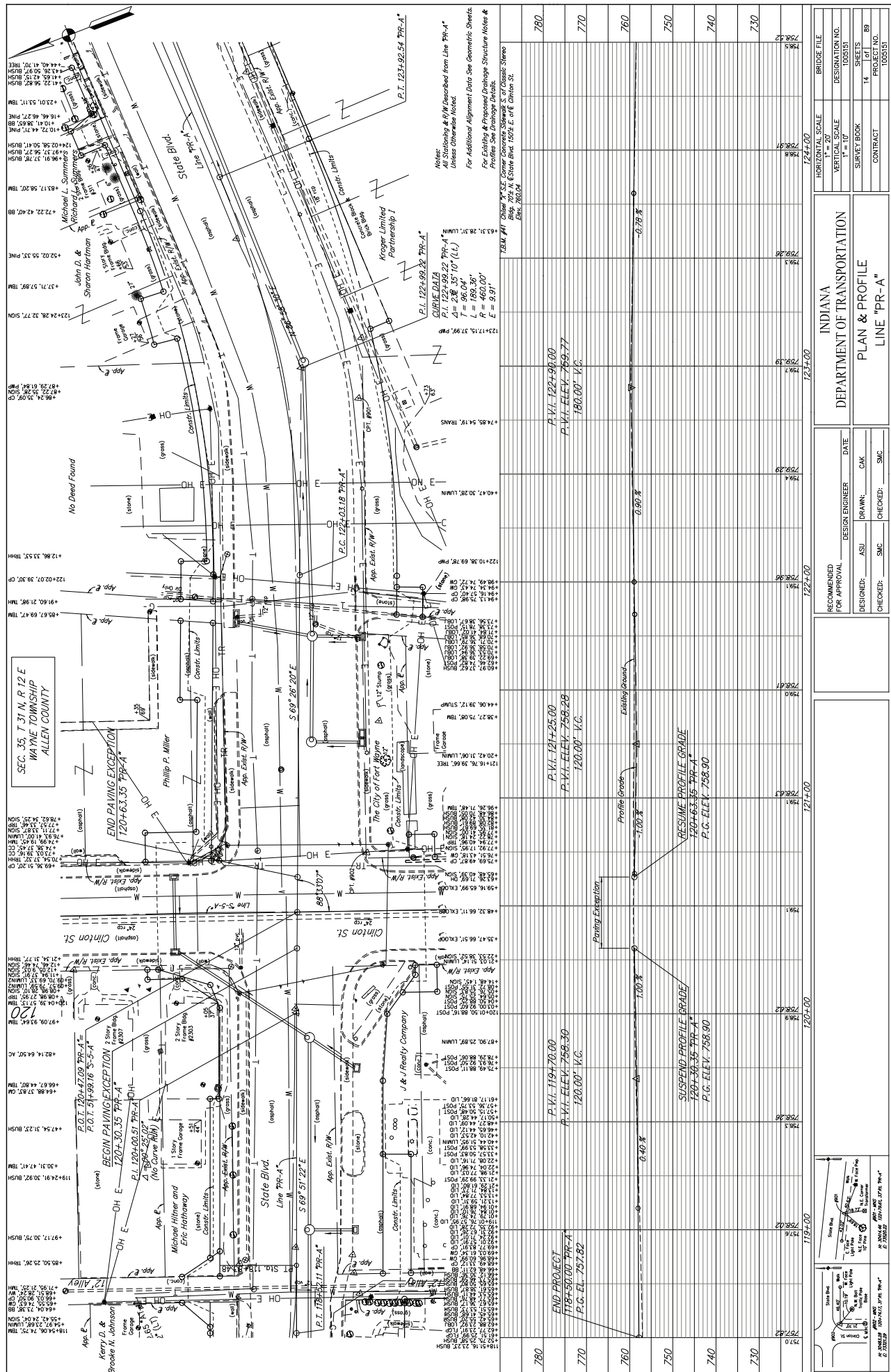
LEGEND	
Direction of Traffic	① Temporary Pavement Marking, Solid, Yellow, 4 in.
Standard Drum	② Temporary Pavement Marking, Solid, White, 4 in.
Temporary Concrete Barrier	Construction Area

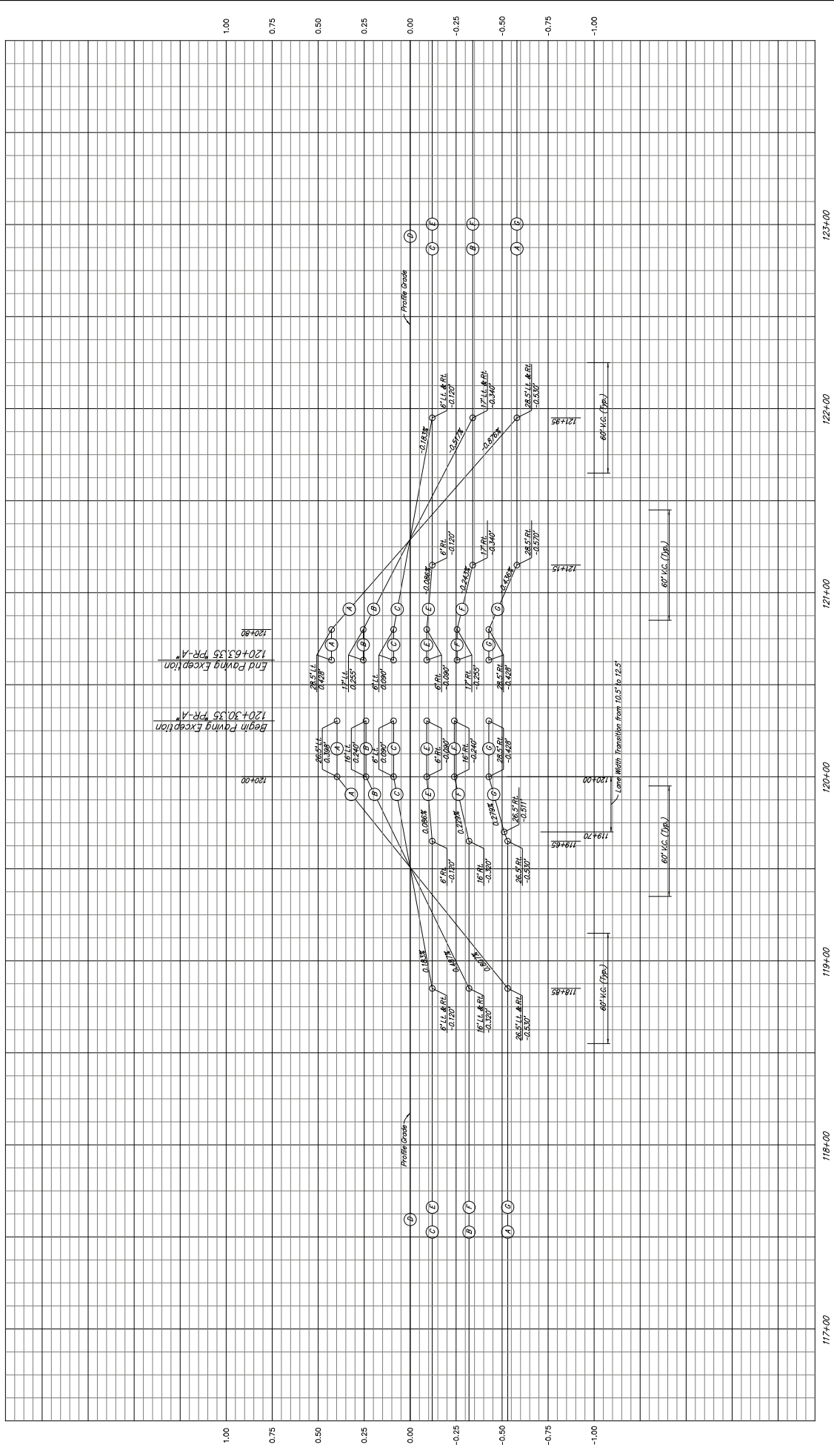
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INDIANA		DEPARTMENT OF TRANSPORTATION		PLAN & PROFILE	
BRIDGE FILE		DESIGN ENGINEER		DATE	
HORIZONTAL SCALE		DESIGNED:		ASU	
VERTICAL SCALE		DRAWN:		CAK	
1" = 10'		CHECKED:		SMC	
SURVEY BOOK		PROJECT NO.		10051	
13		SHEETS		13	
CONTRACT		PROJECT NO.		10051	
10051		PROJECT NO.		10051	





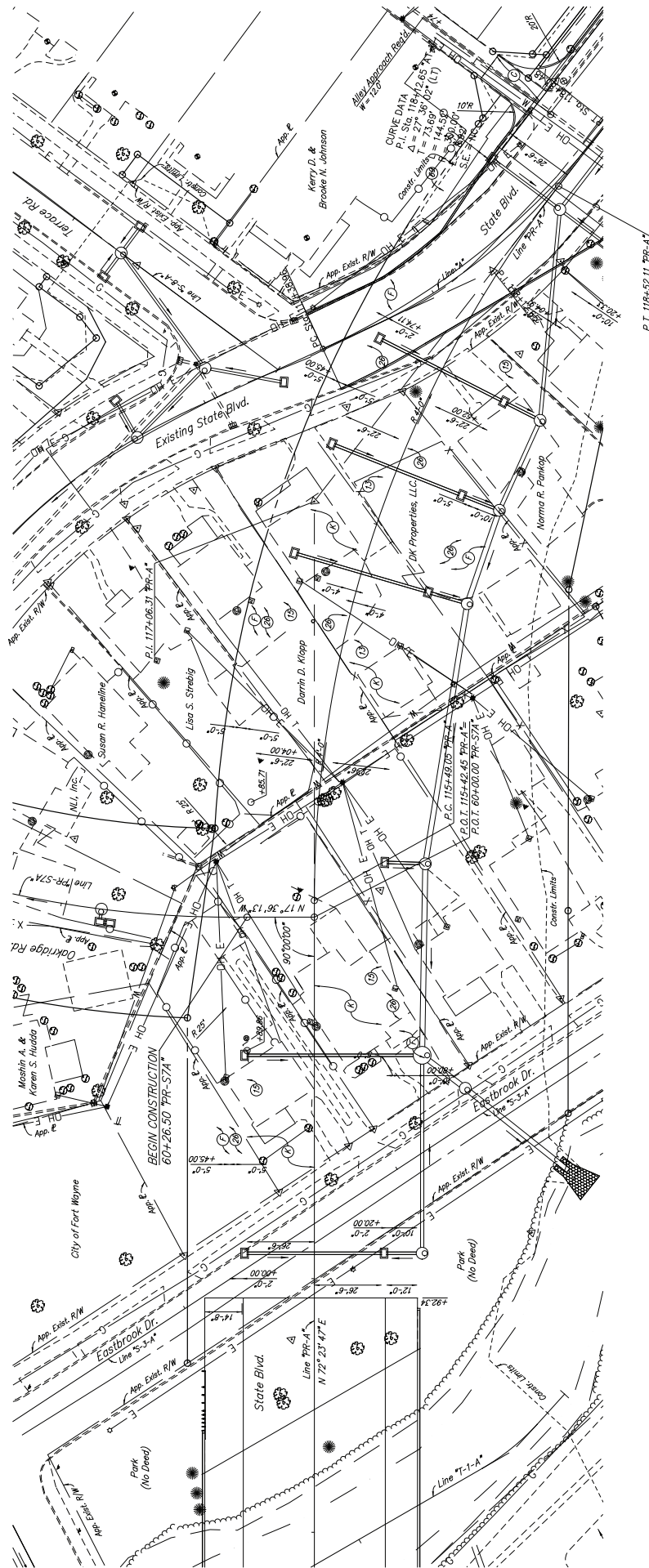


INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
PAVEMENT EDGE		1005151	
TRANSITION DIAGRAM		SURVEY BOOK	
LINE "PR-A"		CONTRACT	
		SHEETS	
		17 of 89	
		PROJECT NO.	
		1005151	

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
		ASU	ASU
DESIGNED:		ASU	ASU
CHECKED:		SMC	SMC

SEC. 35, T. 31 N, R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

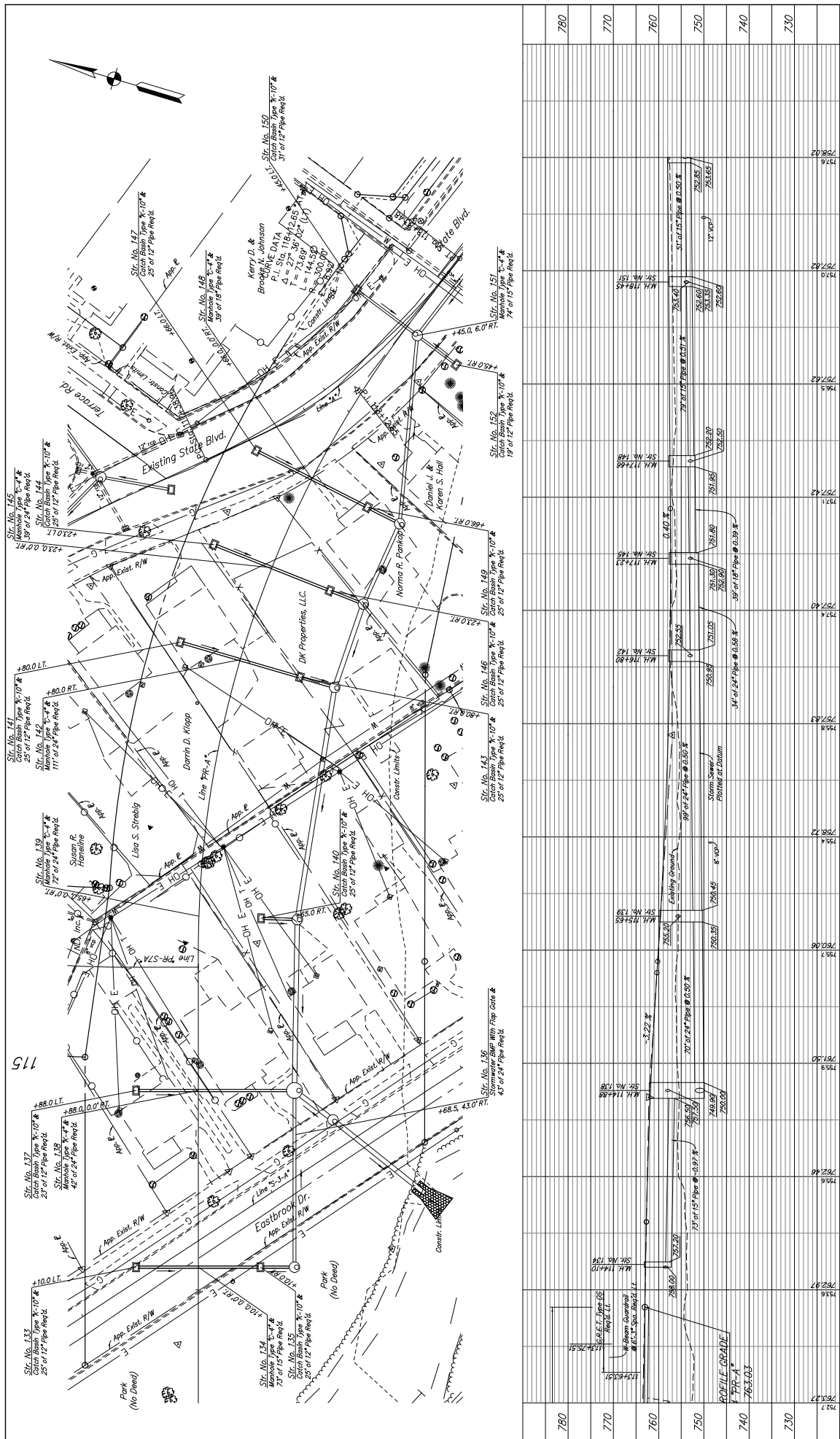
CURVE DATA
P.I. 117+06.31 "PR-A"
Δ = 37° 44' 51" (Rt.)
T = 157.26'
L = 303.06'
R = 460.00'
E = 26.14'



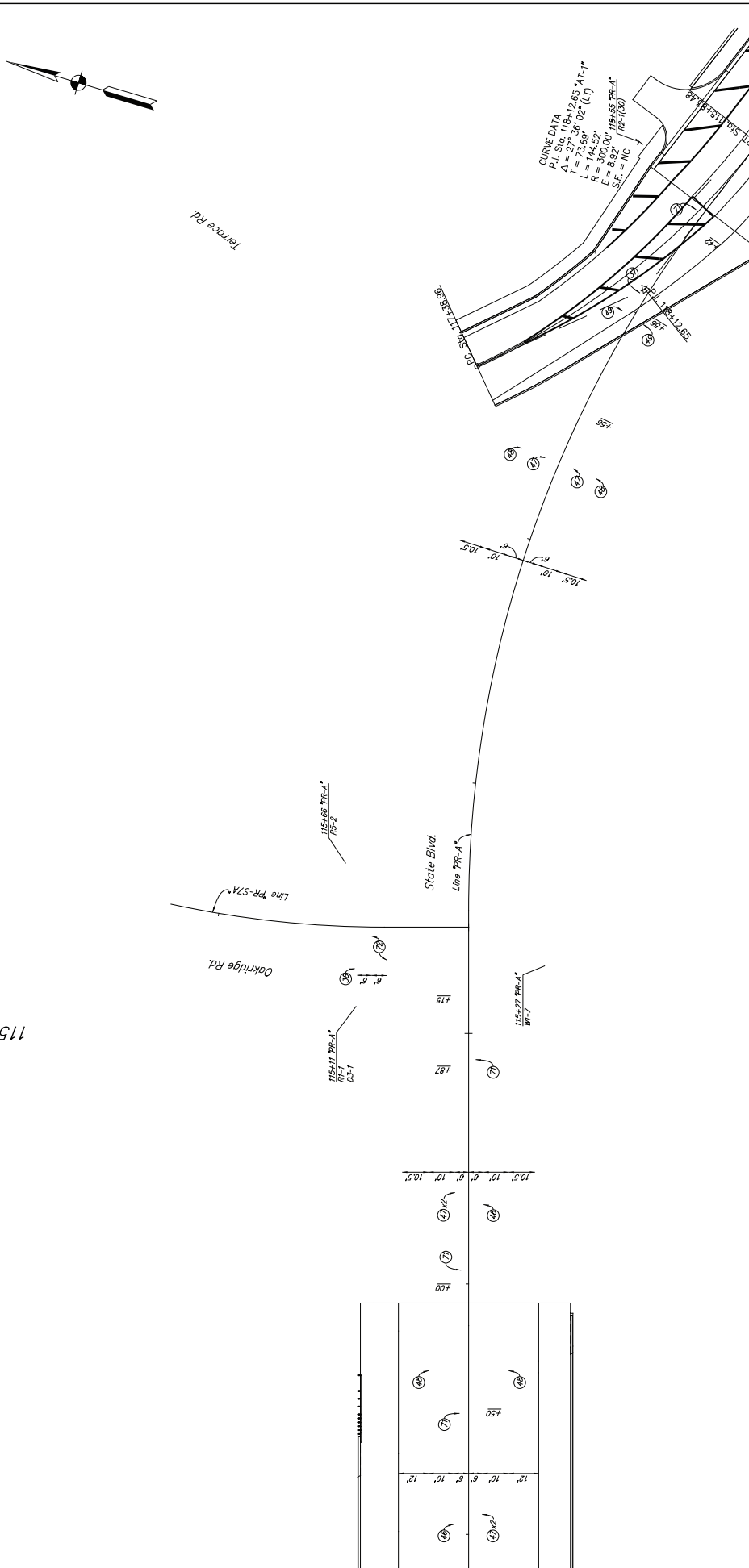
Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Indicated.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

INDIANA				DEPARTMENT OF TRANSPORTATION			
CONSTRUCTION DETAILS				LINE "PR-A"			
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		BRIDGE FILE	
DESIGNED: ASU		DRAWN: CAK		CHECKED: SMC		DESIGNATION NO.	
CHECKED: SMC		CHECKED: SMC		PROJECT NO.		SHEETS	
						18 of 89	
						CONTRACT	
						10551	

- LEGEND**
- 1. Concrete Curb
 - 2. Concrete Curb and Gutter, Modified
 - 3. Concrete Curb and Gutter, Modified
 - 4. Concrete Curb and Gutter, Modified
 - 5. Concrete Curb and Gutter, Modified
 - 6. Concrete Curb and Gutter, Modified
 - 7. Concrete Curb and Gutter, Modified
 - 8. Concrete Curb and Gutter, Modified
 - 9. Concrete Curb and Gutter, Modified
 - 10. Concrete Curb and Gutter, Modified
 - 11. Concrete Curb and Gutter, Modified
 - 12. Concrete Curb and Gutter, Modified
 - 13. Concrete Curb and Gutter, Modified
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 - 18. Concrete Curb and Gutter, Modified
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 - 95. Concrete Curb and Gutter, Modified
 - 96. Concrete Curb and Gutter, Modified
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 - 98. Concrete Curb and Gutter, Modified
 - 99. Concrete Curb and Gutter, Modified
 - 100. Concrete Curb and Gutter, Modified



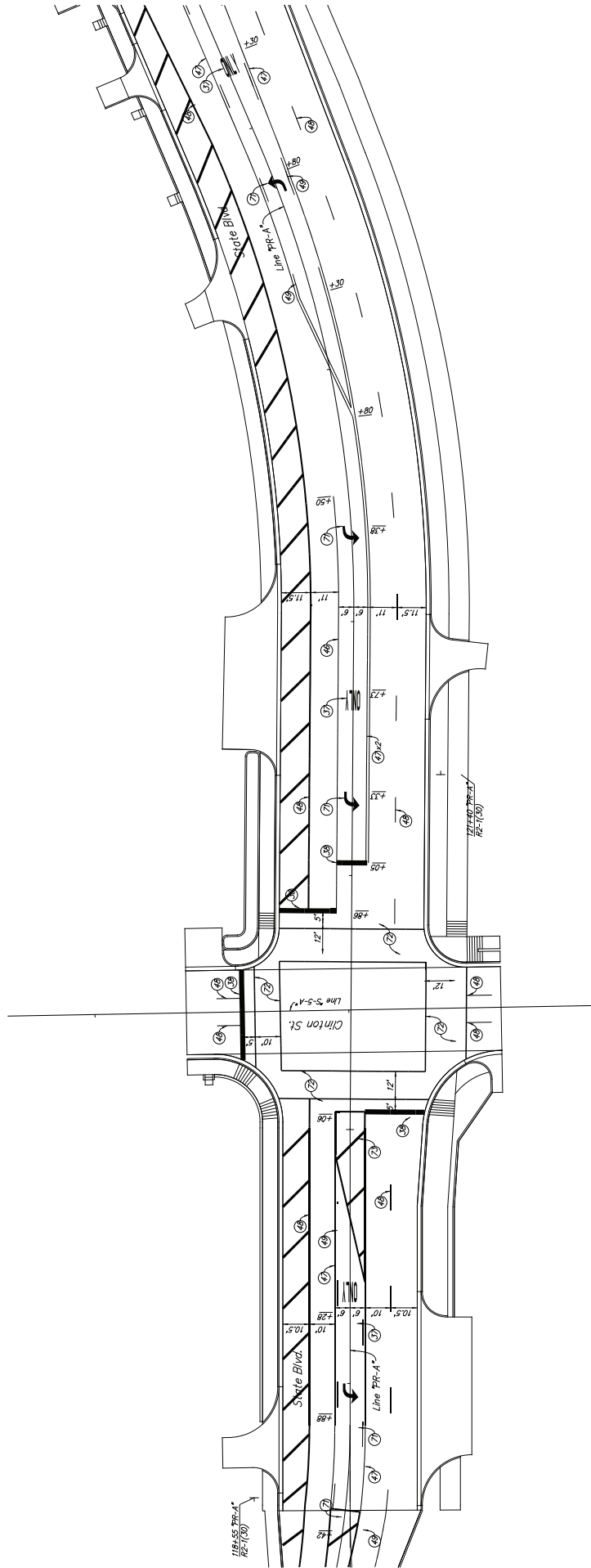
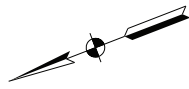
115



CURVE DATA
P.L. ST. 118+12.65
T = 27.118+12.65
L = 73.69
R = 144.52'
E = 800.00
S.E. = 82° 18' 30"
R.S. = 183.70

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK		SHEET NO. 24 of 89	
CHECKED: SMC		CHECKED: SMC		PROJECT NO. 10051	
INDIANA		DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		SURVEY BOOK		DESIGNATION NO. 10051	
		CONTRACT		SHEETS	
				PROJECT NO. 10051	

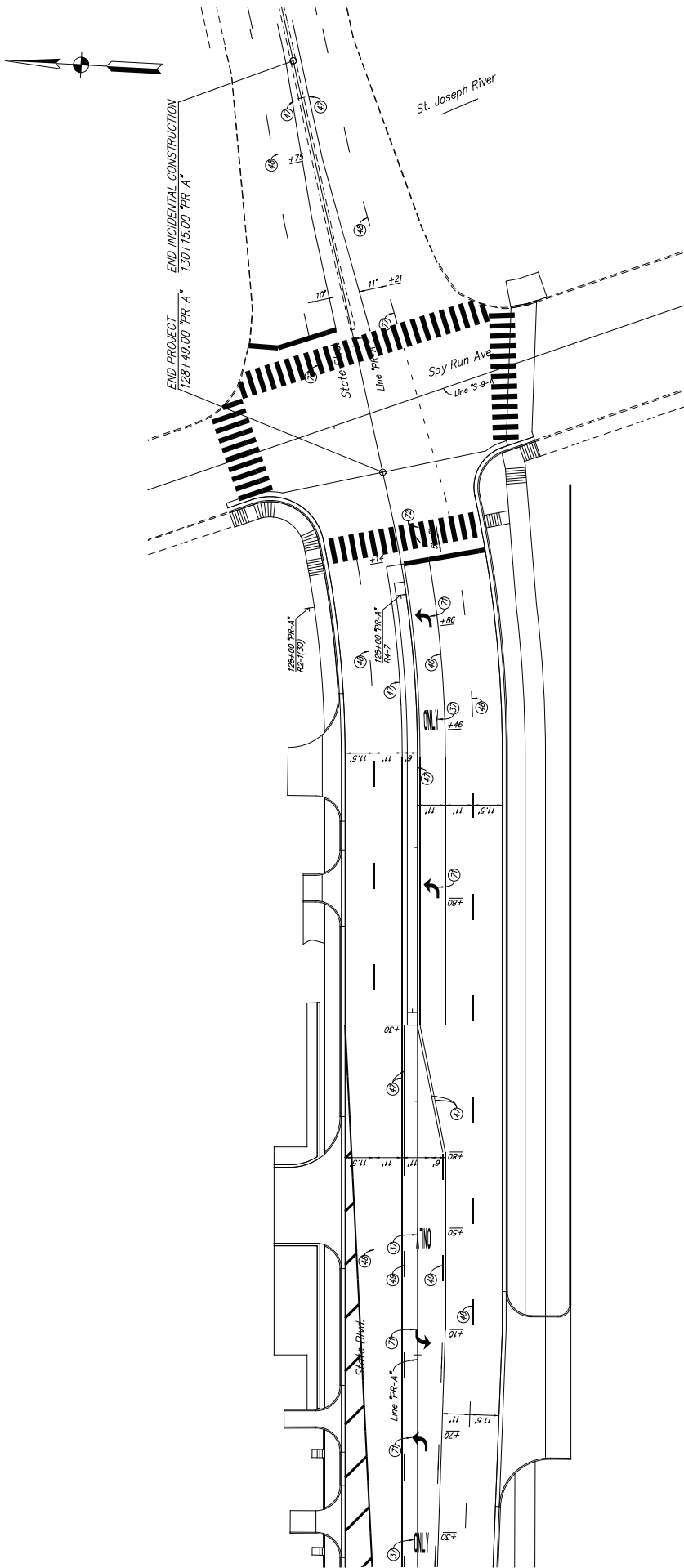
- LEGEND**
- ① Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.
 - ② Line, Thermoplastic, Broken, White, 4 in.
 - ③ Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.
 - ④ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
 - ⑤ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
 - ⑥ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - ⑦ Line, Thermoplastic, Solid, White, 4 in.
 - ⑧ Line, Thermoplastic, Solid, Yellow, 4 in.
 - ⑨ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - ⑩ Line, Thermoplastic, Broken, White, 4 in.



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	SHEETS
		CONTRACT	PROJECT NO.
			1005151
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		CHECKED: SMC	
LEGEND		(23) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.	
(17) Pavement Message Markings, Thermoplastic, Word (message)		(7) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)	
(18) Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.		(25) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)	
(19) Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.		(26) Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.	
(20) Line, Thermoplastic, Solid, White, 4 in.		(27) Pavement Message Markings, Thermoplastic, Lane Indication Arrow	
(21) Line, Thermoplastic, Solid, Yellow, 4 in.		(28) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	
(22) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.			

125

130



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	1005151
		CONTRACT	SHEETS
			26 of 89
			PROJECT NO.
			1005151

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:	ASU	DRAWN:	CAX
CHECKED:	SMC	CHECKED:	SMC

LEGEND

1 Pavement Message Markings, Thermoplastic, Word (message), 4 in. Thermoplastic, Broken, White, 4 in.

2 Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

3 Line, Thermoplastic, Solid, White, 4 in.

4 Line, Thermoplastic, Solid, Yellow, 4 in.

5 Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.

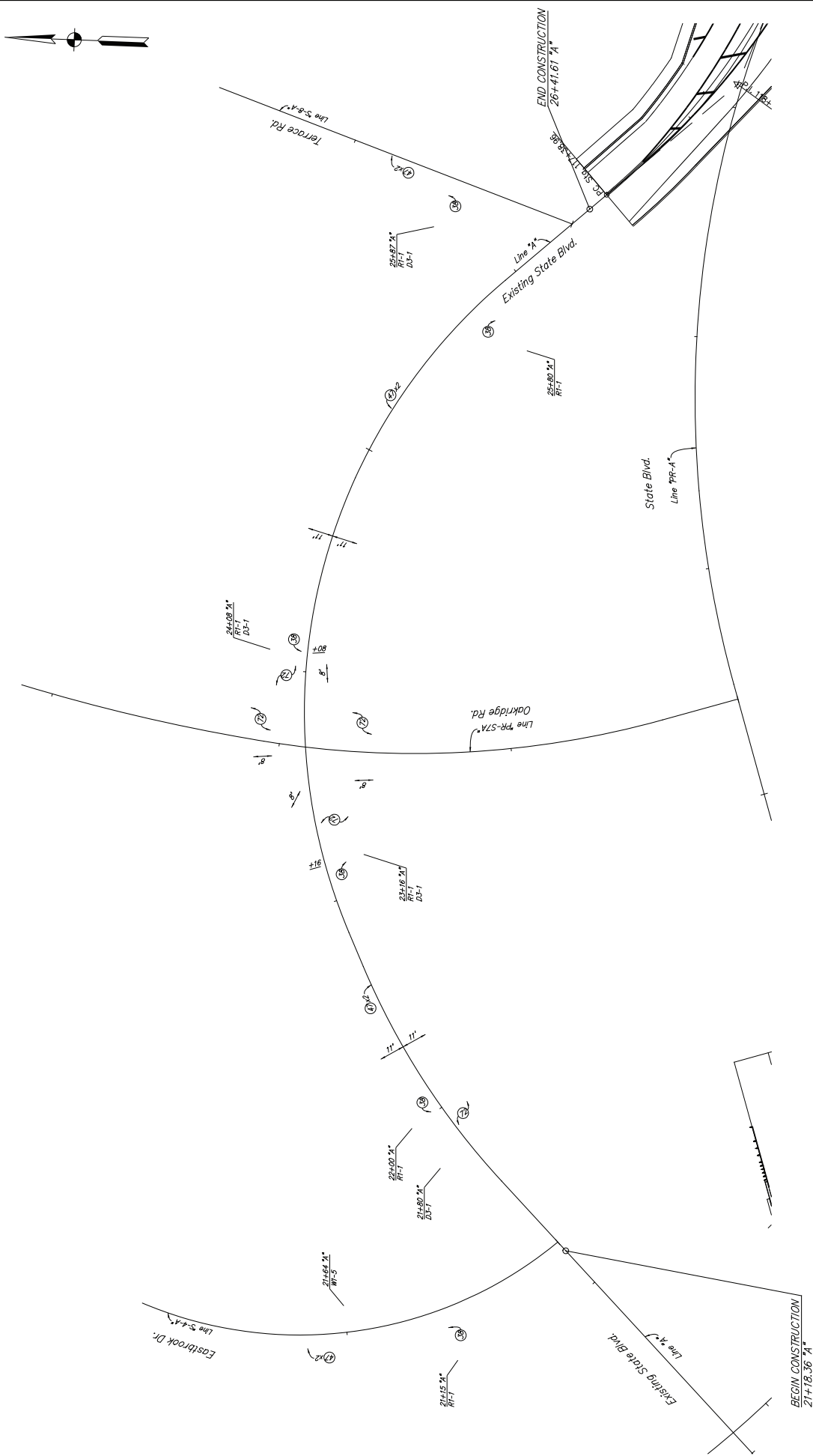
6 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

7 Pavement Message Markings, Thermoplastic, Lane Indication Arrow

8 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

9 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

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INDIANA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING &
SIGNING DETAILS - LINE "A"

RECOMMENDED
FOR APPROVAL

DESIGNED:
CHECKED:

DESIGN ENGINEER
ASU
SMC

DATE
CCK
SMC

HORIZONTAL SCALE
VERTICAL SCALE

SURVEY BOOK
CONTRACT

BRIDGE FILE
DESIGNATION NO.
SHEETS
PROJECT NO.

(23) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.

(24) Line, Thermoplastic, Broken, White, 4 in.

(25) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)

(26) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)

(27) Pavement Markings, Thermoplastic, Word (message), White, 4 in.

(28) Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

(29) Line, Thermoplastic, Solid, White, 4 in.

(30) Line, Thermoplastic, Solid, Yellow, 4 in.

(31) Transverse Markings, Thermoplastic, Lane Indication Arrow

(32) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

LEGEND

(33) Pavement Markings, Thermoplastic, Word (message), White, 4 in.

(34) Line, Thermoplastic, Broken, White, 4 in.

(35) Line, Thermoplastic, Dotted, White, 4 in.

(36) Line, Thermoplastic, Dotted, Yellow, 4 in.

(37) Pavement Markings, Thermoplastic, Word (message), White, 4 in.

(38) Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

(39) Line, Thermoplastic, Solid, White, 4 in.

(40) Line, Thermoplastic, Solid, Yellow, 4 in.

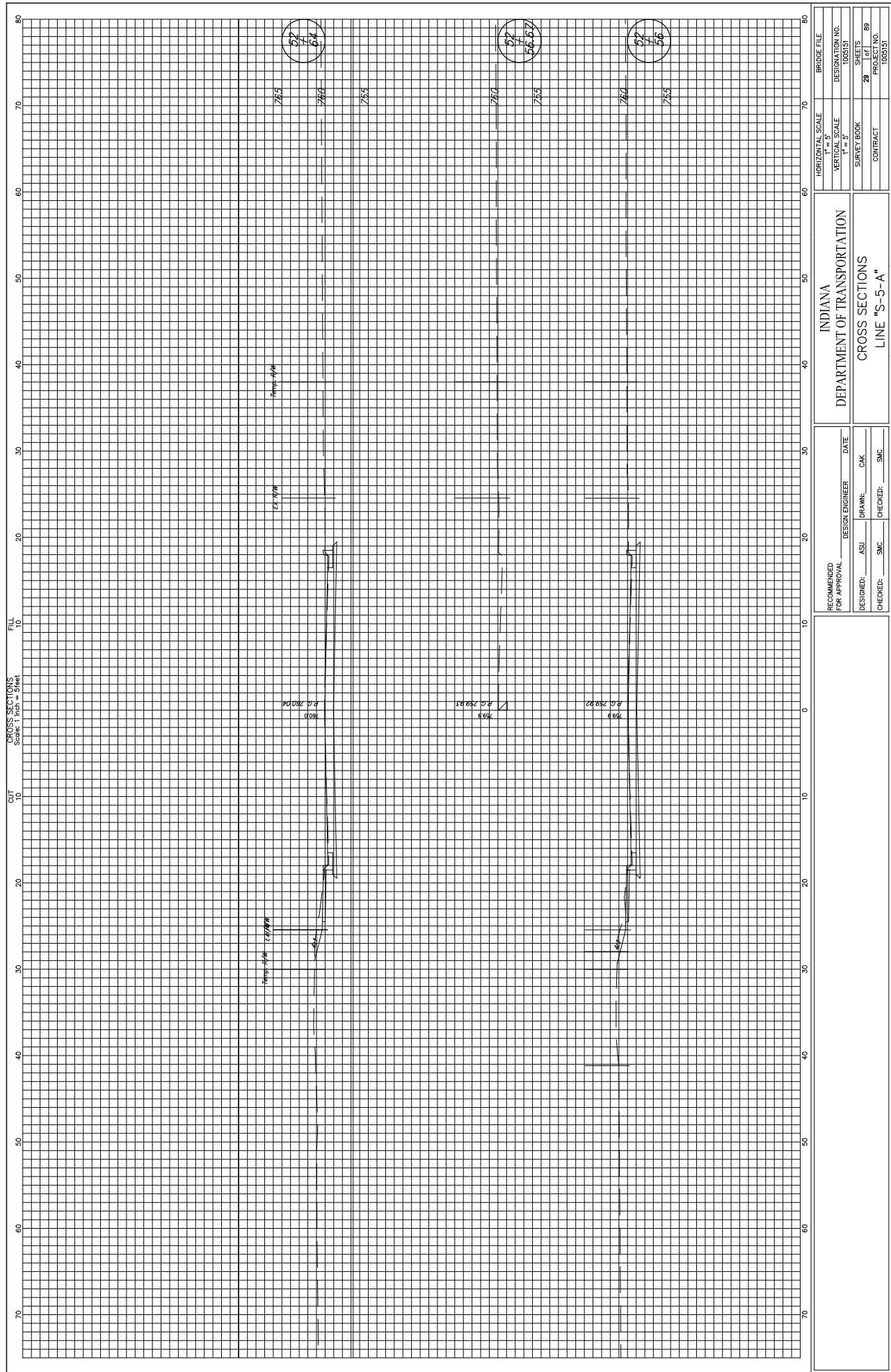
(41) Transverse Markings, Thermoplastic, Lane Indication Arrow

(42) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

7/12/2011 12:45:15 PM P:\H0007\10440 - Drawings\0011404.RD.P1.PK001.dgn

for files

Attachment 3 - 61 of 531



UTILITIES

Electric
Electric Power
Dunbar
PO Box 60
Fort Wayne, IN 46801
Contact: Craig Bernat
(260) 427-2680

Telephone
Frontier Communications
8001 W. Jefferson Blvd., INFAON
Indianapolis, IN 46221
Contact: Ken-Fred Kaler
(260) 461-3620

Water
City of Fort Wayne Utilities
170 Taylor St., Room 480
Fort Wayne, IN 46802
Contact: Craig Bernat
(260) 427-2680

Gas
NIPSCO
801 E. 86th Ave.
Indianapolis, IN 46240
Contact: Jill Bowman
(419) 647-6502

Television
Comcast Cablevision
170 Taylor St., Room 480
Fort Wayne, IN 46802
Contact: John Gaylor
(260) 427-2680

Transmission
American Electric Power
10000 N. Meridian Rd.
Goshanna, OH 43030
Contact: Tim Harrison
(614) 532-1801

GENERAL NOTES

** All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded and shall be planted with grass seed and fertilizer.

The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.

Excavation shall be done in accordance with the specifications by the Engineer before construction.

Existing asphalt pavement located outside the construction limits between Sta. _____ and Sta. _____ shall be removed or directed.

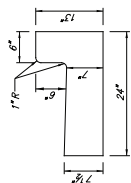
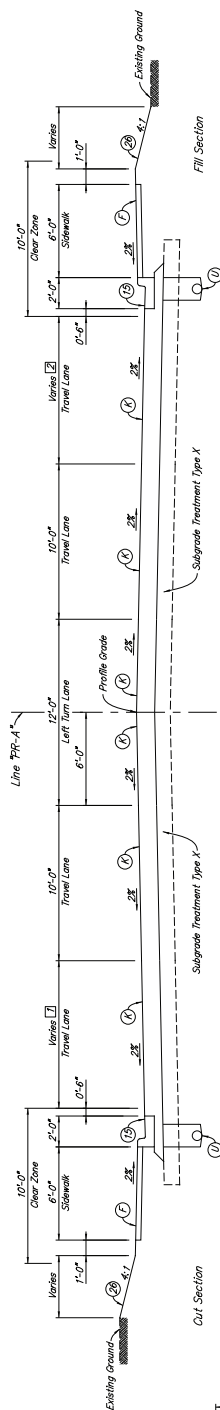
The quantity of peat excavation shown on the plans has been estimated on the basis of treatment by displacement, where each treatment applies.

All limited access right-of-way (L.A. RW) is to be fenced with chain link type fence (CLTF) or turn field type fence (TFF) where specified in the plans.

** REPRESENTS GENERAL NOTES REQUIRED

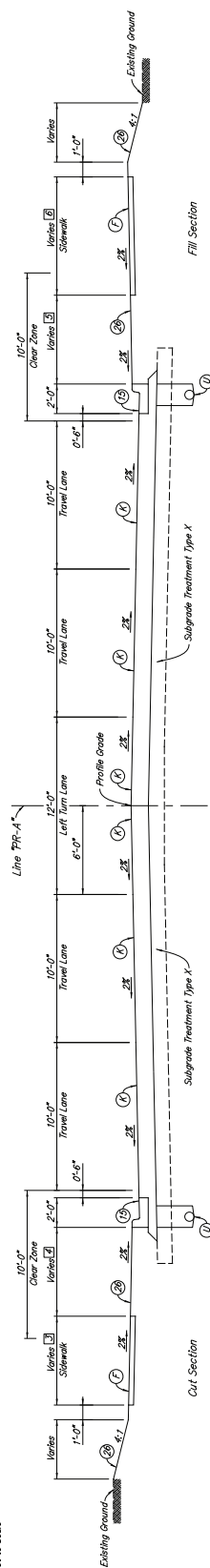
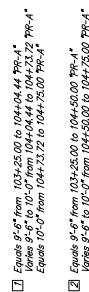
INDEX TO SHEETS

SHEET NO.	DRAWING INDEX
1	1116 Sheet
2	1117 Sheet
3	1118 Sheet
4	1119 Sheet
5	1120 Sheet
6	1121 Sheet
7	1122 Sheet
8	1123 Sheet
9	1124 Sheet
10	1125 Sheet
11	1126 Sheet
12	1127 Sheet
13	1128 Sheet
14	1129 Sheet
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Combined Concrete Curb & Gutter, Modified Detail

Not to Scale

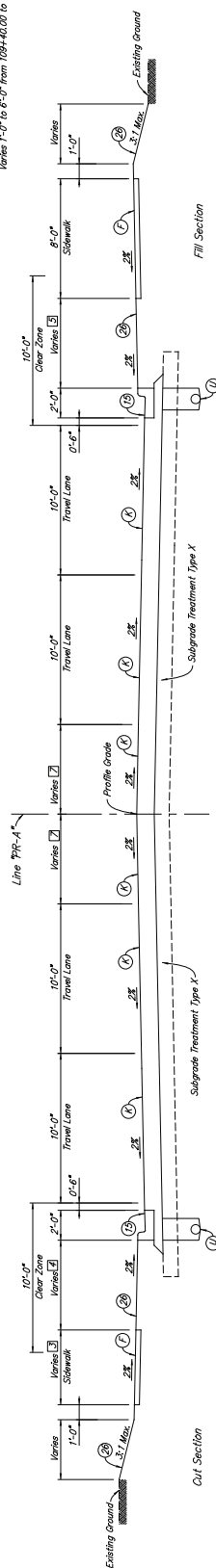
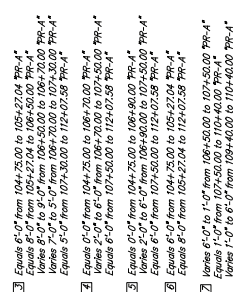


Typical Section - State Boulevard

1104773.00 to 100750.00 PR-A
110440400 to 11240758 "PP-A"

Paving Exception

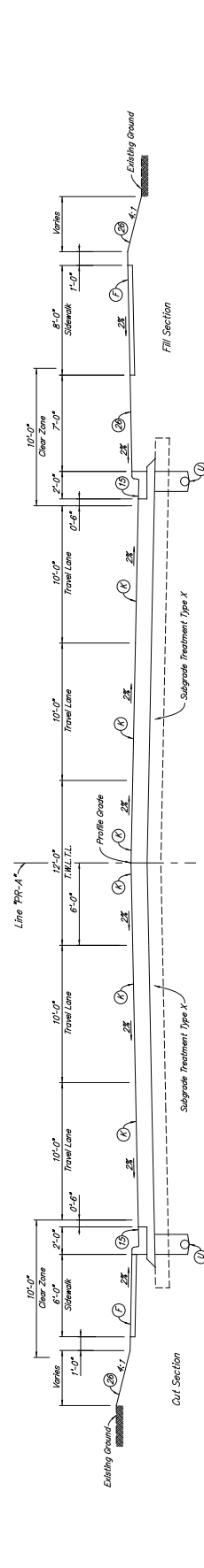
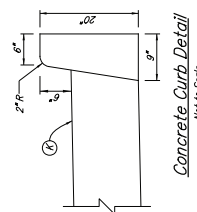
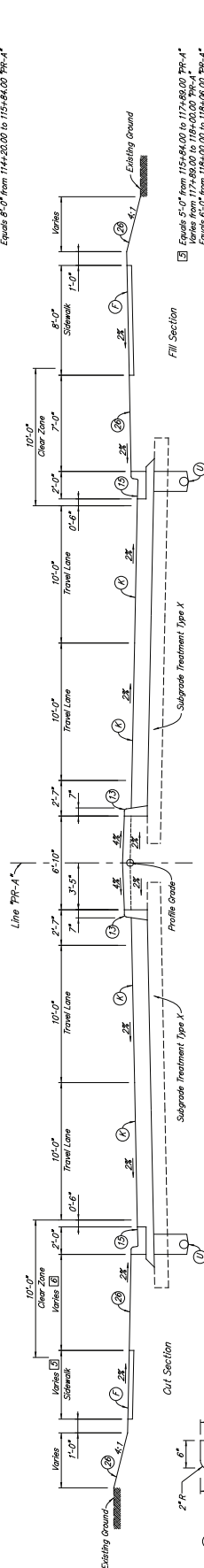
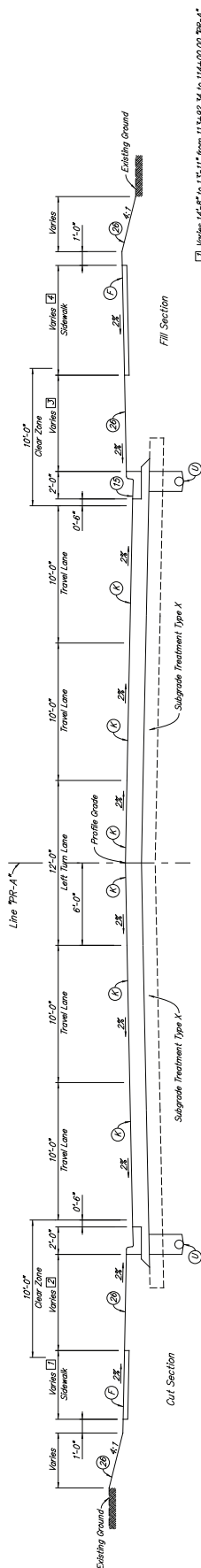
Living Exception
112+07.58 to 113+92.34 "PR-A"



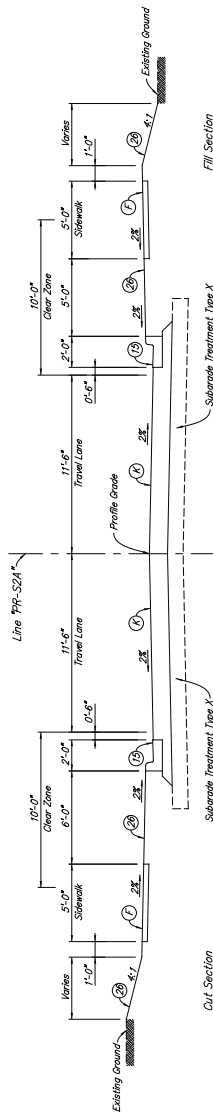
Typical Section - State Boulevard

11 JUL 1971 - JULE DOUBLE
106450 00 to 110410 00 "22-1"

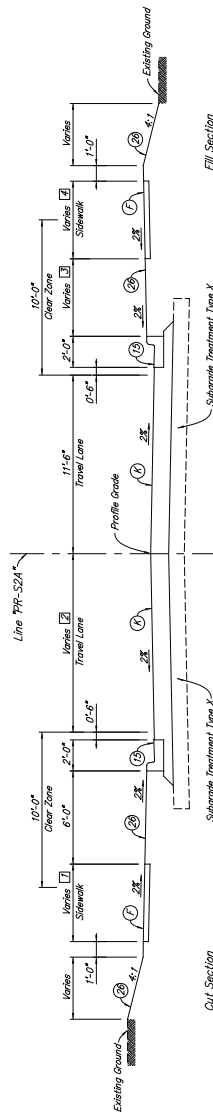
<p>① xxx #19d OC/CA-HMA, 4 in. Surface, <i>or</i> on xxx #19d OC/CA-HMA, 4 in. Intermediate, <i>or</i> on xxx #19d OC/CA-HMA, 4 in. Base, <i>or</i> on xxx #19d OC/CA-HMA, 4 in. Intermediate OC, 19mm on xxx #19d OC/CA-HMA, 4 in. Base, <i>or</i> xxx #19d OC/CA-HMA, 4 in. Base, <i>or</i></p> <p>② Concrete Slabways, 4"</p>	<p>③ Combined Concrete Curb and Gutter, Modified Retaining Wall</p> <p>④ Concrete Curb</p> <p>⑤ Mulched Seeding, Type "B"</p> <p>⑥ Sodding, Nursery</p> <p>⑦ 165 #19d MMA Surface, Type "1", on 275 #19d MMA Intermediate, Type "1", on 6" Compacted Aggregate, No. 53, Base</p> <p>⑧ Underdrain</p>	<p>⑨ Concrete Center Curb, Type "B"</p>	<p>RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____</p> <p>DESIGNED: ASU DRAWN: CAK</p> <p>CHECKED: SMC CHECKED: SMC</p>	<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>TYPICAL CROSS SECTIONS</p> <p>LINE "B" - A"</p>	<p>HORIZONTAL SCALE 1" = 1'-0"</p> <p>VERTICAL SCALE 1" = 1'-0"</p> <p>SURVEY BOOK</p> <p>SHEETS</p> <p>DESIGNATION NO. 102154</p> <p>BRIDGE FILE</p>
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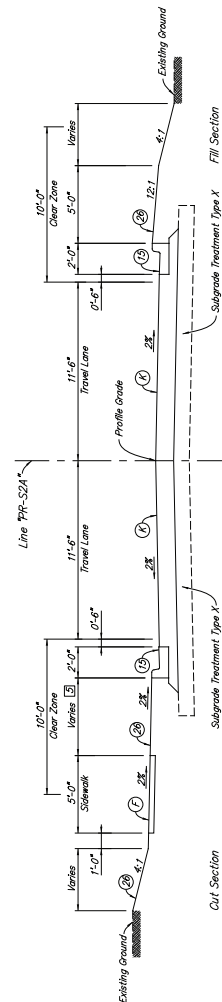
<div style="float: right; width: 100px;">BRIDGE FILE</div> <div style="float: left; width: 100px;">DESIGNATION NO.</div> <div style="clear: both;"></div>						INDIANA DEPARTMENT OF TRANSPORTATION						BRIDGE FILE					
						RECOMMENDED FOR APPROVAL						DATE					
						DESIGNED: ASU						DRAWN: CAK					
						CHECKED: SMC						CHECKED: SMC					
TYPICAL CROSS SECTIONS																	
LINE "PR-A"																	
<div style="text-align: center;">LEGEND</div> <div style="display: flex; justify-content: space-between;"> <div style="width: 45%;"> <p>(A) xxx #Syst. 02/04-HMA, x, w, Surface, xx, on xxx #Syst. 02/04-HMA, x, w, Intermediate, xx, on 275 #Syst. HMA Intermediate, Type "A" #Syst. 02/04-Asphaltic, No. 3, Base</p> <p>(B) Concrete Curb</p> <p>(C) Combined Concrete Curb and Gutter, Modified</p> </div> <div style="width: 45%;"> <p>(D) Underdrain</p> <p>(E) Sodding, Nursery</p> <p>(F) Matched Seeding, Type "D"</p> <p>(G) Retaining Wall</p> </div> </div>																	



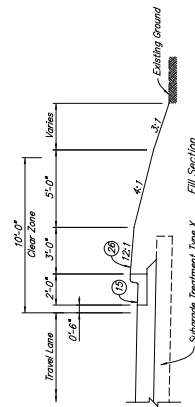
Incidental Construction
Typical Section - Edgehill Avenue
51+60.00 to 52+12.43 Line PR-S2A*



Typical Section - Edgehill Avenue
52+12.43 to 54+66.43 Line "PR-S2A"



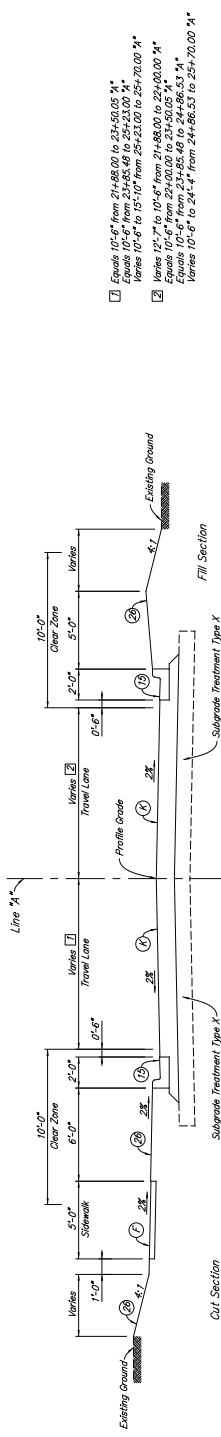
Typical Section - Westbrook Drive
55+27.76 to 58+45.00 Line "PR-S2A"



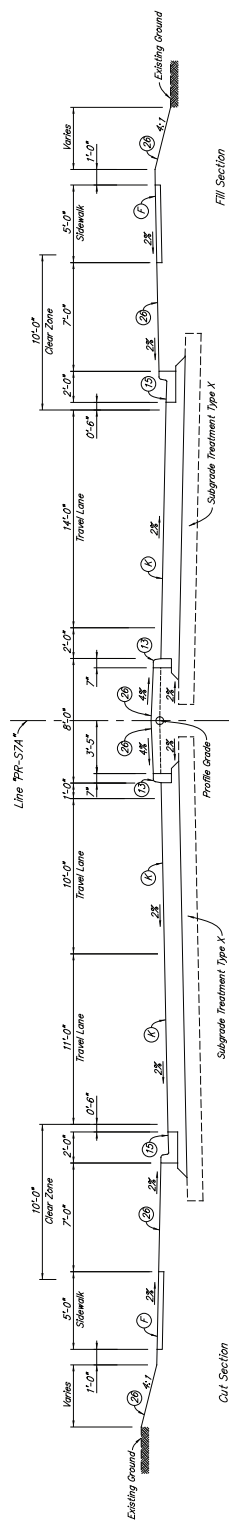
Fill Section Detail
55+27.76 to 56+75.00 Rt. Line "PR-S2A"

- 1 Equates 5-7° from 52°12.43' to 54°25.00° Ppr-S2A*
Equate 5-7° from 54°25.00° to 54°46.43° Ppr-S2A*
Equate 8° from 54°46.43° to 54°46.43° Ppr-S2A*
- 2 Equates 11-6° from 52°12.43' to 53°49.00° Ppr-S2A*
Equate 11-6° from 53°49.00° to 54°10.43° Ppr-S2A*
Equate 15-6° from 54°10.43° to 54°46.43° Ppr-S2A*
- 3 Equates 5-5° from 52°12.43' to 53°45.00° Ppr-S2A*
Equate 5-5° from 53°45.00° to 54°20.13° Ppr-S2A*
Equate 10-5° from 54°20.13' to 54°20.13° Ppr-S2A*
- 4 Equates 15-5° from 52°12.43' to 53°45.00° Ppr-S2A*
Equate 15-5° from 53°45.00° to 54°20.13° Ppr-S2A*
Equate 10-5° from 54°20.13' to 54°20.13° Ppr-S2A*
- 5 Equates 15-4° from 52°12.43' to 53°45.00° Ppr-S2A*
Equate 15-4° from 53°45.00° to 54°20.13° Ppr-S2A*
Equate 10-4° from 54°20.13' to 54°20.13° Ppr-S2A*

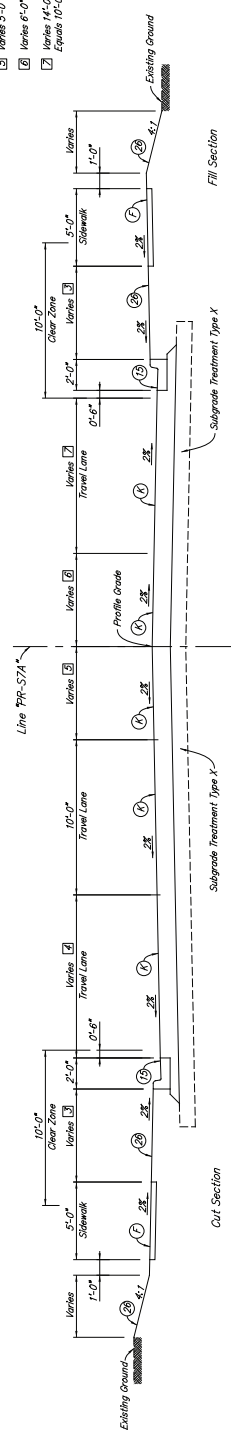
(A) xxx #/yd of C/C-4-HMA, x, ss, Surface, xx, on xxx #/yd of C/C-4-HMA, x, ss, Intermediate, xx, on xxx #/yd of C/C-4-HMA, x, ss, Base, xx, on 6" Compacted Aggregate, No. 53, Base	(B) 185 #/yd. HMA Surface, Type "1"; on 275 #/yd. HMA Intermediate, Type "2"; on 6" Compacted Aggregate, No. 53, Base	(C) Underdrain	(D) Concrete Center Curb, Type D"	(E) Sodding, Nursery	(F) Concrete Curb	(G) Malched Seeding, Type B"	(H) Reaching Wall
(I) xxx #/yd of C/C-4-HMA, x, ss, Surface, xx, on xxx #/yd of C/C-4-HMA, x, ss, Intermediate, xx, on xxx #/yd of C/C-4-HMA, x, ss, Base, xx, on 6" Compacted Aggregate, No. 53, Base	(J) 185 #/yd. HMA Surface, Type "1"; on 275 #/yd. HMA Intermediate, Type "2"; on 6" Compacted Aggregate, No. 53, Base	(K) Underdrain	(L) Concrete Center Curb, Type D"	(M) Sodding, Nursery	(N) Concrete Curb	(O) Malched Seeding, Type B"	(P) Reaching Wall
(Q) Concrete Sawwalk, 4"							



Typical Section - Existing State Blvd.
214+86.00 to 234+50.00 Line 14
234+86.53 to 264+161 Line 14



Typical Section - Oakridge Road
60+26.30 to 61+44.00 Line 14



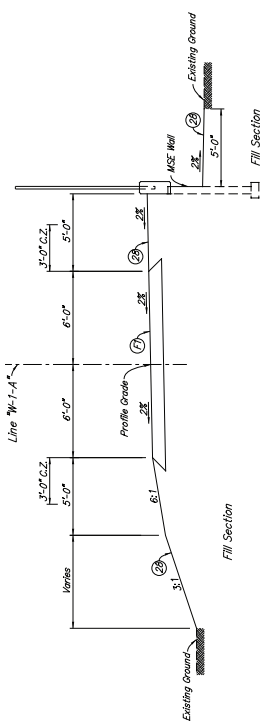
Typical Section - Oakridge Road
61+44.00 to 63+45.00 Line 14

- 1 Equals 10'-0" from 214+86.00 to 234+50.00 Line 14
- 2 Equals 10'-0" from 234+50.00 to 234+86.53 Line 14
- 3 Equals 10'-0" from 234+86.53 to 244+86.53 Line 14
- 4 Equals 10'-0" from 244+86.53 to 254+70.00 Line 14
- 5 Equals 10'-0" from 254+70.00 to 264+161 Line 14

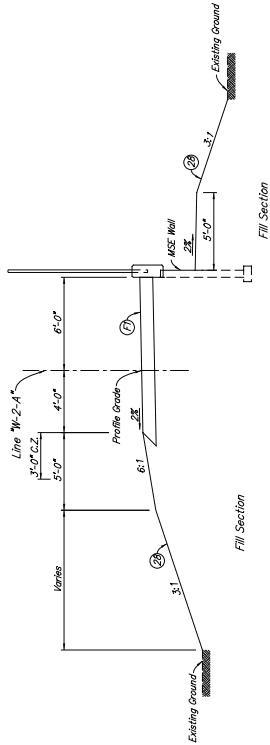
- 6 Equals 7'-0" from 61+44.00 to 61+86.68 Line 14
- 7 Equals 6'-0" from 61+86.68 to 63+45.00 Line 14
- 8 Equals 10'-0" from 61+44.00 to 61+45.55 Line 14
- 9 Equals 10'-0" from 61+45.55 to 62+40.00 Line 14
- 10 Equals 5'-0" from 61+44.00 to 62+34.00 Line 14
- 11 Equals 6'-0" from 61+44.00 to 62+34.00 Line 14
- 12 Equals 10'-0" from 62+34.00 to 63+45.00 Line 14

INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
TYPICAL CROSS SECTIONS		SHEETS	
LINE "A" & "PR-S7A"		PROJECT NO.	
		CONTRACT	
		100514	

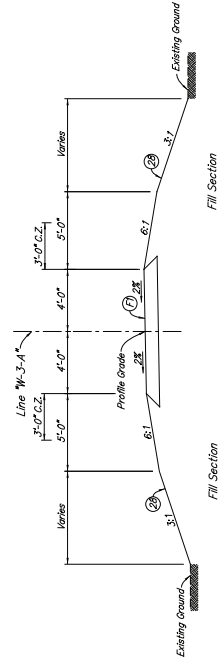
- LEGEND
- 1 165 #3/4" HMA Surface, Type 14, on 275 #3/4" HMA Intermediate, Type 14, on 6" Composite Aggregate, No. 3, Base
 - 2 Concrete Curb and Gutter, Modified
 - 3 Concrete Curb and Gutter, Modified
 - 4 Concrete Curb and Gutter, Modified
 - 5 Concrete Curb and Gutter, Modified
 - 6 Concrete Curb and Gutter, Modified
 - 7 Concrete Curb and Gutter, Modified
 - 8 Concrete Curb and Gutter, Modified
 - 9 Concrete Curb and Gutter, Modified
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 - 36 Concrete Curb and Gutter, Modified
 - 37 Concrete Curb and Gutter, Modified
 - 38 Concrete Curb and Gutter, Modified
 - 39 Concrete Curb and Gutter, Modified
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 - 49 Concrete Curb and Gutter, Modified
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 - 55 Concrete Curb and Gutter, Modified
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 - 89 Concrete Curb and Gutter, Modified
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 - 96 Concrete Curb and Gutter, Modified
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 - 100 Concrete Curb and Gutter, Modified



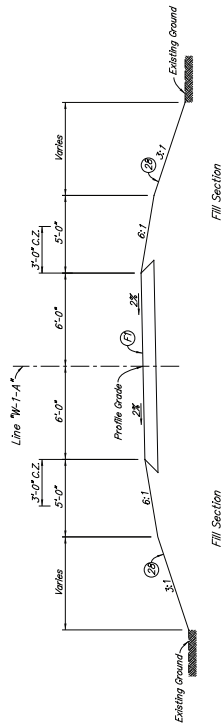
Typical Section Pufferbelly Trail
4+00.00 to 7+02.84 W-1-A
7+98.16 to 10+75.00 W-1-A



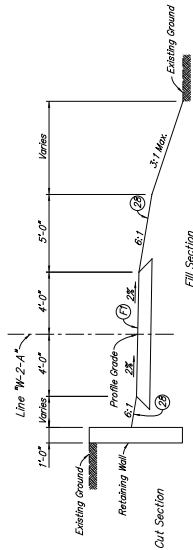
Typical Section Sidewalk Ramp
11+25.00 to 12+15.29 W-2-A



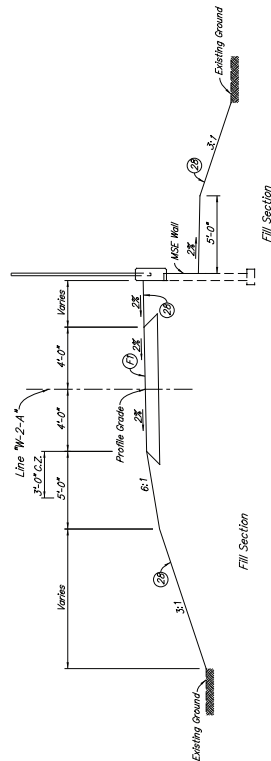
Typical Section Sidewalk Ramp
20+00.00 to 23+01.96 W-3-A



Typical Section Pufferbelly Trail
1+15.00 to 4+00.00 W-1-A
10+75.00 to 11+55.00 W-1-A



Typical Section Sidewalk Ramp
10+00.00 to 11+25.00 W-2-A



Typical Section Sidewalk Ramp
12+15.29 to 12+52.34 W-2-A

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:		ASU	CAK
CHECKED:		SMC	SMC

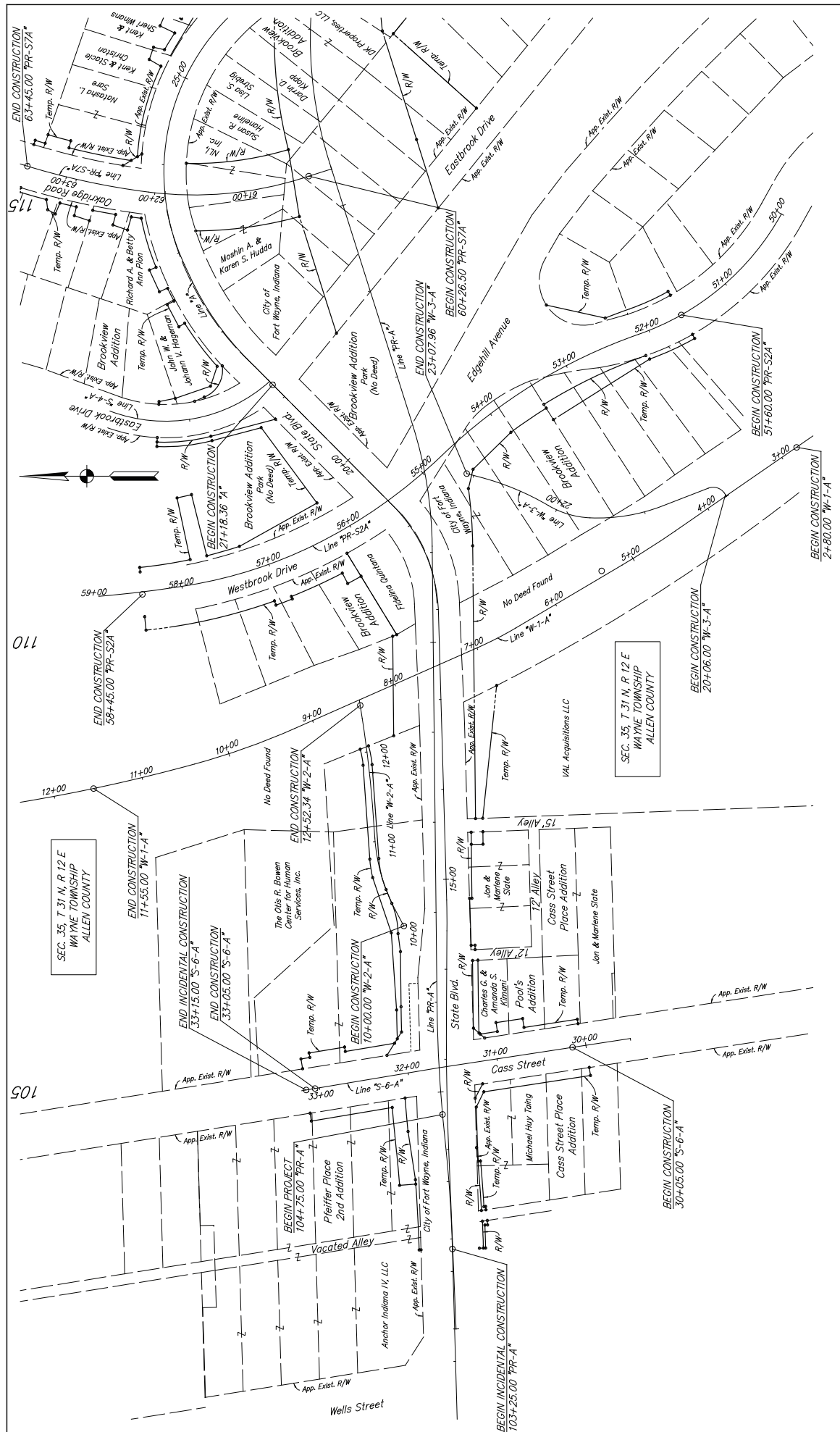
INDIANA	
DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS	
LINE "W-1-A" & "W-2-A" & "W-3-A"	

HORIZONTAL SCALE		BRIDGE FILE
VERTICAL SCALE		DESIGNATION NO.
SURVEY BOOK		SHEETS
CONTRACT		PROJECT NO.
		100514

7/12/2011 12:45:12 PM P:\10007\10040 - Drawings\0011404.RD\12.11.08.dgn

for files

Attachment 3 - 71 of 531

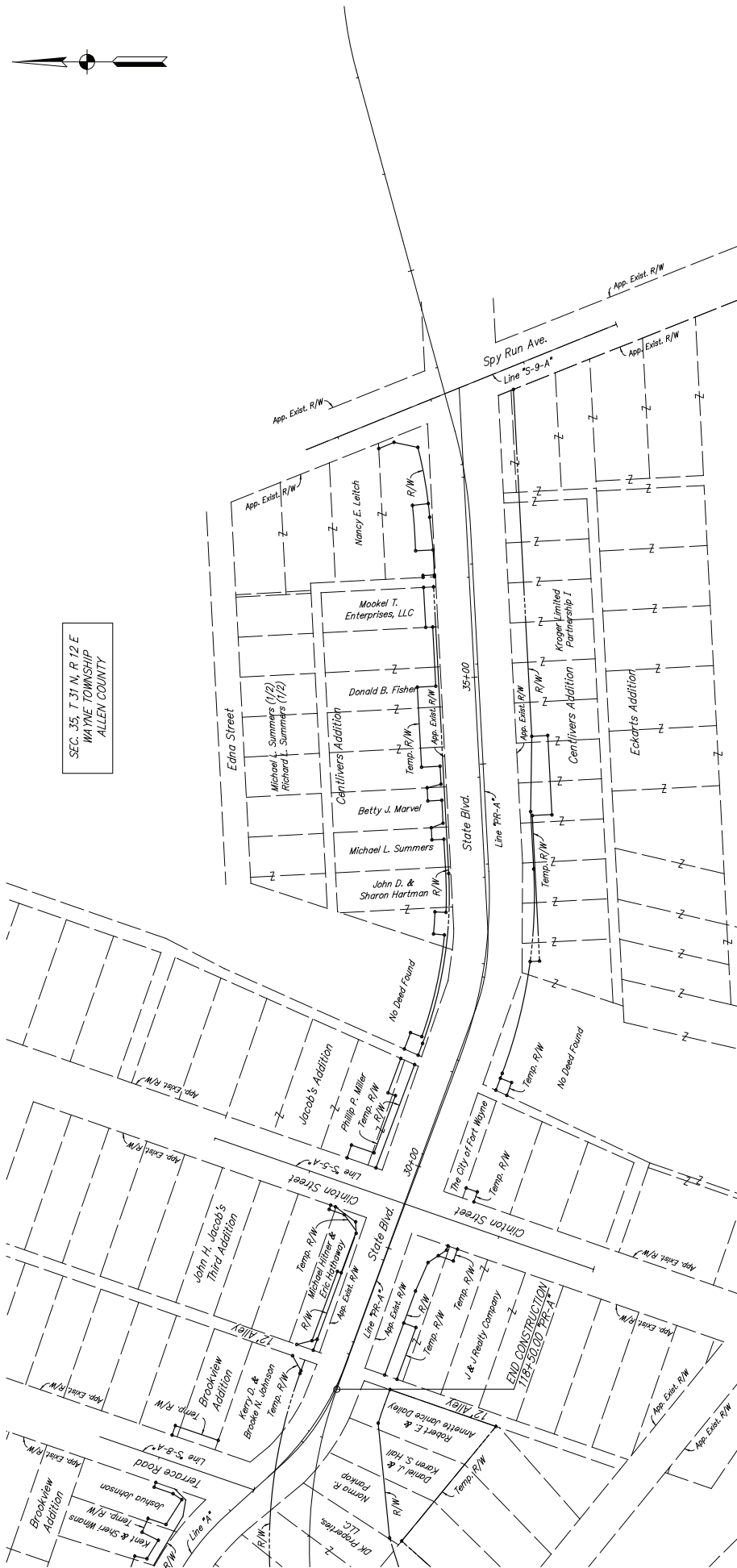


INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
PLAT NO. 1		DESIGNATION NO.	
		1005151	
		SHEETS	
		38 of 89	
		PROJECT NO.	
		1005151	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED: ASU		DRAWN: PJT	
CHECKED: SMC		SIC	

130

125

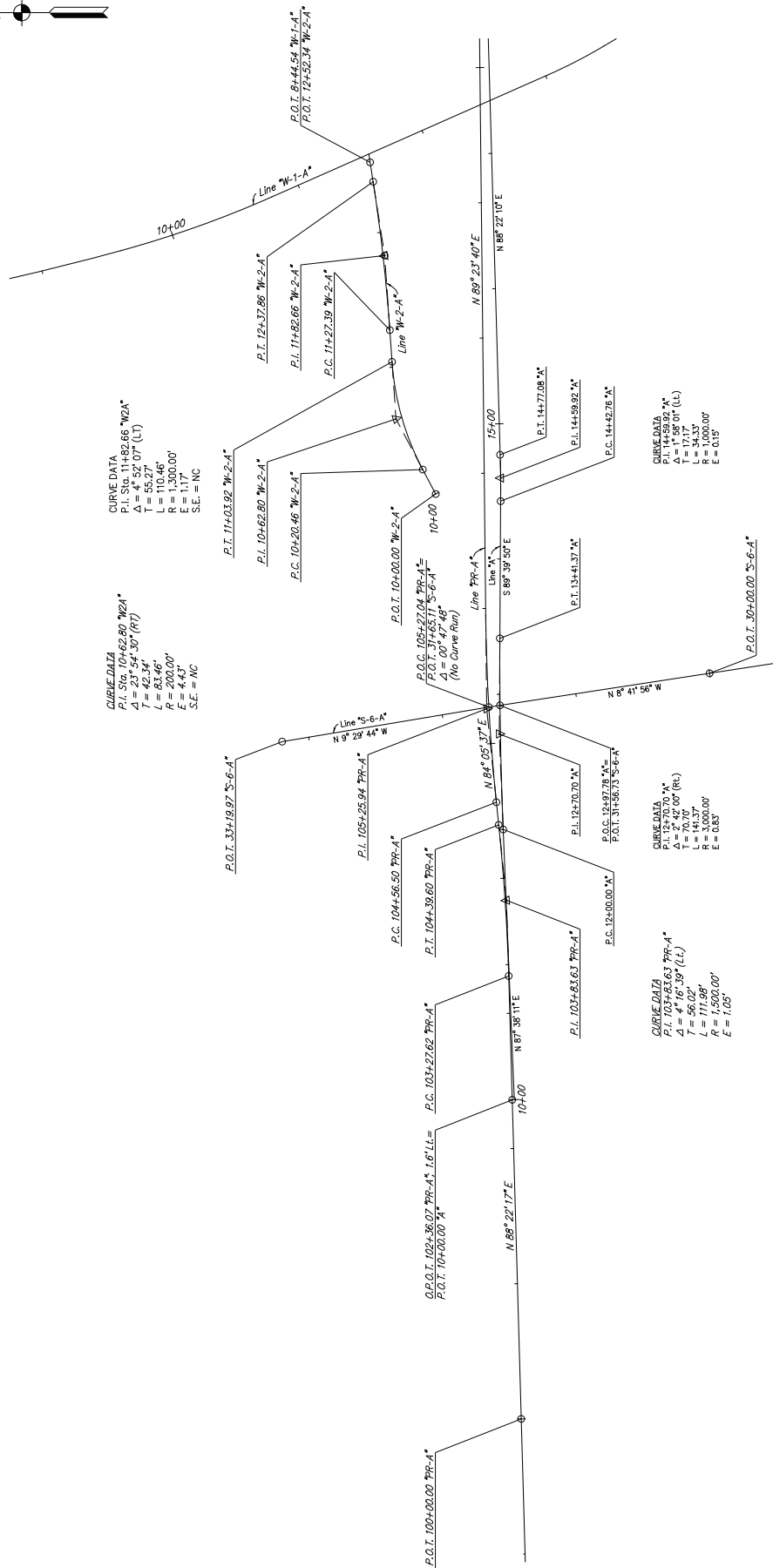
120



SEC. 35, T. 31 N., R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC	
CHECKED: SMC		PROJECT NO.		10051	
HORIZONTAL SCALE		BRIDGE FILE		INDIANA	
VERTICAL SCALE		DESIGNATION NO.		DEPARTMENT OF TRANSPORTATION	
SURVEY BOOK		SHEETS		PLAT NO. 1	
CONTRACT		PROJECT NO.		10051	

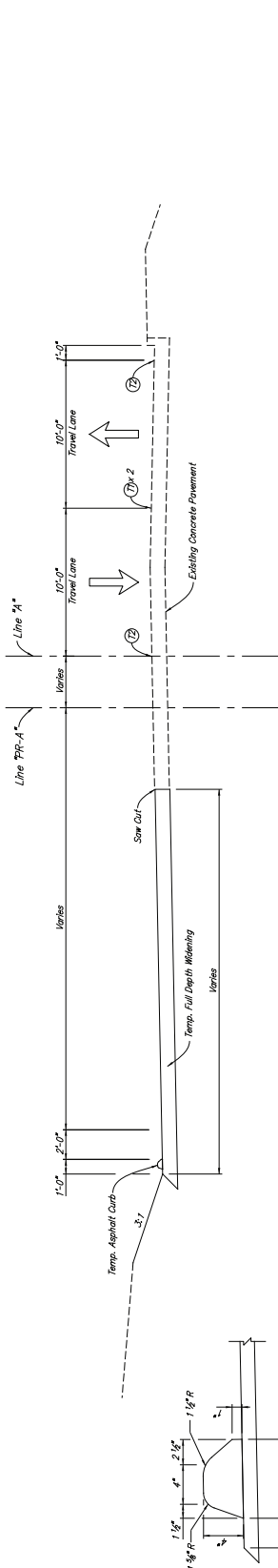


HORIZONTAL SCALE	BRIDGE FILE
1" = 40'	
VERTICAL SCALE	DESIGNATION NO.
	1005151
SURVEY BOOK	SHEETS
	40 of 89
CONTRACT	PROJECT NO.
	1005151

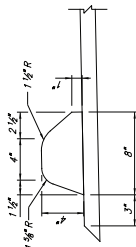
INDIANA
DEPARTMENT OF TRANSPORTATION

GEOMETRIC TIE-UP

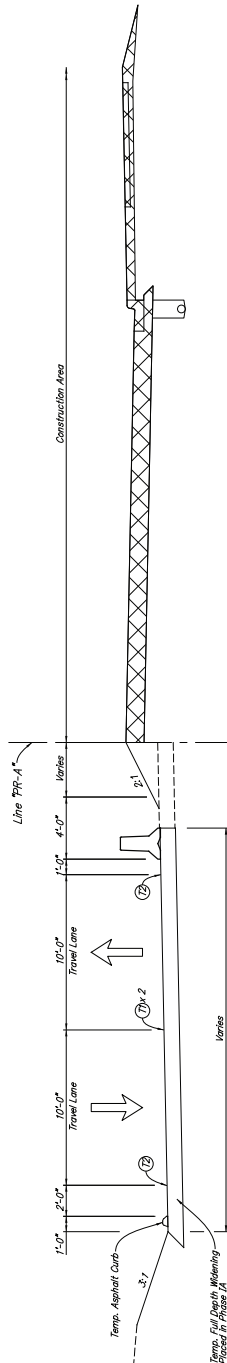
RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: _____	ASU _____	DRAWN: _____	CAK _____
CHECKED: _____	SMC _____	CHECKED: _____	SMC _____



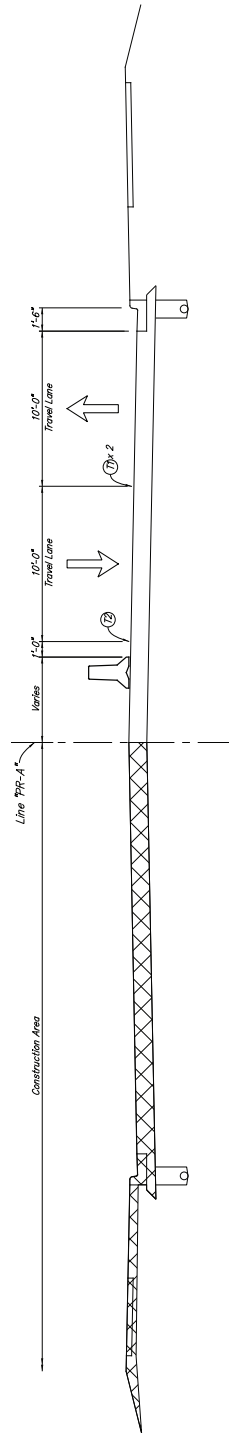
ASPHALT CURB (MOUNTABLE) DETAIL



PHASE I-A



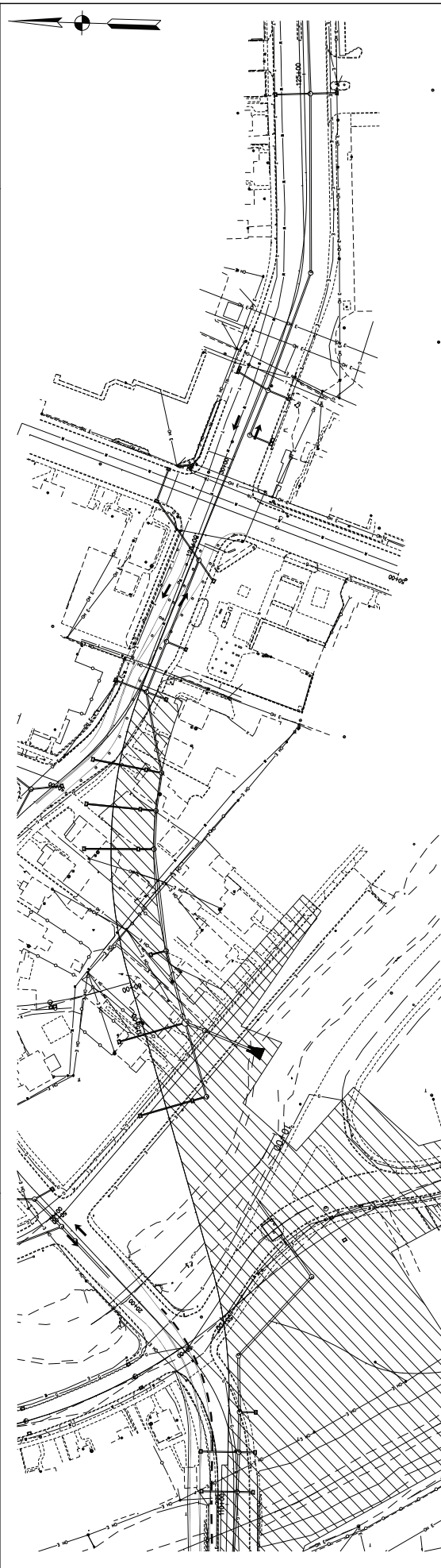
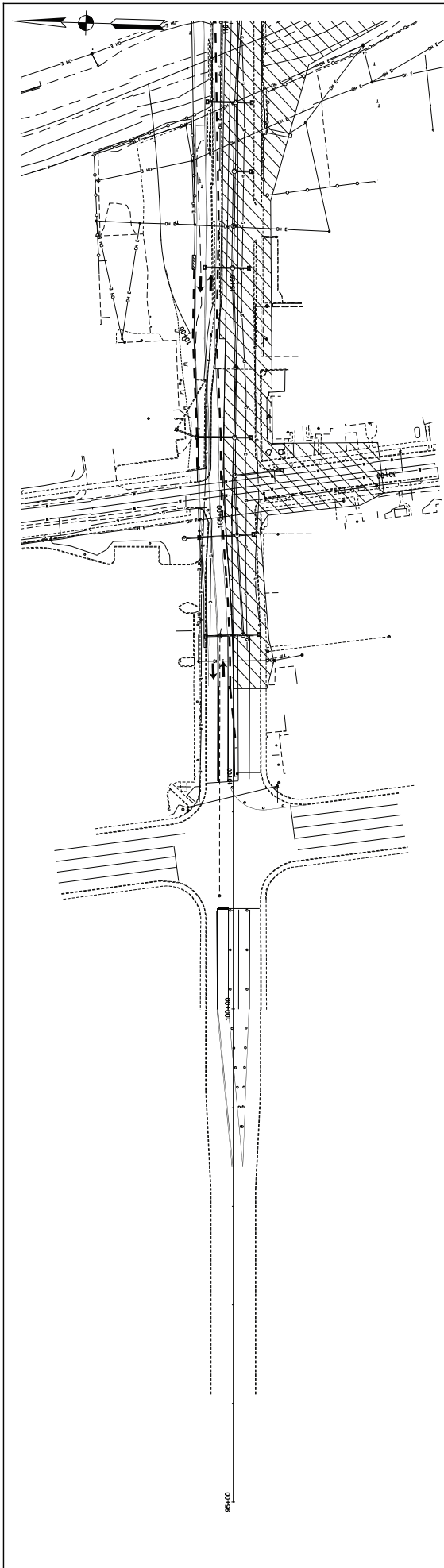
PHASE I



PHASE II

Notes: NMS Surface to be Placed Following Phase II Under Live Traffic

<p>LEGEND</p> <p>Direction of Traffic</p> <p>Standard Drum</p> <p>Temporary Concrete Barrier</p>	<p>⑦ Temporary Pavement Marking, Solid, Yellow, 4 in.</p> <p>⑧ Temporary Pavement Marking, Solid, White, 4 in.</p> <p>Construction Area</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>MAINTENANCE OF TRAFFIC</p> <p>TYPICAL CROSS SECTIONS</p>	<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p> <p>DESIGNED: ASU</p> <p>CHECKED: SMC</p> <p>DRAWN: CAK</p> <p>CHECKED: SMC</p>	<p>HORIZONTAL SCALE</p>	BRIDGE FILE
				<p>VERTICAL SCALE</p>	DESIGNATION NO.
				<p>SURVEY BOOK</p>	100514
				<p>CONTRACT</p>	<p>SHEETS</p> <p>44 of 100</p> <p>PROJECT NO.</p> <p>100514</p>



LEGEND

-----	Temporary Barrier	↓	Direction of Traffic	▲▲	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
---	Detour Route	▨	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		▩	Standard Drum	▲	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

INDIANA		BRIDGE FILE
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.
MAINTENANCE OF TRAFFIC		1005154
PHASE I		SHEETS
		45 of 100
		PROJECT NO.
		1005154

RECOMMENDED FOR APPROVAL

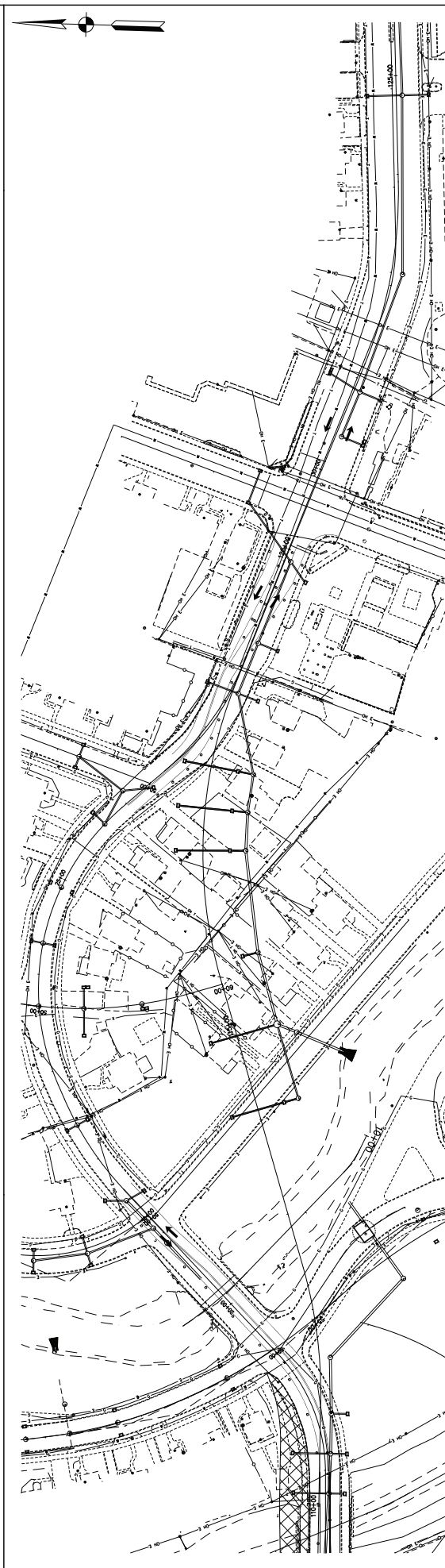
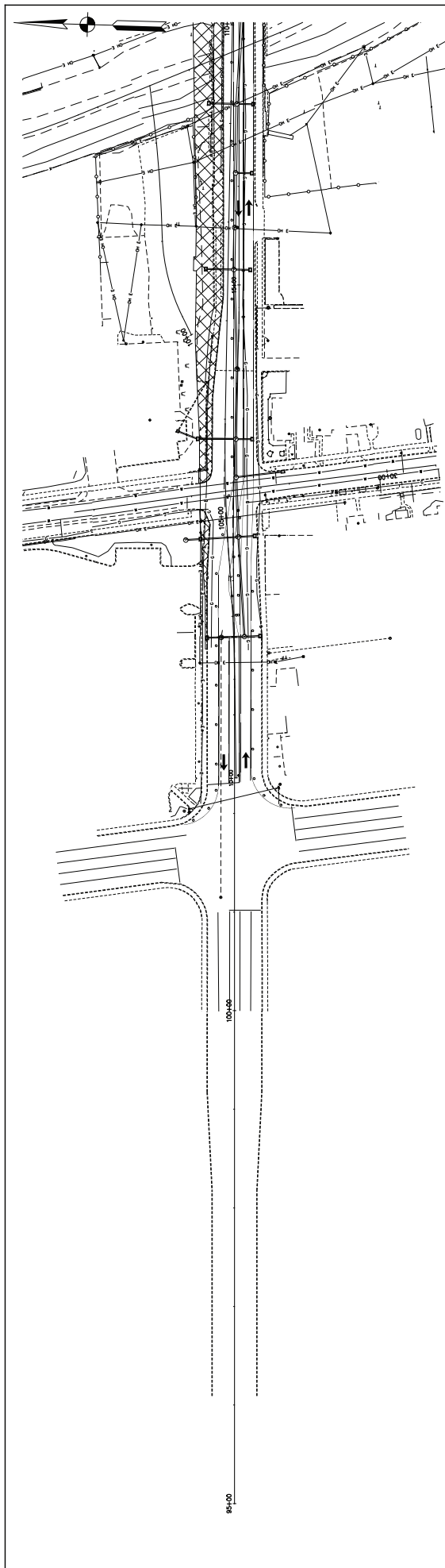
DESIGNED: ASU

CHECKED: SMC

DATE

PJT

SMC



LEGEND

-----	Temporary Barrier	↓	Direction of Traffic	▲	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
---	Detour Route	▨	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		□	Standard Drum	▲	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

INDIANA		BRIDGE FILE
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.
MAINTENANCE OF TRAFFIC		1005154
PHASE I-A		SHEETS
		47 of 100
		PROJECT NO.
		1005154

RECOMMENDED FOR APPROVAL

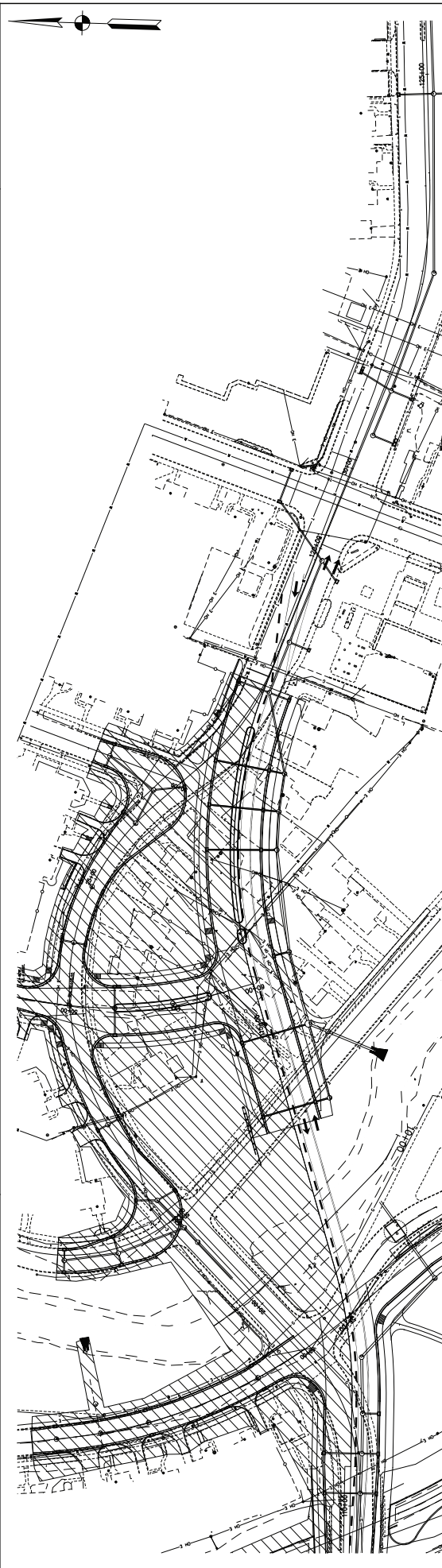
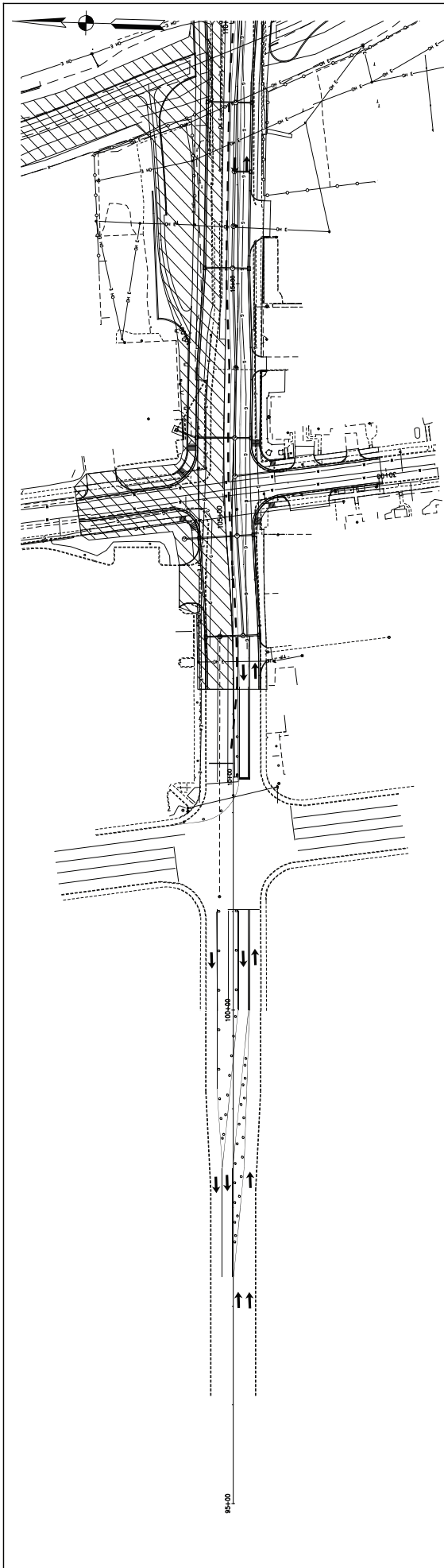
DESIGNED: ASU

CHECKED: SMC

DATE

DRAWN: PJT

CHECKED: SMC



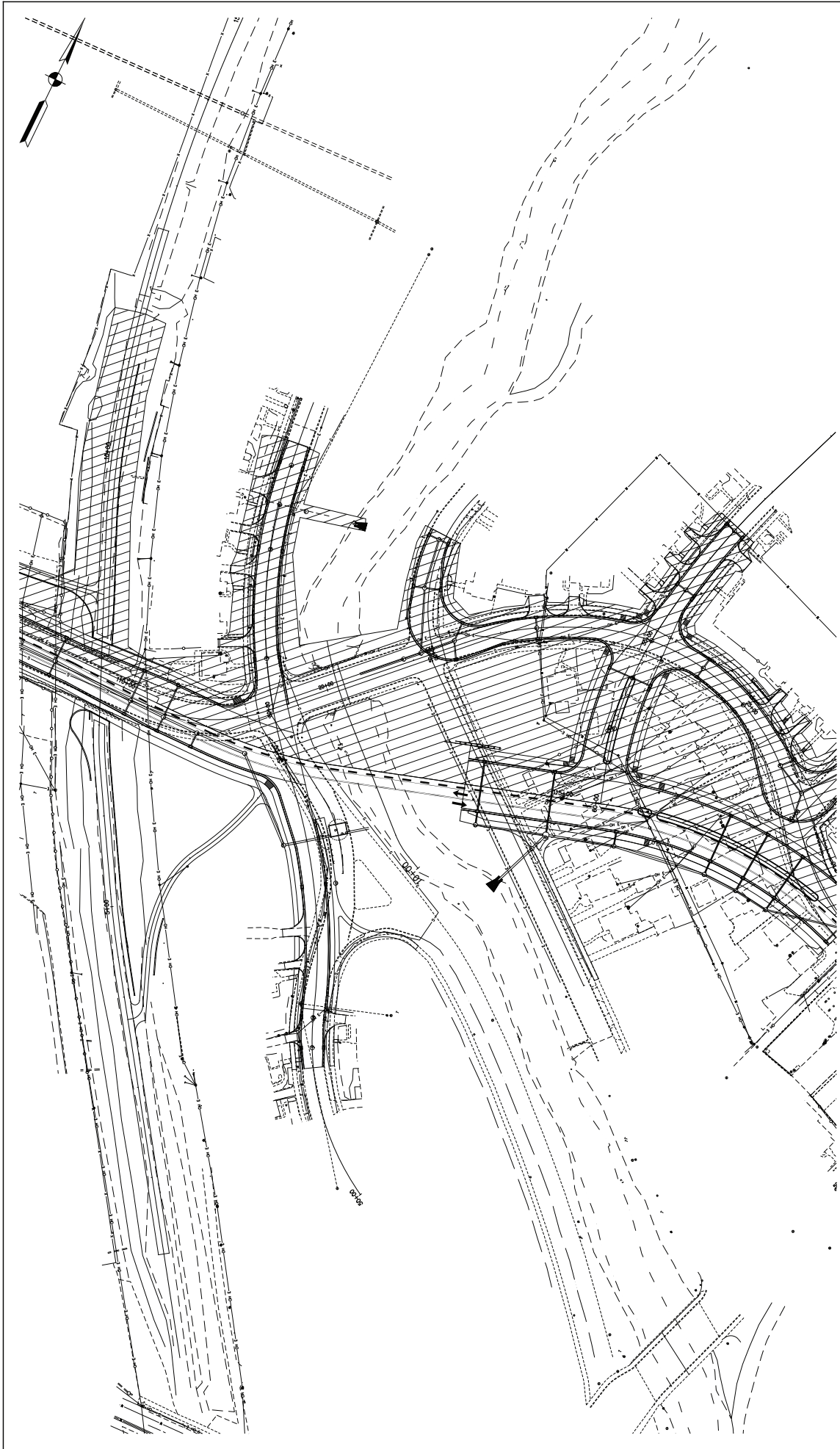
LEGEND

-----	Temporary Barrier	↓	Direction of Traffic	--- Type II Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.	
---	Detour Route	▨	Construction Area	--- Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.	
		▩	Standard Drum	③	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE
		ASU	DRAWN	PJT
DESIGNED:		CHECKED:		SNC
SNC		SNC		SNC

INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE II

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	48 of 100
	PROJECT NO.
	1005154



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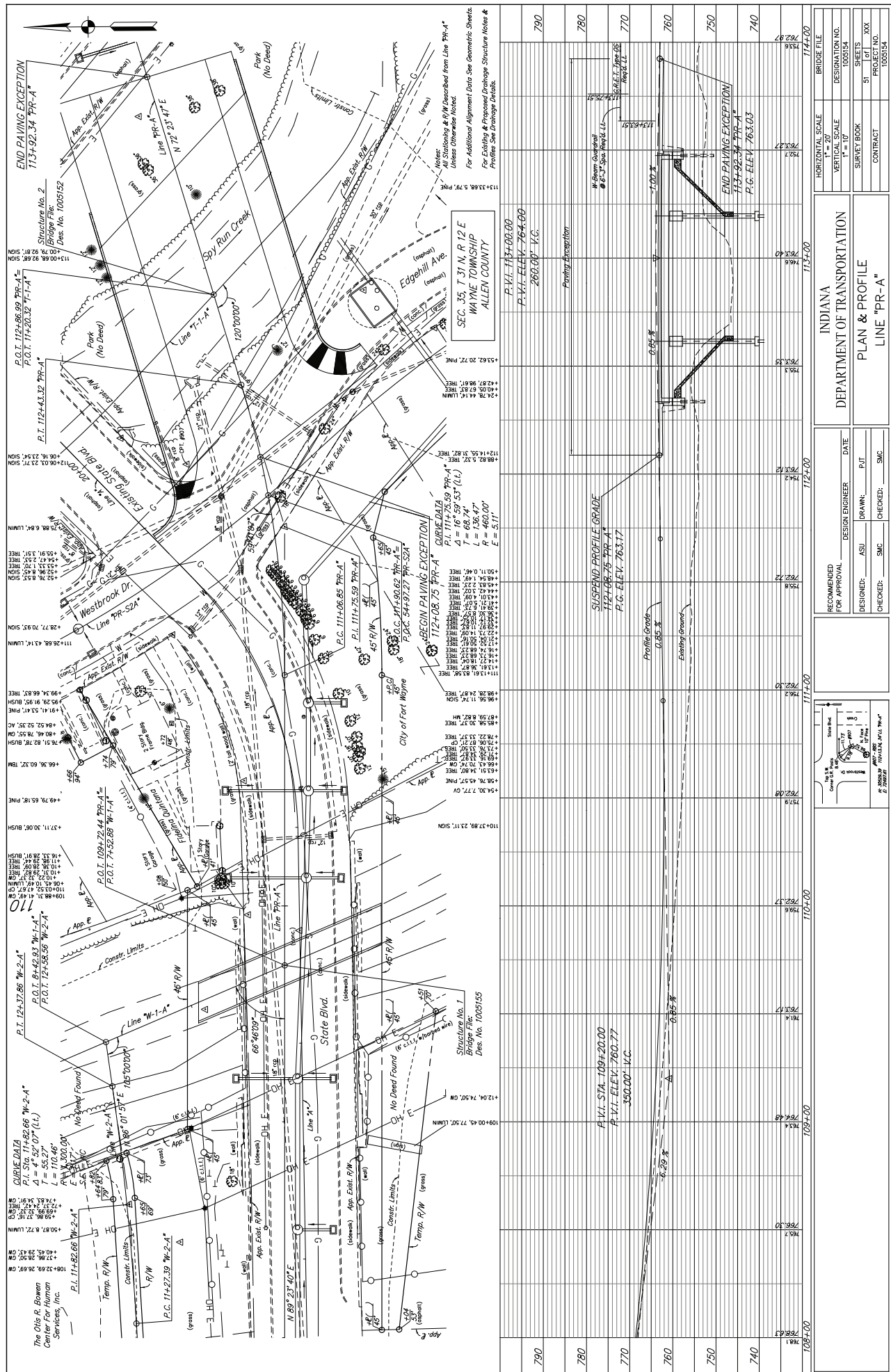
LEGEND

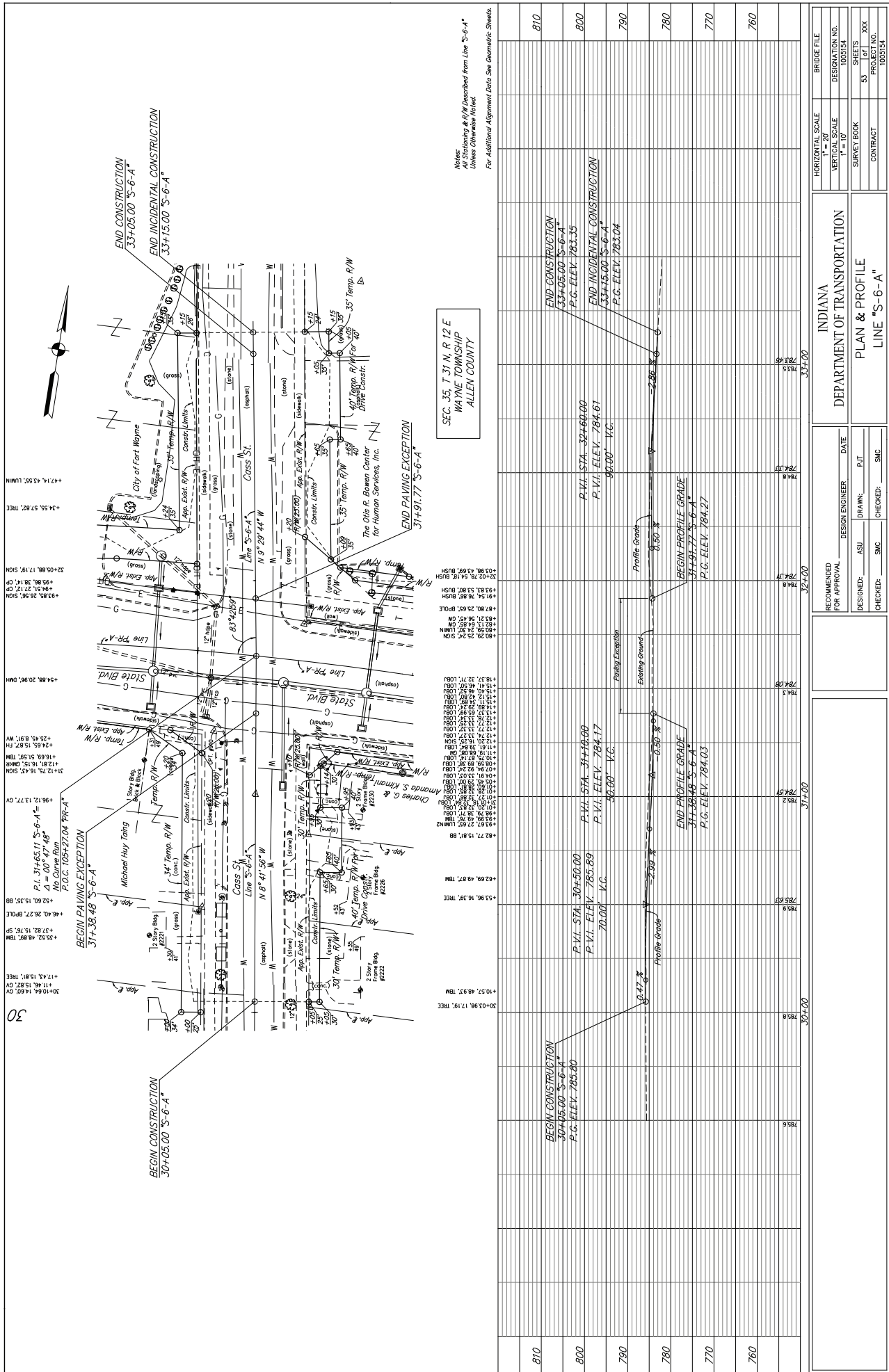
-----	Temporary Barrier	↓	Direction of Traffic	Ⓐ	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
---	Detour Route	⊠	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		Ⓢ	Standard Drum	Ⓢ	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

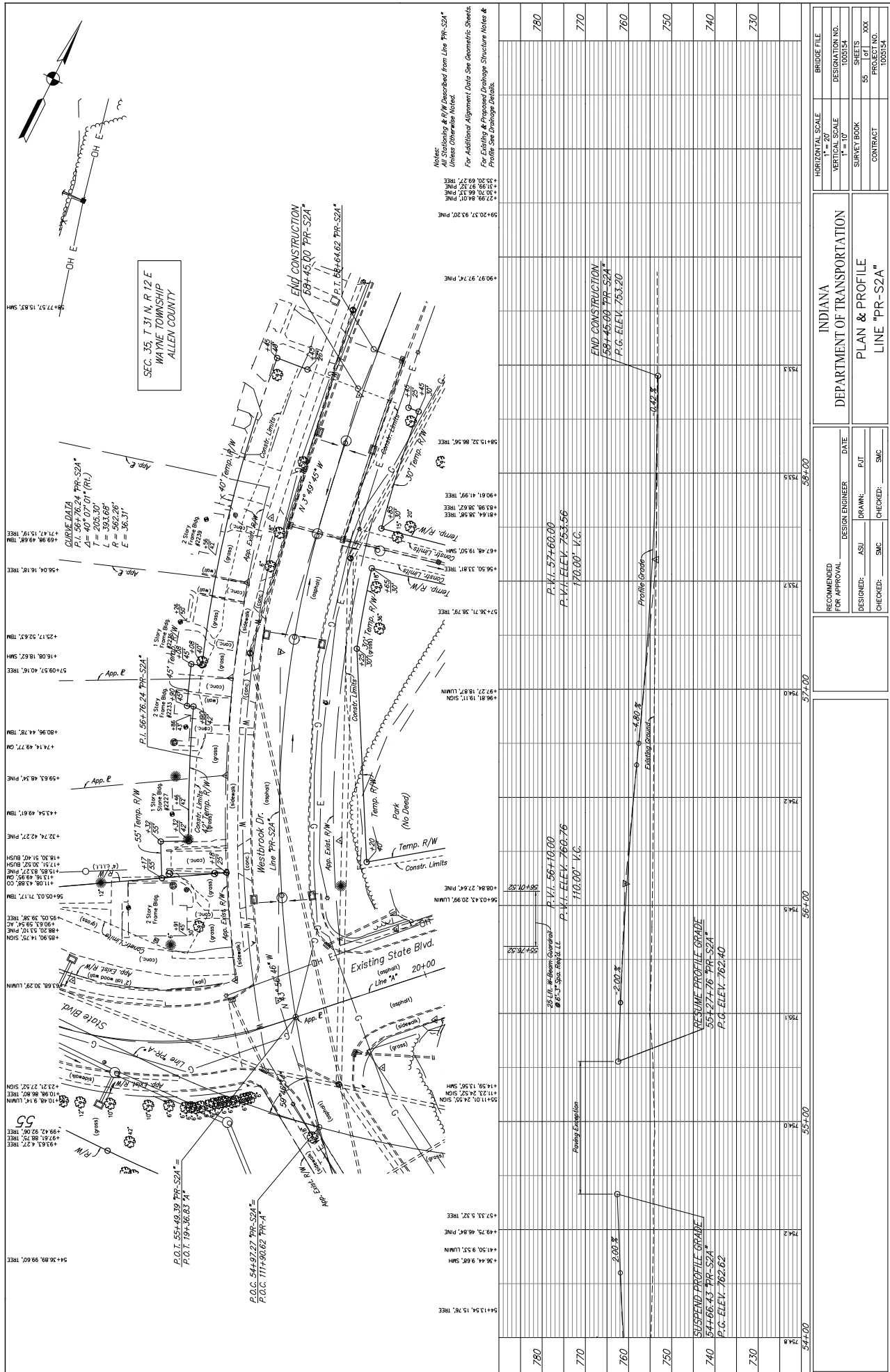
INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE II

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
	100514
SURVEY BOOK	SHEETS
CONTRACT	49 of 100
	PROJECT NO.
	100514



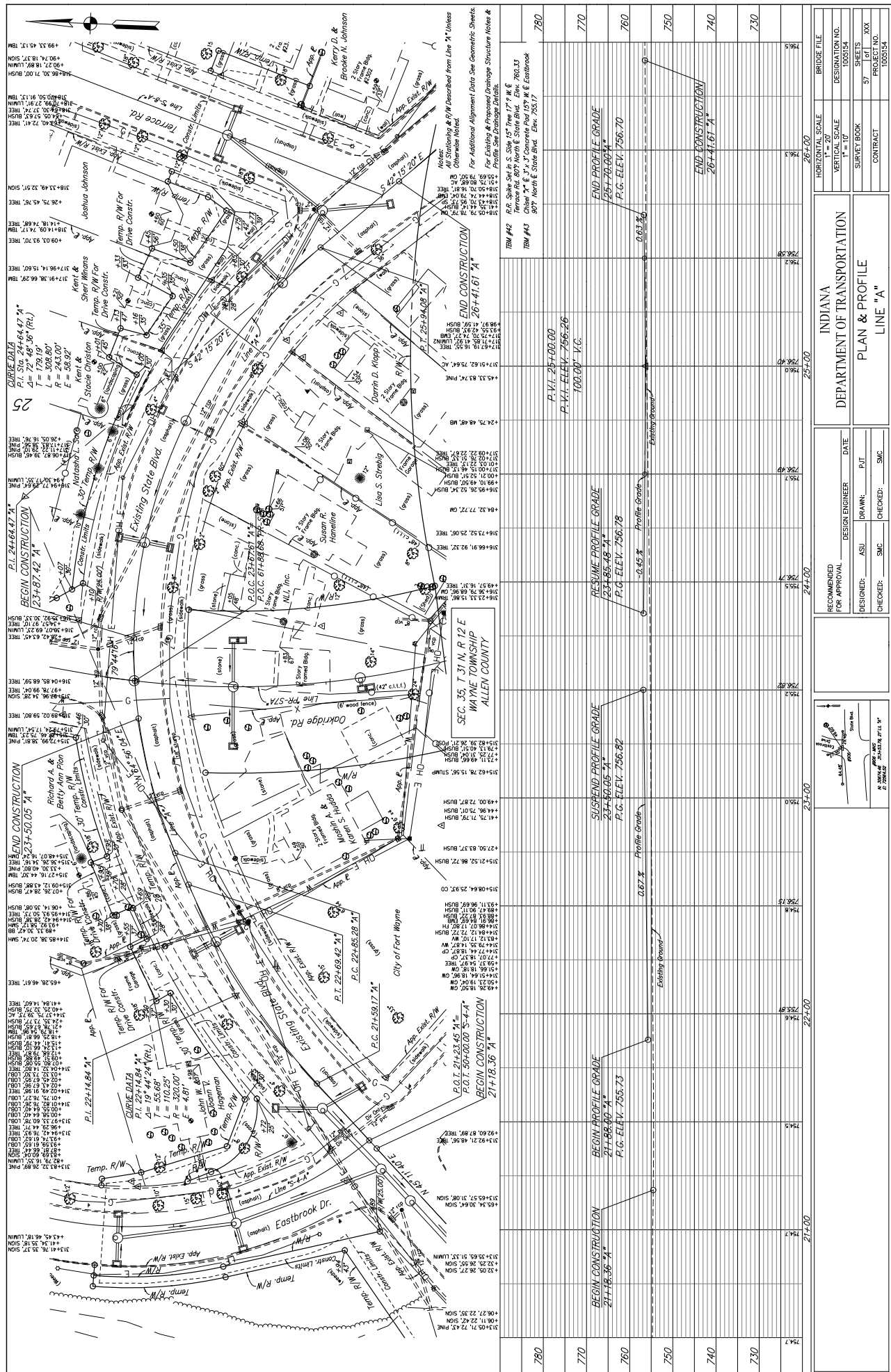


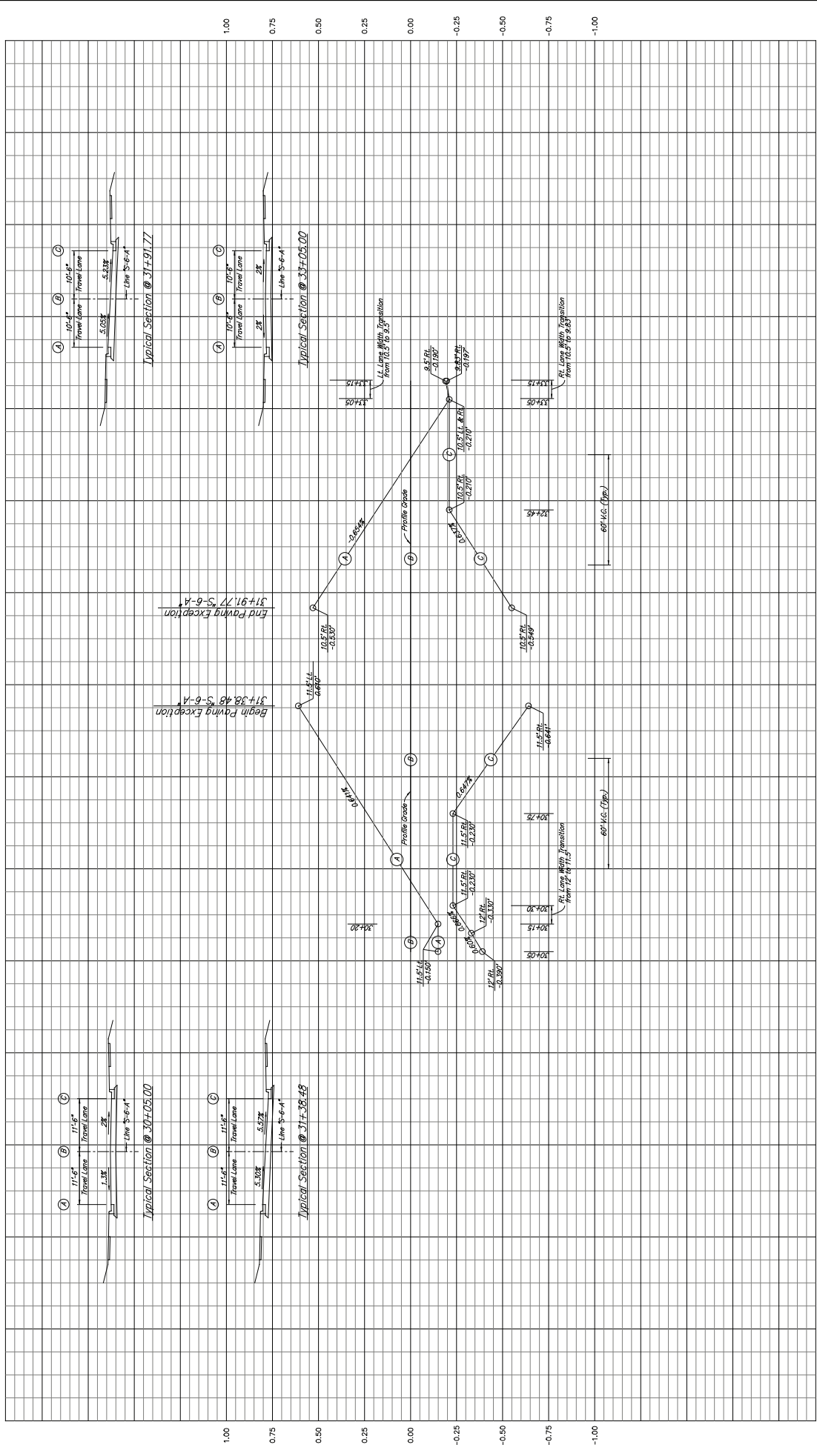




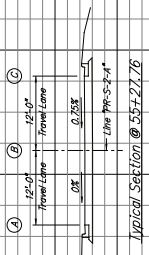
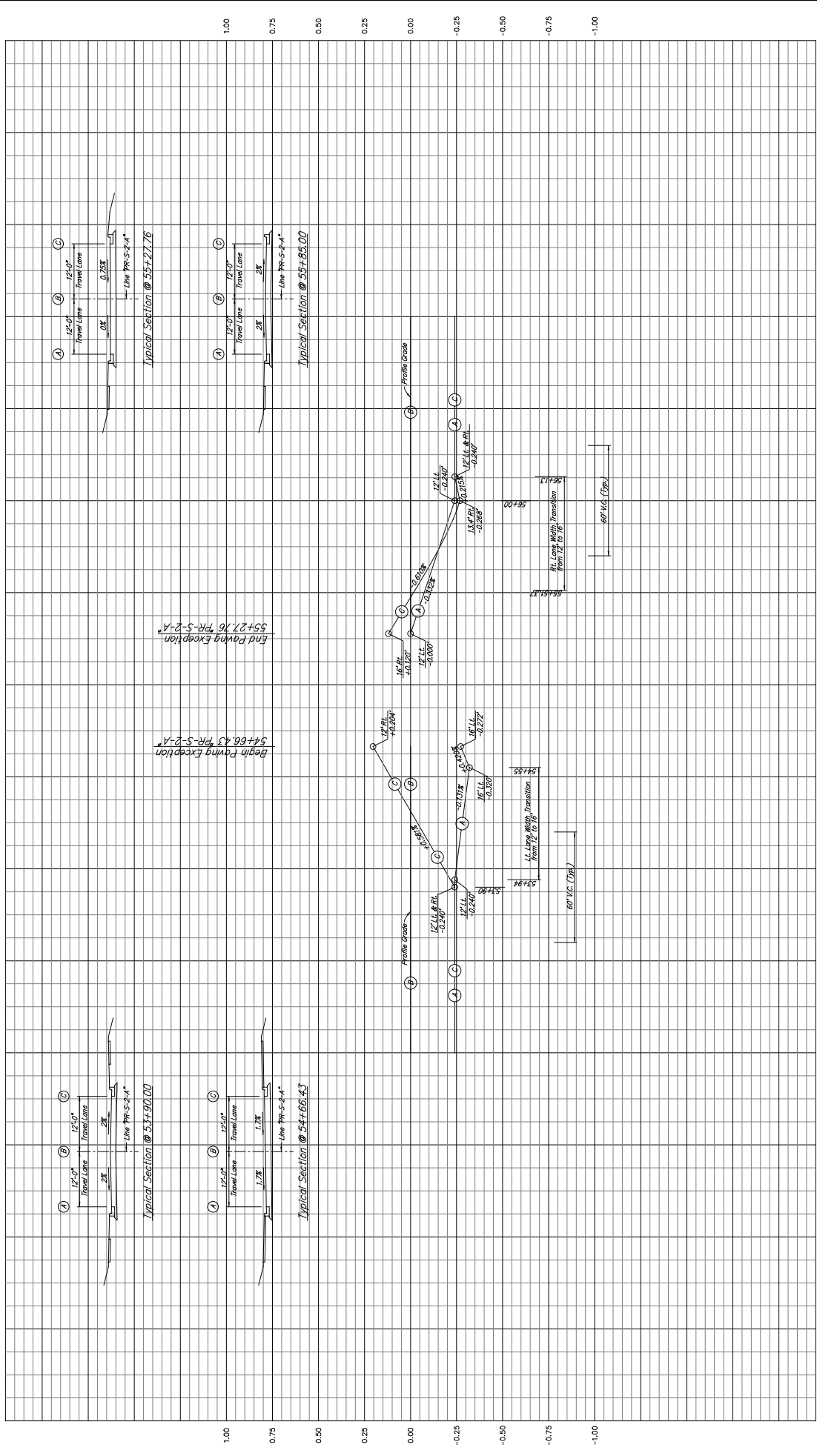
INDIANA		DEPARTMENT OF TRANSPORTATION	
HORIZONTAL SCALE	1" = 10'	DESIGN ENGINEER	DATE
VERTICAL SCALE	1" = 10'	DRAWN	PJT
SURVEY BOOK	55	CHECKED	SMC
CONTRACT	100514	PROJECT NO.	100514

INDIANA		DEPARTMENT OF TRANSPORTATION	
HORIZONTAL SCALE	1" = 10'	DESIGN ENGINEER	DATE
VERTICAL SCALE	1" = 10'	DRAWN	PJT
SURVEY BOOK	55	CHECKED	SMC
CONTRACT	100514	PROJECT NO.	100514





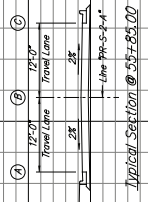
INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
PAVEMENT EDGE		100514	
TRANSITION DIAGRAM		SHEETS	
LINE S-6-A		62 of 100	
		PROJECT NO.	
		100514	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED: ASU		DRAWN: ASU	
CHECKED: SMC		CHECKED: SMC	



Typical Section @ 53+90.00

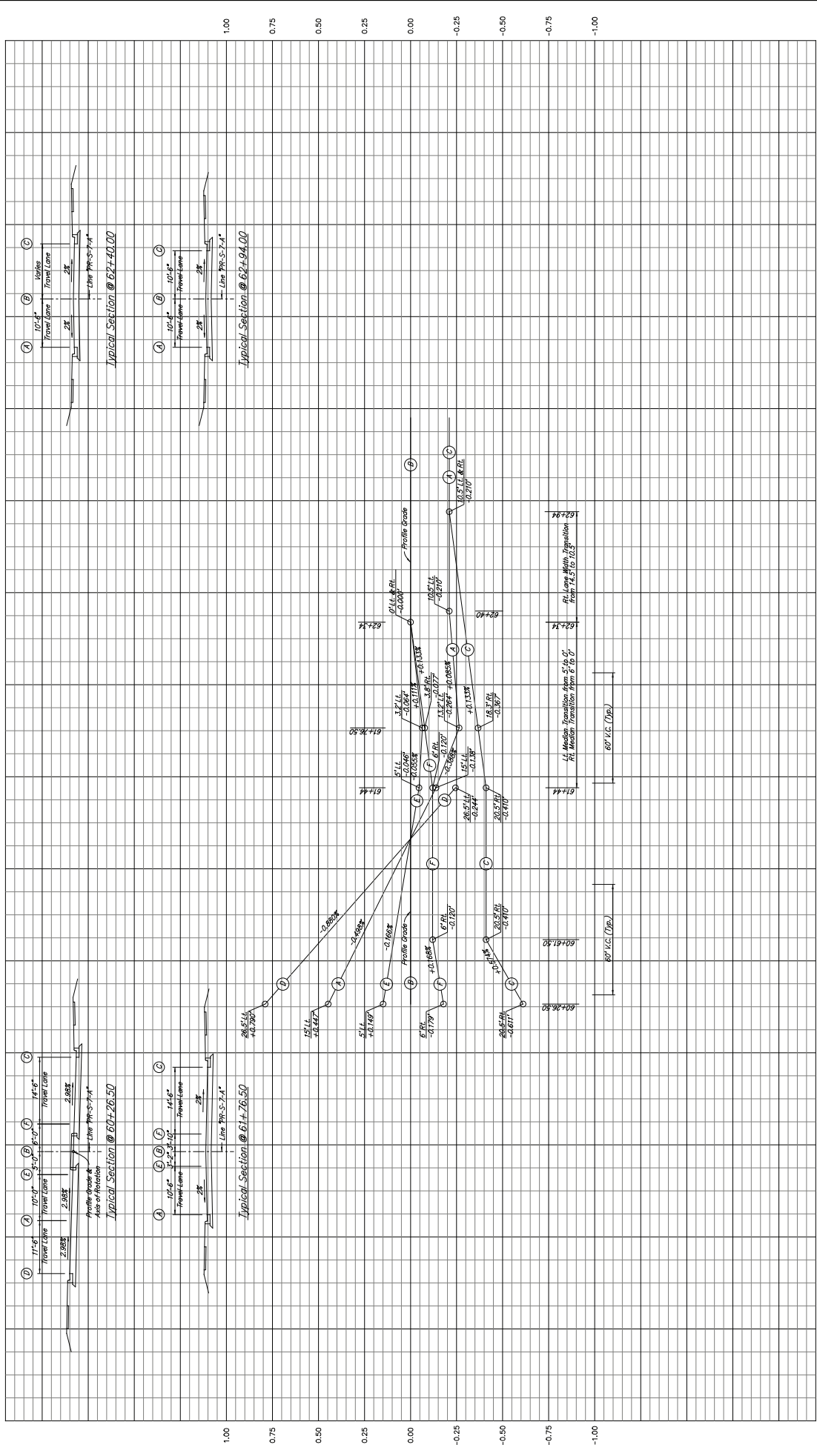
Begin Paving Exception
54+66.43 PIR-S-2-A

End Paving Exception
55+27.16 PIR-S-2-A



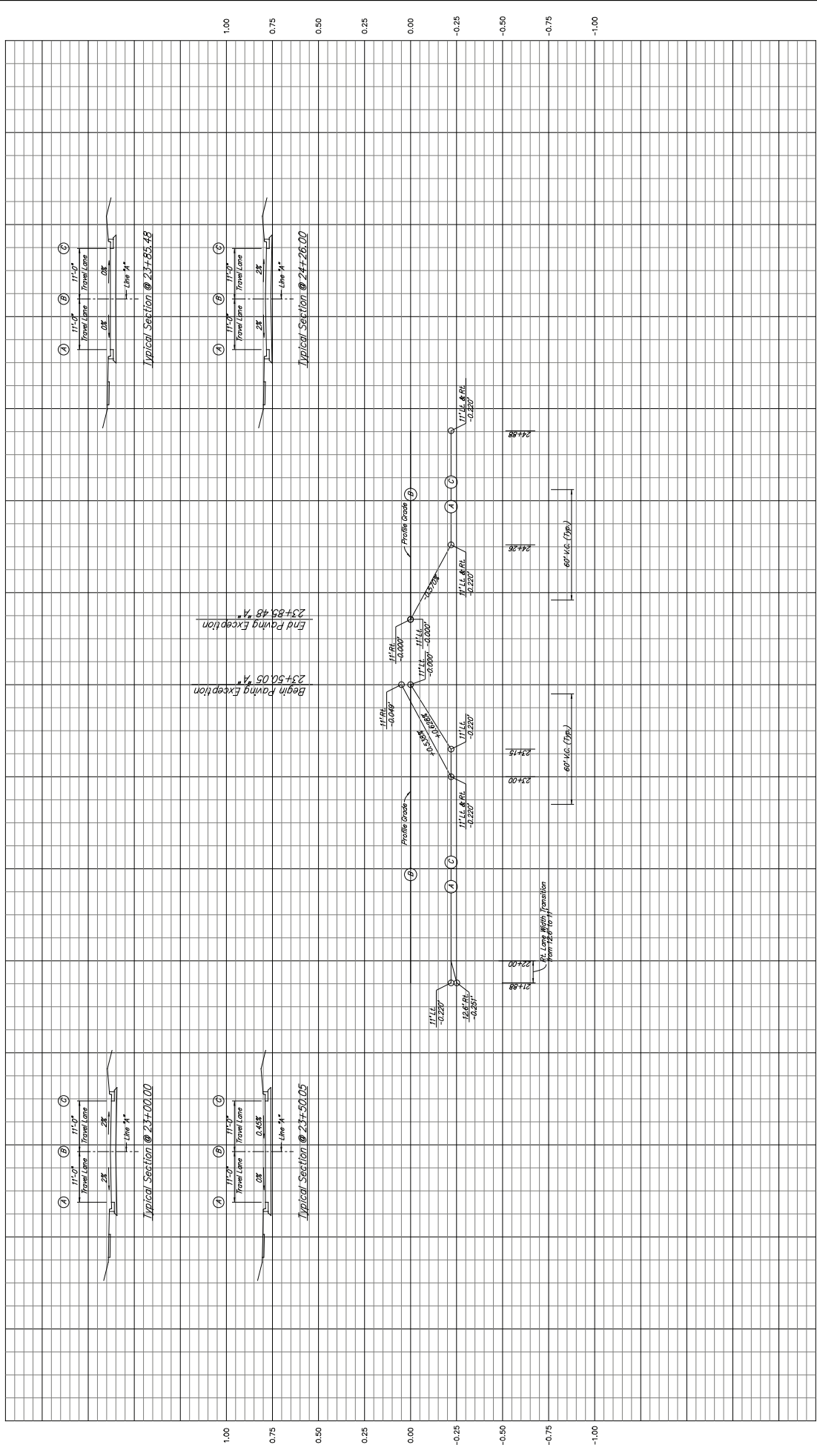
Typical Section @ 55+85.00

INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	
PAVEMENT EDGE TRANSITION DIAGRAM LINE "PR-S-2-A"		VERTICAL SCALE	
		1" = 0.25'	
		DESIGNATION NO.	
		SURVEY BOOK	
		CONTRACT	
		SHEETS	
		63 of 100	
		PROJECT NO.	
		100514	



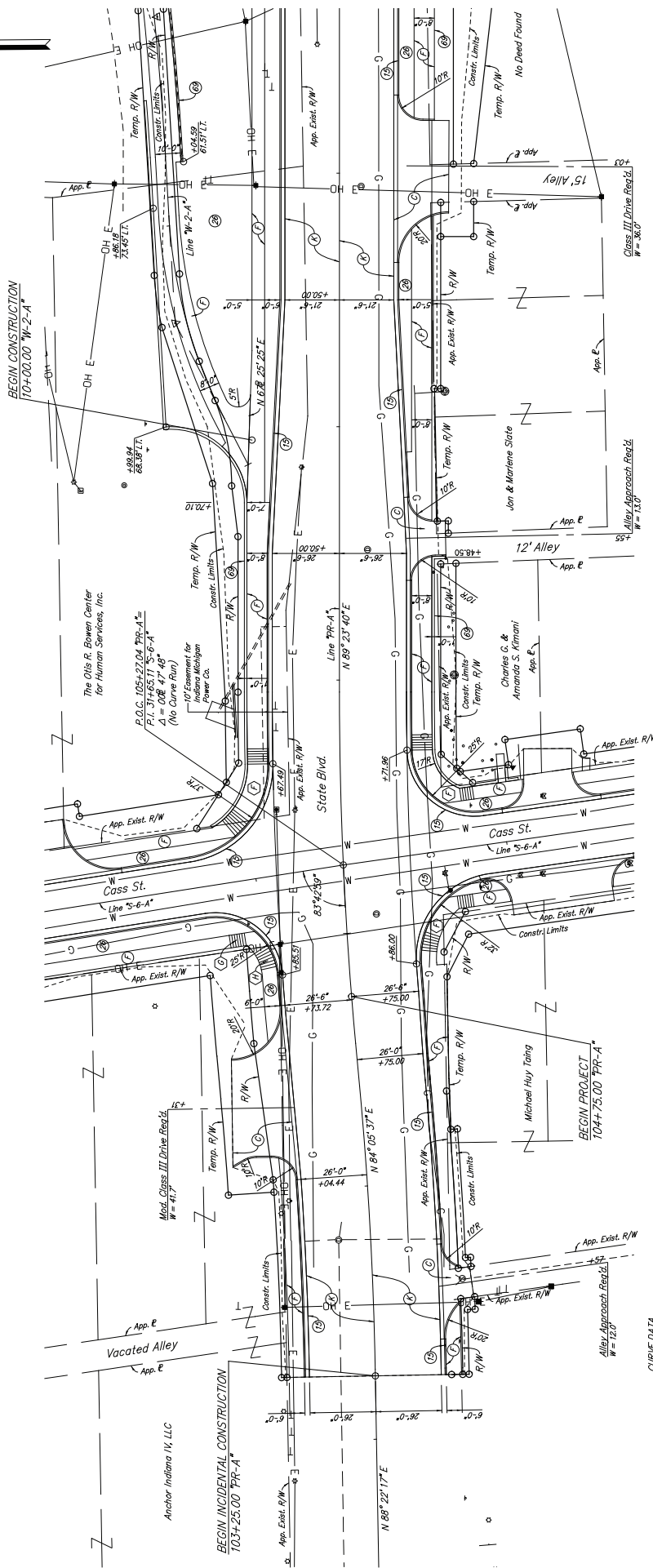
60+00 61+00 62+00 63+00 64+00

<div> <div>INDIANA</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>PAVEMENT EDGE</div> <div>TRANSITION DIAGRAM</div> <div>LINE "PR S-7-A"</div> </div>		<div> <div>RECOMMENDED FOR APPROVAL</div> <div>DESIGNED: ASU</div> <div>CHECKED: SMC</div> </div>		<div> <div>DESIGN ENGINEER</div> <div>DRAWN: ASU</div> <div>CHECKED: SMC</div> </div>		<div> <div>DATE</div> <div>ASU</div> <div>SMC</div> </div>		<div> <div>HORIZONTAL SCALE</div> <div>VERTICAL SCALE</div> <div>SURVEY BOOK</div> <div>CONTRACT</div> </div>		<div> <div>BRIDGE FILE</div> <div>DESIGNATION NO.</div> <div>SHEETS</div> <div>PROJECT NO.</div> </div>	



INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
PAVEMENT EDGE		100514	
TRANSITION DIAGRAM		SHEETS	
LINE "A"		65 of 100	
		PROJECT NO.	
		100514	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED: ASU		DRAWN: ASU	
CHECKED: SMC		CHECKED: SMC	

CURVE DATA
P.I. 1034+25.94 PPR-A*
Δ = 5° 18' 03" (RL)
T = 69.44'
L = 138.78'
R = 1,500.00'
E = 1.61'



CURVE DATA
P.I. 1034+25.94 PPR-A*
Δ = 5° 18' 03" (RL)
T = 69.44'
L = 138.78'
R = 1,500.00'
E = 1.61'

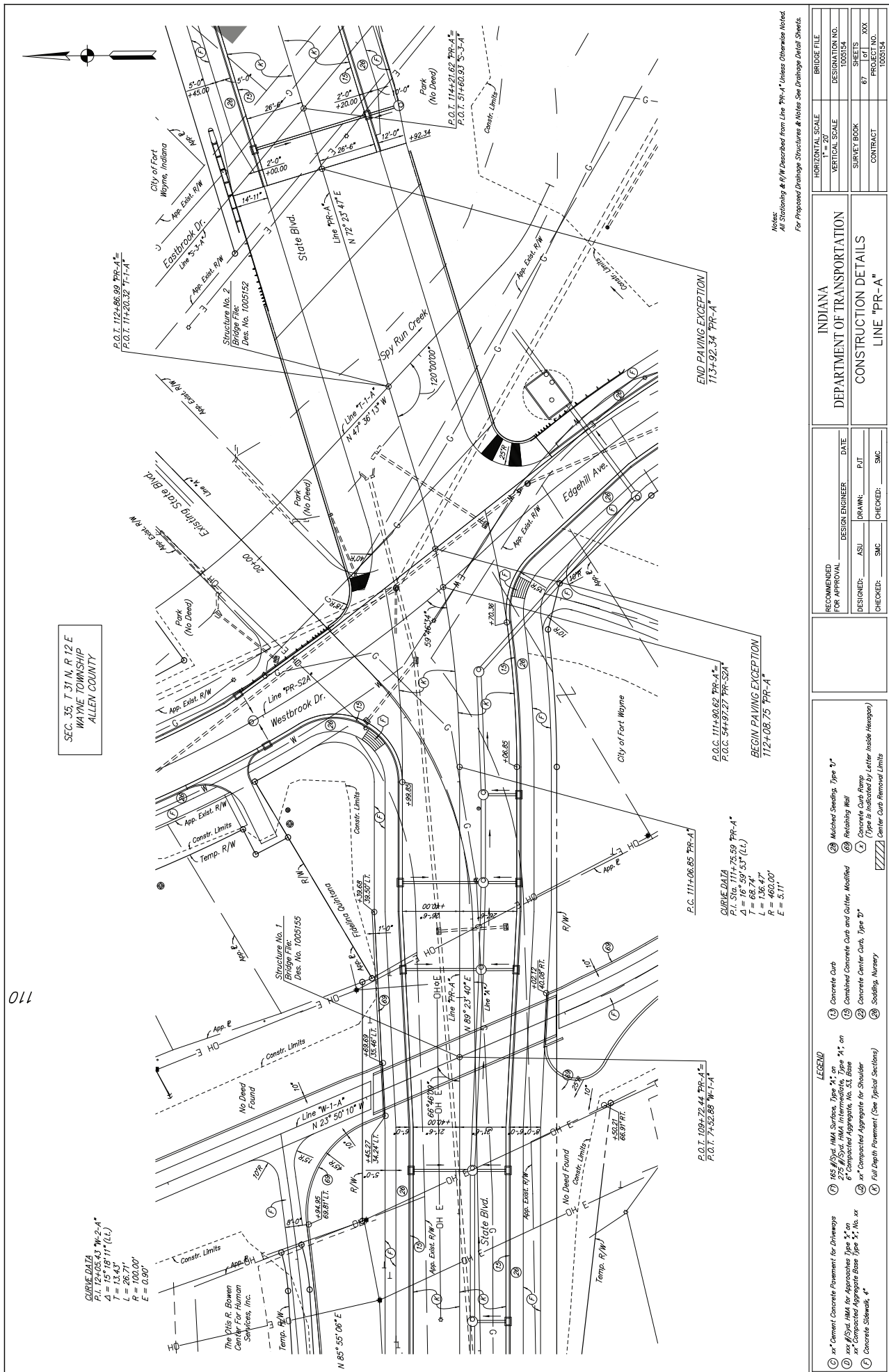
SEC. 35, T. 31 N. R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Indicated
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

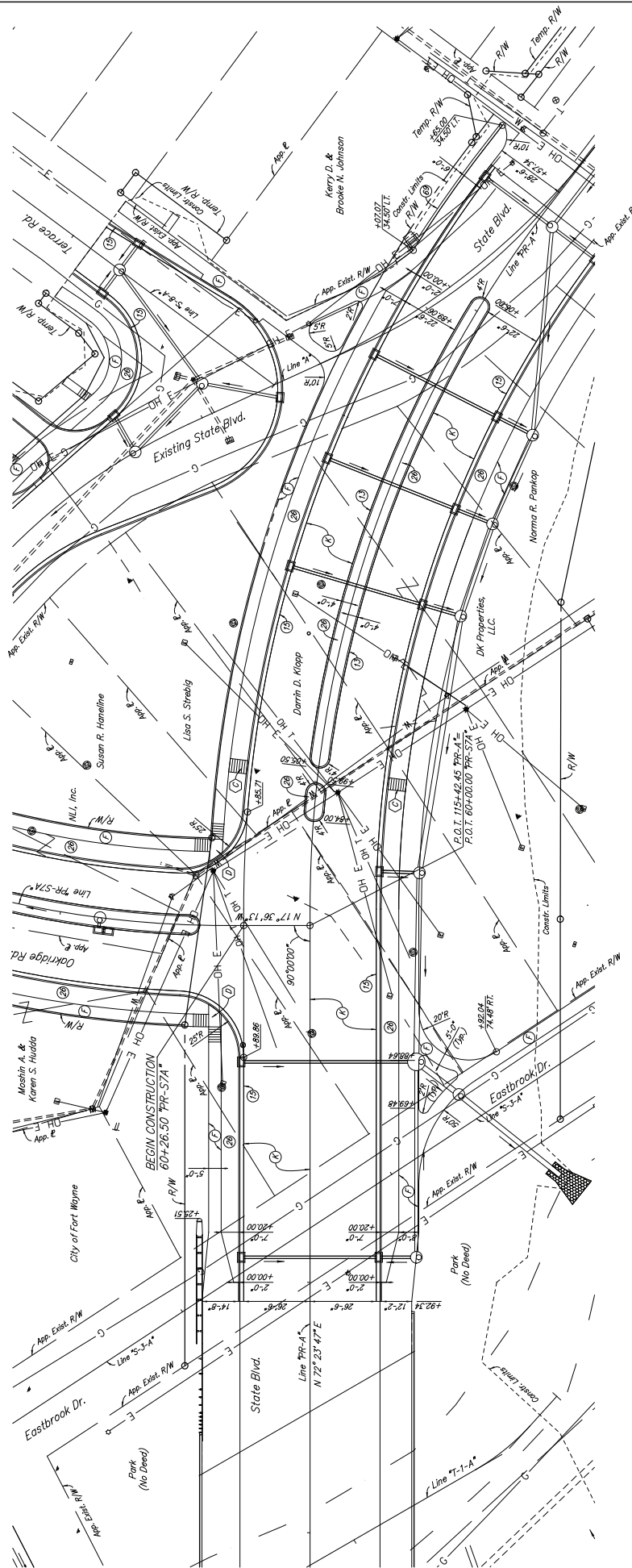
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PAT		SHEET NO.	
CHECKED: SMC		CHECKED: SMC		PROJECT NO.	
				CONTRACT	
				SHEETS	
				66 of 100	
				BRIDGE FILE	
				DESIGNATION NO.	
				100514	
				SURVEY BOOK	
				66 of 100	
				CONTRACT	
				100514	

INDIANA
DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS
LINE "PR-A"

- LEGEND
- 1. Cement Concrete Pavement for Driveways
 - 2. Cement Concrete Pavement for Driveways
 - 3. Cement Concrete Pavement for Driveways
 - 4. Cement Concrete Pavement for Driveways
 - 5. Cement Concrete Pavement for Driveways
 - 6. Cement Concrete Pavement for Driveways
 - 7. Cement Concrete Pavement for Driveways
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 - 11. Cement Concrete Pavement for Driveways
 - 12. Cement Concrete Pavement for Driveways
 - 13. Cement Concrete Pavement for Driveways
 - 14. Cement Concrete Pavement for Driveways
 - 15. Cement Concrete Pavement for Driveways
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 - 79. Cement Concrete Pavement for Driveways
 - 80. Cement Concrete Pavement for Driveways
 - 81. Cement Concrete Pavement for Driveways
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 - 98. Cement Concrete Pavement for Driveways
 - 99. Cement Concrete Pavement for Driveways
 - 100. Cement Concrete Pavement for Driveways



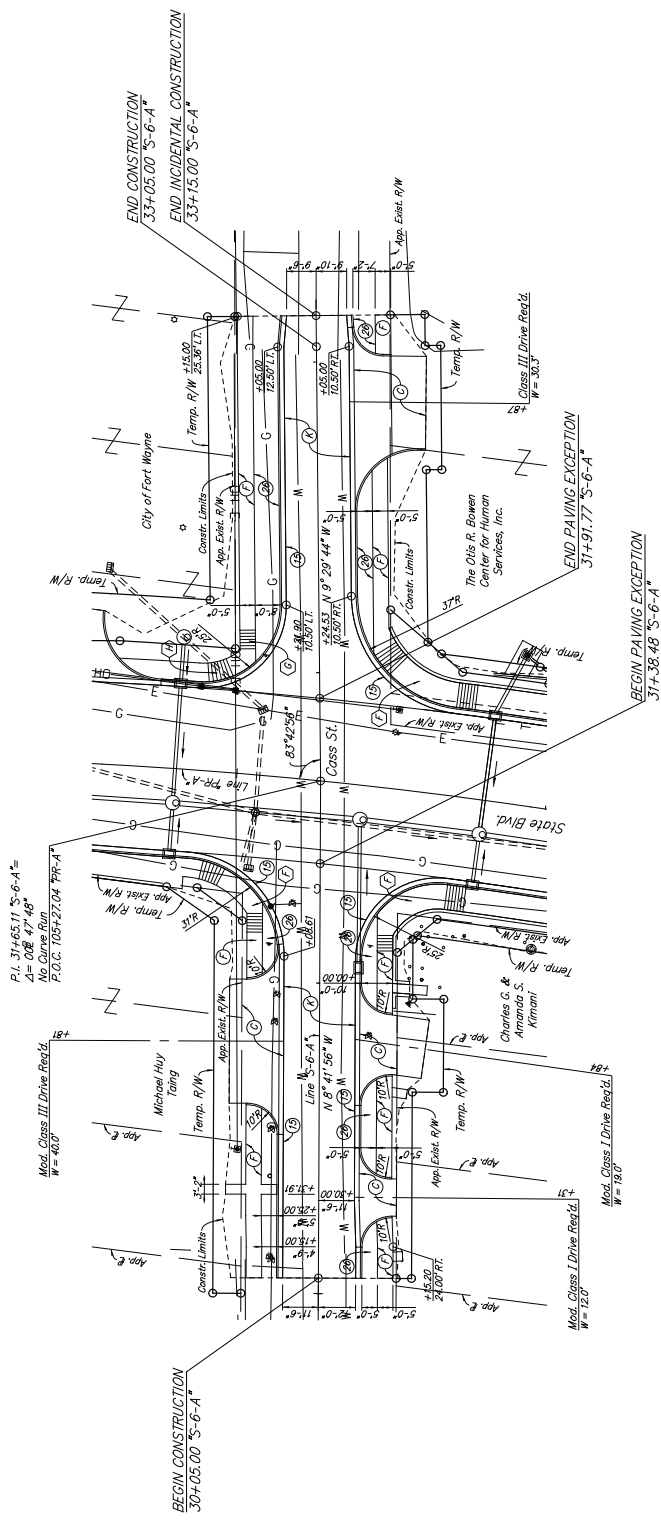
SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PP-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>LEGEND</p> <p>(1) 105 #3yd. HMA Surface, Type 1^a; on 175 #3yd. HMA Intermediate, Type 1^a; on 6" Compacted Aggregate, No. 53, Base</p> <p>(2) 90 #3yd. HMA for Approaches Type 1^a; on 175 #3yd. HMA Intermediate and Base Type 1^a; No. xx Concrete Slabwall, 1^a</p> <p>(3) 90" Cement Concrete Pavement for Driveways</p> <p>(4) Full Depth Pavement (See Typical Sections)</p> <p>(5) Concrete Center Curb, Type 3^b</p> <p>(6) Combined Concrete Curb and Culvert, Modified</p> <p>(7) Retaining Wall</p> <p>(8) Mulched Siding, Type 3^b</p> <p>(9) Sodding, Nursery</p> <p>(10) Center Curb Removal Limits</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS</p> <p>LINE "PR-A"</p>		<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGNED: _____</p> <p>CHECKED: _____</p>	<p>DATE _____</p> <p>DESIGN ENGINEER _____</p> <p>PJT _____</p> <p>CHECKED: _____</p>	<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>SURVEY BOOK _____</p> <p>CONTRACT _____</p> <p>SHEETS _____ of _____</p> <p>PROJECT NO. _____</p> <p>1005154</p>	<p>BRIDGE FILE</p>
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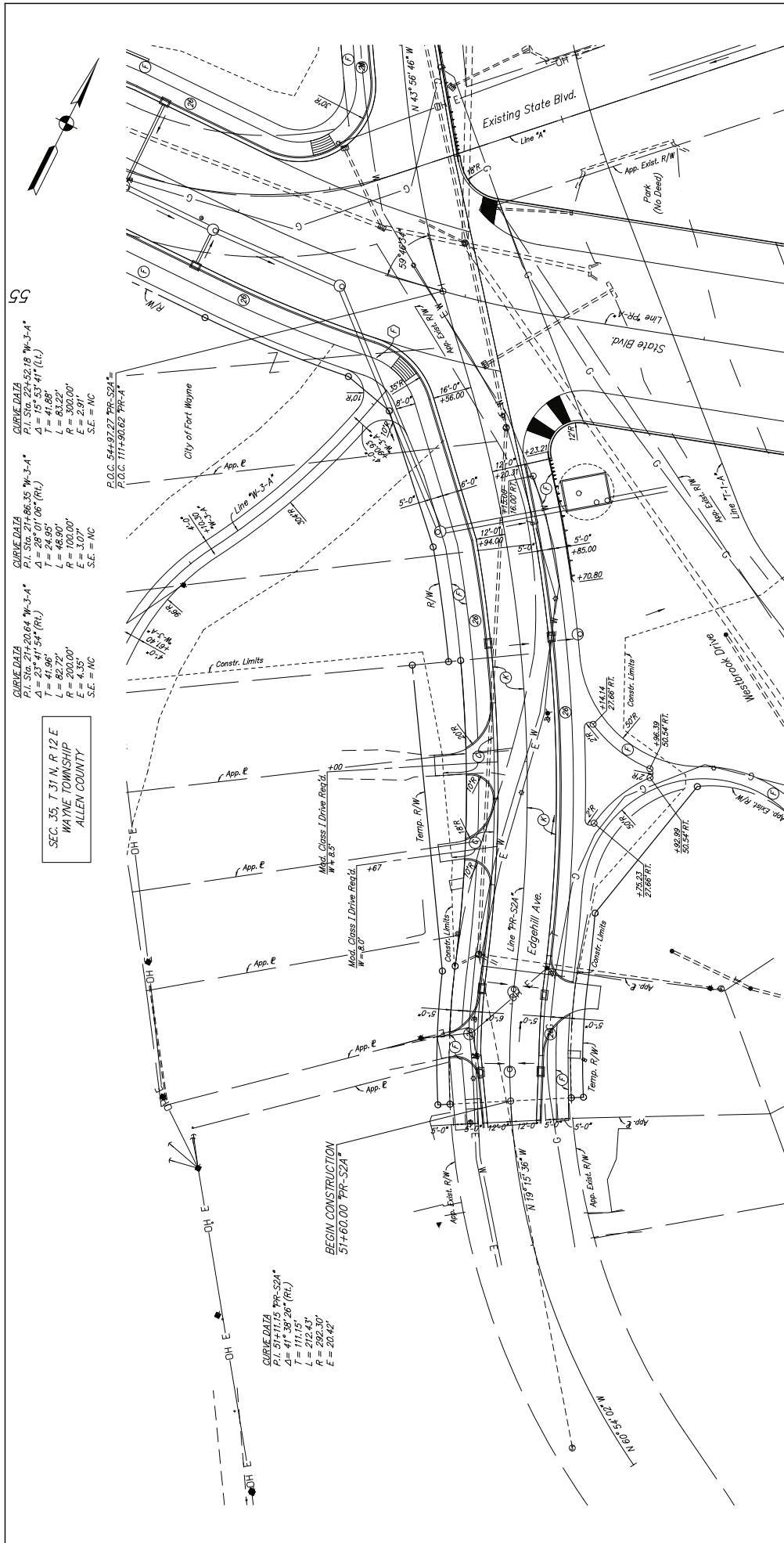
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INDIANA DEPARTMENT OF TRANSPORTATION		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
CONSTRUCTION DETAILS		DESIGNED:		DRAWN:		PIT	
LINE "S-6-A"		CHECKED:		SMC		SMC	
HORIZONTAL SCALE		VERTICAL SCALE		SURVEY BOOK		CONTRACT	
BRIDGE FILE		DESIGNATION NO.		SHEETS		PROJECT NO.	
100514		69		of		100514	

Notes:
All Stationing & R/W Described from Line "S-6-A" Unless Otherwise Noted.

- LEGEND**
- 165 #5yd. HMA Surface, Type 1A; on
 - 275 #5yd. HMA Intermediate, Type 1A; on
 - 6" Compacted Aggregate, No. 53, Base
 - xx" Compacted Aggregate Base, Type 1A; No. xx
 - Concrete Sidewalk, 4"
 - Concrete Curb
 - Combined Concrete Curb and Gutter, Modified
 - Concrete Center Curb, Type 3"
 - Sodding, Nursery
 - Mulched Sanding, Type 1"
 - Retaining Wall
 - Concrete Curb Ramp
 - Concrete Curb Removal Limits

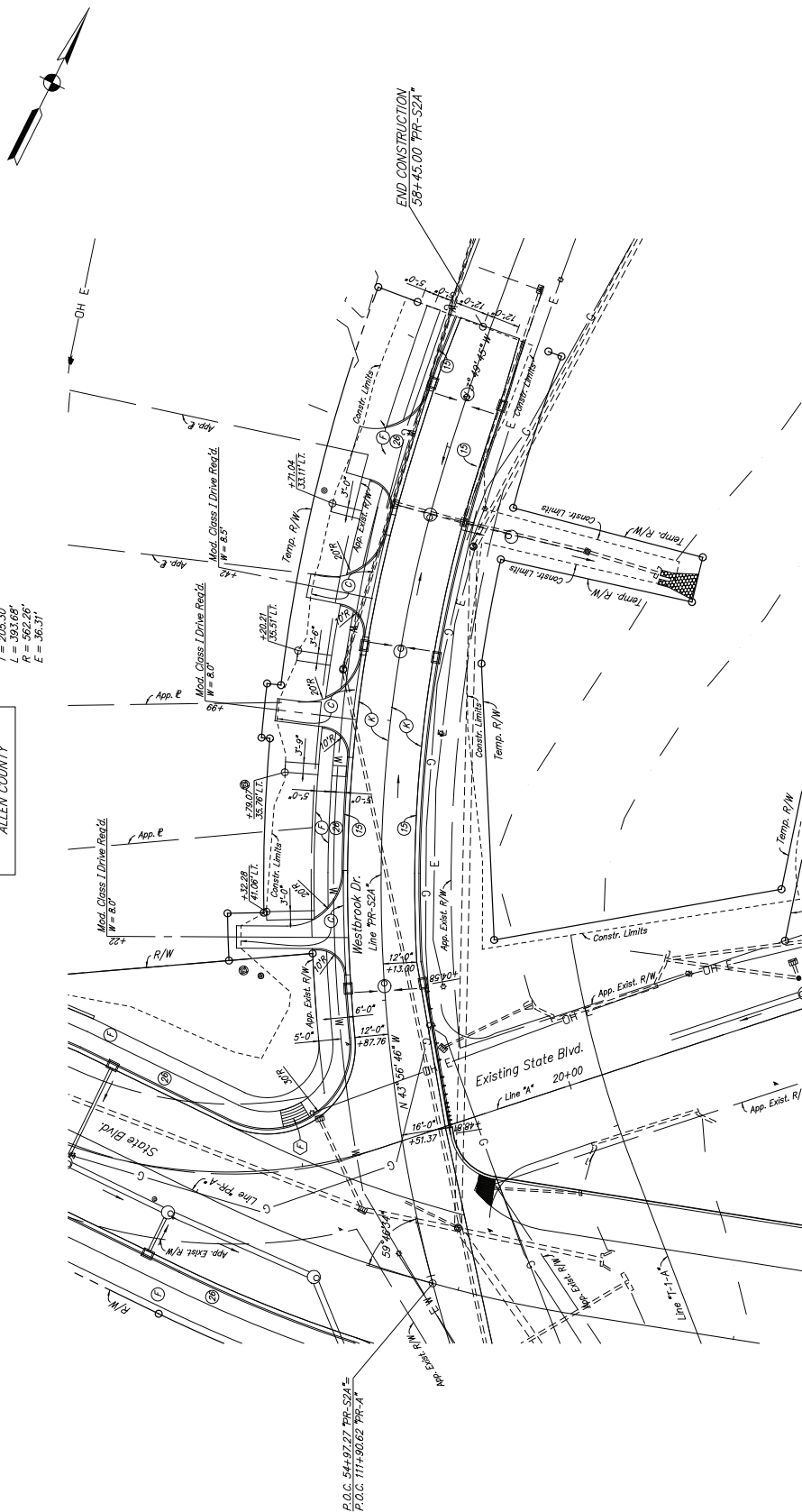


Notes:
 All Stationing & R/W Described from Line PR-S2A Unless Otherwise Noted.
 For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

INDIANA				DEPARTMENT OF TRANSPORTATION				CONSTRUCTION DETAILS			
LINE "PR-S2A"				DESIGNED: ASU				CHECKED: SMC			
DRAWN: CAK				DATE				BRIDGE FILE			
DESIGNED: SMC				DESIGN ENGINEER				DESIGNATION NO.			
FOR APPROVAL				CONTRACT				SHEETS			
SHEET NO.				PROJECT NO.				CONTRACT			
100514				70				100514			

- LEGEND**
- 1. Cement Concrete Pavement for Driveways
 - 2. Cement Concrete Pavement for Driveways
 - 3. Cement Concrete Pavement for Driveways
 - 4. Cement Concrete Pavement for Driveways
 - 5. Cement Concrete Pavement for Driveways
 - 6. Cement Concrete Pavement for Driveways
 - 7. Cement Concrete Pavement for Driveways
 - 8. Cement Concrete Pavement for Driveways
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 - 100. Cement Concrete Pavement for Driveways

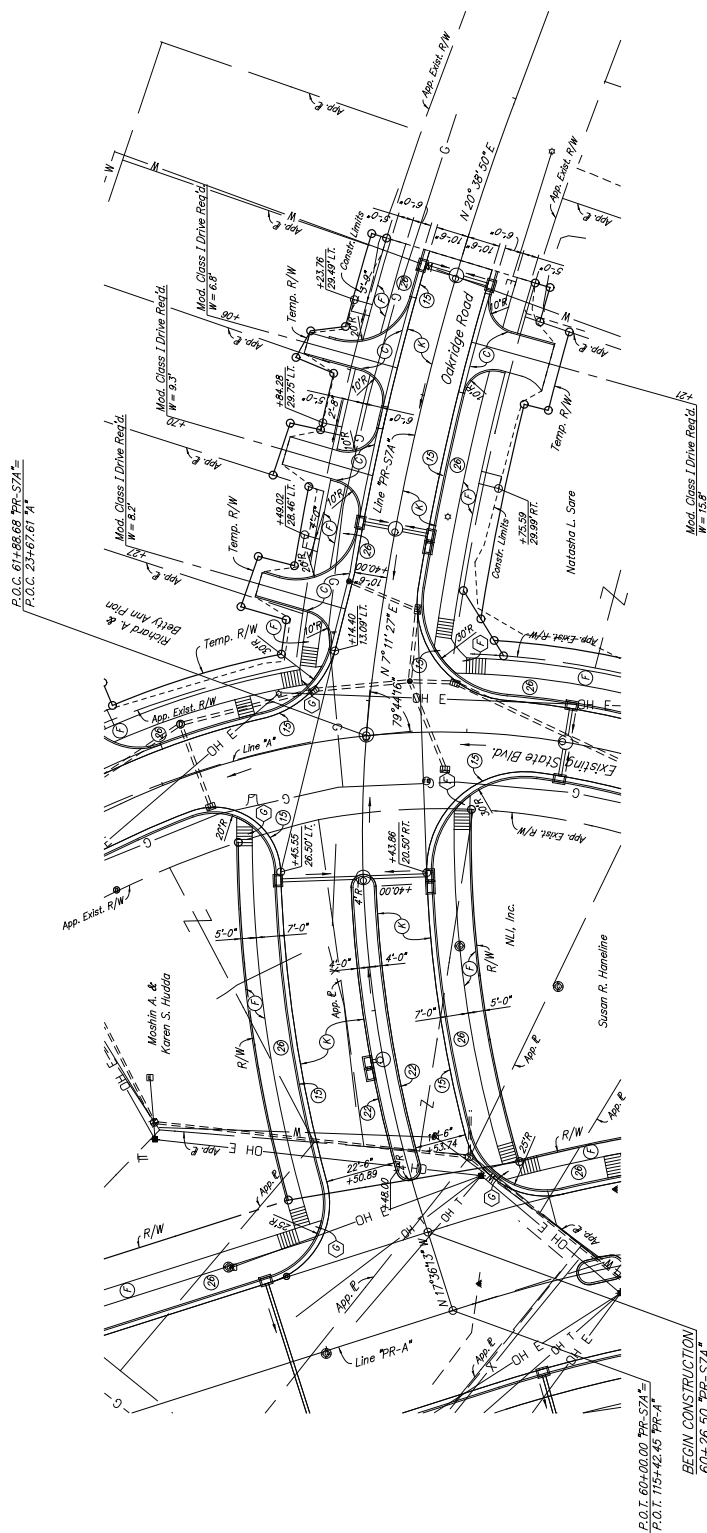
CURVE DATA
P.I. = 56° 76.24' PR-S2A"
Δ = 40° 07' 01" (Rt.)
T = 205.30'
L = 393.68'
R = 562.26'
E = 36.31'



Notes:
All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>LEGEND</p> <p>⑦ 105 #3yd. HMA Surface, Type 1^a; on 175 #3yd. HMA Intermediate, Type 1^a; on 6" Compacted Aggregate, No. 53, Base</p> <p>⑧ 105 #3yd. HMA for Approaches Type 1^a; on 175 #3yd. HMA Intermediate and Base Type 1^a; No. xx Concrete Slabwall, 1^a</p> <p>⑨ 105 #3yd. HMA for Approaches Type 1^a; on 175 #3yd. HMA Intermediate and Base Type 1^a; No. xx Concrete Slabwall, 1^a</p> <p>⑩ Full Depth Pavement (See Typical Sections)</p>		<p>⑪ Concrete Curb</p> <p>⑫ Combined Concrete Curb and Curb, Modified</p> <p>⑬ Concrete Curb Ramp</p> <p>⑭ Concrete Center Curb, Type 2^b</p> <p>⑮ Sodding, Nursery</p>	<p>⑯ Matched Sodding, Type 2^b</p> <p>⑰ Retaining Wall</p> <p>⑱ Concrete Curb Ramp (Type is indicated by Letter inside Hexagon)</p> <p>⑲ Center Curb Removal Limits</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS</p> <p>LINE "PR-S2A"</p>		<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGNED: _____</p> <p>CHECKED: _____</p>	<p>DATE _____</p> <p>DESIGN ENGINEER _____</p> <p>DRAWN: _____</p> <p>CHECKED: _____</p>	<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>SURVEY BOOK _____</p> <p>CONTRACT _____</p>	<p>BRIDGE FILE _____</p> <p>DESIGNATION NO. 100154</p> <p>SHEETS 71 of 100</p> <p>PROJECT NO. 100154</p>
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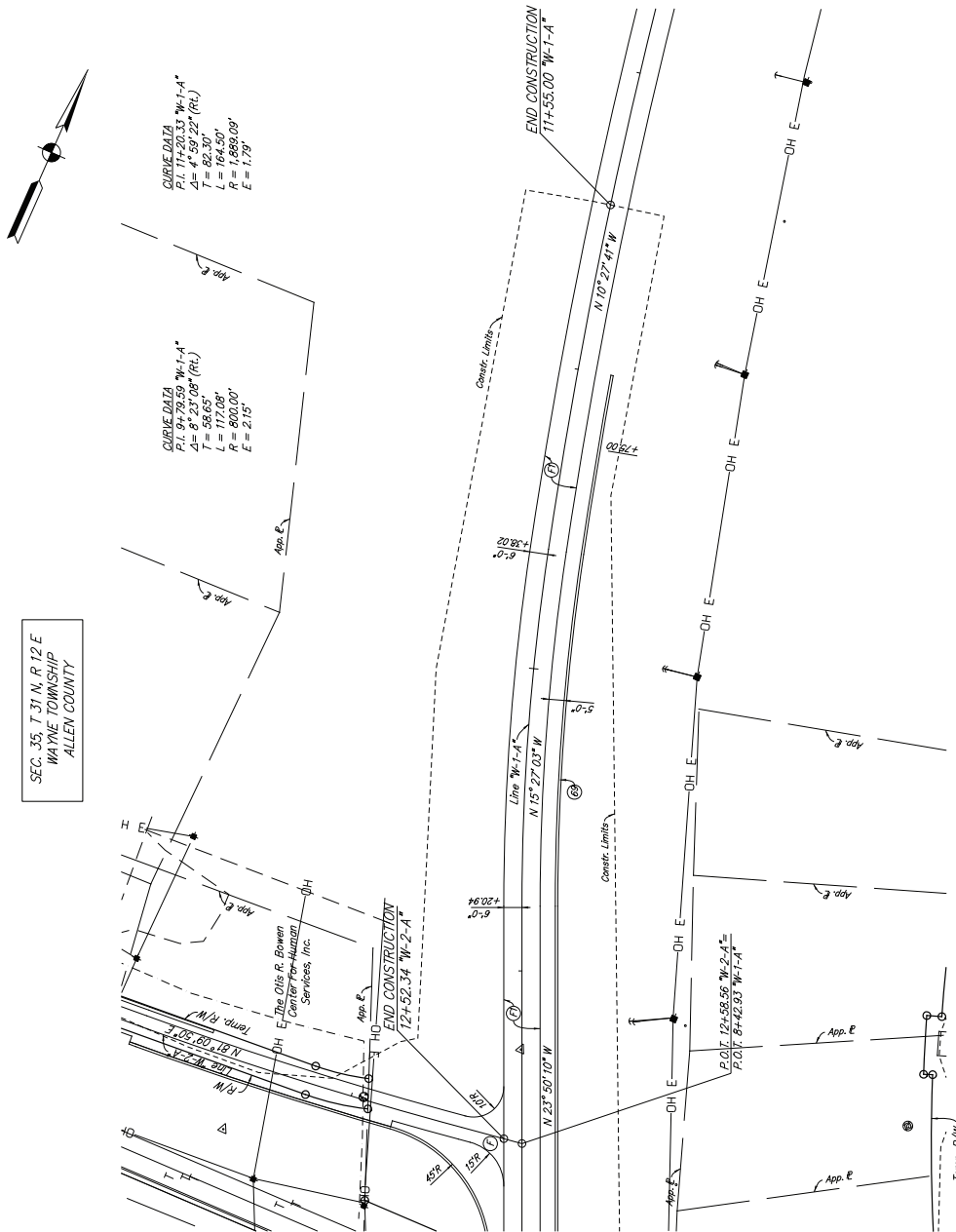
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 $\Delta = 13^{\circ} 27' 23''$ (Rt.)
T = 96.20'
L = 191.51'
R = 815.43'
E = 5.65'

Notes:
All Stationing & R/W Described from Line "PR-S7A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

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SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



INDIANA
DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS
LINE "W-1-A"

RECOMMENDED FOR APPROVAL
DESIGNED: ASU
CHECKED: SMC
DESIGN ENGINEER
DATE
DRAWN: PAT
CHECKED: SMC

BRIDGE FILE
DESIGNATION NO.
100514
SHEETS
75 of 100
PROJECT NO.
100514

CONTRACT
100514

LEGEND

165 #5yd. HMA Surface, Type "A", on 275 #5yd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 33, Base

xx" Compacted Aggregate Base, Type "A", No. xx

Concrete Sidewalk, 4"

Full Depth Pavement (See Typical Sections)

Concrete Curb

Combined Concrete Curb and Gutter, Modified

Concrete Center Curb, Type "B"

Sodding, Nursery

165 #5yd. HMA Surface, Type "A", on 275 #5yd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 33, Base

xx" Compacted Aggregate Base, Type "A", No. xx

Concrete Sidewalk, 4"

Full Depth Pavement (See Typical Sections)

Concrete Curb

Combined Concrete Curb and Gutter, Modified

Concrete Center Curb, Type "B"

Sodding, Nursery

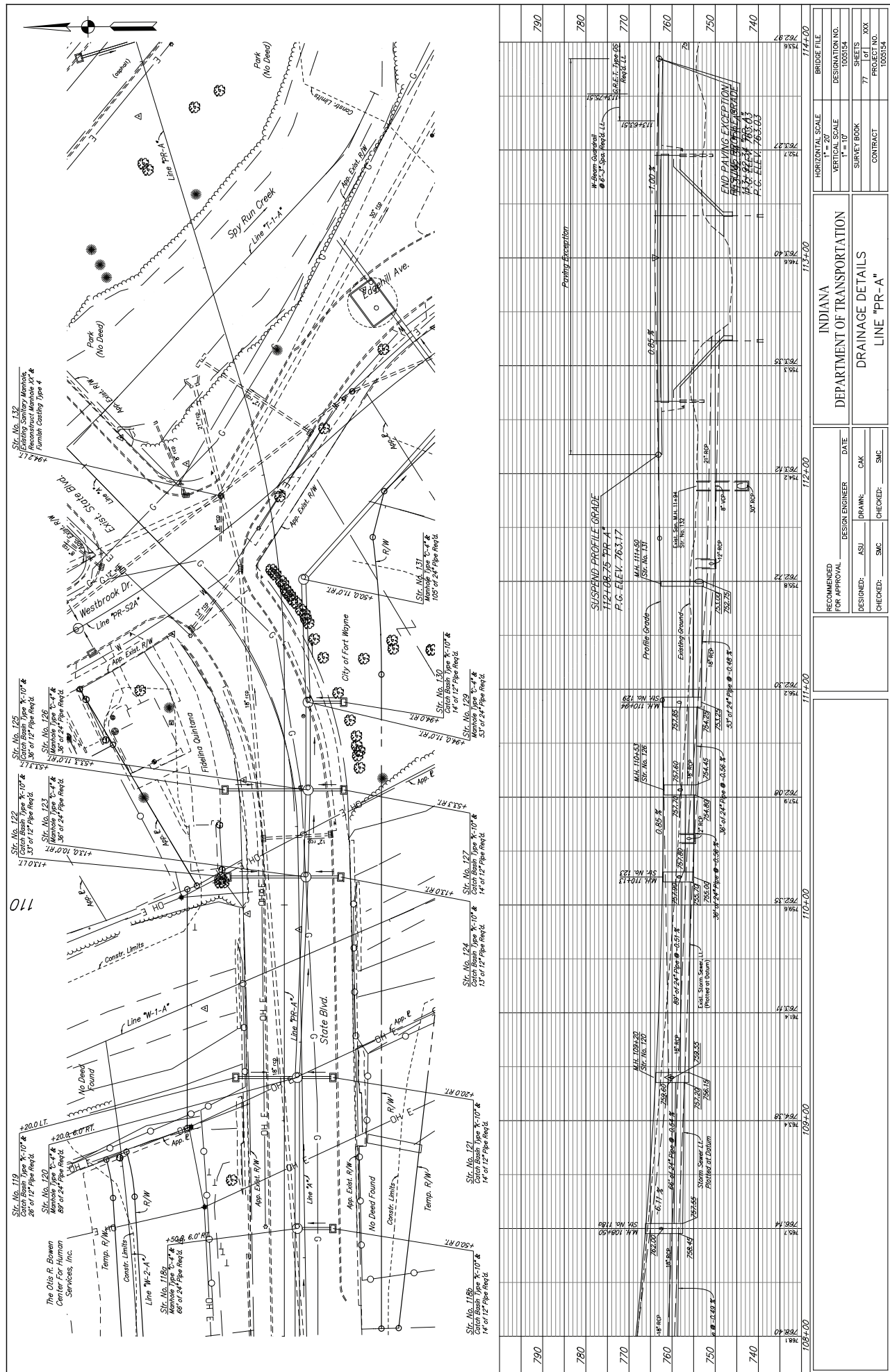
28 Mulched Sanding, Type "V"

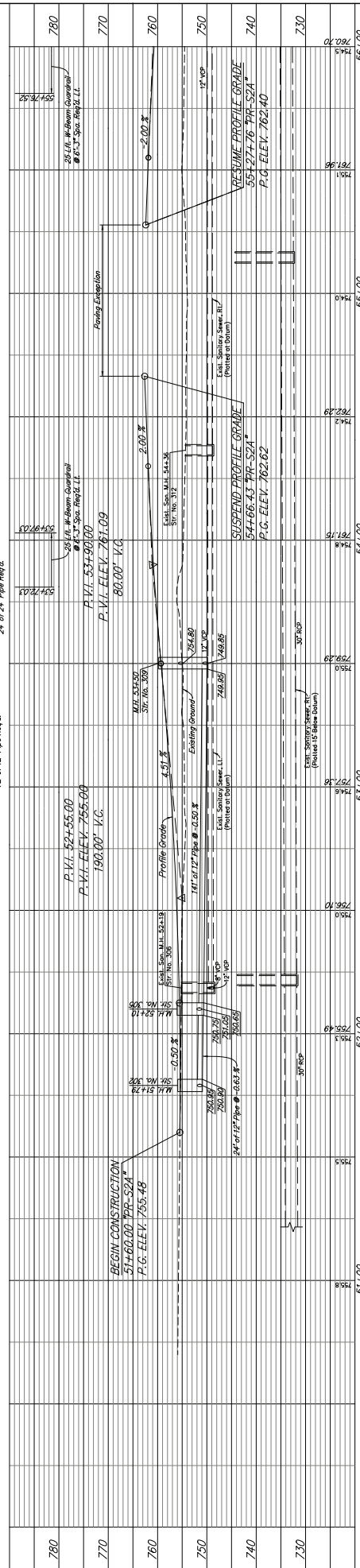
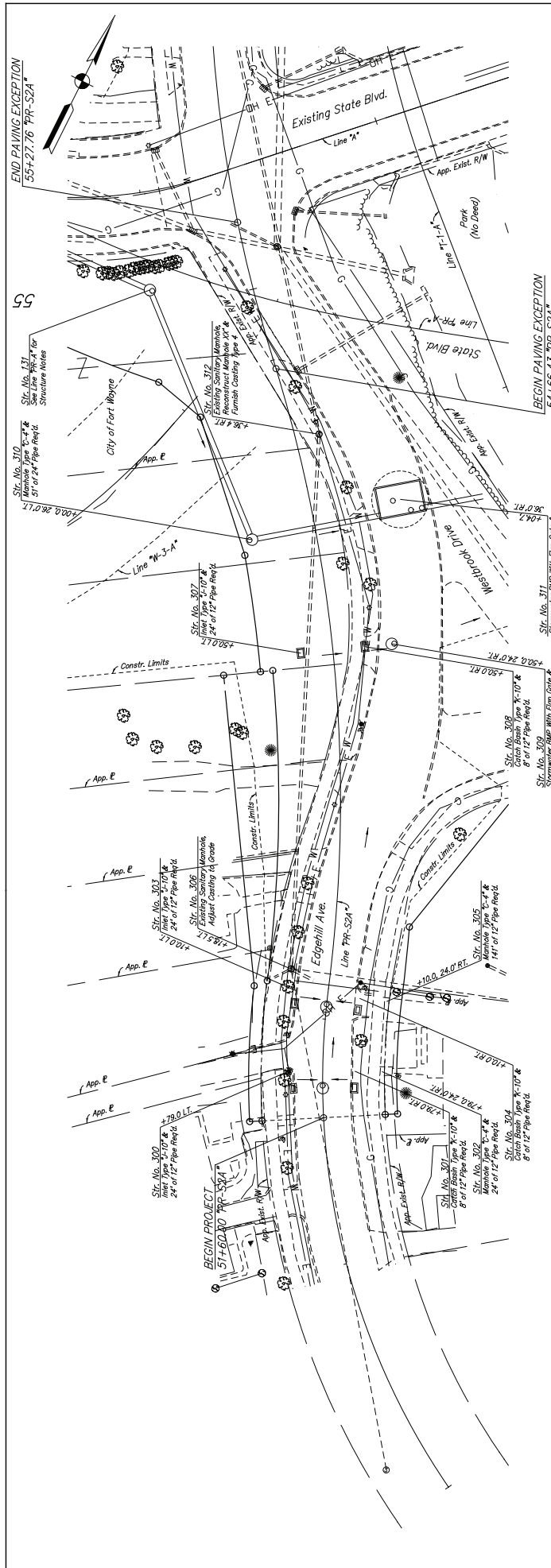
Retaining Wall

Concrete Curb Ramp

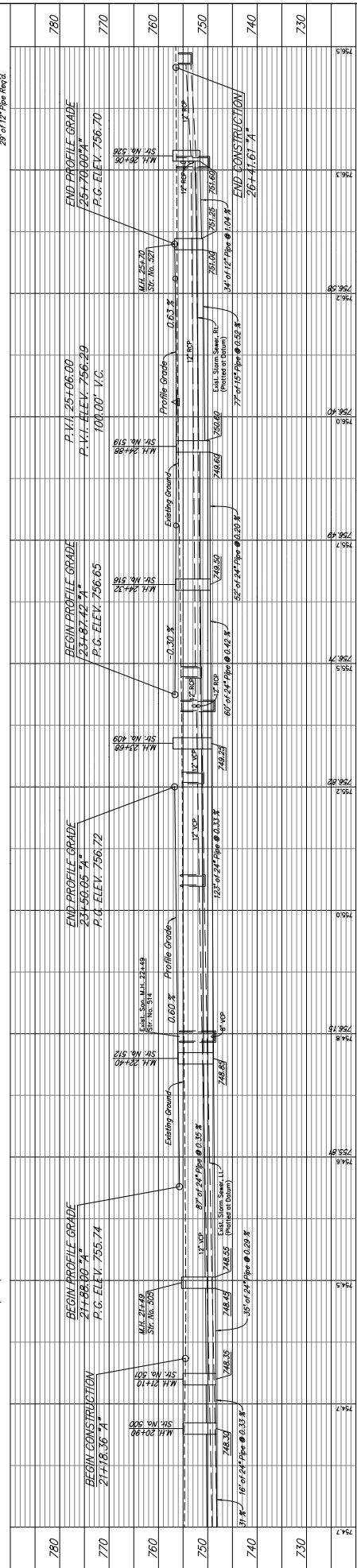
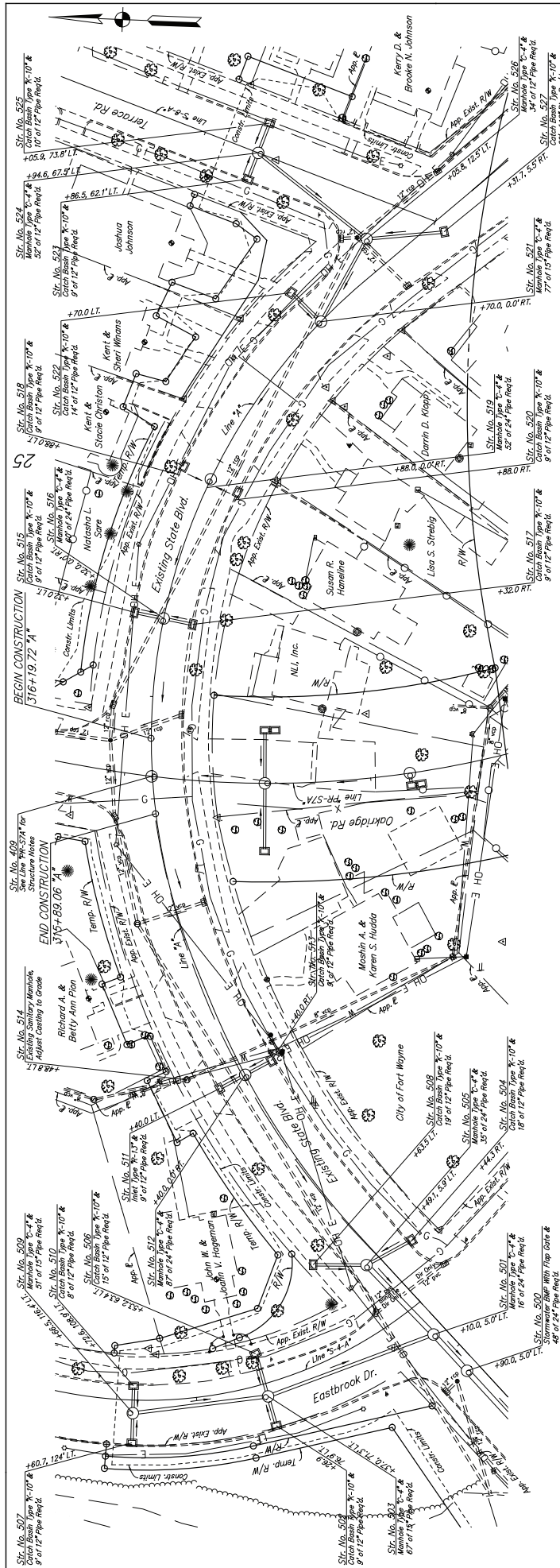
Center Curb Removal Limits

Attachment 3 - 109 of 531

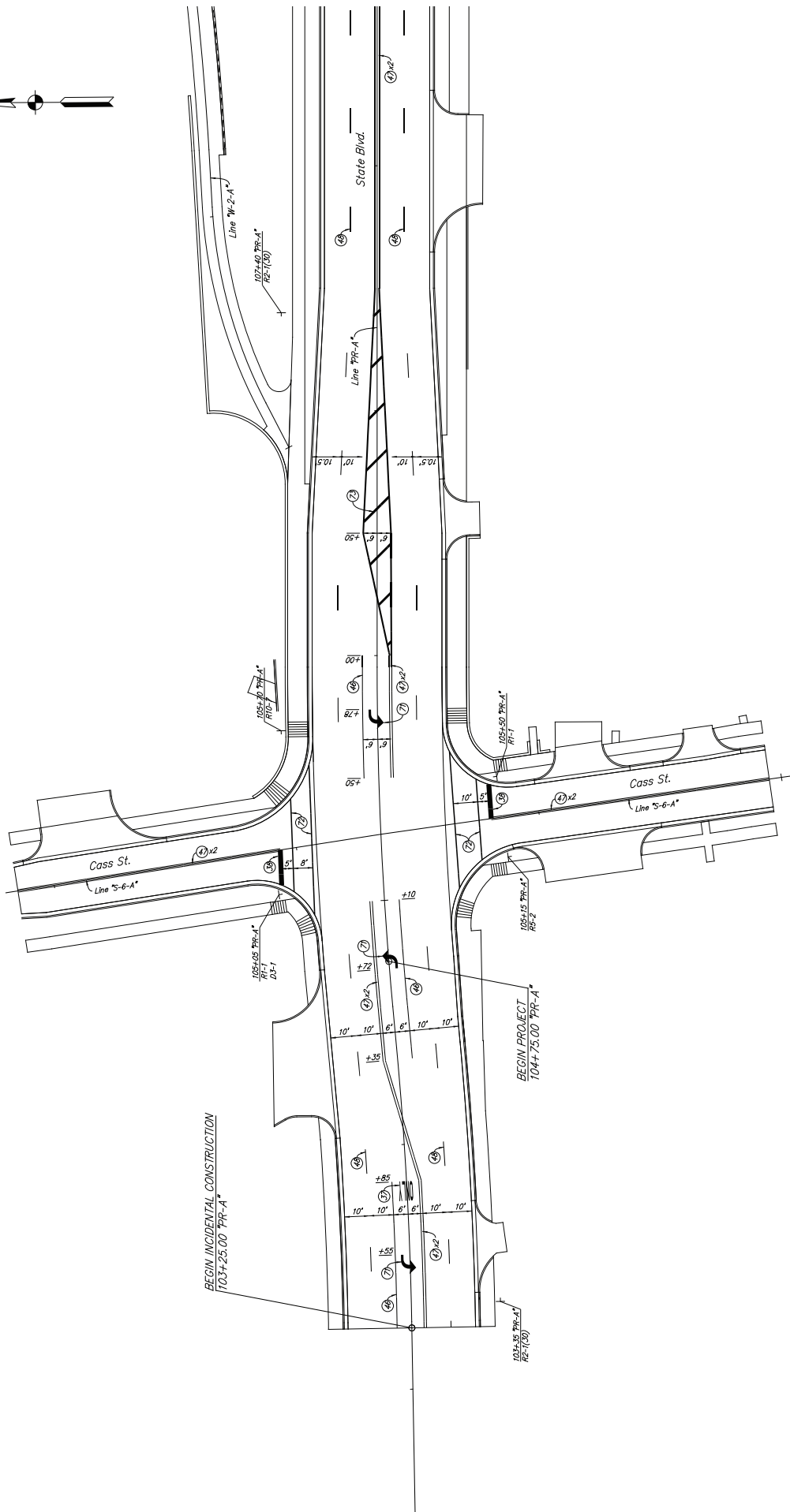




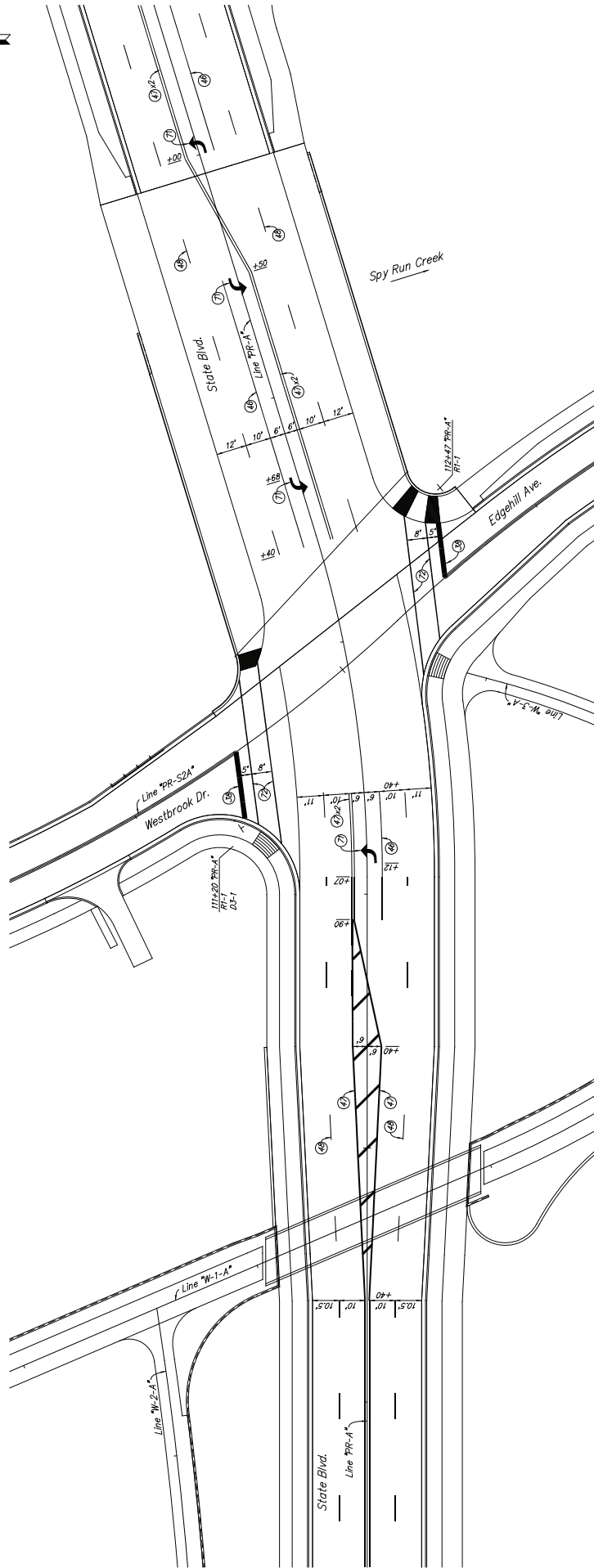
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DESIGNED:	ASU	CHECKED:	SMC	DESIGN ENGINEER:	DATE:	DEPARTMENT OF TRANSPORTATION	BRIDGE FILE
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						LINE "PR-S2A"	100514
							SURVEY BOOK
							79
							SHEETS
							of
							XXX
							PROJECT NO.
							100514



INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
BRIDGE FILE				DESIGN ENGINEER				DRAWN			
HORIZONTAL SCALE				DATE				CAK			
VERTICAL SCALE				ASU				SNC			
DESIGNATION NO.				CHECKED				SNC			
100514				SNC				SNC			
SURVEY BOOK				SNC				SNC			
82				SNC				SNC			
SHEETS				SNC				SNC			
XXX				SNC				SNC			
PROJECT NO.				SNC				SNC			
100514				SNC				SNC			



<p>LEGEND</p> <p>(1) Pavement Message Markings, Thermoplastic, Word (message)</p> <p>(2) Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.</p> <p>(3) Transverse Markings, Thermoplastic, Solid, White, 4 in.</p> <p>(4) Transverse Markings, Thermoplastic, Solid, Yellow, 4 in.</p> <p>(5) Line, Thermoplastic, Solid, Yellow, 4 in.</p> <p>(6) Line, Thermoplastic, Broken, White, 4 in.</p> <p>(7) Line, Thermoplastic, Broken, Yellow, 4 in.</p> <p>(8) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>(9) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p> <p>(10) Pavement Message Markings, Thermoplastic, Lane Indication Arrow</p> <p>(11) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p> <p>(12) Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 12 in.</p>	<p>RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____</p> <p>DESIGNED: _____ ASU _____ DRAWN: _____ PJT _____</p> <p>CHECKED: _____ SMC _____ CHECKED: _____ SMC _____</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>PAVEMENT MARKING & SIGNING DETAILS – LINE "PR-A"</p>	<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>DESIGNATION NO. 1001554</p> <p>BRIDGE FILE _____</p>
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INDIANA
DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING &
SIGNING DETAILS - LINE "PR-A"

RECOMMENDED FOR APPROVAL
DESIGNED: ASU
CHECKED: SMC

DESIGN ENGINEER
DATE
DRAWN: PJT
CHECKED: SMC

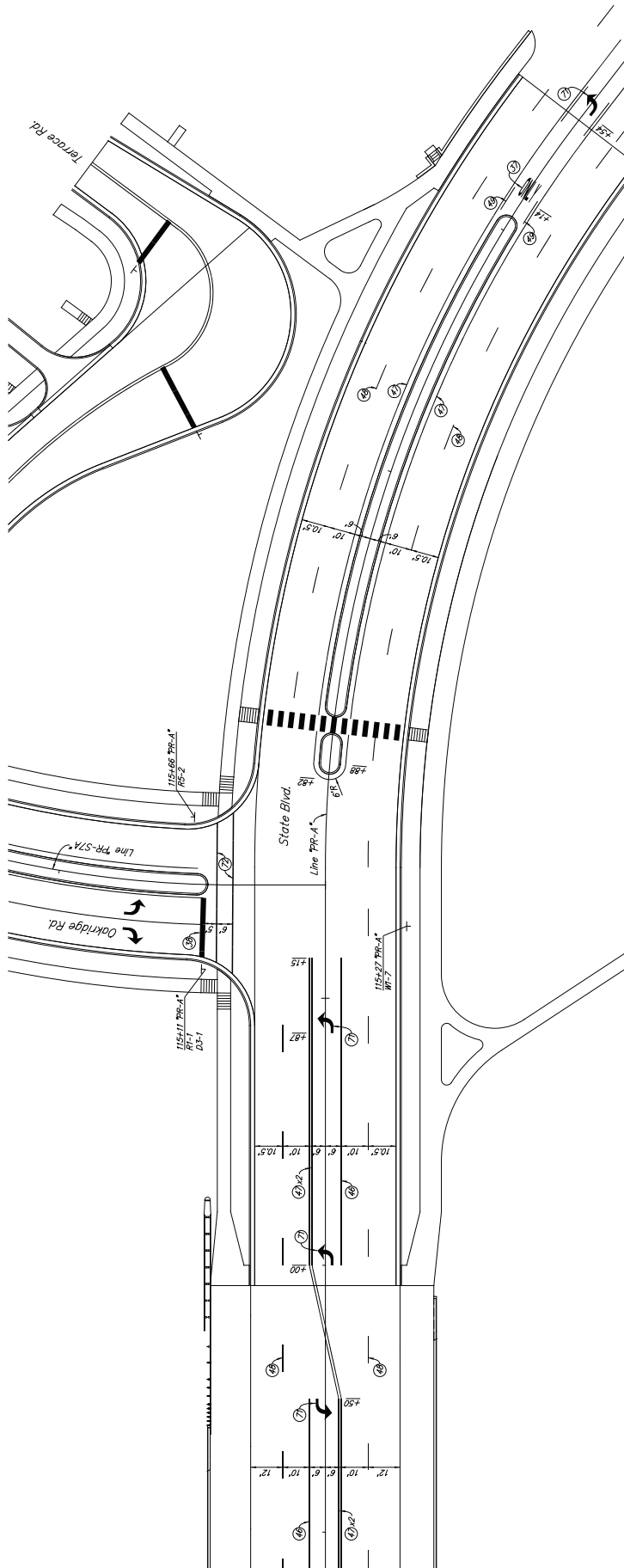
HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	SHEETS
CONTRACT	PROJECT NO.
	100514

LEGEND

① Pavement Message Markings, Thermoplastic, Word (message)	④ Line, Thermoplastic, Broken, White, 4 in.	⑦ Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.
② Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	⑤ Line, Thermoplastic, Broken, Yellow, 4 in.	⑧ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
③ Line, Thermoplastic, Solid, White, 4 in.	⑥ Pavement Message Markings, Thermoplastic, Lane Indication Arrow	⑨ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
④ Line, Thermoplastic, Solid, Yellow, 4 in.	⑦ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	

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115



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	100514
		CONTRACT	SHEETS
			85 of 100
			PROJECT NO.
			100514

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:	ASU	DRAWN:	PJT
CHECKED:	SMC	CHECKED:	SMC

LEGEND

① Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.

② Transverse Markings, Thermoplastic, Broken, White, 4 in.

③ Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

④ Line, Thermoplastic, Solid, White, 4 in.

⑤ Line, Thermoplastic, Solid, Yellow, 4 in.

⑥ Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.

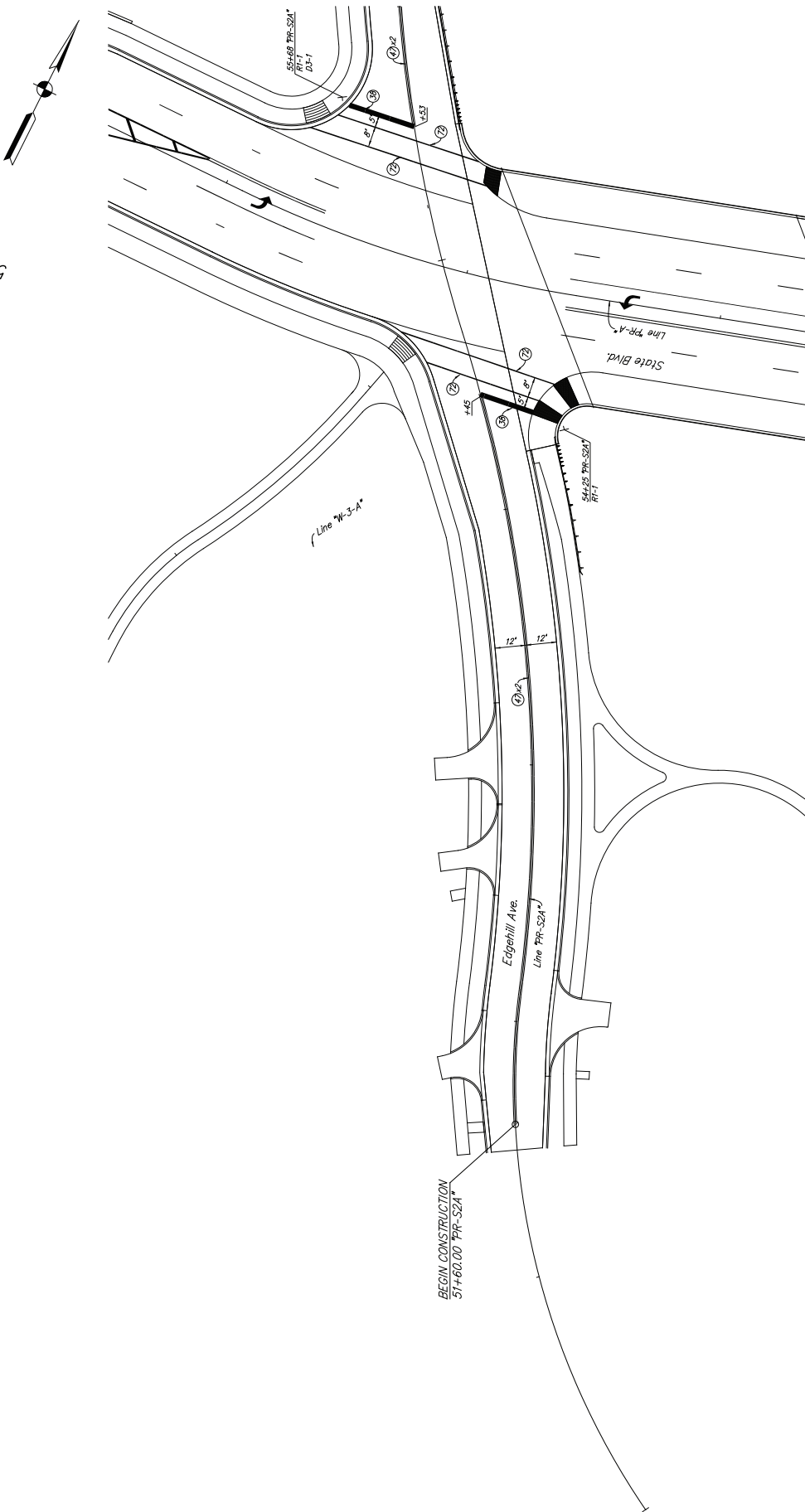
⑦ Pavement Message Markings, Thermoplastic, Lane Indication Arrow

⑧ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

⑨ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

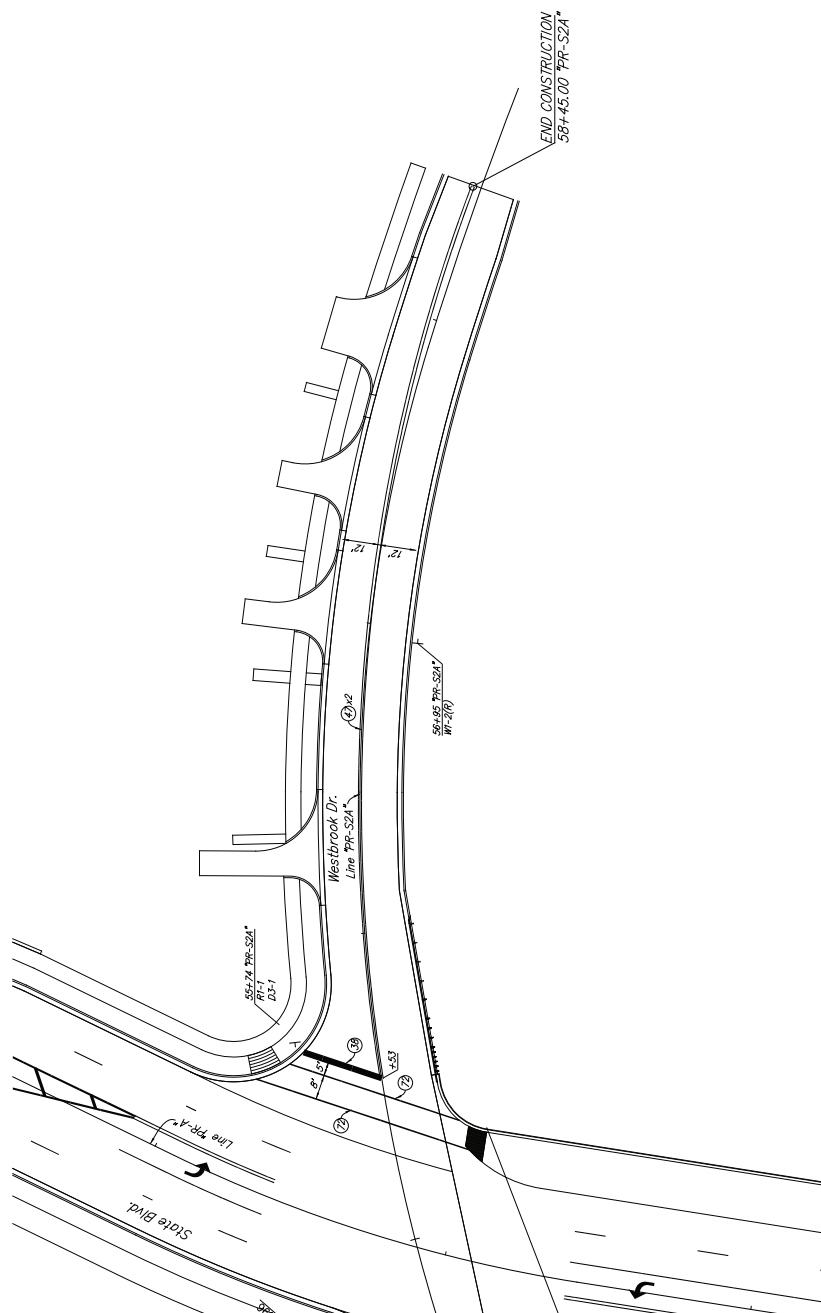
⑩ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

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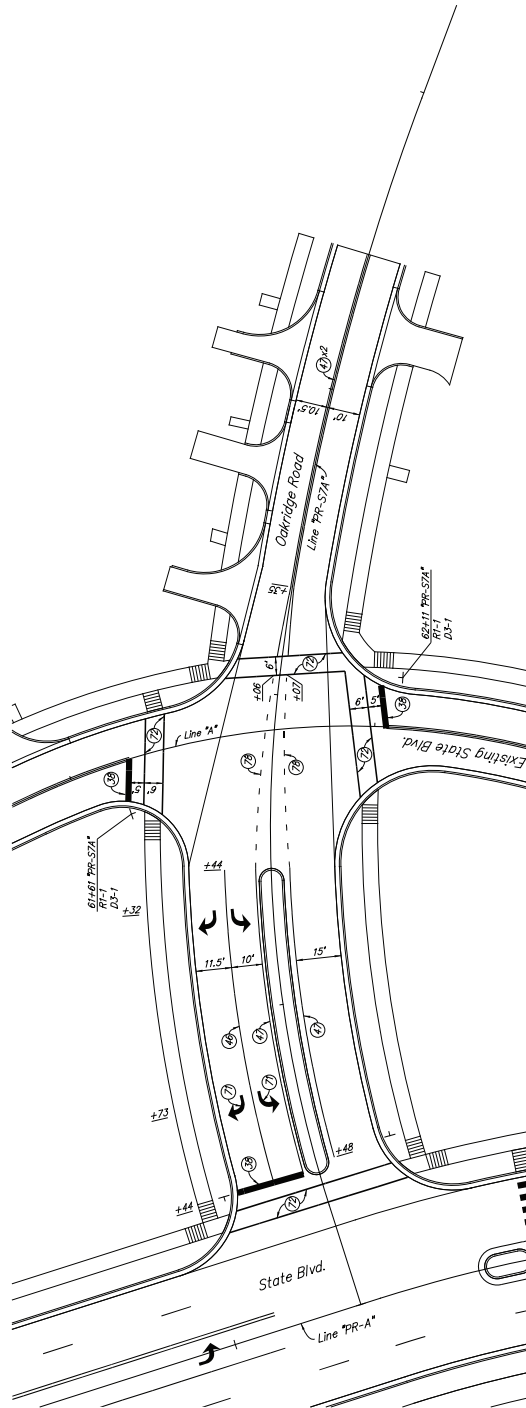


RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
		ASU	DRAWN: CAK	CHECKED: SMC	SMC
DESIGNED:		ASU	DRAWN: CAK	CHECKED: SMC	SMC
CHECKED:		SMC	DRAWN: CAK	CHECKED: SMC	SMC
INDIANA DEPARTMENT OF TRANSPORTATION					
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-S2A"					
HORIZONTAL SCALE			BRIDGE FILE		
VERTICAL SCALE			DESIGNATION NO.		
SURVEY BOOK			SHEETS		
CONTRACT			PROJECT NO.		
			100514		

- LEGEND**
- ① Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.
 - ② Line, Thermoplastic, Broken, White, 4 in.
 - ③ Line, Thermoplastic, Broken, Yellow, 4 in.
 - ④ Line, Thermoplastic, Broken, Yellow, 4 in.
 - ⑤ Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.
 - ⑥ Line, Thermoplastic, Solid, White, 4 in.
 - ⑦ Line, Thermoplastic, Solid, Yellow, 4 in.
 - ⑧ Line, Thermoplastic, Solid, Yellow, 4 in.
 - ⑨ Pavement Message Markings, Thermoplastic, Lane Indication Arrow, White, 6 in.
 - ⑩ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - ⑪ Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.
 - ⑫ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
 - ⑬ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)



<p>⑦ Pavement Message Markings, Thermoplastic, Word (message)</p> <p>⑧ Transverse Message Markings, Thermoplastic, Stop Line, White, 24 in.</p> <p>⑨ Line, Thermoplastic, Solid, White, 4 in.</p> <p>⑩ Line, Thermoplastic, Solid, Yellow, 4 in.</p>	<p>⑪ Pavement Message Markings, Thermoplastic, Broken, White, 4 in.</p> <p>⑫ Transverse Message Markings, Thermoplastic, Broken, Yellow, 4 in.</p> <p>⑬ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>⑭ Pavement Message Markings, Thermoplastic, Lane Indication Arrow</p> <p>⑮ Transverse Message Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p>	<p>⑯ Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.</p> <p>⑰ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>⑱ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p>	<p>INDIANA DEPARTMENT OF TRANSPORTATION</p>	<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p>	<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE</p> <p>DESIGNATION NO. 100514</p>	<p>BRIDGE FILE</p>
			<p>PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-S2A"</p>	<p>DESIGNED: ASU</p> <p>DRAWN: PJT</p> <p>CHECKED: SMC</p> <p>CHECKED: SMC</p>	<p>SURVEY BOOK</p> <p>SHEETS 87 of 101</p> <p>CONTRACT</p> <p>NO. 00551</p>	

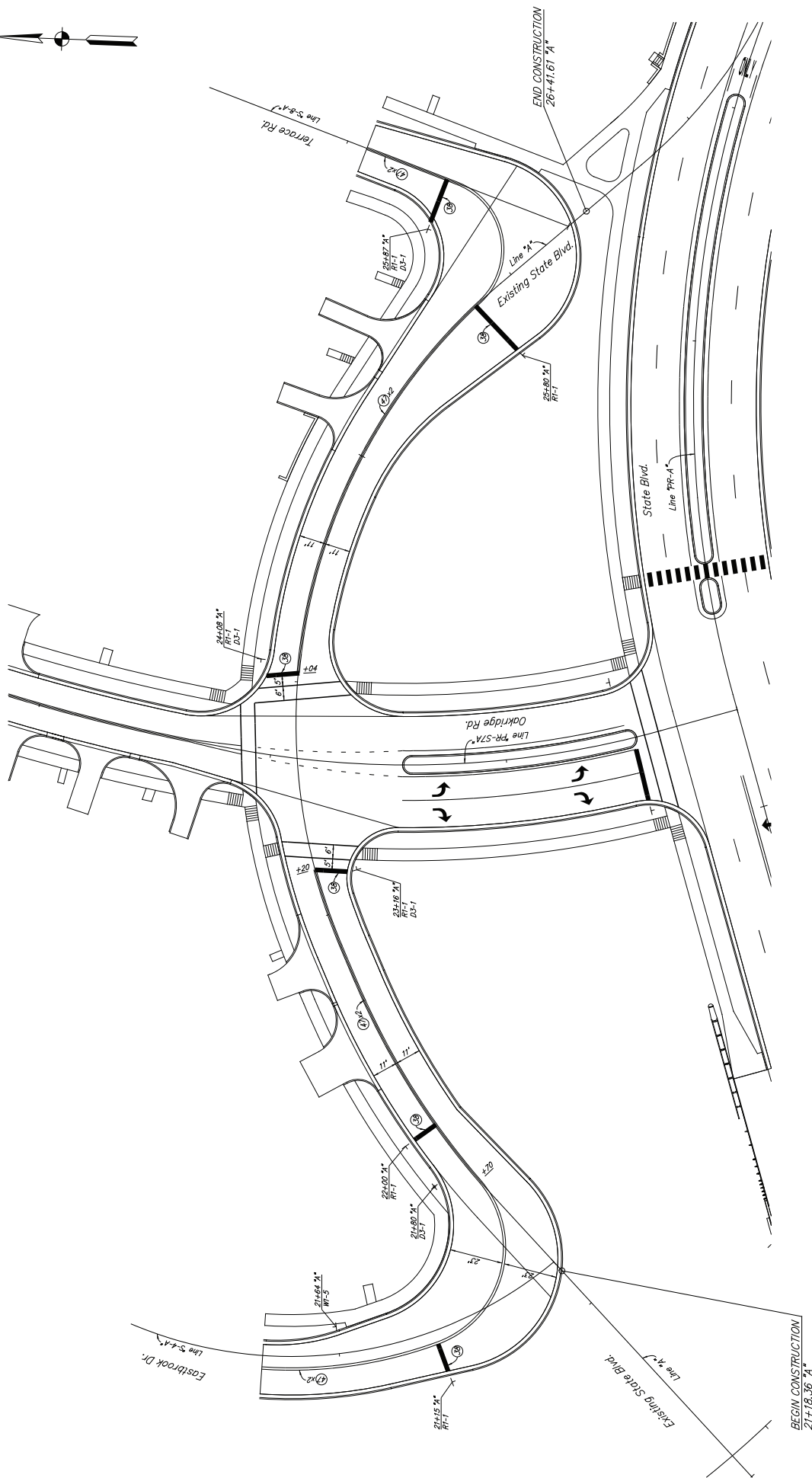


		INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DESIGNATION NO. 100514	
DESIGNED: ASU		DRAWN: PJT		SURVEY BOOK 88 of 100	
CHECKED: SMC		CHECKED: SMC		CONTRACT PROJECT NO. 100514	
				SIGNING DETAILS - LINE "PR-S7A"	

LEGEND

17	Pavement Message Markings, Thermoplastic, Word (message)	64	Line, Thermoplastic, Broken, White, 4 in.
18	Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.	65	Line, Thermoplastic, Broken, Yellow, 4 in.
19	Line, Thermoplastic, Solid, White, 4 in.	66	Pavement Message Markings, Thermoplastic, Lane Indication Arrow
20	Line, Thermoplastic, Solid, Yellow, 4 in.	67	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
21	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)	68	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
22	Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	69	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
23	Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 12 in.	70	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
24	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)	71	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
25	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)	72	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

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porties



<p>① Pavement Message Markings, Thermoplastic, Word (message)</p> <p>② Line, Thermoplastic, Broken, White, 4 in.</p> <p>③ Line, Thermoplastic, Broken, Yellow, 4 in.</p> <p>④ Line, Thermoplastic, Broken, Yellow, 4 in.</p> <p>⑤ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>⑥ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p> <p>⑦ Pavement Message Markings, Thermoplastic, Lane Indication Arrow</p> <p>⑧ Pavement Message Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p>	<p>⑨ Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.</p> <p>⑩ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>⑪ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>PAVEMENT MARKING & SIGNING DETAILS - LINE "A"</p>	<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGNED: _____</p> <p>CHECKED: _____</p>	<p>DATE _____</p> <p>DESIGN ENGINEER _____</p> <p>ASU _____</p> <p>SMC _____</p> <p>DRAWN: _____</p> <p>CHECKED: _____</p>	<p>SURVEY BOOK _____</p> <p>CONTRACT _____</p> <p>SHEETS _____</p> <p>89 _____</p> <p>PROJECT NO. _____</p> <p>1005154 _____</p>	<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>BRIDGE FILE _____</p>
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STATE BLVD., EAST VIEW TOWARD SPY RUN AVE.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., WEST VIEW TOWARD PROPOSED STATE AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., SOUTHWEST VIEW OF THE NEW EXTENSION AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., NORTH VIEW TOWARD HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., EAST VIEW TOWARD NEW BRIDGE OVER SPY RUN CREEK



AMERICAN
STRUCTUREPOINT
INC.

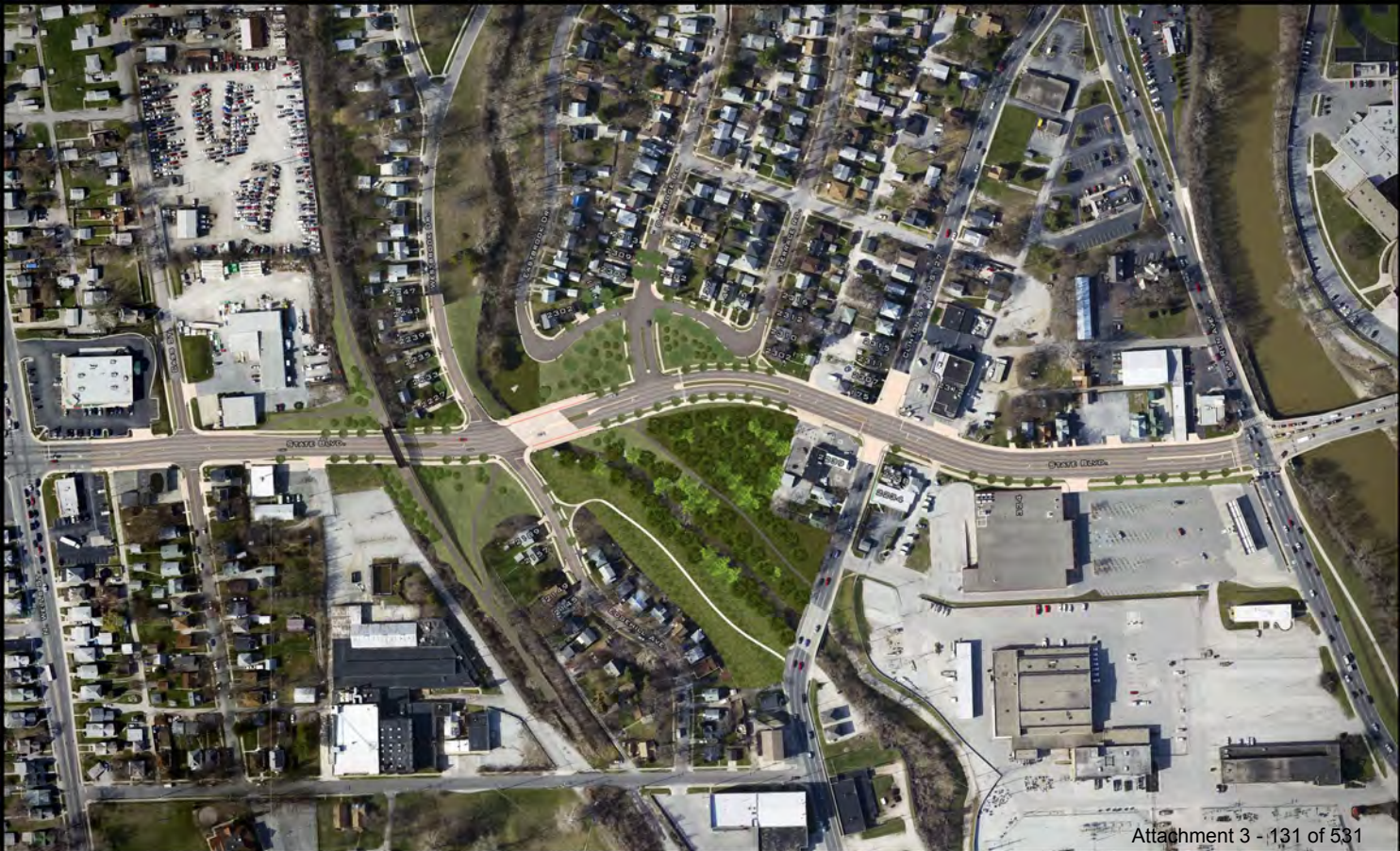


STATE BLVD., WEST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE



STATE BLVD., EAST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE

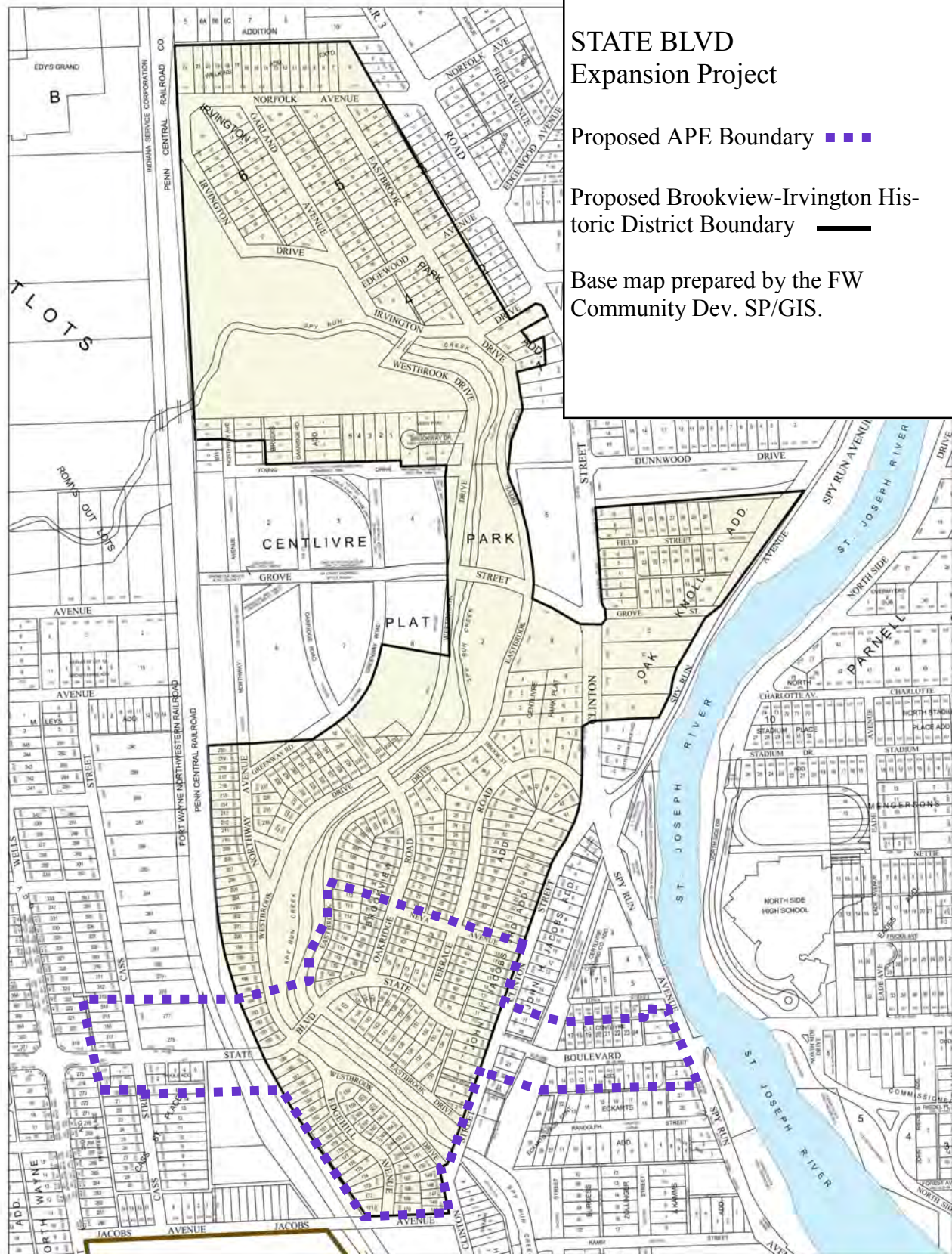




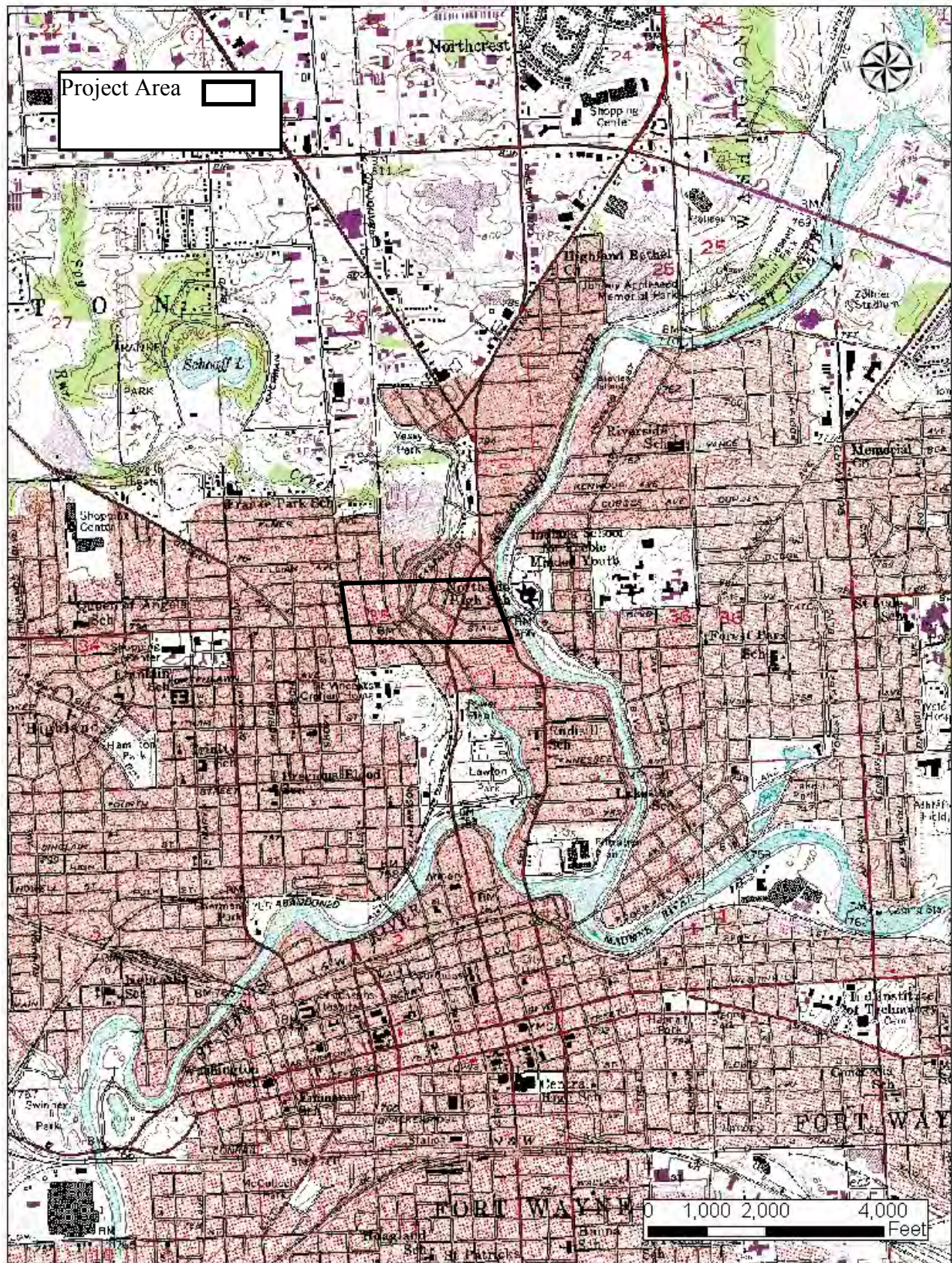
Attachment 3 - 131 of 531

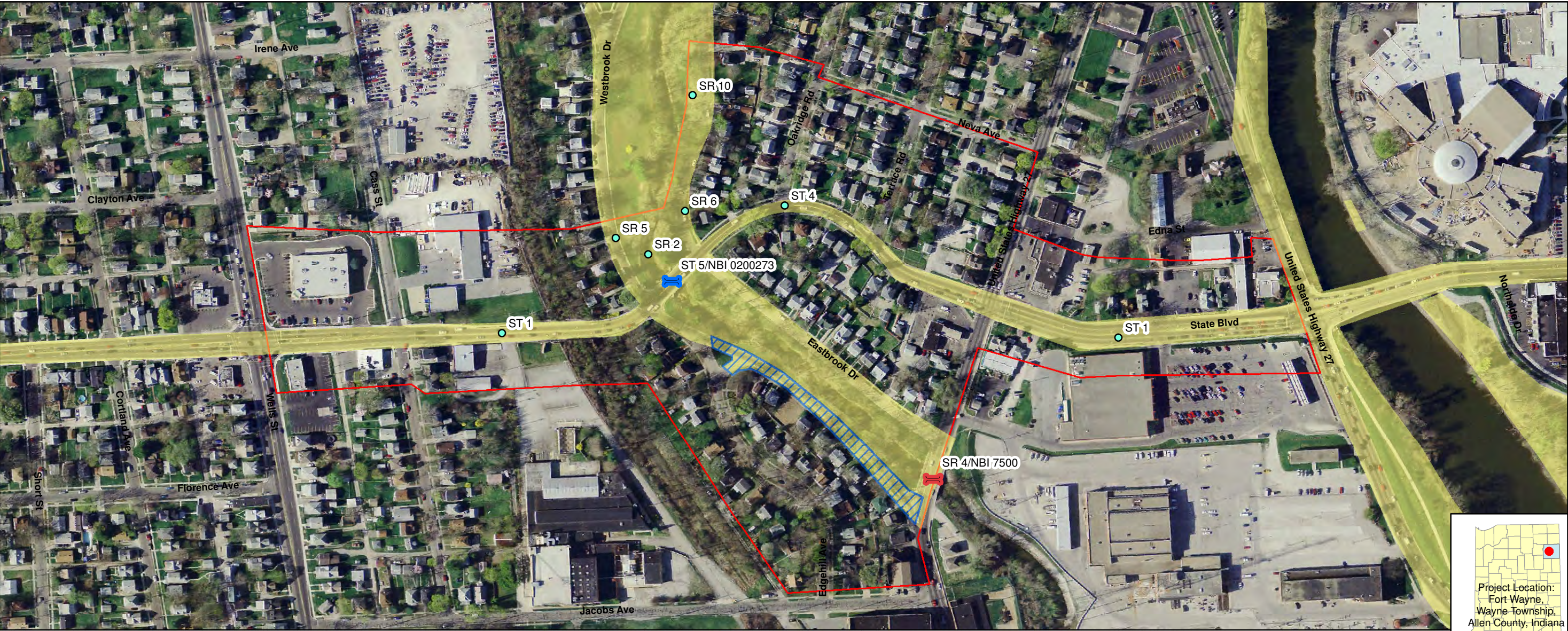


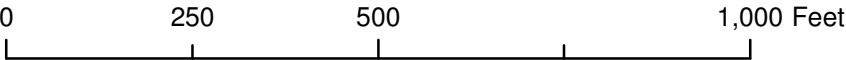
APPENDIX B. Maps



USGS Map














0 250 500 1,000 Feet

Legend

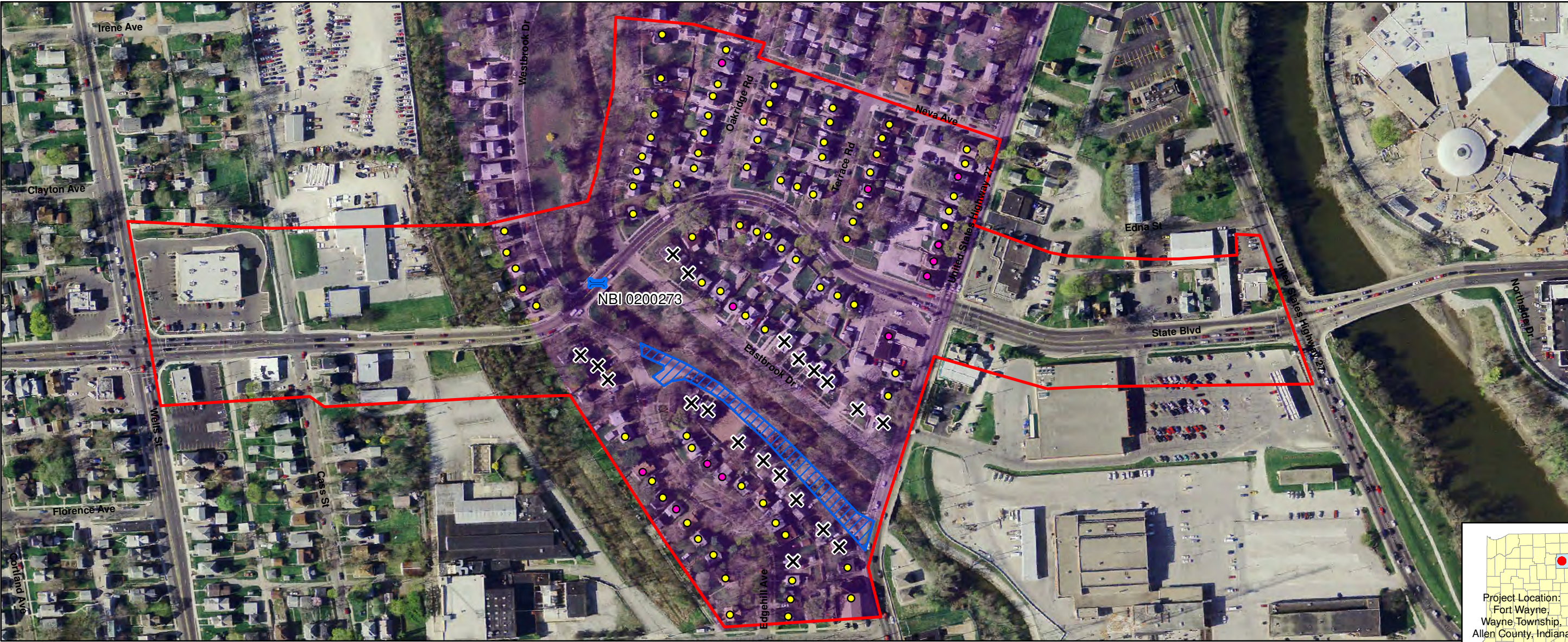
 APE	 Contributing to FWPB	 Road Pavement Removed
	 Non-Contributing Bridge (FWPB)	 FWPB Boundary
	 Determined NR-eligible Bridge	

Project Location:
Fort Wayne,
Wayne Township,
Allen County, Indiana










Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Attachment 3 - 135 of 531



Legend

 APE	 Road Pavement Removed	 Contributing to BIHD
 BIHD	 Determined NR-eligible Bridge	 Non-Contributing to BIHD
		 No Longer Extant



Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

APPENDIX C. Consulting Parties

Individuals or Groups Invited to Join Section 106 Consultation

Indiana State Historic Preservation Officer (SHPO)	ARCH, Inc.
Indiana Department of Transportation (INDOT)	Fort Wayne Historic Preservation Review Board
INDOT—Fort Wayne District,	John Shoaff, Fort Wayne city council member
City of Fort Wayne Engineer	Friends of the Parks of Allen County
Historic Landmarks Foundation of Indiana (now Indiana Landmarks)	Brookview Neighborhood Association
Allen County Historian	Dr. James L. Cooper
Allen County—Fort Wayne Historical Society	Paul Brandenburg, Historic Spans Taskforce

Individuals or Groups Accepting the Invitation to Join Section 106 Consultation, Requesting Consulting Party Status, or Commenting on Project

Indiana SHPO	
City of Fort Wayne	Indiana Historic Spans Taskforce
Allen County Historian	Irvington Park Neighborhood Association
Friends of the Parks of Allen County	Charley Shirmeyer, Northside Galleries
Indiana Landmarks—Northern Regional Office	Albert Cohan, Westbrook 5, LLC
Fort Wayne Historic Preservation Commission	Thomas Niezer, Barret & McNagny, LLP
ARCH, Inc.	Ronald Ross, Martin Riley Architects and Engineers
Brookview Neighborhood Association	Dan Ernst, Earth Source, Inc.
	Jan Dailey, State Boulevard Resident

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY

CITY OF FORT WAYNE

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON

SHAN GUNAWARDENA

ADDRESS

ONE EAST MAIN ST Rm. 750

FORT WAYNE IN 46802

PHONE (260) 427-6169

FAX (260) 427-1269

E-MAIL

shan.gunawardena@

DATE 06/01/09

ci.ftwayne.in.us

Section 106 Consultation

PROJECT ID

State Boulevard Reconstruction

AGENCY

Friends of the Parks of Allen Co

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON

Julie Donnell

ADDRESS

3604 South Washington
Fort Wayne IN
46802

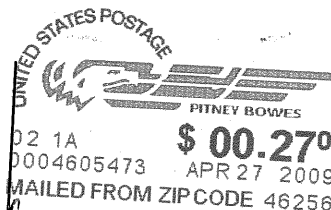
PHONE 260-932-7178 FAX

E-MAIL

juliemarie57@

DATE 5/22/09

earthlink.net



Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

REC'D MAY 28 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY _____

WILL PARTICIPATE ☐

WILL NOT PARTICIPATE ☒

CONTACT PERSON Tom Castaldi

ADDRESS 13707 Black Hollow Ct
FT Wayne
IND 46814

PHONE _____

FAX _____

E-MAIL _____

DATE 3-27-9

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Historic Landmarks Foundation of Indiana

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON Tom Zeiger

ADDRESS 402 W. Washington
South Bend, IN
46601

PHONE 574-232-4534

FAX _____

574-232-5549

E-MAIL tzeiger@

DATE 4-13-09

historiclandmarks.org

Northern
office

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Ft. Wayne Historic Preservation Commission

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON DON ORBAN

ADDRESS One E. Main St. Rm 800
FORT WAYNE, IN 46802

PHONE 260-427-2160 FAX 260-427-1132

E-MAIL don.orban@cityoffortwayne.org DATE 4-2-09

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY ARCH, Inc.

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON Angie Quinn + Michael Galbraith

ADDRESS ARCH, Inc.
437 E. Berry St. Suite 204
Fort Wayne, IN 46802

PHONE 260-426-5117 FAX Same

E-MAIL Aquinn@archfw.org DATE 3/26/09

mgalbraith@archfw.org

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Brookview Neighborhood Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Michelle Briggs Wedaman

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FT. WAYNE IN

260-480-0120 46805

PHONE 260-710-4413 FAX

E-MAIL DATE 5/1/09

mbwedaman@verizon.net

UNITED STATES POSTAGE
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0004605473 APR 27 2009
MAILED FROM ZIP CODE 46256

Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 462

REC'D MAY 4 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY INDIANA HISTORIC SPANNS TASCORP

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON PAUL BRANDENBURG

ADDRESS 5868 CROWN CIRCLE
INDIANAPOLIS IN 46254

PHONE 317-347-1204 FAX 317-347-1206

E-MAIL DATE 1-11-2009

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Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

REC'D MAY 4 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Irvington Park Nbrhd Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Suzanne Slick

ADDRESS 3318 Garland Ave

Fr. Wayne

IN

46805

PHONE 260-482-3991 FAX

E-MAIL sjslick@mac.com

DATE 7/9/09

Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

APPENDIX D. Photographs

STATE BOULEVARD RECONSTRUCTION
PHOTOGRAPHS - Page One Map A
DHPA - INDOT Des. No. 0400587



Photo 001



Photo 002



Photo 003



Photo 004

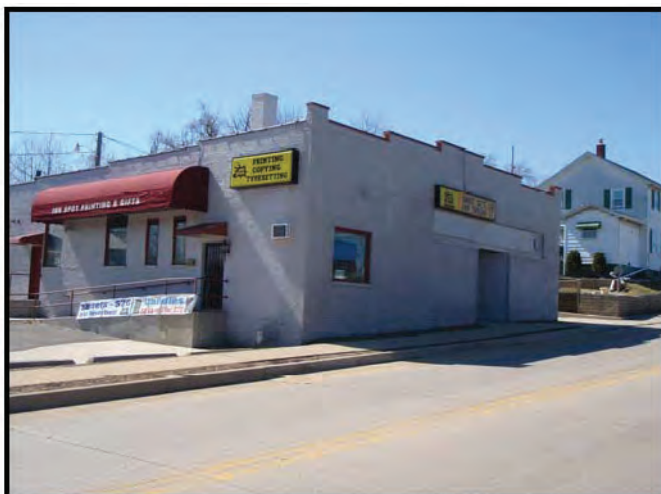


Photo 005

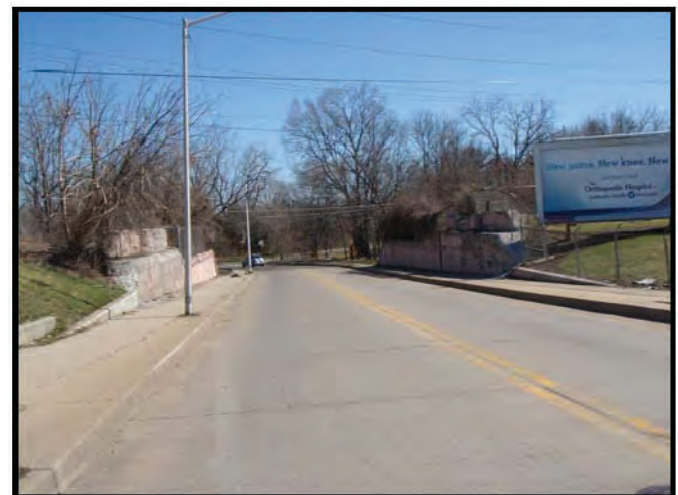


Photo 006



Photo 001A



Photo 001B



Photo 002A



Photo 002B



Photo 007



Photo 008



Photo 009



Photo 010



Photo 011



Photo 012



Photo 008A



Photo 008B



Photo 008C

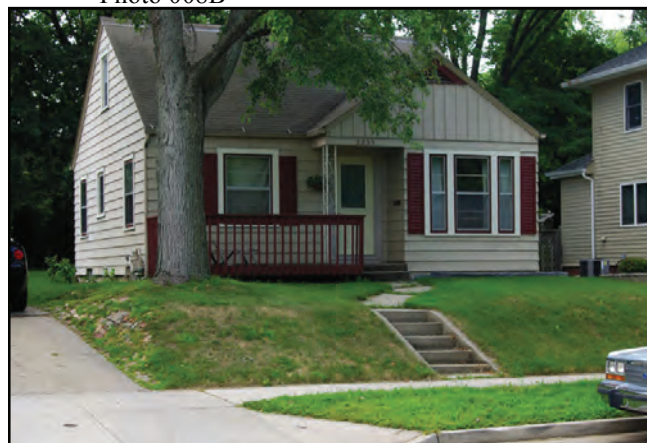


Photo 008D



Photo 008E



Photo 010A



Photo 010B



Photo 010C



Photo 010D



Photo 011A



Photo 011B



Photo 012A



Photo 012B

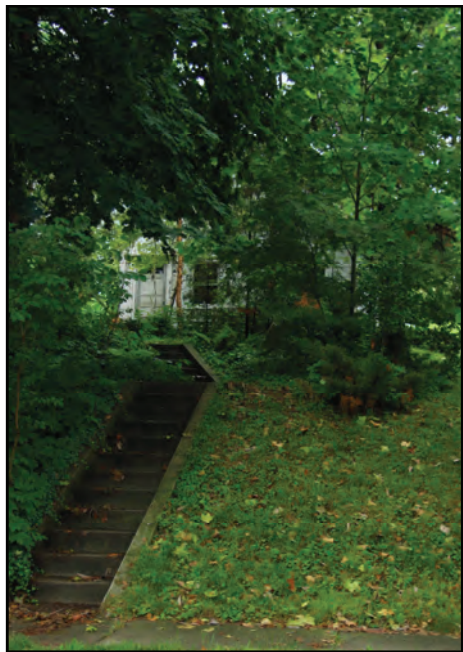


Photo 012C



Photo 012D



Photo 012E



Photo 013



Photo 13A



Photo 13B



Photo 014



Photo 015



Photo 016



Photo 017



Photo 018



Photo 019



Photo 020



Photo 021



Photo 021A

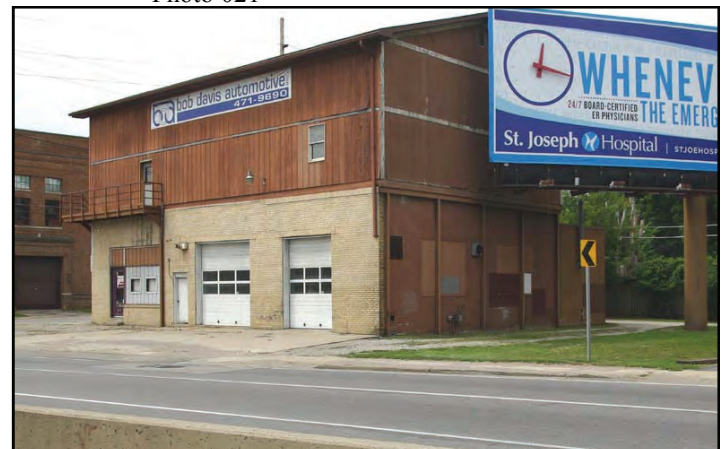


Photo 021B



Photo 022



Photo 023



Photo 024



Photo 025



Photo 026



Photo 027



Photo 025A



Photo 025B



Photo 027A



Photo 027B



Photo 027C



Photo 028



Photo 029



Photo 030



Photo 031



Photo 032



Photo 033



Photo 034



Photo 035



Photo 036



Photo 037



Photo 038



Photo 039



Photo 040



Photo 041



Photo 042



Photo 043



Photo 044



Photo 045



Photo 040A



Photo 040B



Photo 040C



Photo 040D



Photo 040E



Photo041A



Photo 042A



Photo 042B



Photo 042C



Photo 042D



Photo 042E



Photo 044A



Photo 044B



Photo 044C



Photo 044D



Photo 044E



Photo 045A



Photo 046



Photo 047



Photo 048



Photo 049



Photo 050



Photo 051



Photo 052



Photo 053



Photo 054



Photo 055



Photo 056



Photo 057



Photo 058



Photo 059

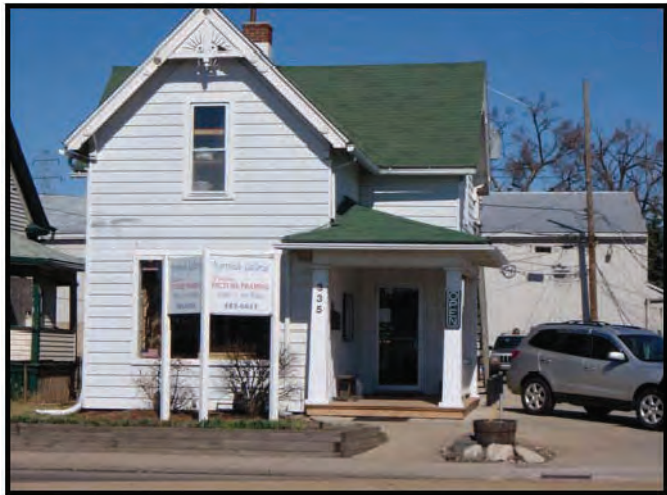


Photo 060



Photo 061



Photo 062



Photo 063



Photo 064



Photo 065



Photo 066



Photo 067



Photo 068



Photo 066A



Photo 067A



Photo 067B



Photo 069



Photo 070



Photo 071



Photo 071A



Photo 071B



Photo 071C



Photo 072

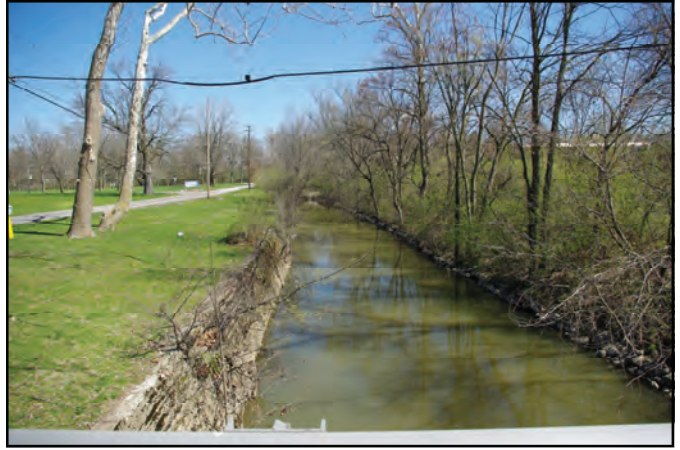


Photo 073



Photo 074



Photo 075



Photo 076



Photo 077



Photo 078



Photo 079



Photo 080



Photo 081



Photo 082



Photo 083



Photo 084



Photo 085



Photo 086



Photo 087



Photo 088



Photo 089



Photo 090



Photo 091



Photo 092



Photo 093



Photo 094



Photo 095



Photo 096



Westbrook looking SE to Edgehill Ave



Westbrook looking NW to State Blvd showing street light



Westbrook looking SE showing filled portion of street and curb



Westbrook looking SE between Edgehill Ave and Spy Run



Looking NE from 2157 Edgehill Ave



Looking SE from 2157 Edgehill Ave



Looking SE from 2232 Eastbrook Dr



SR 4 NC Clinton St Bridge



Looking S to State Blvd from 2320 Oakridge Rd



Looking N to Neva from 2320 Oakridge Rd



Looking SE to State Blvd from Oakridge Rd



Looking SW to State Blvd from Oakridge Rd



Looking SW to Spy Run Bridge from State Blvd



Looking NE to State Blvd from Eastbrook Dr



Looking NE to Eastbrook Dr from State Blvd



Looking SE from 2342 Eastbrook Dr



Terrace Rd looking S to State Blvd



Looking NW from Neva to Oakridge Rd



Looking SW from Neva Ave to Clinton St



Looking W along State Blvd



Looking E across State Blvd



Spy Run Bridge_ST 5_N elevation



Spy Run Bridge_ST 5_Bridge deck



Looking E to Interurban abutments along State Blvd



Looking W along State Blvd from Cass Street



Looking W along State Blvd from Clinton



Looking NW at State Blvd and Spy Run Avenue



09014_Looking NW across State Blvd to 315 E State Blvd



Looking E from 315 W State Blv



Looking W along State Blvd from 315 W State State Blvd



State at Wells looking E into the APE



Spy Run Bridge_ST 5_rail N elevation



09014_315 E State Blvd S and E elevation 6

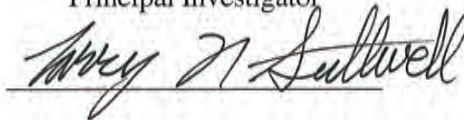
APPENDIX E. Report Summaries

**An Archaeological Field Reconnaissance of the Proposed State Boulevard
Improvements (Des No. 0400587) in Ft. Wayne, Allen County, Indiana**

By

Larry N. Stillwell

Principal Investigator

A handwritten signature in black ink, reading "Larry N. Stillwell", is written over a horizontal line.

Submitted by:

Archaeological Consultants of Ossian

P.O. Box 2374

Muncie, IN 47307

Submitted to:

American Structurepoint, Inc.

Indianapolis, IN

April 2, 2009

Archaeological Consultants of Ossian *Cultural Resource Management Report 09FR55*

Abstract

On March 18, 2009, personnel from Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey of the proposed State Boulevard improvements (Des. No. 0400587) in Ft. Wayne, Allen County, Indiana. The project area totaled approximately 6.43 acres, and was located in an urban setting by the St. Joseph River. Soils within the project area varied from well drained to very poorly drained. Shovel testing and pedestrian walkover survey combined with auger testing was unable to locate any cultural resources within the project limits, and the field reconnaissance determined that the project area was disturbed. It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places. Thus, no further archaeological work is warranted. Project clearance is recommended.

Historic Property Report

State Boulevard Reconstruction

From

Spy Run to Cass Street

Fort Wayne, Allen County



INDOT Des. No. 0400587
Federal Project No.
Prepared for:
StructurePoint
Project No. IN20071404

Prepared by
The Westerly Group, Inc.
Camille B. Fife, Principal Investigator
225 E. Main St., Madison, IN 47250
(812) 273-8826 (F) (812) 265-9606;
wgimadison@aol.com
September, 2009

Executive Summary

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County. Since the project is receiving funding from the Federal Highway Administration (FHWA), it is therefore subject to Section 106 review. More specifically the project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. A new alignment in the section between Westbrook Drive and Terrace Road will provide enhanced roadway safety. The project also includes replacing a bridge over Spy Run Creek and adding a new trail bridge. This report was prepared using information from local and state government repositories, historical societies, local organizations, library, newspapers, and site inspection.

Above-ground resources were identified and evaluated in accordance with Section 106, national Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (Revised January 2001) and Final Rule of Revision of Current Regulations, dated December 12, 2000, and incorporating amendments effective August 4, 2004. As a result of the NHPA, as amended, and CFR Part 800 (Revised January 2001), federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic Properties include buildings, structures, sites, objects, and/or districts. The report follows the Indiana Department of Transportation's (INDOT) Cultural Resource Manual (CRM) guidelines for preparing Historic Property Reports (HPR.)

The project passes through the proposed Brookview-Irvington Park National Register (NR) Historic District. Thirty-eight properties within the proposed historic district were previously surveyed in the Fort Wayne Interim Report(See Table I.) The district boundary encompasses neighborhoods which are significant examples of community planning. The district is eligible for the National Register of Historic Places (NRHP) under criterion C, and criterion A.¹ Taking into account that any effects within part of the district can be considered to effect the entire district, the proposed Area of Potential Effect (APE) for the project has been defined as an area of concentration in the southern portion of the district. This concentrated APE represents the part of the district within which potential alternative alignments for the relocation of State Boulevard might occur. Ninety-four resources were surveyed within this area including two structures. A number of buildings within the APE in the district and one bridge may be impacted by the project, depending upon the preferred alignment. The bridge over Spy Run Creek which is located within the district has been previously determined eligible for listing in the NRHP and contributes to the district.²

Twenty buildings and two structures outside the district, but within the APE for this project, were surveyed. Only one of these buildings was recommended eligible for the NRHP. The section of State Boulevard, outside the historic district was reviewed and recommended not eligible for the NRHP.

This photo was probably taken soon after the State Boulevard overpass was completed in 1927. Both the railroad and the interurban ran above the depressed State Boulevard. Source: Berndt, C. *The Toledo & Chicago Interurban Railway Company, Its Predecessor and Successors.*) **Figure 15**





Historic Property Report
State Boulevard Reconstruction from Spy Run to Cass Street
Additional Information Report
DES No.: 0400587

Prepared for
**Indiana Department of Transportation/
Federal Highway Administration**

Prepared by
WEINTRAUT & ASSOCIATES, INC.

Principal Investigator: Dr. Linda Weintraut

Author: Bethany Natali, M.A.

P.O. Box 5034

Zionsville, Indiana 46077

317.733.9770

Linda@weintrautinc.com

April 2012

Executive Summary:

State Boulevard Reconstruction from Spy Run to Cass Street

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

This report provides additional information to the Historic Property Report (HPR) for the State Boulevard Reconstruction from Spy Run to Cass Street (Westerly Group, Inc., 2009). In 2009, the Westerly Group, Inc. (WGI) identified and evaluated properties more than fifty years of age at that time within the Area of Potential Effect (APE).

In February 2012, American Structurepoint, Inc. contracted with Weintraut & Associates, Inc. (W&A) to prepare an Additional Information Report (AI) to append the HPR. The purpose of the AI is to supplement the

HPR following the inclusion of two new NR-listed resources within the APE. Project historians who meet the Secretary of the Interior's Professional Standards identified and evaluated historic properties within the APE for this project in accordance with Section 106 of the National Historic Preservation Act (1966), as amended and 36 C.F.R. Part 800.

As part of AI investigations for this project, historians identified two districts that were listed in the National Register of Historic Places (NR) after the HPR (2009) was prepared; portions of both districts are contained within the project APE:

- Fort Wayne Park and Boulevard System Historic District (NR, 2010)
- Brookview-Irvington Park Historic District (NR, 2011)

The Bridge over Spy Run (ST-5/NBI No. 0200273) was previously determined eligible for listing in the NR.

Historians believe proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts. Therefore, the recommended finding of effect for this project is: Historic Properties Affected—Adverse Effect.



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION
AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Larry Stillwell

Date (month, day, year): 7/11/12

Project Title: An Archaeological Field Reconnaissance of the Proposed Additional Area Required for the State Boulevard Improvements (Des. No. 0400587) in Fort Wayne, Allen County, Indiana

PROJECT OVERVIEW

Project Description:

The City of Fort Wayne, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map, and in the east ½ of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing two-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the sub-standard horizontal curve. The five lane section will include two new travel lanes in each direction and a center two-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

INDOT Designation Number/ Contract Number: 0400587

Project Number: 12FR66

DHPA Number:

Approved DHPA Plan Number:

Prepared For: American Structurepoint, Inc.

Contact Person: Ms. Briana Hope

Address: 7260 Shadeland Station

City: Indianapolis

State: IN

ZIP Code: 46256-3917

Telephone Number: 317 547 5580

E-mail Address: bhope@structurepoint.com

Principal Investigator: Larry N. Stillwell

Signature:

Company/Institution: Archaeological Consultants of Ossian (ACO)

Address: PO Box 2374

testing methodologies.

Attach photographs documenting disturbances below

Describe Disturbances:

Non-agricultural soil disturbance was noted throughout the project area. In the extreme eastern end of the project commercial development had impacted the few areas of exposed soil that had not been paved (Figure 4). Soils in this area had been graded and filled.

The portion of the project situated between North Clinton Street and the abandoned railroad grade was predominantly residential. Soils had been disturbed in these areas by grading and filling activity associated with home construction, construction of lots and driveways for parking cars, road removal, contouring of the ground for drainage of the neighborhoods, and the removal tracks from a rail line (Figures 4 and 5).

In the western end of the project, soils had been disturbed by residential and commercial development. Most of the project universe was comprised of made land created through commercial development.

No intact naturally occurring soil stratigraphy was encountered during shovel testing of the project.

Comments:

The entire State Boulevard project was 12.8 acres in area. A total of 3.5 acres was originally surveyed by ACO in 2009 (Stillwell 2009). Of the remaining 9.3 acres of additional area examined during the current field reconnaissance, 3.5 acres was covered in existing asphalt pavement. Thus, 5.8 acres of the project was subjected to shovel testing.

RESULTS

☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.

☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.

☒ Phase Ia reconnaissance has located no archaeological resources in the project area.

☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: acres:

Comments:

RECOMMENDATION

☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.

☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.

☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (*1:24,000scale*).
- ☒ Aerial photograph showing the project area, land use and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (*if available*)

Other Attachments:

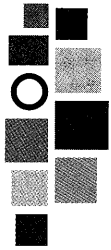
References Cited:

Comments:

Curation

Curation Facility for Project Documentation:

APPENDIX F. Correspondence



AMERICAN
STRUCTUREPOINT
INC.

March 23, 2009

James A. Glass, PhD
State Historic Preservation Officer
Indiana Department of Natural Resources
402 West Washington Street, W247
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map and in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. Please see the attached maps and aerial photographs for specific project location.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

The need for the project originates from the substandard horizontal curve along State Boulevard. A new alignment in this section will provide enhanced roadway safety. Curb and gutter, sidewalks, and a new storm sewer system along with "livable" street amenities, such as lighting and landscaping, will also be conducted. The purpose of the project is to improve traffic flow and roadway and pedestrian safety on State Boulevard.

Land use in the vicinity of the project is primarily residential. The project area will be investigated for archaeological and historic resources for compliance with Section 106 of the National Historic Preservation Act. The results of these investigations will be forwarded to the Indiana Department of Transportation (INDOT) and the Indiana State Historic Preservation Officer for their review and concurrence.

IN20071404

James A. Glass, PhD
March 23, 2009
Page 2

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you are hereby requested to be a consulting party to participate in the Section 106 process. This process involves efforts to identify historic properties potentially affected by the undertaking, assess the effect of the undertaking on historic properties, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties.

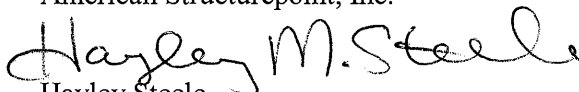
We realize you and/or your organization may want to perform a formal review of properties that may be affected as part of this project and, therefore, may require more detailed information regarding the scope of the project. Please note, preparation of a historical property report is underway and will be sent at a later date to consulting parties.

The following agencies have been invited to be consulting parties:

- State Historic Preservation Officer (SHPO)
- INDOT
- INDOT Fort Wayne District
- City of Fort Wayne Engineer
- Historic Landmarks Foundation of Indiana
- Allen County Historian
- Allen County – Fort Wayne Historical Society
- ARCH, Inc.
- Fort Wayne Historic Preservation Review Board
- John Shoaff, Fort Wayne City Council Member

Per 36 CFR 800.3(f), we hereby request the SHPO notify this office **within 30 days of the receipt of this letter** if the SHPO is aware of any other parties that may be entitled to be consulting parties for this project area.

Very truly yours,
American Structurepoint, Inc.

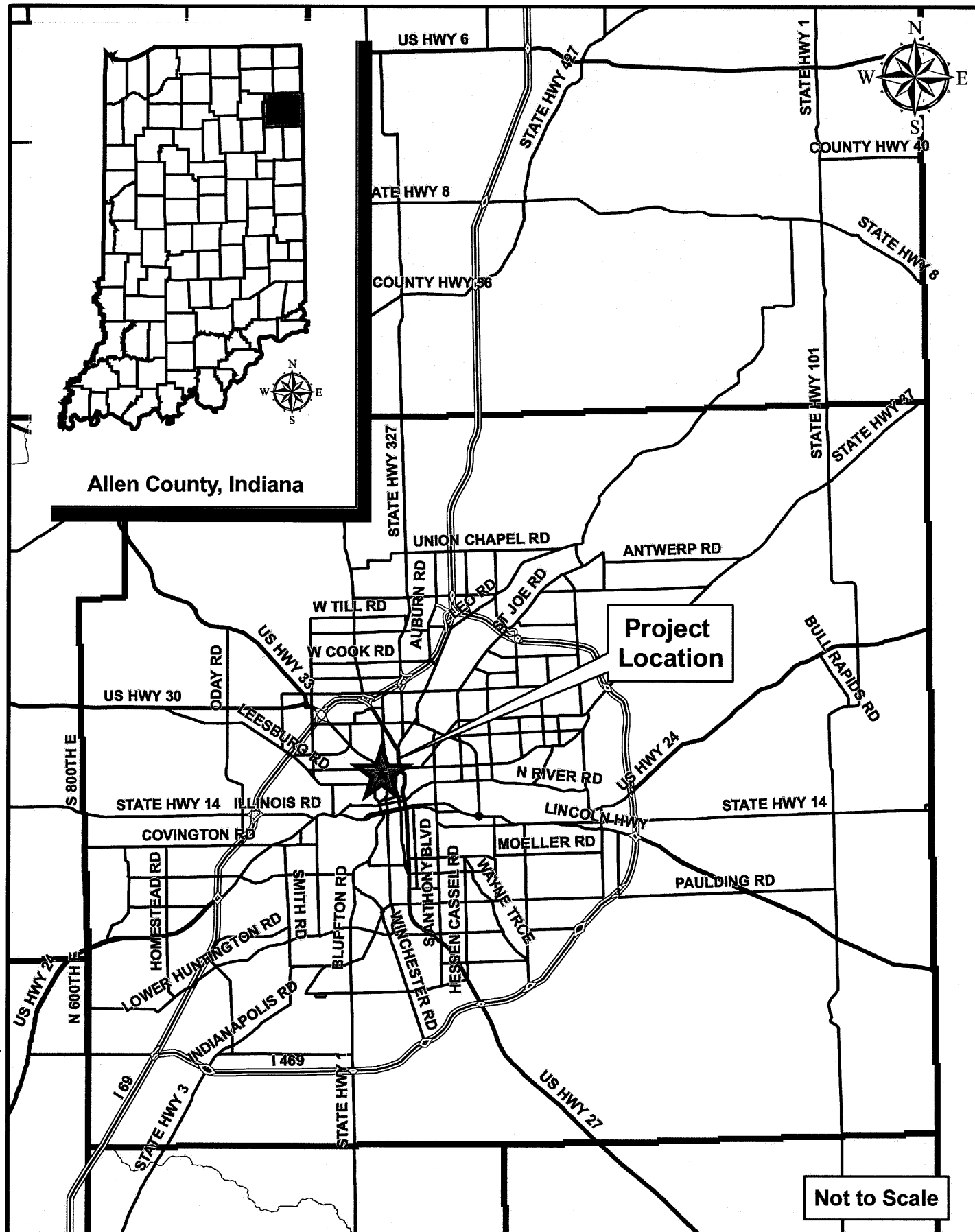

Hayley Steele
Environmental Scientist

HMS:ejp

Enclosures



Allen County, Indiana



Not to Scale



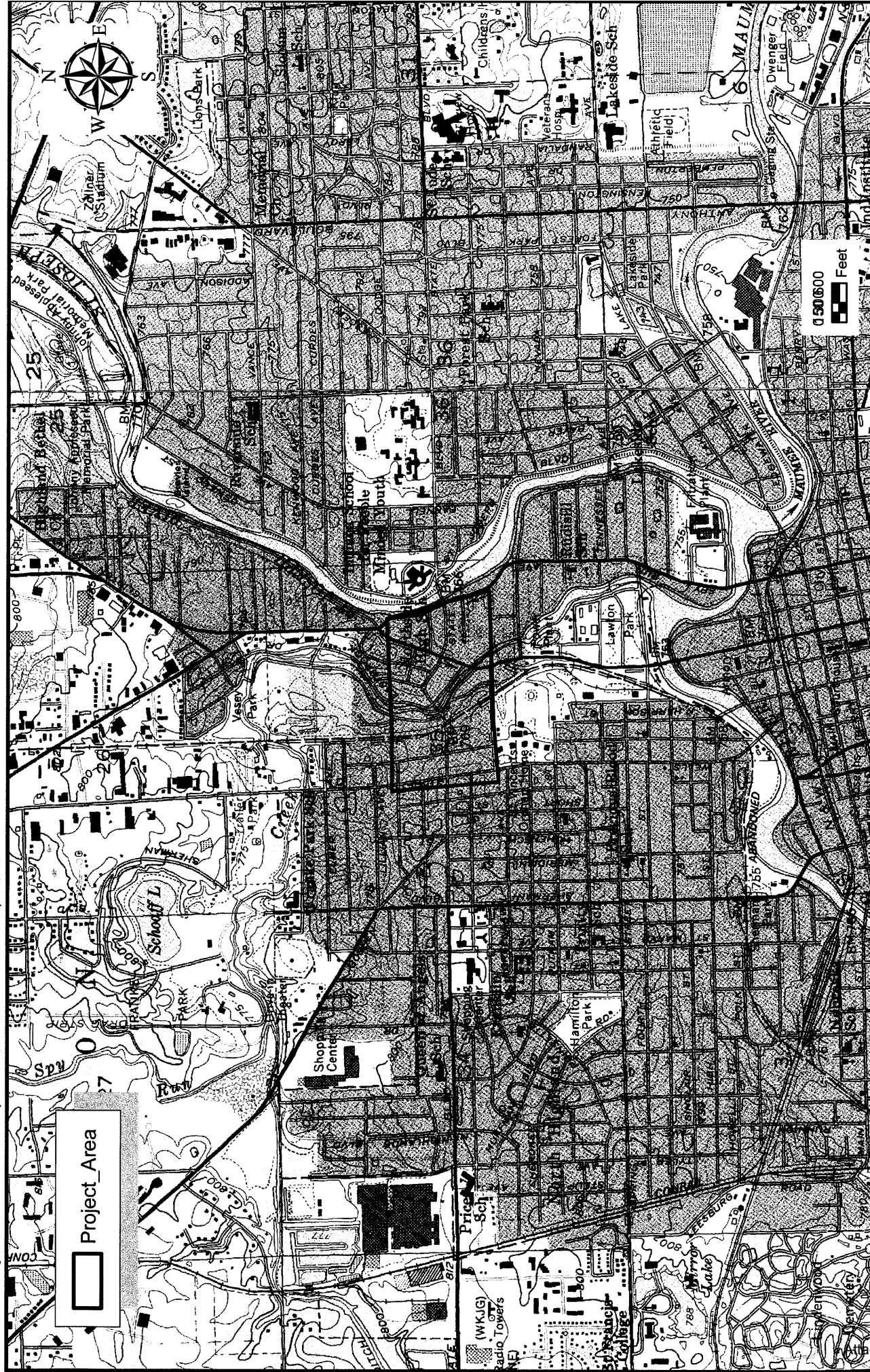
Project Location Map

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 03/12/2009



<p>USGS Topographic Mapping Fort Wayne West and Fort Wayne East Quadrangles</p>	<p>State Boulevard Reconstruction</p> <p>Location: Fort Wayne Township: Wayne County: Allen State: Indiana</p> <p>Date: 03/12/2009</p>	<p>AMERICAN STRUCTUREPOINT INC. www.structurepoint.com ©2007-2009, American Structurepoint, Inc.</p> <p>Applicant: City of Fort Wayne, Board of Public Works 420 City-Council Building One Main Street Fort Wayne, Indiana 46802</p>
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<p>State Boulevard Reconstruction</p> <p>Location: Fort Wayne Township: Wayne County: Allen State: Indiana</p> <p>Date: 03/12/2009</p>	<p>Project Location - 2005 Aerial Photography</p> <p>Applicant: City of Fort Wayne, Board of Public Works 420 City-Council Building One Main Street Fort Wayne, Indiana 46802</p>	<p>AMERICAN STRUCTUREPOINT INC.</p> <p>www.structurepoint.com ©2007-2009 American Structurepoint, Inc.</p>
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The mission of ARCH is
to advocate for the protection
and preservation of historically
and culturally significant assets
and historic places in
Allen County and
northeast Indiana.

Hayley Steele
Environmental Scientist
American Structure Point, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957
4/16/09

Dear Ms. Steele,

Thank you for your invitation to become a Consulting Party in the State Boulevard Reconstruction Project located in Fort Wayne, Allen County, Indiana. (Des. No. 0400587, Project No. IN20071404) We accept your invitation and have returned the enclosed postcard indicating the same. If you have not already done so, I suggest that the following two groups that have a demonstrated interest in the project be added as Consulting Parties.

1. Friends of the Parks, a not-for-profit organization that has a concern for the parks of Allen County has a particular concern for the effects on historic parks and designed landscapes such as those likely to be encountered in this project. Contact person is Julie Donnell, 3604 South Washington Road, Fort Wayne, IN 46802.
2. Brookview Neighborhood Association represents the residents in the historic Brookview neighborhood through which the project will pass. They have a vested legal and economic interest in the project and should be added as Consulting Parties. Contact person is Michelle Briggs-Wedaman, 2326 Eastbrook Drive, Fort Wayne, IN 46805

Sincerely yours,

Michael Galbraith
Historic Preservation Specialist, ARCH, Inc.
mgalbraith@archfw.org

ecc: Michelle Briggs-Wedaman, Brookview Neighborhood Association
John Carr, DHPA
Carrie Chapman, Friends of the Parks
Julie Donnell, Friends of the Parks
Todd Zeiger, HLF





Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



April 23, 2009

Hayley Steele
Environmental Scientist
American Structurepoint, Inc.
7620 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Notification concerning the Reconstruction of State Boulevard (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA # 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed your letter dated March 23, 2009 and received on March 25, 2009, for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana.

Thank you for notifying our office of the proposed project. As you know, a complete analysis of the project is not possible at this time. Please provide the following information to facilitate the identification and analysis of buildings and structures in the area of potential effects:

- ❖ Literature Review
- ❖ Historic Context
- ❖ Research Methodology
- ❖ Property Descriptions
- ❖ National Register of Historic Places eligibility evaluations and recommendations

For further guidance on the indicated information, please refer to Appendix AA of INDOT's Indiana Cultural Resources Manual (<http://www.in.gov/indot/7287.htm>). Please keep in mind that additional information may be requested in the future.

It is our understanding from the information contained in your letter that the project area will be investigated for archaeological resources. Therefore, we will await the archaeological information which will be provided.

Similarly, it appears that you will be providing us with a historic property report regarding aboveground properties within an area of potential effects ("APE") that is yet to be defined. If so, then it appears likely that the kinds of information outlined above will be provided to us in that format. We do have a few comments to offer at this time.

Regarding consulting parties, we agree with the recommendation by Michael Galbraith of ARCH, Inc., in his April 16, 2009 letter to you, that Friends of the Parks and Brookview Neighborhood Association be invited to participate as consulting parties in this Section 106 review. It appears that the State Boulevard Bridge over Spy Run Creek (also known as Allen County Bridge #546 or the Evard Bridge), may be eligible for inclusion in the National Register. Because it appears that this bridge

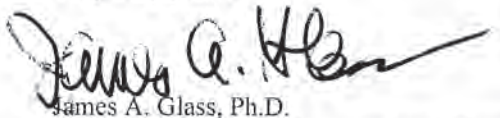
will be affected by the project, we ask that you also invite Paul Brandenburg of the Indiana Historic Spans Task Force (5868 Croton Circle, Indianapolis, Indiana 46254; e-mail: indianabridges@sbcglobal.net) and bridge historian Dr. James L. Cooper (629 East Seminary Street, Greencastle, Indiana 46135; e-mail: jlcooper@ccrtc.com) to participate as consulting parties.

Because this project will physically affect an area at least four city blocks in length, we ask that you carefully delineate the APE to include not only areas where physical effects and obvious visual effects are likely but also areas where foreseeable indirect effects could occur. It appears to us that realigning State Boulevard throughout much of the project area could cause not only a variety of atmospheric effects but also effects such as limiting access to historic properties, either temporarily or permanently, which could alter their uses (*see* 36 C.F.R. § 800.16[d] and [i] and § 800.5[a] and especially [a][2][iv] and [v]).

In a separate undertaking involving INDOT's proposed replacement of the US 27 (Clinton Street) bridge over Spy Run Creek (Des. No. 0200914), we have expressed the opinion that the Brookview-Irvington Park Historic District is eligible for inclusion in the National Register of Historic Places under Criterion C as an example of community planning. ARCH, Inc., has proposed to have an application for nomination of the district to the National Register prepared. Although a formal finding has not yet been made in that project, it is our understanding that FHWA and INDOT preliminarily have agreed with our opinion on the district's eligibility. Presumably, FHWA, INDOT, and we would arrive at a similar conclusion with regard to the undertaking proposed here along State Boulevard, which would cross the district. We have enclosed a map with the preliminary boundary of the district shown in dark lines.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. Questions about buildings or structures should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA # 5903.

Very truly yours,



James A. Glass, Ph.D.

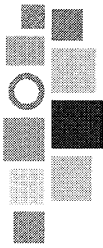
Deputy State Historic Preservation Officer

JAG:ALJ:JLC:jlc

Enclosure

cc: Robert F. Tally, Jr., P.E., Federal Highway Administration, Indiana Division
Michelle Allen, Indiana Department of Transportation

enc: Shaun Miller, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Michael Galbraith, ARCH, Inc.
Julie Donnell, Friends of the Parks
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.
Paul Brandenburg, Indiana Historic Spans Task Force
James L. Cooper, Ph.D., historian



AMERICAN
STRUCTUREPOINT
INC.

7260 Shadeland Station
Indianapolis, Indiana 46256
TEL 317.547.5580
FAX 317.543.0270

www.structurepoint.com


LETTER OF TRANSMITTAL

To: Dr. James Glass State Historic Preservation Officer DNR - Historic Preservation & Archaeology 402 W. Washington Room W274 Indianapolis, Indiana 46204	Date: July 2, 2009	Billing Group:
	Project Number: IN20071404	Billing Phase:
	Project Description: City of Fort Wayne - State Blvd. between Spy Run and Cass Street Fort Wayne, Indiana (IN) Project Des. 0400587 Purchase Order No. 90640023-000	

Deliver by:	We are sending you:		
Time:	<input checked="" type="checkbox"/> Enclosed	via	<input checked="" type="checkbox"/> Mail
Date:	<input type="checkbox"/> Separately		<input type="checkbox"/> Messenger
			<input type="checkbox"/> Overnight
			<input type="checkbox"/> As Requested
			<input type="checkbox"/> For Your Information
			<input checked="" type="checkbox"/> For Your Review & Comment
			<input type="checkbox"/>

Quantity:	Type:	Description:
1	RPT	An Archaeological Field Reconnaissance of the Proposed State Boulevard Improvements (Des. No. 0400587)

Remarks:
Please review the enclosed report and contact me with any questions or comments you may have. Feel free to contact me by phone, (317)547-5580, or by email at hsteele@structurepoint.com . Thank you.

Copies:	Trans:	Enclosure:	To:	Very truly yours, American Structurepoint, Inc.  Hayley Steele
Please return copies to this office.				



AMERICAN
STRUCTUREPOINT
INC.

November 9, 2009

Dr. James A. Glass
Deputy State Historic Preservation Officer
Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room W274
Indianapolis, Indiana 46204

Re: Des. No. 0400587, DHPA #5903
State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. The need for the project originates from the substandard horizontal curve along State Boulevard.

The total project length is approximately 2,300 feet. The existing section of State Boulevard from North Clinton Street to Spy Run will be widened to five lanes along the existing alignment. The existing 2-lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. Project historians from The Westerly Group (WG), who meet or exceed the Secretary of Interior's standards for Section 106 work, identified and evaluated historic properties within the Area of Potential Effect (APE) for this project. Historic properties were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (revised January 2001), Final Rule on Revision of Current Regulations, December 12, 2000, and incorporating amendments effective August 5, 2004. The Historic Properties Report and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on November 6, 2009.

IN20071404

Dr. James A. Glass
November 9, 2009
Page 2

The APE of this undertaking is within the Brookview-Irvington Historic District (District). The District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne, especially the planned suburban developments of the Wildwood Companies. In addition, it is eligible as a designed landscape, the work of Arthur A. Shurcliff. The District contains an estimated 315 individual resources. There are 92 individual buildings within the District that were reviewed to determine their contribution to the District. These were located within the APE and could be directly or indirectly affected by the undertaking. All but 12 of these buildings were deemed to contribute to the District. The bridge over Spy Run Creek, which also contributes to the District, and the Brookview-Irvington Historic District are recommended as eligible for the NRHP. State Boulevard itself, both within the District and to the east and west of it, was analyzed. State Boulevard within the District is recommended individually eligible for the NRHP because of its contribution to the District. State Boulevard outside of the District and within the APE is recommended as not individually eligible for the NRHP. Twenty individual buildings within the APE but outside of the District were analyzed. Nineteen of the 20 buildings are not individually eligible for the NRHP, and one is recommended individually eligible for the NRHP. The former railroad and interurban overpass was evaluated and determined not individually eligible for the NRHP.

Due to the proposed realignment of State Boulevard and the replacement of the bridge over Spy Run, impacts to the Brookview-Irvington Historic District are expected. Because of this, a *preliminary* effect finding of Adverse Affect to Historic Properties is anticipated. Additional effects analyses are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting your review and comment on the Historic Properties Report (HPR) and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the HPR by December 11, 2009.

We are also requesting your dates of availability for a consulting parties meeting. The meeting will occur after SHPO and all consulting parties have had time to review the HPR. We would like to have the meeting during either the week of December 7, 2009, or the week of December 14, 2009. I have attached a chart of the possible days for the meeting. Please return this chart indicating either morning or afternoon on any day that you are available for a consulting parties meeting. After receiving availability from SHPO, the Federal Highway Administration, INDOT, and consulting parties, a meeting will be scheduled and an agenda will be set. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at hsteele@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Hayley M. Steele
Environmental Scientist

HMS:mgn

Enclosures

See distribution list on the next page.

Dr. James A. Glass
November 9, 2009
Page 3

cc: Patrick Carpenter, Historian, Cultural Resources Section – INDOT (via email)
Joyce Newland – Federal Highway Administration
Shan Gunawardena – City of Fort Wayne
Angie Quinn and Michael Galbraith – ARCH, Inc.
Don Orban – Fort Wayne Historic Preservation Commission
Todd Zeiger – Historic Landmarks Foundation of Indiana – Northern Office
Julie Donnell – Friends of the Parks of Allen County
Michelle Briggs-Wedaman – Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg – Indiana Historic Spans Task Force
Charley Shirmeyer – Northside Galleries
Karl Dietsch – Brookview Neighborhood Association
Susan Haneline – Brookview Neighborhood Association
Annette Daily – Brookview Neighborhood Association
Dan Avery – Northeastern Indiana Regional Coordinating Council
Suzanne Slick – Irvington Park Neighborhood Association
Camille Fife – Westerly Group (via email)



M E M O R A N D U M

DATE: December 1, 2009

TO: Ms. Joyce Newland, Federal Highway Administration
Mr. Patrick Carpenter, INDOT Cultural Resources
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Camille Fife, Westerly Group
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association
Mr. John H. Shoaff, Fort Wayne City Council
Dr. James L. Cooper
Mr. Paul Brandenburg, Indiana Historic Spans Task Force
Ms. Susan Haneline, Brookview Neighborhood
Mr. Charley Shirmeyer, Northside Galleries
Mr. Karl Dietsch, Brookview Neighborhood
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council
Ms. Suzanne Slick, Irvington Neighborhood
Ms. Jan Daily, Brookview Neighborhood

FROM: Hayley Steele, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for December 15, 2009 at 9:30 am. The meeting will be held in the City County Building, Room 128. Because several of the consulting parties for the State Boulevard Project are also on a list of consulting parties for a nearby project (US 27 over Spy Run) it was requested the meetings for both projects be held on the same day, and the meeting was therefore scheduled for the morning of the 15th.

The City County Building is located at 1 East Main Street in downtown Fort Wayne. This is between South Calhoun and South Clinton Street along Main Street, approximately 1 mile south of the State Boulevard project area. Parking is available in a parking garage attached to the City County Building.

Please see the attached agenda for the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at hsteele@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

December 15, 2009
9:30 am
City County Building- Room 128
1 East Main Street
Fort Wayne, Indiana

1. Overview of Proposed Project (American Structurepoint)
 - a. Purpose and Need
 - b. Proposed Improvements
 - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Discussion of Potential Mitigation Measures (Westerly Group/Structurepoint)
4. Next Steps (Westerly Group/Structurepoint)
 - a. Development of Memorandum of Agreement
 - b. Follow-up items

162 E State Blvd

Fort Wayne, In. 46805

12/7/2009

Hayley Steele, Environmental Scientist

American Structurepoint, Inc.

7260 Shadeland Station

Indianapolis, In. 46256

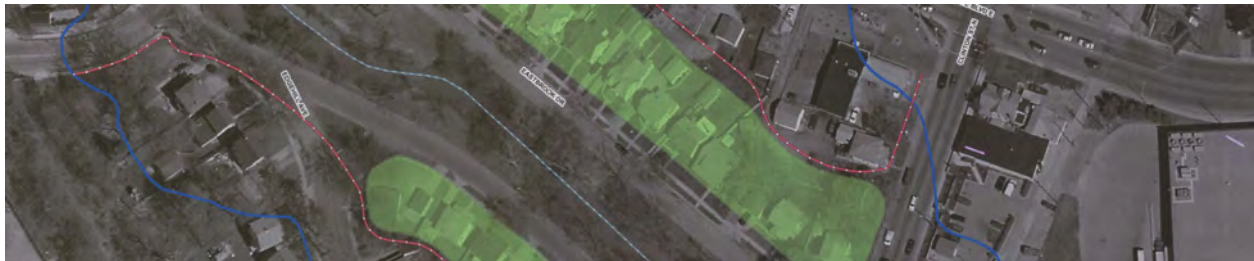
Dear Ms. Steele,

I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effect (APE). The Westerly Group presents the report in a user-friendly format that informs the reader of the project and its anticipated outcome. I appreciate the cautionary notes regarding the preliminary status of the design and the knowledge gained from the report.

One of the recurring themes throughout the report is the mixed-use category that the APE has always had and many residents are acting to preserve its historic nature. As the report notes on page 5, State Boulevard is an urban minor arterial road, (*"Urban minor arterial road" means a route that generally interconnects with and augments an urban principal arterial road and provides service to trips of shorter length and a lower level of travel mobility*) that was designed to slow down traffic by a master landscaper whose examples are limited. The fact that there was a larger design that was never completed makes it likely that the area will become a part of the National Register of Historic Properties under several criterion.

The history of the designers, builders, and the neighborhoods is consistent with the current atmosphere of the APE. The individual descriptions of the properties and the area characteristics allow the reader to understand the primary affected properties and their contribution to the APE. While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling that residents of this neighborhood share.

Furthermore, the recent closing of the Kroger Fuel Center (photo 57) may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek (below).



If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State Boulevard and Spy Run Avenue (as seen in the photo section below) the road would pass behind or around most of the homes in the APE . The designers of the road project could not have foreseen the closing



of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy, it seems only fair that a separate study be conducted factoring in possible land use of the former Kroger Fuel Center.

Sincerely,

Annette "Jan" Dailey



Northern Regional Office
402 W. Washington
South Bend, Indiana 46601
574-232-4534
574-232-5549 (fax)

December 8, 2009

Ms. Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Dear Ms. Steele,

I am writing today to submit comments concerning the Draft Historic Properties report dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). Thank you for your response to my earlier inquiry concerning the scope of our comments at this time. I understand that we are only commenting on the HPR at this time and that we will receive project specific design information for analysis in the future. In partnership with our organizational partner in Fort Wayne, ARCH, we have a number of specific questions and concerns which I will outline below.

1. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne. We believe that the HPR should note that not only is the district eligible but that a National Register nomination is currently being written by ARCH and the Fort Wayne Historic Preservation, with funding from the Indiana Division of Historic Preservation and Archaeology, the boundaries of which will correspond with the proposed boundaries outlined in the draft HPR. The nomination is being drafted to include eligibility not only under Criteria A but also Criterion C as a designated landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. Project Specific Design information: While it would be unusual to combine an HPR with information normally associated with later parts of the Section 106 or 4f process, we wish to note for the record that no maps delineating proposed road changes were included with the narrative description of the project. We note this due to the inclusion at this early stage of the review process language addressing outlining a "preliminary finding of adverse effect" contained in the cover letter as well as the HPR. With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.
3. Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State

Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.

Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties”. While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.

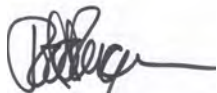
4. Purpose and Need. We note in the cover letter as well as within the HPR the stated Purpose of the project is “*to improve traffic flow, roadway, and pedestrian safety along State Boulevard.*” The Need for the project is stated as “*substandard horizontal curve along State Boulevard*”. We have a number of significant concerns and questions about how the proposed project will advance that purpose and meet the need. Again, it is unusual to get in to the merits of purpose and need and those metrics within the context of a discussion about the Historic Properties Report. Without specific plans it is difficult to analyze what alternatives under review would not only meet the purpose and need of the project but also avoid adverse impacts to the District and the State Boulevard bridge (noted above as an individually eligible and contributing resource to the district.) We anticipate additional questions once we receive more detailed project documents. In an effort to help keep the review moving we offer the following questions based on the narrative supplied in the HPR. We are offering the following based on the stated purpose as outlined in the cover letter and in the Executive Summary on page 3 of the HPR.
 - a For the purposes of our participation in the Section 106 and anticipated 4f review we will utilize the Purpose and Need as outlined in the Cover Letter and on page 3 of the HPR. Alternatively, if this is not correct please forward the project’s Purpose and Need along with the alternatives so that we can evaluate same in a proper manner.
 - b We have a number of questions and concerns about how the project as described in written form in the HPR will meet the stated purpose and need. Again, it is not possible to appropriately review and comment without project maps and designs of alternatives but absent those we can offer the following comments.
 - 1) How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.
 - 2) Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus.
 - 3) Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.
 - 4) Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.
 - 5) Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.
 - 6) Please provide detailed traffic accident studies or report data pertaining to accidents at the “substandard” curve.

- 7) Please provide detailed pedestrian accident studies or report data pertaining to the "substandard curve."
 - 8) How is the proposal to add a new "trail bridge" over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed "trail bridge".
5. "Trail Bridge": We continue to be concerned about an on-going appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future "trail bridge" should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.
 6. Concerning the "'substandard horizontal curve": Page 4 of the HPR states that " the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive." We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.
 7. Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7, We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).
 8. Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard

to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.

The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.

Sincerely,



Todd A. Zeiger

Director, Northern Region

cc. Indiana Division of Historic Preservation and Archaeology
Consulting Parties for State Boulevard Project



AMERICAN
STRUCTUREPOINT
INC.

7260 Shadeland Station, Indianapolis, Indiana 46256
TEL 317.547.5580 FAX 317.543.0270

www.structurepoint.com

M E M O R A N D U M

DATE: December 8, 2009

TO: Ms. Joyce Newland, Federal Highway Administration
Mr. Patrick Carpenter, INDOT Cultural Resources
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Camille Fife, Westerly Group
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association
Mr. John H. Shoaff, Fort Wayne City Council
Dr. James L. Cooper
Mr. Paul Brandenburg, Indiana Historic Spans Task Force
Ms. Susan Haneline, Brookview Neighborhood
Mr. Charley Shirmeyer, Northside Galleries
Mr. Karl Dietsch, Brookview Neighborhood
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council
Ms. Suzanne Slick, Irvington Neighborhood
Ms. Jan Daily, Graduate Student, IPFW, Sociological Practice
Mr. Creager Smith, Historic Preservation Planner
Mr. Albert Cohan, Westbrook 5, LLC
Mr. Thomas M. Niezer, Barrett & McNagny, LLP
Mr. Ronald Ross, AIA, Martin Riley Architects and Engineers

FROM: Hayley Steele, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint Project No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

The agenda for the Consulting Parties Meeting scheduled for December 15, 2009 has been revised. Please feel free to contact me at hsteele@structurepoint.com or by phone at (317) 547-5580 with any questions or concerns.

We look forward to meeting with all of you on December 15, 2009 at 9:30 am.

Thank you

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

December 15, 2009
9:30 am
City County Building- Room 128
1 East Main Street
Fort Wayne, Indiana
(Revised 12/07/09)

1. Overview of Proposed Project (American Structurepoint)
 - a. Purpose and Need
 - b. Proposed Improvements
 - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Future Steps in the Process (Westerly Group/Structurepoint)
 - a. Potential Mitigation Measures
 - b. Development of Memorandum of Agreement
4. Follow-up items



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and preservation of historically
and culturally significant assets
and historic places in
Allen County and
northeast Indiana.

Hayley Steele
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

12/9/09

Dear Ms. Steele,

I am writing in regard to the Historic Property Report (HPR) dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). ARCH would like to thank you for the opportunity to comment and to commend Ms. Fife for the thorough, well-written Report. I have a few specific questions and concerns regarding the report.

1. 1. Regarding the cover letter. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP). We agree that it is likely eligible under Criterion A for its association with community planning and development in Fort Wayne. We also believe that the greater depth provided in the under progress NRHP Nomination will demonstrate eligibility under Criterion A for Architecture and Landscape Architecture. We also agree with your recommendation that the District be considered eligible under Criterion C as a designed landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. The greater depth provided in the under progress NRHP Nomination will hopefully demonstrate that eligibility. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. 2. Regarding maps. The HPR contained no maps, plans or drawings of the proposed project. We have found it difficult to study, review, and analyze this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review? Particularly helpful will be all maps, plans and drawings that detail possible alternatives that may have been studied.
3. Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary (page 3) of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states

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that the "purpose of the project is to improve traffic flow, roadway and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "adding a new trail bridge" as stated on page 3, addresses "traffic flow, roadway and pedestrian safety" along State Boulevard? Need is stated in the cover letter to "originate from the substandard horizontal curve along State Boulevard." I have addressed this in point 5 below.

4. Regarding the "Trail Bridge." On page 3 and page 4 of the HPR a new "trail bridge" is described as being part of the project. Page 4 states that the project includes a "pre-fabricated trail bridge over State Boulevard at the abandoned New York central railroad right-of-way between Cass Street and Westbrook Drive." Besides the safety issues that installation of this bridge raises, there is the issue of project aggregation/co-mingling with which we have been struggling in the US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.
5. Regarding the "substandard horizontal curve." Page 4 of the HPR states that "the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We question the analysis of "substandard" and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower

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environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive."

6. Regarding the Area of Potential Effect (APE). Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State Boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR. Also regarding the APE, were potential changes in traffic use in the Brookview - Irvington Park Historic District studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. 36 CFR 800.16 indicates that the APE shall be established to include "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties" yet page 6 of the HPR indicates that the APE for the State Boulevard reconstruction project was drawn to consider "potential direct impacts on historic districts or properties". We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this District. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.
7. Regarding the Project Description. Plans for this project seem to be quite complete according to the project description on page 7 of the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking."
8. Regarding the homes south of State Boulevard. Page 7 of the HPR states that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. As in point 4 regarding the trail bridge, we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate. The



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City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview - Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.

9. Regarding the seasonal flooding. Page 22 of the HPR states that "At the southern end of the district, between East State Boulevard and North Clinton Street, the creek's seasonal flooding has caused damage to homes." We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview - Irvington Park Historic District has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.

Sincerely yours,

Michael Galbraith
Historic Preservation Specialist, ARCH, Inc.
mgalbraith@archfw.org

emc: Dan Avery, NIRCC
Paul Brandenburg, SPANS
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Karie Brudis, DHPA
Patrick Carpenter, INDOT-CRS
John Carr, DHPA
Dr. James L. Cooper, SPANS
Julie Donnell, Friends of the Parks
Camille Fife, Westerly Group
Joyce Newland, FHWA
Don Orban
John Shoaff, Fort Wayne City Council
Creager Smith
Todd Zeiger, HLF

**Friends of the Parks of Allen County, Inc.
3604 South Washington Road
Fort Wayne, Indiana 46802**

Hayley Steele
Environmental Scientist
American StrucutrePoint
7260 Shadeland Station
Indianapolis, IN 46256
12/10/09

Dear Ms. Steele,

Regarding the proposed State Boulevard reconstruction in Allen County (Des. No. 0400587, DHPA# 5903) :

Since, as I told you several weeks ago, I am unavailable to attend the CP meeting on the 15th, I am following your advice and putting some of the concerns of the Friends of the Parks in writing. This letter is a response to your agenda, as amended in the e-mail exchange you have had with Mr. Galbraith, and a response to the HPR.

While The Friends of the Parks do not doubt the sincerity of your intentions, we continue to fear that in this case that this procedure is an afterthought rather than a respectful and necessary procedure.

Our concerns begin as a result of a meeting that we had with the staff of the City of Fort Wayne and other subsequent public meetings in the summer of 2008.

At the time, and only because we requested it, the “concept” of widening State Street at the bridge of Spy Run Creek, was presented to our board. The city staff presented the project, and though it was called a “concept” at the time, it bore no relation to what one would typically call a *conceptual* drawing except insofar as it was not a *detailed* drawing. Otherwise, the location, widening and straightening of State Boulevard were not conceptual, they were the FUTURE, and the staff felt very justified about taking the houses down, for this and for the flood control project on East Brook, referring to the “transient” nature of the people who live there, and suggesting that ultimately high rise apartments would be more suitable in the area. In point of fact, this thinking is supported by the traffic ideas included in the future North River Now Plan Final Report (the City does not yet own the property to be developed) prepared in 2007, which assumes the widening and straightening of State Boulevard, and other traffic connections to it. I do not include this information by way of criticism, but to underline that at the time it was already clear that a course of action had been decided upon and justified by more than one line of reasoning.

Thankfully we now have established that this place is important, and nationally so.

Despite the above being a bit of a historical digression, that which is pertinent to the current process and how it is carried out is that, **at that time, the concept was already in place and was unalterable, having over time been linked to future and current plans for the downtown, despite protests by the public, the recognition of the district's importance, and the requests of historic preservationists to reconsider. It also appeared tied to other disturbing projects that were underway.** City staff insisted that they had reviewed all suitable alternatives, and that this was the one that made the most sense. And, they noted, it was in keeping with other with other projects for the good of the community including the removal of homes for a flood control project on Eastbrook, and the changes for the bridge on US 27 which is now also involved in a Section 106 process.

Several months later, a contract to American StructurePoint was approved by City Council for an "Engineering Study". It was a contract for 1 million dollars. An engineering study, as one understands it, is a study about "how" to carry something out structurally, in this case, widen and straighten a road and build a new bridge, rather than a study to present alternatives to solve the identified problems. In other words, our concern is that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and, that, after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.

It is particularly appropriate to call this to your attention because of the nature of the historic property in question. There is a difference between "landscape" and "landscaping", which historic preservationists, planners, and traffic engineers in America are beginning to grapple with.

Brookview Neighborhood is a designed landscape, eligible for the National Register of Historic Places as the work of a Master, as the HPR notes, and therefore changes to the interconnecting landscape components should be thoroughly reviewed before any "concept" is put forward at all. In addition to noting that the bridge and the circulation patterns will be affected, as my colleagues have done, we would ask that the project developers respond to any potential changes to the following aspects of the landscape which can be found in the Federal Guidelines for the Treatment of Historic Landscapes:

1. The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?
2. The vegetation. What are the potential impacts on the vegetation of the designed landscape?

3. The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?
4. Vistas and Views
5. What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.

These have been mentioned, but it would be useful to have them further discussed as part of the HPR, before any sort of amelioration is discussed.

This particular historic resource is more than a series of houses of a certain age, which can be easily moved or walled off. It impossible to assume that one can add “landscaping” in the form of extra vegetation or a grass median to successfully ameliorate the transformation of a 2 lane curving road into a 5 lane straight road which is situated in the middle of a historic district known to be a designed cultural landscape, impossible to respond to the totality of the impact that this enormous change will have. “Landscaping” cannot compensate adequately the potential loss of the topography, vegetation, circulation, and visual character of this place, which is a “landscape.”

Further, in this situation, it is not suitable to offer as alternatives that which simply addresses how many yards the bridge might be moved in a certain direction, or what the ingress and egress options should be. Those are details of an established plan rather than real alternatives that address the question of landscape preservation.

Any plan to alter State Boulevard to solve the problems that this concept is intended to solve, problems which so far have not been set forth clearly, should be planned by a qualified expert in preserving historic landscapes.

We also respectfully request that there be consideration of the involvement of The Cultural Landscape Foundation in the proceedings.

We respectfully submit these comments.

Sincerely,

Julie Donnell
President
Friends of the Parks of Allen County, Inc.



Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



December 14, 2009

Hayley M. Steele
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Draft historic property report (The Westerly Group, September 2009) for the proposed State Boulevard Improvements (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed the materials with your cover letter dated November 9, 2009 and received on November 12 for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana. You had requested comments by December 11, but that would have allowed us and the other consulting parties fewer than 30 days in which to respond.

The federal agency official is to determine the area of potential effects ("APE") in consultation with the State Historic Preservation Officer (36 C.F.R. 800.4[a][a]). Your cover letter and page 4 of the historic property report ("HPR") indicate that a new alignment for State Boulevard to the west of Clinton Street will be proposed but that no final alignment has yet been identified. Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, especially in graphic form, to indicate where the alignments that are under consideration would run.

Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in an avoidance or minimization of the adverse effect.

Our initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.

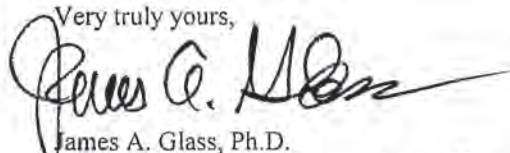
In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places with the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we note that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R.

44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 that took effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. Questions about buildings or structures please should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:ALJ-JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation
Camille Fife, The Westerly Group, Inc.
Larry Stillwell, Archaeological Consultants of Ossian
Michael Galbraith, ARCH, Inc.
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.



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MEETING MINUTES

Location: City of Fort Wayne, City-County Building, Room 128

Date: December 15, 2009 (Minutes revised on February 3, 2010)

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Hayley Steele, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Ken Nicolet (City of Fort Wayne)
Camille Fife (The Westerly Group)
John Carr, Amy Johnson (IDNR, Division of Historic Preservation and Archaeology)
Patrick Carpenter (INDOT, Cultural Resources)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Don Orban (Fort Wayne Historic Preservation Commission)
Annette "Jan" Dailey (Graduate Student, IPFW, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Albert Cohan (Westbrook 5, LLC)
Angie Quinn, Michael Galbraith (ARCH, Inc.)
Karl Dietsch (Brookview Neighborhood Resident)
Michelle Briggs-Wedaman, Scott Simmons (Brookview Neighborhood Association)
Dan Ernst (Earth Source, Inc.)

1. The meeting was held at 9:30 a.m., December 15, 2009, to discuss the State Boulevard Reconstruction Project (Project), the Historic Properties Report written by the Westerly Group, and future steps in the Section 106 process.
2. Rich Zielinski introduced himself and began the meeting with introductions around the room.
3. Scott Crites gave an overview of the Project, including the purpose and need, proposed improvements, four alternatives, and the project schedule (presentation is attached).
4. Camille Fife gave a presentation (attached), including information regarding:
 - a. Section 106 process to date
 - b. Definition of an Area of Potential Effect (APE)
 - c. State Boulevard project area
 - d. The Brookview-Irvington Historic District
 - e. A preliminary APE
5. John Shoaff (Fort Wayne City Council) expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.

6. Michelle Briggs-Wedaman (Brookview Neighborhood Association) also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.
7. Jan Daily (Brookview Neighborhood Resident) also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecks both down Huguenard and along State Boulevard.
8. Angie Quinn (ARCH) discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.
9. Michelle Briggs-Wedaman agreed with Angie about Shurcliff's park-like design. The park setting is one of the primary reasons people chose to live in the neighborhood and how it attracts residents.
10. ARCH questioned Dan Avery (NIRCC) about the alternatives considered and when they were considered for the State Boulevard corridor. Dan explained during the late 70s or early 80s there were a number of studies done that looked at how to improve east-west and north-south travel throughout the urban area. Based on different alternatives, discussions with the public, and a combination of impacts (neighborhoods, parks, homes, etc), State Boulevard was chosen as a prime corridor for improvements to east-west traffic.
11. ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.
12. John Carr asked about the extent of the alignment of State Boulevard that is within the APE as a resulting product of Kessler's plan and of Shurcliff's design. Discussion took place regarding Kessler's plan lacking specific design details and Shurcliff's final product that created State Boulevard. Patrick Carpenter (INDOT) clarified the Spy Run bridge has been determined eligible and is now going through the select/non-select determination. Patrick also gave a brief definition of select and non-select. Although the list has not been finalized, the State Boulevard over Spy Run bridge is deemed non-select right now. The finalized list will be available in March 2010.
13. Camille Fife and ARCH discussed AW Grosvenor being the bridge designer for the State Boulevard over Spy Run bridge. Camille explained AW Grosvenor was not discussed in depth in the Historic Properties Report (HPR) and ARCH expressed the importance of including this information in the HPR and in the documentation of the bridge.
14. Jan Daily asked whether the select/non-select criteria were based on vehicular use of the bridge. Patrick explained this was all part of the analysis of the bridge, and these issues are difficult with a bridge such as the Spy Run bridge because it is concrete. The bridge can not be disassembled or used in part for other projects.
15. Scott explained the bridge is between five and six feet below the 100-year floodplain elevation, and it is completely submerged during flood events. The flood elevation is actually based on the St. Mary's River, so the new bridge will be constructed at an elevation higher than the flood elevation, but will not affect the current flood elevation.
16. Camille discussed the District's eligibility under criteria A and C. ARCH discussed the US 27 project and its eligibility due to community planning, landscape architecture, and architecture. They feel the same issues will arise with the State Boulevard project regarding eligibility criteria and should be agreed upon before mitigation measures are set forth for the project.
17. John Carr clarified the differences between eligibility requirements for properties within a Multiple Property Listing and for individual properties within a district.

18. Camille discussed we anticipate an adverse effect to the Historic District, to State Boulevard, and to the bridge over Spy Run as a result of this project as it has been designed thus far; within the APE and outside of the district, there may be one property that could be adversely affected. She also discussed an eligible property and a property listed on the NR are evaluated on the same level and are treated as equally important resources.
19. American Structurepoint reiterated the fact State Boulevard is remaining in place and the majority of this Project is along existing alignment. The proposed State Boulevard will be realigned to the south of the existing between Terrace Road and Westbrook, allowing the existing State Boulevard to remain in place instead of being completely eliminated. The southern-most alignment was chosen based on anticipated impacts to the neighborhood. As you go north, the substandard curve and the site distance makes the curve not practical to use. The goal in choosing the current alignment was to limit the impacts to the existing State Boulevard and to impact as few houses as possible.
20. American Structurepoint gave an overview of future steps (more meetings, mitigation, memorandum of agreement) in the Section 106 process and how they will be addressed with consulting parties.

GENERAL CONCERNS

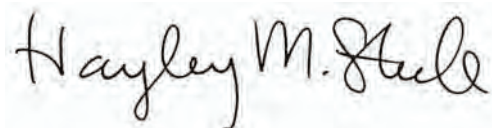
- The purpose and need presented at the meeting are different than those given in the HPR and at previous meetings.
- How will the purpose and need of the project be met with the widening and straightening of State Boulevard? ARCH and the Brookview Neighborhood Association do not feel the purpose and need are strong enough to warrant the project.
- Property values along State Boulevard could possibly decrease as a result of this project.
- Safety of the neighborhood should be a concern in the design of the project.
- The consulting parties would like a time to comment on information being presented as a result of this meeting.

ACTION ITEMS

- American Structurepoint will compile a packet of information including, but not limited to:
 - Purpose and Need of the Project
 - Traffic data
 - Flood studies
 - Alternatives analysis
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Future meetings will be scheduled as design is finalized and mitigation measures are to be discussed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or hsteele@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.



Hayley M. Steele

HMS:mgn

Enclosures



Indiana Department of Natural Resources

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Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



January 27, 2010

Hayley M. Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the meeting minutes that we received on December 28, 2009, for the aforementioned project in Fort Wayne, Allen County, Indiana.

The request we had made in our December 14, 2009 letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to the consulting parties.

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard. It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.

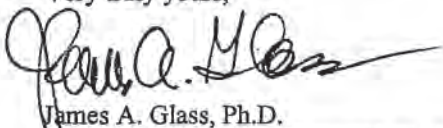
We believe it is important for FHWA to evaluate this project's purpose and need carefully before the Section 106 consultation proceeds much further. Although analysis of purpose and need has not expressly been made a part of the Section 106 review process, it seems to us that it would come into play once an adverse effect has been found formally (see 36 C.F.R. § 800.5), and the consultation turns to the consideration of alternatives that might avoid, minimize, or mitigate an adverse effect (see 36 C.F.R. § 800.6). If purpose and need are not clarified as much as possible now, and the Section 106 process proceeds through the assessment of adverse effects for only the four, very similar alternatives presented to the consulting parties, then we can foresee that by the time the process reaches the point at which it is appropriate to discuss alternatives that might avoid or minimize adverse effects, the applicant and the project planners might be reluctant to invest the time and expense that revisiting purpose and need might entail at that point. Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.

In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc



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M E M O R A N D U M

DATE: February 4, 2010
TO: Consulting Parties and Consulting Parties Meeting Attendees
FROM: Hayley Steele *HS*
RE: Follow-up Information Packet to Consulting Parties Meeting
CC: Scott Crites, Structurepoint

Per your request, enclosed please find a CD containing the City of Fort Wayne 2005 Flood Control Study as well as the traffic data collected for the State Boulevard project corridor.

Moreover, the request to re-evaluate the Purpose and Need is currently being coordinated with the Indiana Department of Transportation (INDOT), the Federal Highway Administration (FHWA), and the City of Fort Wayne. After coordination is complete the alternatives will once again be evaluated, and a determination will be made as to which alternative best meets the Purpose and Need. Once complete, this information will be sent to all consulting parties and attendees of the consulting parties meeting.

Also enclosed, please find the updated meeting minutes for the Consulting Parties meeting that took place on December 15, 2009. Please keep these final meeting minutes for your records.

Please feel free to contact me by phone at (317) 547-5580 or by email at hsteele@structurepoint.com if you have any questions or concerns regarding the attached information. We appreciate your cooperation in the development of this project.



Indiana Department of Natural Resources

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director

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March 10, 2010

Scott Crites
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Final minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903), the notice of the possible availability of the State Boulevard bridge over Spy Run Creek, the March 2005 "Spy Run Creek Flood Protection Study," and crash and traffic volume data, which was collected at various times between 2004 and 2008, inclusive, for various locations within and near the project area.

Dear Mr. Crites:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the aforementioned documents that accompanied your February 4, 2010 cover memorandum, all of which we received on February 8, 2010, for the aforementioned project in Fort Wayne, Allen County, Indiana.

Thank you for providing us with a copy of the final minutes for the December 15 meeting, which your firm modified in response to comments from Section 106 consulting parties.

As you may know, since the notice of the possible availability of the State Boulevard bridge over Spy Run Creek was prepared, the Indiana Department of Transportation has sent notification of the final lists of Select Bridges and Non-Select Bridges, pursuant to the Indiana Historic Bridges Programmatic Agreement. As your firm had anticipated, the State Boulevard bridge is on the list of Non-Select Bridges.

We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the area of potential effects ("APE") of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished by the buyout program and that some of the houses along Eastbrook Drive between State Boulevard and Clinton Street also have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard project, have they been flood-proofed, and what did that flood-proofing entail?

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled

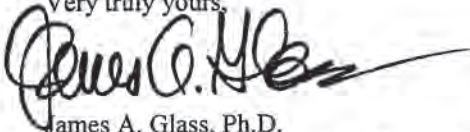
between those points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first blush like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak, weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion we are not sure. Moreover, it appears that a higher volume of traffic—more than 1,700 vehicles during that same hour—traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

The crash data also might yield a useful comparison. In 2008 the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be *realigned* appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned.. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have applied the data correctly. We would appreciate some assistance in that regard.

Another issue about which we are uncertain is whether it might be feasible to *reduce* the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program) that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Robert F. Tally, Jr., P.E., Indiana Division, Federal Highway Administration
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation
Scott Crites, American Structurepoint, Inc.
Hayley Steele, American Structurepoint, Inc.
Camille Fife, The Westerly Group, Inc.
Amy Johnson, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources
John Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources



AMERICAN
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INC.

May 19, 2011

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project and included in two separate letters received by our office. The DHPA letters (dated January 27, 2010, and March 10, 2010) were provided following a December 15, 2009, consulting parties meeting. Copies of the DHPA correspondence are included with this letter for your reference. The Section 106 process has been delayed until recently while we have been further developing the project Purpose and Need Statement in cooperation with INDOT and FHWA. On April 11, 2011, FHWA approved the revised Purpose and Need Statement (attached), allowing the NEPA and Section 106 process to proceed.

DHPA Letter to American Structurepoint, Inc. dated January 27, 2010

1) DHPA Comment - Page 1, Paragraph 2:

The request we had made in our December 14, 2009, letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to consulting parties.

Response:

The project Purpose and Need Statement has been significantly revised and was approved by FHWA and INDOT on April 11, 2011. The Purpose and Need Statement is included with this letter and will be provided to all consulting parties.

2) DHPA Comment – Page 1, Paragraph 3:

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at

Dr. James A. Glass
May 19, 2011
Page 2

Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

Response:

Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.

3) DHPA Comment – Page 1, Paragraph 4:

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.

Response:

The existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the curve radius for a 30 mph roadway is required to be a minimum of 300 ft. Several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.

The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

Dr. James A. Glass
May 19, 2011
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For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing reverse curve found on State Boulevard between Clinton Street and Cass Street. Specifically, the intersections of Eastbrook Drive and Terrace Road will be replaced and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.

4) DHPA Comment – Page 1, Paragraph 4:

It is our understanding that the section of State Boulevard in question is posted for a 20 mph speed limit, but the City of Fort Wane wishes to upgrade that section to a 30 mph limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.

Response:

The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph, which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

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As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

5) DHPA Comment – Page 2, Paragraph 2:

Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass Streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g. Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

Response:

Pursuant to 36 CFR Section 800.4(a)(1), the APE was drawn to encompass properties on both side of the undertaking and/or with a view shed of it. Generally, properties directly adjacent to the undertaking mask the view from the properties not adjacent. The APE was expanded or contracted based on visibility and the possibility of impact of the undertaking on properties within its view shed. Any indirect or cumulative impacts outside of the APE and Section 106 will be evaluated during the NEPA process.

6) DHPA Comment – Page 2, Paragraph 3:

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near the new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given

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to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

Response:

Consideration will be given to this request at the appropriate time.

DHPA Letter to American Structurepoint, Inc., dated March 10, 2010

1) DHPA Comment – Page 1, Paragraph 4:

We appreciate having the “Spy Run Flood Protection Study,” but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgemoor Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne’s flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?

Response:

The flood control study prepared by Christopher B. Burke Engineering, Ltd., for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary’s River.

The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.

The study identifies necessary additional information regarding the flooding issues of the St. Mary’s river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, “This flooding is caused primarily by backwater from the St. Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

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The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.

American Structurepoint is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.

2) DHPA Comment – Page 1, Paragraph 5:

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation; however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006 to 2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 p.m. to 6:00 p.m. That sounds at first like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not, we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

Response:

Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.

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3) **DHPA Comment – Page 2, Paragraph 2:**

The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clint Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which state Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.

Response:

Your interpretation of the traffic and crash data information mentioned in your letter is correct. It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.

4) **DHPA Comment – Page 2, Paragraph 3:**

Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

Response:


Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact

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residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.

It is our intent that the information provided in this letter, in combination with the attached FHWA-approved Purpose and Need Statement shall effectively address the comments and questions brought forth following the December 15, 2009, consulting parties meeting. We realize that a significant amount of time has passed since these comments were brought forth and we appreciate the patience of the DHPA and all consulting parties while we worked with INDOT and FHWA to establish a stronger Purpose and Need Statement. Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining to the project's Purpose and Need Statement, we respectfully request that you provide such comments within 30-days from receipt of this letter. Following this 30-day comment period, we will begin coordination to schedule a Consulting Parties meeting as the next step in continuing the Section 106 process. If I can be of assistance in the interim please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.


Brett W. Lackey
Environmental Scientist

BWL:alo

Enclosures

cc: Karie Brudis
Angie Quinn
Michael Galbraith
Don Orban
Todd Zeiger
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Shan Gunawardena
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Dan Avery
Suzanne Slick
Annette Dailey
Joyce Newland
John H. Shoaff
Patrick Carpenter
Camille Fife
Creager Smith
Albert Cohen
Thomas M. Niezer

 AMERICAN STRUCTUREPOINT, INC.

Dr. James A. Glass

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Ronald Ross

Dan Ernst

John Carr

Amy Johnson

**Friends of the Parks of Allen County, Inc.
PO Box 10152
Fort Wayne, Indiana 46850-0152**

June 14, 2011

Brett Lackey
Environmental Scientist
American Structure Point
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Mr. Lackey:

The Friends of the Parks appreciate being copied on your recent communication to Dr. James Glass dated May 19, 2011.

However, we are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.

Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communication with Doctor Glass, in a formal way under the auspices of whatever form the Section 106 Review continues.

Sincerely Yours,

Julie Donnell
President
Friends of the Parks of Allen County, Inc.

June 14, 2011

Cc: Dr. James Glass
Michael Galbraith

Todd Zeiger

Jill D. Downs
1202 Elmwood Ave.
Fort Wayne, IN 46805

June 15, 2011

RECEIVED
HIST. PRES. & ARCH.

JUN 17 2011

IDNR

Dr. James A. Glass
Deputy State Historic Preservation Officer
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, IN 46204

Dear Dr. Glass:

As chairperson of the Preservation Committee of ARCH, Inc., Allen County's historic preservation organization, I am writing in response to the letter dated May 19, 2011 from American Structurepoint to you regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

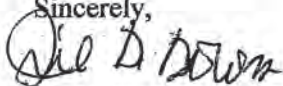
As a nearly lifelong resident of near northeast Fort Wayne, I travel this State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's recent poor attempt to justify its need.

In reading through the May 19 letter, American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than on moving traffic through the area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.

I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.

Thank you for your review of this issue.

Sincerely,



Jill D. Downs

cc: John L. Carr
Team Leader, Historic Structures Review
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
Telephone: 260-459-0231

June 16, 2011

Brett Lackey
American Structurepoint, Inc.
7260 Shadeland Station,
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I'm writing in response to your letter of May 19 to Dr. James Glass of the DHPA addressing questions and concerns posed by Dr. Glass.

First, let me note you said that a Purpose and Need Statement was attached, but I did not find one included with my copy.

In his letter Dr. Glass raises several important concerns, beginning with "it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion." Nor is it clear to me, even after reading your response.

For a three year period, when my mother, who lived on Westbrook Drive, was failing, I made the drive along that stretch almost every day, frequently at the rush hour. On the curve of State Street (as we have always called it), traffic always moved smoothly, but there was often (and always, at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.

Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.

Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the 'Arterial plus Bypass' concept" you refer to on Page 4. This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor", as you are already calling it, as if past errors justify future ones; you argue (bottom of Page 3) that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south - but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelthorn Road - which aligns with and neatly continues the east-west stretch of Coliseum.

Most seriously: while you repeatedly make the case for "continuity" for motorists who wish "to traverse the urban area" (bottom, Page 3), you totally ignore the welfare of the people who live within that area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values of the homes your corridor borders or traverses?

It is in the interest of the whole city to maintain the viability of all these neighborhoods, not only for the sake of the thousands of citizens who live there, but for all our citizens. By driving property values down, as your corridor inevitably will, total city property tax revenues will decline, and this decline will need to be made up elsewhere, or the city will be less able to properly maintain its infrastructure.

Furthermore, one of the prime advantages of living in Fort Wayne, the ability to live in serene, attractive, but affordable neighborhoods close to Downtown, will be diminished by the sum of all the neighborhoods compromised by your efforts to increase cross town traffic. You refer to "The redevelopment of the urban core area" (Page 3, 3rd paragraph from the bottom), something we all want; but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor. Housing stock will be lost or diminished in desirability; your project will end up merely making it a little easier drive across the city at the heavy price of making it less desirable to live in.

Your failure to consider any of these issues no doubt stems from the narrowness of the assignment you were given; but your own profession by now should understand the need to be sensitive to the context in which you design. ISTEA was passed in 1991, the Federal Highway Administration's handbook on *Flexibility in Highway Design* was published in 1995, and the AASHTO handbook on design flexibility (*A Guide for Achieving Flexibility in Highway Design*), which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert (Page 2, 3rd paragraph from the bottom) that standards appropriate for "highway design elements" apply to a road that abuts and serves a modest residential neighborhood!

I am aware that a majority of my colleagues on city council voted to fund your engineering work; but I think many of them thought they heard you, or the city representatives, say that several alternatives would be explored, after meaningful public input. What we finally saw was quite different: one plan, about which you were totally inflexible, with three trivial alternatives as to which neighborhood street would provide the access point to the newly straightened, raised, and lane-doubled thoroughfare. The choice of alternatives was equivalent to asking a condemned man which tree he wanted to be hanged by, and many of the citizens quite rightly felt patronized and insulted.

We on council now face an unpleasant two-fold task of fighting for a properly democratic, participatory process that was denied those citizens, and of fighting to preserve the quality of life and property values now threatened because of they were denied a proper process.

Times have changed, and the damage your plan in its present form will do is even greater than it would have been when it was conceived, back in the 1980s. We now have property tax caps: this means any deterioration of property values means a deterioration of tax revenue we cannot afford. Furthermore, we must enhance the quality of life in our neighborhoods if we are to successfully sell our community as a good place to live and work. The casualness, indifference, and shortsightedness we have shown in the past must be corrected now, if we are to compete

successfully with the progressive cities that have already changed their ways and avoided or even removed corridors that have undermined urban cores and neighborhoods.

I am aware that I address issues that go beyond those directly relevant to your field, Environmental Science (which should be raising issues I haven't even touched on), but it must say something about the attitudes of your principals that they did not see these issues and address them themselves.

Sincerely yours,
John H. Shoaff,
Fort Wayne City Councilman At-large

cc: Dr. James Glass
Consulting parties to the State Boulevard Reconstruction
Members of the Fort Wayne City Council

Lackey, Brett

From: Carr, John [JCarr@dnr.IN.gov]
Sent: Thursday, June 16, 2011 12:12 PM
To: Lackey, Brett
Cc: Hope, Briana; Tharp, Wade
Subject: RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Brett,

Yes, I think 15 days from the consulting parties' receipt of the P&N document is a sufficient extension of time. When do you anticipate they'll receive it?

John L. Carr
IDNR-DHPA
Phone: (317) 233-1949
Fax: (317) 232-0693
E-mail: JCARR@DNR.IN.GOV

Take our survey at www.IN.gov/dnr/historic to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]
Sent: Thursday, June 16, 2011 11:31 AM
To: Carr, John
Cc: Hope, Briana
Subject: RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

John,

Thank you for the information. To clarify on your comment below regarding extending the comment period: do you feel that an extension of 15-days (from receipt of the P&N) would be appropriate or should be give another full 30 days? Any thoughts on the matter would be appreciated.

Thanks

Brett Lackey
American Structurepoint, Inc.

-----Original Message-----

From: Carr, John [mailto:JCarr@dnr.IN.gov]
Sent: Thursday, June 16, 2011 10:55 AM
To: Lackey, Brett
Cc: Slider, Chad; Glass, James
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

Brett,

Page 8 of the 9-page, May 19 letter we received from you refers to the "the attached FHWA-approved Purpose and Need Statement," but the only document in the submission we have is the 9-page letter, itself. So, no, we had not received the P&N document until now.

BTW, I haven't yet had a chance to study that submission closely, but we had intended to comment on it within 30 days of having received it (i.e., no later than June 22). Since we just now received the P&N statement, and since the consulting parties apparently hadn't seen it yet, either, it might be appropriate to extend the comment period somewhat.

John L. Carr
Team Leader for Historic Structures Review Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 W. Washington St., Room W274
Indianapolis, IN 46204
Ph. No.: 317-233-1949 Fax No.: 317-232-0693

Take our survey at www.IN.gov/dnr/historic to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]
Sent: Thursday, June 16, 2011 9:36 AM
To: Carr, John
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

John,

Could you let me know whether or not the DHPA received the revised Purpose and Need Statement (attached) which was to be included with the May 19, 2011 letter? We intended for this to be sent to all consulting parties...

Thanks

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
317.850.0257 | cell
www.structurepoint.com

-----Original Message-----

From: Todd Zeiger [mailto:TZeiger@indianalandmarks.org]
Sent: Thursday, June 16, 2011 9:02 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; John Carr
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision. Checking with other consulting parties they did not either. If it is the intention that the consulting parties were supposed to take the receipt of this carbon copy letter as our opportunity to re-engage in the 106 review process on this project (and I would add an unusual way to conduct a 106 review) then receipt of the revised purpose and need statement would be of utmost importance.

I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period. If it was in fact the intention of American Structurepoint that receipt of the copy of the May 19, 2011, letter to the Indiana Division of Historic Preservation and Archaeology serves as our opportunity to provide substantive comments, we request a 30 day extension - following the receipt of the revised purpose and need statement and replies to our other concerns and comments. If it was the intention that receipt of a copy of correspondence between the consulting engineer and the Indiana Division of Historic Preservation and Archaeology serves as consulting parties opportunity to comment we respectfully disagree with that process and request direct and professional engagement with all the parties involved. This is a significant project with the potential for profound change to historic neighborhoods and the utmost care needs to be taken to follow the process appropriately.

Thank you for your attention to this matter.

Todd Zeiger
Director
Northern Regional Office
Indiana Landmarks

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<http://www.emaildisclaimers.com/>

Lackey, Brett

From: Todd Zeiger [TZeiger@indianalandmarks.org]
Sent: Thursday, June 16, 2011 9:02 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; John Carr
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Importance: High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision. Checking with other consulting parties they did not either. If it is the intention that the consulting parties were supposed to take the receipt of this carbon copy letter as our opportunity to re-engage in the 106 review process on this project (and I would add an unusual way to conduct a 106 review) then receipt of the revised purpose and need statement would be of utmost importance.

I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period. If it was in fact the intention of American Structurepoint that receipt of the copy of the May 19, 2011, letter to the Indiana Division of Historic Preservation and Archaeology serves as our opportunity to provide substantive comments, we request a 30 day extension - following the receipt of the revised purpose and need statement and replies to our other concerns and comments. If it was the intention that receipt of a copy of correspondence between the consulting engineer and the Indiana Division of Historic Preservation and Archaeology serves as consulting parties opportunity to comment we respectfully disagree with that process and request direct and professional engagement with all the parties involved. This is a significant project with the potential for profound change to historic neighborhoods and the utmost care needs to be taken to follow the process appropriately.

Thank you for your attention to this matter.

Todd Zeiger
Director
Northern Regional Office
Indiana Landmarks



AMERICAN
STRUCTUREPOINT
INC.

June 17, 2011

James A. Glass, PhD
Deputy State Historic Preservation Officer
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Dr. Glass:

On May 19, 2011, American Structurepoint, Inc., sent a letter to the Division of Historic Preservation and Archaeology regarding the State Boulevard project. It was recently brought to our attention that the revised project Purpose and Need Statement referenced in the letter was inadvertently omitted from that mailing. Please find the Purpose and Need Statement enclosed with this letter. We apologize for this oversight, and at this time we would like to extend the comment period for an additional 15 days from the receipt of this letter.

Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining, we respectfully request that you provide such comments within 15 days from receipt of this letter. If there are any questions or comments, please contact me at (317) 547-5580 or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Brett W. Lackey
Environmental Scientist

BWL:cgh

Enclosures

IN20071404



Dr. James A. Glass

June 17, 2011

Page 2

cc: Karie Brudis
Angie Quinn
Michael Galbraith
Don Orban
Todd Zeiger
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Shan Gunawardena
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Dan Avery
Suzanne Slick
Annette Dailey
Joyce Newland
John H. Shoaff
Patrick Carpenter
Camille Fife
Creager Smith
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst
John Carr
Amy Johnson

State Boulevard Project DES#0400587

The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E

Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E

Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element. According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.

Curve Radius Table:

Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)
18+66.60	175 ft	300 ft
24+64.47	243 ft	300 ft
27+23.73	210 ft	300 ft

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway's safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a critical Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 ft along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 ft along State Boulevard to safely make the right turn maneuver. As noted in the "Intersection Sight Distance Table" below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard. and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11
State Boulevard. and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38
State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

Lackey, Brett

From: Michael Galbraith [mgalbraith@archfw.org]
Sent: Friday, June 17, 2011 10:46 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; Todd Zeiger; jcarr@dnr.in.gov; Jill Downs; John Shoaff; Julie Donnell; Michelle Briggs-Wedaman
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Dear Dr. Glass and Mr. Lackey-

I am writing today concerning the May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587; DHPA No. 5903) in Fort Wayne, Allen County, Indiana. I have a number of concerns and questions.

1. I am unsure how this letter fits into the Section 106 process and the ability of Consulting Parties and the public to provide meaningful comment and criticism. Although I contacted Mr. Lackey by phone asking whether comment would be accepted from the Consulting Parties in regard to this letter, my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as mere footnote. Our Dec. 8, 2009, comments regarding the HPR and our comments from the initial Consulting Parties (Dec. 15, 2009) remain unaddressed. I request that our concerns and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed, and that we be given time in which to respond to those answers.
2. It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project Purpose and Need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in Purpose and Need.

Michael Galbraith
Preservation Specialist, ARCH, Inc.
818 Lafayette Street, Fort Wayne, IN 46802
mgalbraith@archfw.org
260.4265117

Lackey, Brett

From: Julie Donnell [juliemarie57@earthlink.net]
Sent: Friday, June 17, 2011 11:15 AM
To: Lackey, Brett; James Glass
Cc: Mike Galbraith; Todd Zeiger; Jill Downs; Michelle Briggs Wedaman
Subject: State Boulevard Reconstruction Project (Des. No. 0400587; Project No: IN20071404
Attachments: Structurepoint response 6.13.11.doc

Dear Dr. Glass and Mr. Lackey;

Attached is a letter which outlines the Friends of the Parks' concerns about the the recent communication between DHPA and Structurepoint, which I recieved a copy of earlier in the month.

In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.

We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.

My formal letter is attached.

Sincerely,
Julie Donnell
President, Friends of the Parks of Allen County, Inc.

Julie Donnell
juliemarie57@earthlink.net
EarthLink Revolves Around You.

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

July 1, 2011

Brett Lackey
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I am now in receipt of the Purpose and Need (P and N) statement which you intended but failed to include in your letter of May 19, 2011 to Dr. James Glass. It presents a problem: there are there material discrepancies between the accident counts in your new P and N statement and those sent by Hayley Steele of your firm to the Historic Preservation Specialist (now executive director) of ARCH on February 4, 2010 and also sent by the City of Fort Wayne to the Brookview Neighborhood Association president. Discrepancies occur in every one of the accident totals for the intersections of State Boulevard at Spy Run, Clinton Street, Eastbrook Dr., and Westbrook Dr. in the years 2007 and 2008.

In all but 2 of the 8 instances, your figures, relative to the Feb. 4, 2010 and City accident reports, raise the RMV (Rate per Million entering Vehicles) from 2.0 or below (typically well below) - the threshold above which there is a safety problem - to a number above 2.0. The tables you sent in February 2010 and the table that is part of the new P and N statement are included. A summary of the discrepancies:

	<u>Feb. 2010/City Reports</u>	<u>New P & N Figures</u>
State and Westbrook: 2007	7 accidents;	16 accidents
2008	11	17
State and Eastbrook; 2007	3	17
2008	6	17
State and Clinton: 2007	28	41
2008	33	49
State and Spy Run; 2007	36	34
2008	33	35

Feb. 4, 2010/

Your discrepancies are remarkably consistent in providing support for the argument that you are clearly so eager to make but that the 'city figures will not support, namely that, based on RMVs, there are serious safety problems at the four intersections. Only at the Spy Run intersection do the RMVs remain similar. Calculating from your data, the RMVs, when corrected for the Feb.4, 2010/City figures, work out to (in the order listed above, 2007 listed before 2008): State and Westbrook, 1.3, 1.5.; State and Eastbrook, .43, .92; State and Clinton, 1.9, 2.2; State at Spy Run, 2.16, 2.0.

Two points are noteworthy: the RMVs generated by your figures are much higher than the city's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning of and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.

With respect to the Spy Run and Clinton intersections, it strains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part; namely, to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.

The discrepancies in the accident figures raise questions that are both obvious and serious. I await your response.

Sincerely yours,


John H. Shoaff,
Fort Wayne City Councilman At-large

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
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State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16

Intersection of State Blvd. & Cass St.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1	1		3				1	
Rear End	2			1			2			2		
Left-Turn	1						2			1		
Right-Turn												
Side-Swipe										1	1	
Head On												
Off Road				1								
Pedestrian/Bicycle												
Total	4	0	0	3	1	0	7	0	0	4	2	0
Total per Year	4			4			7			6		

Crash Rate (Crashes per MEV)	0.74
Number of years	4
Entering Vehicles	19,500
Total Accidents	21

Intersection of State Blvd. & Westbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1			3			1		
Rear End	1			2	1		2	1		1	1	
Left-Turn												
Right-Turn												
Side-Swipe	2						1			2		
Head On		1								4	2	
Off Road	3			1			2					
Pedestrian/Bicycle												
Total	7	1	0	4	1	0	8	1	0	8	3	0
Total per Year	8			5			9			11		

Crash Rate (Crashes per MEV)	1.25
Number of years	4
Entering Vehicles	18,070
Total Accidents	33

Intersection of State Blvd. & Eastbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	2			2						1	1	
Rear End							1			1		
Left-Turn										1		
Right-Turn												
Side-Swipe												
Head On	1							1				
Off Road	1						1			3		
Pedestrian/Bicycle												
Total	4	0	0	2	0	0	2	1	0	5	1	0
Total per Year	4			2			3			6		

Crash Rate (Crashes per MEV) 0.36
Number of years 4
Entering Vehicles 17,350
Total Accidents 9

Intersection of State Blvd. & Oakridge Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End	2			3			2					
Left-Turn												
Right-Turn												
Side-Swipe				1								
Head On												
Off Road												
Pedestrian/Bicycle												
Total	2	0	0	3	1	0	2	0	0	0	0	0
Total per Year	2			4			2			0		

Crash Rate (Crashes per MEV) 0.32
Number of years 4
Entering Vehicles 17,250
Total Accidents 8

Intersection of State Blvd. & Terrace Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End										1		
Left-Turn												
Right-Turn												
Side-Swipe												
Head On	1											
Off Road												
Pedestrian/Bicycle												
Total	1	0	0	0	0	0	0	0	0	1	0	0
Total per Year	1			0			0			1		

Crash Rate (Crashes per MEV)	0.08
Number of years	4
Entering Vehicles	17,250
Total Accidents	2

Intersection of State Blvd. & Clinton St. (SB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	4	3		8	1		7	1		3	3	
Rear End	15	2		10	1		10	1		13	2	
Left-Turn	3			2	1		2			2	1	
Right-Turn	1									1		
Side-Swipe	5			2			3	2		6		
Head On	1							1				
Off Road							1			2		
Pedestrian/Bicycle												
Total	29	5	0	22	3	0	23	5	0	27	6	0
Total per Year	34			25			28			33		

Crash Rate (Crashes per MEV)	2.00
Number of years	4
Entering Vehicles	41,000
Total Accidents	120

Intersection of State Blvd. & Spy Run (NB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	3	2		3			3			1	4	
Rear End	13			13	1		17	2		15	3	
Left-Turn	3	2		2	2		6	1		2		
Right-Turn				2								
Side-Swipe	10	1		6			6			6		
Head On										1		
Off Road	1						0	1		1		
Pedestrian/Bicycle		1		1	2							
Total	30	6	0	27	5	0	32	4	0	25	8	0
Total per Year	36			32			36			33		

Crash Rate (Crashes per MEV)	1.91
Number of years	4
Entering Vehicles	49,250
Total Accidents	137

West 200 block of State Blvd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PI	F	
Right Angle												
Rear End	1						1					
Left-Turn												
Right-Turn												
Side-Swipe							2					
Head On										1		
Off Road	2	1					1					
Pedestrian/Bicycle												
Total	3	1	0	0	0	0	4	0	0	0	1	0
Total per Year	4			0			4			1		

Crash Rate (Crashes per 100 Million VM)	3.14.33
Length of Segment	0.12
Number of years	4
ADT	16,500
Total Accidents	9

Division of Historic Preservation & Archaeology 402 W. Washington Street, W274 Indianapolis, IN 46204-2739
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



July 5, 2011

Brett W. Lackey
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Response to SHPO's January 27, 2010, and March 10, 2010, letters and revised purpose and need statement concerning the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated May 19, 2011 and received on May 23 and the revised purpose and need statement submitted with your cover letter dated June 17, 2011 and received on June 20, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Thank you for responding the questions we had raised in our earlier letters. We continue to think, however, that it might be appropriate to expand the Section 106 area of potential effects ("APE"), if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. You indicated on page 4 of your May 19 letter that the proposed APE was based on view sheds of State Boulevard from nearby properties and that indirect impacts outside the APE will be evaluated during the NEPA process. We believe that any reasonably foreseeable direct or indirect effect should be taken into consideration in determining the APE (36 C.F.R. § 800.16[d]). Certainly, view sheds of areas where demolition or construction will take place are appropriate considerations (see 36 C.F.R. § 800.5[a][2][v]), but we think that areas where the character or use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well (see 36 C.F.R. § 800.5[a][2][iv]).

A case in point may be found in one of the rationales given in the revised purpose and need statement in support of the need for this project: "As the Transportation Plan [for the Fort Wayne Urbanized Area] has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor" (page 4). Improvements listed there include widening the St. Joseph River bridge east of Spy Run Avenue, and widening State Boulevard between Spy Run and Cass Street, between Goshen Avenue and Cass, and between Coliseum Boulevard and Maplecrest Road. In retrospect, it seems to us that, given those improvements to the west and east of the project area under review here—especially those closest to this project area—it might have been foreseeable that there eventually would be a perceived need to improve the two-lane stretch of State Boulevard between Cass Street and Spy Run. In a similar way, it seems to us that it now may be foreseeable that traffic patterns on side streets to the north and south of State Boulevard would change and that those changes could affect historic properties. Consequently, we again request that consideration be given to expanding the Section 106 APE, even if such changes in traffic patterns also would be evaluated in the NEPA process.

We have found no basis to challenge the conclusions of the purpose and need statement. We assume that the facts stated there are accurate, and we have no knowledge of any other information that would contradict the conclusions drawn on them. Because you have indicated that the purpose and need statement has been developed with FHWA and Indiana

Department of Transportation involvement and has received FHWA's approval, we assume, therefore, that those agencies believe that the revised purpose and need statement is persuasive. We have been copied on at least some of the consulting parties' responses to your May 19 letter, and they have identified a number of historic preservation, economic, pedestrian safety, and neighborhood livability values that they believe should be promoted. The revised purpose and need statement identifies the transportation mobility, connectivity, and accessibility and traffic safety values that the City of Fort Wayne wishes to promote. The Section 106 process is not designed to choose among those values; rather, it is intended to ensure that a reasonable opportunity is provided for consulting parties and the public to express their views on effects on historic properties and that the federal agency responsible for the project takes those effects into consideration and gives the Advisory Council on Historic Preservation an reasonable opportunity to comment.

The revised purpose and need statement indicates that two other, possible east-west corridors also were considered, but the State Boulevard Corridor was considered the most practical. It goes on to indicate that these three alternatives will be discussed in the NEPA process. We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevant to the Section 106 process, because 36 C.F.R. § 800.6(a) requires that there be consultation about alternatives that might avoid, minimize, or mitigate adverse effects on historic properties. The alternatives presented to the consulting parties so far appear to be minor variations on the State Boulevard Corridor alternative. It is unclear to us how the alternatives analysis for NEPA and Section 106 purposes for this project as a whole would dovetail with Stipulation III.B. of the Historic Bridges PA, which would seem to apply specifically, and perhaps exclusively, to the Non-Select State Boulevard Bridge over Spy Run. Perhaps FHWA can clarify how the alternatives analysis should be handled.

Some of the consulting parties have expressed concerns that although you have responded to the Indiana SHPO's previous questions, not all of theirs have been addressed. In light of these comments, you may want to review previous correspondence from the consulting parties and the minutes of the December 15, 2009 consulting parties meeting and make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 19 letter. Sometimes in Section 106 consultations, there is a tendency to give greater weight to the Indiana SHPO's concerns than to those submitted by other consulting parties. While the 36 C.F.R. Part 800 regulations assign a somewhat different role to the SHPO than to the other consulting parties, the regulations also provide for the federal agency to consider issues and concerns raised by other consulting parties (see 36 C.F.R. §§ 800.5[c][2][i] and 800.6[a]).

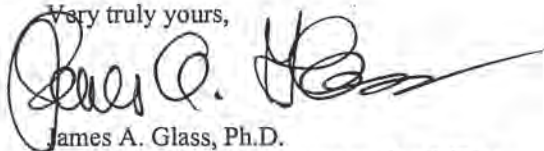
It occurs to us that not all of the consulting parties may be aware of comments being offered by other consulting parties. We are aware that the usual practice is to provide copies of consulting party comments in the documentation that is provided with the finding. However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in response to your May 19 and June 17 letters.

As previously stated in our December 14, 2009, letter to Hayley M. Steele, Environmental Specialist at American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Patrick Carpenter, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Brett W. Lackey, American Structurepoint, Inc.

Lackey, Brett

From: Lackey, Brett
Sent: Wednesday, July 06, 2011 3:17 PM
To: 'aquinn@archfw.org'; 'mgalbraith@archfw.org'; 'tmn@barrettllaw.com'; 'mbwedaman@verizon.net'; 'shan.gunawardena@ci.ft-wayne.in.us'; 'Creager Smith'; 'joyce.newland@dot.gov'; 'jshoaff@proparkwest.com'; 'don.orban@cityoffortwayne.org'; 'juliemarie57@earthlink.net'; 'jandailey59@msn.com'; 'tzeiger@indianalandmarks.org'; 'indianabridges@sbcglobal.net'; 'Carpenter, Patrick A'; Kennedy, Mary; 'sjslick@mac.com'; 'ross@martin-riley.com'; 'danavery@co.allen.in.us'; 'albertcohan@aol.com'; 'jcooper@ccrtc.com'; 'dan@earthsouceinc.net'; 'jcarr@dnr.in.gov'; 'wtharpe1@dnr.in.gov'
Cc: Hope, Briana
Subject: State Boulevard - Section 106 Comments
Attachments: IN20071404.EV.2011-04-21.LTR.Response to SHPO Letters.bwl.pdf; State Blvd P&N Statement to CPs.pdf

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who still intend to comment, please provide comments by next Tuesday July 12th. Please let me know if there are any questions.

Thanks!

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
317.850.0257 | cell
www.structurepoint.com

From: Suzanne [sjslick@me.com]
Sent: Wednesday, July 06, 2011 4:18 PM
To: Lackey, Brett; mayor@ci.ft-wayne.in.us
Cc: aquinn@archfw.org; mgalbraith@archfw.org; tmn@barrettlaw.com; mbwedaman@verizon.net; shan.gunawardena@ci.ft-wayne.in.us; Creager Smith; joyce.newland@dot.gov; jshoaff@proparkwest.com; don.orban@cityoffortwayne.org; juliemarie57@earthlink.net; jandailey59@msn.com; tzeiger@indianalandmarks.org; indianabridges@sbcglobal.net; Carpenter, Patrick A; Kennedy, Mary; rross@martin-riley.com; danavery@co.allen.in.us; albertcohan@aol.com; jcooper@ccrtc.com; dan@earthsouceinc.net; jcarr@dnr.in.gov; wtharpe1@dnr.in.gov; Hope, Briana
Subject: Re: State Boulevard - Section 106 Comments

Dear Brett,

I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State Boulevard correspondence. It seems we are missing a wonderful opportunity to create a beautiful and useful roadway system in this project.

What we will have is a massive concrete thoroughfare that will be unfriendly to pedestrians and bicyclists and probably to drivers, as well. There is no attempt at traffic calming, but a great emphasis on traffic rushing. There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability. Other cities are making great strides in building roadways that are user friendly, and safe for everyone -- not just car and truck drivers -- and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.

Very sincerely,
 Suzanne Slick
 Irvington Park Consulting Party
 Fort Wayne

On Jul 6, 2011, at 3:16 PM, Lackey, Brett wrote:

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who

still intend to comment, please provide comments by next Tuesday July 12th. Please let me know if there are any questions.

Thanks!

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
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317.547.5580 | office
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<http://www.emaildisclaimers.com/><IN20071404.EV.2011-04-21.LTR.Response to SHPO Letters.bwl.pdf><State Blvd P&N Statement to CPs.pdf>

Lackey, Brett

From: Michelle Briggs Wedaman [mbwedaman@frontier.com]
Sent: Thursday, July 07, 2011 9:05 AM
To: Lackey, Brett
Subject: State Blvd Project Ft Wayne contact info update Brookview Neighborhood

Importance: High

7/7/2011

Hi Brett,
Your 7/6/11 email was forwarded to me by another consulting party member.
Could you update my email from the Verizon to:
mbwedaman@frontier.com

I plan on submitting comment on behalf of the neighborhood.

Thanks,
Michelle Briggs Wedaman
Brookview Neighborhood president, Ft Wayne
260.710.4413



**State Boulevard Section 106 Agency Coordination Meeting
MEETING MINUTES**

Location: INDOT Central Office Room N642
Date: 7/13/2011
Project Name: State Boulevard Reconstruction
Project No.: IN20071404
Owner: City of Fort Wayne
Attendees: John Carr – IDNR DHPA
Scott Crites – Structurepoint
Jim Glass – IDNR DHPA
Briana Hope – Structurepoint
Jason Kaiser – INDOT Fort Wayne District
Mary Kennedy – INDOT Cultural Resources Office
Brett Lackey – Structurepoint
Ben Lawrence – INDOT Office of Environmental Services
Joyce Newland – FHWA
Amanda Ricketts – IDNR DHPA
Greg Smith – INDOT Fort Wayne District
Wade Tharp – IDNR DHPA
Minutes By: Brett Lackey

The following notes reflect our understanding of the discussions and decisions made at this meeting.
If you have any questions, additions, or comments, please contact the issuer of these minutes.

ITEMS DISCUSSED:

- ❖ The meeting opened with introductions and an overall project update by American Structurepoint. Structurepoint also explained that the purpose of the agency coordination meeting was to discuss the following items:
 - SHPO's comments on recent Purpose and Need submission
 - How to address Consulting Parties comments
 - Section 4(f)
 - Inviting ACHP involvement
- ❖ As requested in the July 5, 2011 DHPA letter the re-evaluation of extending the APE to the north to accommodate the potential for added traffic through that neighborhood was discussed.
 - American Structurepoint explained that the project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. American Structurepoint also stated that it is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project.
 - DHPA requested that this be stated in a formal response to the July 5, 2011 letter and copied to all consulting parties.

- ❖ DHPA also asked about consulting parties request to extend the APE east and west.
 - American Structurepoint explained that while some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE.
 - INDOT Fort Wayne District added that the travel patterns in the Fort Wayne area are well established and that it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.
- ❖ DHPA asked about the other alternatives discussed in the revised Purpose and Need.
 - American Structurepoint explained that two additional corridors (Butler Rd-Vance Rd and Spring St-Tennessee Ave) were considered and discarded due to the need for new roadway alignments, relocations, historical impacts, park impacts, and potential hazardous waste impacts.
 - All agencies agreed that more details were needed for the other two east-west corridors studied.
 - Strong discussion of alternatives will be included in both the NEPA document and 4(f) document. American Structurepoint pointed out that consulting parties will have the opportunity to review the alternatives analysis as part of the 4(f) process. American Structurepoint will discuss the alternative analysis in more detail as part of the next consulting parties meeting.
- ❖ DHPA suggested that American Structurepoint coordinate with their National Register experts to determine if the project would result in a need to change the district boundaries.
- ❖ DHPA asked if all consulting party comments had been addressed.
 - American Structurepoint commented that the significant comments related to the Section 106 Process were addressed in the May 19, 2011 DHPA response letter.
 - DHPA suggested that American Structurepoint more specifically address the consulting parties issues and comments in coordination specifically addressed to the consulting parties.
 - Structurepoint suggested creating a spreadsheet identifying each consulting party and their specific comment with a response to the comment. Structurepoint committed to sending this document to consulting parties with the invitation to the next consulting parties meeting.
- ❖ American Structurepoint suggested FHWA invite the Advisory Council on Historic Preservation (ACHP) to participate at this time since the project seems to be controversial.
 - All agencies agreed ACHP should be invited to participate in the State Boulevard project now, rather than later in the Section 106 process.

ACTION ITEMS:

- ❖ Structurepoint will respond to the idea of extending the APE in writing via letter to DHPA.
- ❖ Structurepoint will formally request, via letter, FHWA coordinate with ACHP and request their involvement on the project at this time.
- ❖ Structurepoint will produce a chart with questions/answers that addresses all consulting party and agency comments received to-date – this will be provided to all consulting parties prior to next consulting party meeting.

NEXT MEETING:

Consulting Party Meeting (Date TBD)

cc: Attendees
Consulting Parties

Very truly yours,
American Structurepoint, Inc.

A handwritten signature in black ink, appearing to read 'B. Lackey', with a stylized flourish extending to the right.

Brett Lackey
Environmental Scientist



M E M O R A N D U M

DATE: August 15, 2011

TO: Ms. Angie Quinn, ARCH Inc.
Ms. Jill Downs, ARCH Inc.
Mr. Michael Galbraith, ARCH Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Indiana Landmarks
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Civic Neighborhood Association
Dr. James Cooper
Mr. Paul Bandenburg, Indiana Historic Spans Task Force
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Susan Haneline, Brookview Civic Neighborhood Association
Mr. Dan Avery, Northeastern Indiana Regional Coordination Council
Ms. Suzanne Slick, Irvington Park Neighborhood Association
Ms. Jan Dailey, Brookview Civic Neighborhood Association
Ms. Joyce Newland, Federal Highway Association
Mr. John Shoaff, Fort Wayne City Council
Mr. Jason Kaiser, INDOT Fort Wayne District
Mr. Patrick Carpenter, INDOT Cultural Resources
Ms. Mary Kennedy, INDOT Cultural Resources
Ms. Camille Fife, Westerly Group
Mr. Creager Smith, Fort Wayne Office of Planning and Policy
Mr. Albert Cohan, Westbrook 5, LLC
Mr. Thomas Niezer, Barrett & McNagney, LLP
Mr. Ronald Ross, Martin Riley Architects and Engineers
Mr. Dan Ernst, Earth Source Inc.
Dr. James Glass, IDNR DHPA
Mr. John Carr, IDNR DHPA
Ms. Amy Johnson, IDNR DHPA
Ms. Amanda Rickets, IDNR DHPA
Mr. Wade Tharp, IDNR DHPA
Mr. Tom Cain, Fort Wayne Redevelopment

FROM: Brett W. Lackey, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for Thursday, September 1 at 9:30 am. The meeting will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030 located in the Garden Level of Citizens Square.

Please review the enclosed materials prior to the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at blackey@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Agenda
Agency Coordination Meeting Minutes (7/13/2011)
Letter to IDNR DHPA
Letter to FHWA
Historic Bridge Programmatic Agreement Alternatives Analysis
Individual Section 4(f) Alternatives Analysis
Corridor Alternatives Map
Consulting Party Questions/Comments and Responses

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

Thursday, September 1, 2011
9:30 AM
Room 030 (Garden Level)
Citizens Square
200 East Berry Street
Fort Wayne, IN 46802

1. Project Update
 - a. Purpose and Need
 - b. Consulting Party Comments/Responses
2. Project Alternatives Review
 - a. Minimization Measures
3. Future Steps in the Process
 - a. Potential Mitigation Measures
 - b. Development of Memorandum of Agreement
4. Follow-up items



AMERICAN
STRUCTUREPOINT
INC.

August 15, 2011

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN200701404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated July 5, 2011.

The letter states that the DHPA believes that it may be appropriate to expand the APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. The proposed project will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access, but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut-through traffic. The project would likely draw current cut-through traffic out of the neighborhoods, because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established, and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor. The traffic pattern alteration is not anticipated to result in any adverse impacts.

Your letter also indicates that a discussion of the other two possible east-west corridor alternatives mentioned in the project's purpose and need statement would be relevant to the Section 106 process. Alternatives have continued to be developed throughout the Section 106 process and an alternatives analysis will be provided to all consulting parties prior to the next (second) consulting parties meeting. Please see the following descriptions of the two possible east-west corridor alternatives:

Alternative 1: Butler Road – Vance Road Corridor

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard. This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land. This alternative avoids impacts to historic properties identified within the APE of the project; however, the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District, as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue Bridge over the St. Joseph River, a select historic bridge which has been determined eligible for the National Register of Historic Places.



Dr. James A. Glass
August 15, 2011
Page 3

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park. This alternative avoids impacts to historic properties identified within the APE of this project; however, the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Your July 5, 2011, letter also expresses concern that not all consulting parties feel that their comments have been adequately addressed. The letter also indicates that not all consulting parties are aware of comments being provided by other consulting parties. In an effort to both address individual comments and distribute comments amongst consulting parties, we have created a table with all comments received thus far, as well as individual responses to all comments. This document shall be distributed to all consulting parties for review prior to the next consulting parties meeting.

Concern regarding how the project may affect the proposed boundaries of the Brookview – Irvington Park National Register District has also been expressed. As such, at this time we respectfully request the DHPA's assistance in soliciting the opinion of the appropriate National Register authority regarding the potential for the project to impact the currently proposed boundaries of the Brookview – Irvington Park National Register District.

We hope that the information in this letter adequately addresses the concerns expressed in the July 5, 2011, DHPA letter. The information herein will be provided to all consulting parties prior to the next consulting parties meeting and will also be discussed during that meeting. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Brett W. Lackey
Environmental Scientist

BWL:alo

Enclosures

cc: John Carr
Wade Tharp
Amanda Ricketts
Mary Kennedy
Patrick Carpenter

Dr. James A. Glass

August 15, 2011

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Staffan Peterson, Ph.D.
Jason Kaiser
Greg Smith
Joyce Newland
Dan Avery
Creager Smith
Shan Gunawardena
Camille Fife
Angie Quinn
Michael Galbraith
Jill Downs
Todd Zeiger
Don Orban
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Suzanne Slick
Annette Dailey
John Shoaff
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst



AMERICAN
STRUCTUREPOINT
INC.

August 15, 2011

Ms. Joyce Newland
Planning/Environmental Specialist
Indiana Division - Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Re: Request for ACHP Involvement
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Ms. Newland:

The purpose of this letter is to initiate the coordination necessary for involvement of the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana. As you know, ACHP involvement in the Section 106 process of an individual project must be solicited by the project's lead Federal agency. Therefore, at this time we respectfully ask that the Federal Highway Administration (FHWA) requests the involvement of the ACHP in this project.

We believe that ACHP involvement is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. Specifically, the State Boulevard Project meets the criteria of **Part (c)(1)** for “...adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district” and **Part (c)(3)** for “...cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...”

If any additional information or supplemental documentation is needed in order for FHWA to request ACHP involvement, please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Brett W. Lackey
Environmental Scientist

BWL:alo

Ms. Joyce Newland

August 15, 2011

Page 2

cc: James Glass, Ph.D
John Carr
Wade Tharp
Amanda Ricketts
Mary Kennedy
Patrick Carpenter
Staffan Peterson, Ph.D.
Jason Kaiser
Greg Smith
Joyce Newland
Dan Avery
Creager Smith
Shan Gunawardena
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Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Suzanne Slick
Annette Dailey
John Shoaff
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst

Alternatives Analysis – Historic Bridge PA
State Boulevard Bridge No. 546 over Spy Run Creek
State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)

1. Rehabilitation for Continued Vehicular Use (two-way option)

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The

State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

2. Rehabilitation for Continued Vehicular Use (one-way option)

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary’s River which causes the bridge to be overtopped with backwater from the Saint Mary’s River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) “This flooding is caused primarily by backwater from the St. Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

3. Bypass (Non-vehicular use)

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The

State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

4. Replacement

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

5. Relocate and Replacement

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use. Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for vehicular use. While this alternative does meet the project’s purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

Alternatives Analysis – Individual Section 4(f)
State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)

Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly

Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 16 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 14 residential relocation from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

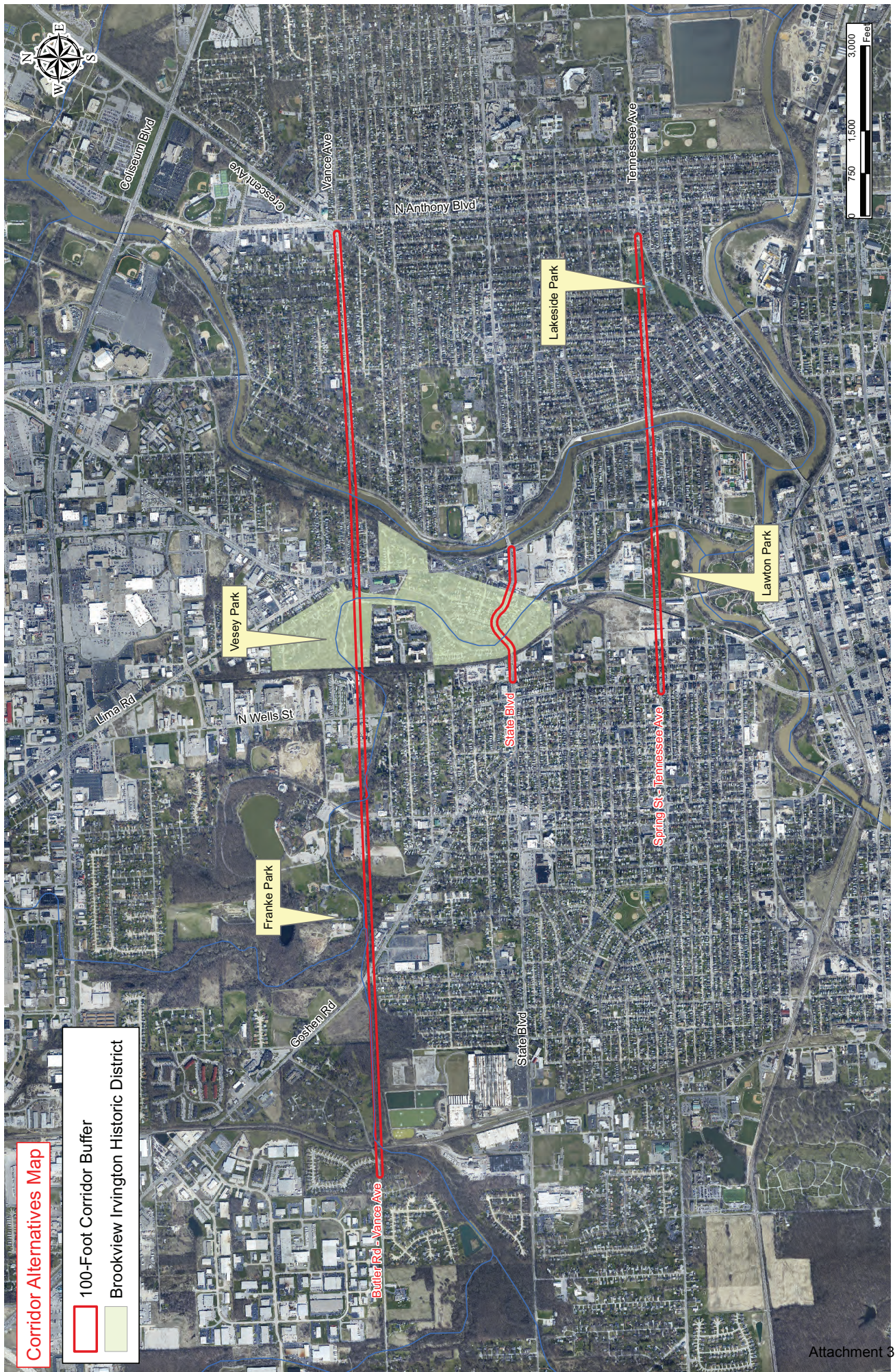
The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage.

This alternative would avoid impacts to historic properties.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.



Comment No.	Name / Organization / Comment Date	Comment	Response
1	Jan Dailey – Brookview Neighborhood Resident 12/7/2009 Letter	The recent closing of the Kroger Fuel Center may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek. If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State boulevard and Spy Run Avenue the road would pass behind or around most of the homes in the APE. The designers of the road project could not have foreseen the closing of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy it seems only fair that separate study be conducted factoring in possible land use of the former Kroger Fuel Center.	<p>The realignment of State Blvd to follow the bank of the Spy Run Creek would require extensive work in the channel of Spy Run Creek and the skew of the crossing would create the need for a longer bridge which would significantly increase the cost of the bridge. This alignment would also increase cost due to significant increase in the amount of required right of way and the demolition of a major commercial structure.</p> <p>A new intersection with Clinton Street would need to be constructed in close proximity to the existing State Blvd/Clinton Street intersection which would create safety concerns. This alignment would also likely cause economic impacts to the existing businesses located along existing State Blvd due to decreased visibility and accessibility</p>
2	Michael Galbraith – ARCH 12/9/2009 Letter	The HPR contained no maps, plans, or drawings of the proposed project. We have found it difficult to study, review, and analyses this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review?	Maps, plans, and drawings have been developed since this comment was received. These maps, plans, and drawings were made available for review at the first CP meeting and additional drawings will be provided at the next CP meeting.
3		Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states that the "Purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "Adding a new trail bridge" as stated on Page 3, addresses "Traffic Flow, Roadway, and Pedestrian Safety" along State boulevard need is stated in the cover letter to "originate from the substandard horizontal curve along State boulevard."	The Purpose and Need for the project has been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.

4	<p data-bbox="196 804 548 1430">It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid Project, possibly with the intention of avoiding future section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.</p> <p data-bbox="196 163 440 777">Pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p data-bbox="467 149 792 777">Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
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5	<p>Regarding the substandard horizontal curve: we question the analysis of "substandard" and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects o the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive."</p>	<p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table border="1"> <thead> <tr> <th>Station Line "A"</th><th>Existing Curve Radius</th><th>Req'd Radius (30 mph)</th></tr> </thead> <tbody> <tr> <td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr> <tr> <td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr> <tr> <td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr> </tbody> </table> <p>The Indiana Service Corporation Historic District is not included in the Project's APE and therefore was not studied for potential effect from this project. No impacts to the Indiana Service Corporation Historic District are anticipated to result from this project.</p>	Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
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6		<p>Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR.</p>												

7		<p>Also regarding the APE, were potential changes in traffic use in the Brookview – Irvington Park Historic District Studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this district. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut through traffic and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
8		<p>Plans for this project seem to be quite complete according to the project description in the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for this undertaking."</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>

		<p>We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. We believe that these projects must be aggregated for Section 106 review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 review that investigation into possible violation of Section 110(k) of the NHPA would be appropriate. The City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur: through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview – Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially</p> <p>(continued on next page)</p>
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			<p>reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
9		<p>We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview – Irvington Park Historic district has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.</p>	<p>The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.</p>

10	Todd Zeiger – Indiana Landmarks 12/8/2009 Letter	<p>With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis will be provided to all CPs prior to the next (second) CPs Meeting.</p>
11		<p>Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.</p> <p>Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include "the geographic area or areas within which an undertaking may <u>directly or indirectly</u> cause alterations in the character or use of historic properties". While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

12		<p>How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.</p>	<p>The purpose and need for this project has been revised since this comment was provided. The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p>
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13		<p>Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus</p>	<p>Detailed accident data compiled by the City was included in the revised purpose and need and provided to all consulting parties.</p> <p>Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As provided in the purpose and need statement, the RMV exceeds 2.0 which indicates that a safety problem exists.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
14		<p>Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
15		<p>Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
16		<p>Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>

17		Please provide detailed traffic accident studies or report data pertaining to accidents at the "substandard" curve.	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
18		Please provide detailed pedestrian accident studies or report data pertaining to the "substandard curve."	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
19		How is the proposal to add a new "trail bridge" over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed "trail bridge".	<p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>

20	<div data-bbox="185 787 1023 1438" data-label="Text"> <p>"Trail Bridge": We continue to be concerned about an ongoing appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future "trail bridge" should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.</p> </div> <div data-bbox="185 128 1023 787" data-label="Text"> <p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p> </div>
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<p>21</p>	<p>Concerning the "substandard horizontal curve": Page 4 of the HPR states that "the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive." We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.</p>	<p>In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table border="1"> <thead> <tr> <th>Station Line "A"</th><th>Existing Curve Radius</th><th>Req'd Radius (30 mph)</th></tr> </thead> <tbody> <tr> <td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr> <tr> <td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr> <tr> <td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr> </tbody> </table>	Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
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<p>22</p>	<p>Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7. We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis will be provided to all CPs prior to the next (second) CPs Meeting.</p>												

		<p>Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgemoor Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.</p> <p>The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.</p>	<p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program or associated voluntary buyout program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
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24	Julie Donnell – Friends of the Parks 12/10/2009 Letter	The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?	Yes – there is a proposed grade change of approximately 7 feet in the area of the new bridge.
25		The vegetation. What are the potential impacts on the vegetation of the designed landscape?	Development of final plans will include appropriate measures to mitigate and/or enhance landscape features within the project area.
26		The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?	With the exception of the new bridge over Spy Run Creek and channel grading immediately adjacent to the proposed bridge, no impacts to the waterway are anticipated from this project.
27		What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.	The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.
28	Dr. James Glass - SHPO 12/14/2009 Letter	Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, specifically in graphic form, to indicate where the alignments that are under consideration would run.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
29		Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in avoidance or minimization of the adverse effect.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.

30		<p>In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we not that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources.</p>	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
31	<p>John Shoaff – City Council 12/15/2009 CP Meeting</p>	<p>John Shoaff expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

32	<p>Michelle Briggs – Wedaman – Brookview Neighborhood Association 12/15/2009 CP Meeting</p>	<p>Michelle Briggs-Wedaman also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
33	<p>Jan Daily – Brookview Neighborhood Resident 12/15/2009 CP Meeting</p>	<p>Jan Daily also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecks both down Huguenard and along State Boulevard.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

34	Angie Quinn – ARCH 12/15/2009 CP Meeting	Angie Quinn discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.	It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.
35	ARCH 12/15/2009 CP Meeting	ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
36	Dr. James Glass - SHPO 1/27/2010 Letter	One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.	Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.

37	<div data-bbox="191 793 738 1434" data-label="Text"> <p>Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.</p> </div> <div data-bbox="191 128 495 777" data-label="Text"> <p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> </div> <div data-bbox="521 128 738 777" data-label="Text"> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> </div> <div data-bbox="768 128 1040 777" data-label="Text"> <p>The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.</p> </div> <div data-bbox="1070 128 1261 777" data-label="Text"> <p>For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> </div> <div data-bbox="1291 128 1427 777" data-label="Text"> <p>The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing curve found on State Boulevard between Clinton Street and Cass Street.</p> </div> <div data-bbox="1456 510 1484 777" data-label="Text"> <p>(continued on next page)</p> </div>
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			<p>Specifically, the intersections of Eastbrook Drive and Terrace Road will be reconstructed and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south utilizing the Oakridge Rd extension. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.</p>
<p>38</p>		<p>It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum</p> <p>(continued on next page)</p>

		<p>Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
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39		<p>In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
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40		<p>Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred-and others that may yet occur-as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.</p>	<p>Consideration will be given to this request at the appropriate time.</p>
41	<p>Dr. James Glass – SHPO 3/10/2010 Letter</p>	<p>We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended – flood-proofing of some homes- has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State</p> <p>(continued on next page)</p>

		<p>Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
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42	<div data-bbox="191 793 1224 1444" data-label="Text"> <p>We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first bluish like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.</p> </div> <div data-bbox="191 126 1224 793" data-label="Text"> <p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the table included in the purpose and need statement..</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p> </div>
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43		<p>The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
44		<p>Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?</p>	<p>Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.</p>

45	Jill Downs - ARCH 6/15/2011 Letter	<p>With the Exception of a relatively short window of time during peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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46	<div data-bbox="191 793 1485 1444" data-label="Text"> <p>My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrians safety.</p> </div> <div data-bbox="191 128 1485 793" data-label="Text"> <p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p> </div>
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			<p>Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
47		<p>The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's poor attempt to justify its need.</p>	<p>Please see the project's purpose and need; approved by INDOT and FHWA.</p>
48		<p>...American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than moving traffic through the area.</p>	<p>Original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

49		<p>It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
50		<p>Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. These congested conditions lead to increased air pollution in the form of emissions, travel time delays for motorists, and loss of productivity. Intersection congestion also causes motorists to attempt to cut through neighboring streets in order to avoid congestion and delays at the intersections. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p>

51		I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.	American Structurepoint is dedicated to following the Section 106 process in its entirety. No deviations from the Section 106 process which would warrant a "New Section 106 Review" have occurred.
52	Todd Zeiger – Indiana Landmarks 6/16/2011 Email	I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.	Additional clarification regarding the opportunity for Consulting Parties to comment was provided. As was intended and as is always the case, Consulting Parties may comment on any information received during the Section 106 process. Every attempt to address each consulting parties comment is being taken: as is evident by this document.
53		Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.
54		I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period.	An additional comment period was provided for the Purpose and Need. Requesting and considering consulting parties concerns is an ongoing effort and a central purpose of the Section 106 process. We are continually working to address your concerns, however this should not preclude additional consulting parties meetings which provide the opportunity for other comments to be provided.
55	John Shoaff – City Council 6/16/2011 Letter	First, let me note you said that a purpose and need statement was attached, but I did not find one included with my copy.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.

56	<p data-bbox="191 793 329 1430">In his letter Dr. Glass raises several important concerns, beginning with "It is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion". Nor is it clear to me, even after reading your response.</p> <p data-bbox="191 128 492 779">Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p data-bbox="521 128 711 779">Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p data-bbox="740 128 1179 779">The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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57		<p>On the curve of State Street, traffic always moved smoothly; but there was often (and always at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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58	<p>Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p>
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		<p>Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
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59	<div data-bbox="191 793 602 1434"> <p>Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the arterial plus bypass concept". This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor," as you are already calling it, as if past errors justify future ones; you argue that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south – but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelhorn Road – which aligns with and neatly continues the east-west stretch of Coliseum.</p> </div> <div data-bbox="191 128 1484 793"> <p>The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northeast Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy</p> <p>(continued on next page)</p> </div>
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			<p>Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p> <p>The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.</p> <p>State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stielhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.</p>
60		<p>You totally ignore the welfare of the people who live within the area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values to the homes your corridor borders or traverses?</p>	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document. This is not appropriate for Section 106.</p>

61		<p>You refer to "The redevelopment of the urban core area" something we all want, but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor.</p>	<p>The mention of "redevelopment of the urban core area" is taken out of context in this comment. As stated in the purpose and need: "The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes." This project is not being presented in order to redevelop the urban core; it is being presented in order to accommodate that redevelopment and the traffic demands it is currently placing on State Boulevard, which will continue to grow over time.</p>
62		<p>ISTEA was passed in 1991, the FHWA handbook on Flexibility in Highway Design was published in 195, and the AASHTO handbook on design flexibility which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert that standards appropriate for "highway design elements" apply to a road that abuts and serves a modest residential neighborhood!</p>	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p>
63	Michael Galbraith - ARCH 6/17/2011 Letter	<p>...my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as a mere footnote. Our Dec 8 2009 comments regarding the HPR and our comments from the initial Consulting Parties (Dec 15 2009) remain unaddressed. I request that our concern and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed and that we be given time in which to respond to those answers.</p>	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
64		<p>It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project purpose and need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in purpose and need.</p>	<p>The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment. The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

65	Julie Donnell – Friends of the Parks 6 /14/2011 - Letter	...We are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment. The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.
66		Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communications with Dr. Glass, in a formal way under the auspices of whatever form the Section 106 review continues.	An additional comment period was provided following distribution of the purpose and need.
67	John Shoauff - 7/1/2011 Letter	There are material discrepancies between the accident counts in your new P and N Statement and those sent by Hayley Steele of your firm to ARCH on 2/4/2010.	The discrepancy lies in the methodology used to identify potentially hazardous locations for system wide planning purposes in comparison to method that compiles crashes based on a conservative distance, usually from an intersection. The information utilized for both methods is based on the same data, which is derived from crash reports filed by state and local law enforcement agencies. The "planning method" is used to assist in the screening and allocation of crashes to specific areas, to identify "hot-spots" where actual crashes exceed expected thresholds. This method compiles crashes within a 250' radius from a single point, generally an intersection. When the distance between intersections is less than 500', this method will compile crashes that are common to both intersection. A micro-analysis, that examines each crash report, is the only way to ascertain the precise type, location and cause of each crash. We have conducted a micro-analysis for the State Boulevard corridor, Cass Street to Spy Run Avenue, that identifies all crashes for the years 2007-2010.
68		The RMVs generated by your figures are much higher than the City's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.	Both sets of crash data were provided to American Structurepoint by NIRCC. See above response to question number 67.

69		<p>With respect to the Spy Run and Clinton intersections, it stains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part: namely to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels.</p> <p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
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70	Dr. James Glass – SHPO 7/5/2011	<p>We continue to think, however, that it may be appropriate to expand the Section 106 APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
71		<p>We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevance to the Section 106 process. The alternatives presented to the consulting parties so far appear to be minor variations in the State Boulevard Corridor alternative.</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>
72		<p>However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in responses to your May 19 and June 17 letters.</p>	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
73		<p>Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.</p>	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
74	Suzanna Slick – Irvington Park 7/6/2011 - Email	<p>I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State boulevard Correspondence.</p>	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document.</p>

75		There is no attempt at traffic calming, but a great emphasis on traffic rushing.	Alleviation of unnecessary congestion is not intended to translate to "traffic rushing." The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.
76		There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability.	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p> <p>It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.</p>
77		Other cities are making great strides in building roadways that are user friendly, and safe for everyone – not just car and truck drivers – and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.	The proposed project is being designed with the central goals of providing a safe roadway for both pedestrians and motorists, while impacting the historic area as little as possible.

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1st, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne.

We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1st, until then please let me know if there are any questions.

Thanks

Brett W. Lackey

Environmental Specialist, Environmental Sciences Group

7260 Shadeland Station

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U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard
FHWA Liaison
Advisory Council on Historic Preservation
1100 Pennsylvania Avenue, NW
Washington, DC 20004

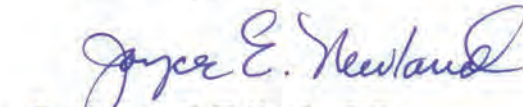
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at joyce.newland@dot.gov.

Sincerely,


Robert F. Tally, Jr., P.E.
Division Administrator

Enclosure



Lackey, Brett

From: Suzanne [sjslick@mac.com]
Sent: Monday, August 29, 2011 5:06 PM
To: Lackey, Brett; mayor@ci.ft-wayne.in.us
Cc: Glass, James; Carr, John; Tharp, Wade; aricketts@dnr.in.gov; Kaiser, Jason; Carpenter, Patrick A; Kennedy, Mary; Newland, Joyce; aquinn@archfw.org; Michael Galbraith; don.orban@cityoffortwayne.org; tzeiger@indianalandmarks.org; juliemarie57@earthlink.net; Michelle Briggs Wedaman; jcooper@ccrtc.com; jandailey59@msn.com; indianabridges@sbcglobal.net; shan.gunawardena@cityoffortwayne.org; danavery@co.allen.in.us; jshoaff@proparkwest.com; creager.smith@cityoffortwayne.org; albertcohan@aol.com; tmn@barrettllaw.com; rross@martin-riley.com; tom.cain@cityoffortwayne.org
Subject: Re: State Boulevard Consulting Party Meeting
Attachments: image001.jpg; ATT00001..htm; image002.jpg; ATT00002..htm; image003.jpg; ATT00003..htm; IN20071404.EV.2011-08-12.Consulting Parties Meeting Memo.pdf; ATT00004..htm

All,

After reading the Consulting Party comments and rebuttals from American Structurepoint I'm not very optimistic about the upcoming meeting -- either Structurepoint is being deliberately obtuse or they refuse to acknowledge our very real concerns about the State Blvd project's impact on our neighborhood and our City. People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive. Neighbors who are intimately familiar with the streets and traffic in the area -- much more familiar than anyone else involved in this discussion -- have weighed in in opposition to this massive alteration of our neighborhood, yet the responders continue to insist that this will improve safety and the level of service delivered to the users. The responses repeat the mantra that safety is of utmost importance and the primary goal, yet language regarding traffic calming seems to be deliberately avoided in the answers. While many cities are moving away from the trend to rush traffic quickly through urban areas and toward a complete streets approach to integrated roadways that encourage and expedite usability by non-motorized "traffic", State Blvd's future seems to be the opposite -- an artery of speeding cars and trucks racing in a straight line at high speed bisecting our quiet, quaint neighborhood, in effect cutting neighbors off from anything on the "other" side of State Blvd. In the list of alternatives, one would expect to find some discussion of the use of standard calming devices like reduced speed, raised crosswalks, chicanes, lateral shifts and roundabouts, for example. The "road diet" approach is not mentioned either. There is nothing remotely related to these approaches in any of the responses, just lots of rhetoric about "lengthy delays and congestion". Look, I drive the Cass to Clinton stretch daily -- there are no major delays and no lost productivity for motorists. Accidents in this stretch are primarily caused by speeding motorists which means as speed increases, as it surely will with a multi-lane straightaway, danger of accidents will increase. Certainly, risk to nonmotorized users will increase greatly. And passing off pedestrian needs to the Pufferbelly Trail project seems like an inadequate solution -- more an afterthought than a priority. Are the experts making these decisions and designing this roadway "improvement" that out of step with my neighborhood and with current best practices in street design? Let me point you to some information that will inform the conversation:

Here is a quote from the Kansas City Walkability Plan - <http://ww4.kcmo.org/planning/walkplan/Aappendix.pdf> :

Traffic calming is a way to design streets using engineering principles to encourage people to drive more slowly. It creates physical and visual cues that induce drivers to travel at appropriate speeds. Traffic calming is self-enforcing. The design of the roadway results in the desired effect without reliance on enforcement or voluntary compliance. Traffic control devices such as signals and signs rely on compliance. While elements such as landscaping and lighting do not force a change in driver behavior, they do provide the visual cues that encourage people to drive more slowly.

The reason traffic calming is such a powerful and compelling tool is that it has proven to be so effective. Some goals of traffic calming are clearly measurable such as increasing safety through fewer and less severe crashes. Others, such as supporting community and livability, are less tangible but equally important.

National Complete Streets Coalition -- <http://www.completestreets.org/>

More at these sites:

<http://ww4.kcmo.org/planning.nsf/plnpres/walkability?opendocument>

<http://www.fhwa.dot.gov/environment/sidewalk2/sidewalks209.htm>

<http://www.ite.org/traffic/tcstate.asp>

http://safety.fhwa.dot.gov/ped_bike/

<http://www.virginiadot.org/business/resources/TrafficCalmingGuideOct2002.pdf>

<http://www.pps.org/articles/livememtraffic/>

<http://cityofsparks.us/sites/default/files/assets/documents/traffic/Traffic%20Calming.pdf>

Fort Wayne is a smart, vibrant city that could achieve so much more in improving livability and healthy neighborhoods, the State Blvd project could be an opportunity to do this. I'm afraid what we will get is a noisy, frenetic, dangerous megastructure that citizens will avoid unless they are speeding through in a car or truck on their way to somewhere else. It is not an appealing image to those of us who will have to tolerate its unavoidable presence in our neighborhood. And we have already lost so very much in the last few years to the flood control buy-out, the blighting of Centlivre and even in the loss of ash trees in our green spaces. If we must have this new roadway, can't it be crafted in a forward-thinking, people-friendly, neighborhood-sustaining fashion?

Sincerely,
Suzanne Slick
Consulting Party for Irvington Park

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1st, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1st, until then please let me know if there are any questions.

Thanks

Brett W. Lackey
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MEETING MINUTES

Location: City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

Date: September 1, 2011

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross
(City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and
Archaeology)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thomson (Allen County Highway Department)
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
 - 1) Project Update
 - 2) Purpose and Need Update
 - 3) Consulting Party Comments and Responses document
 - 4) Alternatives Review
 - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.

18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.

50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or blackey@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.



Brett W. Lackey

BWL:mgn

Enclosures

Lackey, Brett

From: Lackey, Brett
Sent: Thursday, September 08, 2011 12:18 PM
To: Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich
Cc: 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott
Subject: State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
 - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
 - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
 - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
 - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
 - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
 - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
 - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
 - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
 - Either city website or www.structurepoint.com
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
 - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

Brett W. Lackey

Environmental Specialist, Environmental Sciences Group

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C 317.850.0257



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Preserving America's Heritage

September 22, 2011

Robert F. Tally, Jr., P.E.
Division Administrator
FHWA – Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, "Protection of Historic Properties" (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP's regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking's effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004
Phone: 202-606-8503 • Fax: 202-606-8647 • achp@achp.gov • www.achp.gov

Attachment 3 - 350 of 531

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at ngabriel@achp.gov.

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson
Historic Preservation Technician
Office of Federal Agency Programs



M E M O R A N D U M

DATE: September 29, 2011

TO: Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross (City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp, Amy Johnson, Amanda Ricketts (IDNR, Division of Historic Preservation and Arch.)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs, Angie Quinn (ARCH, Inc.)
Michelle Briggs-Wedaman, Karl Dietsch (Brookview Neighborhood Association)
Julie Donnell, Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thornson (Allen County Highway Department)
Todd Zeiger (Indiana Landmarks)
Dr. James Cooper, Paul Brandenburg (Indiana Historic Spans Task Force)
Albert Cohan (Westbrook 5, LLC)
Thomas Neizer (Barrett & McNagney, LLP)
Ronald Ross (Martin Riley Architects and Engineers)
Dan Ernst (Earth Source, Inc.)

FROM: Brett W. Lackey (American Structurepoint, Inc.)

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, Briana Hope, Rich Zielinski (American Structurepoint, Inc.)

Enclosed, please find the following items:

- 1) Consulting Party Meeting Minutes (9/01/2011)
Meeting minutes were prepared for the September 1, 2011 Consulting Parties Meeting. The meeting minutes were prepared based on a digital recording of the meeting.
- 2) Agency Coordination Meeting Minutes (9/02/2011)
A meeting was held on Friday, September 2, 2011, at the American Structurepoint office to discuss the State Boulevard Consulting Party Meeting on September 1, 2011. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The meeting minutes summarize the discussion.

3) Individual Section 4(f) Alternative Analysis (Revised)

Following the September 1, 2011 Consulting Parties Meeting, American Structurepoint evaluated an additional State Boulevard Alternative which includes a 3-lane design. This Alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3D*.

American Structurepoint has also re-evaluated an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south. Additional information regarding this alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3C*.

American Structurepoint has added a discussion of three additional configurations for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway. A discussion of these access alternates (*Access Alternates 1-3*) is included as a subset of *Alternate 3A*.

American Structurepoint has added additional information to *Alternate 4 (No-Build)*.

4) Traffic Data from NIRCC

As requested by Consulting Parties during the September 1, 2011 meeting, additional traffic information regarding the intersection Level of Service has been compiled by NIRCC and is enclosed for your information.

Upon further review of the State Boulevard intersection level of service information, the purpose and need statement has been updated by removing the evening peak Spy Run Avenue eastbound through movement from the deficient category of the purpose and need statement. Although the overall level of service for this intersection approach is deficient (LOS E), the LOS associated with the eastbound through movement is LOS D which is considered acceptable. This revision is located on page 2 of 5 of the purpose and need statement. A copy of page 2 with the revision highlighted is included for your review.

5) ACHP Correspondence

As indicated during the September 1, 2011 Consulting Parties Meeting, the FHWA has initiated coordination with the Advisory Council on Historic Preservation (ACHP). The FHWA requested ACHP involvement in a letter dated August 29, 2011. On September 22, 2011, the ACHP provided a response letter to the FHWA which indicated that additional information will need to be evaluated by the ACHP prior to deciding whether or not the ACHP will choose to be involved in the project's Section 106 process. The requested additional information is currently being prepared and will be submitted to the ACHP in the near future. Copies of the two coordination letters are included for your review.

As requested during the September 1, 2011 Consulting Parties Meeting, the City of Fort Wayne has created an online archive for the project's Section 106 correspondence and documents. This data can be accessed online at <http://www.cityoffortwayne.org/publicworks/west-state-blvd-realignment.html>

At this time we are requesting that all consulting parties review the enclosed materials and provide any comments within 30 days of receipt of this mailing. I can be reached by phone at (317) 547-5580 or by e-mail at blackey@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Minutes (9/01/2011)
Agency Coordination Meeting Minutes (9/02/2011)
Individual Section 4(f) Alternatives Analysis (Revised)
Traffic Data from NIRCC
ACHP Correspondence
Purpose and Need Statement Revision (Page 2 of 5)



MEETING MINUTES

Location: City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

Date: September 1, 2011

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross
(City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and
Archaeology)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thornson (Allen County Highway Department)
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
 - 1) Project Update
 - 2) Purpose and Need Update
 - 3) Consulting Party Comments and Responses document
 - 4) Alternatives Review
 - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.

18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.

50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or blackey@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.

A handwritten signature in black ink, appearing to be "BWL", with a long horizontal line extending to the right.

Brett W. Lackey

BWL:mgn

Enclosures

Lackey, Brett

From: Lackey, Brett
Sent: Thursday, September 08, 2011 12:18 PM
To: Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich
Cc: 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott
Subject: State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
 - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
 - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
 - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
 - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
 - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
 - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
 - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
 - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
 - Either city website or www.structurepoint.com
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
 - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

Brett W. Lackey

Environmental Specialist, Environmental Sciences Group

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Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

Access Alternate 1

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Access Alternate 2 (Preferred Access Alternative)

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.

Access Alternate 3

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 20 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and "mirror" or "flip" the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately 4 to 5-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and

State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project's purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

Alternative 3D: Preferred Alignment with 3-Lane Typical Section

This alternative is similar to Alternative 3A (Preferred Alternative) but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes while correcting the substandard horizontal curve.

By reducing the typical section from 4-lanes (Alternative 3A/Preferred Alternative) to 3-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from 4-lanes to 3-lanes is only 10-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.


















The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

HCM Signalized Intersection Capacity Analysis

1: State Blvd & Clinton St




















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	543	46	200	437	0	0	0	0	126	1707	47
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.99		1.00	1.00						1.00	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3427		1687	1792						4999	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3427		1687	1792						4999	
Peak-hour factor, PHF	0.92	0.84	0.72	0.88	0.85	0.92	0.92	0.92	0.92	0.73	0.84	0.78
Adj. Flow (vph)	0	646	64	227	514	0	0	0	0	173	2032	60
RTOR Reduction (vph)	0	9	0	0	0	0	0	0	0	0	3	0
Lane Group Flow (vph)	0	701	0	227	514	0	0	0	0	0	2262	0
Heavy Vehicles (%)	0%	3%	13%	7%	6%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3						1	
Permitted Phases										1		
Actuated Green, G (s)		18.5		13.5	36.5						34.4	
Effective Green, g (s)		18.5		13.5	36.5						34.4	
Actuated g/C Ratio		0.23		0.17	0.46						0.43	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		792		285	818						2150	
v/s Ratio Prot		c0.20		c0.13	0.29							
v/s Ratio Perm											0.45	
v/c Ratio		0.88		0.80	0.63						1.05	
Uniform Delay, d1		29.7		31.9	16.6						22.8	
Progression Factor		1.00		0.58	1.15						1.00	
Incremental Delay, d2		13.8		11.9	2.0						34.8	
Delay (s)		43.5		30.4	21.0						57.6	
Level of Service		D		C	C						E	
Approach Delay (s)		43.5			23.9			0.0			57.6	
Approach LOS		D			C			A			E	
Intersection Summary												
HCM Average Control Delay			48.2			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.95									
Actuated Cycle Length (s)			80.0			Sum of lost time (s)				13.6		
Intersection Capacity Utilization			75.5%			ICU Level of Service				D		
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

2: State Blvd & Spy Run Ave


















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	93	526	0	0	470	211	131	1166	230	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.97				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1612	3505			1827	1568	1612	4672				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1612	3505			1827	1568	1612	4672				
Peak-hour factor, PHF	0.75	0.90	0.92	0.92	0.85	0.70	0.91	0.93	0.77	0.92	0.92	0.92
Adj. Flow (vph)	124	584	0	0	553	301	144	1254	299	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	29	0	50	0	0	0	0
Lane Group Flow (vph)	124	584	0	0	553	272	144	1503	0	0	0	0
Heavy Vehicles (%)	12%	3%	0%	0%	4%	3%	12%	8%	7%	0%	0%	0%
Turn Type	Prot				Perm			Perm				
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Effective Green, g (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Actuated g/C Ratio	0.08	0.44			0.29	0.29	0.43	0.43				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	131	1542			530	455	695	2015				
v/s Ratio Prot	c0.08	0.17			c0.30			c0.32				
v/s Ratio Perm						0.17	0.09					
v/c Ratio	0.95	0.38			1.04	0.60	0.21	0.75				
Uniform Delay, d1	36.6	15.1			28.4	24.4	14.2	19.1				
Progression Factor	1.56	0.41			1.00	1.00	1.00	1.00				
Incremental Delay, d2	36.4	0.3			50.9	5.7	0.7	2.6				
Delay (s)	93.4	6.5			79.3	30.1	14.9	21.6				
Level of Service	F	A			E	C	B	C				
Approach Delay (s)		21.7			61.9			21.1			0.0	
Approach LOS		C			E			C			A	
Intersection Summary												
HCM Average Control Delay			31.9		HCM Level of Service				C			
HCM Volume to Capacity ratio			0.87									
Actuated Cycle Length (s)			80.0		Sum of lost time (s)				15.8			
Intersection Capacity Utilization			75.5%		ICU Level of Service				D			
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

1: State Blvd & Clinton St




















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	629	67	192	539	0	0	0	0	178	1593	117
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.98		1.00	1.00						0.99	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3445		1687	1810						4955	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3445		1687	1810						4955	
Peak-hour factor, PHF	0.92	0.95	0.83	0.89	0.89	0.92	0.92	0.92	0.92	0.93	0.95	0.75
Adj. Flow (vph)	0	662	81	216	606	0	0	0	0	191	1677	156
RTOR Reduction (vph)	0	8	0	0	0	0	0	0	0	0	8	0
Lane Group Flow (vph)	0	735	0	216	606	0	0	0	0	0	2016	0
Heavy Vehicles (%)	0%	2%	12%	7%	5%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3						1	
Permitted Phases										1		
Actuated Green, G (s)		29.5		17.5	51.5						59.4	
Effective Green, g (s)		29.5		17.5	51.5						59.4	
Actuated g/C Ratio		0.25		0.15	0.43						0.49	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		847		246	777						2453	
v/s Ratio Prot		c0.21		c0.13	0.33							
v/s Ratio Perm											0.41	
v/c Ratio		0.87		0.88	0.78						0.82	
Uniform Delay, d1		43.4		50.2	29.4						25.8	
Progression Factor		1.00		1.51	0.46						1.00	
Incremental Delay, d2		11.7		26.0	5.7						3.3	
Delay (s)		55.1		101.6	19.1						29.0	
Level of Service		E		F	B						C	
Approach Delay (s)		55.1			40.8			0.0			29.0	
Approach LOS		E			D			A			C	
Intersection Summary												
HCM Average Control Delay			37.1			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.84									
Actuated Cycle Length (s)			120.0			Sum of lost time (s)			13.6			
Intersection Capacity Utilization			109.3%			ICU Level of Service			H			
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

2: State Blvd & Spy Run Ave

9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	134	606	0	0	508	275	130	1862	243	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.98				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1770	3539			1863	1615	1719	5036				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1770	3539			1863	1615	1719	5036				
Peak-hour factor, PHF	0.76	0.92	0.92	0.92	0.92	0.82	0.86	0.97	0.92	0.92	0.92	0.92
Adj. Flow (vph)	176	659	0	0	552	335	151	1920	264	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	10	0	15	0	0	0	0
Lane Group Flow (vph)	176	659	0	0	552	325	151	2169	0	0	0	0
Heavy Vehicles (%)	2%	2%	0%	0%	2%	0%	5%	1%	2%	0%	0%	0%
Turn Type	Prot				Perm			Perm				
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Effective Green, g (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Actuated g/C Ratio	0.09	0.44			0.31	0.31	0.47	0.47				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	155	1569			578	501	809	2371				
v/s Ratio Prot	c0.10	0.19			c0.30			c0.43				
v/s Ratio Perm						0.20	0.09					
v/c Ratio	1.14	0.42			0.96	0.65	0.19	0.91				
Uniform Delay, d1	54.8	22.8			40.6	35.7	18.4	29.5				
Progression Factor	0.89	2.00			1.00	1.00	1.00	1.00				
Incremental Delay, d2	92.1	0.4			27.8	6.4	0.5	6.9				
Delay (s)	140.6	46.0			68.4	42.1	18.9	36.4				
Level of Service	F	D			E	D	B	D				
Approach Delay (s)		65.9			58.5			35.3			0.0	
Approach LOS		E			E			D			A	
Intersection Summary												
HCM Average Control Delay		46.7			HCM Level of Service			D				
HCM Volume to Capacity ratio		0.95										
Actuated Cycle Length (s)		120.0			Sum of lost time (s)			15.8				
Intersection Capacity Utilization		109.3%			ICU Level of Service			H				
Analysis Period (min)		15										
c Critical Lane Group												



U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard
FHWA Liaison
Advisory Council on Historic Preservation
1100 Pennsylvania Avenue, NW
Washington, DC 20004

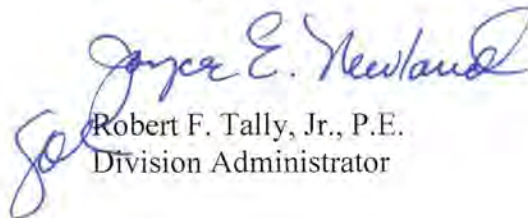
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at joyce.newland@dot.gov.

Sincerely,


Robert F. Tally, Jr., P.E.
Division Administrator

Enclosure





Preserving America's Heritage

September 22, 2011

Robert F. Tally, Jr., P.E.
Division Administrator
FHWA – Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, "Protection of Historic Properties" (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP's regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking's effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004
Phone: 202-606-8503 • Fax: 202-606-8647 • achp@achp.gov • www.achp.gov

Attachment 3 - 377 of 531

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at ngabriel@achp.gov.

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson
Historic Preservation Technician
Office of Federal Agency Programs

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E

Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

Removed



Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



November 7, 2011

Brett W. Lackey
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Effects of the State Boulevard Reconstruction and consideration of alternatives (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated August 15, 2011 and your memorandum dated September 29, 2011 and received which were received on August 16 and September 30, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Your August 15, 2011 letter asked us to offer an opinion on how the National Register boundaries of the listed Brookview-Irvington Park Historic District might be affected by the preferred alternative (now called Alternative 3A in the revised Section 4[f] alternatives analysis). Having considered the marked aerial photograph shown at the last consulting parties meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative. We also think that if the preferred alternative were to be implemented as shown on the marked aerial photograph, one might still be justified in considering the part of the historic district that lies to the southwest of Spy Run (i.e., along Edgehill Avenue, Jacobs Avenue, and Clinton Street south of Westbrook Drive) and south of State Boulevard) to be sufficiently connected to the rest of the district at the Westbrook Avenue-State Boulevard intersection that the buildings and landscapes in that southern end of the district would still contribute to the district's significance. However, that is not an ideal situation from a National Register boundary delineation standpoint. We would anticipate that, even if the right-of-way along the preferred alignment were appropriately landscaped, the eye would perceive be a rather broad and obvious expanse of non-contributing land and mostly straight, new roadway between the current alignment of State Boulevard and the outline that has been left Westbrook, south of Spy Run (as a result of the flood buy-outs and pavement removal). We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished.

The minutes of the September 1, 2011 consulting parties meeting reflect some, but not all, of the comments I offered regarding a couple of other, potential alternatives. One was the widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B). We recall, however, that this alternative was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.

I recall also having asked that an alternative on roughly the same alignment as the preferred alternative (i.e., running south of the existing State Boulevard between Westbrook Drive and a point to the east of Terrace Road) be considered that would have three lanes, instead of four lanes with a left turn lane or landscaped median (captured in part in points 50, 56, and 81 in the minutes). This second alternative would have one through lane in each direction and a center left turn lane. I had asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of

fewer residences than the preferred alternative. It appears that this alternative is essentially what is now called Alternative 3D in the revised Section 4(f) alternatives analysis.

We would like to elaborate on that second alternative, 3D, in light of our observations during a site visit to the project area and the Brookview-Irvington Park Historic District that Wade Tharp, John Carr, and I made following the September 1 meeting. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State. Even if a T-intersection would be practicable only at Eastbrook, perhaps a single, exit-only lane from new State to existing State could be left in place near where the bypass turns southwestward, which would retain westbound access to the bypassed part of State. Narrowing the roadway to three lanes could eliminate the need to take the contributing house at the northwest corner of State Boulevard and Westbrook Drive and leave somewhat more of the back yards of the houses along the south side of State, possibly saving as many as three more houses on State. Narrowing the roadway to three lanes also could reduce the cost of the new bridge over Spy Run and pull the new bridge away from the historic bridge. Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work, and with the installation of bollards both the Eastbrook and Westbrook ends. As a pedestrian bridge, its closure would not be a major impediment to transportation when floodwaters overtop it. We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.

The revised alternatives analysis says that Alternative 3D "is not considered prudent as it does not address the project's entire purpose and need." We wonder, however, whether a project is worthwhile and fundable by FHWA only if it fully meets all the needs and purposes that are identified. We wonder, also, whether there is not room for compromise between purpose and need and other values, such as historic preservation and neighborhood livability and preservation.

In the meeting minutes, points 39 and 40 refer to questions raised about the possibility of a new alignment (now called Alternative 3C) farther to the south than the preferred alternative (Alternative 3A). We do not recall that part of the discussion *verbatim*, but we think that the reference in the point 39 to "reversing the existing alignment/flipping the existing curb to the south" probably refers to a suggestion that the *curve* of the new alignment west of Clinton Street be reversed or flipped, so that most of the new alignment would be farther to the south than the preferred alternative.

The revised alternatives analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection? That curve now is proposed to begin west of the alley between Clinton and Terrace Road. However, if the beginning of the curve could be moved perhaps 100 feet farther east, without changing the radius significantly and without changing significantly the angle of the State-Clinton intersection, most of the new State Boulevard would shift to the east and away from several of the houses on the south side of State between Terrace and Eastbrook Drive. If that shift were coupled with the elimination of the proposed, southward extension of Oakridge that we suggested above for Alternative 3D, then several contributing houses on the south side of State could be avoided by the project. More of the character of the curve of State Boulevard that Arthur Shurcliff designed in 1917 could be preserved if houses were left on both sides of State. Furthermore, we noticed, in the historic properties report and in the National Register nomination for the Fort Wayne Parks and Boulevard System Historic District, that George Kessler's 1912 plan for State Boulevard (or Pfeiffer Boulevard, as he called it) roughly would have aligned State, running westward from its intersection with Spy Run Avenue, with that part of State lying to the west of Spy Run (the stream). It appears to us that shifting the curve of the new State Boulevard toward the east would result in an alignment somewhat closer to that which Kessler had intended (but which was not implemented) between Clinton and Spy Run (the stream), while retaining much of the existing State Boulevard's Shurcliff-designed curve through the Brookview-Irvington Park Historic District.

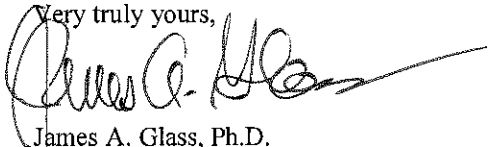
The eastward shifting of the alignment of either Alternative 3A or Alternative 3D probably would result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along existing State Boulevard. Even if the project cost were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan.

As we earlier had stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Patrick Carpenter, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Brett W. Lackey, American Structurepoint, Inc.



AMERICAN
STRUCTUREPOINT
INC.

May 17, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

As discussed at the September 1, 2011, Consulting Parties Meeting, resources previously identified in the Historic Properties Report (HPR) (Westerly Group, Inc, 2009) as eligible for listing in the National Register (NR) have since been listed on the NR, requiring an update to the HPR. Weintraut & Associates, Inc., has prepared an Additional Information (AI) Report to append the HPR. The AI is a supplement to the HPR following the inclusion of two new NR listed resources within the Area of Potential Effect (APE). Historians from Weintraut & Associates identified portions of both the Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011) as contained within the project APE. As previously stated, it is anticipated that the proposed project activities will have an adverse affect on these properties; therefore, an "Adverse Effect" finding continues to be anticipated.

The AI and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. Additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the AI within 30 days of receipt of this letter.

Please also beware that Weintraut & Associates will act as the Qualified Professional associated with the Section 106 process on this project as we continue to move forward. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. The information herein will be provided to all consulting parties.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

IN20071404



AMERICAN STRUCTUREPOINT, INC.

Linda Weintraut, Ph.D.

May 17, 2012

Page 2

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Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT



AMERICAN
STRUCTUREPOINT
INC.

May 22, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

You should have recently received a letter dated May 17, 2012, discussing the Additional Information (AI) Report, prepared by Weintraut & Associates, appending the Historic Properties Report (HPR) prepared for the State Boulevard Project. The report included with the May 17 letter was the original HPR and did not include the AI. The AI was accidentally omitted from the coordination.

Please find enclosed the AI and eligibility recommendations approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. As previously stated, additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. We are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, we have extended the comment period and ask that you reply with comments on the AI within 30 days of receipt of this letter.

We appreciate your cooperation in the development of this project and apologize for any confusion our omission may have caused. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. The information herein will be provided to all consulting parties.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:ead

Enclosures

IN20071404

Dr. Weintraut
May 22, 2012
Page 2

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Greg Smith, INDOT

Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



June 22, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: "Historic Property Report: State Boulevard Reconstruction from Spy Run to Cass Street, Additional Information Report" ("AI Report"; Natali, 4/2012) to append the historic properties report (Fife, 9/2009) for the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials submitted with your cover letters dated May 17 and 22, 2012, and received on May 21 and 24, respectively for the above-indicated project in the City of Fort Wayne, Allen County, Indiana.

We agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the area of potential effects, for inclusion in the National Register of Historic Places

We were asked to address specifically the eligibility of two, particular properties. First, we agree with the AI Report that the ca. 1895, two-story gabled-ell house at 315 East State Boulevard does not appear to possess sufficient historical or architectural significance or integrity to be eligible for inclusion in the National Register. Second, the AI Report notes that the Fort Wayne Park and Boulevard System Historic District National Register nomination characterized the portion of State Boulevard that lies within the Brookview-Irvington Park Historic District as being "individually eligible for the NR" (page 75 of the nomination). We do not consider that comment, which was included in an explanatory note about features of the district under "Section 7. Description" within the National Register nomination form, to confer individual eligibility on State Boulevard or any part of it. Furthermore, based on the information provided in the AI Report, we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the area of potential effects of this project is individually eligible for the National Register. We do not disagree, however, with the Fort Wayne Park and Boulevard System nomination's identification of the portion of State Boulevard in question as a contributing resource to that historic district.

As we stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we note that the historic property report (Fife 9/09) stated, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation

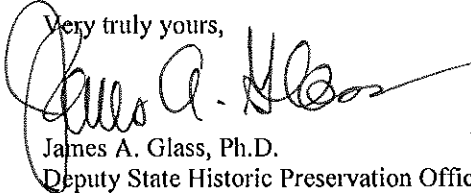
and Archaeology for review before we can comment further (see the list of qualified professional archaeologists at the Division of Historic Preservation and Archaeology website, http://www.in.gov/dnr/historic/bin/qp/qp_archo.pdf).

An example of an area that might still need to be surveyed would be the portion of a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report (Stillwell 4/2/09). If an entire residential lot, rather than just the portion of it that was surveyed, will now have to be acquired for the State Boulevard Reconstruction, then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick A. Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.



6/20/2012 Agency Meeting minutes included in body of letter.

AMERICAN
STRUCTUREPOINT
INC.

July 2, 2012

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated November 7, 2011, and items further discussed in an agency meeting held June 20, 2012. In attendance at the June 20, 2012, agency meeting were Dr. James Glass and John Carr - IDNR DHPA, Shan Gunawardena - City of Fort Wayne, Larry Heil - FHWA, Patrick Carpenter and Mary Kennedy - INDOT CRO, Jason Kaiser - INDOT Fort Wayne District (conference line), Dan Avery - NIRCC (conference line), Briana Hope and Scott Crites - American Structurepoint, and Linda Weintraut - Weintraut and Associates.

The letter states that the DHPA believes the minutes of the September 1, 2011, consulting parties meeting reflect some, but not all, of the comments SHPO offered regarding a couple of other, potential alternatives. We are providing responses to these additional comments.

- ❖ **The widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B) ... was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.**

Alternative 3B includes widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four lanes. By reducing the typical section from four lanes to three lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this Alternative would result in impacts to 20 residential properties within the Brookview-Irvington Historical District, the same number of relocations as the 4-lane typical section on existing alignment. This alternative would also still require a new bridge with additional travel lanes over Spy Run Creek. The widening of two lanes to three lanes as suggested by your office would ultimately result in a feasible alternative. However, the alternative is not considered prudent as it does not address the project purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.



- ❖ [DHPA had] asked that an alternative on roughly the same alignment as the preferred alternative be considered that would have three lanes, instead of four lanes, with a left-turn lane or landscaped median. It appears that this alternative is essentially what is called Alternative 3D in the revised Section 4(f) alternative analysis. [DHPA] asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of fewer residences than the preferred alternative. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State.

As part of the alternatives analysis, Alternative 3D was developed to explore the impacts of reducing the proposed State Boulevard to a 3-lane section with one through lane in each direction and a center median or left-turn lane where required. This option would reduce project costs by reducing pavement costs and allowing a narrower bridge to be constructed over Spy Run Creek. In addition to the reasons Alternative 3D was determined to be unfeasible in the 4(f) alternative analysis, the reduction in lanes would also result in traffic congestion and level of service at the intersections remaining the same as the existing conditions. Northeastern Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently and in the 3-lane section scenario, both intersections exhibit intersection movements having service levels of E or F. The following table (as included and discussed in the purpose and need statement) summarizes the levels of service at the Clinton Street intersection and Spy Run Avenue intersection, with the lane configurations remaining the same as the existing condition due to constructing State Boulevard as a 3-lane section:

**State Boulevard and Spy Run Avenue
 Intersection – Levels of Service**

Morning Peak	LOS Existing
Eastbound Left	F
Westbound Through	E

Evening Peak	LOS Existing
Eastbound Left	F
Eastbound Through	E
Westbound Through	E

**State Street and Clinton Street
 Intersection – Levels of Service**

Morning Peak	LOS Existing
Southbound Through	E

Evening Peak	LOS Existing
Eastbound Through	E
Westbound Left	F

It was also found in our analysis that by reducing the typical section from four lanes to three lanes, the limits of construction are reduced by approximately ten feet in width along both the north and south sides of the proposed State Boulevard. This reduction in construction limit width is not sufficient to significantly reduce impacts along the roadway. This configuration would result in impacts to approximately 15 residential properties within the Brookview-Irvington Historic District, the same as the preferred alternative.

As requested in your November 7, 2011, letter and per subsequent discussions in our June 20, 2012, meeting, alternate configurations for the connection into the Brookview neighborhood were explored.

One possible alternate connection was to extend Eastbrook Avenue to the proposed State Boulevard alignment. The skew of Spy Run Creek in conjunction with the length of the proposed bridge required to span the creek obstruct Eastbrook Avenue from connecting to the proposed State Boulevard roadway. The elevation of proposed State Boulevard near Eastbrook Drive would require the connection between existing State Boulevard and proposed State Boulevard to be constructed with a grade steeper than the 10 percent maximum grade allowed for a low speed urban street per the Indiana Design Manual (IDM). This design would be considered substandard and would contribute to safety concerns by not providing adequate sight distance to safely enter the proposed State Boulevard intersection.

Another possible configuration would be to extend Terrace Road to intersect the proposed State Boulevard alignment. The extension of this intersection would require construction near the midpoint of the State Boulevard mainline curve. In order to minimize impacts, the State Boulevard mainline curve at this location utilizes the maximum curvature allowed per IDM for low speed urban street criteria (460 feet). Constructing an intersection on the curve would increase potential safety concerns due to the geometry of State Boulevard at this location and inadequate sight distance that would not allow a motorist to safely negotiate the proposed intersection with State Boulevard. This scenario would also locate the Terrace Road intersection approximately 250 feet west of the Clinton Street intersection. The minimum intersection spacing per the IDM is 400 feet in order to facilitate traffic and avoid impeding traffic operations and efficiency at both intersections. Locating the intersection at Terrace Road would violate the IDM spacing requirement from the Clinton Street intersection and should be avoided.

After further analyzing and discussing alternate configurations to maintain a connection for the Brookview neighborhood to proposed State Boulevard, it is recommended that the proposed Oakridge Road extension remain as the preferred connection to State Boulevard.

During further discussion at our June 20, 2012, meeting, the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard was also explored. It was agreed that the designer would evaluate potential adjustments where possible to minimize impacts to these houses. The results of this effort will likely minimize impacts, allowing three houses once assumed to be total acquisitions to remain in place.

The following design modifications will be evaluated through the design process to minimize impacts to houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard by reducing the Oakridge Road typical section width:

- Eliminate boulevard roadway section for Oakridge Road
- Eliminate center curbed median and landscaping
- Shorten right-turn lane for vehicles exiting Oakridge Road west on State Boulevard

As agreed at the June 20, 2012, agency meeting, if after the design process is complete it is determined that the houses in question can remain in place, a firm commitment will be made to avoid complete acquisition of these parcels.

- ❖ **Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work.**

The existing State Boulevard bridge is hydraulically inadequate to accommodate a 100-year flood scenario and is an obstruction to water flowing through Spy Run Creek. Allowing this bridge to stay in place would also impede the hydraulic adequacy of the proposed State Boulevard bridge located immediately downstream. The existing concrete girder bridge is in poor condition and would continue to rapidly deteriorate due to continued Spy Run Creek flood events created from the backwater of the St. Mary's River. The close proximity of the western abutment of the existing bridge and the western abutment of the proposed bridge

would also make construction difficult. Due to the poor condition and required bridge hydraulics, it is recommended that the existing State Boulevard bridge be removed.

- ❖ **We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Schurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.**

Existing State Boulevard and Eastbrook Drive curbs and sidewalk will be left place where practicable as a mitigation effort for historic preservation. In addition, other potential mitigation measures were also discussed as part of the June 20, 2012, meeting, including:

- Photo documentation of the neighborhood and structures
 - Research and restore existing State Boulevard streetscape to replicate its look during the early 20th Century
 - Historical markers
 - Distribution locally of literature regarding the historical significance of the area
- ❖ **The revised alternative analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection?**

This suggestion was also discussed in detail at the June 20, 2012, meeting. If the mainline curve is shifted toward the Clinton Street intersection, the mainline State Boulevard roadway would shift slightly to the southeast from the proposed location. Currently, this proposed curve utilizes the maximum curvature allowed per the IDM for low speed urban street criteria and is located approximately 150 feet from the Clinton Street intersection. The proposed curve was designed at its current location in order to provide 150 feet of tangent roadway prior to the Clinton Street intersection, allowing for adequate sight distance to see both the traffic signals and also vehicles that may be stopped at the traffic signal. Relocating or shifting the roadway curve immediately adjacent to the intersection will likely result in increased safety concerns and increased accidents associated with the Clinton Street signal. Shifting the curve to the east would also require the proposed bridge over Spy Run Creek to be constructed at a greater skew, thus lengthening the bridge span and increasing project cost and stream impacts. Due to the associated safety and cost concerns, it is recommended that the mainline State Boulevard curve remain located per the current preferred alternative.

- ❖ **As we earlier stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultant of Ossian. Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, than an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.**

In reviewing the area previously surveyed by Archaeological Consultants of Ossian, it has been determined that there are in fact areas within the limits of the preferred alternative that have not been surveyed.

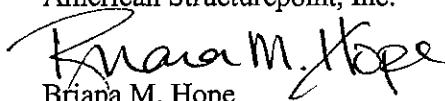
Archaeological Consultants of Ossian is currently performing an archaeological reconnaissance on the additional area and an archaeological report will be provided to your office as soon as it is available.

In addition to the items discussed at the agency meeting regarding the November 7, 2011, DHPA letter and the above responses, it was also determined that the next consulting parties meeting would likely be held in early September once FHWA felt it appropriate to issue a signed finding of adverse effect. The main purpose of the meeting will be to present potential mitigation measures to the consulting parties, as well as solicit additional potential mitigation concepts. The consulting parties will be notified of the date of the next consulting parties meeting concurrently with the distribution and request for comment on FHWA signed finding of adverse effect.

On behalf of Joyce Newland, Larry Heil (FHWA) also reported that the Advisory Council of Historic Preservation had not notified FHWA with a final decision in response to FHWA request for participation in the Section 106 process associated with the State Boulevard Project.

We hope that the information in this letter adequately addresses the concerns expressed in the November 7, 2011, DHPA letter. A copy of this response letter and a copy of the November 7, 2011, DHPA letter will be sent to all consulting parties concurrently with this letter. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

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Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT



AMERICAN
STRUCTUREPOINT
INC.

July 16, 2012

James A. Glass, Ph.D.
State Historic Preservation Officer
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: Indiana Archaeological Short Report
State Boulevard Improvements Project – Additional Area
Fort Wayne, Allen County, Indiana
DHPA No. 5903
Des. No. 0400587
Project No. 20071404

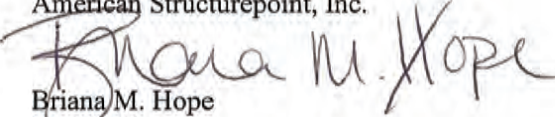
Dear Dr. Glass:

In reviewing the area previously surveyed by Archaeological Consultants of Ossian and brought to our attention in your November 7, 2011 letter, it has been determined that there were in fact areas within the limits of the preferred alternative for the proposed State Boulevard Improvements Project that had not been surveyed. Please find included with this correspondence the Indiana Archaeological Short Report, prepared by Archaeological Consultants of Ossian, for the additional area required for the State Boulevard Improvements project. The short report was reviewed and approved by the Indiana Department of Transportation, Cultural Resources on July 16, 2012.

The Phase I survey detected no properties that are eligible for nomination to the National Register of Historic Places. Since no historically or archaeologically significant sites will be impacted by the proposed undertaking, the Archaeological Consultants of Ossian recommended that project clearance be granted.

To facilitate the development of this project, you are asked to provide any comments you may have regarding this report within 30 days of the receipt of this letter. Please contact me at (317) 547-5580 or by email at bhope@structurepoint.com if you have any questions or require further information.

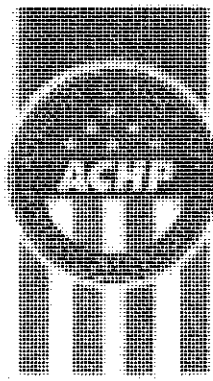
Very truly yours,
American Structurepoint, Inc.


Briana M. Hope
Project Manager

AMB:alo

Enclosures

cc: Shaun Miller, INDOT Cultural Resources



Preserving America's Heritage

July 31, 2012

Mr. Robert F. Tally, Jr.
Administrator
Federal Highway Administration
Indiana Division
575 N. Pennsylvania Street, Rm 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Transportation Project
Indiana*

Dear Mr. Tally:

On August 29, 2011, the Advisory Council on Historic Preservation (ACHP) received your letter requesting our participation in the Section 106 consultation for the reference undertaking. On September 22, 2011, we responded to your letter indicating that additional background information was needed in order to complete our review of the undertaking and determine our participation status. On May 10, 2012, the Federal Highway Administration (FHWA) provided the requested information to us and requested, again.

Since receiving the latest request from Indiana FHWA, we have spoken with FHWA staff and the State Historic Preservation Office (SHPO) regarding the timing and basis of your request that we participate in the Section 106 consultation. Based upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time. However, we are available to assist consulting parties regarding compliance with the Section 106 review process in accordance with Section 800.2(b)(2) of our regulations, "Protection of Historic Properties" (36 CFR Part 800).

Once the Indiana FHWA makes its effect determination for this undertaking, please notify the ACHP if you conclude that there is a finding of adverse effect. We will re-evaluate the undertaking once we receive your notification and advise you whether or not we have changed our decision regarding participation in consultation.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004
Phone: 202-606-8503 • Fax: 202-606-8647 • achn@achn.gov • www.achn.gov

Attachment 3 - 396 of 531

Thank you for contacting the ACHP regarding the consultation. Please don't hesitate to contact Najah Duvall-Gabriel at (202) 606-8585 or via email at ngabriel@achp.gov if you have any questions or concerns.

Sincerely,

A handwritten signature in cursive script, reading "Charlene Dwin Vaughn".

Charlene Dwin Vaughn, AICP

Assistant Director

Office of Federal Agency Programs

Federal Permitting, Licensing and Assistance Section



Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



August 1, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Your July 2, 2012 letter responding to our November 7, 2011 letter and to issues discussed at the June 20, 2012 agency meeting in Indianapolis regarding the State Boulevard Reconstruction Project (Des. No. 0400587; Project No. IN20071404; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your July 2 letter, which we received on July 3, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

Thank you for responding formally to the issues we had raised previously. We especially appreciate the FHWA's encouragement to the City of Fort Wayne and American Structurepoint to investigate the feasibility of preserving some of the houses on the south side of State Boulevard between Terrace Road and Eastbrook Drive and your firm's resulting proposal, on behalf of the City, to attempt to leave in place the houses at 112, 134, and 138 East State Boulevard. We think that if those houses can be avoided by this project, then the remaining segment of the original State Boulevard, as it passes through the Brookview-Irvington Park Historic District, will retain somewhat more of its historic character than it would if all houses on the south side of State Boulevard in that area were to be removed. We have similar hopes for the proposal to leave bypassed curbs and sidewalks in place.

We have no further comments or recommendations at this time for avoidance, minimization, or mitigation of this project's likely effects.

We have received your July 16, 2012 cover letter transmitting the archaeological short report (Stillwell, 7/11/12), which you advised us in your July 2 letter would be forthcoming. We will reply to your July 16 submission in a separate letter.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.
Larry Stillwell, Archaeological Consultants of Ossian, Inc.



Division of Historic Preservation & Archaeology 402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



August 13, 2012

Briana M. Hope
Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana archaeological short report (Stillwell, 7/11/12) of additional areas for the State Boulevard Reconstruction Project (Designation No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("SHPO") has conducted an analysis of the materials dated July 16, 2012, and received on July 16, 2012, for the above indicated project in the City of Fort Wayne, Allen County, Indiana.

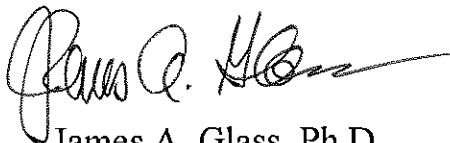
Based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of

Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. If you have questions about buildings or structures please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,

A handwritten signature in black ink, appearing to read "James A. Glass", with a long, sweeping horizontal stroke extending to the right.

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:wt

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E., Indiana District, Federal Highway Administration
Patrick A. Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.
Larry Stillwell, Archaeological Consultants of Ossian, Inc.
John L. Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources
Wade T. Tharp, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources



AMERICAN
STRUCTUREPOINT
INC.

August 29, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street in Fort Wayne, Allen County, Indiana. The project is intended to improve corridor connectivity along State Boulevard for both motorists and pedestrians. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you were invited to be a consulting party to participate in the Section 106 process via mail with a letter dated March 23, 2009. In response to this invitation or as requested, we have recorded your wish to participate as a consulting party to assess the potential impact of this project on historic properties within or adjacent to the project area.

A Historic Properties Report (HPR) was completed in September 2009 by The Westerly Group, Inc. An Additional Information Report was completed in April 2012 by Weintraut & Associates, Inc. Two historic districts listed in the National Register of Historic Places (NR) were identified within the Area of Potential Effect: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). In addition, one historic property was also previously determined eligible for the NR within the Area of Potential Effect: Bridge over Spy Run (NBI No. 0200273).

Enclosed, please find a copy of the August 27, 2012, Federal Highway Administration (FHWA) Findings and Determinations required under procedures established for Section 106 consultation. The approval indicates FHWA concurs with the Area of Potential Effect, the Determination of Properties listed or Eligible for listing in the National Register of Historic Places, and the Determination of Adverse Effect for the proposed State Boulevard Reconstruction Project (DES No. 0400587). Included with this documentation is all correspondence regarding the proposed project and the Section 106 process, including all correspondence since the September 2011 Consulting Parties Meeting.

IN20071404

Linda Weintraut, Ph.D.

August 29, 2012

Page 2

Under the provisions of 36 CFR 800.5(c), the state historic preservation officer and all consulting parties have 30 days from the receipt of an adequately documented FHWA determination to take exception to that determination. You are asked as a consulting party to provide any comments or concerns you have regarding the FHWA Finding of Adverse Effect.

Please provide all comments to:

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Comments must be submitted no later than October 4, 2012. If you do not comment on the Adverse Affect finding by October 4, 2012, your concurrence will be assumed.

In order to initiate the Resolution of Adverse Effects process, we are inviting you to participate in a Consulting Parties Meeting scheduled for:

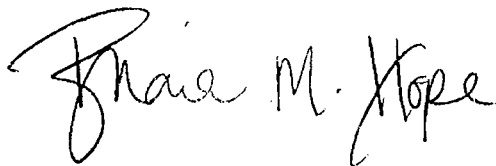
Wednesday, September 19, 2012, at 10 a.m.
Allen County Public Library, Meeting Room A
900 Library Plaza, Fort Wayne, Indiana

The purpose of this meeting will be to discuss Section 106 and potential mitigation measures to offset the Section 106 finding of Adverse Effect. An agenda for the Consulting Parties Meeting will be provided prior to the meeting date. Also enclosed is a copy of the draft Memorandum of Agreement (MOA) to be discussed at the Consulting Parties Meeting. Once the Consulting Parties Meeting is complete and all comments and suggestions have been compiled and taken into consideration, a revised MOA will be distributed to the MOA signatories, as well as provided to the consulting parties for informational purposes.

As you may recall from previous coordination and discussions, FHWA was considering elevating the required National Environmental Policy Act (NEPA) documentation from a Categorical Exclusion Level 4 (CE4) to and Environmental Assessment (EA). Due to the level of public interest associated with this project, FHWA has requested that the environmental document be elevated to an EA. By elevating the proposed project to an EA, a Finding of No Significant Impact (FONSI) will be required to conclude the NEPA process.

We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. We look forward to meeting with you on Wednesday, September 19, 2012, at 10 a.m.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

Page 3

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandeburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Hanġine, Brookview Civic Neighborhood Association
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
Larry Heil, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amy Johnson, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT

September 14, 2012

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

To: Ms. Briana M. Hope

Fm: Karl Dietsch, Resident of Brookview Neighborhood

Cc: Shan Gunawardena, City of Fort Wayne

Subject: Safety issue regarding lack of westbound turnoff lane on State Boulevard for Oakridge Road

Thank you for a copy of the memorandum of agreement received recently for the Spy Run to Cass Street, State Boulevard Reconstruction.

Based on the detailed street blueprints enclosed, I have an issue as follows:

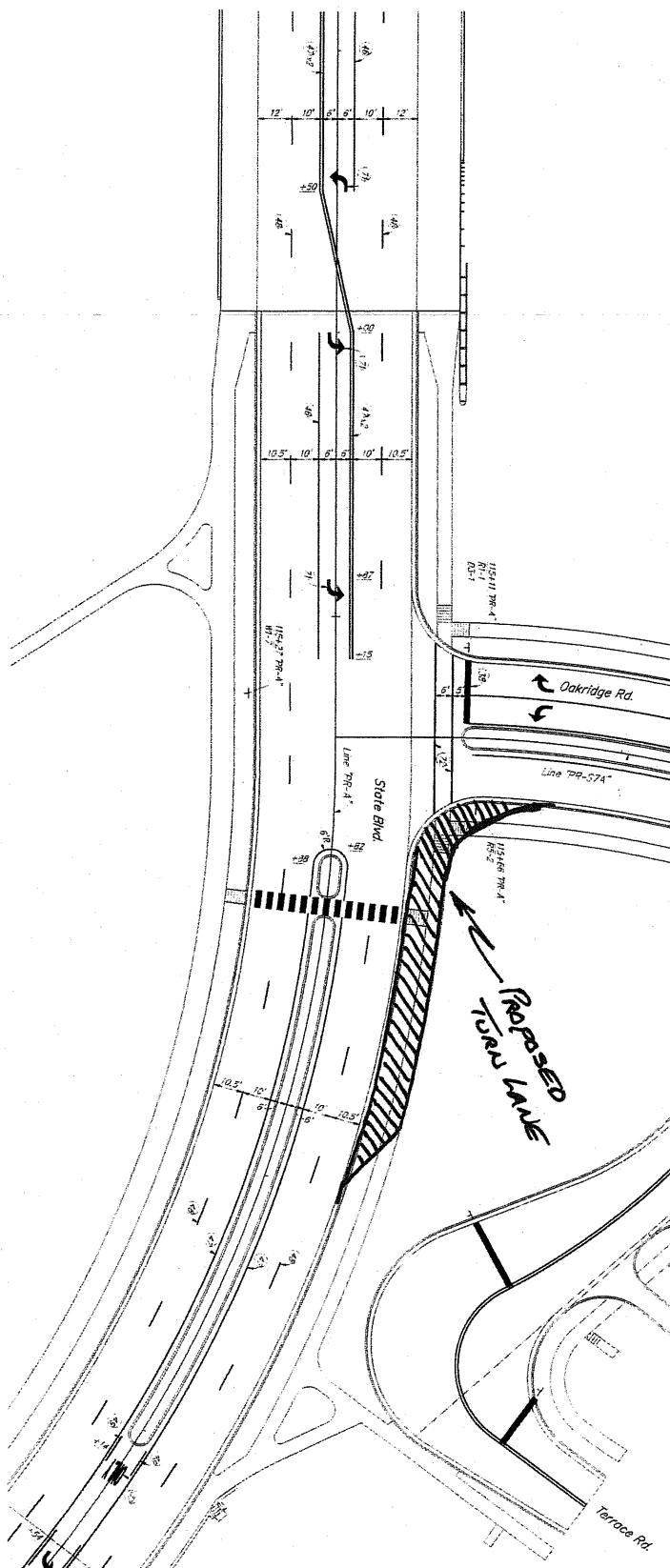
1. Westbound traffic on State Boulevard is lacking a separate turn lane for turning right (north) onto Oakridge Road. This is of concern because westbound traffic on State Boulevard is accelerating from the Clinton Street intersection at the same time some traffic is slowing to make a right turn into Oakridge Road.
2. This safety issue can easily be resolved by having a short turn lane as per my attached print.

Regards,

Karl Dietsch, Resident @ 2313 Oakridge Road
Phone: (260) 484-1399

ATTACHMENT #1
KARL DIETSCH

115



September 17, 2012

To Whom It May Concern,

We as the below listed residents of the Brookview Neighborhood currently reside within the proposed footprint of the E. State Blvd. widening project. We want to take this opportunity to make our voices heard. It seems the only people truly getting an opportunity to voice their opinions are those opposed to the project. We would like to be sure our voices in SUPPORT of the project are not being lost among the more vocal opposition by special interest groups, politicians and Brookview residents whose homes are NOT slated for removal.

Many of us listed on this letter have lived in our residences for many years. We have a very personal interest in this proposed project. Over the years we have seen a substantial increase in traffic along State Blvd. Multiple times throughout the day traffic comes to a standstill from the 300 block of West State Blvd to the 300 block of East State Blvd. This bottleneck makes it nearly impossible to exit our driveways or to enter onto State Blvd from intersecting streets within that area. As the city continues to grow and progress, we feel this issue will only continue to get worse. Those of us on the south side of the 100 block of E. State Blvd also have to contend with large trucks running up off of the curb and driving along our sidewalk / park strips. Because the road is so narrow and the curve is so sharp they do not stay on the paved surface. Listening to the tractor trailers bounce back down onto the pavement has become a constant sound at all hours of the day and night. Add to this the sound of squealing tires as cars try to take the curve at to high a rate of speed and it makes it difficult to enjoy ones time at home.

As residents of this area of State Blvd. and Eastbrook Dr, our quality of life has been greatly affected by the multiple flooding instances that occur yearly in this area. We have to depend on the City of Fort Wayne to place 18"- 36" clay dikes in front of our houses to prevent flood waters from overtaking our residences. These dikes, although necessary... prevent us access to our driveways and property. This leaves us trying to find an alternate location for our vehicles along side streets. Often times our vehicles are stranded in our garages and driveways because we were unlucky enough not get them moved before the thousands of pounds of clay were trucked in. Once the flooding has subsided, we must contend with the mess left behind while the dikes sit and await removal. The issues however are not resolved, even when the clay is taken away. After weeks of the clay sitting in flood water, once removed we are left with massive amounts of mud and debris in the street, our yards and driveways. This flooding halts traffic along one of the cities major east / west streets for days and weeks at a time. This causes an inability of people who live in the area to get to work and take their children to school, not to mention public transportation, school busses and emergency vehicles must also find ways to function when this major artery ceases to be passable. These issues with flooding have, we believe, caused a decrease in our property values and undue financial hardship. Frankly, no one should have to live like this year after year. The fact that the city is trying to address this issue with the State Blvd. widening project gave us hope that a solution was forthcoming. Now... years later it seems this project has become more about personal agendas and political attention then it has about solving these problems. You cannot imagine the stress these delays continue to cause those of us who are most intimately affected. We believe allowing our houses to remain under the guise of historic preservation and community support only makes our problem worse.

The roadway has already been raised for new bridge on Clinton St. south of State Blvd. so it seems counterproductive not to raise the bridge at Spy Run Creek. By delaying or even stopping the State Blvd. project, the lower roadway/bridge on State Blvd. between Westbrook and Eastbrook will only cause flooding in the area to worsen.

It simply does not make sense to not move the road to the south and install a new bridge with the same surface height as the new bridge on Clinton Street. Again, special interest groups, residents outside the footprint for property buyouts and local politicians seems to have gotten so caught up in their own agendas that many of us feel our concerns and support are being drowned out.

We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated and widened to the south of its current location. We also support the removal of the old bridge at Spy Run Creek and the constructing of a new bridge on the relocated State Blvd. We believe that the city / engineers have taken great care with the proposed widening project plans and have the best interest of not only the City of Fort Wayne, but we as the residents who live within the actual footprint of the project. Please do not let those of us desperate for a resolution to these problems continue to be unheard, ignored or worse yet, used as pawns in some sort of political grandstanding.

Please note that of the existing 13 homeowners on Eastbrook and along the south side of E. State Blvd. (between Eastbrook and Clinton), the vast majority want to be listed on this letter as supporting the project and speedy property buyouts. We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issues unresolved.

Again, those of us in favor of the project and the buy out of our homes do not believe that our interests and the interests of the City Of Fort Wayne are being served by delaying or trying to stop this project. We respectfully ask that this project proceed in a timely manner.

Sincerely,

Susan R. Haneline 134 E. State Blvd.

Moshin & Karen Hudda 112 E. State Blvd.

Rex Well, NLI Inc. 128 E. State Blvd.

Lisa Strebig 138 E. State Blvd.

Darrin Klopp 142 and 146 E. State Blvd.

Michael McKinley 2252 Eastbrook Dr.

Peggy S. Roy 2248 Eastbrook Dr.

Joe and Amy Sale 2244 Eastbrook Dr.

Stephanie Mays 2240 Eastbrook Dr.

***Attachments: 3**

- 1. - A photo map with numbers designating homes of persons mentioned in or signing this letter.**
- 2. - 3 pages with signatures or verbal requests to be listed in favor of property buyouts.**
- 3. - Summary and 2 typed pages with property owners information - this corresponds with the above signatures and photo map**



Google earth

© 2012 Google

In summary, 9 out of the 13 property owners are in favor of being bought out. (There are 14 properties shown in this document, but 1 owner has two properties)

Of the 4 remaining properties:

1 property is vacant - no owner information available

1 property is a rental, no landlord contact info. available

1 homeowner does not wish to be bought out

1 home owner was unreachable therefore we are unable to formally list whether they are for or against buyouts.

EASTBROOK DRIVE

2236 Eastbrook Drive

Owner: **KRISTIN A CREAGER & LOIS JEAN LOCKHART**

House 1 on the Map

* Spoke with a male at the residence, he stated he does not wish to sign paper in favor of buyout

2240 Eastbrook Drive

Owner: **ERIKA E MAYS MERRICK & STEPHANIE G MAYS**

260-471-1367 or 260-341-3988

House 2 on the Map

* Signed letter in favor of buyout.

2244 Eastbrook Drive

Owner: **Resource Network Inc Indiana - Joe Sale**

House 3 on the Map

* Signed letter in favor of buyout.

2248 Eastbrook Drive

Owner: **PEGGY S ROY**

260-493-1495

House 4 on the Map

* Signed letter in favor of buyout.

2252 Eastbrook Drive

Owner: **MICHAEL GARY MCKINLEY**

House 5 on the Map

* Spoke with Michael McKinley via telephone on 9/18/12, he made a verbal request to be included in these documents of owners supporting buyouts.

East State Blvd

112 E State Boulevard

Owner: **MOSHIN A HUDDA & KAREN S HUDDA**

260-484-5244

House 6 on Map

* Signed letter in favor of buyout.

128 E State Boulevard

Owner: **NLI INC**

Rex Wells - Verbal signature given on 9/18/2012 via phone

NLI INC / Alliance Property Management

1616 N Harrison St

Fort Wayne, IN 46808 (260) 420-1428

House 7 on Map

** Rental Property,

* Spoke with rep. of NLI INC, owner Rex Wells made a verbal request (via telephone) to be included in these documents of owners supporting buyouts.

134 E State Boulevard

Owner: **SUSAN R HANELINE**

260-482-2200 or 580-2447

House 8 on Map

* Signed letter in favor of buyout.

138 E State Boulevard

Owner: **LISA S STREBIG**

260-760-1956

House 9 on Map

* Signed letter in favor of buyout.

142 E State Boulevard

Owner: **DARRIN D KLOPP**

House 10 on Map

* Verbal request on letter in favor of buyout, contact phone number listed.

146 E State Boulevard

Owner: **DK PROPERTIES LLC**

House 11 on Map

* Verbal request on letter in favor of buyout, contact phone number listed.

154 E State Boulevard

Owner: **NORMA R L/EST PANKOP & PANKOP TRS**

House 12 on Map

This house is a vacant property - no information available on owner

158 E State Boulevard

Owner: **DANIEL J HALL & KAREN S HALL**

**** House 13 on Map**

This is a rental, no landlord contact information available.

162 E State Boulevard

Owner: **ROBERT E DAILEY & JANICE ANNETTE DAILEY**

(260) 483-1303 or 483-6316

**** HOUSE 14 on Map**

This homeowner is a member of the Consulting Parties. Attempts to contact were unsuccessful, so no opinion for or against the buyout is given.

Name of Resident: Moshin & Karen Hudda
Address of Residence: 112 E. State Blvd
Number of people residing in the home: 2 Number on Attached Map 6
Signature: Karen Hudda, Moshin A Hudda
Contact Phone Number: 260-484-5244
Additional Comments: _____

Name of Resident: Joe & Amy Solo
Address of Residence: 2244 East Brook Drive
Number of people residing in the home: 4 Number on Attached Map 3
Signature: Joe Solo
Contact Phone Number: _____
Additional Comments: _____

Name of Resident: Stephanie Mays
Address of Residence: 2240 Eastbrook Drive
Number of people residing in the home: 2 Number on Attached Map 2
Signature: Stephanie Mays
Contact Phone Number: 260 471-1367 341-3988
Additional Comments: _____

Name of Resident: Peggy S. Roy
Address of Residence: 2248 Eastbrook Dr.
Number of people residing in the home: 0 Number on Attached Map 4
Signature: Peggy S. Roy
Additional Comments: _____
Contact Phone Number: 493-1495

* Moved due to city's promise of property acquisition.
Have been renting (at a loss) for 6 years. House
has issues at much expense and finding tenants.
is nearly impossible. I consider this to be a hardship.

Name of Resident: Lisa Strebig
Address of Residence: 138 E. State
Number of people residing in the home: _____ Number on Attached Map 9
Signature: Lisa Strebig
Contact Phone Number: 260-760-1956
Additional Comments: _____

Name of Resident: Susan R. Hanelke
Address of Residence: 134 E. State Blvd
Number of people residing in the home: 2 Number on Attached Map 8
Signature: Susan R. Hanelke
Contact Phone Number: _____
Additional Comments: Author of Attached Letter

Name of Resident: NLI INC.
Address of Residence: 128 E. STATE Blvd
Number of people residing in the home: _____ Number on Attached Map _____
Signature: Verbal Request By Rex Wells of NLI INC to be added
Additional Comments: _____

Contact Phone Number: 420-1428

Contacted NLI 10/16 N. Lincoln FFWAYE INC.
Rep Rex Wells wants to be added to this document.
Request via telephone @ 9/18/2012 - may call him to verify if needed

Name of Resident: Michael McKinley
Address of Residence: 2252 Eastbrook
Number of people residing in the home: _____ Number on Attached Map 5
Signature: Verbal request by phone to show supporting buyout
Contact Phone Number: 443-3219
Additional Comments: I spoke with Mr McKinley he wished
to be listed on the residents in favor of a buyout
concentration on 9/18/12 @ 9pm

Name of Resident: DARIN KLOPP
Address of Residence: 142 E - STATE BLVD.
Number of people residing in the home: _____ Number on Attached Map 11 + 10
Signature: Verbal Request by phone to show supporting Buy out
Contact Phone Number: 249-7606
Additional Comments: _____



bethany w <bethany@weintrautinc.com>

Fwd: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

1 message

Linda Weintraut <linda@weintrautinc.com>

Fri, Sep 21, 2012 at 8:42 AM

To: bethany w <bethany@weintrautinc.com>

----- Forwarded message -----

From: **Hope, Briana** <bhope@structurepoint.com>

Date: Tue, Sep 18, 2012 at 4:12 PM

Subject: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

To: "Hope, Briana" <bhope@structurepoint.com>, "mgalbraith@archfw.org" <mgalbraith@archfw.org>, "aquinn@archfw.org" <aquinn@archfw.org>, "don.orban@cityoffortwayne.org" <don.orban@cityoffortwayne.org>, "tzeiger@indianalandmarks.org" <tzeiger@indianalandmarks.org>, "mbwedaman@frontier.com" <mbwedaman@frontier.com>, "juliemarie57@earthlink.net" <juliemarie57@earthlink.net>, "jlcooper@ccrtc.com" <jlcooper@ccrtc.com>, "indianabridges@sbcglobal.net" <indianabridges@sbcglobal.net>, "shan.gunawardena@cityoffortwayne.org" <shan.gunawardena@cityoffortwayne.org>, "dan.avery@co.allen.in.us" <dan.avery@co.allen.in.us>, "sjslick@mac.com" <sjslick@mac.com>, "jandailey59@msn.com" <jandailey59@msn.com>, "joyce.newland@fhwa.dot.gov" <joyce.newland@fhwa.dot.gov>, "larry.heil@fhwa.dot.gov" <larry.heil@fhwa.dot.gov>, "jshoaff@proparkwest.com" <jshoaff@proparkwest.com>, "jasonkaiser@indot.in.gov" <jasonkaiser@indot.in.gov>, "pacarpenter@indot.in.gov" <pacarpenter@indot.in.gov>, "mkennedy@indot.in.gov" <mkennedy@indot.in.gov>, "linda@weintrautinc.com" <linda@weintrautinc.com>, "creager.smith@cityoffortwayne.org" <creager.smith@cityoffortwayne.org>, "albertcohan@aol.com" <albertcohan@aol.com>, "tmn@barrettllaw.com" <tmn@barrettllaw.com>, "rross@martin-riley.com" <rross@martin-riley.com>, "dan@earthsourceinc.net" <dan@earthsourceinc.net>, "jglass@dnr.in.gov" <jglass@dnr.in.gov>, "jcarr@dnr.in.gov" <jcarr@dnr.in.gov>, "kdietsch@comcast.net" <kdietsch@comcast.net>, "alec.johnson@ci.ft-wayne.in.us" <alec.johnson@ci.ft-wayne.in.us>, "Crites, Scott" <SCrites@structurepoint.com>, "Zielinski, Rich" <RZielinski@structurepoint.com>, "gsmith2@indot.in.gov" <gsmith2@indot.in.gov>, "aricketts@dnr.in.gov" <aricketts@dnr.in.gov>, "wtharp1@dnr.in.gov" <wtharp1@dnr.in.gov>, "tom.cain@cityoffortwayne.org" <tom.cain@cityoffortwayne.org>

Consulting Party Members –

Attached to this e-mail is a copy of the letter sent to the SHPO's Office documenting the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

A hardcopy of the this letter has been placed in the mail. In addition, the information presented in the letter will be discussed as part of tomorrows (Sept. 19th) Consulting Parties Meeting.

Thank you,

Briana

Briana M. Hope

Project Manager, Environmental Sciences Group

7260 Shadeland Station, Indianapolis, Indiana 46256

T 317.547.5580 E bhope@structurepoint.com

F 317.543.0270 W www.structurepoint.com



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<http://www.emaildisclaimers.com/>

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Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
PO Box 5034
4649 Northwestern Drive
Zionsville, Indiana 46077
[317.733.9770](tel:317.733.9770) ext. 310

www.weintrautinc.com



IN20071404.EV.2012-09-18.LTR.ROW Minimization.bmh - Copy.pdf

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AMERICAN
STRUCTUREPOINT
INC.

September 18, 2012

James A. Glass, PhD
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to document the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

After completion of the proposed project, the three existing structures would be located between the existing and proposed State Boulevard roadways. Due to the location of the structures, investigations were needed to assess the impacts to the properties and feasibility of maintaining the existing structures between the existing and proposed roadways.

Minimization of Property Acquisition

In order to minimize acquisition of property associated with these homes, American Structurepoint, Inc., investigated options that evaluated modifications to the Oakridge Road extension to proposed State Boulevard. The first included shortening the right-turn lane and eliminating the landscaped median. This modification provided a reduction in anticipated right-of-way of approximately six feet. Constructing sidewalks adjacent to the curb with retaining wall placed at the back of sidewalks also reduced the anticipated right-of-way by an additional 16 feet.

The use of guardrail was also evaluated in an effort to minimize potential right-of-way acquisition. Currently, the proposed design utilizes the required 4:1 side slope from the proposed State Boulevard roadway, encroaching on the back yards of the homes in question. The roadway through



this area is elevated due to the need of the proposed roadway to tie into a larger bridge required to span the floodplain of the Saint Mary's River. To utilize guardrail would allow for the construction of a 3:1 side slope, resulting in a minor reduction of required right-of-way, but would ultimately be offset by the requirement of a flat shelf needed for installation of the guardrail at the top of the slope immediately adjacent to the roadway. In addition, utilization of guardrail would pose a safety concern for vehicles making a left-hand movement from Oakridge Drive to proposed State Boulevard by introducing a visual obstruction to the west.

Stormwater Drainage

In order to facilitate stormwater drainage adjacent to the homes, a drainage swale will be necessary in all options. The drainage swale will be constructed at the bottom of the proposed roadway fill slope just south of the houses in question. The drainage swale will collect stormwater runoff from both the proposed roadway and the adjacent properties located to the north. The flow will be conveyed west outletting into Spy Run. Currently, the stormwater drainage system is proposed as a vegetated drainage swale.

The option of an enclosed drainage system utilizing inlets was evaluated in this area, but would still require a vegetated swale above the enclosed system to direct water to the inlets. The enclosed drainage system did not provide a reduction of anticipated right-of-way.

Please see attached exhibits showing the anticipated impacts resulting from the proposed construction/right-of-way limits. The distance between each residence and the proposed construction limits/right-of-way is marked, as well as the anticipated elevation differences. The most significant elevation difference of seven feet exists at 112 East State Boulevard and decreases to three feet near 138 East State Boulevard.

Recommendation

The evaluation found that the properties would be significantly impacted by construction of the proposed roadway and drainage swale.

- 112 East State Boulevard – The property would be located approximately 7.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately eight feet from the existing residence. Approximately 62 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.
- 134 East State Boulevard – The property would be located approximately 3.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately ten feet from the existing residence. Approximately 55 percent of the existing backyard/greenspace between the house and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building and significant portion of the existing yard/greenspace would be included in the area to be acquired.

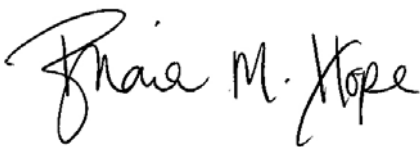
- 138 East State Boulevard – The property would be located approximately three feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately three feet from the existing residence. Approximately 77 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalk, and drainage swale. The existing garage, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.

It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions. The significant reduction in greenspace between the existing residence and proposed roadway, impacts to existing drives, and removal of non-residential structures located on the properties is appropriate justification for the complete acquisition of the parcels in question.

In additional coordination with the Indiana Department of Transportation, the Right-of-Way and Land Acquisition Section advised American Structurepoint representatives that as part of state and federal law, land cannot be purchased from one property owner and given to another to offset the amount of land being acquired. Therefore, the impacts to one parcel cannot be offset by acquiring an adjacent property and giving or selling that acquired property to an adjacent owner. The land acquired from the parcels in question would result in a significant permanent reduction of property and green space.

We hope that the information in this letter adequately resolves the requested investigation of options to minimize impacts at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. This letter is intended for informational purposes only. A copy will be sent to all consulting parties concurrently with this correspondence. If you have any additional questions, please do not hesitate to contact me at (317) 547-5580, or at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

A handwritten signature in black ink that reads "Briana M. Hope". The signature is written in a cursive, flowing style.

Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

Distribution List

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Haneline, adjacent property owner
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, adjacent property owner
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT





MEETING MINUTES

Location: Allen County Public Library, 900 Library Plaza, Meeting Room A, Fort Wayne, Indiana

Date: September 19, 2012

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Consulting Party Rich Zielinski, Scott Crites, Briana Hope, Chris Meador (American Structurepoint, Inc.)

Attendees: Shan Gunawardena, Creager Smith, Tom Cain (City of Fort Wayne)
Dr. Linda Weintraut (Weintraut & Associates)
Dr. James Glass, John Carr (IDNR, Division of Historic Preservation and Archaeology)
Patrick Carpenter (INDOT, Cultural Resources)
Jason Kaiser, Greg Smith (INDOT Fort Wayne District)
Joyce Newland, Larry Heil (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Susan Haneline, Karl Dietsch (Adjacent Property Owner)
Todd Zeiger, Catherine Wright (Indiana Landmarks)
Edward Welling (Friends of the Parks of Allen County)
Dan Ernst (Earth Source, Inc.)

Conference Line: Najah Duvall-Gabriel (Advisory Council on Historic Preservation)

Attendees Frank Saurez (City Public Works), Marty Bender (FWPD/City Council), Shawna
Observing Nicelley, Larraine Weier, Herb Weier, Thomas Roach III, Sarah Krugen Geyman
Meeting:

1. The meeting was held at 10:00 a.m., September 19, 2012, to discuss the following agenda items:
 - 1) Section 106 Update
 - 2) Section 106 Action Items regarding Adverse Effect Finding
 - 3) Additional Mitigation Measures
2. Briana Hope introduced herself and began the meeting with introductions around the room and by stepping through the first item on the agenda, including an update on project progress since the last consulting party meeting (09/2011).
3. Dr. Linda Weintraut presented a PowerPoint presentation briefly recapping the Section 106 process, including identified properties, minimization and avoidance measures, effects, and potential mitigation measures.

4. Susan Haneline asked during the avoidance and minimization portion of the PowerPoint presentation why the owners of the 3 residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes, as they do not want to remain. She thought the property owners should have a say as to whether or not the impact to their property is significant enough to justify leaving the house in place.
 - Larry Heil responded that Section 4(f) requires evaluation of measures to avoid and minimize impacts to the historic district. The evaluation is focused on the historic resources protected by the law. Any time right-of-way is acquired the property owner is reimbursed for the impact to the property, but FHWA is required by law to minimize impacts to the historic district. If there is a way to preserve contributing resources, which the three homes in questions are, FHWA is required to preserve these resources.
5. Following the PowerPoint presentation, Briana Hope opened up the meeting for general discussion regarding the potential mitigation measures proposed and any additional ideas regarding potential mitigation.
6. Michelle Briggs Wedaman stated that she will be providing extensive written comments related to the materials sent with the invitation to the consulting parties meeting, but generally had the following comments:
 - Related to the agenda, feels it is inappropriate to have moved beyond the discussion of purpose and need.
 - Understand timelines and agency requirements but feels the larger issues of real time and real impact on the community and are not guided by the timelines that dictate quick movement on the project but by the guiding principles of the Plan-It Allen report, the comprehensive plan for Allen County. She will be commenting on the relevance of the document in the submitted comments.
 - Context Sensitive Solutions (CSS) – CSS should be applied to the larger issue of the road footprint itself and should occur at the beginning of the project not towards the end of the project or the last stage of a project.
 - The neighborhood supports Sue Haneline and the most directly impacted residents. The neighborhood was not invited to the agency meeting in June and not included in the decisions that were made at the meeting potentially impacting the Oakridge extension and the effort being made to preserve the homes.
 - Encouraged agencies and project sponsors to consider what the final project will look like and to reconsider the outcome of the project and forcing residents to stay.
7. Todd Zeiger had the following comments:
 - Encourages the Advisory Council of Historic Preservation (ACHP) to be involved now and moving forward.
 - Concurs with “adverse effect” but feels it is not complete. Concerned about bifurcation of the district by creating a visual dissection of the neighborhood and district both height-wise and width-wise. The bifurcation needs to be discussed in the effects.
 - Dr. Linda Weintraut stated that the effects document will be updated to include the bifurcation of the historic district.
 - Feels that impacts to individual resources have been minimized by the City of Fort Wayne in what he believes is anticipatory demolition as part of a flood control project. The individual demolitions are directly related to the project because one of the purposes of the project is stated as flood control issues. This is not discussed in official findings and should be.

- Will be responding in writing by October 4th and encourages that before ACHP makes its formal decision that the written comments are included and considered and not just the conversation from the consulting parties meeting.
 - In the letter announcing the consulting parties meeting it states that FHWA is elevating the project to full Environmental Assessment (EA). Questioned why mitigation was being discussed when the full impacts that are going to be discussed in the EA are unknown.
 - Larry Heil responded that when a project has some sensitivity to it FHWA prefers to issue a definitive decision and a Categorical Exclusion (CE) by definition is a type of project excluded from the requirement of NEPA documentation. Because of the sensitivity and public concerns, FHWA wanted to have an official FHWA NEPA decision. The next steps in the EA process include finalizing a Draft 4(f) Analysis, revising the Draft MOA, and finalizing data collection to compile a Draft EA Document to present to the public. Once the Draft EA is released for public involvement, a public hearing will be held and the public will be encouraged to comment and present facts or clarify that facts are inaccurate from a technical standpoint.
8. Karl Dietsch pointed out a safety concern regarding traffic traveling west on new State Boulevard. Traffic will be picking up speed at Oakridge as it is heading west at the same time traffic making a right turn on Oakridge will be slowing down, thus increasing the risk of rear-end accidents. A short right turn lane was suggested for westbound traffic along State at Oakridge Road.
 9. Tom Cain pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in those landscape elements in the documentation need to be discussed. The visual and spatial components of the larger landscape need to be understood so they can be addressed in a mitigation discussion.
 10. Michael Galbraith also stated that he would be providing detailed comments by October 4th and had the following comments:
 - Discussion of mitigation and minimization is a red herring to avoid discussing the issues that have been brought-up and not discussed by the consulting parties.
 - Minimization efforts documented and sent out via e-mail late the evening prior to the consulting parties meeting did not give consulting party members adequate time for review.
 - Larry Heil stated that the letter did not represent FHWA's position.
 - Raised concern about a new Section 106 consultant with brand new information and being able to adjust to Dr. Weintraut as a consultant.
 - Based on the PowerPoint presentation and the May 19th letter from American Structurepoint to Dr. Glass, Mr. Galbraith feels the APE is still too small for the project.
 - Encourages ACHP to consider that this project separates and segregates projects happening in the same geographical location and same time period impacting the same neighborhood and separates them based on agencies. They should be considered amalgamated for review due to their cumulative impacts on the neighborhood. A total of eight federal aid projects are happening in the same area at the same time.
 11. Briana Hope reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed that the consulting parties meeting was an opportunity to verbally express their mitigation comments and ideas.
 12. Michelle Briggs Wedaman expressed the following additional comments:
 - Has not received a traffic study for the area that has been requested since the beginning of the project. How is the need for this project (safety and flooding) documented without a traffic

study? Have issues with the safety and congestion part of the Purpose and Need, specifically related to accidents at intersections, been studied? How will this project calm traffic?

- Larry Heil indicated that FHWA provides funds to help Major Metropolitan Planning Organizations (MPOs) develop reliable traffic models. FHWA reviews the traffic models every four years to evaluate the models. FHWA has a high degree of confidence in the models and utilizes these models around the country to aid in project development.
- 2005 Flood Study is at odds with the project and in no way points to this project as a solution to real life flash flooding.

13. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed her concern that the consulting party members were losing the opportunity to have input related to mitigation.

14. John Shoaff commented that he believes the purpose of the proposed project is to make a major arterial out of a street that runs through a number of neighborhoods. Arterials do not have a good safety record related to pedestrians. Mr. Shoaff also made the following comments:

- Presented a letter signed by 15 neighborhoods opposing the project and stated that the letter would be included as part of his comments submitted by October 4th.
- Stated that the majority of the public does not agree with the Purpose and Need Statement.
- The historical aspects of the neighborhood are great and will be destroyed by this project.
- Concerned about the project having a negative impact on property values.

15. Michael Galbraith reiterated his concern regarding the multiple federal projects involved in the neighborhood that are not be looked at and evaluated cumulatively.

16. Edward Welling agrees with the adverse effect but feels that the discussion of mitigation is premature and that the proposed MOA is an attempt to confuse the process, especially since the FHWA elevated the project to an EA. Mr. Welling also had the following comments:

- Asserted that the APE is not appropriate.
- Turning the roadway into a major arterial and the addition of traffic will impact the quality of life along the corridor.
- Suggested that the discussion of mitigation should be postponed until EA is complete.

17. Susan Haneline commented that 14 houses in the initial footprint were under the impression that the project was not just about traffic flow but also related to flooding. She also had the following comments:

- Every house in the footprint of the project has been impacted by flooding.
- Presented a letter documenting the vast majority of owners in the footprint are requesting a buyout due to loss of property, traffic flow issues, inability to access property, flooding, and financial hardships.
- Feels that being listed in a historic district is making it harder for the city and state to address the concerns of the people in the footprint of the proposed project.
- The majority of the home owners in the footprint of the project were grateful for the opportunity presented by this project to vacate their properties, open up a green space, and retain the original footprint of State Boulevard, but dealing with an elevated roadway just for the city to protect contributing properties is not a long-term solution.
- Does not oppose project and looks at it as an opportunity to not continue to lose value in properties.

- Requested agencies and project sponsors to not forget to put a face with the people that are in the footprint and intimately dealing with the flooding issues and not being able to get out of their driveway because of traffic.
18. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation.
19. Tom Cain commented that preservation of historic buildings depends of the viability of the properties to remain invested in, in the future. He had the following supporting comments:
- Homes that are in a floodplain have limits on how much can be invested in them, their future preservation may not be ensured.
 - The Section 106 process may preserve buildings but it may also create a condition unattainable for preservation and economic life and existence as a structure.
 - The process of preserving specific structures may impact the need to provide mitigation for the larger landscape and planning characteristics of the neighborhood.
 - The preservation of several specific structures may not ensure long term preservation and limit opportunities to mitigate larger landscape design issues which are the more significant components of the neighborhood based on the Arthur Shurcliff plan for the area.
 - Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. The two designers involved, George Kessler and Shurcliff, had differing approaches to composition of roadways, one was a more formal straightforward boulevard with setbacks, and tree lined streets and the other a more curvilinear pattern.
20. Dr. Linda Weintraut commented that the National Historic Preservation Act was established so that historic properties and modern undertakings could exist in harmony; one was not to be at the expense of the other. We are trying to come up with a compromise that allows both to move forward. Dr. Weintraut again encouraged the consulting parties to think about mitigation and how that might offset the adverse impacts of the undertaking.
21. Michael Galbraith commented that he agrees that the Section 106 process was designed to allow historic properties and transportation projects to live in harmony, but what is happening to the neighborhood is not harmonious. This neighborhood is being destroyed by a combination of federal projects that are being executed piecemeal that have destroyed dozens of houses. If the project proceeds it will destroy dozens more, property values, and the historic resource in the process.
22. Dr. James Glass commented that he recalled at previous consulting party meetings some very strong opinions were voiced similar to the ones heard today and that there still does not seem to be a degree of consensus from the community about the project. He stated that presumably the City of Fort Wayne very strongly supports the project, but also heard John Shoaff (City Council Member) express a very different point of view, as well as differing points of view from the neighborhood. He also had the following comments:
- One issue the community needs to consider is: can a consensus be developed on this project. From a section 106 perspective, the State Historic Preservation Office (SHPO) depends on the project sponsor and the community to develop some kind of consensus on the need of the project and whether all the issues in terms of feasibility have been worked out. SHPO then takes that as a starting point for considering effects on historic properties and ways to mitigate and lessen adverse effects.
 - In June, SHPO had a very preliminary meeting with the agencies to consider what the anticipated project design was based on the safety factors and consideration of alternatives. The meeting

was to provide a basis for a starting point at the consulting parties meeting to try to begin a discussion on mitigation to offset some major adverse effects.

- The meeting today is an invitation for consulting party members to provide additional mitigation ideas or alternative ideas.
- The letter yesterday was sort of a wildcard that no one anticipated. We were waiting for this discussion to bring forth the idea on the terms of the 3 houses and the many significant adverse effects. We would welcome any additional ideas for mitigation.

23. Larry Heil commented that it is critical to point out how federal funds are spent in this area. The decisions are made by the Northeastern Indiana Regional Coordinating Council (NIRCC) Policy Board. There is a reason that MPOs are established and required by federal law. These are regional issues and it is within that context that the regional plan that they identify what the arterials are in the region. State Boulevard is and has historically been an arterial. The plan that is developed and adopted by all the officials from the entire region is what guides all investment in the area. None of the decisions are made purely by City of Fort Wayne staff or one or two elected officials. They are made by the policy board which is made up of a group of elected officials so there is a regional perspective.

24. Dr. Linda Weintraut again asked for mitigation measures that consulting party members may wish to put forward during this opportunity of the Section 106 process. She stated that this is the chance to offer mitigation. Dr. Weintraut explained that the agencies and project sponsors are looking for ideas to offset the impacts such as educational programming, CSS, or any other ideas that the community could put forward. This is the consulting party's opportunity in the process to be heard on this issue.

- Dr. John Carr added the request for any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion.

25. Tom Cain commented that the discussion that has occurred for most the meeting, has taken away from the opportunity to talk about anything based on what is in front of us. He stated that he has a fairly lengthy list of observations and suggestions that he didn't feel he had enough time to discuss.

- Larry Heil requested the Tom Cain provide the comments and suggestions in writing and ensured him that FHWA would review them and take them into consideration in developing the MOA. He also reiterated that the purpose of the meeting was to talk about potential mitigation features and the historical elements that can be preserved.

26. Susan Haneline suggested that the State Boulevard curve be considered. The curve will remain and is not being lost, but as a resident of that particular footprint feels nothing is being done to showcase the feature. By allowing homes to remain in the footprint that are being vacated due to flooding and traffic problems, the curve will not be showcased. The homes that are not retaining value or are specific structures of historic significance should be removed. Ms. Haneline said that there are twenty other houses similar in style to the ones in the footprint found within an equal area [of the historic district]. The significance is simply the footprint of the area. By retaining the existing structures, nothing is being done to showcase the beauty of the Brookview Neighborhood or the feeling that the people that live there would like to have. Removing the negative aspect of leaving property owners that do not wish to remain and finding a way to showcase the features that everyone is feels are so important should be a focus of mitigation.

27. Michael Galbraith commented that the call for the five-minute mitigation measure is inappropriate. He feels the scheduling of the meeting in such a hurried fashion before the consulting parties are allowed a full opportunity to comment on the proposed mitigation measures in writing is inappropriate. People deserve an opportunity to review what has been presented to them and an opportunity to comment in writing if they do not feel comfortable arguing in front of 20 to 30 people.

- Jason Kaiser commented that what was presented were ideas for minimization efforts and that the agencies, project sponsors, and representatives are soliciting additional ideas. An effort was

made to give some ideas for minimization and mitigation to help spur further discussion among the consulting party members.

- Patrick Carpenter stated that the rationale for having the current meeting was to have it in the middle of the comment period. He stated that this does not foreclose comments or discussion but provides an opportunity for consulting parties to hear what other people have to say. This opportunity was provided to avoid comments going into a vacuum and allow consulting party members to get an idea of what everyone is thinking. The meeting was also intended to allow consulting party members to hear comments and ideas and be encouraged to formulate new ideas and put those into your comments that are due October 4th. This is an opportunity for people to provide input and not foreclose anything it helps encourage people to get involved.

28. Dr. Linda Weintraut stated that the consulting party members have until October 4, 2012 to submit written comments and encouraged everyone to comment looking at the minimization efforts, mitigation ideas, and thus consider how to creatively mitigate for the adverse effects. Your role as a consulting party member is to offer up ideas regarding mitigation regarding historical properties.

29. Michelle Briggs Wedaman commented that she is here as a representative of the neighborhood but also carrying a letter representing over 11,000 households and businesses. A lot of the comments that have been heard today speak to the public process. Ms. Wedaman said that she feels the public process has not been followed. Commented that for the last 4 plus years we have been involved in this process and the community has been asking the same questions in looking at this complex project. No roadmap has been provided since August 6, 2008 when we started working with the department of public works. The CSS approach process has been designed for sorting out these types of complex issues. We know that both the State and FHWA join us in wanting to spend the \$11 million dollars in a meaningful way that will benefit the community, but we ask you again to really listen to the questions we have, including why other public offices are not represented.

- Where are our policy officers, economic development officers, and historic preservation officer? Why have these and other officers not been allowed at the central planning table for this project?
- Why is the City going ahead with a project that is clearly out of line with the goals of this community, except just a few residents?

We stand behind the residents of State Boulevard in their plea not to be left next to a highway. To do this in the name of historic preservation makes it even more disturbing to us. Please join us in understanding why we are asking these questions.

30. Karl Dietsch commented in regards to the adverse effects that he sees many more positive aspects to this alternative than adverse effects. He will be able to turn onto State Boulevard safely as compared to now due to visual obstructions. He will also be able to always travel west where before he could not due to road closures as a result of flooding.

31. Patrick Carpenter requested that any comments received be included on the City's website.

- Michael Galbraith requested that the link be re-emailed to the all the consulting parties.

32. Briana Hope asked for any closing statement from FHWA and SHPO, thanked everyone for his or her participation, and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will update the online archive for the project's Section 106 correspondence and documents and provide the e-mail address to all consulting parties via e-mail.
- Weintraut and Associates and American Structurepoint will update the effects documentation as needed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or bhope@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:

Enclosures

August 21, 2012

Ms. Brianna M. Hope
Environmental Project Manager
American Structurepoint, Inc.
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Indianapolis, IN 46256

Sara Kruger Geyman
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Fort Wayne, IN 46805

Re: Consulting Party/Mitigation Meeting, August 19, 2012

Ms. Hope,

I am so pleased that I was able to attend this meeting; it was quite informative. It was interesting to see first hand how our government works.


Before stating my comments, I would like to first point out that as a resident, I, along with others were uninvited to this meeting; a meeting of *consulting parties*. My first concern is that residents are not and have not been consulted in this matter. And the refused entry of said residents at this meeting only adds to the concern that plans are made behind closed doors – even against the wishes of the taxpaying citizens. Thankfully, due to our councilman, some members of this meeting acquiesced and we did attend.

Secondly, the grossly unprofessional behavior of Dr. Weintraut of Weintraut and Associates was appalling. The sighs and eye rolls that Dr. Weintraut displayed clearly showed her disdain for community feedback, and was very disrespectful.

Comments, as requested, regarding the issues of this plan are as follows:

- Residents were never, and are still not being included, making this faulty from the start.
- The Purpose and Need for this project has been decided and laid out in an undemocratic way. The purpose and need is still in question. The city has failed to demonstrate how this project will resolve issues. Residents concerns have had no bearing on Public Works, as they adhere to their plan and plow through historical homes, neighborhoods, and lives.

flooding has yet to be addressed. Failing to understand that and continuing with this plan is equal to giving a patient an aspirin to cure their cancer.

- **Mitigation is premature** in a plan and a process that has been faulty from the beginning. It is a **proverbial lollipop** stuck in the hands of residents to quiet them down and distract them from the truth.
- Albeit premature, the suggested mitigation measures were offensive:
 - “Save” three houses (but add an elevated 5 lane highway.) – Lollipop
 - “Take a picture of the bridge” (but destroy the land and neighborhood in which it once stood) – Lollipop
 - “Educational Measures” – (Is this where we tell future generations of the beautiful thriving neighborhoods of Brookview, Northside and Forest Park Blvd. that once upon a time existed but now is a highway?) – Poison lollipop
- Per the Federal Department Representative attending.. *“Addressing speed is not related to this project.”* It should be, because that is what is being invited. It seems the right hand knows not what the left hand is doing.
- This plan does not solve problems, it creates more of them. Destruction, speeding, urban blight, and the need for more of the same as it invites more of the same.
- This project is not a small isolated proposal, but a disastrous massive plan with immeasurable impact on property (both physical and fiscal), lives, entire neighborhoods, ultimately the city itself. Fort Wayne Public Works seem bent on cutting off its nose to spite its face.
- The stated “safety and congestion” solution that this plan claims, are two entirely different things. ‘Safe’ is slow and minimal. To add an element of safety one should consider ADDING curves, reducing the number of vehicles. This plan will add DANGER not safety. 
- Congestion should be relieved by sending traffic to the outlying arteries – Coliseum Blvd., Hwy 469 –for which they were created. Inner city streets should be meant for inner city living and working, and playing.
- Arterials DO NOT offer safety (per a city study), but rather an increased rate and higher number of accidents.
- It's been pointed out that many offices have NOT been included in the making of plans (i.e., The Housing Dept.) Where is the ;

Traffic study? – This has been requested, but not heeded

Environmental impact study?

Cultural impact study?

Housing study?

Death Study Long term effects of highways *and heart attacks* through neighborhoods study? *Per Harvard*

Design professional?

Urban Planning?

My suggestions:

- This massive and poorly managed project should be **halted immediately** and any plans to modify State Boulevard should begin anew, with a publicly transparent planning process which involves residents and resident neighborhood leaders, elected representatives, urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.
- Keep the well designed traffic calming curves and force commercial and lateral traffic to the arteries north – for which they were created.
- Think progressively! Look to the success of other downtown areas. Invite people back in to the urban areas by **CREATING MORE** desirable areas. Discourage speed and urban sprawl, encourage density in our urban core. (Per Pam Holocher, Ft Wayne deputy director of community development.) Narrow the streets, concentrate use, beautify, slow down. Invite a quality of life, and you invite quality people to populate downtown.
- Finally, I would suggest that Dr. Weintraut not be present in future meetings and be replaced by a respectful professional.

Most Sincerely


Susan R. Haneline
134 E. State Blvd
Fort Wayne, IN 46805
Brookview Neighborhood Home Owner

10/01/2012

To Whom It May Concern,

There has been a lot of talk about the historic significance of the Brookview Neighborhood. Its winding roads, park like settings and flowing creek certainly make it sound like the ideal place to live. I have seen this reflected in pictures of the neighborhood. Taken with such care, on a perfect Spring day... the area seems to shine with fairytale beauty. What those pictures do not reflect is many of the real issues in the neighborhood, the reality of the OTHER part of the neighborhood. I have attached 19 pictures of what MY part of the neighborhood looks like when it floods. This is the part of the neighborhood in the actual footprint of the E. State Blvd. road/bridge project. Please remember, as you look at these pictures, the flooding incidents you are seeing are not the exception any more, they are the norm. Some years we have had multiple instances of flooding lasting weeks at a time. As you can see, 3 - 4 foot clay dikes and dirty, contaminated flood water does not exactly project fairytale beauty.

In my part of the neighborhood, the increase in rental properties and vacant homes tell the story of just how many people are leaving the area because of the issues with traffic and flooding. The cries to maintain the neighborhoods historic significance seem to have lost sight of whether it is actually prudent to save something just for the sake of history. The crumbling bridge between Eastbrook and Westbrook Drives is a case in point. It is too narrow, it sits too low and it is in terrible disrepair. Each time the water in the creek rises to several feet below street level, the bridge acts like a damn, preventing the water from flowing underneath. It forces massive amounts of contaminated water to spill out onto our properties and close our driveways and roads. This is a case where practical is getting lost in the desire to maintain or retain something that is simply broken and outdated.

These problems do nothing to showcase what IS historical about the neighborhood. It is time to honestly address these problems and move forward with the road/bridge project. We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood. We certainly do not showcase our humanity when we force homeowners to keep properties that are neither safe, desirable or retain their monetary value. This is not a case of big government trying to run rough shod over the

little guy. This is a case of the little guy, begging government to step in and solve a problem, to do the right thing. Please allow us to be gracefully be removed from the burden of properties that have become undesirable places to live. I again ask you to take a hard and honest look at the attached photos. Can you, in good conscious, really retain our homes in the footprint of this project knowing that the issue of flooding and traffic will not be resolved? If you owned property here, would you not be begging for someone to step in and solve the problem? Brookview Neighborhood was designed to have a park life feel, why not allow the homeowners to be bought out and then, as proposed, turn all that land into green space? Give the neighborhood a true entrance, showcase its unique feel through a thoughtful use of lighting, plaques, hardscape and monuments. I am not saying we should not honor our history, but there is a way to honor history without sacrificing homeowners quality of life.

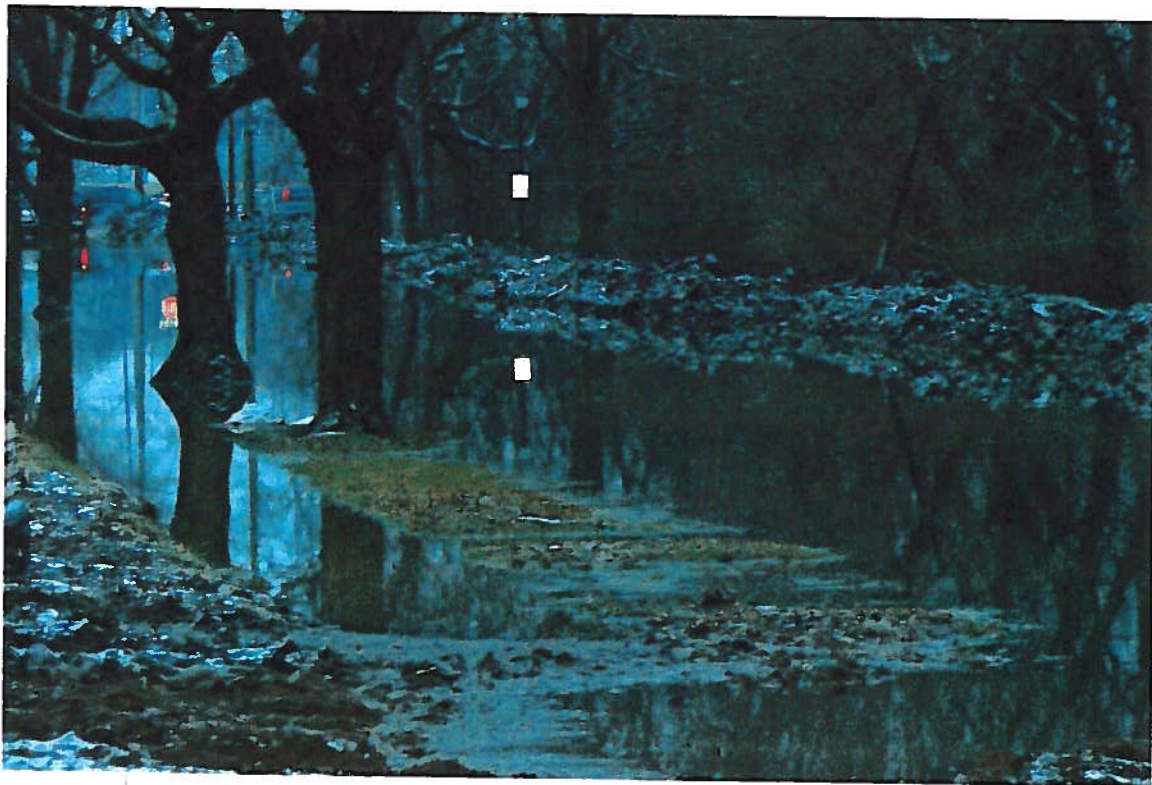
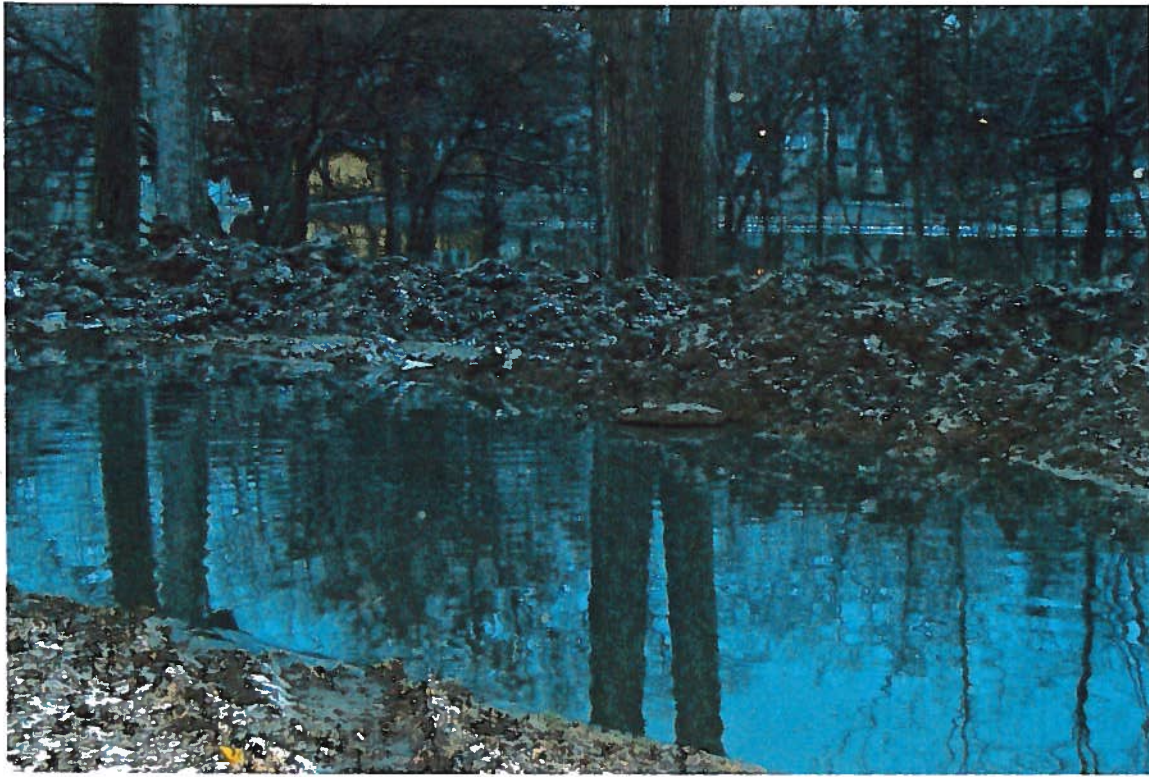
Respectfully,

A handwritten signature in dark ink, appearing to read 'Susan R. Haneline', with a stylized, flowing script.

Susan R. Haneline





















Susan R. Haneline
134 E. State Blvd.
Fort Wayne, IN 46805
Homeowner, Brookview Neighborhood

10/02/2012

To Whom It May Concern,

This letter is in response to questions about how we might best preserve and honor the history of Brookview Neighborhood, while proceeding with the State Boulevard project.

I have done a lot of thinking about the vision of the neighborhood, what it was meant to convey about the city, about progress and the importance of beautiful and aesthetically pleasing urban green spaces.

It is my understanding that the current proposed plan does not remove the historic footprint of the neighborhood. The bridge that is being proposed for removal is, in all honesty, an eyesore and a flooding issue. It has been in a state of disrepair for years. Its removal would improve the overall look of the neighborhood. I do not agree with attempts to fix the bridge in its current location. Even repairing the bridge seems counterproductive. At its current height and size, it would continue to back up the water in the creek which plays a significant part in the flooding of homes in that area. As for the moving of State Blvd, it is my understanding that the old roadway will, for the most part remain intact. It will simply become a part of the residential neighborhood. Perhaps it could simply be renamed something like "Historic State Boulevard". This plan seems to me, to actually enhance historic vision, not cause it to be destroyed. The winding street would still exist, but it would exist as a quiet tree lined boulevard set within a urban neighborhood green space.

As I have stated in previous letters, the homes located in the footprint of this project have been suffering for years due to flooding and traffic flow. The majority of these homeowners would like to be bought out for this project. I honestly believe that these buyouts and the subsequent open green space around the new State Boulevard would improve then neighborhood. To further respect the historic vision, I would suggest the following:

1. Installing historically correct lighting in the area. Perhaps something that reflected lighting styles from the early part of the 1900's. I think reproductions of the old gas style lights would be a wonderful addition to the neighborhood.

2. Plantings and green space that gives the area a park like feel, such as period style benches, groupings of trees and flowers, perhaps even brick style side walks. All these kinds of details and features pay homage to the areas history.

3. Beautiful stone or brick entrance pillars for the neighborhood. Perhaps something similar to the Forrest Park Boulevard area, a feature that would define the neighborhood.

4. I like the idea of a divided new State Blvd, one that is tree lined, perhaps with some perennial cottage or old English type flower beds.

5. I would love to see some small monuments that would tell the story of Arthur Shurcliff and his vision of the neighborhood. Perhaps even pointing out examples in the project that pay homage to his vision.

6. I think the best way to crown the area would be to find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces. Again, I think nod to an old fashion early 1900's park setting would be beautiful. Maybe the addition of an appropriately styled fountain or gazebo could add to the historic feel.

I believe there are many ways to honor our history without halting the progress of a project that I truly believe would benefit the neighborhood, the public and the city.

Respectfully,

A handwritten signature in black ink, appearing to read 'Susan R. Haneline', with a stylized, flowing script.

Susan R. Haneline

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

October 3, 2012

Re: State Boulevard Reconstruction Project
Des. No. 0400587
Project No. IN20071404

Dear Ms. Hope:

As a Councilman elected to represent the welfare and best interests of the citizens of our city, I cannot support the current State Boulevard widening plan in anything like its present form, nor can I support the "concept" as described in the purpose and need statement on which it is based. The attempt to "elevate" (all words in quotation marks are taken from either the P and N statement or the 2030 II Transportation Plan) State Street to a major east-west arterial for regional as well as local traffic is nothing less than a perversion of the proper use of the "By-pass and Arterial concept", a concept which should be used to divert through traffic from residentially populated areas, not to them, as this plan does.

There are two legitimate needs in the Brook View area that must be addressed: the bridge over Spy Run Creek needs serious repair or replacement, and a blind spot at the foot of State Boulevard, near the intersection with Westbrook, needs to be eliminated. Plans responsive to these needs but, unlike the present plan, minimally harmful to the historic district it divides and effectively discouraging of faster traffic and truck traffic should be developed following AASHTO and Federal Highway Administrations Guidelines for public participation in context sensitive traffic project design. I will be pleased to participate in such a process, if properly conducted, and in the development of effective mitigation measures which would integral to it. In any event, discussion of further planning along any lines, mitigation or otherwise, should await the outcome of the Environmental Assessment which I understand is currently under way.

I call your attention to the enclosed letter, signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen. As a councilman at-large I represent them; but not just they will be harmed: the legitimate complaints and distress they convey translate into decreased property values. In turn this translates into a decreased property tax base and decreased tax revenues, which hurts the whole city. In fact this project in its current form will exacerbate the trend of recent decades toward the hollowing out of the city – that is, the migration of homeowners from the city to outlying areas - and it will do so precisely

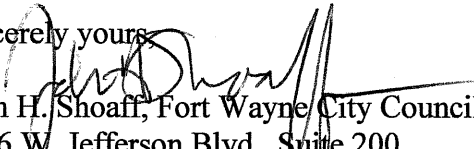
at the time we are trying to revitalize our downtown and at a time when we should be taking advantage of a national countertrend, led by seniors and young people, toward moving back into the city.

Reactions to the last mitigation meeting: Just prior to it Mr. DuMontelle, in a response to an inquiry from Michelle Wedaman, wrote that alternative plans were presented at a public meeting and a preference chosen. He was misinformed. Attendees were presented with just one plan, the only plan the planners have ever shown and about which they have been totally inflexible. The only alternatives offered were three minor ones that had to do with which local street would access the new four and five lane corridor. This fell far short of verbally made promises to explore and offer real alternatives; and, insofar as it was a pretense of doing otherwise, was an insult to the intelligence of my constituents. As I've said elsewhere, it was akin to asking the condemned man which tree he wished to be hanged from.

Mr. DuMontelle might wish to refute my implication that he did not show a proper grasp of the democratic process when he said that he liked the P and N statement because "it was decided by a regional planning body". He could reasonably say that he meant only that NIRCC decides only what recommendation will be sent to the elected body, and that he knows full well that the elected body makes the decision.

Finally, I also suggest that Ms. Weintraut consider excusing herself from any further moderating duties. Her rude and highly unprofessional reactions to speakers she disagreed with made it clear that the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.

Sincerely yours,



John H. Shoaff, Fort Wayne City Councilman At-large
4646 W. Jefferson Blvd., Suite 200
Fort Wayne, IN 46804
Phone: 260 459 0221

September 1, 2012

To: Fort Wayne Common Council members and Mayor Tom Henry

RE: City's State Boulevard reconstruction project Wells/Cass Streets to Spy Run Avenue
State Blvd Reconstruction - Des. No. 0400587

Dear representative of the people of Fort Wayne:

The City of Fort Wayne wants to expand State Boulevard into a primary east-west arterial corridor like Coliseum Boulevard, and is currently preparing to widen State Boulevard's two lanes between Wells Street and Spy Run Avenue to five lanes, straightening the carefully designed traffic-speed-calming curves over Spy Run Creek and through the celebrated National Register of Historic Places Brookview Neighborhood. The City plans to elevate the roadway 7'- 9'; turning this appealing residential district into yet another wide, fast urban highway. State Boulevard is an integral part of Fort Wayne's acclaimed Parks and Boulevard System, recognized in its entirety on the National Register of Historic Places as the Park and Boulevard System Historic District.

This plan displays a failure of the public process. The City claims that this project will address "safety and flooding" but has failed to demonstrate that either will be accomplished, at a cost of more than \$11 million public dollars. The decision-making used to arrive at this unsatisfactory proposal has been unclear, has alienated many of us who have sought input as engaged residents and has failed to address even basic issues about the project's real impacts on our property values (and consequent impact on property tax revenue), land use, safety and flood control, and the character of our neighborhoods. The City's Public Works Department has responded to our concerns only by holding public information meetings where we've been shown again and again the same "preliminary design" engineering drawings with no alternatives offered. The department has not altered its initial plan.

Our concern extends far beyond the additional dozen or so homes which would be razed, adding to the 26 or so recently demolished in the historic Brookview Neighborhood. Roadway design in this area affects the entire length of State Boulevard and the surrounding city. The current proposal will damage all of our property values and overall livability of the area. The decrease in property values will decrease property tax revenue, which will harm the entire city. We predict a migration from the well-tended and well-loved center city historic core, fewer owner-occupied homes, decreased care of properties, blight, and a decreased tax base for the city overall. Any road

projects here must be carefully considered from a comprehensive perspective and understood for what they mean to the people of the community. We are gravely concerned about our property values; quality of life; city attractiveness; safety and transportation accessibility for uses that include local vehicles, biking, walking and public transit; coherence with unique historical character and distinctive natural features; air, water, sound and light quality; and incorporation of meaningful flood control by identification of root causes.

This massive and poorly managed project should be halted immediately, and any plans to modify State Boulevard should begin anew, this time with a publicly transparent planning process which involves us - a working group of resident neighborhood leaders - along with elected representatives, and urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.

Thank you,

Michelle Briggs Wedaman, president, Brookview Neighborhood, 275 households and businesses
Paul Gibson, president, Irvington Park, 80 households
Marcia Simmons, president, Park Place Condominium Association, 175 households
John Meinzen, vice president, Spy Run Neighborhood Association, 500 households
Cynthia Keller, president, Northside Neighborhood Association, 4,000 households and businesses
Shawna Nicelley, president, Forest Park Boulevard, 66 households
Steve Morehead, president, North Anthony Area Association, 1,100 households and businesses
Helene Evans, president, Frances Slocum Association, 1,200 households and businesses
Evelyn Bickel, president, Brentwood Park Association, 290 households
Gretchen Johnson, president, Bloomingdale Neighborhood Association, 1,850 households and businesses
Judi Wire, president, The Wells Corridor Business Association, 50 businesses
Mike Reuille, president, North Franke Park Association, 450 households and businesses
Rick Stoeckley, president, Lincoln Park, 680 households
Linda Spurrier, president, Fall Creek Neighborhood, 107 households
Tom Tiernon, president, Historic Oakdale Neighborhood Association, 700 households

CONTACT:

You can reach us all on this matter via Michelle Briggs Wedaman
260-710-4413
mbwedaman@frontier.com
mailing address: 2326 Eastbrook Drive, Fort Wayne, IN 46805

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

October 3, 2012

Steve Kennedy
Grants Section
Indiana Division of Historic Preservation and Archeology
402 West Washington Street, Room w274
Indianapolis, IN 46204

Dear Mr. Kennedy,

I am very pleased to join the chorus of those urging that the potential North Anthony Boulevard Historic District be listed on the National Register.

This is an area I have known since childhood, when I began making lifelong friends at the nearby Forest Park School. It has held up well over the decades, and has remained a good place to live, close to our downtown. It represents one stretch of the Kessler Park and Boulevard system, appreciated nationally as one of the most intact of George Kessler's urban plans, and recognition on the National Register would further encourage prideful home ownership.

Thank you for consideration of this excellent candidate for the Register.

Sincerely yours,

John H. Shoaff
Fort Wayne City Councilman At-large

Friends of the Parks of Allen County, Inc.
Po Box 10152
Fort Wayne, Indiana 468050

Ms. Briana Hope
Environmental Project Manager
American Structure Point, Inc.
7620 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project
Des. No. 0400587
Project No. IN20071404

The Friends of the Parks of Allen County, Inc. agree that there will be an adverse effect of the Fort Wayne Park and Boulevard System Historic District and the Brookview - Irvington Park Historic District, as well as to the Bridge over Spy Run (NBI No. 0200273).

However we feel that any discussion of mitigation is, at best, premature; at worst, the proposed Memorandum Agreement is a bad faith attempt to confuse an already complicated and unfair process, especially in light of the fact that the Federal Highway Authority has elevated the National Environmental Policy Act (NEPA) Documentation to Environmental Assessment (EA).

We continue to assert that the APE is not comprehensive enough and should include those historic districts along State Boulevard. This project is not just a construction project. It is a project which will continue the ongoing process of turning the historic boulevard into a major (as opposed to "minor") traffic arterial according to the plans that the Northeast Regional Coordinating Council has been following for years. The additional traffic that these plans are intended to generate will degrade the quality of life substantially along the State Boulevard corridor and will negatively impact a number of historic neighborhoods, schools, and other properties. Connectivity (safety), air quality, and visual relationships will be harmed along the entire corridor. We respectfully request that the APE be expanded to include all the properties along State Boulevard.

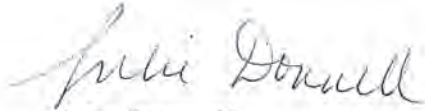
Further, it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.

In conclusion, although I, myself, did not attend the most recent meeting, Ed Welling, Vice President of the Friends of the Parks was there. The Friends are appalled at the continuing disrespect shown to the citizens of Fort Wayne through this process, in

particular, the very unprofessional behavior of Ms. Weintraut and the assertions of Mr. Dumontelle that alternative plans have been presented to the public.

Under these circumstances, the public is not being served properly at all.

Sincerely,

A handwritten signature in cursive script that reads "Julie Donnell".

Julie Donnell
October 3, 2012

Julie Donnell
President,
Friends of the Parks of Allen County, Inc.
September 19, 2012

260-432-7178
juliemarie57@earthlink.net

From: [Suzanne](#)
To: [Hope, Briana](#)
Subject: Fort Wayne State Blvd project - Mitigation response following meeting of 19 September, 2012
Date: Wednesday, October 03, 2012 11:53:56 PM

All,

On finding ways to mitigate the adverse effects of the massive, intrusive State Blvd project on the impacted area, here's a solution: don't build it.

Fact: It is not needed.

Fact: The proposed plan is flawed.

Fact: The APE is inadequate.

Fact: The process has been unfair and undemocratic, and now we can also say -- abusive.

Regarding the meeting of 19 September, 2012, the disrespectful, contemptuous tone and the conduct of our meeting hosts were appalling. The attempts to deny concerned citizens entrance to the meeting was transparently obstructionist. The scoldings and rude interruptions by our hosts and government agency reps were extremely unprofessional and insulting. The facilitator's dramatic eye-rolling, grimacing, smirking, and toe-tapping while those in opposition were trying to speak were quite a stunning display of dramatics. I hope those of you who attempt to operate with integrity were embarrassed by these ridiculous stunts.

At this so-called mitigation meeting we were admonished to provide only facts and simple ideas for mitigation. Some mitigation ideas offered us by the facilitator included taking a couple of photographs of the existing bridge and coming up with a few educational signs and materials about the historic elements that will be destroyed by this colossal project -- as if such trivial efforts would smooth over the loss of a dozen homes and the eruption of a highway in the midst of our quaint historic district.

Here are more facts that have been offered by the consulting parties and repeatedly rejected by Stucturepoint and our government agency reps:

- The P&N rationale is flawed and has shifted and changed depending on the agenda of the moment. It's the curves, no, it's flooding, no, it's congestion, no, it's . . . fill in the blank with some other fabricated reason, but, never with the NIRCC plan goals to push arterial traffic through the north side of Fort Wayne.
- Safety has been given lip service, but substantive traffic calming is never, *has never been*, included in the design plan in *any way*, even though traffic calming

elements are being designed into other Fort Wayne urban roadways. So *clearly* safety is not a priority here. If it were, accurate data would have been provided to the public. Instead we have vague references to crashes that might have occurred somewhere in the *general* area that might or might not have been caused by congestion, or speeding, or, we don't know what, because that information was never provided!

- It is a fact that the proposed roadway will withstand higher speeds; this does not play nicely with traffic calming.
- The current plan is intrusive, and massive yet no substantive design elements have ever been revealed in renderings that the public can easily evaluate, even though, the bifurcation of the historic district is a **devastating** aspect of the plan. Where are street-level renderings of the proposed design that show the elevated bridge and vacated lots where homes once sat?
- The APE is not adequate. The plan must be rejected until it appropriately addresses the entire impacted area in all of its invasive, destructive aspects. How can a project this devastating and monstrous not impact the entire near north side of the City?
- Here's another fact, I pulled onto State (southbound from Eastbrook, turning west onto State) at 8:05 a.m. on 2 October, 2012, a Tuesday morning, and my car was the SOLE vehicle in either lane of the roadway at that time for the entire stretch of the relevant curved area. In fact, my car was the only vehicle in sight at that time. Your purpose and needs congestion argument is not tenable! And, when I came home about 11:00 a.m., there were widely spaced lines of "traffic" -- about six cars -- moving at a steady speed in both directions. I proceeded to Eastbrook made my left turn safely and without having to wait.
- And one more factoid for you to ponder: on my bicycle today at 5:02 p.m. -- afternoon rush-hour -- I had to wait perhaps 30 seconds to safely ride across State Blvd at Cass from south to north. There was a wide gap, with no traffic in either direction **at 5:00 p.m.** So plug that into your expensive traffic model and explain why we need to squander 11+ million dollars on this disastrous project!

With all *due* respect,

Suzanne Slick
Irvington Park Consulting Party

**Jill D. Downs
1202 Elmwood Ave.
Fort Wayne, IN 46805**

October 4, 2012

Briana Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

Dear Ms. Hope:

I am writing to provide my comments to the most recent findings regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

In short, I would agree that this project creates an adverse effect in the area. However, it is clear that the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration. Although American Structurepoint and the Federal Highway Administration feel the project is at a point where mitigation discussions can be held, the majority of those opposed to the project remain unconvinced about its perceived necessity. Because of this, it is difficult to move forward. I think this was painfully obvious at the September 19, 2012, meeting of the consulting parties.

Those opposed to the project were not interested in discussing mitigation for a variety of reasons. These include the fact that the Environmental Assessment has not been completed, the resultant bi-furcation of the historic district and consequences of raising of the road should be added to the list of adverse effects, and that because houses in the area had already been removed by the City of Fort Wayne, there appears to be a lesser impact to the district. Also, it was noted by Linda Weintraut in the meeting that there is another project proposed within the area, this being the Pufferbelly Trail, that is creating an impact on the design of the State Boulevard Reconstruction Project. It would seem as those information about the specifics of the Pufferbelly Trail project should be incorporated into the State Boulevard project. It is premature to discuss mitigation.

Overall, I do not believe there is not a need to reconstruct State Boulevard. As a nearly lifelong resident of near northeast Fort Wayne, I travel the State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. Such projects are even being undertaken within Fort Wayne, specifically the downtown area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area. A recent "Travel Time Delay Study" for fiscal year 2012 indicated that during peak travel times, there is only a 5.7 delay in traveling from Sherman Boulevard to Beacon Street. Just from a fiscal standpoint alone, I do not believe spending millions of dollars to save 5.7 minutes is worthwhile. The State Boulevard Reconstruction Project is not warranted, and this is evident by the poor attempts by the City of Fort Wayne, American Structurepoint, and the Federal Highway Administration to justify its need.

Sincerely,

Jill D. Downs



The mission of ARCH is to advocate for the protection and preservation of historically and culturally significant assets and historic places in Allen County and northeast Indiana.

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter dated August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form, fashion, or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintrout.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. Although the material had literally been sent via e-mail at the close of business hours the day before the meeting, it still played an integral part in the American Structurepoint presentation. At the meeting it was concurred by FHWA and the DHPA that furnishing information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other information provided in the 4(F) document.

Given the breadth and scope of the project, and associated substantial adverse impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,

Michael Galbraith
Executive Director, ARCH, Inc.

Ecc: DHPA, FHWA

BROOKVIEW

NEIGHBORHOOD ASSOCIATION

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to request an additional 30 days to comment on the American Structurepoint letter and packet dated August 29, 2012, about proposed mitigation for State Boulevard Reconstruction in Fort Wayne, IN. We require additional time to incorporate our comments related to the August 29 packet, as additional materials were sent to us from American Structurepoint via email the night before the September 19, 2012 consulting parties meeting. Those materials were then referred to by American Structurepoint during their September 19, 2012 presentation.

Since the last consulting parties meeting notes in 2011, we have received only the May 2012 updates related to historic properties and no indication of or updates on other project alternatives under exploration, and no answers to the very real questions we have about this project. The many questions we asked at both the December 2009 and the September 2011 consulting parties meetings have remained unanswered: questions about the project's Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the real area of impact of this project.

We understand that an environmental assessment is still being completed for this project. How can we discuss mitigation before being enlightened by information contained in that report? How can this group discuss mitigation without being enlightened by information we have all requested about this project?

This project is for us, not a theoretical one. These are our streets and our neighborhood, and we have a right and a responsibility to request and receive not only substantive answers to questions we and others within the consulting party circle have been asking, but ample time to review and comment in writing upon information related to this exploration.

Sincerely,
Michelle Briggs Wedaman
President, Brookview Neighborhood
2326 Eastbrook Drive, Fort Wayne, IN 46805
260.710.4413 mbwedaman@frontier.com

Brookview Beautiful.
An historic garden neighborhood in the heart of the city. This place matters.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



October 4, 2012

Richard J. Marquis
Acting Division Administrator
Indiana Division
Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft memorandum of agreement for the State Boulevard Reconstruction Project (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Mr. Marquis:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed American Structurepoint's letters of August 29, 2012 (with enclosures) and September 18, 2012 (with enclosures), and has taken into consideration the discussion at the September 19, 2012 consulting parties meeting, regarding the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

As we had said in our August 13, 2012 letter, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

We concur with FHWA's August 27, 2012 Section 106 finding of Adverse Effect for this undertaking.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected:

- Fort Wayne Park and Boulevard System;
- Brookview-Irvington Park Historic District; and
- Bridge on State Boulevard over Spy Run (NBI. No. 0200273).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

Furthermore, we wish to offer some comments and suggestions about the draft memorandum of agreement, Version 8/24/2012 ("Draft MOA").

SUGGESTIONS FOR FURTHER MINIMIZATION

We think we understand the issues described in your September 18 letter that would make preservation of the houses at 112, 134, and 138 East State Boulevard problematic. We remain concerned about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District. Having reflected further upon your September 18 letter, we wonder whether, as a minimization measure, it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive. It is our impression that most of the existing sidewalk along the south side of State Boulevard could remain in place, and it seems to us that the existing sidewalk could serve pedestrians who would be walking along the north side of the new alignment, even though the northward bow in the existing State Boulevard would make one's walk slightly farther than if a sidewalk immediately paralleled the new alignment along its north side. We are sympathetic to the concerns of property owners at the September 19 meeting who expressed a preference to have their entire properties along the south side of the current alignment of State Boulevard, rather than to sell only large portions of their yards and have the new proposed right-of-way come within only several feet from their houses. However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect.

It appears to us that if the sidewalk and the grass buffer between the sidewalk and the curb were eliminated from the plans along the north side of the new alignment from Terrace to Eastbrook, and if a railing of some kind were constructed adjacent to the curb, then at least ten feet less right-of way would be needed along that north side of the new alignment. Furthermore, if a retaining wall were constructed near that railing, instead of a sloped embankment and a drainage swale, it appears to us that even less right-of-way would be needed along the north side of the proposed alignment. Also, if there were no sidewalk immediately adjacent to the north side of the new alignment, it appears to us that it might be feasible to eliminate one or both of the new sidewalks that are proposed along the Oakridge Road extension. If there were no sidewalk along the north side of the new alignment, then there would seem to be no need to provide new sidewalks extending southward along the Oakridge extension from the existing State Boulevard to the new alignment of State Boulevard.

We also wonder whether the reconstruction of State Boulevard, which would elevate the roadway above the existing grade as it runs west from Terrace, could be designed to serve to some extent as a levee to prevent most Spy Run floodwaters from reaching the three houses in question on the south side of the existing State Boulevard.

If some or all of the suggestions above prove to be feasible and prudent, then we think they should be incorporated into the memorandum of agreement, in an effort to avoid confusion in the future about the design parameters upon which agreement has been reached.

SUGGESTIONS FOR MITIGATION

Stipulation I. of the Draft MOA appropriately directs that context sensitive solutions be incorporated into the new construction and related landscaping and streetscape design. That stipulation also would establish an advisory team to review and comment on the specifics of that design work, in keeping with the directive contained in Stipulation I.B.ix. of the 2009 "Memorandum of Agreement. . . Regarding the US 27 Southbound Realignment and Bridge Replacement over Spy Run Creek in Fort Wayne, Allen County, Indiana" for future federal projects in the area. The advisory team that was established under that US 27 memorandum of agreement provided useful recommendations for context sensitive solutions for that project. However, we do not believe that there is a need for the Indiana SHPO to be directly involved in all of the meetings and activities of future advisory teams in the area. We believe that the most important input will arrive in the form of the Advisory Team members' recommendations, based on their perceptions of what is best for their community, and of the guidance from FHWA and the Indiana Department of Transportation regarding the feasibility of those recommendations. Consequently, we ask that the Indiana SHPO *not* be given a role in convening advisory team meetings, as is currently proposed in Stipulation I. B. and I.B.vi. of the Draft MOA, and that the Indiana SHPO's participation in meetings of the advisory team be left to the Indiana SHPO's discretion. It would be appropriate, however, for the Indiana SHPO to remain involved in the kind of consultative role that is prescribed in the final sentence of both I.B.vi. and of I.B.viii.

We anticipate that at least one consulting party will be making recommendations for crafting context sensitive solutions in keeping with the natural landscape of the project area and the landscape design philosophy of George Kessler or Arthur Shurcliff. This is an intriguing idea, and we would ask that serious consideration be given to any consulting party recommendations along those lines. It appears that regardless of the particulars of the final design of the reconstructed State Boulevard, a considerable amount of green space will be opened by this project, and how that green space is designed could play an important mitigative role.

We agree that the current State Boulevard bridge over Spy Run (NBI No. 0200273) should be documented photographically, as provided for generally in the Historic Bridges PA and specifically in Stipulation II. of the Draft MOA. We would ask that such photo-documentation be performed in accordance with the version in effect, at that time, of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards." It recently has been brought to our attention that the State Archives, rather than the State Library's Memory Project, is the legally-authorized repository of all state government records that are required to be preserved.

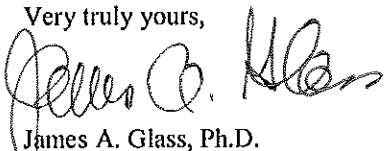
We also request that it be stipulated in the MOA that the portion of the Brookview-Irvington Park Historic District within and immediately adjacent to the proposed project area should be photographically documented. We recommend that streetscape and broad views of the setting of that part of the neighborhood be emphasized, but we think that at least a couple of photographs of each house that is to be demolished also should be included in the documentation. The photographs should be taken from oblique angles so as to document all four elevations of each house.

For both the State Boulevard bridge photographs and the streetscape and district photographs, we request that a set of the photographic images in both print and digital form, saved on a compact disc, and following, as closely as possible the guidance of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards," be provided to our office for ultimate transfer to the State Archives. We also think that a duplicate set of the electronic and print photo-documentation be prepared for and delivered to a local public library or not-for-profit institution that would be capable of and willing to retain the documentation on a permanent basis, so that it would be readily accessible to local researchers.

If you or American Structurepoint, Inc. would find it helpful, we could draft specific MOA stipulation language or modifications to language in Version 8/24/2012 to show how our recommendations might be incorporated into the MOA. If you wish to receive such suggestions of specific language, then, in order to facilitate our drafting efforts, we would appreciate receiving an electronic copy of the MOA in a format that would allow us to show changes and make explanatory comments.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Briana Hope, American Structurepoint, Inc.

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

October 4, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter of August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. In fact, I was unaware of the additional documentation and information therein until attending the meeting itself as my travel time required I leave early that morning and did not have an opportunity to see or review that email. At the meeting it was concurred by FHWA and the DHPA that sending pertinent information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other informant provided in the 4(F) document.

Given the breadth and scope and associated substantial impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,



Todd A. Zeiger
Director, Northern Regional Office

Ecc: Dr. Jim Glass, Indiana Division of Historic Preservation and Archaeology
Mike Galbraith, ARCH



CITY OF FORT WAYNE

THOMAS C. HENRY, MAYOR

October 4, 2012

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

RE: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Ms. Hope:

This letter provides comments on the above-referenced project from both Tom Cain, Fort Wayne Urban Designer, and from Creager Smith, Fort Wayne Historic Preservation Planner. We both agree with the determination that the proposed project would have an adverse effect on three historic properties within the APE.

The bullet points that follow are a list of adverse effects on landscape and planning features, in both the Brookview-Irvington Park Historic District and in the Fort Wayne Park and Boulevard System Historic District, caused by the State Boulevard Reconstruction Project. The list was assembled by Tom Cain, and edited by Creager Smith. It is a suggested starting point for more detailed discussion during any consideration of mitigation of adverse effects. These items are in no particular order; they are the potential basis of mitigation measures, although there may be additional adverse effects that do not appear on the following list.

- Roadway and sidewalk layout changes from historic alignments to new alignments; alteration of the locations, sizes, and shapes of the carefully designed public and private spaces provided by the historic Brookview plat.

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Attachment 3 - 468 of 531

Hope

October 4, 2012

Page 2

- Roadway elevation changes and topographic relationship changes between linear public roadway space and the private linear yards.
- Stream corridor vegetation loss from new road and gap in streamside vegetation left by bridge removal.
- New open space fragments that are not part of the historic Brookview plat.
- Disintegration of public space definition on south side of old State Blvd. alignment by demolition of homes and loss of the private space.
- Alteration of historic relationship of road elevation to former rail bridge embankment and bridge abutments.
- Alteration of road width through former rail bridge abutments.
- Substantially increased width of roadway through neighborhood.
- Changes in pedestrian patterns.
- Alteration of neighborhood street connections to State Blvd, concentrating them into a new extension of Oakridge Rd.
- Removal of later street lighting which is of similar scale to historical lighting for the area.
- Removal of historic rail and Spy Run Creek bridge architecture.
- Loss of street trees.
- Loss of historic streetscape patterns of Eastbrook Dr.
- Alterations of Westbrook Dr. and other segments of neighborhood streets, especially at ends of blocks and at driveways.
- Insertion of access ramps from State Blvd. to the new greenway located on rail embankment.
- Increased height of rail embankments to access new greenway bridge over State Blvd.
- New greenway bridge architecture, and new bridge abutments, over State Blvd.
- Loss of historic neighborhood development pattern of the Brookview plat; the plat is of significance to the National Register listing.

Hope

October 4, 2012

Page 3

- Loss of houses and garages that are contributing resources in the Brookview-Irvington Park Historic District is an adverse effect; however if the historic context of the relationship between public spaces and private buildings is lost due to alterations and destruction of the Brookview plat, then carefully chosen demolitions should be considered as part of a broader mitigation strategy .
- Loss of spatial definition along linear public ways with removal of houses and the street removal along State Blvd. and Eastbrook.
- Medians placed within the roadway are not a character-defining feature of the Fort Wayne Park and Boulevard System as designed by George Kessler, nor are they a feature of the Brookview plat; if proposed they must be very carefully considered.

As this Section 106 process moves forward we are available to assist in the development of mitigating design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on the team.

Sincerely,



Thomas Cain, RLA, ASLA
Senior Urban Designer



Creager Smith
Historic Preservation Planner

From: "Carpenter, Patrick A" <PA_Carpenter@indot.IN.gov>
Date: October 5, 2012 4:17:47 PM EDT
To: "Hope, Briana" <bhope@structurepoint.com>, <mgalbraith@archfw.org>, <aquinn@archfw.org>, <don.orban@cityoffortwayne.org>, <lzeiger@indianalandmarks.org>, <mbwedaman@frontier.com>, "Donnell, Julie" <juliemarie57@earthlink.net>, <jcooper@ccrtc.com>, <indianabridges@sbcglobal.net>, <shan.gunawardena@cityoffortwayne.org>, <dan.avery@co.allen.in.us>, <jslick@mac.com>, <jandailey59@msn.com>, <joyce.newland@fhwa.dot.gov>, "Heil, Larry" <larry.heil@fhwa.dot.gov>, <jshoaff@proparkwest.com>, "Kaiser, Jason" <JASONKAISER@indot.IN.gov>, "Kennedy, Mary" <MKENNEDY@indot.IN.gov>, <linda@weintrautinc.com>, <creager.smith@cityoffortwayne.org>, <albertcohan@aol.com>, <tmn@barrettllaw.com>, <rross@martin-riley.com>, <dan@earthsourceinc.net>, "Glass, James" <JGlass@dnr.IN.gov>, "Carr, John" <JCarr@dnr.IN.gov>, <kdietsch@comcast.net>, <alec.johnson@ci.ft-wayne.in.us>, "Crites, Scott" <SCrites@structurepoint.com>, "Zielinski, Rich" <RZielinski@structurepoint.com>, "Smith, Gregory" <GSmith2@indot.IN.gov>, "Johnson, Amy \"(DNR)\" <AJohnson@dnr.IN.gov>, "Ricketts, Amanda" <ARicketts@dnr.IN.gov>, "Tharp, Wade" <WTharp1@dnr.IN.gov>, <tom.cain@cityoffortwayne.org>, "Hilden, Laura" <lhilden@indot.IN.gov>, "Mcmullen, Kenneth B." <KMCMULLEN@indot.IN.gov>
Subject: Section 106 Consulting Party Comments-State Blvd. (Des. #0400587)

Dear Consulting Parties,

Thank you for your comments . In light of the comments received during the most recent comment period and at the September 19, 2012 Consulting Parties meeting, the finding and 800.11 documentation will be updated. The updated finding, draft MOA and draft 4(f) Evaluation will be released with the Draft Environmental Assessment for an additional 30-day comment period. As such, we are not extending the current comment period, but providing for an additional 30-day comment period for both consulting parties and the public . This comment period will be initiated once the draft Environmental Assessment (EA) is released for public involvement by FHWA. The draft EA is expected to be released by the end of this year. The City of Fort Wayne and their consultants, Structurepoint, will continue to consult with FHWA and SHPO concerning comments received and measures to minimize adverse effects.

Thank you all for your continued participation.

Patrick Carpenter
Manager, Cultural Resources Office
Environmental Services
Indiana Department of Transportation
100 N Senate Ave., IGCN-Rm. N-642
Indianapolis, IN 46204-2216
[317-233-2061](tel:317-233-2061)

Phone Log

October 15, 2012

Conversation between Tom Cain & Linda Weintraut

At approximately 4:30 pm Tom Cain called to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but want to make them within the context of SHPO's assessment of project impacts and that the City addresses all adverse effects.

Cain also stated that impacts to the Brookview neighborhood should be enumerated.

Linda Weintraut said she would consult with Structurepoint and return Cain's call.

Phone Log

October 16, 2012

Conversation between Tom Cain & Linda Weintraut

I returned Tom's call from yesterday. I told him that Structurepoint was very glad to have his input on this project; at the very minimum, we would consult with him prior to the agency meeting and Briana was checking to see if it would be OK if he attends.

Tom spoke about the landscape changes that would be wrought as a result of the undertaking, particularly the changes from private to public space around the undertaking.

He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s.

He would like for mitigation to deal with changes in scale that will occur; tree planting should occur within 3 feet of the roadway (and not the standard 10 feet required on highways.) Tom believes that this would change the scale of the undertaking for the residents. He has other ideas that he will type up and send to Briana and I.

He said that it is important to achieve the "right feel" for the space.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



November 15, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
Post Office Box 5034
Zionsville, Indiana 46077

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana State Historic Preservation Officer's recommended changes to the October 17, 2012 draft of the "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No.)

Dear Dr. Weintraut:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the draft memorandum of agreement submitted with your October 16, 2012 e-mail message, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

In response to your acceptance of our offer to draft specific language for the memorandum of agreement ("MOA") to illustrate the recommendations we made in our October 4, 2012 letter, we have enclosed a marked-up copy of the draft MOA.

The most significant changes to the MOA reflect the extensive comments and recommendations we had offered in our October 4 letter, so we have not explained those changes here. We think that the reasoning behind the other changes is self-evident.

One issue that we noticed in the draft MOA but which we have not attempted to change is what appears to us to be the interchangeable use of "shall" and "will." We are not sure that it matters which word is used, but to the extent that they are being used synonymously, we would suggest that one or the other be used consistently.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 230953 or wtharp1@dnr.IN.gov. If you have questions about issues pertaining to buildings or structures, please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

Enclosure

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



April 1, 2013

Karen Bobo
Acting Division Administrator
Federal Highway Administration, Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Version 2/20/2013) (Des. No. 0400587; Federal Project No. IN20071404; DHPA No. 5903)

Dear Ms. Bobo:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the documents submitted with American Structurepoint's March 1, 2013, letter, which we received on March 4, for the aforementioned project in Allen County, Indiana.

We concur with FHWA's February 27, 2013, Section 106 finding of Adverse Effect for the State Boulevard Reconstruction Project.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected by this project: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and State Boulevard Bridge over Spy Run (NBI No. 0200273).

Thank you for taking in to consideration our comments on an earlier draft of the draft memorandum of agreement ("MOA"). We have some specific suggestions regarding Version 2/20/2013 of the draft MOA.

In the past, the staff of the Advisory Council on Historic Preservation has recommended that in an MOA for a project where the design is important in determining effect, a reference be included in the MOA to the date of the project plans. Doing so clarifies which elements comprised the project at the time the adverse effect finding was made and serves to reduce the likelihood of later disagreement among the signatories to the MOA about the nature of changes to historic properties that the project may have. Although the supporting documentation provide here includes plan drawings, it occurs to us that it could still be early enough in the design process that it might be necessary for the City of Fort Wayne to make revisions to one or more of those drawings before construction begins. At the very least, plans showing how the project will incorporate context sensitive solutions, such as those recommended in stipulations I.A. and I.D.2., will have to be drawn. Consequently, it does not seem feasible to tie the finding and the MOA commitments for this project to any currently existing set of plans.

We think, however, that aside from new construction design issues, the subject most likely to result in future controversy pertains to the identities of the principal buildings that will be demolished by this project (if they are not demolished for other reasons beforehand). Various alternatives and variations on alternatives have been proposed, either by the City or by other consulting parties, during the Section 106 consultation on this project, and some of those proposals would have left more buildings standing than the preferred alternative now would. Instead of citing in this MOA the date of any

existing construction plans, one measure that could be incorporated into the MOA to avoid a misunderstanding about the extent of demolition that would be allowed in this project would be a listing of the street addresses of all principal buildings that will be demolished, including those that are not considered to be individually eligible for the National Register of Historic Places or to contribute to the significance of an eligible historic district. That way, all consulting parties will be able to visualize, in a general way, the geographic boundaries of the demolition that will be necessary to implement the as-yet-unfinished designs of the new State Boulevard alignment and of the related context sensitive solutions. A "whereas" clause in the preamble to the MOA might be the appropriate place to list those street addresses.

In the last line of Stipulation I.D.2., "the rights-of-way" appears to need a gerund in front of it to describe the kind of context sensitive solution that might be applied to rights-of-way within the project footprint.

The second paragraph of Stipulation III. requires that the content and design of the four interpretive plaques be provided to the Indiana SHPO and the (other) consulting parties for review and comment. However, that paragraph seemingly requires the City to attempt to accommodate only the recommendations of the Indiana SHPO. We suggest that the third sentence of that paragraph be revised to read as follows: "If the Indiana SHPO or any other consulting party responds with recommendations, a good faith effort to accommodate the recommendations will be made." We often have found, not surprisingly, that local consulting parties are better able than we are to evaluate the accuracy of the content and the appropriateness of the design of interpretive markers.

In regard to archaeology, we recommend the following revisions to the Stipulation V. ("Post Review Discovery") section of the Draft MOA. We recommend that the second sentence be revised to state, "Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1, 312 IAC 21, and 312 IAC 22, and the most current Guidebook for Indiana Historic Sites and Structures Inventory—Archaeological Sites."

Following your receipt of these comments and those of other consulting parties, and before you circulate a final version of the MOA for signature, please send the revised draft to all consulting parties for review and comment.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about issues pertaining to buildings or structures should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903 and address it to Chad W. Slider, Assistant Director for Environmental Review, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources, 402 West Washington Street, Room W274, Indianapolis, Indiana 46204.

Very truly yours,



Ron McAhron
Deputy Director
Indiana Department of Natural Resources

RM:WTT:JLC:jlc

cc: Christine Meador, American Structurepoint, Inc.

emc: Karen Bobo, Federal Highway Administration, Indiana Division
Joyce Newland, Federal Highway Administration, Indiana Division
Lawrence Heil, P.E., Federal Highway Administration, Indiana Division
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Christine Meador, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

Public Notice

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

The existing reinforced concrete girder, T-beam bridge over Spy Run Creek was constructed in 1927. It is listed as 40.3 feet long, 48.2 feet wide, with a height of 9.5 feet. A.W. Grosvenor and O. Darling are credited as the designers. The July 17, 2006, Structure Inventory and Appraisal Report listed the structure as in poor condition, with a Sufficiency Rating of 27.9 (structurally deficient). The bridge, which is listed as contributing to the proposed Brookview-Irvington Park National Register Historic District, has also been determined eligible for listing in the National Register of Historic Places, according to information in Volume 2 of the Indiana Historic Bridge Inventory. This bridge has been rated as non-select in M&H Architecture, *Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges (draft)*, based on a report by HNTB.

In an effort to preserve this historic bridge, the City of Fort Wayne is notifying interested parties of the availability of the bridge over Spy Run Creek for reuse or salvage of elements that may be stored and used for future repair of similar historic bridges. This notice is being published in accordance with the Programmatic Agreement (PA) among Federal Highway Administration (FHWA), Indiana Department of Transportation (INDOT), State Historic Preservation Officer (SHPO), and Advisory Council on Historic Preservation (ACHP) for the Management and Preservation of Indiana's Historic Bridges, stipulation III.B.2. As required, this notice is being posted a minimum of six months in advance of the public hearing.

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For more information, or to submit an LOI, please contact:

Hayley Steele, Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
Telephone: (317) 547-5580
hsteele@structurepoint.com

The Journal Gazette

Allen County, Indiana

Account # 1064183 - 794026
American Structurepoint Inc

PUBLISHER'S CLAIM

LINE COUNT

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Body -- number of lines _____

Tail -- number of lines _____

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COMPUTATION OF CHARGES

118 lines, 1 column(s) wide equals

118 equivalent lines at \$ 0.371 cents per line

\$ 43.78

Additional charges for notices containing rule or tabular work
(50 per cent of above amount) -

Charge for extra proofs of publication
(\$1.00 for each proof in excess of two) -

TOTAL AMOUNT OF CLAIM

\$ 43.78

DATA FOR COMPUTING COST

Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 1

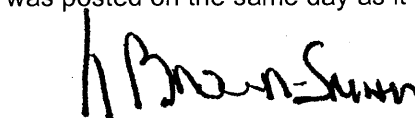
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times.

The dates of publication being as follows:

2/5/2010

Additionally, Newspaper has a Web site and this public notice was posted on the same day as it was published in The Journal Gazette.



T. Brown-Smith
Legal Clerk

Date: February 5, 2010

ATTACH COPY OF ADVERTISEMENT HERE

PUBLIC NOTICE

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

The existing reinforced concrete girder, T-beam bridge over Spy Run Creek was constructed in 1927. It is listed as 40.3 feet long, 48.2 feet wide, with a height of 9.5 feet. A.W. Grosvenor and O. Darling are credited as the designers. The July 17, 2006, Structure Inventory and Appraisal Report listed the structure as in poor condition, with a Sufficiency Rating of 27.9 (structurally deficient). The bridge, which is listed as contributing to the proposed Brookview-Irvington Park National Register Historic District, has also been determined eligible for listing in the National Register of Historic Places, according to information in Volume 2 of the Indiana Historic Bridge Inventory. This bridge has been rated as non-select in M&H Architecture, Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges (draft), based on a report by HNTB.

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Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
Telephone: (317) 547-5580
hsteele@structurepoint.com

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794026

The News-Sentinel

Allen County, Indiana

Account # 1064183 - 794026
American Structurepoint Inc**PUBLISHER'S CLAIM****LINE COUNT**

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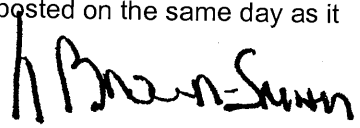
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T. Brown-Smith
Legal Clerk

Attachment 3 - 482 of 531

Date: February 5, 2010

ATTACH COPY OF ADVERTISEMENT HERE

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Telephone: (317) 547-5580
hsteele@structurepoint.com

2--5 794026

Legal Notice
Of
Public Hearing

The City of Fort Wayne will hold a Public Hearing on Wednesday, June 18, 2014, beginning at 6:00 p.m., at the North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana for the proposed State Boulevard Reconstruction Project between Spy Run and Cass Street, Fort Wayne, Allen County. An open house session will take place from 6:00 p.m. until 6:30 p.m. with the formal presentation beginning at 6:30 p.m.

The purpose of the public hearing is to offer all interested persons an opportunity to comment on the environmental document, 800.11(e) documentation for Section 106, and preliminary design plans for the proposed project.

The City of Fort Wayne is developing a federal-aid project to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River. This project, which begins at Cass Street and extends east to Spy Run, has an approximate length of 2,370 feet.

The proposed project involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits.

A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

The proposed project would require an estimated 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an “adverse effect” finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273).

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard.

The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. As part of the Indiana Historic Bridge Inventory project, the bridge was determined to be Non-Select. The bridge has been marketed for re-use for the past six months and information about the bridge can be found on the INDOT Historic Bridge Marketing website: <http://www.in.gov/indot/2532.htm>. The removal or demolition would be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The public hearing will be the last opportunity for a responsible party to step forward and provide the necessary sureties to obtain ownership of the bridge.

In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration’s “adverse effect” finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA’s “adverse effect” finding there is a Section 4(f) use. An Individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute’s preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be *de minimis* with regard to Section 4(f).

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation have agreed this project falls within the guidelines of an Environmental Assessment (EA) document.

The Public Hearing will consist of an informal Q&A session involving the project management team and a formal presentation regarding the project. Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

All comments collected before, during, and for a period of 30 days after the hearing will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

Before and after the formal hearing, the EA document and the 800.11(e) documentation for Section 106 will be available for review. Anyone interested in talking to the engineers about the project may do so before or after the formal hearing. Conversations prior to or after the formal hearing will not be part of the official record.

A copy of the EA document and the 800.11(e) documentation for Section 106 are available for viewing at the following locations on or after June 4, 2014.

- Allen County Public Library, 900 Library Plaza, Fort Wayne, Indiana 46802. Phone: (260) 421-1200
- Allen County Public Library, 2201 Sherman Boulevard, Fort Wayne, Indiana 46808. Phone (260) 421-1335
- Fort Wayne Transportation Engineering Department, Citizens Square, Ste. 210, 200 East Berry Street, Fort Wayne, Indiana 46802. Phone: (260) 427-1356

In accordance with the “Americans With Disabilities Act”, if you have a disability for which the City of Fort Wayne needs to provide accommodations, please call Briana Hope at American Structurepoint, Inc., at (317) 547-5580, by Monday, June 16, 2014.

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled “Early Coordination, Public Involvement and Project Development,” and the Indiana Public Involvement Manual approved by the Federal Highway Administration, US Department of Transportation, on April 1, 2009.

Please direct any questions or comments concerning this project to Briana Hope, American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256. (317) 547-5580, bhope@structurepoint.com. Comments on the proposed project will be accepted for 30 days after the Public Hearing. Comments or concerns brought forth by the public during this process will be addressed in the FONSI request document submitted to the FHWA.

The Journal Gazette

Allen County, Indiana

Account # 1064183 - 1127918

American Structurepoint Inc**PUBLISHER'S CLAIM****LINE COUNT**

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COMPUTATION OF CHARGES198 lines, 2 column(s) wide equals395 equivalent lines at \$ 0.619 cents per line

\$ 244.51

Additional charges for notices containing rule or tabular work
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122.26

Charge for extra proofs of publication
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TOTAL AMOUNT OF CLAIM\$ 366.77**DATA FOR COMPUTING COST**

Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 2

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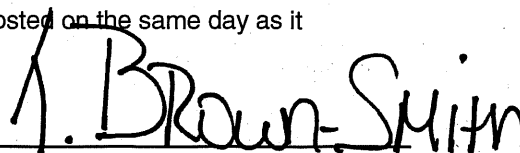
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T. Brown-Smith
Legal Clerk

Date: June 11, 2014

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benefit training schedules



Diana Parker | The Journal Gazette
e Cole of Kendallville fix
r training schedule.

Makes 2 servings.

Meal 3

2 to 3 pounds beef roast
4 to 5 potatoes, peeled
and cut into large chunks
1 pound carrots, cut into
large chunks

¼ to ½ onion, cut into
chunks

Place meat and vegeta-

bles in a slow cooker. Cook
on low 8 to 10 hours. Makes
2 to 3 servings.

Cook's Corner is a weekly
feature. If you know of
someone to be profiled,
write to Cook's Corner, The
Journal Gazette, P.O. Box
88, Fort Wayne, IN
46801-88; fax 461-8648 or
email dparker@jg.net.

Makes 2 servings.

Meal 2

2 boneless chicken
breasts, cooked on the grill
or baked in oven)

1 (2-ounces) instant
mashed potatoes, cooked
according to package direc-
tions

1 (15-ounces) canned
beans

1 (15-ounces) canned
carrots

Optional small salad

Legal Notice Of Public Hearing

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The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the

NOTICE TO CONTRACTORS

NOTICE is hereby given by the Board of Public Works of the City of Fort Wayne, Indiana, that sealed proposals will be received by the Board of Public Works at its office located at Suite 40, Citizens Square, 200 East Berry Street until 8:30 A.M. on WEDNESDAY, 6/18/14 and in Council Chamber, Suite 035 Garden Level, Citizens Square until 8:30 to 9:00 A.M. Eastern Standard Time, on WEDNESDAY, 6/18/14 for the following:

CONTRACT/RESOLUTION NUMBER:
12285
FAIRFIELD/EWING/SUPERIOR
ROUNDAABOUT CONSTRUCTION AND
MAIN STREET

The contractor shall furnish all labor, insurance, equipment, materials and power for the complete performance of the following project:
FAIRFIELD/EWING/SUPERIOR
ROUNDAABOUT CONSTRUCTION AND
MAIN STREET as follows: That it is deemed necessary to improve the project corridor in accordance with the bid documents and drawings.

It is in accordance with the Contract and Documents and Drawing No. ST-2285 Sheets 1 THRU 234 prepared by AMERICAN STRUCTUREPOINT, INC. of the City of Fort Wayne, Indi-

No bid will be accepted from, or contract awarded to any person, firm, or corporation that is in arrears to the City of Fort Wayne, Indiana, upon any debt or contract, or, who has failed to execute, in whole or in part in a satisfactory manner, any contract with the City; or, who is a defaulter as to surety or otherwise, upon any obligation to the City of Fort Wayne, Indiana.

Bidders must obtain copy of said bid documents pursuant to submitting their bids, by paying \$100.00 for each set of bid documents, not refundable at the Office of the Board of Public Works, Suite 240, Citizens Square.

No bid may be withdrawn after the scheduled time for receipt of bids for at least one hundred twenty (120) days to allow review of bids before announcing award of contract. The successful bidder will be required to furnish a satisfactory Labor and Material Payment Bond and Performance Bond each in the amount no less than one hundred (100%) percent of the contract price.

The Board reserves the right to reject any and all bids for failure to comply with applicable laws and/or with the Instructions to Bidders. The Board also reserves the right to waive any defect in any bid.

BOARD OF PUBLIC WORKS
Robert Kennedy, Chairman
Kumar Menon, Member
Mike Avila, Member

ATTEST: Victoria Edwards, Clerk
6-4-11 1128012 hspaxlp

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Services
book!



feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard.

The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. As part of the Indiana Historic Bridge Inventory project, the bridge was determined to be Non-Select. The bridge has been marketed for re-use for the past six months and information about the bridge can be found on the INDOT Historic Bridge Marketing website: <http://www.in.gov/indot/2532.htm>. The removal or demolition would be consistent with the Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges (Historic Bridge PA). The public hearing will be the last opportunity for a responsible party to step forward and provide the necessary sureties to obtain ownership of the bridge.

In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration adverse effect finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA adverse effect finding there is a Section 4(f) use. An individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be de minimis with regard to Section 4(f).

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation have agreed this project falls within the guidelines of an Environmental Assessment (EA) document.

The Public Hearing will consist of an informal Q&A session involving the project management team and a formal presentation regarding the project. Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

All comments collected before, during, and for a period of 30 days after the hearing will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

Before and after the formal hearing, the EA document and the 800.11(e) documentation for Section 106 will be available for review. Anyone interested in talking to the engineers about the project may do so before or after the formal hearing. Conversations prior to or after the formal hearing will not be part of the official record.

A copy of the EA document and the 800.11(e) documentation for Section 106 are available for viewing at the following locations on or after June 4, 2014.

Allen County Public Library, 900 Library Plaza, Fort Wayne, Indiana 46802. Phone: (260) 421-1200

Allen County Public Library, 2201 Sherman Boulevard, Fort Wayne, Indiana 46808. Phone (260) 421-1335

Fort Wayne Transportation Engineering Department, Citizens Square, Ste. 210, 200 East Berry Street, Fort Wayne, Indiana 46802. Phone: (260) 427-1356

In accordance with the Americans With Disabilities Act if you have a disability for which the City of Fort Wayne needs to provide accommodations, please call Briana Hope at American Structurepoint, Inc., at (317) 547-5580, by Monday, June 16, 2014.

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled Early Coordination, Public Involvement and Project Development, and the Indiana Public Involvement Manual approved by the Federal Highway Administration, US Department of Transportation, on April 1, 2009.

Please direct any questions or comments concerning this project to Briana Hope, American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256, (317) 547-5580, bhope@structurepoint.com. Comments on the proposed project will be accepted for 30 days after the Public Hearing. Comments or concerns brought forth by the public during this process will be addressed in the FONSI request document submitted to the FHWA.
6-4-11 1127918 hspaxlp

The News-Sentinel

Allen County, Indiana

Account # 1064183 - 1127918
American Structurepoint Inc**PUBLISHER'S CLAIM****LINE COUNT**Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of the type in which the body of the
advertisement is set) -- number of equivalent lines

Head -- number of lines

Body -- number of lines

Tail -- number of lines

Total number of lines in notice

395**COMPUTATION OF CHARGES**198 lines, 2 column(s) wide equals395 equivalent lines at \$ 0.619 cents per line

\$ 244.51

Additional charges for notices containing rule or tabular work
(50 per cent of above amount)

122.26

Charge for extra proofs of publication

(\$1.00 for each proof in excess of two)

-

TOTAL AMOUNT OF CLAIM\$ 366.77**DATA FOR COMPUTING COST**

Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 2

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 3 times.

The dates of publication being as follows:

6/4/20146/11/2014

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Additionally, Newspaper has a Web site and this public notice was posted on the same day as it was published in The News-Sentinel.

*T. Brown-Smith*T. Brown-Smith
Legal Clerk

Attachment 3 - 492 of 531

Date: June 11, 2014

ATTACH COPY OF ADVERTISEMENT HERE

talks tactics



Associated Press file photo
Jozy Altidore, left, and Clint
Beckham are to tinker with his formations.

Ghana — which eliminated the U.S. from the last two World Cups — then play Portugal and Cristiano Ronaldo six days later. The Americans close the first round against three-time champion Germany.

"You can win a tournament like Spain did two years ago, play a 4-6-0, leave all the strikers out and have midfielders that score all the goals," Klinsmann said. "Systems in the near future have no meaning anymore, really."

NOTICE TO PUBLIC & BIDDERS Advertisement to Bid

The Fort Wayne-Allen County Airport

Beckham hints he might play again

From The Associated Press

LONDON — David Beckham says he might come out of retirement and play soccer again.

The former England captain, who retired last year after a final stint at Paris Saint-Germain, said in a BBC documentary he had "a tough time" after hanging up

his cleats and is tempted to come out of retirement.

The 39-year-old Beckham, who is trying to start a Major League Soccer team and build a stadium in Miami, said "there's never been a player-owner, but maybe?"

Beckham traveled into the Amazon rainforest to make the documentary, which airs Monday.

Legal Notice Of Public Hearing

The City of Fort Wayne will hold a Public Hearing on Wednesday, June 18, 2014, beginning at 6:00 p.m., at the North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana for the proposed State Boulevard Reconstruction Project between Spy Run and Cass Street, Fort Wayne, Allen County. An open house session will take place from 6:00 p.m. until 6:30 p.m. with the formal presentation beginning at 6:30 p.m.

The purpose of the public hearing is to offer all interested persons an opportunity to comment on the environmental document, 800.11(e) documentation for Section 106, and preliminary design plans for the proposed project.

The City of Fort Wayne is developing a federal-aid project to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River. This project, which begins at Cass Street and extends east to Spy Run, has an approximate length of 2,370 feet.

The proposed project involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits.

A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

The proposed project would require an estimated 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an "adverse effect" finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273).

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the district and also the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7 feet) at its eastern end.

the Airport Authority, Suite 209, 3801 W. Ferguson Road, Fort Wayne, Indiana, 46809 for the following:

**SMITH FIELD AIRPORT
Taxiway H Construction Project
Project No. AIP-15**

One set of bid forms, specifications and related legal documents may be ordered or downloaded through Eastern Engineering at www.easternengineering.com. You may also contact Eastern at 260-426-3119. A nonrefundable purchase price of \$85.00 dollars payable to Eastern Engineering must be received before documents will be released via hard copy. A fee of \$37.00 will be charged per download.

Bids will be publicly opened and read aloud at 11:15 a.m. eastern standard/daylight time on the last date for receiving bids in the office shown above. Bids shall be properly executed and addressed to the office and address shown above, together with the documents required by the bid forms, specifications and related legal documents. A bid bond with good and sufficient surety, issued by a company licensed to do business in the State of Indiana, or, in the alternative, a certified check on a solvent bank, equal to ten percent (10%) of the total bid insuring that if the bid is accepted, a contract will be entered into and the performance of its proposal secured, must accompany the Contractor's bid.

It is the policy of the Authority to ensure that Disadvantaged Business Enterprises DBEs, as defined in Part 26, have an equal opportunity to receive and participate in DOT-assisted contracts. It is also our policy:

1. To ensure nondiscrimination in the award and administration of DOT assisted contracts;
2. To create a level playing field on which DBEs can compete fairly for DOT assisted contracts;
3. To ensure that the DBE Program is narrowly tailored in accordance with applicable law;
4. To ensure that only firms that fully meet 49 CFR Part 26 eligibility standards are permitted to participate as DBEs;
5. To help remove barriers to the participation of DBEs in DOT assisted contracts; and
6. To assist the development of firms that can compete successfully in the market place outside the DBE Program.

A 10.5% Disadvantaged Business Enterprise participation goal has been set for the DOT assisted portion of this project.

Wage rates shall not be less than the prevailing wage rates as determined pursuant to the Wage Rate Decision of the Secretary of the U.S. Department of Labor establishing the minimum wage rates which the Contractor shall pay skilled and unskilled labor.

The Bidder must supply all the information required by the bid or proposal form.

A pre bid conference will be held in the terminal at Smith Field Airport, 902 W. Ludwig Road, Fort Wayne, Indiana 46825 on June 10, 2014, at 1:00 p.m., eastern time.

The Fort Wayne Allen County Airport Authority reserves the right to reject any and/or all bids and to waive any irregularities in the bidding procedure.

Robin R. Strasser, CPA
Director of Administration & Finance
Fort Wayne Allen County
Airport Authority
6-2-9 1126460 hspaxlp

tribution feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing a resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations. State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad bridge would also change the character of the district along State Boulevard.

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In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.5(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration adverse effect finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA adverse effect finding there is a Section 4(f) use. An individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be de minimis with regard to Section 4(f).

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Before and after the formal hearing, the EA document and the 800.11(e) documentation for Section 106 will be available for review. Anyone interested in talking to the engineers about the project may do so before or after the formal hearing. Conversations prior to or after the formal hearing will not be part of the official record.

A copy of the EA document and the 800.11(e) documentation for Section 106 are available for viewing at the following locations on or after June 4, 2014.

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Fort Wayne Transportation Engineering Department, Citizens Square, Ste. 210, 200 East Berry Street, Fort Wayne, Indiana 46802. Phone: (260) 427-1356

In accordance with the Americans With Disabilities Act if you have a disability for which the City of Fort Wayne needs to provide accommodations, please call Briana Hope at American Structurepoint, Inc., at (317) 547-5580, by Monday, June 16, 2014.

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled Early Coordination, Public Involvement and Project Development, and the Indiana Public Involvement Manual approved by the Federal Highway Administration, US Department of Transportation, on April 1, 2009.

Please direct any questions or comments concerning this project to Briana Hope, American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256. (317) 547-5580, bhope@structurepoint.com. Comments on the proposed project will be accepted for 30 days after the Public Hearing. Comments or concerns brought forth by the public during this process will be addressed in the FONSI request document submitted to the FHWA.

6-4-11 1127918 hspaxlp



AMERICAN
STRUCTUREPOINT
INC.

June 5, 2014

Mr. Mitchell Zoll
Director
Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Mr. Zoll:

As a result of comments received in response to the August 27, 2012, Federal Highway Administrations (FHWA) Findings and Determinations and the subsequent September 19, 2012, Consulting Party Meeting, the FHWA's Findings and Determinations required under procedures established for Section 106 consultation have been revised and updated along with Version 02/20/2013 of the draft Memorandum of Agreement (MOA).

Enclosed, please find a copy of the updated February 27, 2013, FHWA Findings and Determinations required under procedures established for Section 106 consultation. The approval indicates FHWA concurs with the Area of Potential Effect, the Determination of Properties listed or Eligible for listing in the National Register of Historic Places, and the Determination of Adverse Effect for the proposed State Boulevard Reconstruction Project (DES No. 0400587). Included with this documentation is all correspondence regarding the proposed project and the revised MOA Version 04/11/2013.

Under the provisions of 36 CFR 800.5(c), the state historic preservation officer and all consulting parties have 30 days from the receipt of an adequately documented FHWA determination to take exception to that determination. In a letter dated April 1, 2013, the Indiana SHPO concurred with FHWA's February 27, 2013, Section 106 finding of Adverse Effect for the State Boulevard Reconstruction Project (letter attached). They also provided suggestions regarding Version 02/20/2013 of the draft MOA. You are asked as a consulting party to provide any comments or concerns you have regarding the FHWA Finding of Adverse Effect.

IN20071404



AMERICAN STRUCTUREPOINT, INC.

Mr. Mitchell Zoll
June 5, 2014
Page 2

Please provide all comments to:

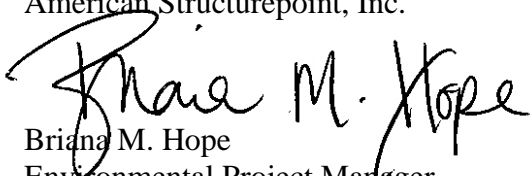
Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Comments must be submitted no later than July 18, 2014. If you do not comment on the Adverse Effect finding by July 18, 2014, your concurrence will be assumed.

The City of Fort Wayne would also like to make you aware of the Public Hearing scheduled for Wednesday, June 18, 2014, beginning at 6:00 p.m., at the North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana, for the proposed State Boulevard Reconstruction Project between Spy Run and Cass Street. The purpose of the public hearing is to offer all interested persons an opportunity to comment on the environmental document, Section 106, and preliminary design plans for the proposed project. A copy of the Notice of Public Hearing has been included with this mailing.

We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

cc: Mitch Zoll, IDNR
Shan Gunawardena, City of Fort Wayne
Joyce Newland, FHWA
Larry Heil, FHWA
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Greg Smith, INDOT
John Carr, IDNR
Amy Johnson, IDNR



Mr. Mitchell Zoll

June 5, 2014

Page 3

Distribution List

Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandeburg, Indiana Historic Spans Task Force
Susan Haneline, Brookview Civic Neighborhood Association
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
John Shoaff, Fort Wayne City Council
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP

DNR

Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



July 3, 2014

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Draft "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Version 4/11/2013)
(Des. No. 0400587; Federal Project No. IN200701404; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the documents submitted with your two, June 5, 2014, letters, which we received on June 6, for the aforementioned project in Allen County, Indiana.

We appreciate the inclusion of two new "whereas clauses" in the preamble of Version 4/11/2013 of the draft MOA that, as we had suggested in our April 1, 2013, letter, provide the street addresses of the properties that will be demolished. It occurs to us, however, that because the State Boulevard Bridge over Spy Run is individually eligible for inclusion in the National Register of Historic Place, in addition to being a major, contributing structure to both of the historic districts that will be adversely affected, reference to that bridge also should be included in the second of the two new "whereas clauses," even though the bridge is not a building and—as far as we know—does not have a street address.

Three other recommendations for changes to Version 2/20/2013 of the draft MOA that we had offered were incorporated into Version 4/11/2013, and we thank you for that, as well.

We noticed, however, that one entire mitigation stipulation that had been in Version 2/20/2013 of the draft MOA has disappeared completely in Version 4/11/2013. Stipulation III.C. in Version 2/20/2013 would have committed the City of Fort Wayne to exploring funding opportunities that would have provided low-cost grants or loans to improve or rehabilitate historic properties within the Brookview-Irvington Park Historic District. We think that such a grant or loan program, if it could be implemented, might provide a direct way of helping to preserve at least some of the contributing properties within the historic district that will be left in place by the State Boulevard Reconstruction. Why was that stipulation eliminated in draft MOA Version 4/11/2013?

The second paragraph of Stipulation III. in draft MOA Version 2/20/2013 would have allowed the Indiana SHPO 30 days in which to comment on the content and design of the four interpretive plaques, but in Version 4/11/2013 that comment period has been shortened to 15 days. We ask that the 30-day comment period be reinstated. We have a fairly full review workload, and the Indiana Department of Transportation reserves the right to ask us, and does ask us from time to time, to expedite our comments on other projects.

We are aware that ARCH, Inc., and Indiana Landmarks presented an alternative plan for the State Boulevard Reconstruction at the June 18, 2014, public hearing in Fort Wayne on the Environmental Assessment for this project, but

we have not attempted to comment on that alternative plan or to factor it into our comments on the draft MOA Version 4/11/2013.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about issues pertaining to buildings or structures should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903.

Very truly yours,



Mitchell K. Zoll
Deputy State Historic Preservation Officer

MKZ:JLC:jlc

emc: Joyce Newland, Federal Highway Administration, Indiana Division
Lawrence Heil, P.E., Federal Highway Administration, Indiana Division
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.



AMERICAN
STRUCTUREPOINT
INC.

December 22, 2014

Mr. Mitch Zoll
Division Director
Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN200701404

Dear Mr. Zoll:

The City of Fort Wayne, in cooperation with the Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT), conducted Section 106 consultation as part of the State Boulevard Reconstruction Project from Spy Run Avenue to Cass Street in Fort Wayne, Allen County, Indiana (Des. No. 0400587/DHPA No. 5903). Pursuant to Section 106 of the National Historic Preservation Act (1966) and 36 CFR Part 800 (2013), federal agencies are required to take into account the effects of their undertakings on both aboveground and archaeological historic properties. The FHWA issued an "adverse effect" finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273) on February 27, 2013. Accordingly, a Memorandum of Agreement (MOA) has been developed to address these adverse effects.

On June 18, 2014, a Public Hearing was held for the proposed project. At the Public Hearing ARCH, Inc. presented an alternative prepared by Storrow Kinsella Associates and Transportation Solutions, LLC. Storrow Kinsella Associates and Transportation Solutions, LLC were commissioned by ARCH, Inc., Indiana Landmarks, Friends of the Parks, and the Brookview-Irvington Park Neighborhood Association to examine the background research developed for the proposed project to determine if there was an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible, and avoided, minimized or mitigated the adverse effect to the neighborhood. As such, it was

IN200701404

 AMERICAN STRUCTUREPOINT, INC.

Mr. Mitch Zoll
December 12, 2014
Page 2

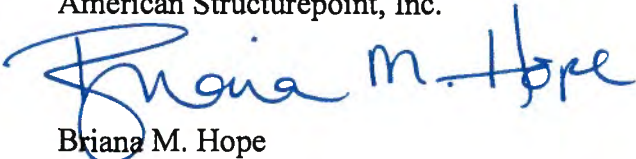
determined through coordination with the FHWA and INDOT that the submitted alternative should be evaluated as part of the environmental process.

In coordination with FHWA and INDOT an Additional Information (AI) study was prepared to analyze and evaluate the Consulting Parties Proposed Alternative (CPPA) utilizing the same criteria as each alternative considered in the May 14, 2014 approved Environmental Assessment (EA). Through the alternative evaluation process, as documented in the AI, it has been determined that the CPPA is not reasonable as it does not sufficiently address the project's purpose and need. A copy of the December 17, 2014 approved AI is attached for your records. In addition, the Draft Section 4(f) Evaluation for Impacts to Historic Properties prepared for the State Boulevard Reconstruction Project was updated to incorporate the evaluation of the CPPA. An Addendum to the State Boulevard Reconstruction Project 800.11(e) Documentation was also prepared. Both of these documents are included as an attachment to the AI.

Finally, in response to your July 3, 2014 letter, the MOA has been updated to reflect your suggestions regarding Version 4/11/2013. Please find enclosed Version 12/15/2014 of the final MOA for your review and signature.

If you have any questions or comments, please do not hesitate to contact me at (317) 547-5580 or at bhope@structurepoint.com. A copy of this letter and all attachments has been provided to consulting parties for informational purposes.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn:slg

Enclosures

Mr. Mitch Zoll

June 5, 2014

Page 3

Distribution List:

Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandeburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Haneline, Brookview Civic Neighborhood Association
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
Larry Heil, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amy Johnson, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT

ADDENDUM to State Boulevard Reconstruction Project
From Spy Run to Cass Street
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Prepared December 12, 2014

Introduction

The City of Fort Wayne, in cooperation with the Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT), conducted Section 106 consultation as part of the State Boulevard Reconstruction Project from Spy Run Avenue to Cass Street in Fort Wayne, Allen County, Indiana (Des. No. 0400587/DHPA No. 5903). Pursuant to Section 106 of the National Historic Preservation Act (1966) and 36 CFR Part 800 (2013), federal agencies are required to take into account the effects of their undertakings on both aboveground and archaeological historic properties. The FHWA issued an “adverse effect” finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273) on February 27, 2013.

This Addendum to the State Boulevard Reconstruction Project 800.11(e) Documentation is to incorporate the Consulting Parties Proposed Alternative (CPPA) as presented by Storrow Kinsella Associates in collaboration with Transportation Solutions, LLC. Through the alternative evaluation process it has been determined that the CPPA is not reasonable as it does not sufficiently address the project’s purpose and need. This document amends the following sections of State Boulevard Reconstruction Project 800.11(e) Documentation with discussion of the CPPA.

Consulting Party Coordination:

On June 18, 2014, a Public Hearing was held for the proposed project. At the Public Hearing and in a letter dated July 18, 2014 (Appendix A – pages 1 to 3) ARCH, Inc. presented an alternative prepared by Storrow Kinsella Associates and Transportation Solutions, LLC (Appendix B – pages 4-14). Storrow Kinsella Associates and Transportation Solutions, LLC were commissioned by ARCH, Inc., Indiana Landmarks, Friends of the Parks, and the Brookview-Irvington Park Neighborhood Association to examine the background research developed for the proposed project to determine if there was an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible, and avoided, minimized or mitigated the adverse effect to the neighborhood. As such, it was determined through coordination with the FHWA and INDOT that the submitted alternative should be evaluated as part of the environmental process.

The following alternative has been evaluated for the State Boulevard Reconstruction project:

Alternative 3E: CPPA - The CPPA, as presented by Storrow Kinsella Associates in collaboration with Transportation Solutions, LLC consists of a two-lane parkway alignment shifted south of existing State Boulevard between Clinton Street and the Westbrook/Edgehill Drive intersection. The transition from existing State Boulevard to the CPPA includes a single lane roundabout at the Westbrook/Edgehill Drive intersection and a two-lane signalized hybrid roundabout at the North Clinton Street intersection. The CPPA includes a new crossing of Spy Run Creek raised above the 100-year flood elevation and a multi-use path separated from the roadway. The multi-use path would utilize the existing Spy Run Creek Bridge. If the deteriorated bridge condition or flooding issues dictate removal, a new multi-use path

bridge would be constructed. Eastbrook Drive would be converted to a cul-de-sac just north of the realigned State Boulevard. Access to existing State Boulevard would be obtained by utilizing the proposed roundabout at Clinton Street. No direct access to the realigned State Boulevard would be provided at Eastbrook Drive, Oakridge Road, or Terrace Road.

This alternative would require the relocation of at least two businesses and one residential property for construction of the Clinton Street roundabout. The CPPA is estimated to cost \$9.6 million. The cost of the CPPA is elevated due to the increased construction cost associated with the larger footprint and increased infrastructure associated with the two proposed roundabouts, the addition of a second pedestrian bridge, the potential for mechanically stabilized earth (MSE) retaining walls needed to keep fill slopes from extending into Spy Run Creek and also associated with the realigned State Boulevard near the proposed Eastbrook Drive cul-de-sac.

The CPPA results in the use of the Brookview-Irvington Historic District, Fort Wayne Park and Boulevard System Historic District, the Bridge over Spy Run Creek, and Vesey Park, all 4(f) resources.

The CPPA is not reasonable as it does not satisfy the Project's purpose and need. Based on a capacity analysis prepared for the CPPA, this alternative would not address the traffic congestion issues established by the Project's primary purpose and need. The intersections of State Boulevard with Spy Run and Clinton Street would not function at an acceptable level of service in the design year. For the CPPA, the overall intersection LOS is E or F during either the AM or PM peak hours in all scenarios analyzed. The CPPA would also likely require a level one design exception* with regards to roadway geometrics as it appears the CPPA utilizes substandard curvature in the proposed relocated segment of State Boulevard resulting in substandard sight distance conditions. Therefore, the CPPA does not appear to address the safety components associated with the sight distance, geometrics, and congestion. However, while not as significant as the need to address congestion and the safety components associated with sight distance, geometrics, and congestion, the CPPA does address the flooding and Greenways Trail System connectivity components of the purpose and need by proposing to elevate the roadway above the 100-year elevation and provide a separated multi-use path. Furthermore, this alternative would require an estimated \$9.6 million project cost, approximately \$1.6 million (20% increase) more than the preferred alternative (3A) presented in the May 14, 2014, approved EA. For these reasons, the CPPA is not considered reasonable and has been eliminated from further consideration.

Addendum Appendices

Appendix A July 18, 2014 Letter from ARCH, Inc. – Pages 1-3

Appendix B The Consulting Parties Proposed Alternative (CPPA) as presented by Storrow Kinsella Associates, in collaboration with Transportation Solutions, LLC – Pages 4-14



The mission of ARCH is to advocate for the protection and preservation of historically and culturally significant assets and historic places in Allen County and northeast Indiana.

July 18, 2014

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Dear Ms. Hope:

I am writing in reference to the June 5, 2014 updated FHWA Findings and Determinations materials provided as part of the Section 106 Review, and the May 2, 2014 Environmental Assessment for the proposed State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana (Des. No. 0400587, DHPA No. 5903, Project No. IN20071404.) Thank you for the opportunity to comment and the work that went into preparing this and the related Environmental Assessment document for the above referenced project. I have a few specific points that I would like to address concerning the document and proposed project.

1. We concur that the project as currently proposed will cause an Adverse Effect under Section 106 and Section 4(f) to the three historic properties (Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge carrying State Boulevard over Spy Run Creek) located within the APE.
2. We believe that purpose of any Section 106 Review is as identified in 36 CFR 800.1(a) to "seek to accommodate historic preservation concerns with the needs of Federal undertakings through consultation among the agency official and other parties with an interest in the effects of the undertaking on historic properties, commencing at the early stages of project planning. The goal of consultation is to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties." Pursuant to that purpose, we believe that the Findings and Determinations materials provided on June 5, 2014 adequately identifies historic properties, and assesses effects of the project as currently proposed. However, we believe that the third portion of the identified goal to "seek ways to avoid, minimize or mitigate any adverse effects on historic properties" is inadequate. To further the discussion, ARCH, Indianal Landmarks, Friends of the Parks, and the Brookview-Irvington Park Neighborhood Association commissioned Storrow Kinsella Associates and Transportation Solutions to examine the background research developed for this project to see if there was

an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible and avoided, minimized or mitigated the adverse effect to the neighborhood. We believe that the alternative plan (the "Alternative"), presented by ARCH to American Structurepoint at the June 18, 2014 Public Hearing for the Environmental Assessment, and provided to American Structurepoint, FHWA, INDOT and Indiana SHPO by e-mail delivery on June 24, 2014 and June 26, 2014, better addresses historic preservation concerns while accommodating the purpose and need of the Federal undertaking.

3. We believe that the Alternative is provided during the completion of the Section 106 Review and is thus governed by the guidance in 36 CFR 800.1(c). This guidance mandates that the Alternative must undergo unrestricted consideration by the agency official, as indicated in 36 CFR 800.1(c), which directs the agency official to complete the Section 106 process in a manner "that such actions do not restrict the subsequent consideration of alternatives to avoid, minimize or mitigate the undertaking's adverse effects on historic properties."

4. We believe that the Alternative is presented in agreement with direction contained in 36 CFR 800.3(b) which directs the agency official to coordinate the Section 106 Review with "... the overall planning schedule for the undertaking and with any reviews required under other authorities..." including NEPA and Section 4(f) of the Department of Transportation Act. We further believe that guidance contained in 36 CFR 800.8(a)(1) encouraging agencies to "consider their section 106 responsibilities as early as possible in the NEPA process..." provides further impetus to consider the Alternative plan under both the 4(f) and NEPA processes.

5. We believe that the Alternative meets the identified Purpose and Need for the project. As stated in the May 2, 2014 Environmental Assessment, the Purpose and Need for the project is as follows;

"The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System."

We believe that the Alternative meets the purpose of the project by improving corridor connectivity for both motorists and pedestrians. The identified problems of congestion, substandard sight distance, and geometrics are all addressed in the Alternative. Roadway flooding as well is addressed, although this remains a concern for both the currently proposed project and the Alternative. The nearly adjacent and recently completed US 27 Bridge Reconstruction project (Des. No. 0200914 and 0101527) experienced roadway flooding a little over a year after the project's conclusion, demonstrating the need for a thorough investigation and remediation of a larger problem. Regarding the need for the project, we believe that the Alternative addresses the identified congestion, substandard sight distance, and geometrics concerns as well as the pedestrian safety concerns.

6. We believe that the Alternative significantly "minimizes, mitigates or avoids" adverse Effect to historic properties as is identified as a purpose of Section 106, and makes "special effort to preserve...historic sites" as is the stated purpose of Section 4(f).

Comparing the enumerated Adverse Effect of the current plan as stated on page 206-209 of the Environmental Assessment, the Alternative proposal results in far less damage under both Sections 106 and 4(f), making it a prudent and feasible alternative that causes the least overall harm. In the Fort Wayne Park and Boulevard System Historic District, and in the Brookview-Irvington Historic District, the Alternative avoids to a much greater degree the removal and disruption of the NRHP-listed State Boulevard, minimizes the amount of new land disruption by using land previously altered through the removal of "flood-buyout" houses, and retains the plan of the Arthur Shurcliff-designed plat. In addition, the Alternative avoids the demolition of NRHP-listed residences entirely. The Alternative mitigates the disruption and bifurcation of the Shurcliff-designed plat by the use of a curvilinear "new" State Boulevard replicating the scale of the "original" State Boulevard, providing the driver and pedestrian alike with a similar feel, design and setting.

7. We believe that the Draft Memorandum of Agreement (MOA) as presented in the May 2, 2014 Environmental Assessment and the June 5, 2014 Section 106 Review will need to be modified, particularly if the Alternative is adopted as the basis for proceeding forward in this project. We agree that the use of Context Sensitive Solutions (CSS) is preferred, but would like to see that any such CSS solutions be implemented where feasible rather than merely "considered" as is suggested in the Draft MOA.

Sincerely,



Michael Galbraith
Executive Director, ARCH, Inc.

Enc; "Alternative" Plan (SKA + Transportation Solutions for ARCH, Inc., Indiana Landmarks, et al)



storror kinsella associates

urban design & planning for places | connections | strategies

December 9, 2013

Mr. Michael Galbraith
Executive Director
ARCH, Inc.
818 Lafayette Street
Fort Wayne, IN 46802

Re: State Boulevard Reconstruction – Alternative Concepts
Fort Wayne, Indiana

Dear Mike and Consulting Parties Team,

Storror Kinsella Associates, in collaboration with Transportation Solutions, LLC has completed our analysis, evaluation and development of a sketch plan reconstruction alternative for the five-lane roadway widening project currently being proposed by the City of Fort Wayne.

We are confident that the resulting report provides a foundation for ARCH, Inc. and the Consulting Parties Team to have a constructive dialog with the City and its consultant. The *Consulting Parties Proposed Alternative* (CPPA) provides a solution that restores Spy Run Creek Parkway continuity, and maintains the integrity of the Brookview-Irvington historic neighborhood while meeting overall economic development, flood control, connectivity and beautification goals.

Please note that this report is a "sketch plan" alternative. We have made a good-faith effort to consider existing conditions and the purpose and need of the project, but must emphasize that additional design and study will be needed before this proposed alternative can be fully incorporated into the City's project.

The costs for the CPPA appear to be of similar magnitude or less than the City's Preferred Alternative, based on comparative lane-miles, bridge cross section and length, and probable right-of-way acquisition. In addition, the long term maintenance costs are comparable or less for the CPPA, based on thoroughfare lane length reduction from 5,175 to 2,100 feet.

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of the proposed alternative.

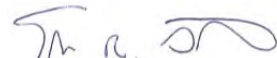
We remain available to answer questions and assist you and the City with moving an improved State Boulevard Reconstruction Project forward into implementation.

Sincerely,
STORROW KINSELLA ASSOCIATES

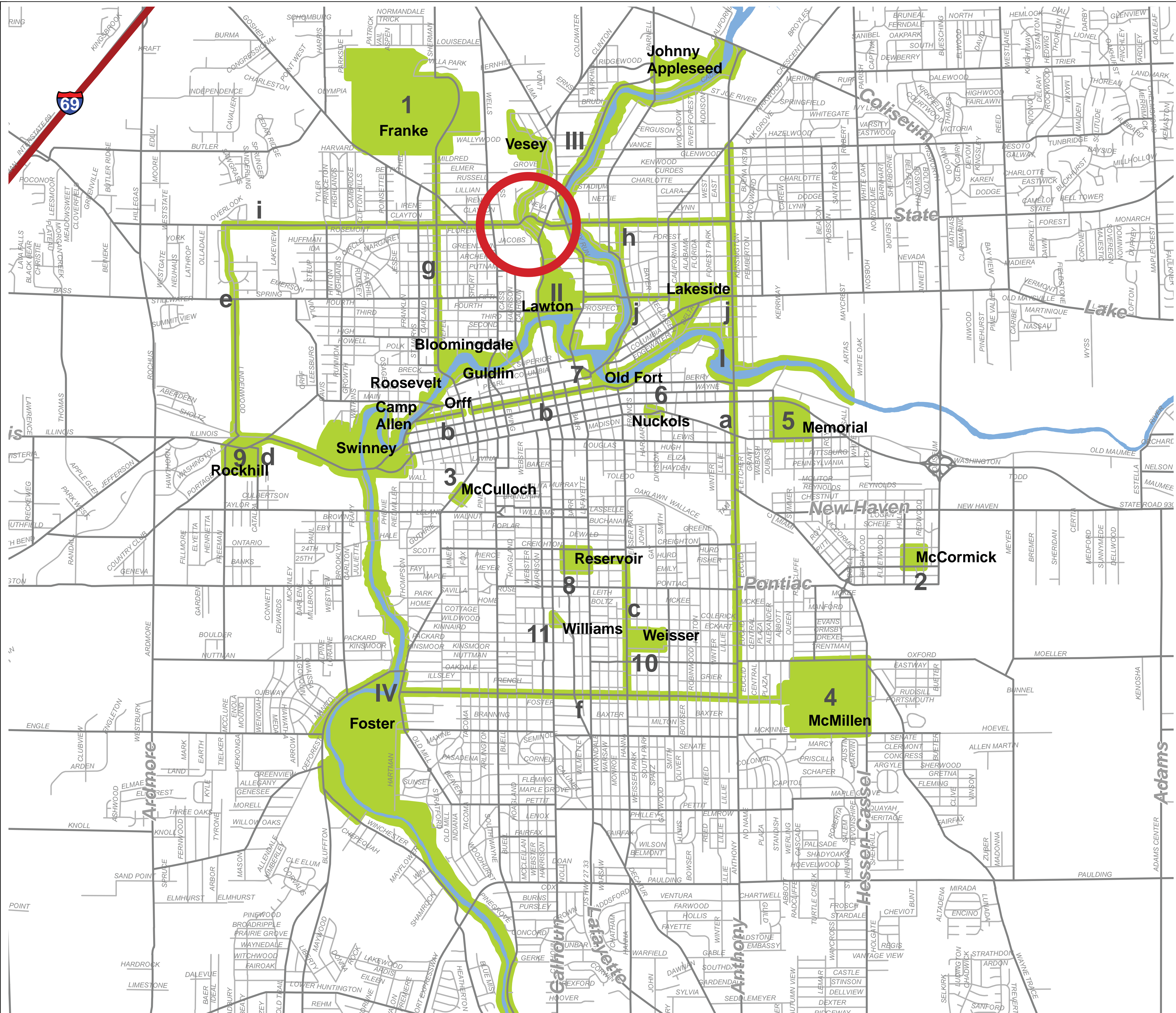

Margaret T. Storror, Principal


John W. Kinsella, Principal

TRANSPORTATION SOLUTIONS, LLC


Thomas R. Sturmer, Principal

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City of Fort Wayne

Park and Boulevard System

Historic District, Fort Wayne, Allen County, Indiana

Key Map

 National Register Boundary

- Parks**
1. Franke
 2. McCormick
 3. McCulloch
 4. McMillen
 5. Memorial
 6. Nuckols
 7. Old Fort
 8. Reservoir
 9. Rockhill
 10. Weisser
 11. Williams
- Parkways (includes riverfront parks):**
- I. Maumee River
 - i. Lakeside
 - II. Spy Run Creek (Brookview)
 - i. Lawton
 - ii. Vesey
 - III. St. Joseph River
 - i. Johnny Appleseed
 - IV. St. Mary's River
 - i. Bloomingdale
 - ii. Camp Allen
 - iii. Foster
 - iv. Guldlin
 - v. Orff/Thieme Drive Overlook
 - vi. Roosevelt
 - vii. Swinney (East & West)

- Boulevards:**
- a. Anthony Boulevard
 - b. Berry Street
 - c. Hanna/Taber Street
 - d. Jefferson Boulevard
 - e. Lindenwood Avenue (Brookside)
 - f. Rudisill Boulevard
 - g. Sherman Boulevard (Kekionga)
 - h. St. Joseph Boulevard
 - i. State Boulevard (Pfeifer)
 - j. Tennessee Avenue/Lake Avenue

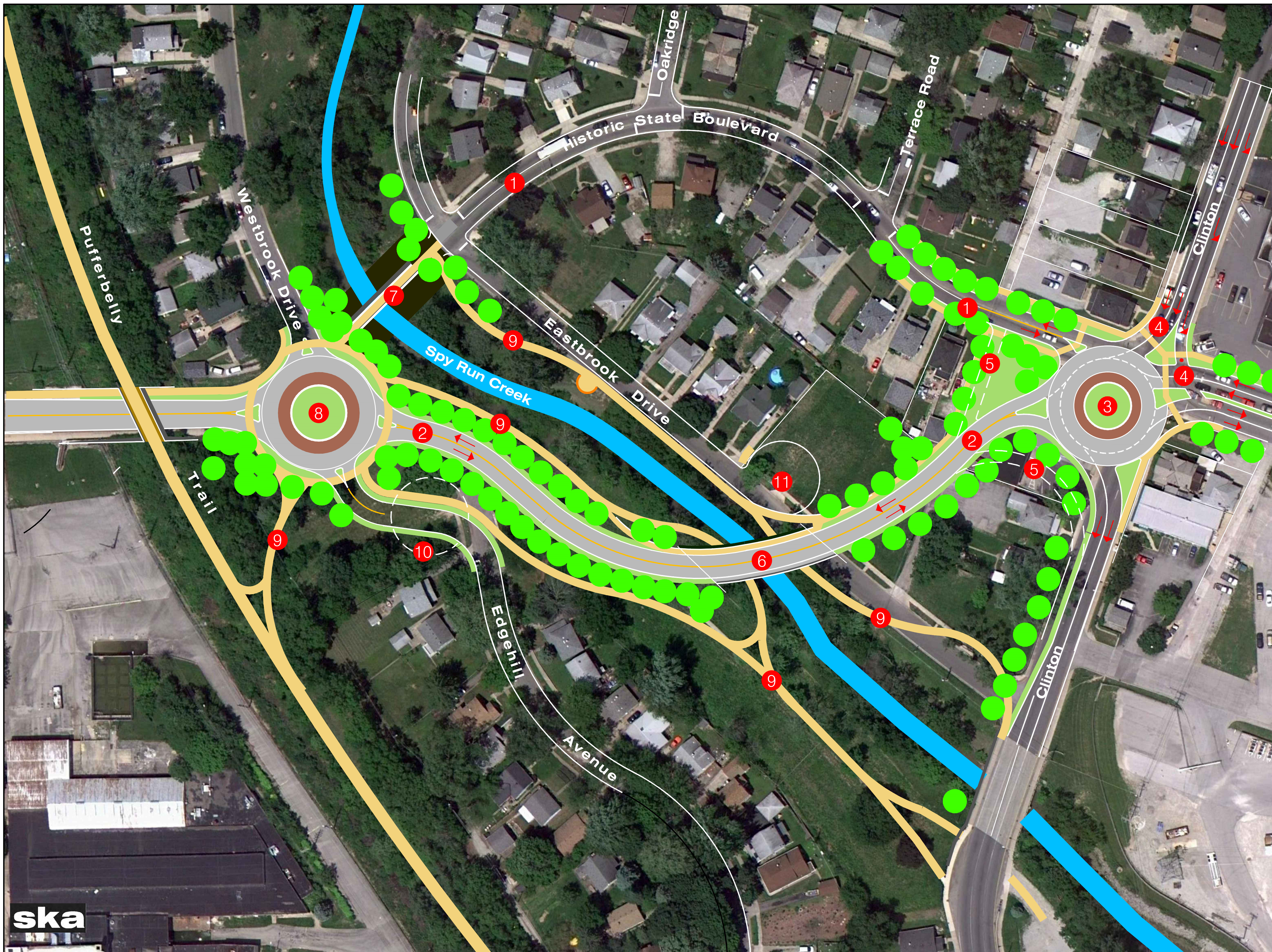


Note: This exhibit based on City of Fort Wayne Park and Boulevard System Historic District Key Map, prepared by The Westerly Group, Inc. and Storrow Kinsella Associates, 5/24/2010.

 Study Area

December 9, 2013

© 2013 SKA X:\1305_FW State Blvd\3Work\04Draw\ExhibitB_CPPA_1305.dwg 12/10/13



Plan Keynotes

- 1 Historic State Boulevard convert as local street & bike boulevard
- 2 New State Boulevard alignment
- 3 Clinton/State 2-lane hybrid urban roundabout with signalized eastbound left turn bypass
- 4 "Smart" roundabout signal -metered approaches to create gaps and balance flow during peak periods, and to provide pedestrian crossing synchronized with those phases (short/rolling yield or stop signal phases)
- 5 Optional bypass lanes to reduce roundabout circulating traffic loads
- 6 New Spy Run Creek 2-lane bridge
- 7 Spy Run Creek bike/ped bridge
 - Retrofit existing bridge or
 - New multi-use path bridge
- 8 Westbrook/Edgehill single lane urban roundabout w/ local street access
- 9 Bike-Ped path system
- 10 Edgehill Avenue cul de sac alternative
- 11 Eastbrook Drive cul de sac

Note: this is a sketch-level diagrammatic concept drawing. Roadway elements are proximate in scale and intended to illustrate general feasibility and proof of concept. Additional traffic engineering and roadway geometric study is required for project scope development.



North



Scale 1"=50'-0"

December 9, 2013

Storow Kinsella Associates urban design & planning for places | connections | strategies
in collaboration with
Transportation Solutions, LLC. | connecting the dots

Consulting Parties:
ARCH, Inc./ Historic Landmarks Foundation of Indiana
Brookview-Irvington Neighborhood Associations

Fort Wayne Historic State Boulevard
Consulting Parties Sketch Plan Alternatives Study
City of Fort Wayne Project/INDOT DES# 0400587

Sketch Plan

B

Attachment 3 - 510 of 531

Appendix B Page 3 of 11

BENEFITS COMPARISON															
Alternative	Connectivity Improvement	New roadway alignment length	Reconstruction alignment length	Residential Impacts	Commercial Impact	Total historic relocations or impacts	Other impacts	State Blvd. historic bridge impacts	State Boulevard Safety Improvement			Congestion Improvement	Reduce flooding	Alternative	
									Motorists	Bicyclists	Pedestrians			Feasible?	Prudent?
City's Preferred Alternative	Regional connectivity improved for through motorist, bicyclist, and pedestrian movements. Connectivity reduced for neighborhood residents.	1035 LF 5 lanes sidewalk or trail both sides	5175 lane feet 2070 sidewalk feet	15	None?	State Blvd historic parkway system compromised. Neighborhood bisected by new raised alignment. Historic bridge removed. Traffic speed and volumes increased through neighborhood.	View sheds affected for some residents. Historic neighborhood context affected. Five-lane raised roadway out of scale with residential neighborhood.	Historic bridge to be removed and replaced for flood elevation and structural deficiency reasons.	A portion of Old State Boulevard will be converted to local use with significant reduction in traffic volumes. New east-west alignment designed to current federal standards. Intersection capacities improved.	No bicycle accommodation currently exists. New multi-use path provided along new alignment.	New sidewalks provided along new alignment. Appropriate pedestrian crossings assumed to be provided at intersections.	Added lanes and intersection improvements reduce travel delays in overall system. The localized portion of State Boulevard will be a lower speed travel environment.	Raised elevation of State Boulevard and larger hydraulic bridge opening will help to reduce localized flooding frequency.	Yes	This is the City's Preferred Alternative. The Purpose and Need items are addressed, however the impact to the historic neighborhood and parkway system is significant.
Consulting Parties Proposed Alternative	State Boulevard corridor (and Greenways Trail System) connectivity improved for through motorist, bicyclist, and pedestrian movements. Historic State Boulevard will function as bicycle boulevard.	1048 LF 2 lanes 2 sidewalks	2096 lane feet 2096 sidewalk feet	One loss of alley access	Three: 1) Gas Station Store acquisition or relocation; 2) impact to garage and storage shed; 3) site impact.	Minimum historic structure impacts. State Boulevard historic integrity respected. Historic Bridge removed or rehabilitated for pedestrian/bicycle use because of deteriorated condition.	One commercial property on south side of State Boulevard at Clinton Street relocated to allow construction of a five-legged hybrid roundabout. Partial impacts on 2 additional commercial properties.	Historic bridge to be rehabilitated or removed and replaced for flood elevation and structural deficiency reasons. Pedestrian bridge with narrower profile impacts flooding to a lesser degree and provides east/west connectivity.	Significantly reduced volumes on existing State Boulevard alignment improves safety. Lower posted speed limit possible. New alignment meets goal of regional connectivity.	Existing State Boulevard converted to a low volume shared use roadway suitable for travel by bicycle.	Vehicle volumes greatly reduced along existing alignment. Sidewalks to be improved along existing alignment and provided along the new alignment.	Regional through traffic given more direct route. Intersection levels-of-service may be improved by a "smart" roundabout at Clinton and a single-lane roundabout at Westbrook.	Flood frequency reduced along new alignment due to raised bridge and roadway elevation.	Yes	This option addresses the Purpose and Need with less impacts to the historic resources. Magnitude of costs similar to the City's Preferred Alternative.

COST (RELATIVE) COMPARISON															
Alternative	New alignment length	New alignment width	Bridge length	Bridge width	Bridge elevation	Residential relocations	Other residential impacts	Commercial relocations	Intersection Types			Bridge removals	New pedestrian bridges	Local Streets Affected	
									Westbrook Drive	Oakridge Drive	Clinton Street			Pavement removed	Pavement added/ rehabilitated
City's Preferred Alternative	1035 Linear Feet (5175 lane feet)	5 Travel Lanes + 2 Sidewalks	150 LF 750 lane feet	Five lanes, curb and gutter, sidewalks and a 10-foot multi-use path on one side.	Bridge raised 7-feet above existing bridge elevation.	15 structures: Est. value \$1M	Any?	None?	Two-way stop controlled + energy and maintenance costs.	New Side-street stop controlled intersection.	Three-way signalized intersection + energy and maintenance costs.	Existing historic bridge.	One over State Boulevard for Pufferbelly Trail.	In vicinity of existing bridge and at both ends of existing alignment between Spy Run Creek and Clinton Street.	Oakridge Drive connector, "bulbouts " at termini of existing alignment.
Consulting Parties Proposed Alternative	1050 Linear Feet (2010 lane feet)	2 Travel Lanes + 2 Sidewalks	140 LF 280 lane feet	Two lanes, curb and gutter, and sidewalks both sides.	Same.	Two residences south of Clinton Street roundabout?	Alley access impacted for one residence. Net Benfit to Contributing Structures in District.	1 complete take and 2 partial Impacts: unknown cost.	Single-lane roundabout. Est. cost: \$500,000 + landscape maintenance costs.	No new intersection.	Multi-lane hybrid roundabout with traffic metering signalization: Est. cost: \$2M + energy and maintenance costs.	Existing historic bridge rehabilitated or replaced.	One over State Boulevard for Pufferbelly Trail, replace existing State Boulevard bridge with a new pedestrian or rehabilitate existing bridge.	In vicinity of existing bridge only.	Rehabilitate existing State Boulevard alignment to create bike boulevard and pedestrian connector.

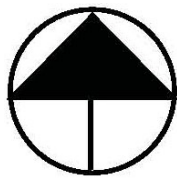
Description: City's Preferred Alternative	New four lane roadway with raised median and/or center turn lane for that portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. Realigned section raised up to 7-feet at new bridge for floodway consideration. New sidewalks and/or multi-use side path along both sides of roadway. New pedestrian bridge and approach ramps for future Pufferbelly Trail.
Description: Consulting Parties Proposed Alternative	Relocate the thoroughfare function of State Boulevard to south of Spy Run Creek from Westbrook Drive east to Clinton Street to minimize impacts to historic properties and parkway. Develop the thoroughfare as a two-lane roadway with sidewalks on both sides between a single-lane roundabout at Westbrook and two-lane hybrid roundabout at Clinton and State with no intersections between them to optimize flow and volume. The roundabouts help condition traffic to a steady state at a reduced speed. The Clinton/State intersection is a "smart" roundabout using advanced technologies consisting of signal metered approaches and traffic sensing to help balance flow, create gaps, and provide for pedestrian connectivity through the roundabout area. Existing bridge rehabilitated or replaced with new pedestrian/bicycle bridge to provide connectivity with future Pufferbelly Trail. Sidewalks along existing alignment to be improved.

Note: Information obtained from Section 106 Findings of Adverse Impacts report (approved August 27, 2012) and other information shared by ARCH. Concept plans for City alternatives not available at this time.

December 9, 2013



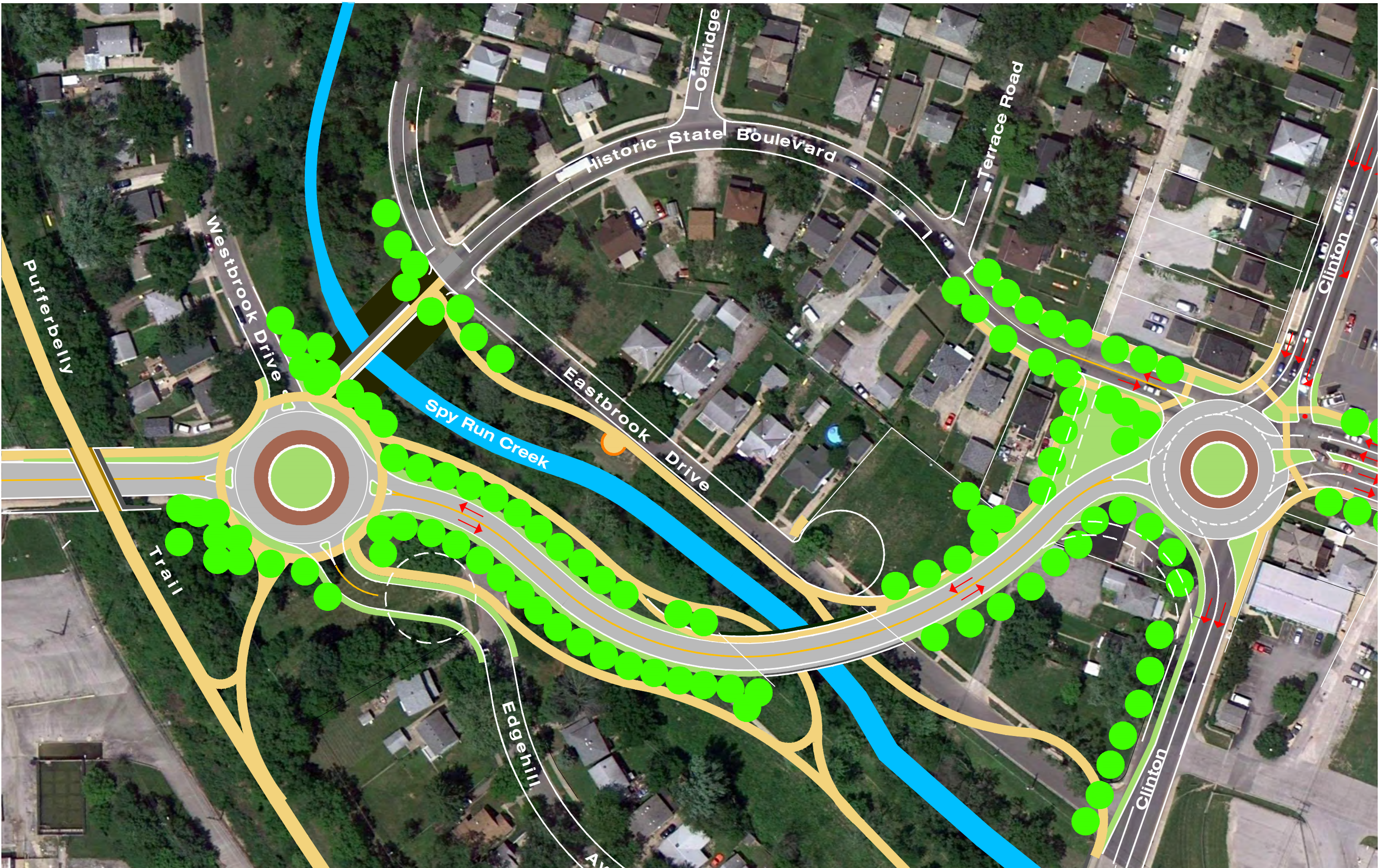
City's Preferred Alternative



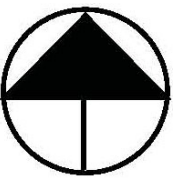
Purpose and Need

Shown above is a model of the City's Preferred Alternative; a new four lane roadway with raised median and/or center turn lane between North Clinton Street and Westbrook Drive. It is replacing a section of the existing two-lane State Boulevard in the City of Fort Wayne Park and Boulevard System Historic District.

The Park and Boulevard System Historic District represents a thoroughfare system designed in the early 1900's for the purpose of economic development, flood control, connectivity, and beauty. The proposed City's Preferred Alternative has many of the same purposes and characteristics, and, without reference to context, will be a significant upgrade to the City's thoroughfare system. However the segment between Clinton Street and Westbrook Drive does have historic context that the scale of the new boulevard will compromise.



Consulting Parties Proposed Alternative



Scale and Appropriateness

Shown above is the Consulting Parties Proposed Alternative, featuring a two-lane roadway able to accommodate the 2005 peak hour traffic volume of 750 vehicles per hour per lane. It restores Spy Run Creek Parkway continuity and maintains the integrity of the Brookview-Irvington Historic neighborhood.

The costs of the Consulting Parties proposed alternative appear to be of similar magnitude or less than the City's Preferred Alternative based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The long term maintenance costs are comparable or less based on thoroughfare length reduction from 5175 to 2100 lane feet.

Fort Wayne Historic State Boulevard Consulting Parties Proposed Alternative

to the City of Fort Wayne, Indiana, State Boulevard Reconstruction Project
INDOT DES# 0400587

prepared for
ARCH, Inc.
by

Storrow Kinsella Associates + Transportation Solutions, LLC

This investigation has been undertaken on behalf of ARCH, Inc., in response to a current City of Fort Wayne proposal to straighten and widen the portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. This section of State Boulevard is within the Brookview-Irvington Historic District neighborhood. Additionally, State Boulevard and Spy Run Creek Parkway, both components of the Fort Wayne Park and Boulevard System Historic District, intersect within the project area.

The project's impact on historic resources gives standing to the concerns of the Consulting Parties.

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1 Purpose of this investigation

The intention of the investigation is to determine if there is a viable alternative to the City proposal, within the State Boulevard corridor, that reasonably addresses the connectivity and congestion mitigation purposes of that proposal, while avoiding its considerable impacts on the integrity of the overlapping historic districts through which it passes. The investigation does not address the viability of alternative corridors identified as part of the project's Section 106 process other than suggesting that they appear to need further study for their potential contribution to overall network east-west connectivity and congestion mitigation. Likewise the investigation does not address aspects of other State Boulevard project segments that lead to this focus area. It does suggest that lessons learned in this focus area could inform the larger system.

A description of the city's preferred alternative and critiques of its impacts by multiple consulting parties are available in the project's Section 106 documentation thus are not repeated here in the interest of brevity. The critiques include but are not limited to concerns regarding induced traffic, inappropriate scale, and disruption to the character and continuity of historic resources in the project area.

2 Description of the Consulting Parties Proposed Alternative (CPPA: See Exhibit B)

The CPPA diverts east-west crosstown traffic through the district of concern as a new 1000-foot+/- long *two-lane parkway* alignment, generally south of Spy Run Creek, a natural divide. It is intended to improve crosstown connectivity and relieve congestion in a manner that does not induce additional traffic volume and with scale and geometrics that respect the intrinsic qualities of both the Brookview-Irvington Historic District and the Historic Park and Boulevard System.

The transition to this parkway from existing State Boulevard occurs at Clinton Street on the east and at the Westbrook/Edgehill intersection on the west. That transition is enabled by a traffic calming single-lane roundabout at Westbrook/Edgehill, and by a two-lane signalized hybrid roundabout at the higher volume North Clinton Street intersection. The two roundabouts bookend a new terrain, uninterrupted two-lane parkway linkage as a system that modifies motorist behavior to a slower but steady-state stream between the roundabouts. This configuration allows less space-consuming geometrics (vertical and horizontal alignment and clear zone constraints) and much fewer vehicular conflict points, while accommodating expected volumes through operational efficiencies achieved by those reductions.

The CPPA parkway alignment replicates the scale and curvilinearity of the existing historic boulevard, while allowing the latter to revert to a low speed/low volume pedestrian-friendly local street and bicycle boulevard. Thus both the historic and the proposed new segment respect the characteristics of the Historic Park and Boulevard System, *and* the Brookview-Irvington Historic District neighborhoods, by their contextual scale and alignment.

A consideration for detailed development of this parkway alignment and its new crossing of Spy Run Creek, both of which will be raised above flood elevation (a project purpose), is that these elements be sensitively designed such that they integrate well with both the creek and with the Edgehill Avenue neighborhood. The narrow roadway cross-section will help facilitate that spatial integration by lessening the need for obtrusive retaining walls. For that reason a multi-purpose path is proposed to be separated from the roadway (other than at the bridge) as part of the existing pathway system, rather than as sidewalks adjacent to the roadway.

The provision of functionally interdependent roundabouts at east and west ends of the approximately 1000-foot distance of the proposed New State Boulevard alignment facilitates use of a two-lane configuration for this segment. The linked roundabouts will modulate traffic flow through this lower speed (but uninterrupted) segment such that the less-than-750 peak hour vehicles per lane per hour, as recorded in 2005, can be accommodated. A hybrid two-lane

roundabout is proposed to replace the Clinton Street/State Boulevard signalized intersection, while a single lane roundabout would occur at the Westbrook/Edgehill intersection with State Boulevard.

3 Clinton Street roundabout capacity

Clinton Street roundabout capacity is proposed to be maximized by several methods to allow the target traffic throughput discussed above:

A

A fairly new roundabout traffic management method, installation of metering signals at roundabout entries, creates gaps in dominant peak period flow to minimize excessive queues and delays at each successive downstream entry. Such roundabout signalization can be more effective than additional roundabout lanes, and can reduce the complexity associated with three-lane roundabouts. The signals would be controlled by queue detectors.¹

B

A left-turn by-pass lane is proposed from southbound Clinton to eastbound State Boulevard to reduce roundabout circulating traffic by an estimated 250 vehicles per hour, based on 2005 traffic volumes. It would be controlled by the queue detector system as well, and could further reduce the need for a third circulating lane.

C

Additional reduction of the Clinton-State roundabout circulating traffic can be affected, if necessary, by providing a westbound bypass (slip) lane from Historic State Boulevard to the new parkway segment, just west of its splitter island, and optionally from the new parkway's eastbound lane to southbound Clinton, immediately south of the roundabout. The contribution of either or both bypass lanes to roundabout efficiency should be determined during detailed roundabout design and weighed against the possible need for additional right-of-way to accommodate them.

4 Pedestrian accommodation

Pedestrian accommodation is shown through the roundabout for probable pedestrian routes and to provide connections to the Pufferbelly Trail system. *Accessibility Guidelines for Pedestrian Facilities in the Public Right of Way* (PROWAG) recommends signalization for pedestrian crosswalks at high-vehicular volume roundabouts, and requires them for crossings of two or more contiguous roundabout lanes. The required signalization can be integrated into the phasing of demand-cycles of the vehicular signal system discussed above for the Clinton Street roundabout to minimize disruption to vehicular flow while still accommodating pedestrian connectivity.

5 Bicycle accommodation

Bicycle travel through this district can be accommodated along Historic State Boulevard which, once converted to local traffic as proposed here, will be well-suited to become a bicycle boulevard. As a local street, all-way stops can be introduced along that segment for additional traffic-calming for bicycle and pedestrian safety. In the interest of a narrow roadway, multi-use paths at a separate and lower elevation alignment would replace sidewalks along the proposed, new two-lane parkway section.

¹ National Cooperative Highway Research Program Report 672 Roundabouts: an Informational Guide
Chapter 7/7.5.1, Signalization/Metering

Should flooding or structural issues dictate removal of the State Boulevard Bridge, a proposed bicycle-pedestrian bridge in its location will provide additional neighborhood connectivity to the Pufferbelly Trail.

Pedestrian accommodations at the roundabouts should be configured to accommodate bicycles for those cyclists not comfortable riding with traffic through the roundabouts.

Additionally the existing trails along Spy Run Creek should be fully integrated with the proposed Pufferbelly Trail (see Exhibit B) to fulfill this project's multimodal objectives.

6 Transit accommodation

Citilink Route #8 serves this area along southbound North Clinton Street, paired with northbound Spy Run Avenue 800 feet to the east. The current North Clinton stop is in a travel lane immediately south of State Boulevard. The Consulting Parties recommend that a bus turnout be provided either south of the roundabout, or more preferably to north of the roundabout between building setback line and existing curb line, to minimize travel lane disruption, but requiring additional permanent right-of-way.

Citilink Route #6 uses east and westbound State Boulevard and north and southbound Westbrook Drive, and is potentially improved by the proposed roundabout at State/Westbrook. Paired in-lane bus stops are recommended on Westbrook just north of the roundabout to better serve this area.

7 Urban design considerations

The roundabout elements, if sensitively designed, can become gateway markers along the Clinton Street procession towards the city center, as well as become markers for this historic district along the park and boulevard system. The parkway section itself can become a beautiful passage through the convergence of the historic parkway and neighborhood, somewhat mending a route that has been compromised over many years of roadway expansion and ad hoc development prior to its historic designation. This is particularly important to a well-developed Section 4F argument that this intervention results in a net benefit to the historic resources it affects (or as in this case, celebrates). Leveraging the project to enhance Spy Run Creek Parkway as a public park, and reduction of existing traffic impacts to the historic neighborhood are compelling benefits that the original City Preferred Alternate could not claim but which the CPPA can...if executed well.

Another consideration is that Spy Run Creek Parkway was compromised several years ago when Westbrook Drive, a classic *City Beautiful* parkway along residential properties on one side and the meandering creek and variable open space of the park on the other, was terminated at Edgehill Avenue just south of State Boulevard, where it now enters a neighborhood street. The CPPA alignment along the south side of the creek restores much of the historic parkway's integrity by taking it to a more contextual terminus.

8 Floodway/Floodplain considerations

Floodway impacts appear lessened by the proposed alternative because of the reduced width of the two-lane roadway in addition to the provision of a comparable bridge opening along the new alignment. Spy Run Creek flood hydrology will require careful analysis and design such that this project lessens flood severity through removal of current impediments and through development of storage capacity potential of the open space surrounding the creek. That potential can be enlarged by investigation during the project's detailed design.

9 Detailed development of the CPPA

Assuming agreement can be reached regarding this proposed alternative, continuing oversight regarding its detailed development and implementation should be integrated into the project development process. It is extremely important to the Consulting Parties that execution of roadway elements and their urban design setting be context sensitive in scale, materials and detail such that the vision of George Kessler and Arthur Shurcliff, for the Park and Boulevard System and for the Brookview-Irvington District, respectively, be honored and can become a model for how the city balances its infrastructure needs with its heritage. With this caveat, the Consulting Parties will support the city's effort to improve this section of State Boulevard.

10 Cost discussion: comparative magnitude of cost

Comparison of costs between the City Preferred Alternative (City) and the Consulting Parties Proposed Alternative (CPPA) are of relative magnitude based on predictable differentials of project scope. Detailed cost analysis is dependent on more detailed development of the proposed alternative design, as well as on a better understanding of the cost basis for the City alternative.

	CPPA	City	Comments
Roadway	2100 lane feet	5175 lane feet, landscaped center median	CPPA option may cost about half of City's alternative
Bridges	New two-lane/140-150' long vehicular bridge New 16' wide x 100' long bike-ped bridge Remove existing two-lane vehicular bridge or retrofit as local traffic and bike boulevard link	5-lane 150' long new vehicular bridge Remove existing two-lane bridge	CPPA bridge costs reduced significantly because of reduced cross section
Major intersections	Two-lane hybrid roundabout at North Clinton Street	One signalized intersection with additional turn lanes at North Clinton Street	Multi-lane hybrid roundabout will cost significantly more than improved conventional signalized intersection
Secondary intersections	One single lane roundabout (minimal secondary neighborhood road improvements)	Two five-lane intersections (including left turn lanes) and substantial reconfiguration of neighborhood streets	Simplified interface with neighborhood streets anticipated to result in a net cost reduction for these elements
Residential acquisition	0	15 (at \$55-75,000 average assessed valuation)	Residential acquisition much less (approaching zero)
Commercial acquisition	1 total, with partial impacts on 2 additional parcels	0	Commercial acquisition much more Combined residential and commercial acquisition-relocation expected to be similar

11 Summary of the Findings

1

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of this new facility.

2

The costs for the CPPA appear to be of similar magnitude or less than the City Preferred Alternative, based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The latter may be achieved through the offset of reduced residential relocation scope compensating for the higher individual valuation of commercial properties in general.

3

The long term maintenance costs are comparable or less for the CPPA, based on thoroughfare length reduction from 5175 to 2100 lane feet.

4

The CPPA provides greater safety through elimination of most intersection left turn conflicts, pedestrian/vehicle conflicts, and through the lower speeds associated with roundabouts, their approaches and linkages. Reduction in accident rates and their severity for roundabouts vs. conventional signalized intersections has been well documented by multiple FHWA and insurance industry studies utilizing data accumulated during the high rate of adoption of roundabouts by state and local agencies because of their safety and efficiency characteristics.

5

The CPPA minimizes negative impacts on historic properties and districts. The removal of existing traffic volume impacts is expected to result in neighborhood stabilization and reinvestment in the project area.

6

The CPPA provides an additional benefit to the historic districts by restoring the sense of Spy Run Creek/Westbrook Drive's parkway continuity, which had been compromised by earlier floodway-roadway improvements that terminated Westbrook Drive at Edgehill Avenue.

7

The CPPA meets the stated goals of the thoroughfare plan and reconciles that plan with the National Register-listed Park and Boulevard System.

8

The CPPA has the support of the affected neighborhoods and remonstrating parties which will facilitate the project moving forward expeditiously.



Indiana Department of Natural Resources

Michael R. Pence, Governor
Cameron F. Clark, Director

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
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December 29, 2014

Patrick Carpenter
Manager, Cultural Resources Office
Environmental Services Division
Indiana Department of Transportation
100 North Senate Avenue, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: Signature request for Memorandum of Agreement (MOA) regarding State Boulevard Reconstruction from Spy
Run to Cass Street in Fort Wayne, Wayne Township, Allen County Indiana
(Designation #0400587; Project No. IN200701404; DHPA #5903)

Dear Mr. Carpenter:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated December 22, 2014 and received on December 29, 2014, for the above indicated project Fort Wayne, Wayne Township, Allen County, Indiana.

Deputy State Historic Preservation Officer ("Indiana SHPO") Mitchell K. Zoll, on behalf of Cameron F. Clark, has signed the enclosed MOA, indicating his approval of the agreement.

We are returning the MOA to you, so that it may be transmitted with the accompanying documentation specified in 36 C.F.R. § 800.11(f) to the Advisory Council on Historic Preservation (36 C.F.R. § 800.6[b][iv]). The MOA may be transmitted to:

Reid J. Nelson, Director
Office of Federal Agency Programs
Advisory Council on Historic Preservation
Pension Building
401 F Street NW, Suite 308
Washington DC 20001-2637

If you have questions about our comments, please contact John Carr of our office at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA 5903.

Very truly yours,


Mitchell K. Zoll
Deputy State Historic Preservation Officer

MKZ:CWS:mkz

Enclosures (1)

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E., Indiana District, Federal Highway Administration
Patrick A. Carpenter, INDOT
Mary Kennedy, INDOT
Shaun Miller, INDOT
Melany Prather, INDOT
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

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MEMORANDUM OF AGREEMENT
BETWEEN THE FEDERAL HIGHWAY ADMINISTRATION AND
THE INDIANA STATE HISTORIC PRESERVATION OFFICER
SUBMITTED TO THE ADVISORY COUNCIL ON HISTORIC PRESERVATION
PURSUANT TO 36 C.F.R. Section 800.6(b)(iv)
REGARDING THE STATE BOULEVARD RECONSTRUCTION FROM SPY RUN TO CASS
STREET
IN FORT WAYNE, WAYNE TOWNSHIP, ALLEN COUNTY, INDIANA

WHEREAS the Federal Highway Administration ("FHWA") proposes to widen and realign a portion of State Boulevard for the State Boulevard Reconstruction from Spy Run to Cass Street (Des No.: 0400587) in, Fort Wayne, Wayne Township, Allen County, Indiana ("Project"); and

WHEREAS the FHWA, in consultation with the Indiana State Historic Preservation Officer ("SHPO"), has defined this State Boulevard Reconstruction from Spy Run to Cass Street's area of potential effects, as the term defined in 36 C.F.R. Section 800.16(d), to be the area extending 250 feet from the alley west of Cass Street to the abandoned New York Central Railroad, encompassing the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has found that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and the State Boulevard Bridge over Spy Run are within the area of potential effects; and

WHEREAS the FHWA and the Indiana SHPO both recognize that the Fort Wayne Park and Boulevard System and Brookview-Irvington Park Historic District are listed in the National Register of Historic Places; and

WHEREAS the FHWA and the Indiana SHPO both recognize that the State Boulevard Bridge over Spy Run has been determined eligible for listing in the National Register of Historic Places per the Indiana Statewide Historic Bridge Inventory; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has determined pursuant to 36 C.F.R. Section 800.5(a) that the State Boulevard Reconstruction from Spy Run to Cass Street will have an adverse effect on the Fort Wayne Park and Boulevard System, Brookview-Irvington Historic District, and the Bridge over Spy Run; and

WHEREAS the FHWA, Indiana Department of Transportation ("INDOT"), the Indiana SHPO, and the Advisory Council on Historic Preservation ("Council") have executed in 2006 a Programmatic Agreement on the Management and Preservation of Indiana's Historic Bridges and the FHWA has determined that the State Boulevard Bridge over Spy Run is "Non-Select" (not considered an excellent example of a given type or not suitable candidate for preservation); and

WHEREAS the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Section 800) to resolve the adverse effect on the Fort Wayne Park and Boulevard System and Brookview-Irvington Historic District; and

WHEREAS the FHWA has consulted with the Indiana SHPO and both recognize that 2244 Eastbrook Drive is a non-contributing resource within the Brookview-Irvington Historic District and will be demolished as part of the project; and

WHEREAS the FHWA has consulted with the Indiana SHPO and both recognize that following properties are contributing resources within the Brookview-Irvington Historic District and will be demolished as part of this undertaking: 2221 Westbrook Drive; 112 East State Boulevard; 128 East State Boulevard; 134 East State Boulevard; 138 East State Boulevard; 142 East State Boulevard; 146 East State Boulevard; 154 East State Blvd; 158 East State Boulevard; 162 East State Boulevard; 2252 Eastbrook Drive; 2248 Eastbrook Drive; 2240 Eastbrook Drive; 2236 Eastbrook Drive; and the State Boulevard Bridge over Spy Run; and

WHEREAS the public was given an opportunity to comment on the undertaking's adverse effect in a notice published on Wednesday June 4, 2014 and June 11, 2014 in *The News-Sentinel* and *The Journal Gazette*; and

WHEREAS the FHWA has notified the Council of the adverse effect and invited the Council's participation in the project, pursuant to 36 CFR Section 800.6(a)(1), in a letter dated August 29, 2011 and in additional material conveyed on May 10, 2012; and

WHEREAS the Council declined to participate in consultation in a letter dated July 31, 2012; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has invited the INDOT and the City of Fort Wayne to participate in the consultation and to become a signatory/signatories to this memorandum of agreement; and

WHEREAS the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Part 800) concerning the scope of work on March 23, 2009; July 2, 2009; November 9, 2009; December 1, 2009; December 15, 2009; December 28, 2009; February 4, 2010; May 19, 2011; June 17, 2011; July 6, 2011; July 13, 2011; August 15, 2011; August 16, 2011; September 1, 2011; September 2, 2011; September 29, 2011; May 22, 2012; June 20, 2012; July 2, 2012; July 16, 2012, and December 18, 2012; and agreed to proceed with the project as proposed April 23, 2009; December 14, 2009; July 5, 2011; November 7, 2011; June 22, 2012; and August 13, 2012; and

NOW, THEREFORE, the FHWA and the Indiana SHPO agree that, upon the submission of a copy of this executed memorandum of agreement, as well as the documentation specified in 36 C.F.R. Section 800.11(e) and (f) to the Council pursuant to 36 C.F.R. Section 800.6[b][1][iv]) and upon the FHWA's approval of the State Boulevard Reconstruction from Spy Run to Cass Street, the FHWA shall ensure that the following stipulations are implemented in order to take into account the effect of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

Stipulations

FHWA will ensure that the following measures are implemented:

I. CONTEXT SENSITIVE SOLUTIONS

A. The City of Fort Wayne shall, where feasible, implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.

B. The City of Fort Wayne shall, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences. The City of Fort Wayne shall provide the Indiana SHPO and consulting parties a dispensation plan for salvaged architectural details

C. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.

D. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:

1. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
2. Context sensitive solutions that may include but not be limited to: protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; and maintaining pedestrian connections along the former Eastbrook and Westbrook drives shall be included among the measures considered.
3. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
4. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), INDOT, the Fort Wayne

Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.

5. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail or US 27) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.

6. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

7. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before scheduled meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.

8. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

9. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.

II. PHOTOGRAPHIC DOCUMENTATION

- A. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."
- B. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
 1. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 2. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
- C. This documentation will include black and white prints of digital photographs and a digital video disc ("DVD") containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" that are in effect at the time.
 1. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 2. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
 3. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
 4. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.

III. EDUCATIONAL PROGRAMMING

The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler's landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps or other WPA era programs in public projects.

The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within thirty (30) days, acceptance will be assumed. If the Indiana SHPO responds with recommendations, a

good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

IV. OBJECTION RESOLUTION PROVISION

Disagreement and misunderstanding about how this memorandum of agreement is or is not being implemented shall be resolved in the following manner:

A. If the Indiana SHPO or any invited signatory to this memorandum of agreement should object in writing to the FHWA regarding any action carried out or proposed with respect to the State Boulevard Reconstruction from Spy Run to Cass Street or implementation of this memorandum of agreement, then the FHWA shall consult with the objecting party to resolve this objection. If after such consultation the FHWA determines that the objection cannot be resolved through consultation, then the FHWA shall forward all documentation relevant to the objection to the Council, including the FHWA's proposed response to the objection. Within 45 days after receipt of all pertinent documentation, the Council shall exercise one of the following options:

1. Provide the FHWA with a staff-level recommendation, which the FHWA shall take into account in reaching a final decision regarding its response to the objection; or
2. Notify the FHWA that the objection will be referred for formal comment pursuant to 36 C.F.R. Section 800.7(c), and proceed to refer the objection and comment. The FHWA shall take into account the Council's comments in reaching a final decision regarding its response to the objection.

B. If comments or recommendations from the Council are provided in accordance with this stipulation, then the FHWA shall take into account any Council comment or recommendations provided in accordance with this stipulation with reference only to the subject of the objection. The FHWA's responsibility to carry out all actions under the memorandum of agreement that are not the subjects of the objection shall remain unchanged.

V. POST REVIEW DISCOVERY

In the event that archaeological artifacts (sites), human remains, or one or more historic aboveground properties—other than Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run—are discovered or that unanticipated effects on historic properties are found during the implementation of this memorandum of agreement, the FHWA shall follow the procedure specified in 36 C.F.R. Section 800.13, as well as and IC 14-21-1-27 and IC 14-21-1-29, by stopping work in the immediate area and informing the Indiana SHPO and the INDOT Cultural Resources Section of such unanticipated discoveries or effects within two (2) business days. Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1 and 312 IAC 21, and the most current Guidebook for Indiana Historic Sites and Structures Inventory – Archaeological Sites.

IV. AMENDMENT

Any signatory to this memorandum of agreement may request that it be amended, whereupon the parties shall consult to consider the proposed amendment. 36 C.F.R. 800.6(c)(7) shall govern the execution of any such amendment.

V. TERMINATION

A. If the terms of this memorandum of agreement have not been implemented within five years of the onset of construction, then this memorandum of agreement shall be considered null and void. In such an event, the FHWA shall so notify the parties to this memorandum of agreement and, if it chooses to continue with the State Boulevard Reconstruction from Spy Run to Cass Street, then it shall reinitiate review of the State Boulevard Reconstruction from Spy Run to Cass Street in accordance with 36 C.F.R. Sections 800.3 through 800.7.

B. Any signatory to the memorandum of agreement may terminate it by providing thirty (30) days notice to the other parties, provided that the parties shall consult during the period prior to termination to seek agreement on amendments or other actions that would avoid termination. In the event of termination, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

C. In the event that the FHWA does not carry out the terms of this memorandum of agreement, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

The execution of this memorandum of agreement by the FHWA, the City of Fort Wayne, and the Indiana SHPO, the submission of it to the Council with the appropriate documentation specified in 36 C.F.R. Section 800.11(e) and (f), and the implementation of its terms evidence that the FHWA has afforded the Council an opportunity to comment on the State Boulevard Reconstruction from Spy Run to Cass Street and its effect on historic properties and that the FHWA has taken into account the effects of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

SIGNATORIES (required):

FEDERAL HIGHWAY ADMINISTRATION

Signed by: Joyce E. Newland Date: December 29, 2014

Name and Title: Joyce E. Newland
(Typed or printed) PLANNING & ENVIRONMENTAL SPECIALIST

INDIANA STATE HISTORIC PRESERVATION OFFICER

Signed by: *M. K. Zill* Date: *12/29/2014*

Name and Title: *Mitchell K. Zill*

(Typed or printed) *Deputy Historic Preservation Officer*

INVITED SIGNATORIES

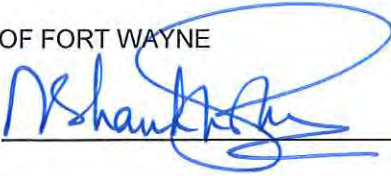
INDIANA DEPARTMENT OF TRANSPORTATION

Signed by:  Date: 12/29/2014

Name and Title: Laura Hilden, Director of Environmental Services.

THE CITY OF FORT WAYNE

Signed by:



Date:

12/29/2014.

Name and Title: SHAN GUNAWARDENA, CITY ENGINEER

(Typed or printed)

(IF an entity has responsibilities under the MOA, include that entity's name here)



U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

January 5, 2015

HDA-IN

Mr. Christopher Wilson
Program Analyst, FHWA Liaison
Advisory Council on Historic Preservation
1100 Pennsylvania Avenue, NW
Washington, DC 20004


Dear Mr. Wilson:

Please find enclosed an executed Memorandum of Agreement (MOA) for the State Boulevard project in the City of Fort Wayne, Allen County. The purpose of the project is to improve corridor connectivity along State Boulevard for motorists and pedestrians. Currently existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians, as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spry Run or the Saint Mary's River.

The 800.11(e) documentation and the signed MOA can be found on the enclosed CD starting on page 47 of the pdf document and concludes on page 565. By forwarding this MOA, FHWA fulfills the requirements of 36 CFR 800.11(f).

If you require further information please contact Joyce Newland of this office by e-mail Joyce.Newland@dot.gov or by phone at (317) 226-5353.

Sincerely,


Richard J. Marquis
Division Administrator

Enclosure

Attachment 4

Final Section 4(f) Documentation – Pages 1-68

**Section 4(f) Evaluation
for
Impacts to Historic Properties**

**State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587**

**City of Fort Wayne
200 E. Berry Street
Fort Wayne, Indiana 46802**

**Indiana Department of Transportation
Office of Environmental Services
642 Government Center North
100 North Senate Avenue
Indianapolis, Indiana 46204**

**Federal Highway Administration
254 Federal Office Building
575 North Pennsylvania Street
Indianapolis, Indiana 46204**

December 29, 2014

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Appendix 3	Capacity Analysis of the Consulting Parties Proposed Alternative (CPPA) for State Boulevard and Clinton Street Intersection

Introduction

Section 4(f) of the US Department of Transportation Act (DOT Act) of 1966 [49 U.S.C. 303 (c)] states the use of any land from a significant publicly owned park or recreation area, wildlife and waterfowl refuge, or private or publically owned historic site on or considered eligible for the National Register of Historic Places (NR) shall not be allowed unless:

- a. There is no feasible and prudent alternative to the use of such land.
- b. The proposed project includes all possible planning to minimize harm to such park, recreational area, wildlife and waterfowl refuge, or historic site resulting from such use.

Pursuant to regulations at 23 CFR Part 774, a full evaluation is required to determine the most feasible federal-aid route that causes the least overall harm considering the following factors:

- a. The ability to mitigate adverse impacts to each Section 4(f) property, including measures that benefit the property
- b. The relative severity of the remaining harm, after mitigation to the protected activities, attributes, or features that qualify each property for Section 4(f) protection
- c. The relative significance of each Section 4(f) property
- d. The views of the official(s) with jurisdiction over each Section 4(f) property
- e. The degree to which each alternative meets the purpose and need of the project
- f. After reasonable mitigation, the magnitude of any adverse impacts to resources not protected by Section 4(f)
- g. Substantial differences in costs among the alternatives

The purpose of this report is to evaluate and summarize the proposed project's purpose and need, reasonable alternatives, the Section 4(f) resources, the 4(f) resources that are used by these alternatives; avoidance alternatives that relate to these 4(f) resources, and all possible planning to minimize harm, if the resources cannot be avoided.

Proposed Action

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

The project extends from Cass Street to the west and Spy Run Avenue to the east, an overall project length of 2,370 feet. The current proposed alternative involves widening the existing 2-lane section of State Boulevard between Cass Street and Clinton Street to four lanes and correcting the substandard horizontal curve. In this segment, State Boulevard would have four 10-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot wide raised median and a 2-way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard roadway geometrics, as well as alleviate roadway flooding at Spy Run Creek. The

horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four 11-foot travel lanes, two in each direction, separated by a 12-foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway alignments. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

Combined concrete curb and gutters, including curb inlets and storm sewer, would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New sidewalks, varying in width from five feet to ten feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five feet wide, would be installed between the back of the curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail which would be constructed by others.

Purpose and Need for the Proposed Action

The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity, and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps

to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project would reduce existing congestion and improve traffic flow. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one westbound lane. East of Clinton Street, State Boulevard is a 2-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E
Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E
Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project would reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway’s safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 feet. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards. For further reference to the IDM see http://www.in.gov/indot/design_manual/design_manual_2013.htm.

Curve Radius Table:

Station Line “A”	Existing Curve Radius	Required Radius (30 mph)
18+66.60	175 feet	300 feet
24+64.47	243 feet	300 feet
27+23.73	210 feet	300 feet

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway’s safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a critical Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 feet along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 feet along State Boulevard to safely make the right turn maneuver. As noted in the “Intersection Sight Distance Table” below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (feet)	Required Sight Distance (feet)
Cass Street (South)	LT	300	420
Cass Street (South)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists for the years 2007 to 2009 and for both 2010 and 2011 at State Boulevard and Clinton Street.

Crash Location	2007				2008				2009				2010				2011			
	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV
State Boulevard and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11	9	1	0	1.26	12	3	0	1.69
State Boulevard and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38	30	3	0	2.04	36	8	0	2.45
State Boulevard And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48	27	7	0	1.63	43	11	0	2.60
State Boulevard and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16	9	1	0	1.26	12	3	0	1.69

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near

the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is five years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Seven out of the 17 years (1978, 1982, 2003, 2004, 2005, 2008, and 2009), State Boulevard was closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

Description of the Section 4(f) Resources

Three historic properties and one park were identified within the limits of the proposed project.

The Westerly Group, Inc. (Westerly) and Weintraut & Associates Historians, Inc. (Weintraut) were contracted by American Structurepoint, Inc. to prepare a Historic Properties and Section 106 Documentation and Findings. Westerly and Weintraut, in conjunction with recommendations and comment from the State Historic Preservation Officer (SHPO) and consulting parties, determined three historic properties listed in or eligible for listing in the National Register of Historic Places (NR) would be affected by the undertaking. The three properties include the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek.

The park identified as being affected by the undertaking includes the greenway portion of Vesey Park running parallel to Spy Run Creek, along both the east and west banks.

Fort Wayne Park and Boulevard System Historic District (NR, 2010): The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of 11 parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NR in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is from 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized."

Brookview-Irvington Park Historic District (NR, 2011): The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total of 424 contributing resources including houses, garages, and the combined plats of the district, as well as the previously determined eligible bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965, representing the construction dates of most buildings within the historic district, and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination.

Bridge over Spy Run Creek (NBI No. 0200273): The bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010). The bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

Vesey Park: Additionally, Vesey Park was noted in the project limits. This park is operated by the City of Fort Wayne Parks Department and includes the green space along Spy Run between Eastbrook Drive and Westbrook Drive. It connects the larger portion of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary's River. The park features open space among the trees with areas for picnicking and views of Spy Run Creek.

With the exception of the structures discussed, no other significant features are on the affected properties. No known covenants or other restrictions or conditions would relate to the acquisition of the necessary right-of-way from any of the properties.

Alternatives

Avoidance Alternative

There are no alternatives that can simultaneously meet the project's purpose and need while also avoiding all Section 4(f) resources. All the reasonable alternatives use 4(f) resources. Given the extensive north-south boundaries of the Brookview-Irvington Park Historic District, and the east-west nature of the transportation corridor need, no other avoidance alternatives, besides the No Build Alternative, were identified that would not result in a use a Section 4(f) resource.

Alternative 4: No Build

With the No Build Alternative, there would be no use of resources subject to Section 4(f) provisions. This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate. The existing roadway would continue to flood causing continued problems with accessibility and pavement deterioration. Traffic accidents would most likely continue to increase as the current congestion issues would not be addressed. The existing bridge over Spy Run Creek is currently rated structurally deficient and the estimated remaining life of the superstructure is five years. This structure is in immediate need of replacement due to the condition. East-west connectivity would continue to be a problem for the overall transportation network. The No Build Alternative would likely result in the complete failure of the structure over Spy Run Creek.

The No Build Alternative would not meet any of the needs of the project; therefore, is not considered a feasible and prudent alternative.

Initial 4(f) Use Alternatives Considered and Screened

Alternative 1: Butler Road – Vance Road Corridor

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of 125 residential relocations and 15 commercial relocations would be required. This alternative would also result in impacts to the Franke Park Elementary School and the Fort Wayne Children's Zoo. Of the approximate 2.25 miles of new roadway alignment required for this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

Alternative 1 results in the use of the Brookview-Irvington Historic District (northern extents), Vesey Park, and Franke Park, all 4(f) resources.

This alternative is not reasonable as it does not address any of the project's purpose and need. Alternative 1 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential and commercial relocations for construction and approximately 2.0 miles of new roadway through existing forested land. For these reasons, Alternative 1 has been eliminated from further consideration.

Alternative 2: Spring Street – Tennessee Avenue

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

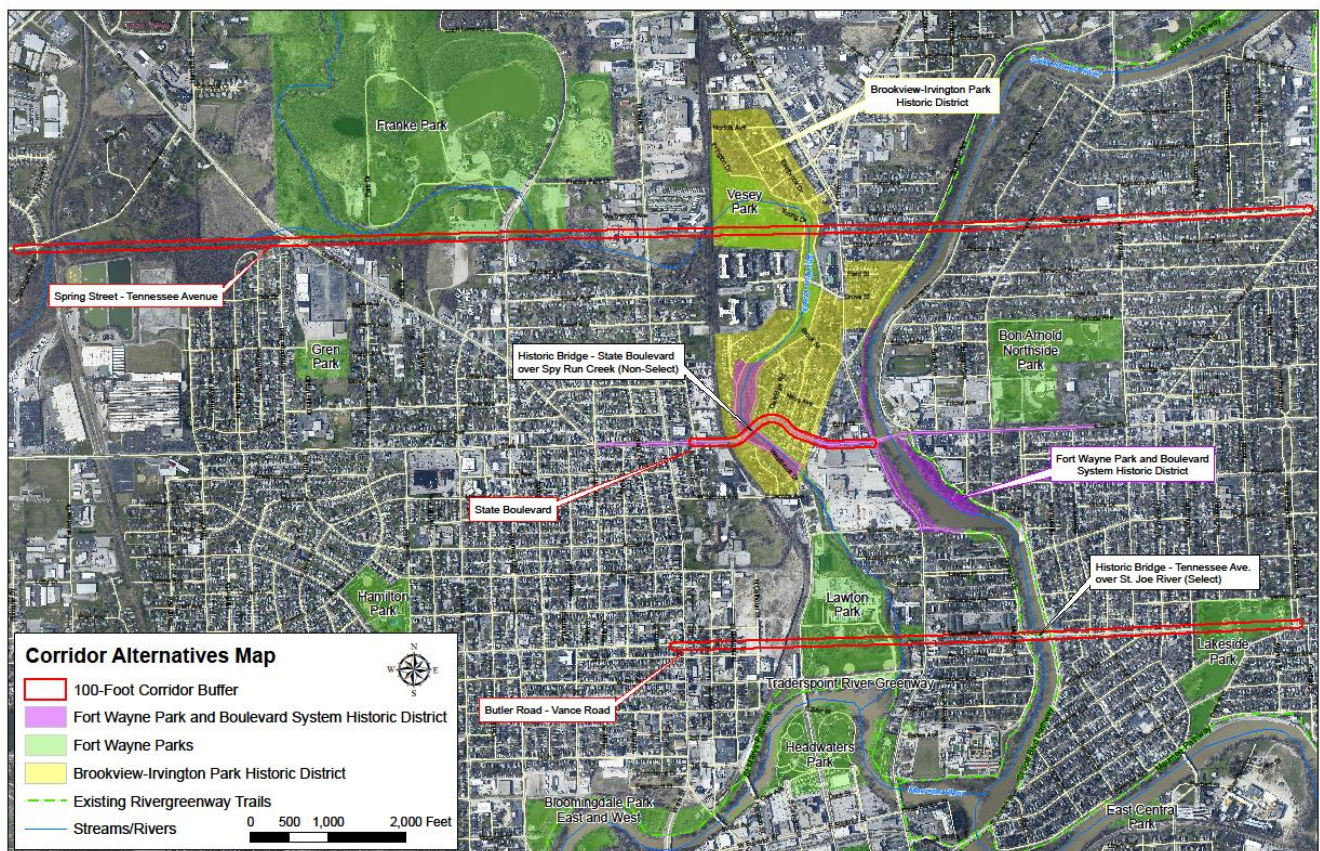
This alternative would require approximately 0.60 mile of new roadway alignment in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of 75 residential relocations and 15 commercial relocations would be required. This alternative would also result in impacts or relocations to the Science Central Museum, Lakeside Park, and Lawton Park.

This alternative would result in the use of 4(f) resources including Lakeside Park, Lawton Park, and the NRHP eligible bridge over the St. Joseph River.

This alternative is not reasonable as it does not address any part of the project's purpose and need. Alternative 2 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential, commercial, and recreational property impacts/relocations for construction. For these reasons, Alternative 2 has been eliminated from further consideration.

Alternatives 1 and 2



Alternative 3E: Consulting Parties Proposed Alternative (CPPA)

The CPPA, as presented by Storrow Kinsella Associates in collaboration with Transportation Solutions, LLC consists of a two-lane parkway alignment shifted south of existing State Boulevard between Clinton Street and the Westbrook/Edgehill Drive intersection. The transition from existing State Boulevard to the CPPA includes a single lane roundabout at the Westbrook/Edgehill Drive intersection and a two-lane signalized hybrid roundabout at the North Clinton Street intersection. The CPPA includes a new crossing of Spy Run Creek raised above the 100-year flood elevation and a multi-use path separated from the roadway. The multi-use path would utilize the existing Spy Run Creek bridge. If the deteriorated bridge condition or flooding issues dictate removal, a new multi-use path bridge would be constructed. Eastbrook Drive would be converted to a cul-de-sac just north of the realigned State Boulevard. Access to existing State Boulevard would be obtained by utilizing the

proposed roundabout at Clinton Street. No direct access to the realigned State Boulevard would be provided at Eastbrook Drive, Oakridge Road, or Terrace Road.

This alternative would require the relocation of at least two businesses and one residential property for construction of the Clinton Street roundabout. The CPPA is estimated to cost \$9.6 million. The cost of the CPPA is elevated due to the increased construction cost associated with the larger footprint and increased infrastructure associated with the two proposed roundabouts, the addition of a second pedestrian bridge, the potential for mechanically stabilized earth (MSE) retaining walls needed to keep fill slopes from extending into Spy Run Creek and also associated with the realigned State Boulevard near the proposed Eastbrook Drive cul-de-sac.

The CPPA results in the use of the Brookview-Irvington Historic District, Fort Wayne Park and Boulevard System Historic District, the Bridge over Spy Run Creek, and Vesey Park, all 4(f) resources.

The CPPA is not reasonable as it does not satisfy the Project's purpose and need. Based on a capacity analysis prepared for the CPPA, this alternative would not address the traffic congestion issues established by the Project's primary purpose and need. The intersections of State Boulevard with Spy Run and Clinton Street would not function at an acceptable level of service in the design year. For the CPPA, the overall intersection LOS is E or F during either the AM or PM peak hours in all scenarios analyzed. The CPPA would also likely require a level one design exception* with regards to roadway geometrics as it appears the CPPA utilizes substandard curvature in the proposed relocated segment of State Boulevard resulting in substandard sight distance conditions. Therefore, the CPPA does not appear to address the safety components associated with the sight distance, geometrics, and congestion. However, while not as significant as the need to address congestion and the safety components associated with sight distance, geometrics, and congestion, the CPPA does address the flooding and Greenways Trail System connectivity components of the purpose and need by proposing to elevate the roadway above the 100-year elevation and provide a separated multi-use path. Furthermore, this alternative would require an estimated \$9.6 million project cost, approximately \$1.6 million (20% increase) more than the preferred alternative (3A) presented in the May 14, 2014, approved EA. For these reasons, the CPPA is not considered reasonable and has been eliminated from further consideration.

*A design exception is a request for an exception to specific design criteria, required when an element of a proposed design does not meet the standard design criteria as set forth in the Indiana Design Manual. A design exception is submitted to and approved by INDOT. Level one design exceptions are those exceptions related to highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability.

Alternative 3E CPPA



4(f) Use Alternatives Retained for Further Consideration

Alternative 3A: Substandard Horizontal Curve Correction with 4-Lane Typical Section

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes and correcting the substandard horizontal curve. State Boulevard would have four 10-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot wide raised median and a 2-way left turn lane. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard roadway geometrics, as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four 11-foot travel lanes, two in each direction, separated by a 12-foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway alignments. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs. New sidewalks, varying in width from five feet to ten feet would be constructed on both sides of the roadway. As a part of this alternative, a new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

Alternative 3A would result in the use of The Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the bridge over Spy Run Creek (NBI No. 0200273). The

Federal Highway Administration (FHWA) has determined the Section 106 finding of “Adverse Effect” is appropriate for the properties listed. The following summarizes anticipated use of Section 4(f) properties by the proposed project.

Fort Wayne Park and Boulevard System Historic District - The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard as the project would acquire all residential properties located along this portion of the roadway rendering the street unnecessary. Eastbrook Drive would be converted to a cul-de-sac north of State Boulevard, eliminating the existing Eastbrook Drive and State Boulevard intersection. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property, as the existing bridge does not provide a sufficient waterway opening and is in poor condition. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly Trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District. FHWA has determined the appropriate Section 106 finding is “Adverse Effect” and there is a Section 4(f) use.

As mitigation for the impacts to the district, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the present State Boulevard bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements.

Brookview-Irvington Park Historic District - The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). One residential property was identified as individually eligible along the State Boulevard corridor; however, no portions of this property would be converted to a transportation use. The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard. FHWA has determined the appropriate Section 106 finding is “Adverse Effect” and there is a Section 4(f) use.

As mitigation for the impacts to the district, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the

present State Boulevard Bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements. In addition, the City of Fort Wayne would make an effort to salvage architectural details from homes demolished for use in other District residences, as well as explore funding opportunities to provide low cost grants/loans to improve/rehabilitate historic resources within the Brookview-Irvington Historic District.

The bridge over Spy Run Creek (NBI No. 0200273) – The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. The removal or demolition would be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The pending removal or demolition of the bridge is considered an adverse effect.

This alternative addresses the project’s purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Based on a capacity analysis prepared for Alternative 3A (Appendix 3), the intersections of State Boulevard with Spy Run and Clinton Street would function at an acceptable level of service (LOS D or better) in the design year. Alternative 3A also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. Alternative 3A fully satisfies the projects purpose and need.

Table 1 identifies Section 4(f) resources, their location, and use by the proposed construction (Alternative 3A) on each of the resources.

Table 1: Summary of Section 4(f) Resources and Anticipated Use (Alternative 3A)				
Section 4(f) Resource	Location	Right-of-Way to be Acquired	Structures to be Removed	Section 4(f) Use
Fort Wayne Park and Boulevard System Historic District (NR 2010)	Includes Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and bridge over Spy Run Creek (NBI No. 0200273)	0.60 acre permanent	State Boulevard, Eastbrook Drive, bridge over Spy Run Creek	Permanent right-of-way acquisition and removal of contributing resources from historic location
Brookview-Irvington Park Historic District (NR 2011)	Bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street and Jacobs Avenue	2.6 acre permanent	15 contributing residential structures (not individually NRHP eligible), bridge over Spy Run Creek (non-select)	Permanent right-of-way acquisition and removal of contributing resources from historic location
Bridge over Spy Run Creek (NBI. 0200273)	State Boulevard at Spy Run Creek	None	bridge over Spy Run Creek (non-select)	Programmatic Section 4(f) for Historic Bridges ¹
Vesey Park	Along both east and west banks of Spy Run Creek between Westbrook Drive and Eastbrook Drive	0.55 acre permanent, 0.12 acre temporary	None	<i>De minimis</i> ¹

1. *Programmatic Section 4(f) Evaluation for Historic Bridges and De minimis Section 4(f) evaluation will be completed as part of the Environmental Assessment. Further discussion of this Section 4(f) use will not be included in this document*

Alternative 3A



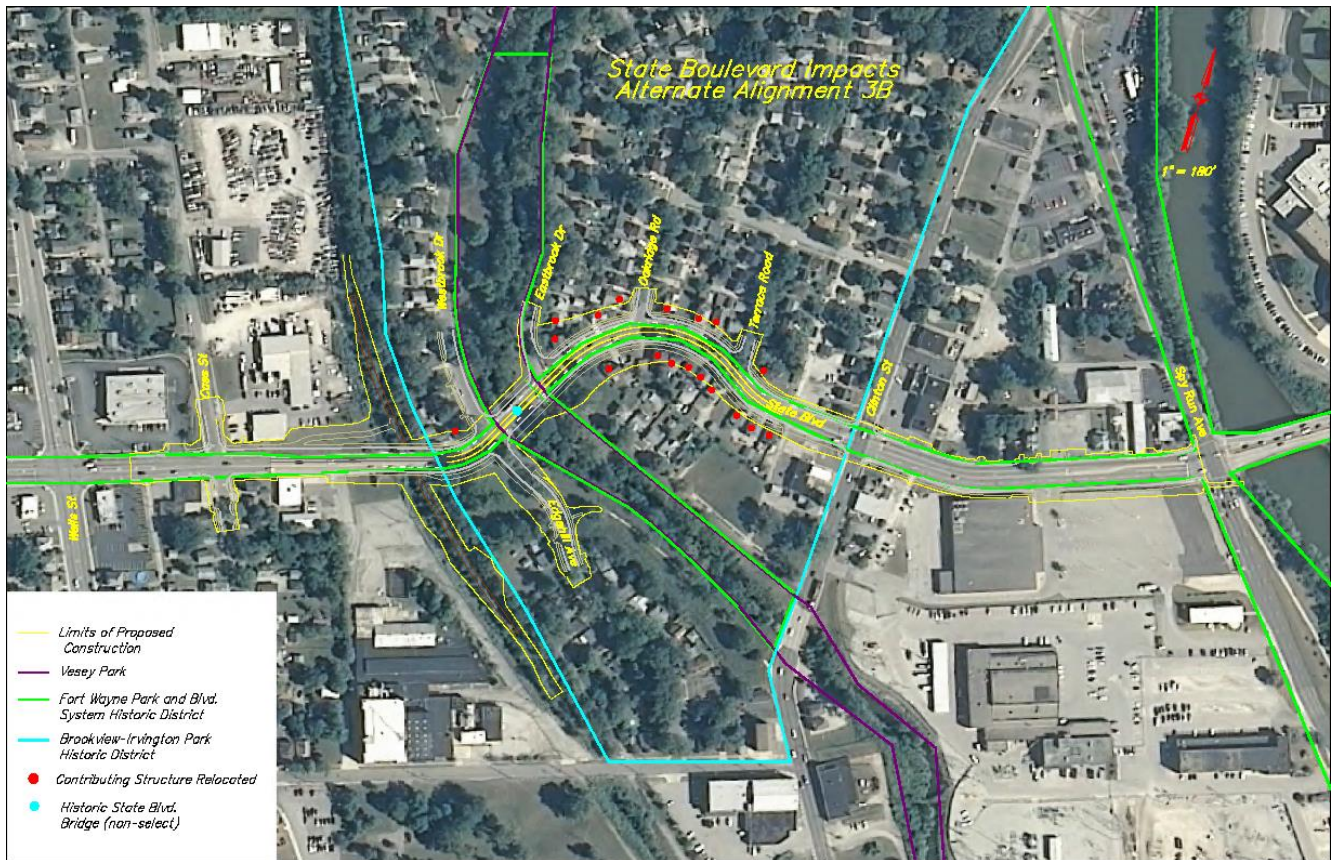
Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge over Spy Run Creek at an elevation 7 feet above the existing bridge elevation. The overall alternative length is 2,700 feet.

This alternative would require approximately 18 residential relocations (contributing properties) from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard along on the existing alignment.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. Based on a capacity analysis prepared for Alternative 3B (Appendix 3), the intersections of State Boulevard with Spy Run and Clinton Street would function at an acceptable level of service (LOS D or better) in the design year. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. Furthermore, this alternative requires a higher number of residential and historic property relocations for construction as compared to other alternatives.

Alternative 3B



Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and constructing the new alignment for 4-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street and “mirror” or “flip” the alignment to the south. The roadway would be designed to meet current roadway geometric standards. The existing intersection of State Boulevard with Eastbrook Drive would be eliminated and converted to a cul-de-sac. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Terrace Road and State Boulevard. The Terrace Road extension would be required to provide access to the neighborhood north of existing State Boulevard as a result of access restrictions due to Clinton Street being a one-way south roadway. This alternative would also require a new bridge over Spy Run Creek at an elevation seven feet above the existing bridge elevation.

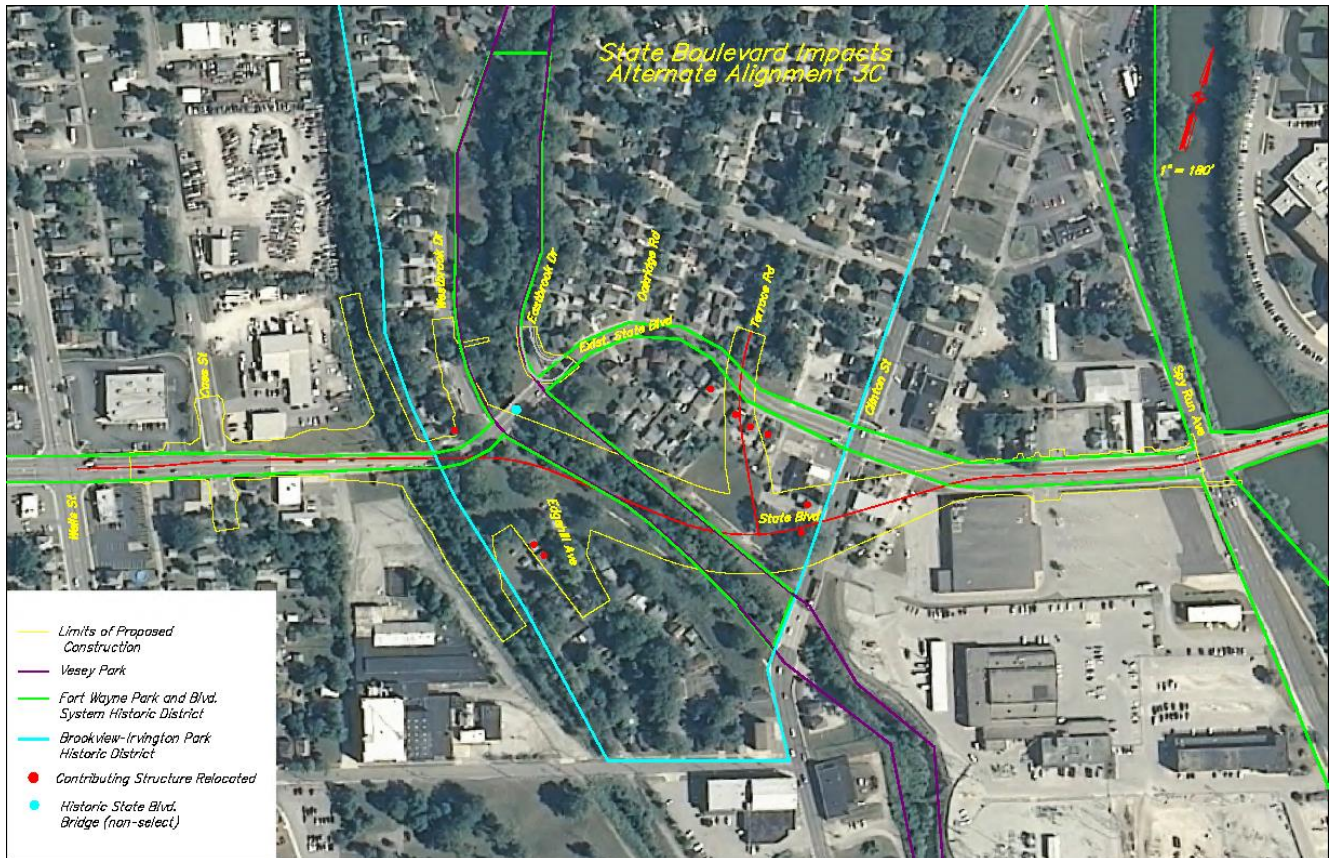
Similar to Alternative 3A, the realignment of State Boulevard and change in elevation would result in the bifurcation of the Brookview-Irvington Park Historic District. Contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the existing bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of Eastbrook Drive (north of State Boulevard) to a cul-de-sac, the replacement of the bridge over Spy Run Creek, and the removal of five contributing properties, the landscape of the area would be modified altering the character and setting of the district. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard. Furthermore, the realignment of State Boulevard would require the acquisition of right-of-way from the Fort Wayne Park and Boulevard System Historic District, again altering the historic location of State Boulevard. The realigned State Boulevard profile would have a significant increase in vertical elevation

(approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). The prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly Trail) would be constructed over the contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing new visual element to the Fort Wayne Park and Boulevard System Historic District.

While this alternative would reduce the number of contributing property relocations on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Boulevard would cross Spy Run Creek; impacts to the creek would be increased by approximately 330 linear feet for the purposes of re-grading. The new bridge length would be approximately 250 feet longer than the bridge design included in Alternatives 3A or 3D. This alternative would also require construction of a new intersection of State Boulevard with Clinton Street. The new intersection would be built in close proximity to the new Terrace Road intersection which would significantly impede traffic operations and efficiency as well as increase project costs due to additional traffic signal work. The increased length of the proposed bridge combined with relocating the roadway south would also require the intersection of State Boulevard and Clinton Street to be raised two to three feet, thus causing additional reconstruction along Clinton Street (approximately 500 feet) and further increasing project costs. In addition to the nine residential relocations that are also considered contributing resources, this alternative would result in the relocation of four commercial businesses, including the gas station at the southwest corner of Clinton Street and State Boulevard, a plumbing business on the southeast corner, a dog grooming business located just south of the gas station, and a storage unit business located on the southwest corner of Spy Run Avenue and State Boulevard.

Alternative 3C addresses the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. Based on a capacity analysis prepared for Alternative 3C (Appendix 3), the intersections of State Boulevard with Spy Run and Clinton Street would function at an acceptable level of service (LOS D or better) in the design year. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street which would create new operational and safety issues due to its close proximity to the new Terrace Road intersection. Project costs associated with Alternative 3C are an estimated \$3.9 million dollars more than any other alternative due to increased impacts to commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street.

Alternative 3C



Alternative 3D: Substandard Horizontal Curve Correction with a 3-Lane Typical Section

This alternative is similar to Alternative 3A but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes and correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have two ten foot travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes would be separated by a twelve-foot wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes would be separated by a twelve foot two way left turn lane. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four eleven foot travel lanes, two in each direction, separated by a twelve foot two way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway. As a part of this project, the new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

By reducing the typical section from 4-lanes (Alternative 3A) to 3-lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this alternative would continue to result in the same 4(f) use as Alternative 3A to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run Creek.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3D does not

fully address corridor connectivity or traffic congestion concerns along the corridor. This alternative would not address the congestion concerns at the intersection of State Boulevard and Clinton Street. This intersection currently functions at a low Level of Service. Based on a capacity analysis prepared for Alternative 3D (Appendix 3), the intersections of State Boulevard with Spy Run and Clinton Street would not function at an acceptable level of service in the design year. Both intersections are expected to have at least one movement function at a LOS E or F during the PM peak hour. While the dedicated left-turn lane may help alleviate some traffic congestion along the corridor, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain. Furthermore, this alternative would result in the same use of 4(f) resources as compared to Alternative 3A.

Alternative 3D



Measures to Minimize Harm

The proposed State Boulevard Reconstruction Project has been designed to reduce and minimize the use of each of the identified 4(f) resources. In an effort to minimize the overall footprint of the proposed roadway, a 3-lane typical section was considered and evaluated. It was determined that a 3-lane typical section would reduce the construction limits by approximately ten feet on each side of the roadway. Because the reduction would only be ten feet, the alternative would still result in the same use of 4(f) resources as the 4-lane typical section to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the bridge over Spy Run Creek. In addition, a 3-lane typical section would address some of the project's safety concerns and the project's substandard geometrics; however, a 3-lane typical section would not address corridor connectivity or traffic congestion concerns along State Boulevard. Traffic congestion concerns would not be addressed at the intersection of State Boulevard and Clinton Street and the Level of Service would remain at an

unacceptable level. The congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would also still remain.

The modification of the proposed Oakridge Road extension was evaluated to minimize the number of total parcel acquisitions of contributing properties (112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard) between existing State Boulevard and proposed State Boulevard. Shortening the right-turn lane and eliminating the landscaped median, constructing sidewalks adjacent to the curb with retaining wall placed at the back of sidewalks, the use of guardrail, and enclosed drainage systems utilizing inlets were all options evaluated. The evaluated aspects did not result in a significant reduction of property impact. It was concluded that the significant reduction in greenspace between the existing residence and proposed roadway, impacts to existing drives, and removal of non-residential structures located on the properties would still likely result in a total parcel acquisition outcome.

Alternatives 3A, 3C, and 3D shift State Boulevard from its historical location; however, existing curvature of State Boulevard could be maintained between Eastbrook Drive and Terrace Road. In addition, the relocation of State Boulevard associated with Alternatives 3A and 3C would require the acquisition of the remaining homes along Eastbrook Drive (south of State Boulevard), resulting in the elimination of this portion of Eastbrook Drive (contributing resource). In an effort to further minimize the use of identified Section 4(f) resources, the existing curb lines of Eastbrook Drive would remain in place where possible along this portion of the roadway.

Mitigation

Mitigation measures have been detailed in a Memorandum of Agreement (MOA) executed by consulting parties December 29, 2014. The MOA includes the following mitigation measures for historic properties:

FHWA will ensure that the following measures are implemented:

I. CONTEXT SENSITIVE SOLUTIONS

A. The City of Fort Wayne shall, where feasible, implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.

B. The City of Fort Wayne, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences. The City of Fort Wyane shall provide the Indiana SHPO and consulting parties a dispensation plan for salvaged architectural details.

C. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.

D. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:

1. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
2. Context sensitive solutions that may include but not be limited to: protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; and maintaining pedestrian connections along the former Eastbrook and Westbrook drives shall be included among the measures considered.
3. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
4. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), INDOT, the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.
5. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail or US 27) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.
6. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the

Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

7. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before scheduled meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.

8. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

9. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.

II. PHOTOGRAPHIC DOCUMENTATION

- A. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."
- B. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
 - 1. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - 2. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
- C. This documentation will include black and white prints of digital photographs and a digital video disc ("DVD") containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" that are in effect at the time.
 - 1. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 - 2. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;

3. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
4. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.

4(f) Least Overall Harm Analysis

This section compares and summarizes the use of Section 4(f) resources associated with each alternative evaluated in the Section 4(f) Evaluation and leads to a determination of the alternative with the least overall harm to Section 4(f) properties.

Each of the remaining four alternatives (3A, 3B, 3C, and 3D) result in the use of an identified 4(f) resources. Table 2 presents the comparison of alternatives showing the evaluation and use of the identified section 4(f) properties.

Table 2: Summary of Alternatives Evaluation and Use of Section 4(f) Properties

Project Impacts/Effects	Alternative 3A: Substandard Horizontal Curve Correction	Alternative 3B: State Boulevard on Existing Alignment	Alternative 3C: Shift State Boulevard Alignment South	Alternative 3D: 3 Lane Typical Section
Use of 4(f) Resources				
Brookview-Irvington Park Historic District	15 Contributing Property Relocations (residential)	18 Contributing Property Relocations (residential)	9 Contributing Property Relocations (residential)	15 Contributing Property Relocations (residential)
Fort Wayne Park and Boulevard System Historic District	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced	Clearing/altering landscape, State Blvd widened and elevated, bridge over Spy Run Creek replaced	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced
Bridge over Spy Run Creek (Non-Select Historic Bridge)	Replaced	Replaced	Replaced	Replaced
Vesey Park	0.605 ac permanent, 0.122 ac temp ROW	0.313 ac permanent, 0.055 ac temp ROW	1.46 ac permanent, 0.092 ac temp ROW	0.517 ac permanent, 0.143 ac temp ROW
Factors for Consideration (774.3(c)(1)(i-vii))				
Ability to mitigate adverse effects	Moderate	Low	Mod-High	Moderate
Relative severity of remaining harm after mitigation	High	Highest	Moderate	High
Relative significance of each Section 4(f) property	High	Mod-High	Mod-High	High
Views of officials with jurisdiction(SHPO)-Adverse Effect for all alternatives	Severe	Most Severe	Less Severe	Severe
Relative satisfaction of Purpose and Need	High	Low	Mod-High	Mod-Low
Magnitude of any adverse effects to non-4(f) resources				
Neighborhood cohesion	+	=	=	+
Environmental Justice	=	=	=	=
Business Relocations/ Encroachments Outside of Historic Districts	=	=	--	=
CAC/Public Involvement	No Consensus	No Consensus	No Consensus	No Consensus
Additional residential building relocations	=	=	-	=
Natural Resources (streams, wetlands, forest)	-	-	--	-
Project Costs Estimates (millions)**	8	8.5	13.5	7.5

Key: ++ Very Positive Effect; + Positive Effect, = Status Quo; - Negative Effect; -- Very Negative Effect

Alternative 3A – Sub-standard Horizontal Curve Correction

Alternative 3A would require the relocation of 15 contributing properties from the Brookview-Irvington Park Historic District. In addition, the realignment of State Boulevard and change in elevation would cause a bifurcation of the district and the removal of contributing features from their historical location. A similar use of 4(f) resources, resulting from the alteration and removal of contributing features from their historical location would also occur to the Fort Wayne Park and Boulevard System Historic District. Alternative 3A also requires the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park.

Alternative 3A addresses the project's purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Alternative 3A also elevates the roadway above of the 100-year floodplain. Alternative 3A fully satisfies the project's purpose and need.

Alternative 3B – Existing Alignment Improvements

Alternative 3B would result in a similar use of Section 4(f) resources as Alternate 3A to properties from the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. This alternative would also require the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park. However, Alternative 3B would result in the relocation of 18 contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. This alternative would require a higher number of residential and historic property relocations for construction as compared to other alternatives. Alternative 3D would not meet all the needs for the project.

Alternative 3C – Southern Most Alignment

Alternative 3C would result in similar use of Section 4(f) resources as 3A and 3B to properties in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. This alternative would also require the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park. However, Alternative 3C would only result in the relocation of nine contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3C would address the flooding issue by elevating the roadway above of the 100-year floodplain. It would also address some of the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. However, it introduces a new intersection at State Boulevard and Clinton Street which would create new congestion and traffic operational issues due to its close proximity to the Terrace Road intersection. Due to the introduction of new congestion and traffic operational issues, Alternative 3C would not meet all of the needs for the project.

Alternative 3D – 3-Lane Typical Section

Alternative 3D would result in the exact same use of Section 4(f) resources as Alternative 3A, including the relocation of 15 contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain. However, Alternative 3D does not fully address corridor connectivity or congestion along State Boulevard. This alternative would not address the congestion at the intersection of State Boulevard Clinton Street. The congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersection would still remain. Alternative 3D would not meet all of the needs for the project.

Conclusion

Alternative 3B results in the most overall harm to Section 4(f) resources, requiring the relocation of 18 contributing properties. Alternative 3D and 3A result in the same use of Section 4(f) resources. However, Alternative 3A better satisfies the project's purpose and need. Alternative 3C causes the least harm to Section 4(f) resources with the anticipated relocation of only nine contributing properties.

The magnitude of adverse effects to non-4(f) resources associated with Alternative 3C is significant. Alternative 3C would also result in the relocation of four commercial businesses. Project costs associated with Alternative 3C would be an estimated five million dollars more than any other alternative due to the required relocation of the commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street. Alternative 3C addresses the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve and also elevates the roadway above of the 100-year floodplain. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street, creating traffic operational issues due to its close proximity to the new Terrace Road intersection with State Boulevard. Therefore, Alternative 3C does not sufficiently satisfy the purpose and need of the project.

Alternative 3A is the only alternative that fully addresses the project's purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Alternative 3A also elevates the roadway above of the 100-year floodplain. While Alternative 3A has a greater number of contributing property relocations than Alternative 3C, the relative significance, value, and use of the 4(f) resource in Alternative 3A does not exceed the magnitude of adverse effects to non-Section 4(f) resources in Alternative 3C. In addition, the contributing properties relocated by Alternative 3A do not possess any unique features, when compared to the remaining properties in the Brookview-Irvington Park Historic District, which would make them individually eligible for the NR. Representative photographs of the relocated structures can be seen in Appendix 2. A significant portion of the contributing properties to be relocated by Alternative 3A are also located in areas that flood multiple times a year and thus continue to deteriorate at a relatively rapid rate.

In summary, the reduction of harm to Section 4(f) resources resulting from Alternative 3C does not outweigh the harm to non-Section 4(f) resources and properties adversely affected by this alternative. Therefore, among the remaining build alternatives which use 4(f) resources, Alternative 3A is considered the alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the four identified 4(f) resources.

Agency Coordination

During the course of consultation, the following organizations have responded affirmatively to the invitation to join consultation: City of Fort Wayne; Friends of the Parks of Allen County; Allen County Historian; Indiana Landmarks—Northern Regional Office; Fort Wayne Historic Preservation Commission; ARCH, Inc.; Brookview Neighborhood Association; Indiana Historic Spans Taskforce; Irvington Park Neighborhood Association. Additionally, the following individuals or organizations participated in or requested to join consultation: Charley Shirmeyer, Northside Galleries; Albert Cohan, Westbrook 5, LLC; Thomas Niezer, Barret & McNaghy, LLP; Ronald Ross, Martin Riley Architects and Engineers; Dan Ernst, Earth Source, Inc.; Jan Dailey, State Boulevard Resident. (See Appendix B: Consulting Parties.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks of Allen County and Brookview Neighborhood Association be invited to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On April 23, 2009, SHPO wrote in response to the notification concerning the reconstruction of State Boulevard and requested a literature review, historic context, research methodology, property descriptions, and NR eligibility evaluations and recommendations to aid analysis of the project. SHPO recommended the Friends of the Parks and Boulevard Neighborhood Association, Indiana Historic Spans Task Force, and bridge historian Dr. James L. Cooper be invited to participate as consulting parties. (See Appendix F: Correspondence and Meeting Minutes.)

On December 7, 2009, Jan Dailey, State Boulevard Resident, wrote in response to the HPR: "I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effects." In regard to the project, she stated, "While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling that residents of this neighborhood share." She also requested that "a separate study be conducted in possible land use of the former Kroger Fuel Center." (See Appendix F: Correspondence and Meeting Minutes.)

On December 8, 2009, Indiana Landmarks—Northern Regional Office wrote in response to the HPR. Landmarks agreed that Brookview-Irvington Park Historic District is eligible for the NR and suggested modifications to the HPR recommendations in light of NR nominations being composed by ARCH, Inc. Indiana Landmarks also requested more information on the proposed design in order to comment on a preliminary effect finding. Indiana Landmarks disagreed with the APE, asked some preliminary questions regarding the purpose and need in relation to historic properties, questioned the appropriateness of including a "trail bridge" in this Section 106 investigation, expressed the opinion that the "substandard horizontal curve" was a "character defining" element of the Brookview-Irvington Park historic district, and expressed the need for a "broad range of alternatives" to be included as part of the project options, and expressed concerns about the impacts of a different project on this Section 106 undertaking. (See Appendix F: Correspondence and Meeting Minutes.)

On December 9, 2009, ARCH, Inc. wrote in response to the HPR. Arch, Inc. agreed with the recommendation of eligibility for the Brookview-Irvington Park Historic District, noting that an NR nomination was being prepared. ARCH, Inc. requested the inclusion of proposed design maps, requested more detailed data regarding the project purpose and need, questioned the inclusion of the "trail bridge" in this Section 106 study, expressed the opinion that the "substandard horizontal curve" was a "character defining" element of the Brookview-Irvington Park historic district, disagreed with the APE, stated the importance of consulting "early in the undertaking's planning," expressed concerns about the impacts of a different project on this Section 106 undertaking and specifically stated "we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 CFR 470) would be appropriate." Finally, ARCH, Inc. agreed with statements regarding flooding in the area, but stated they "contend that this is an issue which is recent."

In a letter dated December 10, 2009, Julie Donnell, president of the Friends of the Parks of Allen County, Inc. wrote in response to the meeting agenda and HPR. Donnell expressed concern over the project's Section 106 process, including the concern "that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and that after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date." The letter also commented on the Brookview Neighborhood, concurring with other consulting party comments on the resource and posing questions regarding the project's effects on the landscape, and expressed the integral importance of the landscape in the Brookview neighborhood's integrity. The letter requested considering the inclusion of the Cultural Landscape Foundation in the Section 106 process. (See Appendix F: Correspondence and Meeting Minutes.)

On December 14, 2009, SHPO wrote in response to the Draft HPR. Regarding the APE, SHPO wrote that “we are not yet prepared to comment on the adequacy of the APE.” SHPO commented on the HPR in the same letter, stating, “[o]ur initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.” SHPO also wrote in response to the archaeological report that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [NR] within the area which was surveyed for this project by Archaeological Consultants of Ossian,” but noted that the final alignment was not yet determined and that further archaeological investigations may be necessary. SHPO asked for more information on the project alignment and the purpose and need. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held December 15, 2009, in Fort Wayne, consulting parties expressed concern with the APE used in the HPR, noted the importance of the “park-like setting” to the Brookview neighborhood, and questioned the selection of alternatives. (See Appendix F: Correspondence and Meeting Minutes.)

On January 27, 2010, SHPO responded to minutes of the consulting party meeting held December 15, 2009. SHPO requested more information regarding the purpose and need but stated that perhaps their questions would be answered in the forthcoming information packet for consulting parties. SHPO expressed concern about the purpose and need of the project. SHPO also asked for “clarification” on “the substandard nature of the roadway curvature on State Boulevard,” especially in light of statements from consulting parties “that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard.” SHPO also stated “[w]e believe it is important for FHWA to evaluate this project’s purpose and need carefully before the Section 106 consultation proceeds much further. . . Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.” Regarding the APE, SHPO asked some questions given the list of the alternatives provided at the December 15, 2009, consulting party meeting as well as in light of statements from consulting parties. “If . . . diversion of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.” SHPO also stated that “we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.” (See Appendix F: Correspondence and Meeting Minutes.)

SHPO wrote on March 10, 2010, in response to the revised meeting minutes from the December 15, 2009, meeting. In the letter, SHPO stated that the Spy Run Bridge had been finalized as a Non-Select, NR-eligible bridge per the Indiana Statewide Historic Bridge Inventory. SHPO restated the understanding that Arthur Shurcliff intended “that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses . . . that contribute to the Brookview-Irvington Park Historic District are demolished.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 15, 2011, Jill D. Downs, chairperson of the Preservation Committee of ARCH, Inc., wrote to the Deputy SHPO regarding American Structurepoint’s May 19, 2011, letter. Downs questioned whether the revised purpose and need would “trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.” (See Appendix F: Correspondence.)

On June 16, 2011, John H. Shoaff wrote that as a member of the city council, they “face an unpleasant two-fold task of fighting for a properly democratic, participatory process...” (See Appendix F: Correspondence and Meeting Minutes.)

On June 16, 2012, Todd Zeiger, Indiana Landmarks, sent an email asking for clarification of whether consulting parties were to comment on the May 19, 2012, letter and requesting a thirty day extension to the review period. (See Appendix F: Correspondence and Meeting Minutes.)

On June 17, 2011, Julie Donnell of the Friends of the Parks of Allen County sent an email to American Structurepoint conveying her letter dated June 14, 2011, in which she requested an additional thirty days of review. She expressed surprise that changes were made to purpose and need without “communicating this.” In the text of the email, Donnell wrote: “In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.” The email also said, “We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 1, 2011, John H. Shoaff wrote to point out discrepancies in traffic numbers presented. (See Appendix F: Correspondence and Meeting Minutes.)

On July 5, 2011, SHPO responded to American Structurepoint’s letter of May 19, 2011. In their letter, SHPO wrote that it appeared appropriate to expand the APE “if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project” and stated foreseeable “areas where the character of use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well.” SHPO also requested American Structurepoint review previous correspondence and meeting minutes and “make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 10 letter.” SHPO also suggested that American Structurepoint share comments “that have been or shortly will be received in response to your May 19 and June 17 letters.” The letter re-stated comments from December 14, 2009, regarding the archaeology report. (See Appendix F: Correspondence and Meeting Minutes.)

Suzanne Slick, of the Irvington Park Neighborhood Association, sent an email on July 6, 2011, expressing disappointment with the project’s evaluation of impacts to neighborhood residents. The letter also stated, “There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood, and little interest in its usability.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 7, 2011, Michelle Briggs Wedaman of the Brookview Neighborhood Association emailed American Structurepoint and asked that her email address be updated in the project record and that she would provide comments on behalf of the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

At an Agency Coordination meeting held July 13, 2011, SHPO suggested that American Structurepoint coordinate to evaluate if the project would result in a need to change the NR district boundaries. SHPO also suggested that American Structurepoint more specifically address the consulting party issues and comments in coordination. It was also agreed upon that the ACHP should be invited to participate in the State Boulevard project at this stage in the Section 106 process, rather than later. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2011, Suzanne Slick wrote regarding the consulting party comment and response form. Slick wrote regarding the consultation process, “People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are

pat, formulaic, vague and evasive.” Slick expressed concern with the proposed project and provided links to websites associated with various aspects encountered in this project. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held September 1, 2011, consulting parties questioned the response process and whether all comments had been shared. Consulting parties were encouraged to respond to any Section 106 correspondence, even if the thirty day time period had passed. An effort would be made to post all Section 106 documentation on the City of Fort Wayne’s website. Consulting parties suggested that the project include consultation with a professional landscape architect. It was also noted that the State Boulevard curve is included in the Fort Wayne Park and Boulevard System Historic District which is different from the Brookview-Irvington Historic District. SHPO requested the consultant “look at the implications of reduction the width of a new alignment. . .[and]. . . evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.” (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, at the Agency Meeting with FHWA and INDOT, FHWA stated it would follow-up on its invitation to the ACHP, noting that the ACHP’s involvement in the process would be beneficial. During the meeting it was agreed that American Structurepoint would provide consulting parties with a more elaborate alternatives analysis, would look into developing a Section 106 page for this project on the City of Fort Wayne’s website, and that an addendum to the HPR would be prepared. (See Appendix F: Correspondence and Meeting Minutes.)

The ACHP responded to FHWA’s invitation to join consultation on September 22, 2011. ACHP requested additional documentation in order to “determine whether our participation in the consultation to resolve adverse effects is warranted.” (See Appendix F: Correspondence and Meeting Minutes.)

On November 7, 2011, SHPO responded to the material conveyed August 15, 2011, and September 29, 2011. Regarding the Brookview-Irvington Park Historic District, SHPO stated, “Having considered the marked aerial photograph shown at the last consulting party meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative.” However, SHPO added, the proposed realignment of State Boulevard within the district “is not an ideal situation from a [NR] boundary delineation standpoint.” Further, SHPO stated, “We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished.” SHPO also offered additional comments from the September consulting party meeting that had not been recorded in the meeting minutes regarding the alternatives analysis. SHPO also questioned the feasibility of converting the existing Spy Run Bridge into a pedestrian bridge. SHPO stated they would also recommend, “where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff’s landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.” SHPO also suggested shifting the proposed alignment somewhat to the east to better reflect Kessler’s original plan for connecting State Boulevard. SHPO noted that this change may “result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along State Boulevard. Even if the project costs were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff’s design of the Brookview-Irvington Park Historic District, while largely meeting the city’s purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler’s plan.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an Agency meeting was held to discuss the State Boulevard Project. At the meeting, American Structurepoint reviewed the responses to the SHPO letter of November 7, 2011, and agreed to send them in writing. It was decided to hold a meeting with consulting parties in early September to discuss the Additional Information HPR, to present the preferred alternative and to discuss the MOA. Mitigation ideas from

that meeting included: Advisory team similar to US 27; Photographic documentation of bridge over Spy Run; Restore character of State Boulevard within the district; and Educational mitigation.

On June 22, 2012, SHPO provided comment on the AI Report. In the letter, SHPO stated, “we agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the [APE], for inclusion in the [NR].” SHPO agreed that the house at 315 East State Boulevard “does not appear to possess sufficient historical or architectural significance or integrity to be eligible of inclusion in the [NR].” SHPO also commented on the explanatory note contained in the Fort Wayne Park and Boulevard NR nomination form which stated the portion of State Boulevard within the Brookview-Irvington Historic District was individually eligible for the NR. SHPO stated, “we do not consider that comment . . . to confer individual eligibility on State Boulevard or any part of it.” SHPO further stated, “we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the [APE] is individually eligible” for the NR, but added “[w]e do not disagree, however, with the Fort Wayne Park and Boulevard system nomination identification of the portion of State Boulevard in question as a contributing resource to that historic district.” (See Appendix F: Correspondence and Meeting Minutes.)

Regarding archaeology, SHPO stated, “Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence of absence of archaeological resources.” SHPO noted that one example of areas that may need archaeological survey included “a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report.” If the entire lot would need to be acquired as part of the project, “then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated July 31, 2012, the ACHP wrote that “[b]ased upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time.” However, the Council did request to be notified in the event of an Adverse Effect finding and at that time the Council would “re-evaluate the undertaking . . . and advise you whether or not we have changed our decision regarding participation in consultation.” (See Appendix F: Correspondence and Meeting Minutes.)

On August 13, 2012, the Indiana SHPO concurred with the archaeology short report (Stilwell, July 11, 2012) that “no further investigations appear necessary at these additional portions of the project area” and that the office had not identified any archaeological resources listed or eligible for listing in the NR. (See Appendix F: Correspondence and Meeting Minutes.)

At the consulting party meeting held on September 19, 2012, consulting parties were asked to provide input into mitigation for the proposed undertaking. Most comments focused on purpose and need for the project; some spoke about traffic issues. Michelle Briggs Wedaman (Brookview Neighborhood Association) asked for context sensitive solutions at the beginning of the project rather than the end. Susan Haneline (property owner) asked why the owners of the three residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes. Todd Zeiger (Indiana Landmarks) encouraged the involvement of the ACHP because he feels that there was anticipatory demolition as part of a flood control project. He asked that it be noted in this documentation that there is a bifurcation of the district. Tom Cain (City of Fort Wayne) pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in those landscape elements needs discussion in the documentation. The visual and special components of the larger landscape need to be understood so they can be addressed in a mitigation discussion. Michael Galbraith (ARCH, Inc.) encouraged ACHP involvement, objected to the change

in historic consultant, asserted that the APE is inappropriate, and raised the question of cumulative impacts. Edward Welling (Friends of the Parks of Allen County) said that mitigation is premature since the APE is not appropriate; the MOA should be postponed until Environmental Assessment is complete. Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. Dr. James Glass (Deputy SHPO) expressed reservations that consensus can be developed for this project; he stated that this meeting was the time for consulting parties to put forth mitigation ideas. John Carr (SHPO staff) requested any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion. Mr. Galbraith objected to the timing of the consulting party meeting; Patrick Carpenter, manager of the INDOT-CRO, said that the timing was established so that consulting parties could discuss mitigation and formulate new ideas. Ms. Wedamen said that she did not believe that the public process has been followed. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 14, 2012, Karl Dietsch wrote regarding a safety issue in the proposed project area. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 17, 2012, 11 residents of the Brookview Neighborhood jointly submitted a letter regarding the State Boulevard project. The letter expressed support of the project. The residents stated, “We STRONGLY support the buyout of our homes thereby allowing for State Boulevard to be relocated to the south of its current location” and went on to conclude, “We are NOT in favor of finding ways to retain our homes within the footprint of the project; we feel this will lessen our property values, continue to cause issues with access to our homes, and leave the constant flooding issue unresolved.” (See Appendix F: Correspondence and Meeting Minutes.)

Sara Kruger Geyman, a member of the public, wrote in response to the meeting held September 19, 2012. (Note that the letter conveying responses to the consulting party meetings was dated August 21, 2012, and is likely a typo.) Geyman expressed concern “that residents are not and have not been consulted in this matter” and expressed dissatisfaction with meeting’s facilitation. Geyman offered comments to the project in general, objecting to its necessity and, regarding Section 106, stating: “Mitigation is premature in a plan and a process that has been faulty from the beginning. It is a proverbial lollipop stuck in the hands of resident to quiet them down and distract them from the truth.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 1, 2012, Susan R. Haneline, a Brookview neighborhood homeowner, expressed support for the project, noting that the current problems with flooding and bridge deterioration “do nothing to showcase what IS historical about the neighborhood.” Haneline added, “We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don’t have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood.” Haneline’s letter also included photographs showing recent flooding in the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

Susan Haneline submitted an additional letter dated October 2, 2012. Haneline stated the current proposed design, “seems . . . to actually enhance historic vision, not cause it to be destroyed.” Haneline offered suggestions to “respect the historic vision,” including: 1.) “Installing historically correct lighting in the area”; 2.) “Plantings and green space that gives the area a park like feel, such as period style benches, grouping of trees and flowers, perhaps even brick style sidewalks”; 3.) “stone or brick entrance pillars for the neighborhood”; 4.) adding trees and flower beds to the bifurcated State Boulevard; 5.) “small monuments” conveying the history of the neighborhood and Arthur Shurcliff; 6.) “find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 3, 2012, John Shoaff wrote regarding the project, consulting party meeting, and 800.11 materials. Shoaff wrote, "I cannot support the current State Boulevard widening plan in anything like its present form. . ." In particular, Shoaff objected to plans to elevate the road as a "perversion of the proper use of the 'Bypass and Arterial concept' . . ." Shoaff identified "two legitimate needs" in the Brookview neighborhood: the repair or replacement of the bridge over Spy Run Creek and the elimination of a "blind spot at the foot of State Boulevard, near the intersection with Westbrook." Shoaff stated that project plans should address these needs but be "minimally harmful to the historic district." Shoaff added that discussion of project planning and mitigation discussion "should await the outcome of the Environmental Assessment." (See Appendix F: Correspondence and Meeting Minutes.)

Shoaff also included comments on the September 19, 2012, consulting party meeting. Shoaff responded to comments received by Michelle Briggs Wedaman from FHWA's representative. Shoaff objected to the facilitation of the meeting stating "the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government."

Shoaff enclosed letter "signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen." The letter objected to the State Boulevard project. (See Appendix F: Correspondence and Meeting Minutes.)

Also on October 3, 2012, Suzanne Slick wrote regarding the project and the consulting party meeting of September 19, 2012. Slick stated that not building the project is preferable to mitigation and objected to the facilitation of the consulting party meeting. The letter re-stated some comments offered previously by consulting parties regarding the Purpose and Need and design. Slick objected to the traffic data previously supplied by American Structurepoint and offered two examples in which she found low-volume traffic while utilizing the State Boulevard. Slick stated the APE was inappropriate. (See Appendix F: Correspondence and Meeting Minutes.)

Julie Downs, Friends of the Parks of Allen County, submitted comments via a letter dated October 3, 2012. Downs stated the Friends of the Parks of Allen County agreed with the finding of adverse effect for the project but added "any discussion of mitigation is, at best, premature; at worst, the proposed [MOA] is a bad faith attempt to confuse an already complicated and unfair process." Downs also stated the "APE is not comprehensive enough and should include historic districts along State Boulevard" and "it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete." Finally, on behalf of members of the Friends of the Parks of Allen County who attended the September 19, 2012, consulting party meeting, Downs objected to the facilitation of the meeting and concluded, "Under these circumstances, the public is not being served properly at all." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Jill Downs wrote regarding the 800.11(e) and draft MOA. Downs agreed with the project's adverse effect finding but noted "the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration" and pointed to the September 19, 2012, consulting party meeting as proof of this breakdown. She stated it was premature to discuss mitigation because the Environmental Assessment had not been completed; the bifurcation of the district, elevation of State Boulevard, and the Pufferbelly Trail project should be added to the list of adverse effects; the Pufferbelly Trail project should be incorporated into the effects discussion; and the project has not fully accounted for the previous removal of several homes by the City of Fort Wayne which creates the impression of less impact as a result of the project. Downs concluded by stating she did not see the need to reconstruct State Boulevard. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michael Galbraith of ARCH, Inc., wrote formally requesting an extension of the thirty-day comment period for the proposed MOA and mitigation measures. Galbraith stated, "We do not in any form, fashion, or manner concur with the proposed mitigation as present either in the draft supplied with the

FHWA 4(f) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintraut. Galbraith also stated that “we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact.” (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michelle Briggs Wedaman of the Brookview Neighborhood Association, wrote requesting a thirty-day extension of the consulting party comment period to incorporate the material provided on September 18, 2012, into their comments. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the revised Section 800.11 documentation in the Environmental Assessment.) Wedaman stated that previous questions from the December 2009 and September 2011 consulting party meetings “have remained unanswered,” particularly those dealing “Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the area of impact of this project.” Wedaman questioned how an appropriate discussion of mitigation could take place prior to the completion of the environmental assessment. (See Appendix F: Correspondence and Meeting Minutes.)

The SHPO wrote in response to the project in a letter dated October 4, 2012. SHPO concurred with the opinion of the archaeological short report, the Section 106 finding of effect and that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and Bridge on State Boulevard over Spy Run would all be adversely affected as part of this undertaking. SHPO expressed concern “about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District and suggested some minimization measures. In particular, SHPO wondered if “it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive.” SHPO expressed sympathy for the preference of some property owners along the south side of State Boulevard who preferred to have their entire property, rather than a smaller portion, purchased; “however, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect.”

SHPO also offered suggestions for design for minimizing impacts and suggestions for mitigation, including an advisory team, use of context-sensitive designs, photographic documentation of the bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Todd Zeiger of Indiana Landmarks—Northern Regional Office wrote formally requesting a thirty-day extension on the comment period in light of the material conveyed September 18, 2012. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Zeiger stated “We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(f) compliance document.” Zeiger added “we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other informant provided in the 4(F) document.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Tom Cain, Fort Wayne urban designer and Creager Smith, Fort Wayne historic preservation planner, wrote regarding the project. Both agreed with the project’s adverse effect finding. The letter listed twenty-one specific adverse effects of the project on the landscape to serve as the “potential basis of mitigation measures.” Cain and Smith also stated “we are available to assist in the development of mitigation

design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on a team.” (See Appendix F: Correspondence and Meeting Minutes.)

On October 15, 2012, Tom Cain, City of Fort Wayne, called W&A to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but wanted to make suggestions within the context of SHPO’s assessment of project impacts so that the City may address all adverse effects. Cain also stated that impacts to the Brookview neighborhood should be enumerated. (See Appendix F: Correspondence and Meeting Minutes.)

On October 16, 2012, W&A contacted Tom Cain in response to his phone call the previous day. W&A explained that American Structurepoint was very glad to have his input on this project and, at a minimum, would consult with him prior to the agency meeting. Cain spoke about the landscape changes that would take place as a result of the undertaking, particularly the changes from private to public space around the undertaking. He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s. Cain stated he would like for mitigation to deal with changes in scale that will occur; tree planting should occur within three feet of the roadway (and not the standard ten feet required on highways.) Cain stated this would change the scale of the undertaking for the residents. Cain also stated he would convey additional mitigation suggestions via email and stated the importance of achieving the “right feel” for the space. (See Appendix F: Correspondence and Meeting Minutes.)

On November 15, 2012, SHPO wrote in response to American Structurepoint’s offer to draft specific language for the MOA. (See Appendix F: Correspondence and Meeting Minutes.)

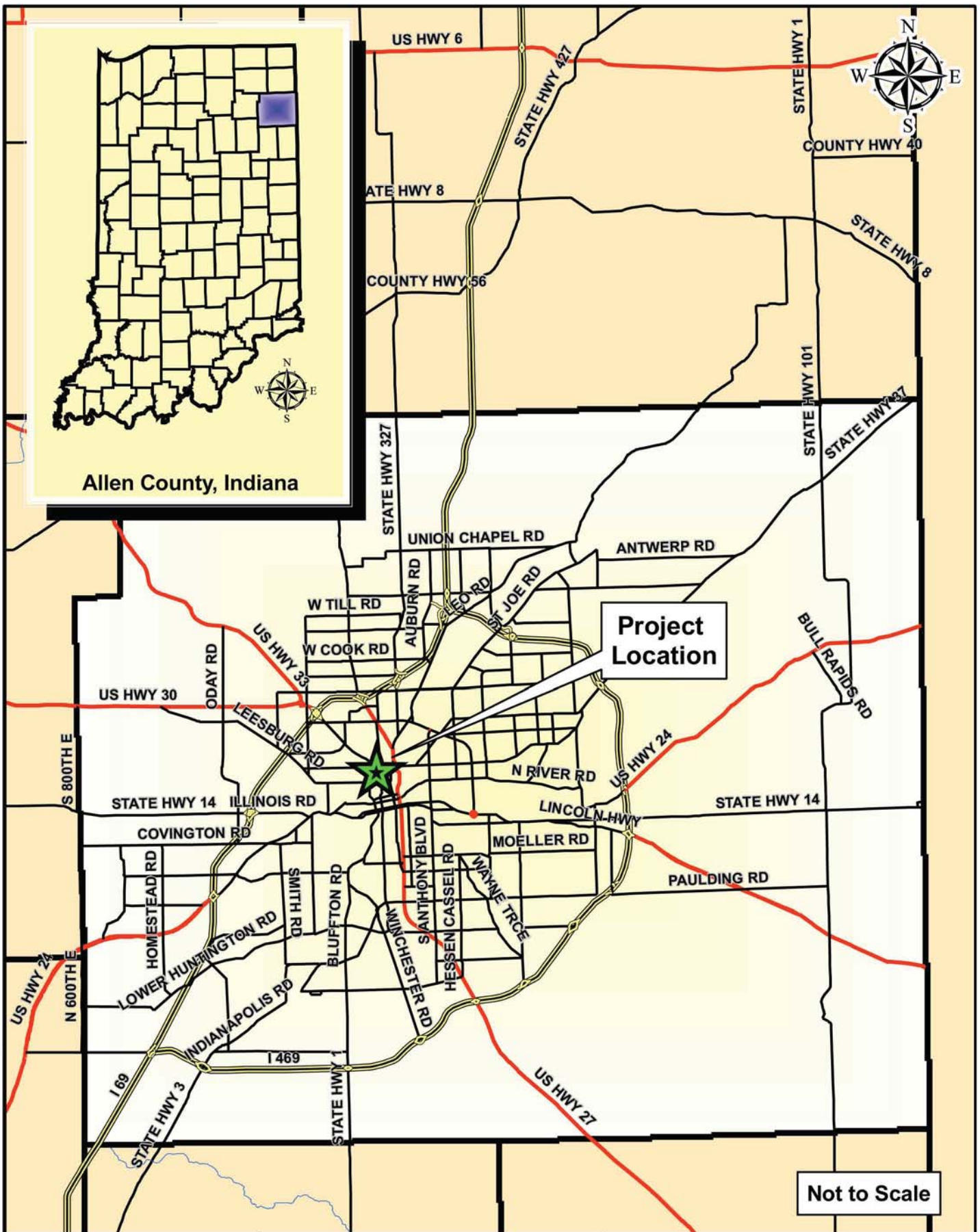
On December 18, 2012, American Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain (landscape architect/City of Fort Wayne) made the presentation. Cain’s plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A, Plans.) Dr. James Glass (SHPO) expressed appreciation for the effort Mr. Cain had put forth for a thoughtful landscape plan. Dr. Glass said that his office needed time to digest but that he understood Mr. Cain’s point that in a Section 106 sense, there was a need to mitigate for the houses and for the loss of historic character. He also understood that there are larger issues of flood control and engineering that make this project difficult. There was discussion of other resources that may be preserved as far as compensation for the lost historic resources (houses and landscaping). It was agreed that SHPO would be given time to digest the landscape design presented at the meeting and that the City and its consultants would look for additional ways to mitigate, such as grants to rehabilitate the facades of existing houses (if practical and legally viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the bridge over Spy Run. Mary Ann Naber (FHWA preservation officer) suggested that the attendees look at the mitigation provided in Tampa. (See Appendix F: Correspondence and Meeting Minutes.)

On June 18, 2014, a Public Hearing was held for the proposed project. At the Public Hearing and in a letter dated July 18, 2014 (Appendix A – pages 1 to 3) ARCH, Inc. presented an alternative prepared by Storrow Kinsella Associates and Transportation Solutions, LLC. Storrow Kinsella Associates and Transportation Solutions, LLC were commissioned by ARCH, Inc., Indiana Landmarks, Friends of the Parks, and the

Brookview-Irvington Park Neighborhood Association (Appendix B – pages 4-14) to examine the background research developed for the proposed project to determine if there was an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible, and avoided, minimized or mitigated the adverse effect to the neighborhood. As such, it was determined through coordination with the FHWA and INDOT that the submitted alternative should be evaluated as part of the environmental process.

Appendix 1 – Mapping

State of Indiana Map



Project Location Map

Applicant: City of Fort Wayne, Board of Public Works
 420 City-Council Building
 One Main Street
 Fort Wayne, Indiana 46802

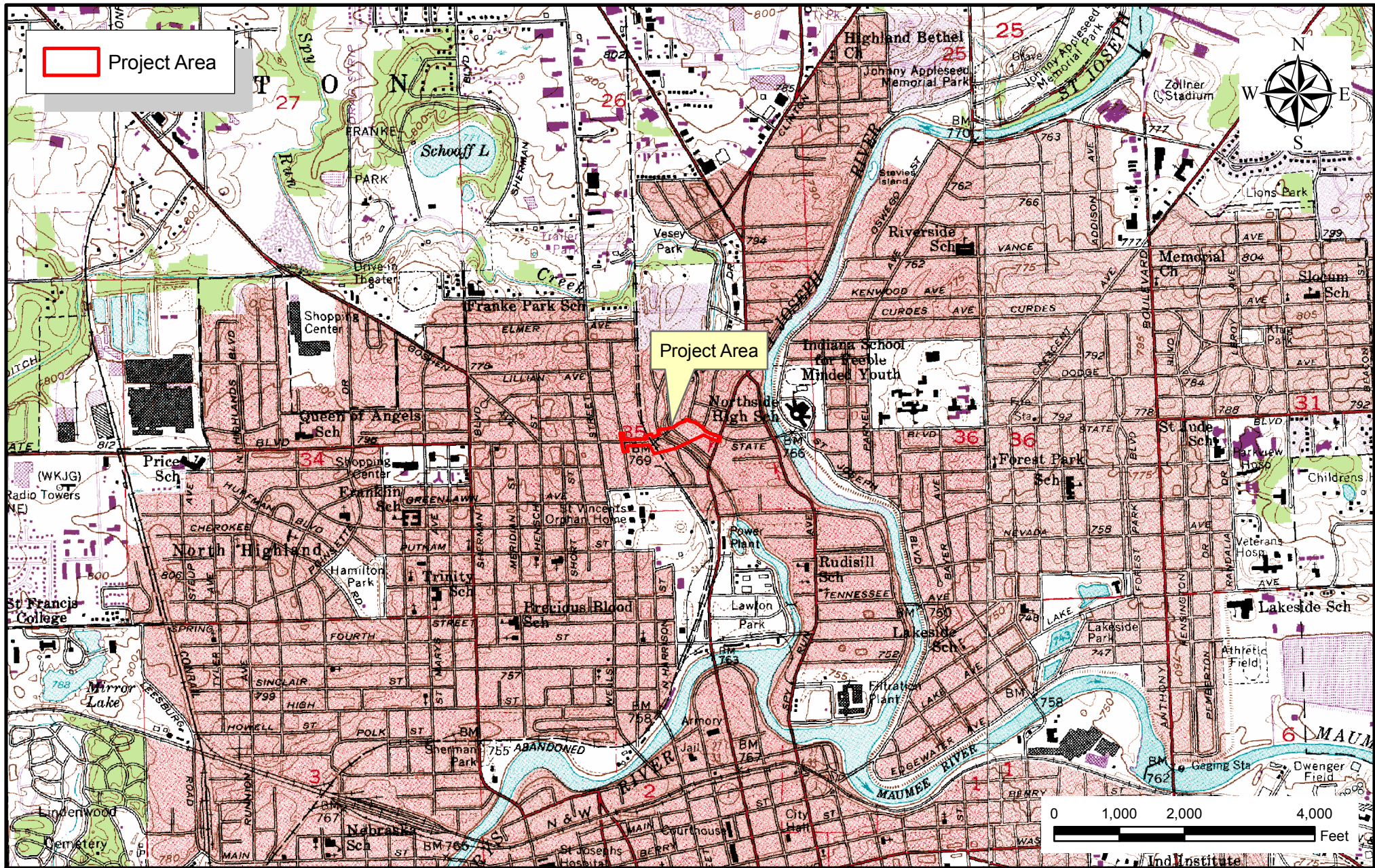
State Boulevard Reconstruction

Location: Fort Wayne
 Township: Wayne
 County: Allen
 State: Indiana

Attachment 4 - 41 of 68

Date: 03/12/2009

USGS Topographic Map



USGS Topographic Mapping Fort Wayne West and Fort Wayne East Quadrangles

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction Des. No. 0400587

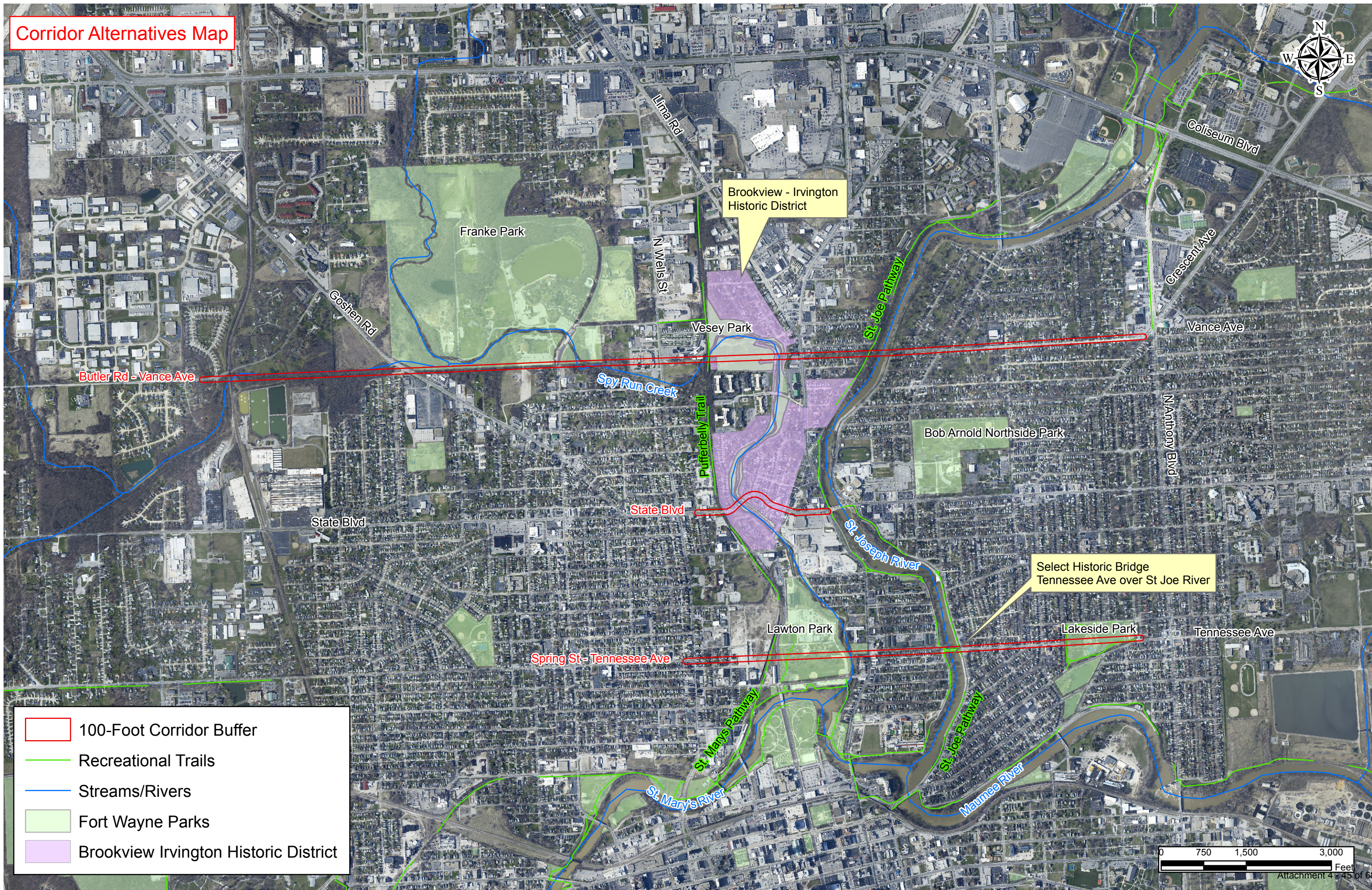
Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 07/22/2010

Attachment 4 - 43 of 68

Corridor Alternatives

Corridor Alternatives Map



See Attachment 3 Pages 1-531

Appendix 2 – Section 106 800.11(e) Documentation

Section 106 Findings and Determinations

Section 800.6(a)(3) Documentation

- A. Plans**
- B. APE Maps and Site Plans**
- C. Consulting Parties List**
- D. Photos**
- E. Report Summaries**
- F. Correspondence**
- G. Memorandum of Agreement (MOA)**

Addendum to State Boulevard Reconstruction Project November 2014

Appendix 3 - Capacity Analysis of the Consulting Parties Proposed Alternative (CPPA) for State Boulevard and Clinton Street Intersection



M E M O R A N D U M

DATE: November 20, 2014
TO: Scott Crites, PE, American Structurepoint
FROM: Jeromy Grenard, PE, PTOE, American Structurepoint
RE: Capacity Analysis of the Consulting Parties Proposed Alternative (CPPA) for State Boulevard and Clinton Street Intersection
CC: Briana Hope, American Structurepoint

The primary purpose of this analysis is to analyze an alternative intersection treatment at the existing Clinton Street and State Boulevard intersection, as prepared and presented in a letter dated December 9, 2103 by Storrow Kinsella Associates and Transportations Solutions. The alternative intersection treatment was commissioned by ARCH with the intent of identifying options to reduce impacts of the proposed State Boulevard project on the surrounding neighborhood. A schematic of the Consulting Parties Proposed Alternative (CPPA) is included in **Figure 1**. The secondary purpose of this memo is to document the 2009 and 2030 traffic operations for all other alternatives considered in the environmental document (Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4).

The CPPA includes a two-lane roundabout with a southbound left turn bypass lane. The accommodation of this bypass lane into the design of the roundabout would require that the entering and exiting flow on the east leg (State Boulevard) of the intersection be signalized. The CPPA also includes approach metering in order to provide gaps for certain approaches when heavy flows begin to dominate upstream approaches. Roundabout metering consists of one or more legs with queue detectors and one or more legs with metering signals. Once the queue extends such that the queue detectors are activated, the metering signal(s) turns red, stopping traffic on upstream approaches temporarily and allowing the queue to dissipate on the downstream approaches.

In a meeting with the City of Fort Wayne, Indiana Department of Transportation (INDOT), and the Federal Highway Administration (FHWA) regarding the CPPA, FHWA and INDOT directed American Structurepoint to analyze the capacity of the CPPA without the southbound left turn bypass. Because a left turn bypass at a roundabout is an unprecedented treatment in the United States, the decision was made by FHWA and INDOT that such treatment was not desirable from a drivers' expectancy standpoint. For the CPPA as shown in **Figure 1** of this memo, a left turn bypass from southbound Clinton Street to eastbound State Boulevard requires a motorist to complete a left turn movement by deflecting to the left of the splitter island when approaching the north leg of the roundabout. It is counterintuitive for the motorist to be required to be on the left side of

Table 1: Intersection Turning Movement Data

Approach (Street Name)	Destination	2005 AM Peak Hour Volume	2005 PM Peak Hour Volume	2009 AM Peak Hour Volume	2009 PM Peak Hour Volume	2030 AM Peak Hour Volume	2030 PM Peak Hour Volume
North (Clinton Street)	Historic State Blvd	5	5	5	5	5	5
	State Blvd (W)	44	98	47	117	52	129
	Clinton St	1,597	1,594	1,707	1,593	1,886	1,760
	State Blvd (E)	106	227	126	178	139	197
West (Historic State Blvd)	State Blvd (W)	5	5	5	5	5	5
	Clinton St	5	5	5	5	5	5
	State Blvd (E)	5	5	5	5	5	5
Southwest (State Blvd)	Clinton St	30	31	46	67	51	74
	State Blvd (E)	420	679	543	629	600	695
	Historic State Blvd	5	5	5	5	5	5
East (State Blvd)	Historic State Blvd	5	5	5	5	5	5
	State Blvd (W)	417	570	437	539	483	596
	Clinton St	140	196	200	192	221	212
TOTAL		2,784	3,425	3,136	3,345	3,462	3,693

CPPA Analysis Scenarios*Scenario 1:*

- Original 2005 AM and PM Peak Hour traffic volumes
- CPPA Lane Configurations for roundabout and all approaches, except no southbound left-turn bypass. Lane configurations are shown in **Figure 2**.
- Roundabout metering – queue detectors installed on the north approach and metering signal on the east leg of State Boulevard

Scenario 2:

- 2009 AM and PM Peak Hour traffic volumes from NIRCC
- Same lane configurations as Scenario 1
- Roundabout metering – queue detectors installed on the north approach and metering signal on the east leg of State Boulevard

Scenario 3:

- 2009 AM and PM Peak Hour traffic volumes from NIRCC
- Addition of one southbound approach lane that acts as a dedicated left turn lane for southbound to eastbound traffic (in lieu of a left turn bypass). Lane configurations are shown in **Figure 3**.
- Roundabout metering – queue detectors installed on the north approach and metering signal on the east leg of State Boulevard

Figure 2: Scenario 1 and Scenario 2 Lane Configurations

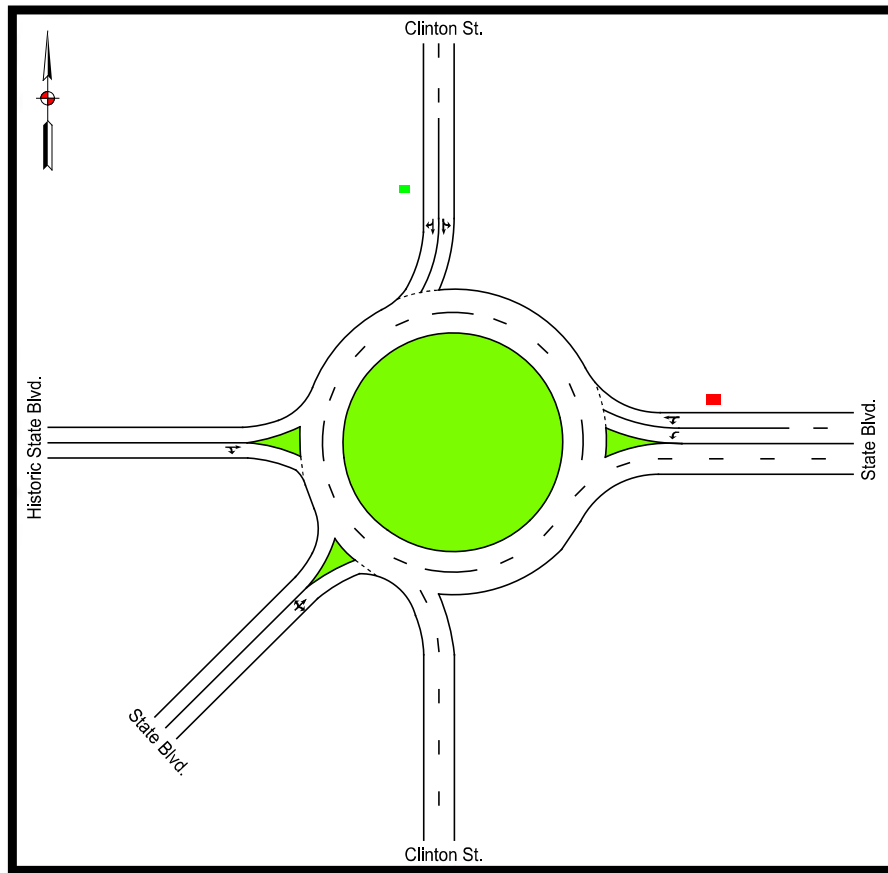
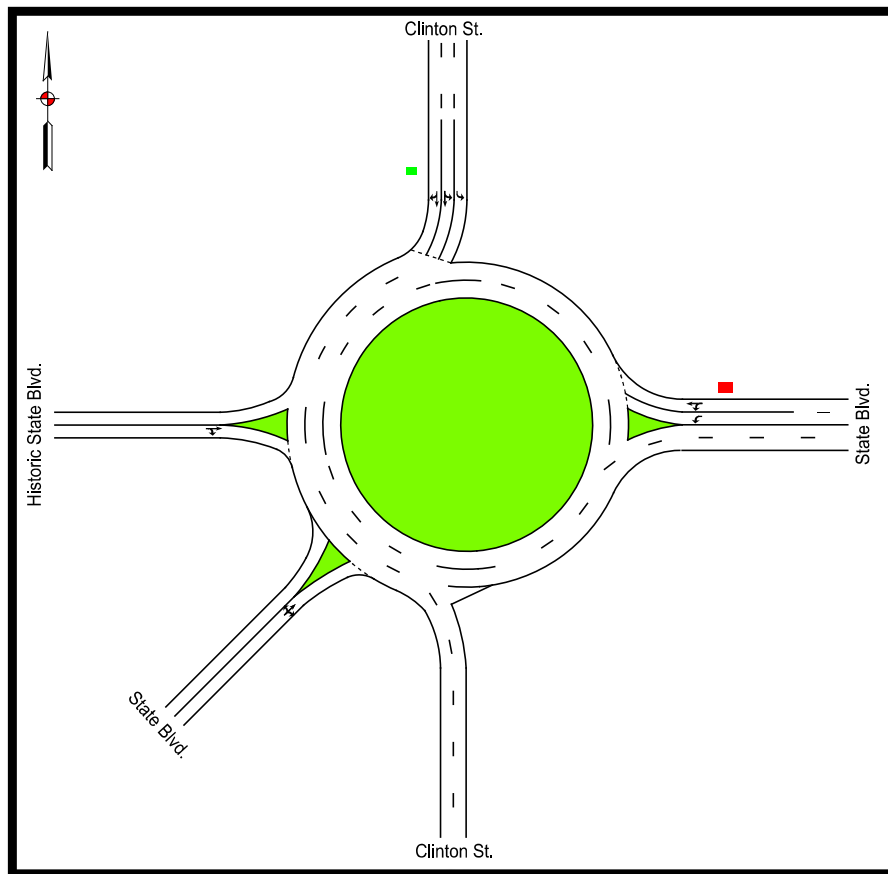


Figure 3: Scenario 3 Lane Configurations



CPPA Capacity Analysis

The results of the SIDRA capacity analysis are summarized in **Table 2**. It is noted that a number of approach metering options were explored. Because of the heavy southbound flow on Clinton Street, this approach cannot be metered. Doing so caused a level of service F on the approach. The final metering configuration involved placing queue detectors on the north approach of Clinton Street and metering signals on the east approach of State Boulevard.

The SIDRA output is also attached to this memorandum.

Table 2: Capacity Analysis Results for the Clinton Street and State Boulevard Intersection

		Scenario 1		Scenario 2		Scenario 3	
Approach (Street Name)	Destination	Year 2005 AM Peak Hour LOS Delay (sec/veh)	Year 2005 PM Peak Hour LOS Delay (sec/veh)	Year 2009 AM Peak Hour LOS Delay (sec/veh)	Year 2009 PM Peak Hour LOS Delay (sec/veh)	Year 2009 AM Peak Hour LOS Delay (sec/veh)	Year 2009 PM Peak Hour LOS Delay (sec/veh)
North (Clinton Street)	Historic State Blvd	A 6.5	A 4.5	F* 44.3	F* 31.3	A 5.7	A 6.2
	State Blvd (W)	A 5.9	A 3.8	F* 43.7	F* 30.7	A 5.3	A 5.7
	Clinton St	A 5.7	A 4.1	F* 44.6	F* 30.6	A 5.7	A 6.1
	State Blvd (E)	B 11.3	B 10.2	F* 51.4	F* 36.1	A 9.4	A 9.6
West (Historic State Blvd)	State Blvd (W)	A 7.7	A 6.8	A 9.2	A 8.0	A 8.2	A 7.9
	Clinton St	A 7.6	A 6.7	A 9.0	A 7.8	A 8.2	A 8.0
	State Blvd (E)	A 7.4	A 6.5	A 8.8	A 7.7	A 8.1	A 8.0
Southwest (State Blvd)	Clinton St	F* 41.2	F 274.2	F 413.2	F 248.1	F 389.8	F 263.1
	State Blvd (E)	F* 40.5	F 273.6	F 412.5	F 247.4	F 389.2	F 262.5
	Historic State Blvd	F* 48.1	F 281.1	F 420.0	F 254.9	F 396.3	F 269.6
East (State Blvd)	Historic State Blvd	C 27.3	F* 97.3	F* 66.8	F* 42.2	C 29.6	F* 41.8
	State Blvd (W)	C 32.2	F* 102.3	F* 71.7	F* 47.2	C 34.5	F* 46.7
	Clinton St	B 15.9	B 14.9	B 18.8	B 15.4	B 16.4	B 15.3
Overall Intersection		B 16.4	E 78.7	F 117.2	E 77.5	F 83.7	E 66.7

* LOS F is due to volume to capacity (v/c) ratio exceeding 1.0.

Year 2030 CPPA Capacity Analysis

Capacity analysis was run for the CPPA Scenario 2 and Scenario 3 lane configurations and metering signal configurations in the year 2030. **Table 3** summarizes the results of this analysis for the CPPA lane configurations.

Table 3: Year 2030 Capacity Analysis Results for the CPPA Alternative at the Clinton Street and State Boulevard Intersection

Approach (Street Name)	Destination	2030 - CPPA		2030 - CPPA Modified to 3-Lane	
		Year 2030 AM Peak Hour LOS Delay (sec/veh)	Year 2030 PM Peak Hour LOS Delay (sec/veh)	Year 2030 AM Peak Hour LOS Delay (sec/veh)	Year 2030 PM Peak Hour LOS Delay (sec/veh)
North (Clinton Street)	Historic State Blvd	F 67.6	F 96.3	A 9.2	B 10.6
	State Blvd (W)	F 61.8	F 90.4	A 4.5	C 28.1
	Clinton St	F 61.8	F 90.3	A 4.1	C 28.3
	State Blvd (E)	F 62.5	F 90.9	A 4.6	C 28.7
West (Historic State Blvd)	State Blvd (W)	A 8.9	A 6.9	A 7.2	A 9.5
	Clinton St	A 9.1	A 7.0	A 7.3	A 9.6
	State Blvd (E)	A 9.3	A 7.2	A 7.3	A 9.6
Southwest (State Blvd)	Clinton St	F 265.9	F 442.7	F 255.4	F 452.9
	State Blvd (E)	F 258.4	F 435.2	F 248.3	F 445.7
	Historic State Blvd	F 259.1	F 435.9	F 248.9	F 446.4
East (State Blvd)	Historic State Blvd	B 18.1	B 14.4	B 14.9	B 15.1
	State Blvd (W)	F 132.5	F 196.5	F 83.0	F* 62.2
	Clinton St	F 127.5	F 191.6	F 78.1	F* 57.2
Overall Intersection		F 102.6	F 175.1	F 83.7	F 117.7

* LOS F is due to volume to capacity (v/c) ratio exceeding 1.0.

Environmental Assessment (EA) Alternatives Capacity Analysis

A capacity analysis has also been performed for the EA Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4 at the intersections of Clinton Street / State Boulevard and Spy Run Avenue/State Boulevard, respectively. The purpose of this analysis is to document the existing operations of the two intersections, as well as the anticipated operations in the year 2030.

Tables 4 and 5 summarize the capacity analysis results for each of the intersections. In these tables, Alternatives 1, 2, 3D, and 4 have been grouped together because the intersection lane configurations are the same for each of these alternatives. Likewise, Alternatives 3A, 3B, and 3C have been grouped together for the same reason. The preferred alternative identified in the EA is Alternative 3A.

Alternatives 1, 2, 3D, and 4 are the same as the existing intersection lane configurations. For this reason, the capacity analysis was run with existing signal timings. When multiple movements displayed LOS E or F in the year 2030, the capacity analysis for these alternatives was rerun with optimized signal timings. The operations were slightly improved; however, there are still movements that are LOS E and F.

Table 4: Capacity Analysis Results for Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4 at the Clinton Street and State Boulevard Intersection

Approach (Street Name)	Movement	2009 Alt 1, 2, 3D, 4 & Existing Configuration Existing Signal Timing		2030 Alt 1, 2, 3D, 4 & Existing Configuration Existing Signal Timing		2030 Alt 1, 2, 3D, 4 & Existing Configuration Optimized Signals		2030 Alt 3A, 3B, 3C (Proposed Configuration) Optimized Signals	
		AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)
North (Clinton St)	L/TH/R	E 59.0	C 29.2	F 105.5	C 34.7	E 64.2	C 34.7	D 44.5	D 36.4
West (State Blvd)	TH/R	D 44.2	E 55.3	E 60.1	E 68.9	E 67.7	E 68.9	D 46.7	D 40.4
East (State Blvd)	L	E 56.9	F 86.4	E 60.6	F 99.5	E 70.0	E 73.3	D 52.5	D 39.3
	TH	C 23.6	D 48.7	C 25.4	D 53.3	B 11.2	D 37.9	C 25.0	A 6.1

**Table 5: Capacity Analysis Results for Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4
at the Spy Run Avenue and State Boulevard Intersection**

Approach (Street Name)	Movement	2009 Alt 1, 2, 3D, 4 & Existing Configuration Existing Signal Timing		2030 Alt 1, 2, 3D, 4 & Existing Configuration Existing Signal Timing		2030 Alt 1, 2, 3D, 4 & Existing Configuration Optimized Signals		2030 Alt 3A, 3B, 3C (Proposed Configuration) Optimized Signals	
		AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)	AM Peak Hour LOS Delay (sec/veh)	PM Peak Hour LOS Delay (sec/veh)
West (State Blvd)	L	E 76.2	F 152.4	E 78.8	F 188.8	D 39.7	F 166.3	C 35.0	D 54.1
	TH	C 31.4	C 21.8	C 31.6	C 23.7	A 9.6	B 17.3	A 5.5	C 31.7
South (Spy Run)	L/TH/R	C 20.4	D 35.3	C 23.0	D 51.7	D 35.8	D 54.0	C 26.3	D 39.6
East (State Blvd)	TH/R	E 60.0	E 55.5	F 86.1	E 72.9	D 46.0	E 72.9	C 29.0	D 46.1

Conclusions

Consulting Parties Preferred Alternative

Based on the capacity analysis results shown in **Tables 2 and 3**, it can be concluded that a roundabout with approach metering will not provide acceptable levels of service (LOS) to alleviate traffic congestion and meet the purpose and need of the project. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area. A LOS of A thru D is considered acceptable and is an indicator of acceptable delay and level of intersection congestion. As shown in **Tables 2 and 3**, the overall intersection LOS is E or F in all but one peak hour. The unacceptable LOS associated with the analysis of the CPPA indicates that the intersection would exhibit intersection traffic operations at LOS E or F and would not function at an acceptable level of congestion, and thus this alternative would not meet the purpose and need of the State Boulevard project.

EA Alternatives 1, 2, 3A, 3B, 3C, 3D, and 4

The year 2009 and 2030 analysis for Alternatives 1, 2, 3D, and 4 shows that the current lane configurations are not operating acceptably. This would only become worse in the future, even with optimized traffic signal timings. On the contrary, Alternatives 3A, 3B, and 3C would operate acceptably in the year 2030. For these alternatives, there were no movements that operated worse than LOS D.

The preferred alternative per the EA is Alternative 3A, and thus the preferred alternative does meet the purpose and need of the State Boulevard project in regard to traffic operations.



U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

December 30, 2014

In Reply Refer To:
HDA-IN

Willie R. Taylor, Director
Natural Resources Management Team
Office of Environmental Policy
Division of Planning
Office of the Secretary
U.S. Department of the Interior MS-2462-MIB
1849 C Street
Washington DC, NW 20240

Dear Mr. Taylor:

The Federal Highway Administration requests the US Department of Interior to review the draft Section 4(f) document again for the State Boulevard Added Travel Lanes Project in the City of Fort Wayne, Allen County, Indiana. In the Department of Interior's (DOI) July 8, 2013 letter (9043.1 ER 13/0365), it was stated that DOI could not concur due to lack of evidence that all parties had agreed to the mitigation measures. The draft 4(f) now includes completion of the consultation process with a signed Memorandum of Agreement by all the parties.

To restate, the purpose of the project is to improve corridor connectivity along State Boulevard for motorists and pedestrians. Currently existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians, as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spry Run or the Saint Mary's River.

Please provide comments by February 16, 2014. Efforts have been made to avoid and minimize impacts to the 4(f) resources, but the project will impact the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District and the Bridge over Spy Run.

We have enclosed one hard copy of the State Boulevard draft Section 4(f) document and 12 CDs.

If you have any questions, please call Joyce Newland, Planning & Environmental Specialist, at (317) 226-5353 or e-mail at joyce.newland@dot.gov.

Sincerely,



Richard J. Marquis
Division Administrator

Enclosures



IN REPLY REFER TO:

United States Department of the Interior

OFFICE OF THE SECRETARY
Office of Environmental Policy and Compliance
Custom House, Room 244
200 Chestnut Street
Philadelphia, Pennsylvania 19106-2904

February 12, 2014

9043.1
ER 15/0027

Rick Marquis
Division Administrator
Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Dear Mr. Marquis:

The Department of the Interior (Department) has reviewed the Final Section 4(f) Evaluation for the State Boulevard Reconstruction Project, Fort Wayne, Allen County, Indiana. The Department offers the following comments and recommendations for your consideration:

Section 4(f) Comments

This document considers effects to four identified properties in the project study area eligible to be considered under Section 4(f) of the Department of Transportation Act of 1966 (codified at 49 U.S.C. 303§ 771.135) associated with the State Boulevard project. The State Boulevard project extends from Spy Run Avenue (U.S. 27 northbound) to Cass Street within the city of Fort Wayne, Indiana; it is intended to reduce existing congestion and improve traffic flow. Currently State Boulevard is a 4-lane road from east of Maplecrest Road to Spy Run Avenue, then reduces to 3 lanes west of Spy Run Avenue. East of Clinton Street, State Boulevard is a 2-lane road with 1 travel lane in each direction. To the east of the project area, Goshen Road merges into State Boulevard that has the effect of doubling the daily traffic volume.

The draft section 4(f) evaluation, prepared by Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA), considered the impacts to three properties eligible for the National Register of Historic Places. It considered a portion of the Fort Wayne Park and Boulevard System Historic District, which includes the system of 11 parks, 4 parkways, and 10 boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The draft 4(f) also considered impacts to a portion of the Brookview-Irvington Park Historic District containing 424 contributing resources including houses, garages, and the combined plats of the district, as well as the Bridge over Spy Run, a

reinforced concrete girder, T-Beam bridge constructed in 1927, determined eligible on its own. The draft 4(f) evaluation also considered impacts to Vesey Park operated by the City of Fort Wayne Parks Department; it includes green space along Spy Run between Eastbrook Drive and Westbrook Drive. The park features open space with areas for picnicking and views to Spy Run Creek.

The Department concurred with the FHWA and the INDOT on a determination of no feasible or prudent alternative to the preferred alternative, if built as proposed, which would result in impacts to eligible properties. We had previously withheld our concurrence with the measures to minimize harm to the historic properties because there was no evidence that all parties, including the State Historic Preservation Officer, had agreed to the mitigation measures, nor was there evidence in the evaluation that the Memorandum of Agreement (MOA) been signed. A copy of the signed and executed MOA has since been provided to us and we hereby remove our objections.

The Department has a continuing interest in working with the FHWA and the INDOT to ensure impacts to resources of concern to the Department are adequately addressed. For issues concerning section 4(f) resources, please contact Regional Environmental Coordinator Nick Chevance, Midwest Regional Office, National Park Service, 601 Riverfront Drive, Omaha, Nebraska 68102, telephone 402-661-1844.

We appreciate the opportunity to provide these comments.

Sincerely,

A handwritten signature in black ink, appearing to read 'Lindy Nelson', with a stylized flourish extending to the right.

Lindy Nelson
Regional Environmental Officer

cc: NPS, Chevance



U.S. Department
of Transportation

Federal Highway
Administration

Memorandum

Subject: 4(f) Legal Sufficiency Review
State Boulevard Project
City of Fort Wayne
Allen County, Indiana

Date: February 13, 2015

From: Richard J. Marquis 
Division Administrator

In Reply Refer To:
HDA-IN

To: Glen Harris
Assistant Chief Counsel
4749 W. Lincoln Highway (RTE 30)
Suite 600
Matteson, IL 60443

The Indiana Division Office is requesting a 4(f) legal sufficiency review of the State Boulevard Added Travel Lanes Project. The individual 4(f) document has been electronically provided to you via the US Department of Transportation File Transfer Protocol site.

On February 13, 2015, the Department of Interior concurred with that there are no feasible or prudent alternatives to the proposed use of the 4(f) resources and that efforts to minimize harm have been taken. The DOI's letter is attached your information.

If you have any questions, please direct them to Joyce Newland, Planning & Environmental Specialist, 317-226-5353 or joyce.newland@dot.gov.

Attachment

cc: Hazem Isawi





U.S. Department
of Transportation

**Federal Highway
Administration**

Memorandum

Subject: Statement of Legal Sufficiency
Des. No.: 0400587
State Boulevard Project
Fort Wayne, Allen County, Indiana

Date: February 18, 2015

From: Hazem Isawi
Attorney Advisor (HCC-MW)

In Reply Refer To: HCC-MW

To: Richard J. Marquis
Division Administrator (HDA-IN)

I have reviewed the above-referenced Final Section 4(f) Evaluation. The various comments submitted on the draft appear to be satisfied. I find the document legally sufficient.



AMERICAN
STRUCTUREPOINT
INC.

January 16, 2013

Mr. Steve McDaniel
Deputy Director of Parks
705 East State Boulevard
Fort Wayne, Indiana 46805

Re: Section 4(f) Coordination
State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Project No. IN20071404

Dear Mr. McDaniel:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle Map, in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet.

The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes, while correcting the sub-standard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center, 2-way, left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

In order to construct the project, right-of-way will need to be acquired from Vesey Park located southeast of the existing State Boulevard Bridge. The existing State Boulevard Bridge will be removed as part of the project and a new bridge constructed on new alignment. Removal of the existing bridge will require the acquisition of 0.12 acres of temporary right-of-way for construction and grading. Approximately 0.55 acre of land is proposed to be acquired in the location of the new bridge over Spy Run Creek. Approximately 0.021 acre is proposed to be acquired along the western edge of Eastbrook Avenue on the north side of existing State Boulevard. An additional 0.033 acre is proposed to be acquired for an underground storm sewer outfall east of Westbrook Avenue. During design, efforts will be made to minimize the amount of right-of-way to be acquired. No impacts are anticipated to park equipment or features.



AMERICAN STRUCTUREPOINT, INC.

Mr. Steve McDaniel

January 16, 2013

Page 2

Because Vesey Park is significant and publically owned, it is considered a Section 4(f) resource. Analysis of potential impacts to Section 4(f) resources is required for any federally funded transportation project. The Federal Highway Administration (FHWA) has developed guidance for projects that do not have significant impacts to Section 4(f) properties. Impacts evaluated under this guidance are termed *de minimis* if they meet certain criteria.

The following criteria must apply for a project to be considered to have *de minimis* impacts to a section 4(f) resource.

1. An official or officials with jurisdiction over the resource must agree in writing that the proposed project will not adversely affect the activities, features, and attributes that qualify the resource for protection under Section 4(f).
2. The transportation use of the Section 4(f) resource, together with any impact avoidance, minimization, and mitigation or enhancement measures incorporated into the project, does not adversely affect the activities, features, and attributes that qualify the resource for protection under Section 4(f).
3. The official(s) with jurisdiction over the property are informed of FHWA or Federal Transit Administration (FTA) intent to make the *de minimis* impact finding based on their written concurrence that the project will not adversely affect the activities, features, and attributes that qualify the property for protection under Section 4(f).
4. The public has been afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the Section 4(f) resource.

The project fulfills or will fulfill all criteria listed above and is not expected to adversely affect the Section 4(f) property. Per criteria Item 1 above, if you agree the proposed State Boulevard project will not adversely affect the Section 4(f) resource under your jurisdiction, we respectfully request your written concurrence. Your written statement of approval must be obtained in order to complete the Section 4(f) *de minimis* evaluation and will be included in the environmental documentation for the project.

Project mapping has been included with this letter for your reference. Please feel free to provide your correspondence via standard mail, email, or fax as you would prefer. To facilitate the development of this project, your timely cooperation will be greatly appreciated.

Please contact me at (317) 547-5580 or cmeadeor@structurepoint.com if there are any questions or if additional information is needed.

Very truly yours,
American Structurepoint, Inc.



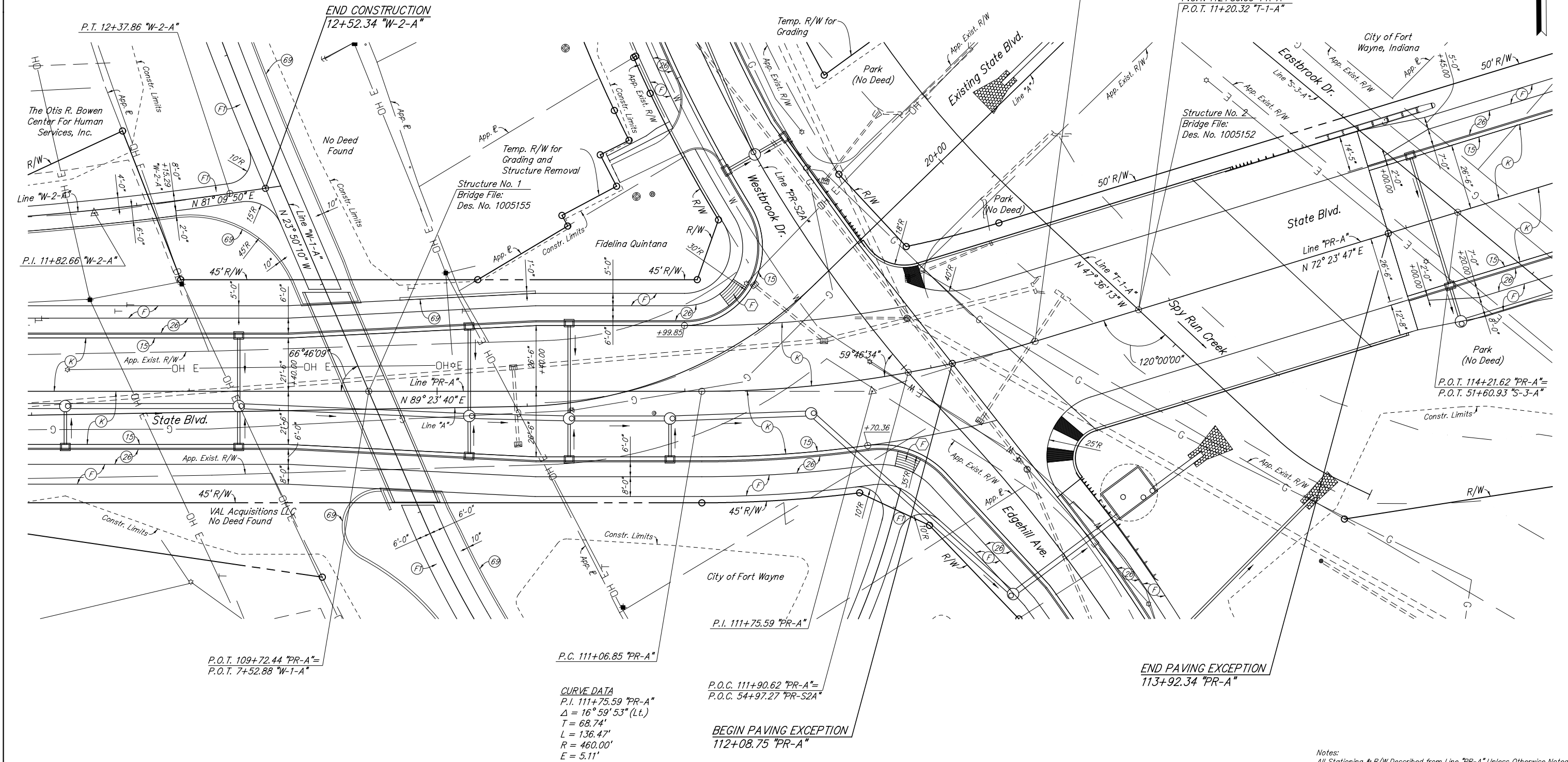
Christine Meador
Environmental Scientist

CAM:alo

Enclosures

CURVE DATA
P.I. 11+82.66 "W2A"
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T = 55.27'
L = 110.46'
R = 1,300.00'
E = 1.17'

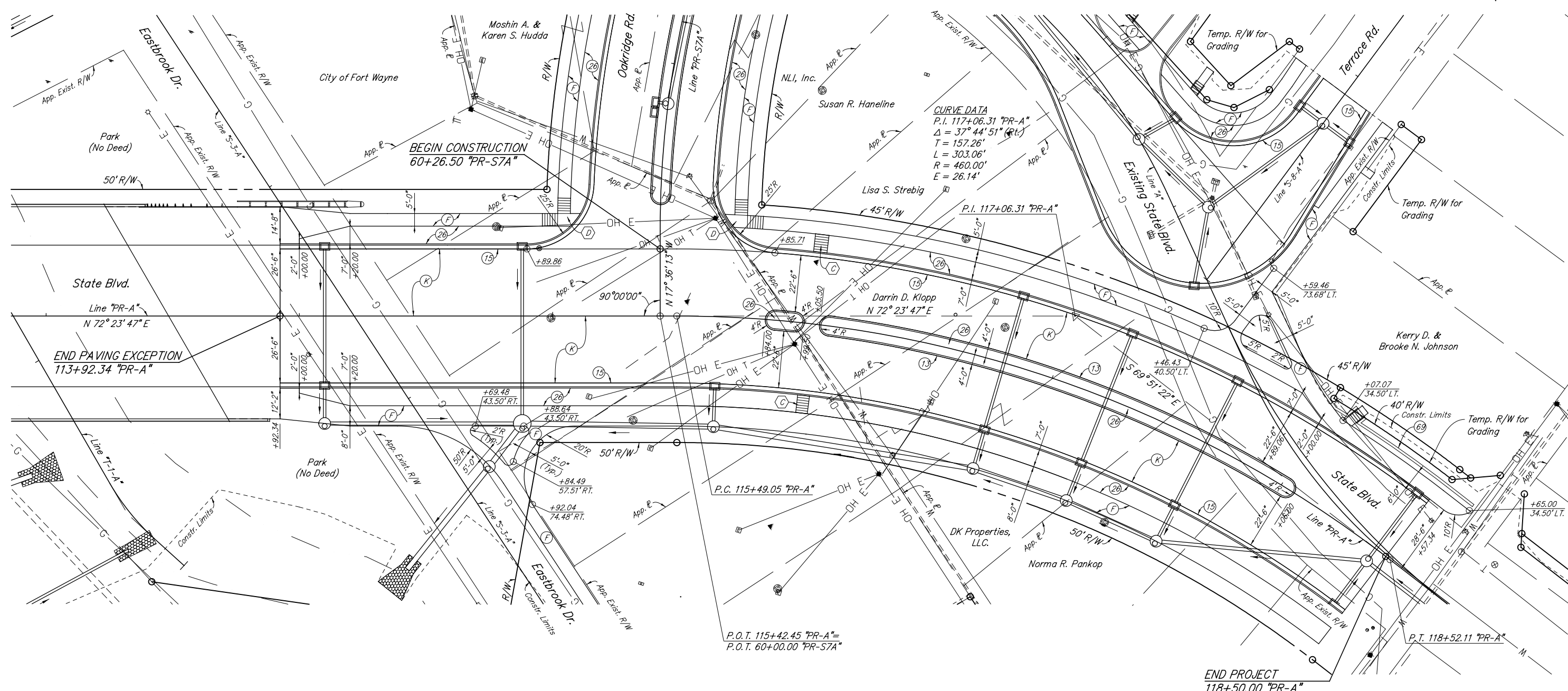
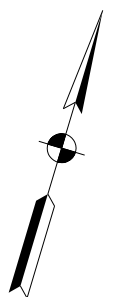
SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>(C) PCCP for Approaches, 6"</p> <p>(C1) PCCP for Approaches, 9"</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"</p>		<p>LEGEND</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</p> <p>(F) Concrete Sidewalk, 4"</p> <p>(F1) Shared Use Path Pavement (See Typical Sections)</p> <p>(V) Compacted Aggregate, No. 53</p>		<p>(K) Full Depth Pavement (See Typical Sections)</p> <p>(13) Concrete Curb</p> <p>(14) Integral Concrete Curb</p> <p>(15) Curb and Gutter, Concrete, Modified</p> <p>(22) Concrete Center Curb, Type "D"</p>		<p>(26) Sodding, Nursery</p> <p>(28) Mulched Seeding, Type "U"</p> <p>(69) Retaining Wall</p> <p>(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)</p> <p>Center Curb Removal Limits</p>		<p>RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE</p> <p>DESIGNED: ASU DRAWN: PJT</p> <p>CHECKED: SMC CHECKED: SMC</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS LINE "PR-A"</p>		<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE NONE</p> <p>SURVEY BOOK CONTRACT R-33130</p>		<p>BRIDGE FILE DESIGNATION NO. 1005154</p> <p>SHEETS 38 of 138 PROJECT NO. 1005154</p>	
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SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

LEGEND	
(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Shared Use Path Pavement (See Typical Sections)
	(V) Compacted Aggregate, No. 53
(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(14) Integral Concrete Curb	(69) Retaining Wall
(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

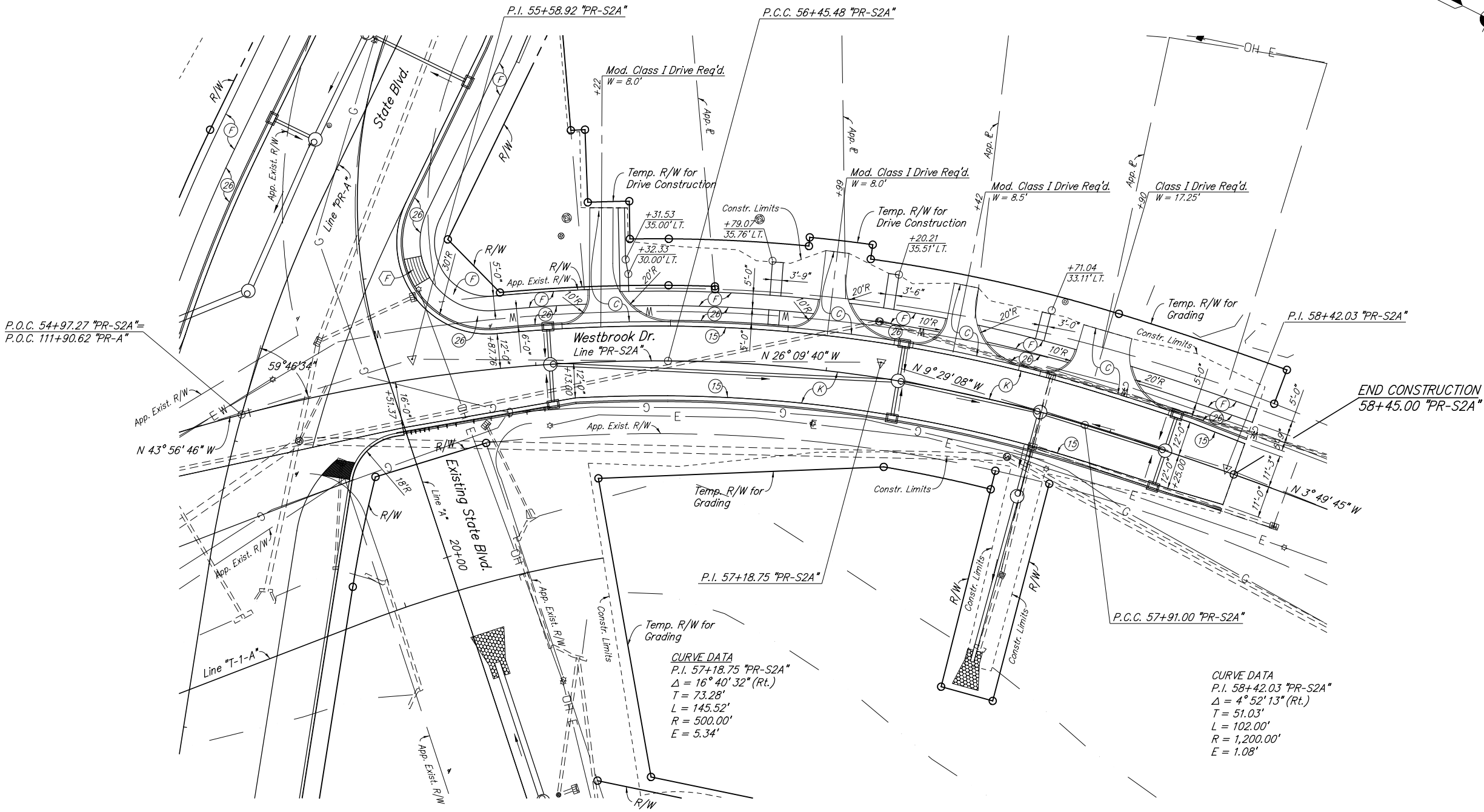
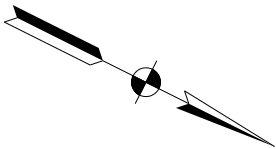
RECOMMENDED FOR APPROVAL _____		
DESIGNED: ASU	DRAWN: PJT	DATE _____
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS
CONTRACT R-33130	39 of 138 PROJECT NO. 1005154

CURVE DATA
P.I. 55+58.92 "PR-S2A"
 $\Delta = 17^\circ 47' 06"$ (Rt.)
T = 87.97'
L = 174.53'
R = 562.26'
E = 6.84'

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(14) Integral Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"				
	(F) Shared Use Path Pavement (See Typical Sections)	(15) Curb and Gutter, Concrete, Modified	(69) Retaining Wall	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
	(V) Compacted Aggregate, No. 53	(22) Concrete Center Curb, Type "D"		
				Center Curb Removal Limits

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK			
CHECKED: SMC		CHECKED: SMC			

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-S2A"	

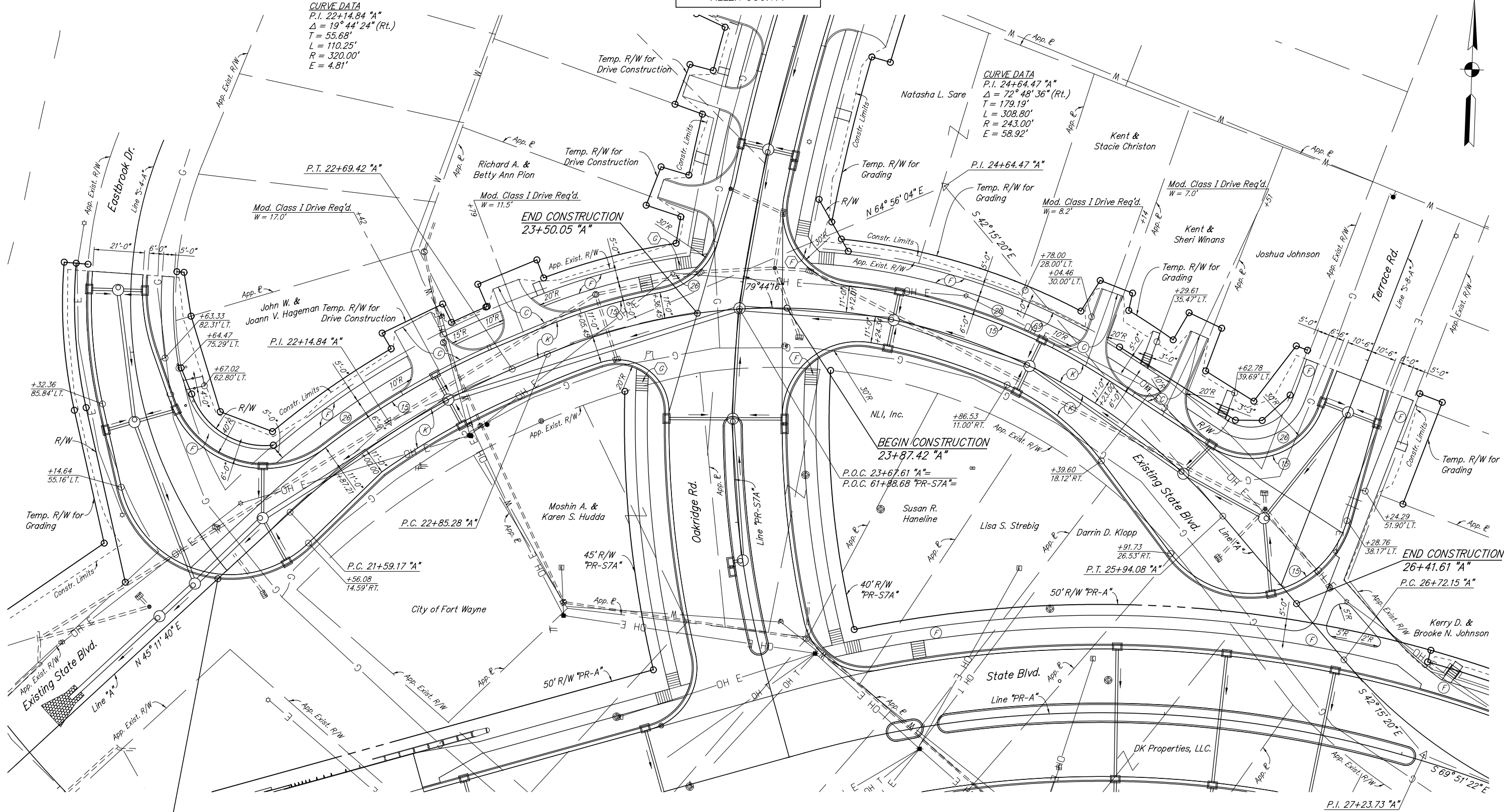
HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
VERTICAL SCALE NONE		DESIGNATION NO. 1005154	
SURVEY BOOK		SHEETS	
CONTRACT R-33130		42 of 138	PROJECT NO. 1005154

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

25

CURVE DATA
P.I. 22+14.84 "A"
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L = 110.25'
R = 320.00'
E = 4.81'

CURVE DATA
P.I. 24+64.47 "A"
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T = 179.19'
L = 308.80'
R = 243.00'
E = 58.92'



BEGIN CONSTRUCTION
21+18.36 "A"

END CONSTRUCTION
23+50.05 "A"

BEGIN CONSTRUCTION
23+87.42 "A"

END CONSTRUCTION
26+41.61 "A"

Notes:
All Stationing & R/W Described from Line "A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C1) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F1) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall
	(V) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU DRAWN: CAK

CHECKED: SMC CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS
CONTRACT R-33130	44 of 138 PROJECT NO. 1005154

Attachment 4-67 of 68



FORT WAYNE
PARKS AND
RECREATION

Play well...Live well



January 23, 2013

Christine Meador
Environmental Scientist
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

Re: Section 4(f) Coordination
State Blvd Reconstruction
Fort Wayne, IN 46805
Des. No. 0400587
Project Number - IN20071404

Dear Ms Meador:

The staff of the Fort Wayne Parks and Recreation Department has reviewed your packet dated January 16, 2013 with regards to improvements proposed for State Boulevard Reconstruction in Fort Wayne, Indiana. It appears that there will not be any impact to the recreational activities, features and attributes of the land currently being used as recreational space.

If you have any questions of the Fort Wayne Parks and Recreation Department regarding the Park land in the scope of work, please feel free to contact me at your earliest convenience.

Sincerely,

Steve McDaniel
Deputy Director of Park Maintenance
Fort Wayne Parks and Recreation Department
705 E. State Blvd.
Fort Wayne, IN 46805

Cc: Al Moll, Director of the Fort Wayne Parks and Recreation
Shan Gunawardena, City of Fort Wayne Engineer

Attachment 5

**Official Public Hearing Transcript and the disposition of the comments received
(Certification of Public Involvement) – Pages 1-198**



AMERICAN
STRUCTUREPOINT
INC.

September 12, 2014

Ms. Mary Wright, Public Hearings Examiner
INDOT - Hearing Section
Indiana Government Center North, Room N642
100 North Senate Avenue
Indianapolis, Indiana 46204

Re: Request for Public Hearing Certification
Des No. 0400587
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana

Dear Ms. Wright:

We are transmitting herewith the following materials in support of our request for certification of public hearing requirements.

1. Legal notice of public hearing
2. Publisher's affidavit from The News-Sentinel
3. Publisher's affidavit from The Journal Gazette
3. Complete mailing list, including property owners
4. Hearing sign-in sheets
5. Presentation slides from the public hearing
6. Information packet distributed at the public hearing
7. Transcribed verbal comments
8. Written public comments
9. Summary of verbal comments at the public hearing and responses
10. Cover page of the Environmental Assessment Des. No. 0400587

The legal notice of public hearing was published in *The News-Sentinel* and *The Journal Gazette* on June 4, 2014, and again on June 11, 2014. The advertised date of the hearing was Wednesday June 18, 2014 at 6 p.m. at North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana.

Seventy-Seven people signed in. A total of twenty-two people provided verbal formal public comment. The opportunity for additional written comment was given with an established deadline of July 18, 2014. Forty additional written comments were submitted. A summary of all comments and responses is included with this letter.

Please contact me at (317) 547-5580 or by e-mail at bhope@structurepoint.com, if there are any questions or if additional information is needed.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BWL:cgh

Enclosures

Legal Notice
Of
Public Hearing

The City of Fort Wayne will hold a Public Hearing on Wednesday, June 18, 2014, beginning at 6:00 p.m., at the North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana for the proposed State Boulevard Reconstruction Project between Spy Run and Cass Street, Fort Wayne, Allen County. An open house session will take place from 6:00 p.m. until 6:30 p.m. with the formal presentation beginning at 6:30 p.m.

The purpose of the public hearing is to offer all interested persons an opportunity to comment on the environmental document, 800.11(e) documentation for Section 106, and preliminary design plans for the proposed project.

The City of Fort Wayne is developing a federal-aid project to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River. This project, which begins at Cass Street and extends east to Spy Run, has an approximate length of 2,370 feet.

The proposed project involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits.

A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

The proposed project would require an estimated 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an “adverse effect” finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273).

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard.

The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. As part of the Indiana Historic Bridge Inventory project, the bridge was determined to be Non-Select. The bridge has been marketed for re-use for the past six months and information about the bridge can be found on the INDOT Historic Bridge Marketing website: <http://www.in.gov/indot/2532.htm>. The removal or demolition would be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The public hearing will be the last opportunity for a responsible party to step forward and provide the necessary sureties to obtain ownership of the bridge.

In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration’s “adverse effect” finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA’s “adverse effect” finding there is a Section 4(f) use. An Individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute’s preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be *de minimis* with regard to Section 4(f).

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation have agreed this project falls within the guidelines of an Environmental Assessment (EA) document.

The Public Hearing will consist of an informal Q&A session involving the project management team and a formal presentation regarding the project. Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

All comments collected before, during, and for a period of 30 days after the hearing will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

Before and after the formal hearing, the EA document and the 800.11(e) documentation for Section 106 will be available for review. Anyone interested in talking to the engineers about the project may do so before or after the formal hearing. Conversations prior to or after the formal hearing will not be part of the official record.

A copy of the EA document and the 800.11(e) documentation for Section 106 are available for viewing at the following locations on or after June 4, 2014.

- Allen County Public Library, 900 Library Plaza, Fort Wayne, Indiana 46802. Phone: (260) 421-1200
- Allen County Public Library, 2201 Sherman Boulevard, Fort Wayne, Indiana 46808. Phone (260) 421-1335
- Fort Wayne Transportation Engineering Department, Citizens Square, Ste. 210, 200 East Berry Street, Fort Wayne, Indiana 46802. Phone: (260) 427-1356

In accordance with the “Americans With Disabilities Act”, if you have a disability for which the City of Fort Wayne needs to provide accommodations, please call Briana Hope at American Structurepoint, Inc., at (317) 547-5580, by Monday, June 16, 2014.

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled “Early Coordination, Public Involvement and Project Development,” and the Indiana Public Involvement Manual approved by the Federal Highway Administration, US Department of Transportation, on April 1, 2009.

Please direct any questions or comments concerning this project to Briana Hope, American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256. (317) 547-5580, bhope@structurepoint.com. Comments on the proposed project will be accepted for 30 days after the Public Hearing. Comments or concerns brought forth by the public during this process will be addressed in the FONSI request document submitted to the FHWA.

The Journal Gazette

Allen County, Indiana

Account # 1064183 - 1127918

American Structurepoint Inc**PUBLISHER'S CLAIM****LINE COUNT**

Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of the type in which the body of the
advertisement is set) -- number of equivalent lines

Head -- number of lines

Body -- number of lines

Tail -- number of lines

Total number of lines in notice

395

COMPUTATION OF CHARGES198 lines, 2 column(s) wide equals395 equivalent lines at \$ 0.619 cents per line

\$ 244.51

Additional charges for notices containing rule or tabular work
(50 per cent of above amount)

122.26

Charge for extra proofs of publication
(\$1.00 for each proof in excess of two)

-

TOTAL AMOUNT OF CLAIM\$ 366.77**DATA FOR COMPUTING COST**

Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 2

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing
account is just and correct, that the amount claimed is legally due, after allowing all just credits,
and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width
and type size, which was duly published in said paper 2 times.

The dates of publication being as follows:

6/4/20146/11/2014

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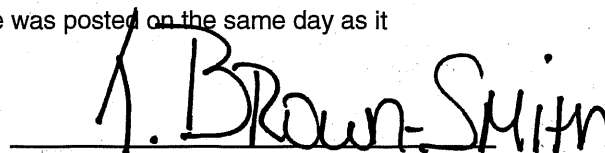
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Additionally, Newspaper has a Web site and this public notice was posted on the same day as it
was published in The Journal Gazette.



T. Brown-Smith
Legal Clerk

Date: June 11, 2014

ATTACH COPY OF ADVERTISEMENT HERE

benefit training schedules



Diana Parker | The Journal Gazette
e Cole of Kendallville fix
r training schedule.

Makes 2 servings.

Meal 3

2 to 3 pounds beef roast
4 to 5 potatoes, peeled
and cut into large chunks
1 pound carrots, cut into
large chunks

¼ to ½ onion, cut into
chunks

Place meat and vegeta-

bles in a slow cooker. Cook
on low 8 to 10 hours. Makes
2 to 3 servings.

Cook's Corner is a weekly
feature. If you know of
someone to be profiled,
write to Cook's Corner, The
Journal Gazette, P.O. Box
88, Fort Wayne, IN
46801-88; fax 461-8648 or
email dparker@jg.net.

Makes 2 servings.

Meal 2

2 boneless chicken
breasts, cooked on the grill
or baked in oven)

1 (2-ounces) instant
mashed potatoes, cooked
according to package direc-
tions

1 (15-ounces) canned
beans

1 (15-ounces) canned
carrots

Optional small salad

Legal Notice Of Public Hearing

The City of Fort Wayne will hold a Public Hearing on Wednesday, June 18, 2014, beginning at 6:00 p.m., at the North Side High School located at 475 East State Boulevard, Fort Wayne, Indiana for the proposed State Boulevard Reconstruction Project between Spy Run and Cass Street, Fort Wayne, Allen County. An open house session will take place from 6:00 p.m. until 6:30 p.m. with the formal presentation beginning at 6:30 p.m.

The purpose of the public hearing is to offer all interested persons an opportunity to comment on the environmental document, 800.11(e) documentation for Section 106, and preliminary design plans for the proposed project.

The City of Fort Wayne is developing a federal-aid project to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River. This project, which begins at Cass Street and extends east to Spy Run, has an approximate length of 2,370 feet.

The proposed project involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits.

A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

The proposed project would require an estimated 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an "adverse effect" finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273).

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the

NOTICE TO CONTRACTORS

NOTICE is hereby given by the Board of Public Works of the City of Fort Wayne, Indiana, that sealed proposals will be received by the Board of Public Works at its office located at Suite 40, Citizens Square, 200 East Berry Street until 8:30 A.M. on WEDNESDAY, 6/18/14 and in Council Chamber, Suite 035 Garden Level, Citizens Square until 8:30 to 9:00 A.M. Eastern Standard Time, on WEDNESDAY, 6/18/14 for the following:

CONTRACT/RESOLUTION NUMBER:
12285
FAIRFIELD/EWING/SUPERIOR
ROUNDAABOUT CONSTRUCTION AND
MAIN STREET

The contractor shall furnish all labor, insurance, equipment, materials and power for the complete performance of the following project:
FAIRFIELD/EWING/SUPERIOR
ROUNDAABOUT CONSTRUCTION AND
MAIN STREET as follows: That it is deemed necessary to improve the project corridor in accordance with the bid documents and drawings.

It is in accordance with the Contract and Documents and Drawing No. ST-2285 Sheets 1 THRU 234 prepared by AMERICAN STRUCTUREPOINT, INC. of the City of Fort Wayne, Indi-

No bid will be accepted from, or contract awarded to any person, firm, or corporation that is in arrears to the City of Fort Wayne, Indiana, upon any debt or contract, or, who has failed to execute, in whole or in part in a satisfactory manner, any contract with the City; or, who is a defaulter as to surety or otherwise, upon any obligation to the City of Fort Wayne, Indiana.

Bidders must obtain copy of said bid documents pursuant to submitting their bids, by paying \$100.00 for each set of bid documents, not refundable at the Office of the Board of Public Works, Suite 240, Citizens Square.

No bid may be withdrawn after the scheduled time for receipt of bids for at least one hundred twenty (120) days to allow review of bids before announcing award of contract. The successful bidder will be required to furnish a satisfactory Labor and Material Payment Bond and Performance Bond each in the amount no less than one hundred (100%) percent of the contract price.

The Board reserves the right to reject any and all bids for failure to comply with applicable laws and/or with the Instructions to Bidders. The Board also reserves the right to waive any defect in any bid.

BOARD OF PUBLIC WORKS
Robert Kennedy, Chairman
Kumar Menon, Member
Mike Avila, Member

ATTEST: Victoria Edwards, Clerk
6-4-11 1128012 hspaxlp

re in...

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Services

book!



feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard.

The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. As part of the Indiana Historic Bridge Inventory project, the bridge was determined to be Non-Select. The bridge has been marketed for re-use for the past six months and information about the bridge can be found on the INDOT Historic Bridge Marketing website: <http://www.in.gov/indot/2532.htm>. The removal or demolition would be consistent with the Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges (Historic Bridge PA). The public hearing will be the last opportunity for a responsible party to step forward and provide the necessary sureties to obtain ownership of the bridge.

In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration adverse effect finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA adverse effect finding there is a Section 4(f) use. An individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be de minimis with regard to Section 4(f).

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation have agreed this project falls within the guidelines of an Environmental Assessment (EA) document.

The Public Hearing will consist of an informal Q&A session involving the project management team and a formal presentation regarding the project. Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

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6-4-11 1127918 hspaxlp

The News-Sentinel

Allen County, Indiana

Account # 1064183 - 1127918
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-

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Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 2

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I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 3 times.

The dates of publication being as follows:

6/4/20146/11/2014

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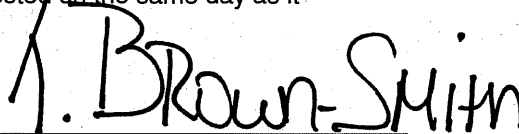
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T. Brown-Smith
Legal Clerk

Date: June 11, 2014

ATTACH COPY OF ADVERTISEMENT HERE

talks tactics



Associated Press file photo
Jozy Altidore, left, and Clint
Beckham are to tinker with his formations.

Ghana — which eliminated the U.S. from the last two World Cups — then play Portugal and Cristiano Ronaldo six days later. The Americans close the first round against three-time champion Germany.

"You can win a tournament like Spain did two years ago, play a 4-6-0, leave all the strikers out and have midfielders that score all the goals," Klinsmann said. "Systems in the near future have no meaning anymore, really."

Beckham hints he might play again

From The Associated Press

LONDON — David Beckham says he might come out of retirement and play soccer again.

The former England captain, who retired last year after a final stint at Paris Saint-Germain, said in a BBC documentary he had "a tough time" after hanging up

his cleats and is tempted to come out of retirement.

The 39-year-old Beckham, who is trying to start a Major League Soccer team and build a stadium in Miami, said "there's never been a player-owner, but maybe?"

Beckham traveled into the Amazon rainforest to make the documentary, which airs Monday.

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The proposed project would require an estimated 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an "adverse effect" finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273).

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7 feet) at its eastern end.

NOTICE TO PUBLIC & BIDDERS Advertisement to Bid

The Fort Wayne-Allen County Airport

the Airport Authority, Suite 209, 3801 W. Ferguson Road, Fort Wayne, Indiana, 46809 for the following:

**SMITH FIELD AIRPORT
Taxiway H Construction Project
Project No. AIP-15**

One set of bid forms, specifications and related legal documents may be ordered or downloaded through Eastern Engineering at www.easternengineering.com. You may also contact Eastern at 260-426-3119. A nonrefundable purchase price of \$85.00 dollars payable to Eastern Engineering must be received before documents will be released via hard copy. A fee of \$37.00 will be charged per download.

Bids will be publicly opened and read aloud at 11:15 a.m. eastern standard/daylight time on the last date for receiving bids in the office shown above. Bids shall be properly executed and addressed to the office and address shown above, together with the documents required by the bid forms, specifications and related legal documents. A bid bond with good and sufficient surety, issued by a company licensed to do business in the State of Indiana, or, in the alternative, a certified check on a solvent bank, equal to ten percent (10%) of the total bid insuring that if the bid is accepted, a contract will be entered into and the performance of its proposal secured, must accompany the Contractor's bid.

It is the policy of the Authority to ensure that Disadvantaged Business Enterprises DBEs, as defined in Part 26, have an equal opportunity to receive and participate in DOT-assisted contracts. It is also our policy:

1. To ensure nondiscrimination in the award and administration of DOT assisted contracts;
2. To create a level playing field on which DBEs can compete fairly for DOT assisted contracts;
3. To ensure that the DBE Program is narrowly tailored in accordance with applicable law;
4. To ensure that only firms that fully meet 49 CFR Part 26 eligibility standards are permitted to participate as DBEs;
5. To help remove barriers to the participation of DBEs in DOT assisted contracts; and
6. To assist the development of firms that can compete successfully in the market place outside the DBE Program.

A 10.5% Disadvantaged Business Enterprise participation goal has been set for the DOT assisted portion of this project.

Wage rates shall not be less than the prevailing wage rates as determined pursuant to the Wage Rate Decision of the Secretary of the U.S. Department of Labor establishing the minimum wage rates which the Contractor shall pay skilled and unskilled labor.

The Bidder must supply all the information required by the bid or proposal form.

A pre bid conference will be held in the terminal at Smith Field Airport, 902 W. Ludwig Road, Fort Wayne, Indiana 46825 on June 10, 2014, at 1:00 p.m., eastern time.

The Fort Wayne Allen County Airport Authority reserves the right to reject any and/or all bids and to waive any irregularities in the bidding procedure.

Robin R. Strasser, CPA
Director of Administration & Finance
Fort Wayne Allen County
Airport Authority
6-2-9 1126460 hspaxlp

tributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

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In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e), and 800.5(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at the locations referenced below. This documentation serves as the basis for the Federal Highway Administration adverse effect finding. The views of the public on this finding are being sought.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA adverse effect finding there is a Section 4(f) use. An individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be de minimis with regard to Section 4(f).

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Fort Wayne Transportation Engineering Department, Citizens Square, Ste. 210, 200 East Berry Street, Fort Wayne, Indiana 46802. Phone: (260) 427-1356

In accordance with the Americans With Disabilities Act if you have a disability for which the City of Fort Wayne needs to provide accommodations, please call Briana Hope at American Structurepoint, Inc., at (317) 547-5580, by Monday, June 16, 2014.

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled Early Coordination, Public Involvement and Project Development, and the Indiana Public Involvement Manual approved by the Federal Highway Administration, US Department of Transportation, on April 1, 2009.

Please direct any questions or comments concerning this project to Briana Hope, American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256. (317) 547-5580, bhope@structurepoint.com. Comments on the proposed project will be accepted for 30 days after the Public Hearing. Comments or concerns brought forth by the public during this process will be addressed in the FONSI request document submitted to the FHWA.

6-4-11 1127918 hspaxlp

Public Hearing Mailing List

Contact Name	Agency Company	Department	Address 1	Address 2	City	State	Zip code
City of Fort Wayne Board of Public Works			1 East Main Street		Fort Wayne	IN	46802
Kroger Limited Partnership I			5960 Castleway West Drive		Indianapolis	IN	46250
J & J Realty Co.			1800 Magnavox Way		Fort Wayne	IN	46804
Daniel J. & Karen S. Hall			19 EMS T32 Lane		Leesburg	IN	46538
DK Properties LLC			142 E State Blvd		Fort Wayne	IN	46805
Darrin D. Klopp			142 E State Blvd		Fort Wayne	IN	46805
Lisa S. Strebeg			138 E State Blvd		Fort Wayne	IN	46805
Susan R. Haneline			134 E State Blvd		Fort Wayne	IN	46805
NLI Inc.			1616 N Harrison St		Fort Wayne	IN	46808
Moshin A. & Karen S. Hudda			112 E State Blvd		Fort Wayne	IN	46805
Val Acquisition Co LLC			7108 Covington Rd		Fort Wayne	IN	46804
Charles G. & Amanda S. Kimani			2230 Cass Street		Fort Wayne	IN	46808
Michael Huy Taing			301-303 W State Blvd		Fort Wayne	IN	46808
Richard A. & Betty Ann Pion			111 E State Blvd		Fort Wayne	IN	46805
Nancy E. Leitch C/O Aquarius IV LLC			2811 E State Blvd		Fort Wayne	IN	46805
Mookel T Enterprises LLC			335 E State Blvd		Fort Wayne	IN	46805
Donald B. Fisher			1639 Cape Coral Parkway E Ste 208		Cape Coral	FL	33904
Betty Marvel			315 E State Blvd		Fort Wayne	IN	46805
Richard L. & Michael L. Summers			311 E State Blvd		Fort Wayne	IN	46805
John D. & Sharon Hartman			11821 Linden Grove Dr		Fort Wayne	IN	46845
Michael G Hinter & Eric L. Hathaway			175 E State Blvd		Fort Wayne	IN	46805
Kerry D. & Brooke N. Johnson			2302 Terrace Rd		Fort Wayne	IN	46805
Joshua Johnson			145 E State Blvd		Fort Wayne	IN	46805
Joshua T. Geary			141 E State Blve		Fort Wayne	IN	46805
Kent & Stacie Christon			137 E State Blvd		Fort Wayne	IN	46805
Natasha L. Sare			2302 Oakridge Rd		Fort Wayne	IN	46805
John W. & Joann V. Hageman			327 Dunwood Dr		Fort Wayne	IN	46805
Fidelina Quintana			2221 Westbrook Dr		Fort Wayne	IN	46805
Fort Wayne City of Department of Redevelopment			1 E Main Street RM 840		Fort Wayne	IN	46802
Philip M. Miller			3212 N Clinton St		Fort Wayne	IN	46805
Otis R Bown Center for Human Services Inc.			850 N Harrison St		Warsaw	IN	46580
Anchor Indiana IV LLC Walgreen Co. Attn: Tax Dept			300 Wilmot Dr		Deerfield	IL	60015
City of Fort Wayne, Indiana			One Main Street RM 350		Fort Wayne	IN	46802
John & Marlene Slate			215 W State Blvd		Fort Wayne	IN	46805
Norma Pankop			154 E State Blvd		Fort Wayne	IN	46805
Robert & Annette Janice Dailey			162 E State Blvd		Fort Wayne	IN	46805
	Allen County Board of Commissioners		Citizens Square, Suite 410	200 East Berry Street	Fort Wayne	IN	46802
	City Council Administrator		Citizens Square, Suite 120	200 East Berry Street	Fort Wayne	IN	46802
Mayor Tom Henry	City of Fort Wayne		Citizens Square	200 East Berry Street	Fort Wayne	IN	46802
Palermo Galindo	City of Fort Wayne		Citizens Square	200 East Berry Street	Fort Wayne	IN	46802
Mr. Mitch Zoll	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	402 West Washington Street, W274		Indianapolis	IN	46204
Ms. Angie Quinn	ARCH, Inc.		818 Lafayette Street		Fort Wayne	IN	46802
Ms. Jill Downs	ARCH, Inc.		1202 Elmwood Avenue		Fort Wayne	IN	46805
Mr. Michael Galbraith	ARCH, Inc.		818 Lafayette Street		Fort Wayne	IN	46802
Mr. Don Orban	Fort Wayne Historic Preservation Commission		Citizens Square - Suite 320	200 East Berry Street	Fort Wayne	IN	46802
Mr. Todd Zeiger	Indiana Landmarks	Northern Regional Office	402 West Washington		South Bend	IN	46601
Ms. Julie Donnell	Friends of the Parks of Allen County		PO Box 10152		Fort Wayne	IN	46850
Ms. Michelle Briggs-Wedaman	Brookview Neighborhood Association		2326 Eastbrook Drive		Fort Wayne	IN	46805
Dr. James L. Cooper			629 East Seminary Street		Greencastle	IN	46135
Mr. Paul Brandenburg	Indiana Historic Spans Task Force		5868 Croton Circle		Indianapolis	IN	46254
Mr. Shan Gunawardena	City of Fort Wayne		Citizens Square - Suite 200	200 East Berry Street	Fort Wayne	IN	46802
Ms. Susan Haneline	Adjacent Property Owner		134 East State Boulevard		Fort Wayne	IN	46805
Mr. Charley Shirmeyer	Northside Galleries		335 East State Boulevard		Fort Wayne	IN	46805
Mr. Karl Dietsch			2313 Oakridge Road		Fort Wayne	IN	46805
Mr. Dan Avery	Northeastern Indiana Regional Coordinating Council		Citizens Square - Suite 230	200 East Berry Street	Fort Wayne	IN	46802
Ms. Suzanne Slick	Irvington Park Neighborhood Association		3318 Garland Avenue		Fort Wayne	IN	46805
Ms. Annette "Jan" Dailey	Graduate Student, IPFW		162 East State Boulevard		Fort Wayne	IN	46805
Ms. Joyce Newland	Federal Highway Administration		Federal Office Building, Room 254	575 North Pennsylvania Street	Indianapolis	IN	46204

Contact Name	Agency Company	Department	Address 1	Address 2	City	State	Zip code
Mr. Larry Heil	Federal Highway Administration		Federal Office Building, Room 255	576 North Pennsylvania Street	Indianapolis	IN	46204
Mr. John H. Shoaff	Fort Wayne City Council		4646 West Jefferson Boulevard		Fort Wayne	IN	46804
Mr. Jason Kaiser	INDOT, Fort Wayne District		5333 Hatfield Road		Fort Wayne	IN	46808
Mr. Patrick Carpenter	INDOT, Cultural Resources		100 North Senate Avenue, N642		Indianapolis	IN	46204
Ms. Mary Kennedy	INDOT, Cultural Resources		100 North Senate Avenue, N642		Indianapolis	IN	46204
Mr. Creager Smith	Fort Wayne Office of Planning and Policy		Citizens Square - Suite 320	200 East Berry Street	Fort Wayne	IN	46802
Mr. Albert Cohan	Westbrook 5, LLC		15490 Ventura Boulevard, Suite 200		Sherman Oaks	CA	91403
Mr. Thomas M. Niezer	Barrett & McNagny, LLP		215 East Berry Street		Fort Wayne	IN	46802
Mr. Ronald Ross	Martin Riley Architects and Engineers		221 West Baker Street		Fort Wayne	IN	46802
Mr. Dan Ernst	Earth Source, Inc.		14921 Hand Road		Fort Wayne	IN	46802
Mr. John Carr	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	402 West Washington Street, W274		Indianapolis	IN	46204
Ms. Amanda Ricketts	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	402 West Washington Street, W274		Indianapolis	IN	46204
Mr. Wade Tharp	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	402 West Washington Street, W274		Indianapolis	IN	46204
Mr. Tom Cain	Fort Wayne Redevelopment Department		Citizens Square - Suite 320	200 East Berry Street	Fort Wayne	IN	46802
Linda Weintraut, Ph.D.	Weintraut & Associates, Inc		4649 Northwestern Drive		Zionsville	IN	46077
Ms. Najah Duvall-Gabriel	Advisory Council on Historic Preservation		1100 Pennsylvania Avenue NW, Suite 803	Old Post Office Building	Washington	DC	20004
Mr. Greg Smith	INDOT, Fort Wayne District		5333 Hatfield Road		Fort Wayne	IN	46808

State Boulevard Reconstruction Project
PUBLIC HEARING SIGN-IN SHEET
Wednesday, June 18, 2014
North Side High School, Fort Wayne, Indiana

<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Chad Eviston	3355 Eastbrook Dr Fort Wayne IN 46805	chad.eviston@gmail.com
Creager Smith	200 E. Berry Ste. 320 FW 46802	creager.smith@cityoffortwayne.org
Bob Kennedy	200 E BERRY ST DIRECTOR OF PUBLIC WORKS	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Ben Gerald Brown	2115 Edgemoor Ave	gbdesign@flash.net

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
KARL DIETSCH	2313 OAKRIDGE ROAD	KDIETSCH@COMCAST.NET
Amy WORMAN	2302 OAKRIDGE	ASWORMAN@gmail.com
CATHERINE DIETSCH	2313 OAKRIDGE R	
BOB BAKER	3221 RIVER FOREST FW	BOBANITA1739@MSN.COM
(Audry Stevenson) Audry Stevenson	1515 Dodge Ave FW	gaudie1515@gmail.com
Darrell Henline ^{Classic Stereo} and video	2312 N Clinton St 46805	darrell.henline@classicstereotv igot2sing igot2sing@aol.com
TOM SHOAFF	2130 Forest PK Blvd	Tmshoaff@gmail.com

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Randy Spicth	WANE-TV	
Rachel Banks	Wane TV	
Peggy Roy	2121 Skyhawk Dr. 46815	psr42@yahoo.com
LINDA KAISEA	1029 CURDES AVE 46805	lindaskaiser@gmail
Bill Stephan	2100 Goshen AVE ^{FW} FW	bill.stephan@barnicenter.org
DON BODERER	1508 ARCHER AVE FW IN	DONBODERER@FRONTIER.COM
CYNTHIA KELLER	1916 CRESCENT	Cinkeller@Frontier.com

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Julianne Fleischer	2201 Cass St	juliemp42@yahoo.com
Tim Hall	1004 CASS ST.	tim@fwoutfitters.com
Phil McCague	2020 Cortland Ave	Phillie@yahoo.com
MICHELE JONES	2001 BAYER AVE	Joneswhorses@yahoo.com.
Kent & Stacie Christon	137 E. State Blvd	staciechriston@comcast.net
JILL DOWNS	1202 ELMWOOD AVE. ^{FW} 46805	andyandjill@juno.com
MARTY BENDOR	FT. WAYNE CITY COUNCIL AT LARGE	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
MATT JONES	2001 Bayer Ave. Ft. Wayne, IN	jones4horses@yahoo.com
SCOTT SIMMONS	2732 EASTBROOK DR	4552202@AOL.com
Mark Hefty	2702 Florida Dr	
Pat Landon	1320 Shoreview Dr	
DARRIN KLOPP	146 E-STATE BLVD.	
JOHN SHOUFF	3604 S. WASH. RD. - 02	jhshuff jhshuff@propanetwest.com
Rachel Bateman	1805 Crescent Ave 46805	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
BILL HARTMAN	9541 MAJOR WOODS ROAD FORT WAYNE IN 46804	bill.hartman@co.alen.in.us
Betty Pion (Pion)	111 East State Blvd	
DAVE & LINDA KLOTZ	175 E. STATE	linda.lousfurniture@yahoo.com
Janet Schasker	2013 Alabama Ave	JSchasker@Frontier.com
Nigel Perry	2134 754 Cass St 46808	No Computer
Del	9400 Lake av	
MARK A. Andrews	2227 Westbrook Drive	atk60ssharf@yahoo.com

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
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HB Walker	1012 Kenwood Ave.	
Joyce Newland	575	joyce.newland@dot.gov
Susan Handline	134 E. State Blvd	susan.handline@gmail.com
Will Wendling	2201 N. Anthony Blvd.	Fortwayneww@aol.com
Bob & Joy Ward	2227 N. Clinton ST	JoyWARD@yahoo.com
Mimi Strong	2535 WESTBROOK DR	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
John D Hartman	2234 N. Clinton St	Jhartman@northsideplumbingf.w.com
Fidelina Quintana	2221 Westbrook Dr	
KEUT CHRISTON	137 E STATE	
Jernelle + Fred Webb	Nature Corner Antiques 2307 Spy Run Ave	F.J.WEBB@FRONTIER.COM
Nancy & Earl Wass	2410 St Marys Av	
Russell Jeli	City Council	
Rob Sitoff	2551 WESTBROOK DR.	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Michelle Briggs Wedaman Mike Wedaman	2326 Eastbrook Dr.	mbwedaman@frontier.com
MIKE MCKINLEY	2252 EASTBROOK DR	mmckayak@aol.com
Lorraine Neier	2526 West Dr	herb101@frontier
Jon Slate	215 West State	INKSPOT.PRINTING 2@frontier.com
Karen Schwichtenberg	3450 River Forest Dr	kenlight.karen@gmail.com
Mark Handy	2335 Oakridge Rd	mrhandy0614@msn.com
Kathleen Martinez	517 Charlotte Ave.	kjmartinez1008@gmail.com

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
Stephanie Mays	2240 Eastbrook Drive	mommays97@gmail
Sally Kainer	537 Charlotte Ave	
Phil Hutson	2100 Goshen Road Fort Wayne, IN 46808	phil.hutson@bowencenter.org
Peggy Brady	2314 Eastbrook Dr 46805	pbbbrady@frontier.com
Dave Messmann	2121 Edgemoor Ave - 46805	powersmessmann@yahoo.com
HERBERT WELER	2526 WEST DRIVE 46805	herbnelor@frontier.com
STEVE ROBINSON	2130 FLORIDA DR 2130 46805	

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<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
MICHAEL GALBRAITH	818 LAFAYETTE, FW, IN, 46802	MGALBRAITH@ARCHFW.ORG
Karen Davis (Davis)	city of FW.	karadavis@cofw
PATRICK ZAHARAKO	COFW	patrick.zaharako@cofw.org
Julie Peebles	1621 magnolia Ln Fort Wayne, IN 46825	julieann1979@frontier.com
JED SWAIDNER	524, LAWTON (46805) PLACE	N/A
WFF T		
Tom Cain	1301 W. Jefferson Bl. FW 46802	tom.cain@cityoffortwayne.org



State Boulevard Reconstruction Project
PUBLIC HEARING SIGN-IN SHEET
Wednesday, June 18, 2014
North Side High School, Fort Wayne, Indiana

7:40 PM
Wet to learn W

<u>NAME</u>	<u>ADDRESS</u>	<u>E-MAIL</u>
EARL BRAUNLIN, III	717 West Jefferson 46802	226-3494 No computer
Danielle Tuck	2315 Terrace Rd 46805	danielle.tuck@frontier.com
REGINALD CONVERSE	905 COLUMBIA AVE 46805	
Michael Lianez	6925 Hillside Oaks Dr	elackworkgreen@gmail.com

State Boulevard Reconstruction Project

SPEAKER SIGN-IN SHEET

**Wednesday, June 18, 2014
North Side High School, Fort Wayne, Indiana**

NAME

ADDRESS

✓	KARL DIETSCH (DEECH)	2313 CAMBRIDGE ROAD
✓	KARL DIETSCH (DEECH)	ON BEHALF OF 3 PERSONS
✓	Jon Slate (Jon Slate)	215 W 56th
0	Earl BRAWNLIN, mp. (Earl Brawnlin)	717 West Jefferson
✓	HERB WEIER	8526 WEST DR 46805
*✓	JOHN SHIOFF	CITY COUNCILMAN AT-LARGE

State Boulevard Reconstruction Project

SPEAKER SIGN-IN SHEET

Wednesday, June 18, 2014
North Side High School, Fort Wayne, Indiana

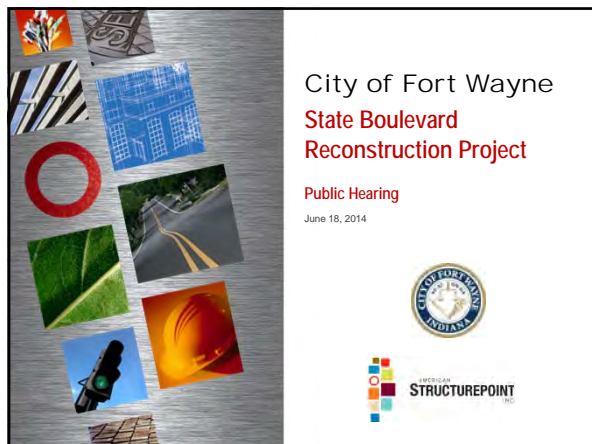
NAME

ADDRESS

✓ Darrell Henline, Controller Classic Stereo	2312 N Clinton St 46805
✓ Laurie Weier (Weer)	2526 West Dr 46805
✓ H B Walker	1012 Kenwood Ave.
✓ Susan Henline (Haneline)	134 E. State Blvd.
✓ Matt Jones Michele	2001 Bayer Ave. Ft. Wayne, 46805
✓ Mark Hefty	2702 Florida Dr
hank mdures ✓ MARK A. Andrews	2227 westbrook Dr.
✓ MICHAEL GALBRATH	818 LAFAYETTE, FW, IN, 46802

Impromptu Speakers

-
- Julie Peoples
- Tom SHORFF
- Todd Emsw?
- Danielle Tuck
- Peggy Grady
- Robert Shorff
- Julie D^{UN}hill?
- Peggy Roy
- John Hartman?



Public Hearing Purpose

- » **Presentation of proposed project**
 - Project description
 - Right-of-way impacts
 - Anticipated project costs and schedule
 - Land acquisition process
- » **Present Environmental Document**
 - Impacts to Natural and Human Environment
- » **Individual Comment Period**

AMERICAN
STRUCTUREPOINT

Project Team

- » **City of Fort Wayne**
 - Shan Gunawardena, City Engineer
 - Patrick Zaharako, Assistant City Engineer
- » **American Structurepoint**
 - Richard Zielinski, PE, Project Development Director
 - Scott Crites, PE, Project Manager
 - Briana Hope, Environmental Specialist
- » **Public Officials**

AMERICAN
STRUCTUREPOINT

Legal Notice of Public Hearing

- » **Public notice of hearing was advertised in the *Journal Gazette* and *News-Sentinel* on June 4th and 11, 2014**
- » **A copy of the Legal Notice was mailed to as many affected property owners as possible**
- » **Mailing list sign-in sheet**

AMERICAN
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Documents Available for Review

- » **Environmental Document/Section 106**
 - Viewing locations:
 - City of Fort Wayne Transportation Engineering Department**
Citizens Square
200 E. Berry Street
Fort Wayne, Indiana 46802
 - Allen County Public Library, Main Library**
900 Library Plaza
Fort Wayne, Indiana 46802
 - Allen County Public Library, Little Turtle Branch**
2201 Sherman Boulevard
Fort Wayne, Indiana 46808
- www.cityoffortwayne.org

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STRUCTUREPOINT

Public Comment Process

- » **Formal public statements - tonight**
- » **Comment sheet in information packet**
 - Mail comments to American Structurepoint
 - Comment deadline – July 18, 2014
- » **Comment to project representatives after formal hearing is complete**
- » **All Comments will be addressed in writing**

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STRUCTUREPOINT

Formal Public Statements

- » Will be recorded and transcribed for public record
- » Public Statements/Comments will not be addressed tonight as part of this hearing but will be addressed in writing as part of the environmental process
- » Public Notice will advertise availability of responses to public comments
- » Talk with project team after formal hearing

STRUCTUREPOINT

Location Map



STRUCTUREPOINT

Purpose and Need for Improvements

- » **Need - Deficiencies**
 - Substandard roadway geometrics & sight distance
 - Curves are too tight for travel speed and unsafe
 - Curves are too tight for appropriate sight distance
 - Existing traffic congestion
 - Intersections not operating efficiently – traffic back ups
 - Inconsistent number of lanes through overall corridor
 - Existing Roadway Flooding
 - Lack of connectivity between existing and proposed trail systems

STRUCTUREPOINT

Purpose and Need for Improvements

- » **Purpose**
 - Improve safety for motorist, pedestrians, bicyclists
 - Address substandard sight distance
 - Improve connectivity for motorists
 - Improve connectivity pedestrians/bicyclists
 - Address roadway flooding concerns
 - Alleviate traffic congestion

STRUCTUREPOINT

Alternatives Studied

- » **Do-Nothing Alternative**
 - Do-nothing alternative does not meet the purposed and need
- » **Alternative 1 – Butler Rd. – Vance Rd. Corridor**
 - 3.25 total miles of roadway construction required
 - 2.25 miles on new alignment, 2 miles through existing forested land
 - 125 residential relocations, 15 commercial relocations
 - Results in impacts to the Brookview-Irvington Historic District, Vesey Park, and Franke Park
 - Does not address the purpose and need
 - Does not address connectivity along State Blvd corridor, substandard horizontal curve, roadway flooding
- » **Alternative 2 – Spring St. – Tennessee Ave. Corridor**
 - 1.5 total miles of roadway construction required
 - 0.6 miles on new alignment
 - 75 residential relocations, 15 commercial relocations
 - Results in impacts to the Historic Tennessee Ave. Bridge, Science Central Museum, Lakeside Park, and Lawton Park
 - Does not address any of the purpose and need
 - Does not address connectivity along State Blvd corridor, substandard horizontal curve, roadway flooding

STRUCTUREPOINT

Alternatives Studied – 1 & 2



STRUCTUREPOINT

Alternatives Studied

- » **Alternative 3B – Widen State Blvd. on Existing Alignment**
 - Addresses flooding, congestion, and connectivity
 - 0.51 total miles of roadway construction required
 - Impacts additional properties along the north side of the roadway compared to preferred alternative
 - 18 residential relocations
 - This alternative requires a higher number of historic property relocations as compared to other alternatives
 - Does not address the purpose and need
 - Does not address substandard horizontal curve and safety issues resulting from substandard sight distance and roadway geometrics.

STRUCTUREPOINT

Alternatives Studied

- » **Alternative 3C – Shift State Blvd. Alignment South**
 - Addresses safety, congestion, and roadway flooding
 - 9 residential relocations, 4 commercial business relocations
 - Introduces new intersection at State Blvd and Clinton St. creating new operational and safety issues due to proximity of new Terrace Road intersection
 - Increases required bridge length approx. 250 ft.
 - Creates operational and safety concerns with Clinton St. intersection, increased cost, and increased commercial property impacts
 - Project cost estimated at \$5 million more than any other alternative

STRUCTUREPOINT

Alternatives Studied – 3C



STRUCTUREPOINT

Alternatives Studied

- » **Alternative 3D – Substandard Horizontal Curve Correction with a 3-Lane Typical Section**
 - Addresses some safety concerns, substandard geometrics, and flooding
 - Would result in similar property impacts even though footprint is reduced 10 ft on each side of the roadway
 - Would not alleviate traffic congestion at intersections where 4 lanes of traffic transitions into 2 lanes
 - Connectivity would not be fully addressed

STRUCTUREPOINT

Alternatives Studied – 3D



STRUCTUREPOINT

Alternative 3A - Recommended

- » **Alternative 3A – Substandard Horizontal Curve Correction with a 5-Lane Typical Section**
 - Addresses the Purpose and Need to the fullest extent
 - Congestion, connectivity, substandard geometrics & sight distance, flooding, safety
 - Minimizes impacts to cultural resources, residential and commercial relocations
 - Consistent with transportation planning

STRUCTUREPOINT

Project Description

- » **Realign State Blvd. to correct substandard S-curves between Westbrook Dr. and Terrace Rd.**
 - Safety, congestion, and sight distance
- » **Add lanes to be consistent with overall corridor**
 - Safety and congestion
- » **Construct new bridge over Spy Run Creek**
 - Grade of bridge will be raised, new bridge with appropriate waterway opening will alleviate flooding
- » **Construct pedestrian bridge over State Blvd along with sidewalk connections to the Pufferbelly Trail**
 - Improve pedestrian connectivity, connect bicyclists and pedestrians to Greenway system

STRUCTUREPOINT

Project Description

- » **Extend Oakridge Rd. to the new State Blvd alignment**
 - Eastbrook Dr. and Terrace Rd. will connect to Oakridge Rd. via. the existing State Blvd. roadway
 - Eastbrook Dr. will be removed south of State Blvd.
- » **Landscaping and Lighting**

STRUCTUREPOINT

Recommended Alternative – 3A



STRUCTUREPOINT

Project Description

- » **Pavement reconstruction from Wells Street to Spy Run Avenue**
 - Phase 1 – Clinton Street to Spy Run Avenue
 - Two 11-foot travel lanes each direction
 - 12-foot center left turn lane
 - Phase 2 – Wells Street to Clinton Street
 - Two 10-foot travel lanes each direction
 - Dedicated center left turn lane where required
 - Center landscaped median where left turn lanes are not required
 - Storm sewer, curb and gutter, decorative lighting, landscaping
- » **Pedestrian facilities**
 - 8-foot sidewalk on the south side of State Blvd.
 - 5-foot and 6-foot sidewalk on the north side of State Blvd
 - Pedestrian bridge over State Blvd at the existing railroad crossing
 - Sidewalk connections to Pufferbelly Trail

STRUCTUREPOINT

Project Description



STRUCTUREPOINT



STRUCTUREPOINT

Project Description

- » **Maintenance of Traffic**
 - Maintain one lane of traffic in each direction
 - Maintain access to all properties during construction
- » **Design Criteria**
 - Design speed – 35 mph
 - Posted Speed – 30 mph
 - 80% federally funded
 - Design follows all federal, state, and local guidelines
- » **Landscaping & Lighting**
 - Trees
 - Green Space
 - Ornamental Lighting
 - Landscaped Center Median

STRUCTUREPOINT

Project Description



STRUCTUREPOINT

Project Description



STRUCTUREPOINT

Project Description



STRUCTUREPOINT

Project Costs and Schedule

- » **Project Costs**
 - Approximately \$11.4 million
 - Includes design, R/W, and construction
- » **Project Schedule**
 - Phase 1 - Clinton To Spy Run - Anticipated to begin construction in spring of 2017
 - Phase 2 – Wells to Clinton - Anticipated to begin construction in spring of 2018

STRUCTUREPOINT

New Right-of-Way Requirements

- » **Land Use**
 - Mix of residential, commercial, and recreation
- » **Relocations**
 - 15 residential relocations are anticipated

STRUCTUREPOINT

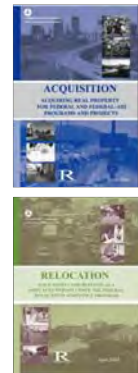
New Right-of-Way Requirements

Land Use Impact	Amount (acres)
Residential	2.19
Commercial	1.06
Agricultural	0
Recreation Area	0.55
Wetlands	0
Other	0
TOTAL	3.80

STRUCTUREPOINT

Land Acquisition Process

- » **How land is purchased by local agencies**
 - Explains the process of buying property needed for highway improvements
- » **Relocation Assistance Program**
 - Explains the process of relocation
- » **Land Acquisition Process must follow the Uniform Act of 1970**



STRUCTUREPOINT

Land Acquisition Process

- » **“Uniform Act” of 1970**
 - All federal, state, and local governments must comply
 - Requires just compensation
- » **Acquisition process**
 - Appraisals
 - Review appraisals
 - Negotiations

STRUCTUREPOINT

Land Acquisition Process

- » **Amount of compensation cannot be less than fair market value**
- » **Partial acquisition**
 - Agency will state amount to be paid of the part of the land to be acquired.
 - Separate amount will be stated for damages to the portion retained.
 - If remainder has little or no value, agency will consider purchasing remainder.

STRUCTUREPOINT

Land Acquisition Process

- » **Agreement**
 - When agreement is reached, owner will be asked to sign an option to buy, purchase agreement, easement, or deed.
- » **No Agreement**
 - Mediation
 - Condemnation

STRUCTUREPOINT

Environmental Assessment

- » **An Environmental Assessment (EA) is prepared for actions in which the significance of the environmental impact is not clearly established**
- » **The EA examines potential social, economic, and environmental factors**
- » **The EA allows the FHWA and INDOT to determine whether or not an Environmental Impact Statement (EIS) is needed.**

STRUCTUREPOINT

Environmental Assessment

- » EA reviewed by INDOT and FHWA and approved the release for public involvement on May 14, 2014
- » A Finding of No Significant Impact (FONSI) is anticipated for the project
- » A FONSI is issued for a project if the undertaking is a major action but is determined to not result in a significant impact, based on the EA findings.

STRUCTUREPOINT

Environmental Assessment

Scope of Environmental Study

- » Evaluate impacts to natural and human environment
 - Waterways, wetlands, and endangered species
 - Historic properties
 - Social and economic factors

STRUCTUREPOINT

Environmental Assessment

Coordination and Community Involvement

- » 24 federal, state, and local agencies asked to provide input
- » Coordinated with several federal, state, and local historic preservation organizations
- » Public Hearing (June 18, 2014)
 - EA reviewed by INDOT/FHWA approved and released for public involvement on May 14, 2014

STRUCTUREPOINT

Environmental Assessment

Public Involvement

- » A total of 51 meetings have taken place
 - 5 Public Information Meetings
 - 13 Neighborhood Association Meetings
 - 3 Open House Events
 - 27 Other Group/Individual Meetings
 - 3 Consulting Party Meetings
 - Public Hearing tonight

STRUCTUREPOINT

Environmental Assessment

» Historic Properties

- State and local preservation organizations consulted
- Archaeological reconnaissance and Historic Properties investigation performed
 - Fort Wayne Park and Boulevard System Historic District (NRHP, 2010)
 - Brookview-Irvington Park Historic District (NRHP, 2011)

STRUCTUREPOINT

Environmental Assessment

» Historic Properties (Cont.)

- Bridge over Spy Run Creek (NBI No. 0200273)
 - o Eligible for listing on National Register of Historic Places
 - o Considered "Non-Select" Bridge
 - o consistent with the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA)
 - o Marketed for re-use for more than six months
 - o Last opportunity for a responsible party to step forward to obtain ownership of the bridge

STRUCTUREPOINT

Environmental Assessment

» Impacts to Historic Properties

- FHWA issued a determination of "Adverse Effect" on **February 27, 2013** for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273)
- Indiana State Historic Preservation Officer (SHPO) concurred on April 1, 2013.
- Views of the public being sought regarding the effect of the proposed project on the historic elements

STRUCTUREPOINT

Environmental Assessment

» "Adverse Effect" Finding

- "Adverse Effect" finding is based on visual and physical impacts to the identified resources
 - Acquisition of ROW from Districts
 - Alter historic locations
 - Removal on contributing resources
 - o Structures and landscapes
 - Create visual barrier
 - Bifurcation of the District
 - Introduction of new visual elements
 - o Pedestrian Bridge, access ramps, retaining walls
 - Alteration landscape and character of the setting

STRUCTUREPOINT

Environmental Assessment

» Resolving "Adverse Effects" to Historic Properties

- If an undertaking is determined to cause an "Adverse Effect" to a historic resource, then the undertaking must include measures to resolve and/or mitigate the "Adverse Effect"
- The specific measures to resolve and/or mitigate the "Adverse Effects" are stipulated in a "Memorandum of Agreement" (MOA)
- MOA is executed by FHWA, Indiana SHPO, INDOT, and City of Fort Wayne
- Signed MOA is then submitted to the Advisory Council on Historic Preservation

STRUCTUREPOINT

Environmental Assessment

» Proposed Mitigation (Draft MOA Stipulations)

- Context Sensitive Solutions
- Salvage architectural details from demolished homes
- Explore funding opportunities for low cost grants/loans to improve/rehabilitate historic resources within Historic District
- Advisory Team to ensure project is designed in a manner that respects historic qualities of the district
- Photographic documentation of the State Blvd bridge over Spy Run
- Photographic documentation of altered part of historic district
- Photographs put on DVD
- Addition of four interpretative plaques placed in accessible locations to be reviewed and acceptable by SHPO and Consulting Parties

STRUCTUREPOINT

Environmental Assessment

» Section 4(f) Resources

- Fort Wayne Park and Boulevard System Historic District
 - Individual Section 4(f) Evaluation performed
- Brookview-Irvington Park Historic District
 - Individual Section 4(f) Evaluation performed
- Bridge over Spy Run Creek
 - Historic Bridge PA
- Vesey Park
 - *de minimis* impact

STRUCTUREPOINT

Environmental Assessment

"Waters of the US"

» Wetland delineation complete

- One jurisdictional waterway
 - Spy Run Creek



STRUCTUREPOINT

Environmental Requirements

Permits Expected to be Required

- » Section 404 Permit RGP – US Army Corps of Engineers
- » Section 401 Water Quality Certification – Indiana Department of Environmental Management (IDEM)
- » Construction in a Floodway – Indiana Department of Natural Resources
- » Rule 5 Erosion Control – IDEM

STRUCTUREPOINT

Environmental Assessment

Traffic Noise Study

- » Noise Analysis performed to determine potential noise impacts from project
- » Project not anticipated to result in noise impacts
- » Noise analysis conducted according to INDOT Traffic Noise Policy
 - Reviewed by INDOT and determined to be technically sufficient on October 18, 2011

STRUCTUREPOINT

Public Comments

American Structurepoint, Inc.

- » 7260 Shadeland Station
Indianapolis, Indiana 46250
Attn: Briana Hope

Email

- » bhope@structurepoint.com

Respectfully request comments postmarked by

- » July 18, 2014

All substantial comments will be reviewed, evaluated, and given full consideration during the decision-making process.

STRUCTUREPOINT

Public Comments

Comment for Public Record

- » Come up to podium
- » State your name
- » State your comment
- » Speak clearly into microphone for ease of transcribing
- » Responses will be answered in subsequent project documentation

Please limit your comments to 2-3 minutes so all persons have an opportunity to speak and to be respectful of other people's time

STRUCTUREPOINT

PUBLIC HEARING

**State Boulevard Reconstruction Project
between Spy Run Avenue and Cass Street
Fort Wayne, Allen County, Indiana**

Designation No. 0400587

INFORMATION PACKET



AMERICAN
STRUCTUREPOINT
INC.



**Wednesday, June 18, 2014
Formal Presentation: 6:30 p.m.**

**North Side High School
475 East State Boulevard
Fort Wayne, Indiana**

Prepared By:

American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
(317) 547-5580



June 18, 2014

Dear Concerned Citizens, Local Residents, and Elected Public Officials:

Welcome to the City of Fort Wayne's public hearing regarding the proposed State Boulevard Reconstruction Project.

The purpose of the public hearing is to explain the proposed project and receive comments, concerns, and suggestions. We appreciate the opportunity to be here this evening and look forward to listening to the concerns and issues that are important to this community. Comments can be presented in several ways.

1. You may verbally express your comments here tonight during the public statement session held after the formal presentation. You may do this by signing the Speaker's Schedule located at the table with the handout materials. **All statements will be recorded and then transcribed in order to be included in the official public hearing transcript.**
2. You may complete one of the comment sheets (attached) and return utilizing one of the following methods below.
 - a. Hand deliver to one of the hearing representatives from American Structurepoint in attendance this evening
 - b. Mail to Briana Hope at American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, Indiana 46256
 - c. Fax to Briana Hope at (317) 543-0270
 - d. Email to bhope@structurepoint.com

You may mail your comments at any time during the next four weeks. **Should you choose to submit comments by mail, please have them postmarked by July 18, 2013.** All comments submitted will become part of the transcript and be addressed in subsequent project documentation, along with the verbal comments presented here tonight.

All substantial comments received will be evaluated and responded to in writing within subsequent project documentation. The documentation will address all concerns raised during the public hearing process and describe the final decisions reached following careful consideration of the views and concerns of the public.

The approved environmental document along with the 800.11(e) documentation for Section 106 is available for public review and inspection on the City of Fort Wayne's Website (<http://www.cityoffortwayne.org/publicworks/west-state-blvd-realignment.html>) and at the following locations.

**Allen County Public Library
900 Library Plaza
Fort Wayne, Indiana 46802**

**Allen County Public Library
2201 Sherman Boulevard
Fort Wayne, Indiana 46808**

**Fort Wayne Transportation
Engineering Department
Citizens Square, Ste. 210
200 East Berry Street
Fort Wayne, Indiana 46802**

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled "Early Coordination, Public Involvement and Project Development," and the Indiana Public Involvement/Public Hearing Procedures for Federal-Aid Project Development approved by the Federal Highway Administration, US Department of Transportation on July 8, 1997.



Hearing Agenda

American Structurepoint Staff will be available in the display area to explain proposed project details and environmental impacts before and after the official hearing.

- | | |
|---|--|
| 1. Meeting Called to Order | Richard Zielinski, PE
American Structurepoint |
| 2. Explanation of the Hearing Purpose and Process | Richard Zielinski, PE
American Structurepoint |
| 3. Project Description | Richard Zielinski, PE
American Structurepoint |
| 4. Environmental Document Approval | Briana Hope
American Structurepoint |
| 5. Public Statement Session | Richard Zielinski, PE
American Structurepoint |
| 6. Adjournment | Richard Zielinski, PE
American Structurepoint |
| 7. Questions and Answers – Engineers and project representatives will be available in the display area to answer questions. | |



Project Description

The City of Fort Wayne is developing a Federal-aid project to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River. This project, which begins at Cass Street and extends east to Spy Run, has an approximate length of 2,370 feet.

The proposed project involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek.

The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits.

A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which will be constructed by others.

The proposed project would require 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.



For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated construction cost of the project is \$10,372,000.

Description of Right-of-Way

The project will require the purchase of right-of-way. Refer to the pamphlets distributed at this meeting: ACQUISITION – “Acquiring Real Property for Federal and Federal-Aid Programs and Projects” and RELOCATION – “Your Rights and Benefits as a Displaced Person Under the Federal Relocation Assistance Program”. Acquisition and relocation information can also be viewed at www.fhwa.dot.gov/realestate.

It is anticipated the project will require 15 residential relocations, approximately 3.80 acres of permanent right-of-way and 2.50 acres of temporary right-of-way. The right-of-way to be acquired will be primarily residential.

Project Schedule

Milestone	Anticipated Construction Date
Phase 1 - Clinton Street to Spy Run Avenue	2017
Phase 2 – Wells Street to Clinton Street	2018

Estimated Project Cost Summary

The estimated total construction cost for the State Boulevard Reconstruction Project is \$10.4 million (2015). The total project cost estimate includes design, right-of-way acquisition costs, and construction costs.

Environmental Document Approval

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) have reviewed the Environmental Assessment (EA) prepared by American Structurepoint for this project and approved the document on May 14, 2014. The EA evaluates the impact of the roadway reconstruction project on the natural and human environment. No areas of potentially significant impacts have been identified. A Finding of No Significant Impact (FONSI) is anticipated for the project. A FONSI is issued for a project if the undertaking is a major action but is determined to not result in a significant impact based on the EA documentation.

The project has been coordinated with several state and local historic preservation organizations, including the State Historic Preservation Officer (SHPO), in compliance with FHWA procedures for Indiana projects. The proposed action impacts items listed on or eligible for inclusion on the National Register of Historic Places (NRHP). The FHWA has issued an “adverse effect” finding for the project due to impacts to the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010), Brookview-Irvington Park Historic District (NRHP, 2011) and the Bridge over Spy Run Creek (NBI No. 0200273). The State Historic Preservation Officer (SHPO) concurred with the finding on April 1, 2013.



If an undertaking is determined to cause an “Adverse Effect” to a historic resource, then the undertaking must include measures to resolve and/or mitigate the “Adverse Effect.” The specific measures to resolve and/or mitigate the “Adverse Effects” are stipulated in a “Memorandum of Agreement” (MOA). The MOA is executed by FHWA, Indiana SHPO, INDOT, and the City of Fort Wayne. A Draft MOA was prepared to outline the proposed ‘Adverse Effect’ the project would have on the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District and the proposed mitigation for those adverse impacts. The Bridge over Spy Run Creek falls within the scope of the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA); and therefore, does not require an MOA for the adverse effect the project will have on the resource. The Draft MOA was distributed to the IDNR-DHPA and consulting parties at the same time the Environmental Assessment was advertised as available for public review. Once the MOA is finalized and signed it will be forwarded to the Advisory Council on Historic Preservation for their information and record

The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7 feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District.

As mitigation for the impacts to the Fort Wayne Park and Boulevard System Historic District, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the present State Boulevard bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements.

The undertaking would require the removal of 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be



modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard.

As mitigation for the impacts to the Brookview-Irvington Park Historic District, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the present State Boulevard Bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements. In addition, the City of Fort Wayne would make an effort to salvage architectural details from homes demolished for use in other District residences, as well as explore funding opportunities to provide low cost grants/loans to improve/rehabilitate historic resources within the Brookview-Irvington Historic District.

The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. As part of the Indiana Historic Bridge Inventory project, the bridge was determined to be Non-Select. The bridge has been marketed for re-use for the past six months and information about the bridge can be found on the INDOT Historic Bridge Marketing website: <http://www.in.gov/indot/2532.htm>. The removal or demolition would be consistent with the Historic Bridge PA. The public hearing will be the last opportunity for a responsible party to step forward and provide the necessary sureties to obtain ownership of the bridge.

The Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek have been identified within the limits of the proposed project as Section 4(f) Resources. As a result of FHWA's "adverse effect" finding there is a Section 4(f) use. An Individual Section 4(f) evaluation has also been prepared documenting the preferred alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the identified 4(f) resources. Additionally, 0.55 acre of permanent right-of-way and 0.12 acre of temporary right-of-way will be acquired from Vesey Park, which is also a Section 4(f) resource. This impact has been determined to be *de minimis* with regard to Section 4(f).

In addition to the historic resources related to this project, the EA also considered a broad range of potential social, economic, and environmental factors. Twenty-four federal, state, and local agencies and individuals were contacted and asked to provide input regarding the project's impacts on areas in which they have either jurisdiction or special expertise.

A noise analysis was performed to determine potential noise impacts from the project. Based on the noise analysis, the project is not anticipated to result in noise impacts. The noise analysis was conducted according to INDOT Traffic Noise Policy. The Noise Analysis was reviewed by INDOT and determined to be technically sufficient on October 18, 2011.

A wetland delineation was completed and identified one jurisdictional waterway, Spy Run Creek. Minor impacts to Spy Run Creek are anticipated as a result of the proposed project. Implementation of appropriate best management practices should limit impacts to ecological resources. It is expected the

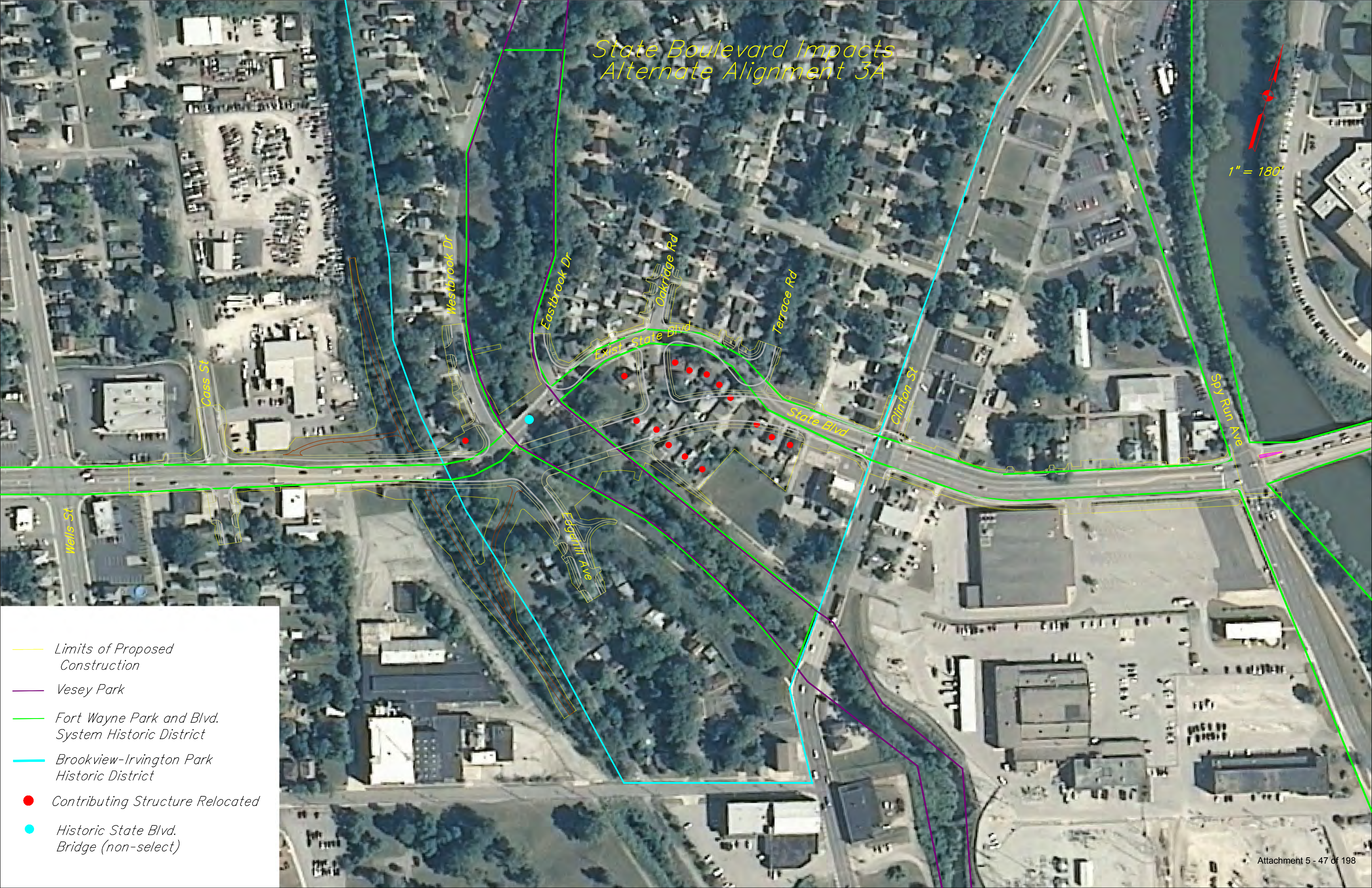


project will require a Section 404 permit issued by the US Army Corps of Engineers and a Section 401 Water Quality Certification from the Indiana Department of Environmental Management (IDEM) for potential impacts to Spy Run Creek. A Rule 5 Erosion Control Notice of Intent from IDEM will be required.

Significant efforts were made to engage and involve the public in the project planning process. Early coordination was initiated with representatives of the community. On multiple occasions the City of Fort Wayne met with neighborhood associations, business owners, adjacent property owners, and interested groups. The City met with these individuals to help explain the project, provide project updates, and address comments and concerns. Meeting with these groups, individuals, and representatives further helped the City ensure the public was involved in the planning process. In addition, five public information meetings and three open-house style public information meetings were conducted to further attempt to engage the public. Significant efforts were made to encourage participation in the meetings, including public notices and press releases published in the *Fort Wayne Journal Gazette*.

State Boulevard Impacts Alternate Alignment 3A

1" = 180'



- Limits of Proposed Construction
- Vesey Park
- Fort Wayne Park and Blvd. System Historic District
- Brookview-Irvington Park Historic District
- Contributing Structure Relocated
- Historic State Blvd. Bridge (non-select)

Public Hearing Transcription
Formal Verbal Public Comment
State Boulevard Reconstruction Project
Wednesday June 18, 2014
North Side High School, Fort Wayne, Indiana

John Shoaff - Ok, I want to make sure you hear me clearly without any problems. Okay, can we get one of those things for our City Council? Casual reader of the environmental assessment...inaudible...might be misleading of the impression of public neighborhood meetings cited in the document gave the public ample opportunity for significant input. This is not the case. The primary features...inaudible....and so many of our citizens strenuously object to were dictated by a request for proposal was sent out November 2007. It directed successful respondent to engineer a plan that would widen the existing 2 lanes of State Boulevard between Clayton and Cass Streets to 5 lanes. Structurepoint designed, as instructed and the public has never been shown any other possibilities. Instead the idea has been promoted that there has only been one solution to the various problems and there are some real problems in this area. Part of this fallacy, this is a fallacy is that 5 lanes are essential to alleviate the problems. This plan with 5 lanes to pass the increased beyond reason designed ramp speeds to 35mph, a speed almost certainly fatal to a stuck pedestrian. It is throwback to an earlier time and type of thinking. A 20th century dinosaur is being dragged into the 21st century where conditions are very different than they were thirty or forty years ago. Conditions have changed, our visions have changed, our priorities have changed. Our street system is under severe financial pressure. We have fifty to sixty million dollar backlog on much needed street repairs. Partly, this is because we have been told by experts and we are learning for ourselves how are roads are overbuilt. Revenues and back taxes that pay for these repairs are down because driving is down it has been dropping nationally since the year 2007. So we are in a squeeze. We should be looking for solutions to traffic problems and use the minimum amount possible of paving, not the maximum. What we build has to be repaired or rebuilt in twenty or twenty five years or so for the second time around, we pay for all of it all eventually. The economic consequences are made worse by the impact on property values. Residential values in the area will go down. If you doubt it, ask the 14 neighborhood residents who signed the letter opposing the project in its present form. There is however a most significant partnering change in recent years to which we can respond. It is a counter migration back into the city. This is something we must encourage in every possible way. I spent the morning in a planning session for downtown renewal. There we discussed the importance of protecting the power neighborhoods and the northside neighborhood was specifically named. A project like this one creates a corridor that planners call a barrier, separating the neighborhoods from downtown that weakness and desirability of living in them. It should not be allowed to go forward in the present form. Thank you.

Okay, our first speaker is Darrell Henline.

Darrell Henline – Classic Stereo Controller – To all who are in attendance, we are very much in favor of the proposed State Boulevard new construction project as stated in today's legal notice of public hearing and as the renderings reflect on the CityofFortWayne.org website. Classic Stereo has been located in the northeast corner of Clinton and State for 48 years. All of which the current owner, Phil Miller has been with the business. I've been with the company for over 36 years. Over these many years we have seen how the daily congestion, accidents, and flooding have adversely affect businesses and homeowners alike. With the addition of extra lanes in each direction and turn lanes as indicated, the design should smoothen the traffic flow to and through the intersections of State Boulevard and Spy Run and Clinton Street and other adjoining streets in the area. The dreaded morning and late afternoon-evening rushes should have shorter and fewer delay times at the traffic signals. The elimination of the snake curves and the installation of new street lighting should also provide healthier traffic conditions as motorists, bikers, and pedestrians are able to view the traffic clearer while entering or crossing State Boulevard. The project also appears to address in a positive manner, the flooding issues that have constantly bombarded the community. Adding the appeal of the new greeneries beautifies the driving through the area. We might also note that any discussion of cutting lanes, lane restrictions, or adding round-a-bouts would not address the needs and the goals that have been predetermined and deemed necessary in previous public hearings, as well as means help with the pertinent City departments. In fact, these ideas may actually be detrimental to and serve as an actual downgrade to current traffic and environmental conditions in the area. In conclusion, we are in desperate need of this proposal as stated in the legal notice of public hearing as the renderings reflect on the CityofFortWayne.org website. I might add that Phil Miller, president and owner of Classic Stereo and Video would like to formally apologize for not attending tonight's hearing. He had originally planned to participate in this discussion, but he's been working on a job site out of town the last few days and won't be able to be here tonight. Thank you for this public forum. Sincerely, Phil Miller, President, Owner of Classic Stereo, and Darrell Henline, Controller

Next is Laurie Weier.

Laurie Weier – I'm Laurie Weier and I live on West Drive very close to State. My concern in all of this is for that people who live along State Street all the way west and all the way east. If it becomes so convenient to go so fast and get where you're going in a big hurry, there's going to be a lot more accidents, there will be a lot more children who may be riding bicycles and all this traffic is going in such fast speeds. I would like to know if you're ever really going to control the traffic? The speed of the traffic? We already have a 35mph on East State near where I live in the East State Village area. It has never ever been monitored. I've never seen a car stopped for speed, so I guess what I'm talking mostly is the safety of the people who use the street and who live along the street and there are more important things than just getting where you want to go in two minutes. The quality of life around this street. State Street is a beautiful street. People live both east and west along the street and have loved it. I hope they will continue to do so because we will take control of what's going on and those five lanes. Thank you.

Good comments so far. Thank you. Next is H.B. Walker

Good evening everyone. **Howie Walker**. I live in the northside area but specifically in the area where some of you are effected by, so for that reason I just want to offer a couple of things as kind of an outsider viewpoint. Something to maybe think about. Maybe it's too late for it, but it seems that one of the main concerns is traffic and getting from point A to point B is always a big concern for everyone. Although as the lady just mentioned, there's more to life than just that too. It would be nice if we could have progress in Fort Wayne and at the same time not forget our history. One of the ideas that I'd like to propose at least if maybe you would consider an overpass that would allow traffic to move and especially emergency vehicles things like that and at the same token, allow the neighborhood to remain the neighborhood. Still have its historic routes, friendliness and charm. And also, it could be tied in, I can't remember the name of the street, but basically goes northwest and the overpass could be tied into Five Points going off Goshen Road and give a lot of access. At the same time it would give a pedestrian and bike route a lot more availability in the neighborhood. Also, I see it as an opportunity that Fort Wayne could really tie some other projects together if we could get some kind of a high speed transportation system to help with the traffic flow and maybe tie it into the coliseum for people that work there. Give them access to where they can leave their cars out away from the City. We could turn down town into a nice green area, a lot of bikes, golf carts, electric vehicles. And this would help create some revenue for the City. Because there could be tolls on those kind of vehicles, or renting bikes, or whatever the case is, it will certainly bring in more businesses downtown, and certainly, hopefully draw some more businesses too. And then also the river development. This is certainly going to affect that. We have some areas that are already pretty wide open, for example the old Kroger facility. Now there's a wide open parking lot there that could really host a lot of activities, art fairs, crafts, different festivals that would tie right in with the river venue. So State boarding tournaments, a lot of different things. I know that's not the issue tonight, but the same token, I hope that we can foresee in the future tying everything together in Fort Wayne and making our City a real City of progress. Change for the sake of change is not necessarily good change, but keeping everything the same does not allow for progress either. Thank you.

Thank you very much. Susan Haneline

Hi, I'm **Susan Haneline**. I am actually a homeowner involved in the actual footprint of the project. Our house in the 100 block of State, so this is a very personal topic for me. First of all I want to say that the vast majority of the residents living in that footprint support this project whole heartedly. We have come to every meeting, we have done everything that we can to make sure that everyone understands those of us that have a personal stake in this support it. If you don't believe there's a safety issue, I invite you to come sit on my front porch, hang out there, and you can see what a safety issue is. My daughter just turned 18 and is heading off to college in two months. She never in her life played in our front yard because it wasn't safe. You talk about speed limits, trucks, there's trucks that go down there all the time. It's a truck route for trucks that are making deliveries along State Boulevard. It always has been, It always will be. It's not safe in the matter that it is now. It's been 2-lane streets. The trucks come off up the street, they run over the sidewalks, again, it's not safe. These are the things that we're trying to get people understand. Flooding – huge issue. I just dumped thousands and thousands and thousands of dollars into my property several months ago due to flooding. This will allow, these improvements are

going to allow the water to move better through the area. It's going to make traffic through the area better. It's going to make it safer. You talk about environmental issues. Those of us that live there, that have to pay to have our homes professionally cleaned up after flooding can talk to you about environmental impact. The water is contaminated. We can't live in our homes until they are properly cleaned up. We pay extra money for flood insurance. It's a hardship for those of us living there. This is not a case of big Government coming in and saying we want to take these people's property. This is a case of these people saying please come in and help us. We support this project. We love the idea of more green space in the neighborhood. We love the idea of having our children to be able to play away from the main access of the road. The road itself doesn't particularly change the historic curb remains as what we consider old state or whatever you want to call it. You're backing the traffic away from the neighborhood. You're backing away from where the children play, the people walk, the people bicycle. Again I thank you for your time, and again know that there are many of us out here that attend these every single time, supporting and asking for speedy resolutions to this problem. Thank you.

Hello my name is **Mark Heffy**, I grew up at 2327 Oak Ridge Road. I used to rake leaves for Mr. Sickles as a kid. I believe the bridge project is needed, but I don't want anyone in the room to believe for one minute that it's going to alleviate the flooding problem. I really got upset when the zoo flooded last year. And the reason that happened when they raised the dykes years ago two feet, the one spot they didn't raise was the Lawton Park Dam and put in a pumping station big enough to accommodate all the new development up Highway 3 and the Tributary Ditch going up Bass Road to Thomas Road addition. Without that the homeowners on the north side of Eastbrook and Westbrook can expect continued flooding. Twice in my lifetime did water cross State Street from 1962 when I was born until I left for the service in 1983. I come back and it's a weekly occurrence, which the water it doesn't flow towards the rivers, it's backing up. And we finally flooded out the crown jewel of Fort Wayne, the Zoo that actually had to close last year. I think that's wrong. That problem with the Lawton Park Dam needs to be addressed, and a pumping station needs to be put in. But I do believe the State Street Bridge project needs to go forward. That's all I have to say.

Mark Andrews, 2227 Westbrook Drive. I'm right in the center of this also, and those of you that live in our neighborhood remember that a year ago we had a little rain and that we had quite a flood. The Clinton Street bridge was closed, all of Clinton Street was closed, so we can talk about it all we want but the flood program like Mark Heffy said it does all backup, now we have Pasco and Kelly Chevrolet clear out north, so this project really won't do that much for flood control. We say all we want to do is just make sure State Street doesn't get closed. Although, us in the neighborhood will still have to put up with water. The other thing is they're not talking about in this meeting. I don't know if it doesn't apply to this meeting or not is the 70-Inch or 60-inch water main that's going to go up

My name is **Michael Gowbrath** and I'm the Executive Director of Arch. I'd like to begin by acknowledging all of the people that put together this document. I'd like to thank American Structurepoint, and the City of Fort Wayne, and INDOT and SHIPPO, and Federal Highways. It's a truly massive document. It took hours and hours and hours of time. I'll reserve the bulk of my comments for our official written response, but I'd like to begin by saying a couple of different things. 1. We agree that the project as proposed involve substantial controversy. Concerning community, and natural resources. All of those

viewpoints deserve to be heard. We agree that under Section 106 of the National Historic Preservation Act that there's adverse affects, but we disagree that there's no pertinent fees of alternative under section 4F. We would like to present this alternative concept here onto American Structurepoint and ask that it be reviewed. Arch, along with Indiana Landmarks and a number of other consulting parties engage Storrow Kinsella, a landscape architecture firm and Transportation Solutions, a traffic engineering firm, to review the current preferred alternative, and the purpose and need for the projects. We believe that this proposed alternative concept meets that stated purpose and need, is prudent and, feasible and results in alternative meeting the four least overall contest. As this meeting has a tightly scripted agenda, Arch will be holding our own Public Information meeting to further this concept on June 30th, at 7pm in the lower level of the Psi Ote Barn in Northside Park. Thank you.

Next is Karl Diestch. He is signed up twice. He is going to do a personal presentation and then he's going to read a letter for the second time on behalf of three other people who could not be here this evening.

My name is **Kark Diestch** and I'm a Fort Wayne resident. Congratulations to all involved in the final plans of the State Boulevard project. This well thought out City plan to create a State Boulevard improvement between Wells Street and Spy Run have stood up to the test of time and logic. I appreciate City officials being cooperative and explaining the various design stages of this project in the past six plus years. Many hearings have been held for all to attend and offer constructive comments both verbally and in writing. As a resident of the Brookview neighborhood and living among the closest homes to and affected by the future State Boulevard project, I am aware of the strong approval to the City's plan by a majority of the neighbors. Approval waiting of the City's plan is 3 to 1 on Oakridge Road and 2 to 1 on Terrace Road. In the northwest quadrant of Fort Wayne 36 association leaders are in favor of the City's plan for a ratio of 6 to 1. The City has stated from the beginning that the State Boulevard project's purpose was to build a safer, high capacity, and elevated street, while replacing the Spy Run Creek Bridge. The final drawing presented here tonight is a result of all the input that has been made to the designers within the time limits all of us have followed. Not all inputs of mine and others have been included, but I accept this as the final decisions are not ours. The City's final design is great and fulfills the project's stated purpose. The environmental impact of the City's project is nil, due to the fact that all homes to be removed are already environmentally damaged from flood waters filling their basements. This makes the City's plan as drawn for the State Boulevard project a win win for the homeowners, City, and all future users of State Boulevard. Let's build it. Thank you for letting me speak.

Okay, you get your second 3 minutes here for a letter that was written by others that you would like to read briefly.

These are the words of a **letter from John Modezjewski, Bud Mendenhall, and John Meinzen**. Sirs, we have asked Carl Deech to speak for our neighborhoods at this meeting tonight, as we have a real neighborhood event that is taking place tonight. The Drug and Gang Awareness Night in Hamilton Park is a real neighborhood issue for many of the neighborhoods in the northwest area of Fort Wayne have come together as a group of concerned neighborhoods to stand against a real problem, not a manufactured issue that the State Boulevard tractors have been using to solve progress and construction of a much needed improvement to the northwest area in the City of Fort Wayne. We need

the State Boulevard project to rebuild as designed by the City of Fort Wayne, and approved by the Citizens of Fort Wayne and all the meetings given by the City of Fort Wayne Board of Works and the Traffic Engineering Department. The project needs to be started as soon as possible. Stop the posturing of a minority of citizens and the grand stain of our elected officials and let's get the project moving forward as designed. Thanks, John Modezjewski, President of North Highlands Neighborhood Association and President of the Northwest Urban Development Coalition, Bud Mendenhall, President of Bloomington Neighborhood, Vice President of the Northwest Urban Development Coalition, John Meinen, Spy Run Neighborhood Association, Founder and Leader of the Core Group. Thank you for your time.

Next is Mr. John Slate

Hello there. Everybody out there I know or may not know me, I'm **John Slate**. I own X-spot Printing and I guess one of my biggest concerns is this gentleman this evening said that traffic would be open for both on State during the total project as I recall. I guess, I would just like to know a little further in detail on how he plans on doing this. Also, when there is a thing built across State there, how do you plan on having both lanes of traffic going at that time while you're constructing this? Those are a couple questions that I would like to have addressed and I hope that he can do that. Also, to get into the businesses, I guess, I would like that addressed as to how there will be access to a business along there, such as mine. I've seen a lot of businesses that have nearly gone under because they've had to wait months and months where it took time to construct these things. So, I guess those are some of the issues that I would hope that this gentleman can address to us. Also, is there, can you give us a guarantee that once this is done, is the flooding issue totally going to be taken care of for all of these people out here. I think a lot of these people would like to know that. You know people that's got flooding issues now is there going to be 11.5 million dollars spent and then possibly six months after that there is still flooding going on. I don't think that we want to have that kind of an issue at that time. I just want some sort of figuring out how that's all going to happen and that it can be some sort of a guarantee. So, I guess those are the things that I would like to have addressed before the project goes further. Thank you very much.

Next is Herb Weier.

I'm **Herb Weier** and I live in the North side neighborhood. My concern not only with this project affecting Spy Run Creek area, I'm worried about what it's going to do to the area of East State on the other side of the St. Joe River. The lanes are not as wide as the lanes that are going to be produced over the new bridge. That's going to create a problem especially when you have big trucks start running down the street. The other thing is that I would like to know the reason why they designed the new road for 35mph but they're going to post it at 30. Well, you know what's going to happen? It's not going to be 30mph, it's going to be 35 or more. I'm involved with the East State Village attending to the flower pots we have and you sort of take your life in your own hands when you try to maintain those pots with the car traffic, because they do create a 30mph speed limit. So, this was my number one main objection to what is going to happen elsewhere. The other thing is with flood control there are not enough containment ponds up north of the Spy Run Creek. I worked up in that area for 30 years and I remember

that whenever the rain would come the parking lot of the company that I worked for would just be pouring that water right into the Spy Run Creek. Therefore, I think that they did not address anything in regard to the, at least it wasn't evident tonight, that they address the containment ponds that should be installed in that area. Thank you.

That is everyone that is on the list. Now we would like to open it up to if anybody else would like to comment. This lady over here. I will bring the microphone over to you.

My name is **Vivian Braun** and I live at that nice red house on Westbrook and....inaudible. I've lived there for fifty years. But before that I came from Philadelphia. I know you've heard of it. It's a City of great history and we have two signs in our neighborhood designating us as a historic area. Have you all seen it? So, I have two things to comment on. How would you like it if they decided to put a McDonalds next to Independence Hall and how would you feel if the American flag hanging up there designed in Philadelphia by Betsy Ross suddenly became a house next to a 4-lane highway? You know, we are historic. We should keep it that way. Thank you.

I'm going to ask you to come back up here. Just need you to state your name.

Hello, my name is **Julie Peebles** and I lived at the corner of State and Eastbrook for six years. I moved in September. I had a 2-bedroom house and 3 kids, and it wasn't working, so I had to move. But, I have an opinion on this topic that I would like to share. I met with Shaun a couple years ago and I wanted to see pretty much what you've got in your hand tonight (the map). I wanted to know what they were doing, how it was going to be, what it was going to look like, because I've watched my kids play in this area. I live very close to the affected area and it was very personal to me. This was my first home I purchased by myself, so I felt very passionately about all of this. No disrespect to anybody here, but I fail to see how moving the traffic away from my home decreases my property values. I fail to see how moving a road south detracts from the historical value of the neighborhood. People are speeding now, where I'm hearing a lot of concerns about this 30mph. People are going 50 down the hill from Wells towards Clinton. That's happening, that's the reality. The traffic is there. The speeding is there, so I'm having a hard time understanding how my property values are going to go down. I feel like the people who believe that have never sat in their yard and watched a car comes flying down State from Wells at 45mph and slam into the bridge. You've never actually sat out in your yard and watch that happen, because it's there people. I cannot tell you how many times that guardrail has been replaced in the six years that I lived there. Those of you that believe that this is going to harm the neighborhood; you've never had twelve motorcycles race each other down the hill at 3 in the morning gunning their engines and waking up your children. That's not your reality. That is mine for six years. You've never watched a car race down Eastbrook, you know, taking a shortcut from Lima to State. That's happening now. You know, my kids played in that yard for six years. My dog, my children walked that dog through the neighborhood, so I didn't like the idea that I had to clench in fear every time my kids wanted to leave the house to go to a friend's worrying who was going to come racing down Eastbrook Drive to get to State 3 seconds sooner. Move the traffic away from the historical neighborhood and you know you're going to see the improvement of life, you're going to see the increase in property values. All of this being said, my last comment that I have down here was those of you that don't see the value, you've

never actually have to leave your house for work at 7:30 in the morning and had to wait 20 or more minutes for an opening in traffic just to leave your own home. So, I appreciate your concerns, but this project is going to preserve the historical beauty of the neighborhood and not take away from it. Thanks!

Thank you! Okay, the gentleman in the pink shirt. White shirt. Pink shirt. If you can come up here that would be helpful. Thank you. If you could state your name for me.

My name is **Tom Shoaff** and I grew up on Westbrook and I went to school and came back I bought a house on Eastbrook and collectively I've been there over 40 years. So, I have very strong feelings about that neighborhood. I loved being there when I was kid, I enjoyed it when I came back and I started raising my children. I now live off of State Street, but further to the east. I have read this poem that we've been given about why this project is going through and throughout this poem they talked about needing an east/west thoroughfare on the north side of Fort Wayne, and this is where they're going to put it. Now I understand your concerns about the traffic and the congestion, but putting in a thoroughfare only means one thing. More lanes, more traffic and the businesses want it and they want it for their trucks. This road is not going to cure the amenities of your neighborhood that you're trying to protect. It's just going to make it bigger. Now what there is, which is available is an alternative that has a southern passage which takes traffic off of State Street and it doesn't speed it up. The idea of connecting the west side of the properties of Fort Wayne with the east side as a thoroughfare only makes sense if you're talking about trucks and big busy intersections. It doesn't take away from what you dislike now. It doesn't save anything that you're trying to save. The alternative is a much easier passage through Fort Wayne without creating congestion. It gets rid of the speed and it doesn't provide for truck passage. I hope you get a chance to look at it. My understanding is the people that have put that together at great expense and with great energy have not been allowed to present that to the City of Fort Wayne. They asked for a meeting, the City of Fort Wayne said we don't want to meet with you. You ought to be asking for it. You ought to be seeing it. I want your neighborhood preserved because I love that neighborhood, and my children, and I want what's best for you. And what the City is providing is not what you want. They tell you they are protecting your neighborhood, but they are turning your neighborhood into a thoroughfare. Just like Ardmore. It's going to be the north side Ardmore of Fort Wayne. I don't think that's good for anybody.

My name is **Todd Ernest**, born and raised in this neighborhood within a 1-mile radius. I actually built this room that we're all sitting in tonight. Very concerned about this issue as the other years and years previous, they say they're going to reduce flooding by Run Creek and the Army Corps of Engineers, the City all combined have really not fixed the problem. They did the Clinton Street Bridge just the other year heading southbound there. That didn't fix nobodies basement problems in the neighborhood, did it? I don't know if this should be made in to two separate issues as I look at it. One for Highway Department, straightening a road out and alleviating some tension and traffic, and the other issue being the flooding, which I feel is most important to the neighborhood I grew up in. The flooding is documented for years. It comes from northwest, Cook Road and everything. The industrial parks and everything, the water comes down, and then as Mr. Shoaff said, the dam down there is too short. The water just backs up into these neighborhoods. So what are we getting into here is to beautify and

straighten out a road or are we going to help our community here where we were born and raised about the flooding? The engineers need to fix the flooding I believe before a new State Street is built right there. It's huge for insurance purposes. These people that live in the neighborhood. If you look at it. I've been in construction 30 years. You build a bridge, fine and dandy. It may straighten the road around your house and make it a nice drive, but that ain't going to help that water below the bridge and keep your basement from flooding. These engineers need to look at that and they could have fixed it years ago, but maybe they could take the extra water run-off and give it to the people in Hometown. Who knows? But I think the flooding issue for our Fort Wayne Zoo and this community needs to be issued before spending millions of Federal dollars straightening that road. That's my opinion and I want the people of the neighborhood to say somebody speaking up every time their basements flood. Talk, speak up and talk to right people. That needs fixed before we straighten the road out. Straightening the road out would be a good thing, There's kids and school buses there, but let's fix the initial thing that we haven't fixed in 40 or 50 years prior first. Okay? Thank you.

Anyone else that would like to come up and make a comment?

Hi, I'm **Danielle Tuck** and I live on Terrace, just off State. I feel like this is going to happen and whatever happens, happens quickly. I do have one question that I'd just like answered is making sure that the access from Oakbrook for the neighborhood. It didn't look like there was a stop light or a turn lane or any way to get on and off and I have to do the dive off Terrace or Oakbrook already to get on and off State going left or onto our street. So, just want to make sure that's addressed or how that's going to be addressed. How are we getting, left turns in and out of neighborhoods?

Thank you very much. Is there anyone else who would like to make a comment?

Hello, I'm **Peggy Grady**. I have lived at 2314 Eastbrook for 38 years. I am one of the four or five of the oldest still left in the neighborhood and I am for the new street and bridge. We need something. I am especially sorry for those people who have been told that their houses were going to be bought out six years ago and the flooding is a problem. It needs to be addressed first and we all agree to that. We did have a back up tsunami last year on June 1st. In 37 years our basement has never had any water. All of our neighbor have, but not us. Why on June 1st did we have roaring in 8-10 inches before we could do a thing about it and because the one gentleman here, he lost the foundation on the south side of his house. The only way that could happen is if there was a back up tsunami, and that does need to be addressed. But, I am sorry for these people that are having to wait out because they because they had to replace their furnace, their water heater, their washers, their dryers, and because this whole project has been stalled and stalled and stalled poor people can't even be bought out to get out of there. And Vivian, she has lived there fifty-some years and she looks out her window everyday and sees all of these broken down properties across the street. No one can put money back into properties when they don't know when they're going to be bought out. It is time we get over and move this forward and get done with all of this other stuff.

My name is **Robert Shoaff**, I didn't really plan to speak tonight as much as my two brothers already did. But I am concerned that we're confusing this flood issue with a traffic issue here. Two absolutely

separate issues. What the City is proposing is a traffic project and has got nothing to do with flooding. Now if people that flood and want to be bought out, should be bought out. They should have been bought out six years ago probably. But it has nothing to do with turning State Street into a 5-lane highway. Now, if I were to boil this issue down to our neighborhood, I would say can our neighborhood survive another Clinton Street? That's what we have, we have a 5-lane highway going through our next door neighborhood and I'm old enough to remember when I could cross Clinton Street on my bicycle and it was a 2-lane highway, my best friend lived across on the other side. I passed across that road several times a day sometimes and if that were a 5-lane highway when I was growing up I might have survived, but my mother wouldn't. She'd have gone to an early grave. Just ask yourself, well okay that's not a concern for us now, but how about the other people who have families with small kids? A lot of them look at houses in our neighborhood and they say we're not going live here. It's not safe for our kids. Turning State Street into a 5-lane highway like Clinton Street is not going to help the safety of our kids. That's all I've got to say.

My name is **Julie Dunell** and I'm the President of Friends of the Parks Allen County and I just wanted to urge everyone to come to Arch's Public meeting to see the alternative on June 30th. I think you will admire what the engineers and the landscape architects from Indianapolis have come up with. When the Friends of the Parks learned about this project in 2007 or 2008 the RMP had already been let for 5-lanes, and so no other alternative had been explored publically for the rest of us. We have always hoped that this design should have been held under the...inaudible...of a qualified landscape architect, and Arch will be presenting a plan that is done in that manner so I hope you'll all have a cane to see it. Thank you. As I remember what Mike said it's at Psi Ote Barn, lower level, June 30th, 7 o'clock.

My name is **Peggy Roy** and I live at 2121, I do not live, I do own a home in the affected area, 2248. I am south of State Street. I was one of five homes that was left there after the initial buy out by the City, they bought out all of Westbrook, they bought homes on both ends of my street there in Eastbrook. All part of the flood purchase. In 2007 after my first appraisal, the City called me and told me that INDOT was sending in and it was all a Federally funded INDOT project and that they would not be acquiring my home. That was 2007. Me and my four other neighbors have been living in limbo for 8 years. It is now a rental property for me and if any of you know what it's like to be a landlord all it involves that follow that, it is not a pleasant situation. It is not something I asked for, it's not something I wanted to do. Had I known that the City was going to back out, I would have never had moved out of that house. That property is costing me thousands. June 1st of last year as everybody has stated before flood, 5 ½ inches of rain in a short period of time, was the first time the water ever got up to my front door, let alone broke out my basement windows and flooded all the way up to the first floor. It was an absolute nightmare. With the help of Insurance companies I put \$15,000 on the repairs into that home just so I could rent it temporarily until somebody acquires my property. I don't know whether this whole street straightening thing is going to solve the flooding issue or not. It may help, but I don't think it's going to carve it completely. All I'm asking is that somebody and I thank you...inaudible...for bringing that up and mentioning us on the south side of State Street and you know the hardships that we went through. I don't care when this project starts. I don't care if it happens at all, but I've been told for six, seven, eight years that my property would be acquired and I'm telling you right now it's time, I'm over this.

Something needs to happen, something needs to happen soon. I have a home in disrepair. I can't put money into this home because it's going to be torn down. I have to keep bringing it. I don't want to. I want it to be done with. I want it to be over with. Thanks for listening.

I'm **John Hartman** of Northside Plumbing, 2238 and 2234 North Clinton, In the past my basements have flooded and they didn't on June 1st so the guys that did that must have done something right, so thank you.

Okay, so we've been going for about 40 minutes. Is there any other comments? You're certainly welcome to comment. If you don't make any comments here, if you fill out the comment sheet, those are considered formal comments. Take the time to think it through, send us based on the information we gave you earlier that's in your information packet. We will be sticking around to answer your questions. The engineers will be here to help you out, so if there's nothing else we very much appreciate you coming out tonight and helping us gather some comments on the project. Thank you.

From: [Attendant](#) on behalf of [Fax_Inbound](#)
To: [Hope, Briana](#)
Subject: Fax from Robert Shoaff
Date: Monday, July 21, 2014 8:04:22 AM
Attachments: [recv0355-001.pdf](#)

-----Original Message-----

From: 3175430270 [<mailto:itsupport@structurepoint.com>]
Sent: Friday, July 18, 2014 5:03 PM
To: Fax, Inbound
Subject: Fax Server: Fax Received - From: " To: '3175430270'

Attached is your fax document.

Name: 3175430270(T)
Remote ID:
Port: 1
Fax Number: T
Status: pass

Direction: receive
Number of Pages: 1
Elapsed Time: 23 Seconds
Total Bytes: 24971
Email Attachments: 1

Init Time: 07/18/2014 16:03:50
Off Hook Time: 07/18/2014 17:01:52
Connect Time: 07/18/2014 17:02:06

Resolution: Standard
Width: 1728
Height: Variable
Baud Rate: 14400
Data Compression: mh
Error Correction: on
Scan Line Time: 0



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by Friday, July 18, 2014. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date: Wednesday, June 18, 2014
Project: State Boulevard Reconstruction Project

Name: (Please print) ROBERT N. SHOAEF
Address: 2551 WESTBROOK DR,

COMMENTS: I RESPECTFULLY REQUEST THAT
THE CITY OF FORT WAYNE AND THE FORT WAYNE
CITY COUNCIL ALLOW THE RESIDENTS OF
THE NEIGHBORHOODS AFFECTED TO HAVE SOME
MEANINGFUL INPUT TO THE PLANNING PROCESS.
THIS WOULD INCLUDE SERIOUS CONSIDERATION OF
ALTERNATIVE PLANS LIKE THE ARCH PLAN. SO
FEAR, THE CITY'S ATTITUDE HAS BEEN THAT ITS
PLAN IS THE ONLY PLAN AND THE RESIDENTS
CAN TAKE IT AND LIKE IT.

SIGNATURE: Robert N Shoaff



storrow kinsella associates

urban design & planning for places | connections | strategies

December 9, 2013

Mr. Michael Galbraith
Executive Director
ARCH, Inc.
818 Lafayette Street
Fort Wayne, IN 46802

Re: State Boulevard Reconstruction – Alternative Concepts
Fort Wayne, Indiana

Dear Mike and Consulting Parties Team,

Storrow Kinsella Associates, in collaboration with Transportation Solutions, LLC has completed our analysis, evaluation and development of a sketch plan reconstruction alternative for the five-lane roadway widening project currently being proposed by the City of Fort Wayne.

We are confident that the resulting report provides a foundation for ARCH, Inc. and the Consulting Parties Team to have a constructive dialog with the City and its consultant. The *Consulting Parties Proposed Alternative* (CPPA) provides a solution that restores Spy Run Creek Parkway continuity, and maintains the integrity of the Brookview-Irvington historic neighborhood while meeting overall economic development, flood control, connectivity and beautification goals.

Please note that this report is a "sketch plan" alternative. We have made a good-faith effort to consider existing conditions and the purpose and need of the project, but must emphasize that additional design and study will be needed before this proposed alternative can be fully incorporated into the City's project.


The costs for the CPPA appear to be of similar magnitude or less than the City's Preferred Alternative, based on comparative lane-miles, bridge cross section and length, and probable right-of-way acquisition. In addition, the long term maintenance costs are comparable or less for the CPPA, based on thoroughfare lane length reduction from 5,175 to 2,100 feet.

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of the proposed alternative.


We remain available to answer questions and assist you and the City with moving an improved State Boulevard Reconstruction Project forward into implementation.

Sincerely,
STORROW KINSELLA ASSOCIATES


Margaret T. Storrow, Principal


John W. Kinsella, Principal

TRANSPORTATION SOLUTIONS, LLC


Thomas R. Sturmer, Principal

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Fort Wayne Historic State Boulevard Consulting Parties Proposed Alternative

to the City of Fort Wayne, Indiana, State Boulevard Reconstruction Project
INDOT DES# 0400587

prepared for
ARCH, Inc.
by

Storrow Kinsella Associates + Transportation Solutions, LLC

This investigation has been undertaken on behalf of ARCH, Inc., in response to a current City of Fort Wayne proposal to straighten and widen the portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. This section of State Boulevard is within the Brookview-Irvington Historic District neighborhood. Additionally, State Boulevard and Spy Run Creek Parkway, both components of the Fort Wayne Park and Boulevard System Historic District, intersect within the project area.

The project's impact on historic resources gives standing to the concerns of the Consulting Parties.

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1 Purpose of this investigation

The intention of the investigation is to determine if there is a viable alternative to the City proposal, within the State Boulevard corridor, that reasonably addresses the connectivity and congestion mitigation purposes of that proposal, while avoiding its considerable impacts on the integrity of the overlapping historic districts through which it passes. The investigation does not address the viability of alternative corridors identified as part of the project's Section 106 process other than suggesting that they appear to need further study for their potential contribution to overall network east-west connectivity and congestion mitigation. Likewise the investigation does not address aspects of other State Boulevard project segments that lead to this focus area. It does suggest that lessons learned in this focus area could inform the larger system.

A description of the city's preferred alternative and critiques of its impacts by multiple consulting parties are available in the project's Section 106 documentation thus are not repeated here in the interest of brevity. The critiques include but are not limited to concerns regarding induced traffic, inappropriate scale, and disruption to the character and continuity of historic resources in the project area.

2 Description of the Consulting Parties Proposed Alternative (CPPA: See Exhibit B)

The CPPA diverts east-west crosstown traffic through the district of concern as a new 1000-foot+/- long *two-lane parkway* alignment, generally south of Spy Run Creek, a natural divide. It is intended to improve crosstown connectivity and relieve congestion in a manner that does not induce additional traffic volume and with scale and geometrics that respect the intrinsic qualities of both the Brookview-Irvington Historic District and the Historic Park and Boulevard System.

The transition to this parkway from existing State Boulevard occurs at Clinton Street on the east and at the Westbrook/Edgehill intersection on the west. That transition is enabled by a traffic calming single-lane roundabout at Westbrook/Edgehill, and by a two-lane signalized hybrid roundabout at the higher volume North Clinton Street intersection. The two roundabouts bookend a new terrain, uninterrupted two-lane parkway linkage as a system that modifies motorist behavior to a slower but steady-state stream between the roundabouts. This configuration allows less space-consuming geometrics (vertical and horizontal alignment and clear zone constraints) and much fewer vehicular conflict points, while accommodating expected volumes through operational efficiencies achieved by those reductions.

The CPPA parkway alignment replicates the scale and curvilinearity of the existing historic boulevard, while allowing the latter to revert to a low speed/low volume pedestrian-friendly local street and bicycle boulevard. Thus both the historic and the proposed new segment respect the characteristics of the Historic Park and Boulevard System, *and* the Brookview-Irvington Historic District neighborhoods, by their contextual scale and alignment.

A consideration for detailed development of this parkway alignment and its new crossing of Spy Run Creek, both of which will be raised above flood elevation (a project purpose), is that these elements be sensitively designed such that they integrate well with both the creek and with the Edgehill Avenue neighborhood. The narrow roadway cross-section will help facilitate that spatial integration by lessening the need for obtrusive retaining walls. For that reason a multi-purpose path is proposed to be separated from the roadway (other than at the bridge) as part of the existing pathway system, rather than as sidewalks adjacent to the roadway.

The provision of functionally interdependent roundabouts at east and west ends of the approximately 1000-foot distance of the proposed New State Boulevard alignment facilitates use of a two-lane configuration for this segment. The linked roundabouts will modulate traffic flow through this lower speed (but uninterrupted) segment such that the less-than-750 peak hour vehicles per lane per hour, as recorded in 2005, can be accommodated. A hybrid two-lane

roundabout is proposed to replace the Clinton Street/State Boulevard signalized intersection, while a single lane roundabout would occur at the Westbrook/Edgehill intersection with State Boulevard.

3 Clinton Street roundabout capacity

Clinton Street roundabout capacity is proposed to be maximized by several methods to allow the target traffic throughput discussed above:

A

A fairly new roundabout traffic management method, installation of metering signals at roundabout entries, creates gaps in dominant peak period flow to minimize excessive queues and delays at each successive downstream entry. Such roundabout signalization can be more effective than additional roundabout lanes, and can reduce the complexity associated with three-lane roundabouts. The signals would be controlled by queue detectors.¹

B

A left-turn by-pass lane is proposed from southbound Clinton to eastbound State Boulevard to reduce roundabout circulating traffic by an estimated 250 vehicles per hour, based on 2005 traffic volumes. It would be controlled by the queue detector system as well, and could further reduce the need for a third circulating lane.

C

Additional reduction of the Clinton-State roundabout circulating traffic can be affected, if necessary, by providing a westbound bypass (slip) lane from Historic State Boulevard to the new parkway segment, just west of its splitter island, and optionally from the new parkway's eastbound lane to southbound Clinton, immediately south of the roundabout. The contribution of either or both bypass lanes to roundabout efficiency should be determined during detailed roundabout design and weighed against the possible need for additional right-of-way to accommodate them.

4 Pedestrian accommodation

Pedestrian accommodation is shown through the roundabout for probable pedestrian routes and to provide connections to the Pufferbelly Trail system. *Accessibility Guidelines for Pedestrian Facilities in the Public Right of Way* (PROWAG) recommends signalization for pedestrian crosswalks at high-vehicular volume roundabouts, and requires them for crossings of two or more contiguous roundabout lanes. The required signalization can be integrated into the phasing of demand-cycles of the vehicular signal system discussed above for the Clinton Street roundabout to minimize disruption to vehicular flow while still accommodating pedestrian connectivity.

5 Bicycle accommodation

Bicycle travel through this district can be accommodated along Historic State Boulevard which, once converted to local traffic as proposed here, will be well-suited to become a bicycle boulevard. As a local street, all-way stops can be introduced along that segment for additional traffic-calming for bicycle and pedestrian safety. In the interest of a narrow roadway, multi-use paths at a separate and lower elevation alignment would replace sidewalks along the proposed, new two-lane parkway section.

¹ National Cooperative Highway Research Program Report 672 Roundabouts: an Informational Guide Chapter 7/7.5.1, Signalization/Metering

Should flooding or structural issues dictate removal of the State Boulevard Bridge, a proposed bicycle-pedestrian bridge in its location will provide additional neighborhood connectivity to the Pufferbelly Trail.

Pedestrian accommodations at the roundabouts should be configured to accommodate bicycles for those cyclists not comfortable riding with traffic through the roundabouts.

Additionally the existing trails along Spy Run Creek should be fully integrated with the proposed Pufferbelly Trail (see Exhibit B) to fulfill this project's multimodal objectives.

6 Transit accommodation

Citilink Route #8 serves this area along southbound North Clinton Street, paired with northbound Spy Run Avenue 800 feet to the east. The current North Clinton stop is in a travel lane immediately south of State Boulevard. The Consulting Parties recommend that a bus turnout be provided either south of the roundabout, or more preferably to north of the roundabout between building setback line and existing curb line, to minimize travel lane disruption, but requiring additional permanent right-of-way.

Citilink Route #6 uses east and westbound State Boulevard and north and southbound Westbrook Drive, and is potentially improved by the proposed roundabout at State/Westbrook. Paired in-lane bus stops are recommended on Westbrook just north of the roundabout to better serve this area.

7 Urban design considerations

The roundabout elements, if sensitively designed, can become gateway markers along the Clinton Street procession towards the city center, as well as become markers for this historic district along the park and boulevard system. The parkway section itself can become a beautiful passage through the convergence of the historic parkway and neighborhood, somewhat mending a route that has been compromised over many years of roadway expansion and ad hoc development prior to its historic designation. This is particularly important to a well-developed Section 4F argument that this intervention results in a net benefit to the historic resources it affects (or as in this case, celebrates). Leveraging the project to enhance Spy Run Creek Parkway as a public park, and reduction of existing traffic impacts to the historic neighborhood are compelling benefits that the original City Preferred Alternate could not claim but which the CPPA can....if executed well.

Another consideration is that Spy Run Creek Parkway was compromised several years ago when Westbrook Drive, a classic *City Beautiful* parkway along residential properties on one side and the meandering creek and variable open space of the park on the other, was terminated at Edgehill Avenue just south of State Boulevard, where it now enters a neighborhood street. The CPPA alignment along the south side of the creek restores much of the historic parkway's integrity by taking it to a more contextual terminus.

8 Floodway/Floodplain considerations

Floodway impacts appear lessened by the proposed alternative because of the reduced width of the two-lane roadway in addition to the provision of a comparable bridge opening along the new alignment. Spy Run Creek flood hydrology will require careful analysis and design such that this project lessens flood severity through removal of current impediments and through development of storage capacity potential of the open space surrounding the creek. That potential can be enlarged by investigation during the project's detailed design.

Fort Wayne Historic State Boulevard: Consulting Parties Proposed Alternative

Reference: INDOT DES# 0400587

12/10/2013

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9 Detailed development of the CPPA

Assuming agreement can be reached regarding this proposed alternative, continuing oversight regarding its detailed development and implementation should be integrated into the project development process. It is extremely important to the Consulting Parties that execution of roadway elements and their urban design setting be context sensitive in scale, materials and detail such that the vision of George Kessler and Arthur Shurcliff, for the Park and Boulevard System and for the Brookview-Irvington District, respectively, be honored and can become a model for how the city balances its infrastructure needs with its heritage. With this caveat, the Consulting Parties will support the city's effort to improve this section of State Boulevard.

10 Cost discussion: comparative magnitude of cost

Comparison of costs between the City Preferred Alternative (City) and the Consulting Parties Proposed Alternative (CPPA) are of relative magnitude based on predictable differentials of project scope. Detailed cost analysis is dependent on more detailed development of the proposed alternative design, as well as on a better understanding of the cost basis for the City alternative.

	CPPA	City	Comments
Roadway	2100 lane feet	5175 lane feet, landscaped center median	CPPA option may cost about half of City's alternative
Bridges	New two-lane/140-150' long vehicular bridge New 16' wide x 100' long bike-ped bridge Remove existing two-lane vehicular bridge or retrofit as local traffic and bike boulevard link	5-lane 150' long new vehicular bridge Remove existing two-lane bridge	CPPA bridge costs reduced significantly because of reduced cross section
Major intersections	Two-lane hybrid roundabout at North Clinton Street	One signalized intersection with additional turn lanes at North Clinton Street	Multi-lane hybrid roundabout will cost significantly more than improved conventional signalized intersection
Secondary intersections	One single lane roundabout (minimal secondary neighborhood road improvements)	Two five-lane intersections (including left turn lanes) and substantial reconfiguration of neighborhood streets	Simplified interface with neighborhood streets anticipated to result in a net cost reduction for these elements
Residential acquisition	0	15 (at \$55-75,000 average assessed valuation)	Residential acquisition much less (approaching zero)
Commercial acquisition	1 total, with partial impacts on 2 additional parcels	0	Commercial acquisition much more Combined residential and commercial acquisition-relocation expected to be similar

11 Summary of the Findings

1

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of this new facility.

2

The costs for the CPPA appear to be of similar magnitude or less than the City Preferred Alternative, based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The latter may be achieved through the offset of reduced residential relocation scope compensating for the higher individual valuation of commercial properties in general.

3

The long term maintenance costs are comparable or less for the CPPA, based on thoroughfare length reduction from 5175 to 2100 lane feet.

4

The CPPA provides greater safety through elimination of most intersection left turn conflicts, pedestrian/vehicle conflicts, and through the lower speeds associated with roundabouts, their approaches and linkages. Reduction in accident rates and their severity for roundabouts vs. conventional signalized intersections has been well documented by multiple FHWA and insurance industry studies utilizing data accumulated during the high rate of adoption of roundabouts by state and local agencies because of their safety and efficiency characteristics.

5

The CPPA minimizes negative impacts on historic properties and districts. The removal of existing traffic volume impacts is expected to result in neighborhood stabilization and reinvestment in the project area.

6

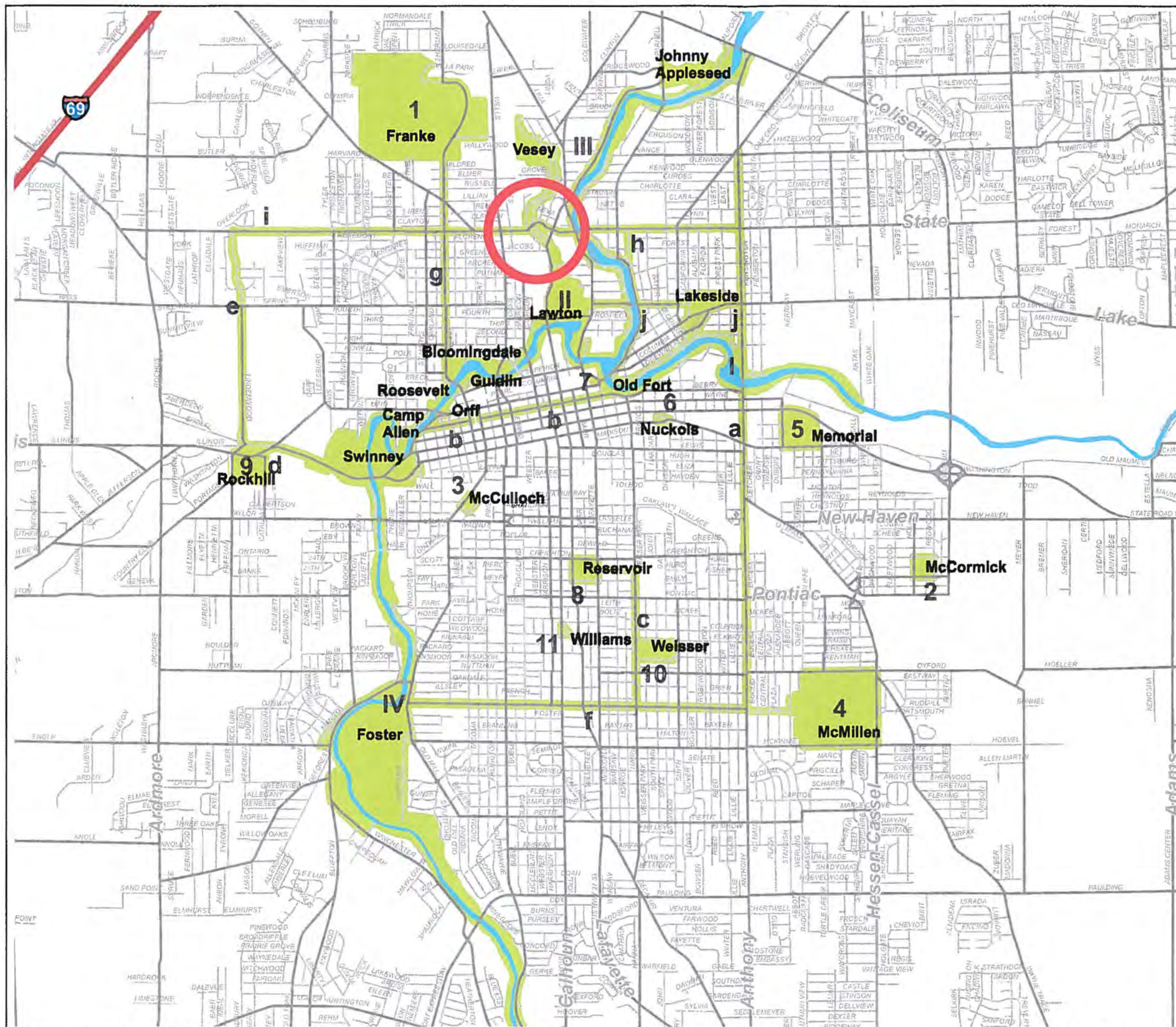
The CPPA provides an additional benefit to the historic districts by restoring the sense of Spy Run Creek/Westbrook Drive's parkway continuity, which had been compromised by earlier floodway-roadway improvements that terminated Westbrook Drive at Edgehill Avenue.

7

The CPPA meets the stated goals of the thoroughfare plan and reconciles that plan with the National Register-listed Park and Boulevard System.

8

The CPPA has the support of the affected neighborhoods and remonstrating parties which will facilitate the project moving forward expeditiously.



City of Fort Wayne

Park and Boulevard System

Historic District, Fort Wayne, Allen County, Indiana

Key Map

National Register Boundary

Parks

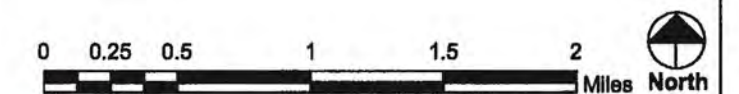
1. Franke
2. McCormick
3. McCulloch
4. McMillen
5. Memorial
6. Nuckols
7. Old Fort
8. Reservoir
9. Rockhill
10. Weisser
11. Williams

Parkways (includes riverfront parks):

- i. Maumee River
 - i. Lakeside
- ii. Spy Run Creek (Brookview)
 - i. Lawton
 - ii. Vesey
- iii. St. Joseph River
 - i. Johnny Appleseed
- IV. St. Mary's River
 - i. Bloomingdale
 - ii. Camp Allen
 - iii. Foster
 - iv. Guldin
 - v. Orff/Thieme Drive Overlook
 - vi. Roosevelt
 - vii. Swinney (East & West)

Boulevards:

- a. Anthony Boulevard
- b. Berry Street
- c. Hanna/Taber Street
- d. Jefferson Boulevard
- e. Lindenwood Avenue (Brookside)
- f. Rudisill Boulevard
- g. Sherman Boulevard (Kekionga)
- h. St. Joseph Boulevard
- i. State Boulevard (Pfeifer)
- j. Tennessee Avenue/Lake Avenue



Note: This exhibit based on City of Fort Wayne Park and Boulevard System Historic District Key Map, prepared by The Westery Group, Inc. and Storrow Kinsella Associates, 5/24/2010.



Study Area

December 9, 2013

Storrow Kinsella Associates urban design & planning for places | connections | strategies
in collaboration with
Transportation Solutions, LLC. | connecting the dots

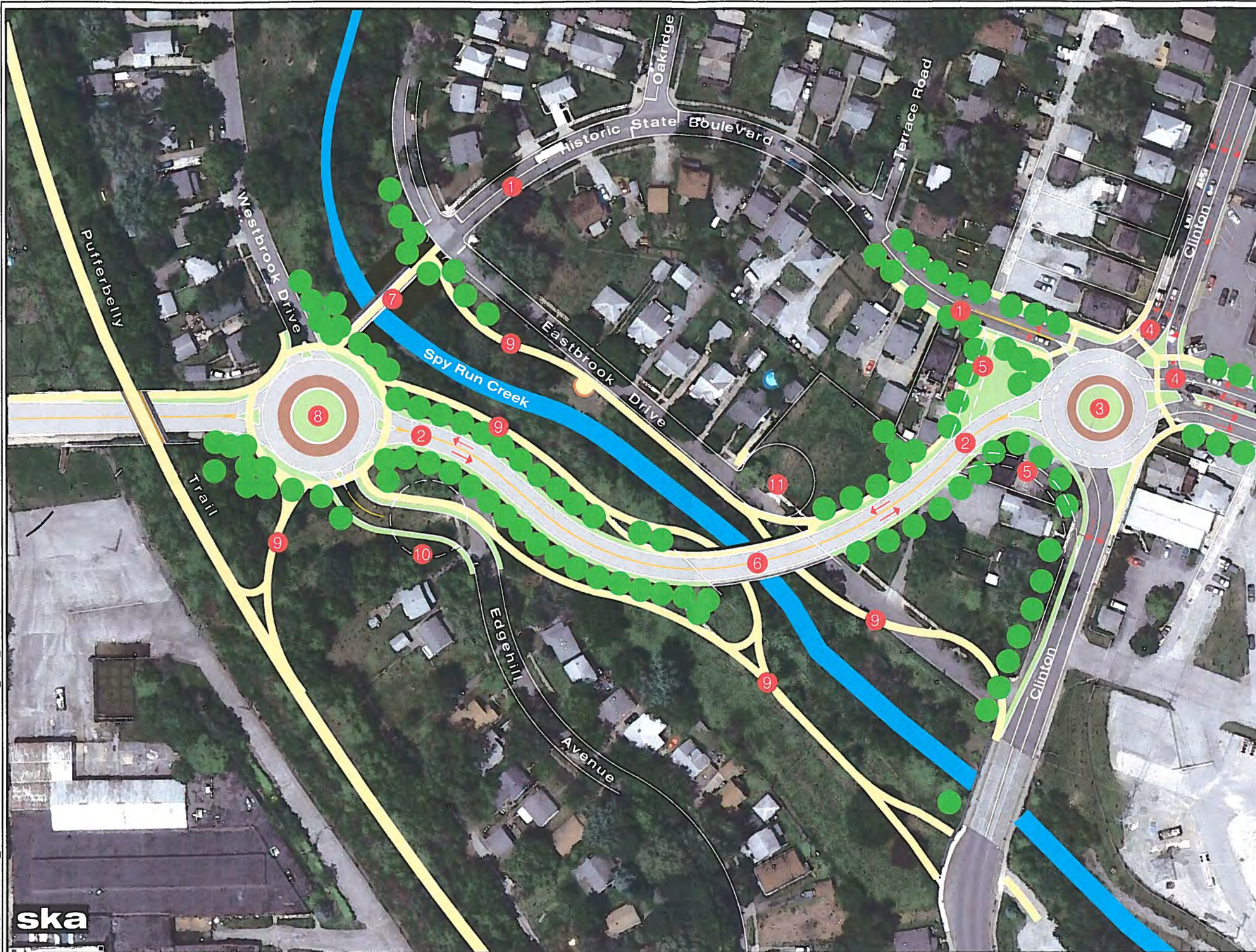
Consulting Parties:
ARCH, Inc./ Historic Landmarks Foundation of Indiana
Brookview-Irvington Neighborhood Associations

Fort Wayne Historic State Boulevard
Consulting Parties Sketch Plan Alternatives Study
City of Fort Wayne Project/INDOT DES# 0400587

Context Map

A

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Plan Keynotes

- 1 Historic State Boulevard convert as local street & bike boulevard
- 2 New State Boulevard alignment
- 3 Clinton/State 2-lane hybrid urban roundabout with signalized eastbound left turn bypass
- 4 "Smart" roundabout signal -metered approaches to create gaps and balance flow during peak periods, and to provide pedestrian crossing synchronized with those phases (short/rolling yield or stop signal phases)
- 5 Optional bypass lanes to reduce roundabout circulating traffic loads
- 6 New Spy Run Creek 2-lane bridge
- 7 Spy Run Creek bike/ped bridge
 - Retrofit existing bridge or
 - New multi-use path bridge
- 8 Westbrook/Edgehill single lane urban roundabout w/ local street access
- 9 Bike-Ped path system
- 10 Edgehill Avenue cul de sac alternative
- 11 Eastbrook Drive cul de sac

Note: this is a sketch-level diagrammatic concept drawing. Roadway elements are proximate in scale and intended to illustrate general feasibility and proof of concept. Additional traffic engineering and roadway geometric study is required for project scope development.



0 50' 100'
Scale 1"=50'-0"

December 9, 2013

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Consulting Parties:
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Brookview-Irvington Neighborhood Associations

Fort Wayne Historic State Boulevard
Consulting Parties Sketch Plan Alternatives Study
City of Fort Wayne Project/INDOT DES# 0400587

Sketch Plan

B

BENEFITS COMPARISON

Alternative	Connectivity Improvement	New roadway alignment length	Reconstruction alignment length	Residential Impacts	Commercial Impact	Total historic relocations or impacts	Other Impacts	State Blvd. historic bridge impacts	State Boulevard Safety Improvement			Congestion Improvement	Reduce flooding	Alternative	
									Motorists	Bicyclists	Pedestrians			Feasible?	Prudent?
City's Preferred Alternative	Regional connectivity improved for through motorist, bicyclist, and pedestrian movements. Connectivity reduced for neighborhood residents.	1035 LF 5 lanes sidewalk or trail both sides	5175 lane feet 2070 sidewalk feet	15	None?	State Blvd historic parkway system compromised. Neighborhood bisected by new raised alignment. Historic bridge removed. Traffic speed and volumes increased through neighborhood.	View sheds affected for some residents. Historic neighborhood context affected. Five-lane raised roadway out of scale with residential neighborhood.	Historic bridge to be removed and replaced for flood elevation and structural deficiency reasons.	A portion of Old State Boulevard will be converted to local use with significant reduction in traffic volumes. New east-west alignment designed to current federal standards. Intersection capacities improved.	No bicycle accommodation currently exists. New multi-use path provided along new alignment.	New sidewalks provided along new alignment. Appropriate pedestrian crossings assumed to be provided at intersections.	Added lanes and intersection improvements reduce travel delays in overall system. The localized portion of State Boulevard will be a lower speed travel environment.	Raised elevation of State Boulevard and larger hydraulic bridge opening will help to reduce localized flooding frequency.	Yes	This is the City's Preferred Alternative. The Purpose and Need items are addressed, however the impact to the historic neighborhood and parkway system is significant.
Consulting Parties Proposed Alternative	State Boulevard corridor (and Greenways Trail System) connectivity improved for through motorist, bicyclist, and pedestrian movements. Historic State Boulevard will function as bicycle boulevard.	1048 LF 2 lanes 2 sidewalks	2096 lane feet 2096 sidewalk feet	One loss of alley access	Three: 1) Gas Station Store acquisition or relocation; 2) impact to garage and storage shed; 3) site impact.	Minimum historic structure impacts. State Boulevard historic integrity respected. Historic Bridge removed or rehabilitated for pedestrian/bicycle use because of deteriorated condition.	One commercial property on south side of State Boulevard at Clinton Street relocated to allow construction of a five-legged hybrid roundabout. Partial impacts on 2 additional commercial properties.	Historic bridge to be rehabilitated or removed and replaced for flood elevation and structural deficiency reasons. Pedestrian bridge with narrower profile impacts flooding to a lesser degree and provides east/west connectivity.	Significantly reduced volumes on existing State Boulevard alignment improves safety. Lower posted speed limit possible. New alignment meets goal of regional connectivity.	Existing State Boulevard converted to a low volume shared use roadway suitable for travel by bicycle.	Vehicle volumes greatly reduced along existing alignment. Sidewalks to be improved along existing alignment and provided along the new alignment.	Regional through traffic given more direct route. Intersection levels-of-service may be improved by a "smart" roundabout at Clinton and a single-lane roundabout at Westbrook.	Flood frequency reduced along new alignment due to raised bridge and roadway elevation.	Yes	This option addresses the Purpose and Need with less impacts to the historic resources. Magnitude of costs similar to the City's Preferred Alternative.

COST (RELATIVE) COMPARISON

Alternative	New alignment length	New alignment width	Bridge length	Bridge width	Bridge elevation	Residential relocations	Other residential impacts	Commercial relocations	Intersection Types			Bridge removals	New pedestrian bridges	Local Streets Affected	
									Westbrook Drive	Oakridge Drive	Clinton Street			Pavement removed	Pavement added/rehabilitated
City's Preferred Alternative	1035 Linear Feet (5175 lane feet)	5 Travel Lanes + 2 Sidewalks	150 LF 750 lane feet	Five lanes, curb and gutter, sidewalks and a 10-foot multi-use path on one side.	Bridge raised 7-feet above existing bridge elevation.	15 structures. Est. value \$1M	Any?	None?	Two-way stop controlled + energy and maintenance costs.	New Side-street stop controlled intersection.	Three-way signalized intersection + energy and maintenance costs.	Existing historic bridge.	One over State Boulevard for Pufferbelly Trail.	In vicinity of existing bridge and at both ends of existing alignment between Spy Run Creek and Clinton Street.	Oakridge Drive connector, "bulbouts" at termini of existing alignment.
Consulting Parties Proposed Alternative	1050 Linear Feet (2010 lane feet)	2 Travel Lanes + 2 Sidewalks	140 LF 280 lane feet	Two lanes, curb and gutter, and sidewalks both sides.	Same.	Two residences south of Clinton Street roundabout?	Alley access impacted for one residence. Net Benefit to Contributing Structures in District.	1 complete take and 2 partial impacts: unknown cost.	Single-lane roundabout. Est. cost: \$500,000 + landscape maintenance costs.	No new intersection.	Multi-lane hybrid roundabout with traffic metering signalization: Est. cost: \$2M + energy and maintenance costs.	Existing historic bridge rehabilitated or replaced.	One over State Boulevard for Pufferbelly Trail, replace existing State Boulevard bridge with a new pedestrian or rehabilitate existing bridge.	In vicinity of existing bridge only.	Rehabilitate existing State Boulevard alignment to create bike boulevard and pedestrian connector.

Description: City's Preferred Alternative	New four lane roadway with raised median and/or center turn lane for that portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. Realigned section raised up to 7-feet at new bridge for floodway consideration. New sidewalks and/or multi-use side path along both sides of roadway. New pedestrian bridge and approach ramps for future Pufferbelly Trail.
Description: Consulting Parties Proposed Alternative	Relocate the thoroughfare function of State Boulevard to south of Spy Run Creek from Westbrook Drive east to Clinton Street to minimize impacts to historic properties and parkway. Develop the thoroughfare as a two-lane roadway with sidewalks on both sides between a single-lane roundabout at Westbrook and two-lane hybrid roundabout at Clinton and State with no intersections between them to optimize flow and volume. The roundabouts help condition traffic to a steady state at a reduced speed. The Clinton/State intersection is a "smart" roundabout using advanced technologies consisting of signal metered approaches and traffic sensing to help balance flow, create gaps, and provide for pedestrian connectivity through the roundabout area. Existing bridge rehabilitated or replaced with new pedestrian/bicycle bridge to provide connectivity with future Pufferbelly Trail. Sidewalks along existing alignment to be improved.

Note: Information obtained from Section 106 Findings of Adverse impacts report (approved August 27, 2012) and other information shared by ARCH. Concept plans for City alternatives not available at this time.

December 9, 2013



City's Preferred Alternative



Purpose and Need

Shown above is a model of the City's Preferred Alternative; a new four lane roadway with raised median and/or center turn lane between North Clinton Street and Westbrook Drive. It is replacing a section of the existing two-lane State Boulevard in the City of Fort Wayne Park and Boulevard System Historic District.

The Park and Boulevard System Historic District represents a thoroughfare system designed in the early 1900's for the purpose of economic development, flood control, connectivity, and beauty. The proposed City's Preferred Alternative has many of the same purposes and characteristics, and, without reference to context, will be a significant upgrade to the City's thoroughfare system. However the segment between Clinton Street and Westbrook Drive does have historic context that the scale of the new boulevard will compromise.



Consulting Parties Proposed Alternative



Scale and Appropriateness

Shown above is the Consulting Parties Proposed Alternative, featuring a two-lane roadway able to accommodate the 2005 peak hour traffic volume of 750 vehicles per hour per lane. It restores Spy Run Creek Parkway continuity and maintains the integrity of the Brookview-Irvington Historic neighborhood.

The costs of the Consulting Parties proposed alternative appear to be of similar magnitude or less than the City's Preferred Alternative based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The long term maintenance costs are comparable or less based on thoroughfare length reduction from 5175 to 2100 lane feet.

Talk at Final Hearing on the State Boulevard Project

My name is Karl Dietsch, a Fort Wayne Resident.

Congratulations to all involved in the final plans of the State Boulevard Project. The well thought out city plan to create a State Boulevard improvement between Wells Street and Spy Run has stood up to the test of time and logic. I appreciate city officials being cooperative in explaining the various design stages of this project in the past years. Many hearings have been held for all to attend and offer constructive comments both verbally and in writing.

As a resident of the Brookview Neighborhood and living among the closest homes to and affected by the future State Boulevard Project, I am aware of the strong approval to the city's plan by a majority of my neighbors. Approval ratio of the city's plan is 3:1 on Oakridge Road and 2:1 on Terrace Road. In the Northwest Quadrant, 36 association leaders are in favor of the city's plan for a ratio of 6:1.

The city has stated from the beginning that the State Boulevard Project's purpose was to **build a safer, higher capacity and elevated street while replacing the Spy Run Creek Bridge**. The final drawing presented here tonight is the result of all input that has been made to the designers within the time limits all of us have followed. Not all inputs of mine and others have been included but I accept this as the final decisions are not ours. The city's final design is great and fulfills the project's stated purpose.

In the early days of this project, Phase I and II were to be built in 2012 and 2013. Today's schedule is for 2016 and 2017. Why the 4 year delay? Much opposition was formed by a minority of the Brookview Neighborhood when an association was created to fight the State Boulevard Project rather than to represent the views of all members in the association. This association got support from City Councilman John Shoaff. He in turn got support of Arch of Fort Wayne (of which he has been a past president of the board of directors) and the support of Indiana Landmark (of which his wife is a director). Indiana Landmark placed the project on the 2013 Indiana Landmark 10 most critical endangered sites list. This April 2014, the project was removed from their 2014 list.

Around the same time, Arch of Fort Wayne placed this project on their "2014 most endangered list". Arch has gone out of its way to discredit this project in unethical ways. The city sponsored 3 open viewing to educate the public on the city's project plans. Arch encouraged the public at the 3 viewings to fill out Arch's survey slips (not the cities). These survey slips asked for input on the design of 2, 3 and 5 lane project construction but left out a choice of 4 lanes as on the city's plan. Arch encouraged people to fill out their comment cards and stated that repetition is a good thing because each card is a vote. No cards said who was responsible for the survey and no signature line was provided on the cards. I consider this as fraudulent activity and a discredit to Arch of Fort Wayne.

The environmental impact of the city's project is nil due to the fact that all houses to be removed are already environmentally damaged from flood waters filling their basements. This makes the city's plans as drawn for the State Boulevard Project, a win-win for the home owners, city and all future users of State Boulevard. Let's Build It!

Karl Dietsch, a Fort Wayne Resident

Sirs,

We have asked Karl Dietsch to speak for our neighborhoods at this meeting tonight, as we have a real neighborhood event that is taking place tonight. The Drug and Gang Awareness Night in Hamilton Park is a real neighborhood issue where many of the neighborhoods in the North West area of Fort Wayne have come together as a group of concerned neighborhoods to stand against a real problem, not a manufactured issue that the State Boulevard detractors have been using to stall the progress and construction of a much need improvement to the North West area in the City of Fort Wayne.

We need the State Boulevard project to be built as designed by the City of Fort Wayne and approved by the citizens of Fort Wayne at all of the meetings given by the City of Fort Wayne Board of Works and the Traffic Engineering Department. The project needs to be started as soon as possible!! Stop the posturing of a monitory of citizens and the grandstanding of our elected officials and let's get the project moving forward as designed.

Thanks

John E. Modezjewski
President of North Highlands Neighborhood Association
President of the North West Urban Development Coalition

Bud Mendenhall
President of Bloomingdale Neighborhood
Vice President of the North West Urban Development Coalition

John Meinzen
Spy Run Neighborhood Association
Founder and Leader of the Core Group



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by **Friday, July 18, 2014**. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date: **Wednesday, June 18, 2014**
Project: **State Boulevard Reconstruction Project**

Name: (Please print)

Susan R. Haneline

Address:

134 E. State Blvd.

COMMENTS:

As someone whose home is in the actual footprint of the project I am in favor of the project. It will benefit the neighborhood in moving traffic away from the actual homes and provide relief in flooding and increase safety for everyone. The vast majority of us in the actual footprint of the project support this project. As things are currently our home values have fallen we are paying out thousands of dollars in flood repair/insurance. Trying to get in and out of our driveway on State Blvd is a lesson in patience and risk taking. Please do not further delay this project it is something that will benefit so many people.

SIGNATURE:

Susan Haneline



2312 N Clinton St, Fort Wayne IN 46805, Ph 260-483-0553, Fx 260-484-1156

June 18, 2014

City of Fort Wayne
Public Hearing 6:00pm
North Side High School
475 E State Bl
Fort Wayne IN 46805

Re: Proposed State Boulevard Reconstruction Project as stated in the Legal Notice of Public Hearing

To All Who Are In Attendance,

We are very much in favor of the Proposed State Boulevard Reconstruction Project as stated in today's Legal Notice of Public Hearing and as the renderings reflect on the cityoffortwayne.org website.

Classic Stereo has been located at the North East Corner of Clinton Street and State Boulevard for 48 years; all of which Phil Miller has been with the business. I have been with the company for over 36 years. Over these many years, we have seen how the daily congestion, accidents and flooding adversely affect businesses and homeowners alike.

With the addition of extra lanes in each direction and turn lanes as indicated, the design should smoothen the traffic flow to and through the intersections of State Boulevard and Spy Run, Clinton Street, Wells Street and the other adjoining streets in the area. The dreaded morning and late afternoon/evening rushes should have shorter and fewer delay times at the traffic signals. The elimination of the snake curves and the installation of new street lighting should also provide healthier traffic conditions as motorists, bikers and pedestrians are able to view traffic clearer while entering or crossing State Boulevard. The Project also appears to address, in a positive manner, the flooding issues that have constantly bombarded the community. Adding the appeal of the new greeneries beautifies the drive through the area.

We might also note that any discussion of cutting lanes, lane restrictions or adding roundabouts would not address the needs and goals that have been predetermined and deemed necessary in previous public hearings, as well as meetings held within pertinent city departments. And, in fact, these ideas may actually be detrimental to and serve as an actual down-grade to current traffic and environmental conditions in the area.

In conclusion, we are in desperate need of this Proposal as stated in the Legal Notice of Public Hearing and as the renderings reflect on the cityoffortwayne.org website.

I might add that Phil Miller, President and Owner of Classic Stereo and Video, would like to formally apologize for not attending tonight's hearing. He had originally planned to participate in the discussion. But, he has been working at a jobsite out of town the last two days, and won't be able to be here.

Thank you for this public forum.

Sincerely,

Two handwritten signatures are shown. The first signature is in cursive and appears to read "Phil Miller". The second signature is also in cursive and appears to read "Darrell Henline".

Phil Miller, President/Owner
Darrell Henline, Controller

American Structure Point
Attn: Briana Hope
7260 Shadeland Station
Indianapolis, In. 46256
July 18, 2014

To whom it may concern:

My name is Sue Beck I live in the Irvington Park Neighborhood near the planned projected area and am writing in strong opposition to the city's preferred alternative for State Blvd reconstruction, 3A. Rather than an improvement upon existing conditions, I feel that the city's plan will be highly damaging to overall safety along State Blvd and our surrounding neighborhood, and will negatively impact flash flooding, neighborhood character, our property values, not to mention quality of life.

I do support the Storrow Kinsella Associates/Transportation Solutions LLC- prepared conceptual solution presented by Arch/ Indiana Landmarks and others. I think (feel) we should keep State Blvd to, two lanes, I think they should remove the remains of the train over pass, and widen the street in that area, that would take most of the curve away, then replace the aging bridge over the Spy Run Creek, Eastbrook and Westbrook are such a very historic neighborhood, why does anyone want to destroy that? not to mention all the kids from North Side High School that walks down that street twice a day every day, when we have heavy snow like we did last year the kids had to walk in the street, because the sidewalks were not safe, so IF there were four lanes how dangerous would that be?

Brookview and Irvington Park both want to keep our neighborhood safe, clean, quiet, serene for our children, and our grandchildren, most of us have lived here around 25-30 year some even longer, when a house goes up for sale here it is sold BEFORE a sign goes in the yard in a lot of the times that is what Eastbrook and Westbrook means to us, no we don't want 4 lanes, please keep our neighborhood family safe.

Sincerely,



Sue Beck
3322 Garland Ave.
Fort Wayne, In 46805
Ladibug3322@aol.com
260-483-2866

Ms. Brianna Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project
DES. NO. 0400587
Federal Project Number IN20071404

Dear Ms. Hope:

The Friends of the Parks of Allen County, Inc. appreciates the opportunity to respond to the Environmental Assessment document pertaining to the above referenced project.

The first part of the assessment enumerates a number of public hearings and meetings with the public and with private citizens about the project. These meetings took place AFTER the City and NIRCC developed an RFP for a five lane project and AFTER the engineering drawings were underway. (November 29, 2007.) Therefore, they do not correspond to the requirements of Context Sensitive Design, nor are they evidence that the City and NIRCC did due diligence in considering the needs of the neighborhood, and the important historic significance of the landscape that the project will affect. At no time was the public offered an alternative that would address the very strong concerns of neighborhoods up and down the corridor, or respect the newly uncovered understanding of this area a nationally significant landscape.

The EA asserts that a *de minimus* finding is warranted for Vesey Park. This aspect of the Environmental Assessment should be reconsidered, because the EA definition of Vesey Park appears to conflate Vesey Park with a separate Parkway called Brookview Parkway in the National Register Nomination. The EA explains Vesey Park as follows: Vesey Park "is operated by the Fort Wayne Parks and Recreation Department and includes the greenspace between Eastbrook and Westbrook Drive connecting the larger partition of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary's River." This greenspace between Eastbrook and Westbrook Drive is, in fact, Brookview Parkway, and this parkway will be affected significantly by the project. Having it included in the definition of Vesey Park is probably incorrect and the finding of *de minimus* may apply to Vesey Park, but Vesey Park does not include Brookview Parkway where the project is actually located.

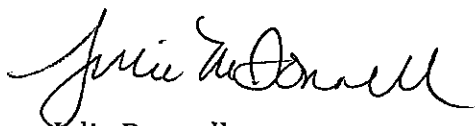
The EA asserts that the Fort Wayne Parks and Recreation Department wrote a letter of support for the *de minimus* finding for Vesey Park. The letter in question does not "support" anything, but simply states that it "appears that there will not be any impact to the *recreational* activities, features, and attributes of the land currently being used as recreational space." It may be that the Parks and Recreation Department only considered Vesey Park, and not the affected Brookview Parkway. Neither the request from Structurepoint nor the letter from the Parks and Recreation Department, found in appendix J, reference the Parkway, and the letter from the Parks and Recreation Department refers only to recreational resources, rather than the full set of values that are included in evaluating a significant historic landscape.

Our board supports the alternative concept that was commissioned by ARCH and Indiana Landmarks and which was presented at the recent public hearing for the Environmental Assessment in June and later to the public at a public hearing on June 30 of this year. In 2007 our board fashioned a position that any treatment of State Boulevard and Brookview neighborhood should only go forward under the auspices of a landscape architect who understood the multiple values that are part of a significant American designed landscape. Storrow Kinsella, such a firm in Indianapolis, with the aid of a qualified traffic engineer, developed this concept. It addresses the Purpose and Needs Statement as well as the concerns of historic preservationists, neighborhood groups and those affected up and down the State Boulevard corridor.

The alternative concept is preferable because it retains the distinctive landscape features of the fine design work of Arthur Schurcliff and will help maintain the character of the neighborhoods up and down the corridor.

With all of the advances in knowledge and technology in this century, this community deserves a better solution than the out of date one that NIRCC and the City Administration have put forward.

Sincerely,

A handwritten signature in black ink, appearing to read "Julie Donnell". The signature is fluid and cursive, with the first name "Julie" written in a larger, more prominent script than the last name "Donnell".

Julie Donnell
President, Friends of the Parks of Allen County, Inc.
July 16, 2014



The mission of ARCH is to advocate for the protection and preservation of historically and culturally significant assets and historic places in Allen County and northeast Indiana.

July 18, 2014

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Dear Ms. Hope:

I am writing in reference to the June 5, 2014 updated FHWA Findings and Determinations materials provided as part of the Section 106 Review, and the May 2, 2014 Environmental Assessment for the proposed State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana (Des. No. 0400587, DHPA No. 5903, Project No. IN20071404.) Thank you for the opportunity to comment and the work that went into preparing this and the related Environmental Assessment document for the above referenced project. I have a few specific points that I would like to address concerning the document and proposed project.

1. We concur that the project as currently proposed will cause an Adverse Effect under Section 106 and Section 4(f) to the three historic properties (Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge carrying State Boulevard over Spy Run Creek) located within the APE.
2. We believe that purpose of any Section 106 Review is as identified in 36 CFR 800.1(a) to "seek to accommodate historic preservation concerns with the needs of Federal undertakings through consultation among the agency official and other parties with an interest in the effects of the undertaking on historic properties, commencing at the early stages of project planning. The goal of consultation is to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties." Pursuant to that purpose, we believe that the Findings and Determinations materials provided on June 5, 2014 adequately identifies historic properties, and assesses effects of the project as currently proposed. However, we believe that the third portion of the identified goal to "seek ways to avoid, minimize or mitigate any adverse effects on historic properties" is inadequate. To further the discussion, ARCH, Indianal Landmarks, Friends of the Parks, and the Brookview-Irvington Park Neighborhood Association commissioned Storrow Kinsella Associates and Transportation Solutions to examine the background research developed for this project to see if there was

an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible and avoided, minimized or mitigated the adverse effect to the neighborhood. We believe that the alternative plan (the "Alternative"), presented by ARCH to American Structurepoint at the June 18, 2014 Public Hearing for the Environmental Assessment, and provided to American Structurepoint, FHWA, INDOT and Indiana SHPO by e-mail delivery on June 24, 2014 and June 26, 2014, better addresses historic preservation concerns while accommodating the purpose and need of the Federal undertaking.

3. We believe that the Alternative is provided during the completion of the Section 106 Review and is thus governed by the guidance in 36 CFR 800.1(c). This guidance mandates that the Alternative must undergo unrestricted consideration by the agency official, as indicated in 36 CFR 800.1(c), which directs the agency official to complete the Section 106 process in a manner "that such actions do not restrict the subsequent consideration of alternatives to avoid, minimize or mitigate the undertaking's adverse effects on historic properties."

4. We believe that the Alternative is presented in agreement with direction contained in 36 CFR 800.3(b) which directs the agency official to coordinate the Section 106 Review with "... the overall planning schedule for the undertaking and with any reviews required under other authorities..." including NEPA and Section 4(f) of the Department of Transportation Act. We further believe that guidance contained in 36 CFR 800.8(a)(1) encouraging agencies to "consider their section 106 responsibilities as early as possible in the NEPA process..." provides further impetus to consider the Alternative plan under both the 4(f) and NEPA processes.

5. We believe that the Alternative meets the identified Purpose and Need for the project. As stated in the May 2, 2014 Environmental Assessment, the Purpose and Need for the project is as follows;

"The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System."

We believe that the Alternative meets the purpose of the project by improving corridor connectivity for both motorists and pedestrians. The identified problems of congestion, substandard sight distance, and geometrics are all addressed in the Alternative. Roadway flooding as well is addressed, although this remains a concern for both the currently proposed project and the Alternative. The nearly adjacent and recently completed US 27 Bridge Reconstruction project (Des. No. 0200914 and 0101527) experienced roadway flooding a little over a year after the project's conclusion, demonstrating the need for a thorough investigation and remediation of a larger problem. Regarding the need for the project, we believe that the Alternative addresses the identified congestion, substandard sight distance, and geometrics concerns as well as the pedestrian safety concerns.

6. We believe that the Alternative significantly "minimizes, mitigates or avoids" adverse Effect to historic properties as is identified as a purpose of Section 106, and makes "special effort to preserve...historic sites" as is the stated purpose of Section 4(f).

Comparing the enumerated Adverse Effect of the current plan as stated on page 206-209 of the Environmental Assessment, the Alternative proposal results in far less damage under both Sections 106 and 4(f), making it a prudent and feasible alternative that causes the least overall harm. In the Fort Wayne Park and Boulevard System Historic District, and in the Brookview-Irvington Historic District, the Alternative avoids to a much greater degree the removal and disruption of the NRHP-listed State Boulevard, minimizes the amount of new land disruption by using land previously altered through the removal of "flood-buyout" houses, and retains the plan of the Arthur Shurcliff-designed plat. In addition, the Alternative avoids the demolition of NRHP-listed residences entirely. The Alternative mitigates the disruption and bifurcation of the Shurcliff-designed plat by the use of a curvilinear "new" State Boulevard replicating the scale of the "original" State Boulevard, providing the driver and pedestrian alike with a similar feel, design and setting.

7. We believe that the Draft Memorandum of Agreement (MOA) as presented in the May 2, 2014 Environmental Assessment and the June 5, 2014 Section 106 Review will need to be modified, particularly if the Alternative is adopted as the basis for proceeding forward in this project. We agree that the use of Context Sensitive Solutions (CSS) is preferred, but would like to see that any such CSS solutions be implemented where feasible rather than merely "considered" as is suggested in the Draft MOA.

Sincerely,



Michael Galbraith

Executive Director, ARCH, Inc.

Enc; "Alternative" Plan (SKA + Transportation Solutions for ARCH, Inc., Indiana Landmarks, et al)



storrow kinsella associates

urban design & planning for places | connections | strategies

December 9, 2013

Mr. Michael Galbraith
Executive Director
ARCH, Inc.
818 Lafayette Street
Fort Wayne, IN 46802

Re: State Boulevard Reconstruction – Alternative Concepts
Fort Wayne, Indiana

Dear Mike and Consulting Parties Team,

Storrow Kinsella Associates, in collaboration with Transportation Solutions, LLC has completed our analysis, evaluation and development of a sketch plan reconstruction alternative for the five-lane roadway widening project currently being proposed by the City of Fort Wayne.

We are confident that the resulting report provides a foundation for ARCH, Inc. and the Consulting Parties Team to have a constructive dialog with the City and its consultant. The *Consulting Parties Proposed Alternative* (CPPA) provides a solution that restores Spy Run Creek Parkway continuity, and maintains the integrity of the Brookview-Irvington historic neighborhood while meeting overall economic development, flood control, connectivity and beautification goals.

Please note that this report is a "sketch plan" alternative. We have made a good-faith effort to consider existing conditions and the purpose and need of the project, but must emphasize that additional design and study will be needed before this proposed alternative can be fully incorporated into the City's project.

The costs for the CPPA appear to be of similar magnitude or less than the City's Preferred Alternative, based on comparative lane-miles, bridge cross section and length, and probable right-of-way acquisition. In addition, the long term maintenance costs are comparable or less for the CPPA, based on thoroughfare lane length reduction from 5,175 to 2,100 feet.

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of the proposed alternative.

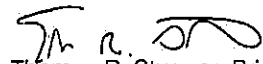
We remain available to answer questions and assist you and the City with moving an improved State Boulevard Reconstruction Project forward into implementation.

Sincerely,
STORROW KINSELLA ASSOCIATES


Margaret T. Storrow, Principal


John W. Kinsella, Principal

TRANSPORTATION SOLUTIONS, LLC


Thomas R. Sturmer, Principal

File: X:\1305_FW State Blvd\3Work\05Report\3Final\131209_CovLetter_ARCH_StateBlvd_1305.docx

724 North Park Avenue Indianapolis IN 46202 | 317.639.3420
www.storrowkinsella.com

Fort Wayne Historic State Boulevard Consulting Parties Proposed Alternative

to the City of Fort Wayne, Indiana, State Boulevard Reconstruction Project
INDOT DES# 0400587

prepared for
ARCH, Inc.
by

Storrow Kinsella Associates + Transportation Solutions, LLC

This investigation has been undertaken on behalf of ARCH, Inc., in response to a current City of Fort Wayne proposal to straighten and widen the portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. This section of State Boulevard is within the Brookview-Irvington Historic District neighborhood. Additionally, State Boulevard and Spy Run Creek Parkway, both components of the Fort Wayne Park and Boulevard System Historic District, intersect within the project area.

The project's impact on historic resources gives standing to the concerns of the Consulting Parties.

	Section
Purpose of this investigation	1
Description of the Consulting Parties Proposed Alternative	2
Clinton Street roundabout capacity	3
Pedestrian accommodation	4
Bicycle accommodation	5
Transit accommodation	6
Urban design considerations	7
Floodway/Floodplain considerations	8
Detailed development of the Consulting Parties Proposed Alternative	9
Cost discussion/comparative magnitude of cost	10
Summary of the Findings	11
Context Plan: Fort Wayne Park and Boulevard System Historic District	A
Proposed Alternative Sketch Plan	B
Comparison Chart	C
Comparison Plans	D

1 Purpose of this investigation

The intention of the investigation is to determine if there is a viable alternative to the City proposal, within the State Boulevard corridor, that reasonably addresses the connectivity and congestion mitigation purposes of that proposal, while avoiding its considerable impacts on the integrity of the overlapping historic districts through which it passes. The investigation does not address the viability of alternative corridors identified as part of the project's Section 106 process other than suggesting that they appear to need further study for their potential contribution to overall network east-west connectivity and congestion mitigation. Likewise the investigation does not address aspects of other State Boulevard project segments that lead to this focus area. It does suggest that lessons learned in this focus area could inform the larger system.

A description of the city's preferred alternative and critiques of its impacts by multiple consulting parties are available in the project's Section 106 documentation thus are not repeated here in the interest of brevity. The critiques include but are not limited to concerns regarding induced traffic, inappropriate scale, and disruption to the character and continuity of historic resources in the project area.

2 Description of the Consulting Parties Proposed Alternative (CPPA: See Exhibit B)

The CPPA diverts east-west crosstown traffic through the district of concern as a new 1000-foot+/- long *two-lane parkway* alignment, generally south of Spy Run Creek, a natural divide. It is intended to improve crosstown connectivity and relieve congestion in a manner that does not induce additional traffic volume and with scale and geometrics that respect the intrinsic qualities of both the Brookview-Irvington Historic District and the Historic Park and Boulevard System.

The transition to this parkway from existing State Boulevard occurs at Clinton Street on the east and at the Westbrook/Edgehill intersection on the west. That transition is enabled by a traffic calming single-lane roundabout at Westbrook/Edgehill, and by a two-lane signalized hybrid roundabout at the higher volume North Clinton Street intersection. The two roundabouts bookend a new terrain, uninterrupted two-lane parkway linkage as a system that modifies motorist behavior to a slower but steady-state stream between the roundabouts. This configuration allows less space-consuming geometrics (vertical and horizontal alignment and clear zone constraints) and much fewer vehicular conflict points, while accommodating expected volumes through operational efficiencies achieved by those reductions.

The CPPA parkway alignment replicates the scale and curvilinearity of the existing historic boulevard, while allowing the latter to revert to a low speed/low volume pedestrian-friendly local street and bicycle boulevard. Thus both the historic and the proposed new segment respect the characteristics of the Historic Park and Boulevard System, *and* the Brookview-Irvington Historic District neighborhoods, by their contextual scale and alignment.

A consideration for detailed development of this parkway alignment and its new crossing of Spy Run Creek, both of which will be raised above flood elevation (a project purpose), is that these elements be sensitively designed such that they integrate well with both the creek and with the Edgehill Avenue neighborhood. The narrow roadway cross-section will help facilitate that spatial integration by lessening the need for obtrusive retaining walls. For that reason a multi-purpose path is proposed to be separated from the roadway (other than at the bridge) as part of the existing pathway system, rather than as sidewalks adjacent to the roadway.

The provision of functionally interdependent roundabouts at east and west ends of the approximately 1000-foot distance of the proposed New State Boulevard alignment facilitates use of a two-lane configuration for this segment. The linked roundabouts will modulate traffic flow through this lower speed (but uninterrupted) segment such that the less-than-750 peak hour vehicles per lane per hour, as recorded in 2005, can be accommodated. A hybrid two-lane

roundabout is proposed to replace the Clinton Street/State Boulevard signalized intersection, while a single lane roundabout would occur at the Westbrook/Edgehill intersection with State Boulevard.

3 Clinton Street roundabout capacity

Clinton Street roundabout capacity is proposed to be maximized by several methods to allow the target traffic throughput discussed above:

A

A fairly new roundabout traffic management method, installation of metering signals at roundabout entries, creates gaps in dominant peak period flow to minimize excessive queues and delays at each successive downstream entry. Such roundabout signalization can be more effective than additional roundabout lanes, and can reduce the complexity associated with three-lane roundabouts. The signals would be controlled by queue detectors.¹

B

A left-turn by-pass lane is proposed from southbound Clinton to eastbound State Boulevard to reduce roundabout circulating traffic by an estimated 250 vehicles per hour, based on 2005 traffic volumes. It would be controlled by the queue detector system as well, and could further reduce the need for a third circulating lane.

C

Additional reduction of the Clinton-State roundabout circulating traffic can be affected, if necessary, by providing a westbound bypass (slip) lane from Historic State Boulevard to the new parkway segment, just west of its splitter island, and optionally from the new parkway's eastbound lane to southbound Clinton, immediately south of the roundabout. The contribution of either or both bypass lanes to roundabout efficiency should be determined during detailed roundabout design and weighed against the possible need for additional right-of-way to accommodate them.

4 Pedestrian accommodation

Pedestrian accommodation is shown through the roundabout for probable pedestrian routes and to provide connections to the Pufferbelly Trail system. *Accessibility Guidelines for Pedestrian Facilities in the Public Right of Way* (PROWAG) recommends signalization for pedestrian crosswalks at high-vehicular volume roundabouts, and requires them for crossings of two or more contiguous roundabout lanes. The required signalization can be integrated into the phasing of demand-cycles of the vehicular signal system discussed above for the Clinton Street roundabout to minimize disruption to vehicular flow while still accommodating pedestrian connectivity.

5 Bicycle accommodation

Bicycle travel through this district can be accommodated along Historic State Boulevard which, once converted to local traffic as proposed here, will be well-suited to become a bicycle boulevard. As a local street, all-way stops can be introduced along that segment for additional traffic-calming for bicycle and pedestrian safety. In the interest of a narrow roadway, multi-use paths at a separate and lower elevation alignment would replace sidewalks along the proposed, new two-lane parkway section.

¹ *National Cooperative Highway Research Program Report 672 Roundabouts: an Informational Guide Chapter 7/7.5.1, Signalization/Metering*

Should flooding or structural issues dictate removal of the State Boulevard Bridge, a proposed bicycle-pedestrian bridge in its location will provide additional neighborhood connectivity to the Pufferbelly Trail.

Pedestrian accommodations at the roundabouts should be configured to accommodate bicycles for those cyclists not comfortable riding with traffic through the roundabouts.

Additionally the existing trails along Spy Run Creek should be fully integrated with the proposed Pufferbelly Trail (see Exhibit B) to fulfill this project's multimodal objectives.

6 Transit accommodation

Citilink Route #8 serves this area along southbound North Clinton Street, paired with northbound Spy Run Avenue 800 feet to the east. The current North Clinton stop is in a travel lane immediately south of State Boulevard. The Consulting Parties recommend that a bus turnout be provided either south of the roundabout, or more preferably to north of the roundabout between building setback line and existing curb line, to minimize travel lane disruption, but requiring additional permanent right-of-way.

Citilink Route #6 uses east and westbound State Boulevard and north and southbound Westbrook Drive, and is potentially improved by the proposed roundabout at State/Westbrook. Paired in-lane bus stops are recommended on Westbrook just north of the roundabout to better serve this area.

7 Urban design considerations

The roundabout elements, if sensitively designed, can become gateway markers along the Clinton Street procession towards the city center, as well as become markers for this historic district along the park and boulevard system. The parkway section itself can become a beautiful passage through the convergence of the historic parkway and neighborhood, somewhat mending a route that has been compromised over many years of roadway expansion and ad hoc development prior to its historic designation. This is particularly important to a well-developed Section 4F argument that this intervention results in a net benefit to the historic resources it affects (or as in this case, celebrates). Leveraging the project to enhance Spy Run Creek Parkway as a public park, and reduction of existing traffic impacts to the historic neighborhood are compelling benefits that the original City Preferred Alternate could not claim but which the CPPA can....if executed well.

Another consideration is that Spy Run Creek Parkway was compromised several years ago when Westbrook Drive, a classic *City Beautiful* parkway along residential properties on one side and the meandering creek and variable open space of the park on the other, was terminated at Edgehill Avenue just south of State Boulevard, where it now enters a neighborhood street. The CPPA alignment along the south side of the creek restores much of the historic parkway's integrity by taking it to a more contextual terminus.

8 Floodway/Floodplain considerations

Floodway impacts appear lessened by the proposed alternative because of the reduced width of the two-lane roadway in addition to the provision of a comparable bridge opening along the new alignment. Spy Run Creek flood hydrology will require careful analysis and design such that this project lessens flood severity through removal of current impediments and through development of storage capacity potential of the open space surrounding the creek. That potential can be enlarged by investigation during the project's detailed design.

Fort Wayne Historic State Boulevard: Consulting Parties Proposed Alternative

Reference: INDOT DES# 0400587

12/10/2013

Page 5 of 6

9 Detailed development of the CPPA

Assuming agreement can be reached regarding this proposed alternative, continuing oversight regarding its detailed development and implementation should be integrated into the project development process. It is extremely important to the Consulting Parties that execution of roadway elements and their urban design setting be context sensitive in scale, materials and detail such that the vision of George Kessler and Arthur Shurcliff, for the Park and Boulevard System and for the Brookview-Irvington District, respectively, be honored and can become a model for how the city balances its infrastructure needs with its heritage. With this caveat, the Consulting Parties will support the city's effort to improve this section of State Boulevard.

10 Cost discussion: comparative magnitude of cost

Comparison of costs between the City Preferred Alternative (City) and the Consulting Parties Proposed Alternative (CPPA) are of relative magnitude based on predictable differentials of project scope. Detailed cost analysis is dependent on more detailed development of the proposed alternative design, as well as on a better understanding of the cost basis for the City alternative.

	CPPA	City	Comments
Roadway	2100 lane feet	5175 lane feet, landscaped center median	CPPA option may cost about half of City's alternative
Bridges	New two-lane/140-150' long vehicular bridge New 16' wide x 100' long bike-ped bridge Remove existing two-lane vehicular bridge or retrofit as local traffic and bike boulevard link	5-lane 150' long new vehicular bridge Remove existing two-lane bridge	CPPA bridge costs reduced significantly because of reduced cross section
Major intersections	Two-lane hybrid roundabout at North Clinton Street	One signalized intersection with additional turn lanes at North Clinton Street	Multi-lane hybrid roundabout will cost significantly more than improved conventional signalized intersection
Secondary intersections	One single lane roundabout (minimal secondary neighborhood road improvements)	Two five-lane intersections (including left turn lanes) and substantial reconfiguration of neighborhood streets	Simplified interface with neighborhood streets anticipated to result in a net cost reduction for these elements
Residential acquisition	0	15 (at \$55-75,000 average assessed valuation)	Residential acquisition much less (approaching zero)
Commercial acquisition	1 total, with partial impacts on 2 additional parcels	0	Commercial acquisition much more Combined residential and commercial acquisition-relocation expected to be similar

11 Summary of the Findings

1

The CPPA, as a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of this new facility.

2

The costs for the CPPA appear to be of similar magnitude or less than the City Preferred Alternative, based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The latter may be achieved through the offset of reduced residential relocation scope compensating for the higher individual valuation of commercial properties in general.

3

The long term maintenance costs are comparable or less for the CPPA, based on thoroughfare length reduction from 5175 to 2100 lane feet.

4

The CPPA provides greater safety through elimination of most intersection left turn conflicts, pedestrian/vehicle conflicts, and through the lower speeds associated with roundabouts, their approaches and linkages. Reduction in accident rates and their severity for roundabouts vs. conventional signalized intersections has been well documented by multiple FHWA and insurance industry studies utilizing data accumulated during the high rate of adoption of roundabouts by state and local agencies because of their safety and efficiency characteristics.

5

The CPPA minimizes negative impacts on historic properties and districts. The removal of existing traffic volume impacts is expected to result in neighborhood stabilization and reinvestment in the project area.

6

The CPPA provides an additional benefit to the historic districts by restoring the sense of Spy Run Creek/Westbrook Drive's parkway continuity, which had been compromised by earlier floodway-roadway improvements that terminated Westbrook Drive at Edgehill Avenue.

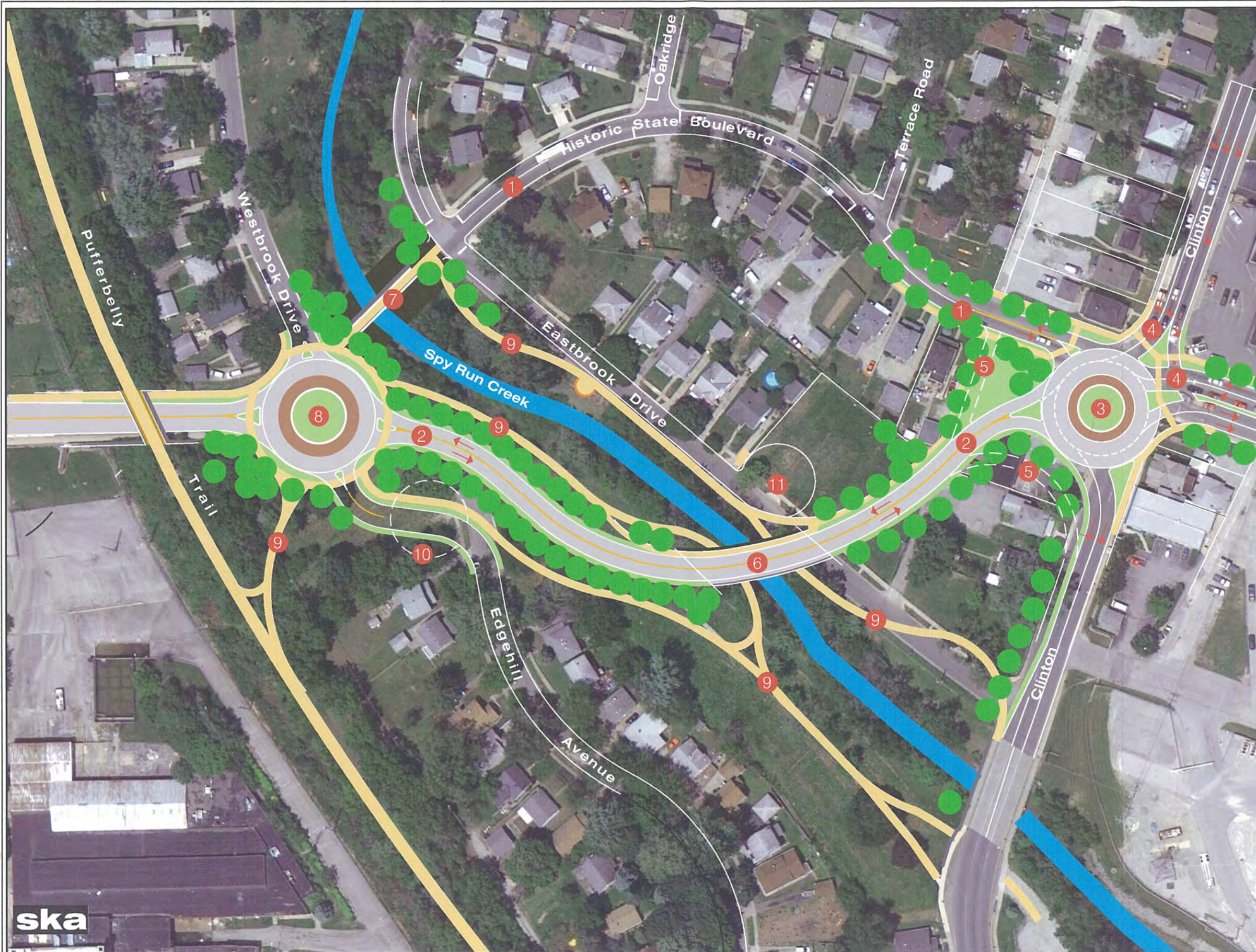
7

The CPPA meets the stated goals of the thoroughfare plan and reconciles that plan with the National Register-listed Park and Boulevard System.

8

The CPPA has the support of the affected neighborhoods and remonstrating parties which will facilitate the project moving forward expeditiously.

© 2013 SKA X:\1305_FW State Blvd\3Work\04Draw\ExhibitB_CPPA_1305.dwg 12/10/13



Plan Keynotes

- 1 Historic State Boulevard convert as local street & bike boulevard
- 2 New State Boulevard alignment
- 3 Clinton/State 2-lane hybrid urban roundabout with signalized eastbound left turn bypass
- 4 "Smart" roundabout signal -metered approaches to create gaps and balance flow during peak periods, and to provide pedestrian crossing synchronized with those phases (short/rolling yield or stop signal phases)
- 5 Optional bypass lanes to reduce roundabout circulating traffic loads
- 6 New Spy Run Creek 2-lane bridge
- 7 Spy Run Creek bike/ped bridge
 - Retrofit existing bridge or
 - New multi-use path bridge
- 8 Westbrook/Edgehill single lane urban roundabout w/ local street access
- 9 Bike-Ped path system
- 10 Edgehill Avenue cul de sac alternative
- 11 Eastbrook Drive cul de sac

Note: this is a sketch-level diagrammatic concept drawing. Roadway elements are proximate in scale and intended to illustrate general feasibility and proof of concept. Additional traffic engineering and roadway geometric study is required for project scope development.



0 50' 100'
Scale 1"=50'-0"

December 9, 2013

Storow Kinsella Associates urban design & planning for places | connections | strategies
in collaboration with
Transportation Solutions, LLC. | connecting the dots

Consulting Parties:
ARCH, Inc./ Historic Landmarks Foundation of Indiana
Brookview-Irvington Neighborhood Associations

Fort Wayne Historic State Boulevard
Consulting Parties Sketch Plan Alternatives Study
City of Fort Wayne Project/INDOT DES# 0400587

Sketch Plan

B

BENEFITS COMPARISON

Alternative	Connectivity Improvement	New roadway alignment length	Reconstruction alignment length	Residential Impacts	Commercial Impact	Total historic relocations or impacts	Other impacts	State Blvd. historic bridge impacts	State Boulevard Safety Improvement			Congestion Improvement	Reduce flooding	Alternative	
									Motorists	Bicyclists	Pedestrians			Feasible?	Prudent?
City's Preferred Alternative	Regional connectivity improved for through motorist, bicyclist, and pedestrian movements. Connectivity reduced for neighborhood residents.	1035 LF 5 lanes sidewalk or trail both sides	5175 lane feet 2070 sidewalk feet	15	None?	State Blvd historic parkway system compromised. Neighborhood bisected by new raised alignment. Historic bridge removed. Traffic speed and volumes increased through neighborhood.	View sheds affected for some residents. Historic neighborhood context affected. Five-lane raised roadway out of scale with residential neighborhood.	Historic bridge to be removed and replaced for flood elevation and structural deficiency reasons.	A portion of Old State Boulevard will be converted to local use with significant reduction in traffic volumes. New east-west alignment designed to current federal standards. Intersection capacities improved.	No bicycle accommodation currently exists. New multi-use path provided along new alignment.	New sidewalks provided along new alignment. Appropriate pedestrian crossings assumed to be provided at intersections.	Added lanes and intersection improvements reduce travel delays in overall system. The localized portion of State Boulevard will be a lower speed travel environment.	Raised elevation of State Boulevard and larger hydraulic bridge opening will help to reduce localized flooding frequency.	Yes	This is the City's Preferred Alternative. The Purpose and Need items are addressed, however the impact to the historic neighborhood and parkway system is significant.
Consulting Parties Proposed Alternative	State Boulevard corridor (and Greenways Trail System) connectivity improved for through motorist, bicyclist, and pedestrian movements. Historic State Boulevard will function as bicycle boulevard.	1048 LF 2 lanes 2 sidewalks	2096 lane feet 2096 sidewalk feet	One loss of alley access	Three: 1) Gas Station Store acquisition or relocation; 2) impact to garage and storage shed; 3) site impact.	Minimum historic structure impacts. State Boulevard historic integrity respected. Historic Bridge removed or rehabilitated for pedestrian/bicycle use because of deteriorated condition.	One commercial property on south side of State Boulevard at Clinton Street relocated to allow construction of a five-legged hybrid roundabout. Partial impacts on 2 additional commercial properties.	Historic bridge to be rehabilitated or removed and replaced for flood elevation and structural deficiency reasons. Pedestrian bridge with narrower profile impacts flooding to a lesser degree and provides east/west connectivity.	Significantly reduced volumes on existing State Boulevard alignment improves safety. Lower posted speed limit possible. New alignment meets goal of regional connectivity.	Existing State Boulevard converted to a low volume shared use roadway suitable for travel by bicycle.	Vehicle volumes greatly reduced along existing alignment. Sidewalks to be improved along existing alignment and provided along the new alignment.	Regional through traffic given more direct route. Intersection levels-of-service may be improved by a "smart" roundabout at Clinton and a single-lane roundabout at Westbrook.	Flood frequency reduced along new alignment due to raised bridge and roadway elevation.	Yes	This option addresses the Purpose and Need with less impacts to the historic resources. Magnitude of costs similar to the City's Preferred Alternative.

COST (RELATIVE) COMPARISON

Alternative	New alignment length	New alignment width	Bridge length	Bridge width	Bridge elevation	Residential relocations	Other residential impacts	Commercial relocations	Intersection Types			Bridge removals	New pedestrian bridges	Local Streets Affected	
									Westbrook Drive	Oakridge Drive	Clinton Street			Pavement removed	Pavement added/rehabilitated
City's Preferred Alternative	1035 Linear Feet (5175 lane feet)	5 Travel Lanes + 2 Sidewalks	150 LF 750 lane feet	Five lanes, curb and gutter, sidewalks and a 10-foot multi-use path on one side.	Bridge raised 7-feet above existing bridge elevation.	15 structures: Est. value \$1M	Any?	None?	Two-way stop controlled + energy and maintenance costs.	New Side-street stop controlled intersection.	Three-way signalized intersection + energy and maintenance costs.	Existing historic bridge.	One over State Boulevard for Pufferbelly Trail.	In vicinity of existing bridge and at both ends of existing alignment between Spy Run Creek and Clinton Street.	Oakridge Drive connector, "bulbouts " at termini of existing alignment.
Consulting Parties Proposed Alternative	1050 Linear Feet (2010 lane feet)	2 Travel Lanes + 2 Sidewalks	140 LF 280 lane feet	Two lanes, curb and gutter, and sidewalks both sides.	Same.	Two residences south of Clinton Street roundabout?	Alley access impacted for one residence. Net Benfit to Contributing Structures in District.	1 complete take and 2 partial impacts: unknown cost.	Single-lane roundabout. Est. cost: \$500,000 + landscape maintenance costs.	No new intersection.	Multi-lane hybrid roundabout with traffic metering signalization: Est. cost: \$2M + energy and maintenance costs.	Existing historic bridge rehabilitated or replaced.	One over State Boulevard for Pufferbelly Trail, replace existing State Boulevard bridge with a new pedestrian or rehabilitate existing bridge.	In vicinity of existing bridge only.	Rehabilitate existing State Boulevard alignment to create bike boulevard and pedestrian connector.

Description: City's Preferred Alternative	New four lane roadway with raised median and/or center turn lane for that portion of State Boulevard that lies between North Clinton Street and Westbrook Drive. Realigned section raised up to 7-feet at new bridge for floodway consideration. New sidewalks and/or multi-use side path along both sides of roadway. New pedestrian bridge and approach ramps for future Pufferbelly Trail.
Description: Consulting Parties Proposed Alternative	Relocate the thoroughfare function of State Boulevard to south of Spy Run Creek from Westbrook Drive east to Clinton Street to minimize impacts to historic properties and parkway. Develop the thoroughfare as a two-lane roadway with sidewalks on both sides between a single-lane roundabout at Westbrook and two-lane hybrid roundabout at Clinton and State with no intersections between them to optimize flow and volume. The roundabouts help condition traffic to a steady state at a reduced speed. The Clinton/State intersection is a "smart" roundabout using advanced technologies consisting of signal metered approaches and traffic sensing to help balance flow, create gaps, and provide for pedestrian connectivity through the roundabout area. Existing bridge rehabilitated or replaced with new pedestrian/bicycle bridge to provide connectivity with future Pufferbelly Trail. Sidewalks along existing alignment to be improved.

Note: Information obtained from Section 106 Findings of Adverse Impacts report (approved August 27, 2012) and other information shared by ARCH. Concept plans for City alternatives not available at this time.

December 9, 2013



City's Preferred Alternative



Purpose and Need

Shown above is a model of the City's Preferred Alternative; a new four lane roadway with raised median and/or center turn lane between North Clinton Street and Westbrook Drive. It is replacing a section of the existing two-lane State Boulevard in the City of Fort Wayne Park and Boulevard System Historic District.

The Park and Boulevard System Historic District represents a thoroughfare system designed in the early 1900's for the purpose of economic development, flood control, connectivity, and beauty. The proposed City's Preferred Alternative has many of the same purposes and characteristics, and, without reference to context, will be a significant upgrade to the City's thoroughfare system. However the segment between Clinton Street and Westbrook Drive does have historic context that the scale of the new boulevard will compromise.



Consulting Parties Proposed Alternative



Scale and Appropriateness

Shown above is the Consulting Parties Proposed Alternative, featuring a two-lane roadway able to accommodate the 2005 peak hour traffic volume of 750 vehicles per hour per lane. It restores Spy Run Creek Parkway continuity and maintains the integrity of the Brookview-Irvington Historic neighborhood.

The costs of the Consulting Parties proposed alternative appear to be of similar magnitude or less than the City's Preferred Alternative based on comparative lane-miles, bridge cross-section and length, and probable acquisition scope. The long term maintenance costs are comparable or less based on thoroughfare length reduction from 5175 to 2100 lane feet.

Note: Model of proposed City Preferred Alternative at Spy Run Creek prepared by ARCH, Inc.

December 9, 2013

City of Fort Wayne

Park and Boulevard System

Historic District, Fort Wayne, Allen County, Indiana

Key Map

 National Register Boundary

Parks

1. Franke
2. McCormick
3. McCulloch
4. McMillen
5. Memorial
6. Nuckols
7. Old Fort
8. Reservoir
9. Rockhill
10. Weisser
11. Williams

Parkways (includes riverfront parks):

- I. Maumee River
 - i. Lakeside
- II. Spy Run Creek (Brookview)
 - i. Lawton
 - ii. Vesey
- III. St. Joseph River
 - i. Johnny Appleseed
- IV. St. Mary's River
 - i. Bloomingdale
 - ii. Camp Allen
 - iii. Foster
 - iv. Guldlin
 - v. Orff/Thieme Drive Overlook
 - vi. Roosevelt
 - vii. Swinney (East & West)

Boulevards:

- a. Anthony Boulevard
- b. Berry Street
- c. Hanna/Taber Street
- d. Jefferson Boulevard
- e. Lindenwood Avenue (Brookside)
- f. Rudisill Boulevard
- g. Sherman Boulevard (Kekionga)
- h. St. Joseph Boulevard
- i. State Boulevard (Pfeifer)
- j. Tennessee Avenue/Lake Avenue

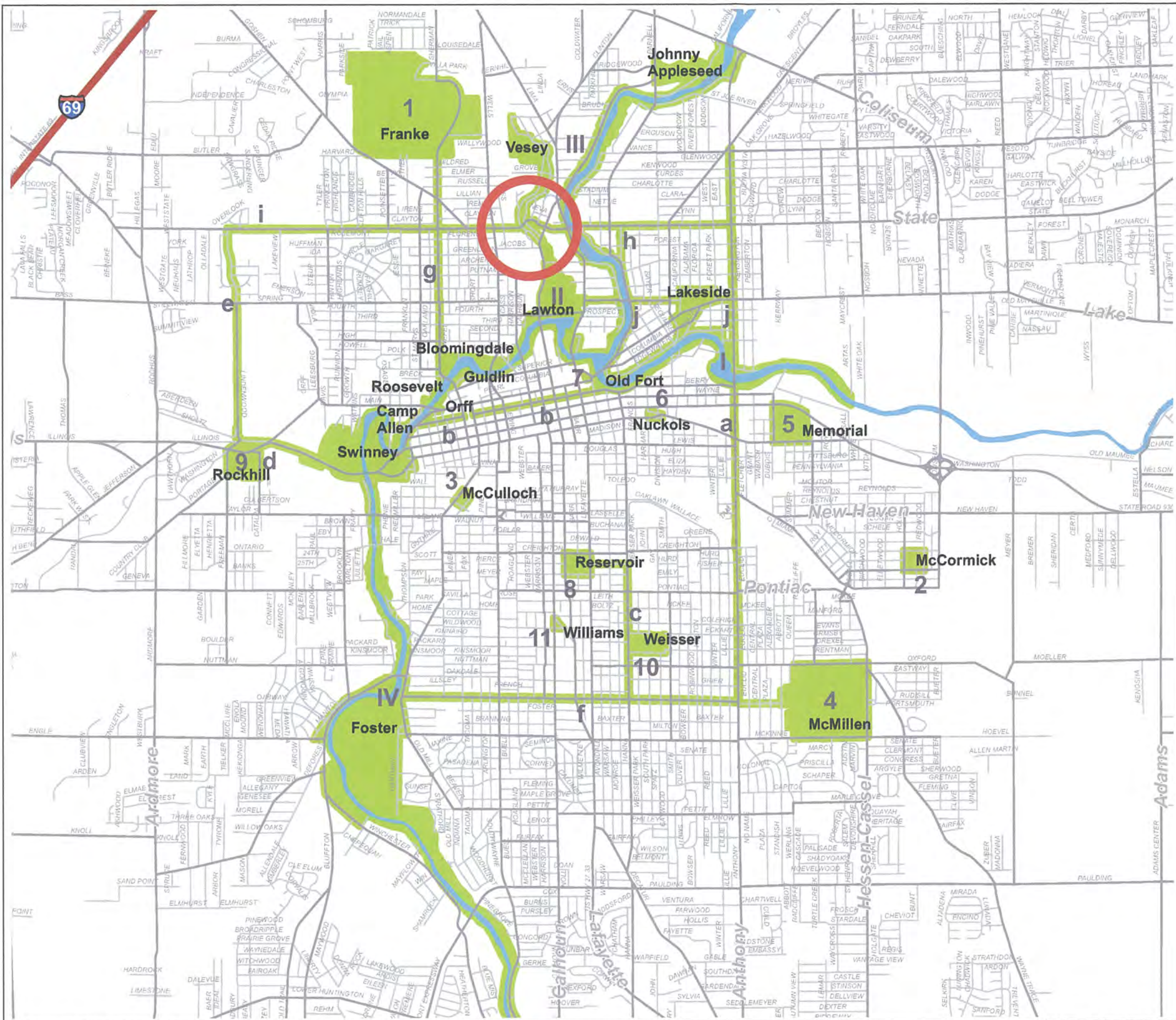


Note: This exhibit based on City of Fort Wayne Park and Boulevard System Historic District Key Map, prepared by The Westerly Group, Inc. and Storrow Kinsella Associates, 5/24/2010.



Study Area

December 9, 2013



Storrow Kinsella Associates urban design & planning for places | connections | strategies
in collaboration with
Transportation Solutions, LLC. | connecting the dots

Consulting Parties:
ARCH, Inc./ Historic Landmarks Foundation of Indiana
Brookview-Irvington Neighborhood Associations

Fort Wayne Historic State Boulevard
Consulting Parties Sketch Plan Alternatives Study
City of Fort Wayne Project/INDOT DES# 0400587

Context Map

A

July 17, 2014

Regarding State Boulevard, Fort Wayne, IN

To whom it may concern:

Greetings and Salutations My Friends,

I am writing in brief to express my opposition to the city's proposed plan (which they have called alternative 3A) for the State Street project. I do believe that this proposal if implemented will not only shun the citizens that live in the Eastbrook and Westbrook and rest of the Brookview Neighborhood area but will also create a greater health risk to our area's children going to and from school. It will also totally negate the integrity and natural beauty of one of our city's oldest and characteristic parks, Brookview Park, and of a big part of our neighborhood. For numerous years now the city has been planning a renovation of this area of town. I agree that something can be done here, due to high levels of traffic, and I want to believe a true and genuine concern for the citizens that live here (including myself and my wife at 2740 Eastbrook Dr.) plus a hill that keeps the guard rail manufacturers in business year round. These school children are the city's future, our future council-people and civil servants and possibly even our Mayor, and they will have to maneuver the speedy cacophony of traffic to accomplish their education. The current slow curve that State Street follows was built that way for a purpose; to slow people down to enjoy an oasis of country life while living in the city.

During recent years during this planning I have sensed a great lack of transparency from the city. My feelings about that were confirmed when in 2008 I learned from a neighbor who had phoned the city for information and shared it with the rest of us living in the area and who invited the City engineer to come and meet with us, not from the City asking, or even telling us of their plans, just three weeks before demolition of Westbrook Drive south of State Street where the city was installing a Rain Garden south of State. This was an attempt to address the flooding issue and I respect that, and appreciate that our neighborhood's intervention in the city plans there informed us and also managed to keep the sidewalk that follows the former Westbrook Drive that takes you to Jacobs and Clinton St.

I am in support of the conceptual 2-lane alternate proposal to the city's plan presented recently by ARCH. I do believe this plan will be all-inclusive just by its simplicity. I get a sense of it being personable and friendly. The route that this alternate plan suggests would lower the number of houses that would need to be demolished. The curve and two round-a-bouts will keep traffic in a safe and controlled environment and slow people down to enjoy an oasis of country life while living in the city. The insertion of bicycle paths will keep the children and others away from the traffic and allow them to grow up and be inspired in their own civil careers.

Change is not hard, resistance to change is hard.

Thank you for your attention,

Kevin Donovan and Lori Donovan

2740 Eastbrook Drive, Fort Wayne, 46805

Brookview Neighborhood

4/17/14

Over the past few years, an emphasis has been placed on re-energizing downtown Fort Wayne. Part of the plan includes encouraging people to live in the city proper. I have resided in Irvington Park for the past twenty-one years with an appreciation for this unique, historical neighborhood.

Residents of this area and adjoining Brookview talk frequently about preserving the ambience that makes living here attractive. The widening of State Street will impact this area in a way that concerns me. If it isn't completed with neighborhood preservation as a priority, the project will be a serious blow to the area's integrity. Well maintained family homes, surrounded by old growth trees, a creek and wetlands should be treasured.

Careful planning can and should be used to insure that one of Fort Wayne's best residential areas remains intact.

Granted, State Street is in need of some kind of re-design. My hope is that this can be accomplished with minimal impact. Neighborhoods like Brookview and Irvington Park should be valued for their unique character and preserved.

James F. Mowbr
3330 Eastbrook Dr.
Fort Wayne, IN
46805

P.S. I'm opposed to cities plan 3-A concerning State Street widening.

I would support a plan that would incorporate some of the ideas expressed by Storrow Kinsella/transportation solutions LLC.

State Blvd Recon; project 0400587*Alternative "3A"*

Barbara Puffin <720barbara@gmail.com>

Fri, Jul 18, 2014 at 4:05 PM

Draft To: bhope@structurepoint.com

Hello Briana and company,

I moved into my house on Eastbrook Drive in the Brookview Addition in Fort Wayne, Indiana in November of 1980. The thing that drew me to it was the neighbourhood. I was introduced to the neighbourhood by a friend that lived in a lovely little white house with a lovely veranda at the curve that follows the creek north of State Boulevard. We bought our house; I loved the neighbourhood—it reminded me of New England with curves and hills. About a year after we bought our house, our friends moved to a house two houses toward State because their family was growing and they wanted to stay in the neighbourhood with its character and thoughtful design (she was an interior designer) and I bought the little white house from them for my mother to move into; she moved from Massachusetts where I was raised. My house sits five houses to the north of the little white house, making my house one block north of State.

So, I have lived here awhile and have a fair amount of emotional investment in addition to whatever financial investment would be figured into that equation. I raised a daughter in this neighbourhood. She saved a kitty from crossing State Blvd when she was nine. The saved kitty went to graduate school with her some years later.

That's a little background of why this neighborhood means something and I'd like to see that respected. I didn't find out until 2008 why the neighborhood reminded me of my home in New England. It was designed by Arthur Shurcliff, native of Boston—and I had grown up 13 miles southwest of Boston.

What I am writing to you about is to encourage that, for the State Blvd reconstruction, the proposed plan created by Storrow/Kinsela Transportation Solutions, LLC, presented by ARCH this past June to be the design, because I think it would have been something that Shurcliff would have designed. It respects the historic nature and flavour in a way that the the city of Fort Wayne engineers have not in their proposed solution to handle the traffic issues. Progress is here, needs are understood, but let us do the work with respect to the ambience. With no turns on the "Brookview bypass", east/west traffic would flow nicely without interruption. With the roundabouts, the flow would be enhanced.

This project issue has gone on way too long. Let's make it a statement; this beautiful merging of design old and new, with sensitivity to the historical nature of the area.

Respectfully,



Barbara A. Brenneman
2526 Eastbrook Drive, Fort Wayne, IN 46805

Please note: ~~I will be printing this out and sending it via US mail as well.~~ I know how busy your inbox is in the cyberworld.

*As you can see, you should
have a copy in your email
in box as well.*

SINES & COMPANY

701 SOUTH CLINTON STREET, SUITE 322

FORT WAYNE, INDIANA 46802

TEL (219) 420-7201

FAX (219) 422-9201

7/18/14

To: American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax (317) 543-0270

Dear Ms Hope:
Enclosed is my Comment
Sheet.

Thank you,

A large, stylized handwritten signature in cursive script, likely belonging to J. Sines, written in dark ink.



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by **Friday, July 18, 2014**. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

→ American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date:

Fri
Wednesday, June 18, 2014

Project:

State Boulevard Reconstruction Project

Fort Wayne

Name: (Please print)

James W. Sines

Address:

3322 Irvington Drive, Fort Wayne, IN 46805

COMMENTS:

As a resident of Irvington Park for over 37 years, and a former Association President, I am concerned about the State Boulevard Reconstruction Project proposed by the City of Fort Wayne. This plan calls for a 4 lane route straightened from the current State Boulevard route. This plan does not provide for safe or sufficient access to and from other neighborhood streets. Instead, I strongly favor an alternative plan proposed by ARCH and Indiana Landmarks calling for a 2 lane route, curving south of the current route, with traffic lights and dedicated connections, and allowing for safe turns onto these neighborhood streets.

SIGNATURE:

James W. Sines

7/16/14

IN REFERENCE TO:

ALTERNATIVE CITY WANTS/ PLANS:

3A

ALTERNATIVE PROPOSED BY COMMUNITY:

PREPARED BY: STORROW KINSELLA/
TRANSPORTATION SOLUTIONS, LLC

PRESENTED 6/20/14 BY ARCH

I AM FOR THIS PLAN TO WIDEN
STATE STREET BY MOVING IT FURTHER
SOUTH, PRESENTED BY ARCH. I LIKE
THAT HEAVY TRAFFIC WILL NOT DIRECTLY
BORDER MY HISTORIC NEIGHBORHOOD. MY
STREET SEES A LOT OF "CUTTHROUGH" TRAFFIC, FROM
CLINTON TO STATE, MAKING IT DANGEROUS FOR
KIDS AT PLAY. THIS PLAN WOULD HELP PREVENT
THIS, I ALSO LIKE THAT OUR NEIGHBORHOOD
WILL NOT BE DIVIDED BY 5-LANES OF ROAD
AS IN THE CITY'S PLAN. THE CITY'S PLAN ALSO
MAKES IT DIFFICULT FOR ME TO TURN LEFT/EAST
ON STATE. HOPING YOU REALLY CONSIDER THIS NEW
DESIGN.

2510 TERRACE RD.

FT. WAYNE, IN 46805

Sincerely,

Julie Ratliff

JULIE RATLIFF



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by **Friday, July 18, 2014**. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date: **Wednesday, June 18, 2014**
Project: **State Boulevard Reconstruction Project**

Name: (Please print) Ms. Nigel J. Perry
Address: 2134 Cass St.
Fort Wayne, IN 46808

COMMENTS: I would like the Spy Run to Cass Street project to be limited to 3 lanes. The 5 lane project will have a greater impact on the environment. 5 lanes also means more money for maintenance. I don't like the median that has to be maintained as it grows. I do not want State Blvd to become a east west truck route. The reason there are so many accidents on the south corner of Westbrook Dr. and State Blvd is speed. I did not hear mentioned how wrecker from Kelley Shrecker Service is to negotiate the new lanes. I bet Cass Street from State to Jacobs Avenue will be used. School buses will also have to use the narrower lanes proposed. In winter this seems to be an added safety concern.

(over)

SIGNATURE: Ms. Nigel J. Perry

44
This is a 2 issue problem. It will not solve the flooding problem of the Spy Run Creek. The flooding is definitely an environmental problem and raising the bridge to 9 feet will do little to help. Fort Payne goofed when building started North of the City and no retainer ponds were required.

To put a 5 lane expressway in will divide ^{the} North from downtown, ~~which the~~ discouraging citizens from enjoying the progress the city planners has made with the downtown area.

They have Coliseum Blvd for trucks ^{and} ~~an~~ I 469 clear around the city.

Who wants to use the Rufferbelly Trail with all the emissions from the trucks to ruin our lungs).

Fix the bridge, the sidewalks for safety, enforce the speed limit we now have in force so cars and trucks don't endanger homes along the ~~state~~ Blvd.

The City should buy out the homes of the people that have been promised a buyout for almost 7-8 years.

Thank You for listening. Sincerely, ^{Ms.} Nigel D. Perry



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by **Friday, July 18, 2014**. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date: **Wednesday, June 18, 2014**
Project: **State Boulevard Reconstruction Project**

Name: (Please print) JULIANNA FLEISCHER

Address: 2201 CASS STREET

COMMENTS: I agree with others at the meeting
of the importance of addressing the
continual flooding dilemma. It would
be a waste of time and money to
complete the State Boulevard Reconstruction
Project and still have flooding. It's
a shame the \$4.4 million earmarked
for a "round-about" at Wells and
Fairfield can't be used to address
and resolve the flooding situation.
If there are alternatives to the current
proposal can we hear about them
first. I don't oppose the project
but I do still fear the access
for residents in 2100 and 2200 block
of Cass along with access from
Oakridge has not been →

SIGNATURE: _____

addressed. We keep being told that with 4 lanes it will make it easier for us to access State Blvd.

I know I represent only a 2 block area on Cass Street but please don't let us fall into the bureaucratic pit.

It is currently difficult to access State Blvd from 2100, 2200 block of Cass and the same is true if we use Jacobs and try to access Wells St. (specially trying to make a left turn)

In ending, please just start the project so we can see an end.

^{The area}
It gets more run down every year because people do not want to maintain properties the city will end of buying. I hope these people are paid the "fair market value" of their properties back in 2008 before this all started. Hope things can be resolved soon for everyone.

Julianne Flescher

JOHN H. SHOAF
FORT WAYNE CITY COUNCILMAN

4646 West Jefferson Boulevard
phone: 260 432 7178

Fort Wayne, Indiana 46804
email: jhshoaff@proparkwest.com

July 15, 2014

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project Des. No. 0400587
Response to the environmental document,
Section 106 review, and preliminary design plans

Dear Ms. Hope,

Because good CSS (context sensitive solutions) practices were not observed, and because the engineers (American Structurepoint, Inc.) were required by the terms of a City RFP of November 29, 2007 to engineer a five lane roadway of city-specified contour, they were not free or well informed enough to pursue an optimal solution to the State Boulevard situation. In fact they were required to produce a plan that has been determined to have an adverse impact on the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the Bridge over Spy Run.

Acting on better information about the historical importance of the Brookview and Irvington Park neighborhoods, and in response to widespread opposition to the city's current proposal voiced repeatedly after the city's intentions as expressed in the 2007 RFP were made known, Indiana Landmarks, ARCH, and the Friends of the Parks of Allen County retained the services of well regarded engineers and planners Storrow-Kinsella Associates, whose firm produced a plan that demonstrates a "prudent and feasible alternative" to the AS plan and its multiple adverse impacts. Because it would be good for the city, and because it meets the requirement of federal statute that a prudent and feasible alternative, when available, must be chosen over proposals that have adverse impacts, this plan should be the basis for the final plan.

The plan proposed by SKA holds great promise for the directly affected Irvington Park and Brookview neighborhoods and for the city of Fort Wayne at large. Relative to the five-lane plan dictated by the city, the SK plan has only two lanes, and they are gracefully curved and moved well south of the Brookview neighborhood. In scale and character they complement the roads laid out by Arthur Shurcliff. Reduction to two lanes significantly reduces hard surface and the large amounts of fill that will be brought into a floodplain, greatly reducing the road's contribution to the flash flooding that is such serious threat to this neighborhood. It also reduces the heat island effect, and avoids the imposition of an overscaled, out-of-character roadway that will overwhelm the modestly scaled, charming neighborhood. Planners call major traffic carriers of this scale

“barriers”, and this one will create the appearance of a barrier between this neighborhood and downtown, at the very time we should be strengthening the relationship between them.

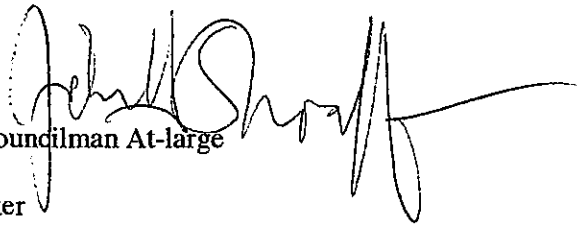
The city’s preferred plan is a dinosaur, an anachronism dragged from the 20th Century into the 21st: the concept for the city’s plan goes back 30 or 40 years, when migration from the city was in full sway, and traffic volumes were growing everywhere. Now however, things are very different. Traffic volumes nationwide peaked in 2007, fell significantly in the recession, and then leveled off at lower levels. With respect to State Boulevard, Dana is stabilizing its operation at about 1/10 its former work force, and Parkview hospital has largely moved out, significantly lowering present and future traffic needs. Gasoline tax revenues have fallen, and the Federal Highway Fund faces bankruptcy. Furthermore, cities everywhere are learning that they must rebuild their federally-paid-for projects at their own expense. Cities - including Fort Wayne - are learning that after overbuilding their road systems (a common consensus among urban planners) in the 20th century, they must now repair and rebuild them with declining gas tax revenues in the 21st.

And we live with other unpleasant consequences of overbuilding; we have exacerbated urban flash flooding; we increase the heat island effect and unhealthy air. We all intuitively understand that vehicle exhaust is unhealthy, but now studies now have found definitive correlations between living near heavy traffic and heart disease, respiratory problems, and autism; plans like the city-preferred solution that appear to be designed to attract new traffic into heavily populated areas should be avoided.

The proper solution to the State Boulevard problems should be as economical of hard surfaces and as modest in scale as conditions and traffic needs permit, and above all it should support a major and very encouraging trend of our times - the powerful counter-migration back into the city. One of the attractions to the many who now seek the urban experience is the charm, uniqueness, and closeness to downtown of a city’s historic neighborhoods. The best way a city can foster this counter-migration is to protect and enhance the pleasure of living in these neighborhoods. This means, among other things, slower traffic, cleaner air, and roads scaled to a neighborhood’s residential scale.

SKA has demonstrated that a road fully responsive to the traffic needs of the area can be two lanes wide, a significant distance from the homes, and in character and scale much more like the roads of the Brookview neighborhood: it is the road that Arthur Shurcliff might have designed himself, and it or something like it is the road design that should be part of the historic Brookview neighborhood.

Sincerely,
John H. Shoaff
Fort Wayne City Councilman At-large

A handwritten signature in black ink, appearing to read 'John H. Shoaff', with a long horizontal flourish extending to the right.

Cc: Patrick Carpenter
Chad Slider
Joyce Newland
Todd Zeiger
Michael Galbraith

From: [Rene" Jackson](#)
To: [Hope, Briana](#)
Date: Friday, July 18, 2014 11:29:38 PM

Dear Briana,

As a former resident of Fort Wayne, I am very concerned about the plan to build a multi-lane arterial through one of the cities quaint historic neighborhoods. I understand that this plan is going to require 12-15 homes being demolished in this historic district. If there is an alternative that does not have this severe impact on the Brookview-Irvington Park neighborhood and can preserve the character of the neighborhood, it should be the preferred approach to the project. Fort Wayne's historic urban core and vibrant downtown are among its most appealing qualities, they should be preserved and protected.

Sincerely,
Rene' L Jackson

From: [Michelle Briggs Wedaman](#)
To: [Hope, Briana](#)
Subject: abbreviated comments on City of Fort Wayne, IN State Boulevard Reconstruction Project, Comments in response to Environmental Assessment Des. No. 0400587
Date: Friday, July 18, 2014 11:01:06 PM

July 18, 2014

RE: City of Fort Wayne, IN State Boulevard Reconstruction Project
Comments in response to Environmental Assessment Des. No. 0400587, dated May 2, 2014

Please note that these are my abbreviated comments and that I am also submitting extended comments this weekend with some attachments. I respectfully request that my comments be combined and considered in their entirety. Thanks.

To all whom it concerns:

My family and I live in the Brookview Neighborhood at 2326 Eastbrook Drive and also own 2418 Eastbrook Drive, both properties in the block north of the planned project area. A Consulting Party on this project, I am writing in strong opposition to the City's preferred alternative (3A) for State Blvd reconstruction. Drawn to Fort Wayne from Seattle by the pleasing center-city tranquility of this City Beautiful neighborhood where my family has owned homes since the 1930s, I can speak to the deep personal attachment so many residents have for this welcoming and appealing historic neighborhood of curving streets and sidewalks and parks, where repair and improved care of public spaces is needed and welcomed.

However, rather than an improvement upon existing conditions, I feel that the City's plan (3A) will be highly damaging to overall safety and accessibility for users traveling along and seeking to cross State Boulevard (vehicle traffic, walkers, bikers, and public transit and school buses), safety in our surrounding neighborhood, and will also negatively impact neighborhood flash flooding, neighborhood character, quality of life and our property values.

The City points to numerous community meetings, and though they have modified their plan to connect Oakridge Road rather than Terrace/Eastbrook to "new State", have included some appropriate street lighting, landscaping and sidewalk/trail and finally agreed in 2013 to provide drawings of their proposal for the public to see, the most important element of the plan – the new road's straight route and 5+ lane width - have not changed from the City's "preliminary route drawing" I saw in July/August 2008 when I contacted the city engineer to inquire about the State Blvd. project (and learned of the City's immediate and uncommunicated plans to remove Westbrook Drive south of State and install a Rain Garden at that site) and invited him to a walk-around on-site neighborhood meeting, prior to the City hiring American Structure Point to explore and engineer possible alternatives.

The City's stated need for the project – to improve State Blvd. roadway flooding, replace its aging Spy Run Creek bridge and relieve vehicle congestion – can be addressed with solutions not yet publicly considered by the City. Less damaging alternatives for road width and route within the neighborhood have not been presented or discussed, despite my and others' repeated requests for them over the past six years.

I am writing in strong support of the Storrow Kinsella Associates /Transportation Solutions LLC-prepared conceptual solution publicly presented by ARCH/Indiana Landmarks and others in June 2014, and urge that the City immediately be required to pursue that concepts as the preferred alternative. Maintaining State Blvd's two lanes, curving it gently south and allowing dedicated neighborhood street exits/entrances to "new State" at Westbrook Drive and "Old State Blvd at Clinton St/US 27" via roundabouts will allow neighborhood vehicle traffic to safely enter and travel both east and west on State Blvd from both north and south of State Blvd, elevate the bridge over Spy Run Creek, enhance traffic flow on State Blvd and be in keeping with the neighborhood's character, design and family-friendly residential value. I urge pursuit of this promising and appealing solution, and ask again why the City has been refusing to even discuss it as a possibility with community and professional representatives seeking to do so for the past year, and why the City did not create such a feasible and less impactful alternative themselves, one that meets the project's stated Purpose and Need, incorporates best practices for traffic calming with its smaller footprint while meeting the needs of improved traffic flow, would be appealing and safe to walkers and bikers, enhance rather than destroy the Districts, and that I and many of my fellow residents will be excited about and welcome.

I urge that the City be actively guided and assisted by the Federal Highways Administration in following the FHA-recommended Context Sensitive Solutions for achieving the superior design that meaningfully considers, incorporates and better meets the needs of residents like myself who live in the project shadow. The project design must be informed not only by its direct context of the Brookview-Irvington Park Historic District but also of the adjoining neighborhoods, commercial districts, schools and parks all along the State Boulevard corridor, as well as its important gateway to downtown role and potential, rather than the isolated and artificial box of the APE that has been drawn around the immediate project area.

I also urge the City to correct area flooding and flash flooding directly by addressing its root causes, along with immediate purchase of any flood-prone homes (on Eastbrook Drive south of State/State Blvd) using flood-related funds, rather than accomplishing any desired flood buy-outs of homes via this transportation project, which has confused the "issues" within our community.

Thanks for the opportunity to comment and participate in this important process.

Sincerely,

Michelle Briggs Wedaman

2326 Eastbrook Drive and 2418 Eastbrook Drive, Fort Wayne, IN 46805

mbwedaman@frontier.com

260-710-4413

From: [Arbor Building](#)
To: [Hope, Briana](#)
Subject: comment on City of Fort Wayne, IN State Boulevard Reconstruction Project Des. No. 0400587
Date: Friday, July 18, 2014 8:51:45 PM

July 18, 2014

RE: Comments on City of Fort Wayne, IN State Boulevard Reconstruction Project Des. No. 0400587

To whom it may concern:

I own properties and live in the Brookview Neighborhood at 2326 Eastbrook Drive and 2418 Eastbrook Drive in the block north of the planned State Blvd project area and I am writing to express my opposition to the City's preferred alternative (3A) for State Blvd reconstruction. This plan will be damaging to and will devalue my property, and will make this neighborhood much less desirable as a residential area.

I am in strong support of the Storrow Kinsella Associates /Transportation Solutions LLC-prepared conceptual solution presented by ARCH/Indiana Landmarks and others in June 2014. Maintaining State Blvd as two lanes, curving it south and allowing dedicated neighborhood street exits/entrances to "new State" at Westbrook Drive and "Old State Blvd at Clinton St/US 27" via roundabouts will allow neighborhood vehicle traffic to safely enter and travel both east and west on State Blvd from both north and south of State Blvd, raise the Spy Run bridge, allow smooth traffic flow on State Blvd away from neighborhood driveways and preserve the neighborhood's character, additional green space, and family-friendly residential property value.

I further request that the City correct area flooding and flash flooding directly by addressing its root causes, namely the continued massive developments upstream, which has put the neighborhood (as well as the city) in the situation it is in. I also urge the immediate purchase of any flood-prone homes (on Eastbrook Drive south of State/State Blvd) using flood-related funds, rather than accomplishing any desired flood buy-outs of homes via this transportation project.

Sincerely,
Mike Wedaman
2418 Eastbrook Drive and 2326 Eastbrook Drive and, Fort Wayne, IN 46805
260-710-0064

From: [Carol Lawton](#)
To: [Hope, Briana](#)
Subject: Comment on State Blvd. project
Date: Friday, July 18, 2014 3:25:26 PM

Dear Ms. Hope,

I am writing to ask that further consideration be given to the alternative plan for the State Blvd. project in Fort Wayne that has been proposed by Storrow Kinsella Associates. As a resident of the Irvington Park neighborhood, I often travel the current Eastbrook-State intersection by car and also by bicycle. The alternative plan seems to have traffic-calming characteristics more in keeping with the surrounding neighborhoods and the potential to be more accommodating of pedestrians and bicyclists. For these reasons, I hope the alternative plan can be given serious consideration.

Sincerely,
Carol Lawton
3314 Irvington Drive
Fort Wayne, IN 46805

From: [Catherine Hill](#)
To: [Hope, Briana](#)
Subject: Re: EA for City of FW State Blvd. Reconstruction Project
Date: Friday, July 18, 2014 11:50:55 AM

To whom it may concern:

My family of four lives at 734 E. State across from Northside Park. Although the proposed elevation, widening and straightening project between Spy Run and Cass Street is not in my neighborhood, I am writing to express my concern as it affects my family, my home and my adjacent neighborhood (Northside)as well. I have friends in the Brookside neighborhood, my family and I travel that corridor frequently (and have never experienced traffic flow issues), by car as well as on foot and by bicycle.

While I applaud the adjustments to the plan that have already been made to address foot and bicycle traffic safety, I remain gravely concerned about the current reconstruction plan. It will speed up and attract more traffic- making State Blvd. yet another boring, ugly “highway” in the middle of our City. The logical conclusion is that property values will plummet, responsible homeowners willing to invest in a valuable neighborhood near downtown like me will leave. How sad! This is in complete opposition to all the exciting news and plans I am hearing about Downtown Improvement and the many opportunities for growth and development in neighborhoods adjacent to downtown.

I am a big fan of our historic neighborhoods! My own home was just added to the National Register of Historic Homes. The primary reason I choose to live on the near north side is because of the character of the homes and neighborhoods. **We have a HUGE opportunity with the Storrow Kinsella Associates/Transportation Solutions LLC proposed solution that is already supported by ARCH, Indiana Landmarks, and many neighbors in the Brookside and Northside Neighborhood Associations.** This option addresses all of the issues (flooding, traffic congestion, and bridge improvements) with the added benefit of becoming a major improvement to the aesthetics of the area. I am confident my neighbors and I, along with the whole city would be proud of a road reconstruction project that takes advantage of best practices in planning and traffic engineering. It seem to me that if we’re going to spend the money anyway, let’s PLEASE spend it right.

Thank you for your time and consideration,
Catherine Hill



Catherine Hill

Director, Vera Bradley Foundation & Corporate Philanthropy
12420 Stonebridge Road
Roanoke, Indiana 46783
260-207-5186 **O**
260-417-2833 **M**
260-484-2278 **F**
chill@verabradley.com
verabradley.com
verabradley.org

Last Minute Brookview Resident Survey

On Wednesday evening, July 16, 2014, at 6:30 pm, the Brookview Neighborhood Association (BNA) held a meeting to discuss both the ARCH and City plan for the State Boulevard Project. After much discussion, we decided that each member of BNA would file separately with American Structurepoint because with so many different views among our members, no single common view was agreeable to all.

Prior to the meeting, the brochure delivered door to door announcing the meeting, requested all BNA members to take the Brookview Resident Survey online at www.surveymonkey.com/s/BQB66XZ by [7/16/2014](http://www.surveymonkey.com/s/BQB66XZ).

Early during the meeting, when minutes were asked to be read for the last BNA meeting, we were told no minutes were available and the last BNA meeting took place more than a year ago. The question was asked, when was the Brookview Resident Survey approved by the BNA? We were told 2 weeks ago. Again no minutes were made available for approving. The BNA meeting of 2 weeks ago had not been announced to all, if any BNA residents. Even more interesting is the survey itself. The BNA president selected the online "basic" free application software online for the survey from Survey Monkey. This survey selection out of 4 choices has a limit of 100 responses per survey.

Since the announcement brochure of the 07-16-2014 BNA meeting asked all BNA residents to fill out the survey by the 07-16-2014 meeting, but failed to do the following:

- 1) Did not disclose that the survey was limited to 100 responses
- 2) Did not have approval of the BNA at an open announced BNA meeting
- 3) No minutes exist of the BNA meeting of 2 weeks ago for member approval at 07-16-2014 meeting

I request that any reporting of the Brookview Resident Survey, for reasons mentioned above, to not be considered a BNA approved or accurate survey of BNA members that reflects the views of most of our 275 household and business members, numbering approximately 800 to 1000 residents.

Karl Dietsch

2313 Oakridge Road

Fort Wayne Indiana 46805

From: [Karen](#)
To: [Hope, Briana](#)
Subject: State Blvd. Reconsrtuction Project
Date: Thursday, July 17, 2014 10:04:04 PM

Dear Ms. Hope,

I am a resident of a neighborhood east of the portion of State Blvd. that contains the Brookview/Irvington Park neighborhoods. My neighborhood, like the Brookview/Irvington Park neighborhoods, is a National Register neighborhood, and like many residents who have chosen to live in the city, I value the attributes of this boulevard neighborhood.

There are currently 2 proposals with regard to the reconstruction of State Blvd. I recently attended the public meeting hosted by ARCH and after hearing their alternative proposal, I believe it most closely fits the criteria for choosing which proposal to implement. It is my understanding that the project that is most prudent and feasible should be the project chosen. The City's proposal would build a 4 lane, with a 5th lane as a median, thoroughfare through the neighborhood, demolishing 14-15 homes in the process. It would clearly change the character of this historic neighborhood, would increase the speed of traffic and adversely affect neighborhoods to the east and west. There is also concern about being able to execute a left hand turn out of either side of State Blvd. due to the existence of the median. The alternative proposal creates a by-pass around the neighborhood, does not require the demolition of any homes and only requires the demolition of 1 commercial property. This proposal should have no adverse affect on adjoining neighborhoods and leaves the current street virtually intact. This plan clearly does the least harm to this historic neighborhood! That makes this alternative plan the most prudent and feasible. I urge you to adopt it or to at least incorporate the majority of its components.

Sincerely,

Karen E. Richards
1913 Forest Park Blvd.
Fort Wayne, IN 46805

From: [Eileen Lee](#)
To: [Hope, Briana](#)
Subject: project INDOT DES#0400587 State Boulevard Reconstruction Project
Date: Thursday, July 17, 2014 2:19:18 PM

Please consider my comments regarding the State Blvd. Reconstruction Project referenced above.

After many years and meetings with city officials, I remain displeased with the State Boulevard Reconstruction project that has been presented to our community. To raise the bridge, straighten the road, expand the number of lanes to 4, plus a 5th turn lane will create a high speed road in the middle of a wonderful, historic community and thereby ultimately create a wasteland where a wonderland once stood.

Also this project neglects to address the situation of creek flooding in the area. It merely raises the bridge high enough so that it would not be a problem in moving traffic through. This road is similar in size to Coliseum Boulevard which circles the north and east sides of Fort Wayne, and is inappropriate for running through a city and community.

Recently our community was presented with a consulting proposed alternative plan (CPPA) to the plan INDOT DES#0400587. This plan was created by Storrow Kinsella Associates - Transportation Solutions, LLC at the request of ARCH, Inc. I am much in favor of this CPPA plan as a better solution to replacing the bridge, creating a length of road between Westbrook Drive and Clinton Street free of interchanges, and holding to the historic character of the Brookview community.

The CPAA plan allows for open spaces to remain and to be expanded upon that may also serve to hold water during times of creek overflow.

The CPAA plan deserves further consideration, study, and development. There are many positives to the use of traffic circles to facilitate traffic flow at city-like moderate speeds, pedestrian and bicycle traffic are considered, and there will be less roadway water runoff during times of heavy rains.

Thank you for taking my comments into consideration.

Eileen R. Lee
2402 Eastbrook Drive
Fort Wayne, IN 46805

From: [Paul Gibson](#)
To: [Hope, Briana](#)
Subject: State Blvd. alternate concept, I prefer it.
Date: Wednesday, July 16, 2014 7:09:30 AM

To whom it may concern,

I am commenting on a few of the benefits proposed by this alternate concept. I prefer the 2 lane State Blvd. proposal opposed to the City of Fort Wayne's 5 lane proposal. Two or three lanes of traffic should handle the flow of traffic sufficiently. Five lanes of traffic proposed by the City of Fort Wayne appears unnecessary. State Blvd is only 2 lanes West of Wells Street. The City has said that there are no plans of expanding lane size or number East or West of this project. I like the proposed Southern curve to State Blvd. This will take traffic away from the neighborhood. There are less houses, properties for the city to purchase and demolish for road construction. The curve preserves the historical design philosophy and intentions of the original designer.

Please incorporate / consider some these benefits for the State Blvd road construction project.

Keep Fort Wayne a beautiful place to live.

Respectively,

Paul Gibson

Vice President

Irvington Park Association

From: [Colin Graham](#)
To: [Hope, Briana](#)
Subject: State Boulevard Project Fort Wayne
Date: Tuesday, July 08, 2014 8:11:24 PM

Dear Sir or Madam,

I live with my family at 2401 Oakridge Road, part of the Brookview/Irvington Park Historic District, and wish to voice my strong opposition to the proposed State Boulevard Alternate Concept. We will be directly affected by this project and I have seen the City's plan and feel it is much better. The congestion and confusion of the roundabouts will cause issues with traffic flow and convenience for my family and general population. As a former archaeologist and historian with IPFW, I fully understand the 106 process and the importance of historic preservation, however I strongly disagree with the alternate plan and do not feel it adds any benefit to our historic district and general area. I hope that all parties will accept the original City proposal and move forward quickly with this project.

Regards,

Colin Graham
Jennafer Graham



Comment Sheet

Please provide your comments, concerns, and/or suggestions regarding the proposed State Boulevard Reconstruction Project, State Boulevard from Spy Run Avenue to Cass Street. Your comments are important to us, and we sincerely appreciate your time and participation during the public involvement process. Please submit comments by **Friday, July 18, 2014**. Comments may be mailed, faxed, or submitted via email to the address/fax number below.

American Structurepoint, Inc.
Briana Hope
7260 Shadeland Station
Indianapolis, Indiana 46256-3957
Fax: (317) 543-0270
Email: bhope@structurepoint.com

Meeting Date: **Wednesday, June 18, 2014**
Project: **State Boulevard Reconstruction Project**

Name: (Please print) Jon Slate TNK SPOT PRINTING

Address: 215 West State Blvd. Fort Wayne, IN 46808

COMMENTS: My first concern is there would be equal access to the businesses

as there is currently during construction and that it would be in writing.
There should be some form of compensation during construction for any
loss of business.

My second concern is that the proposed end result that shows a picture
viewing west on state toward my property not only blocks my current sign
and approx. half of my 35 foot canopy with trees in the small grass medium
of which the trees are completely not necessary.

My third concern would be the ease of turning left as cars are coming from
the east into my parking lot. With this in mind I feel that this is a very
dangerous situation for cars waiting to turn left into my parking lot but
do not want a median put in the middle.

I feel that making this 4 lanes that kids going home from North Side H.S.
that cross nearly anywhere is creating a very dangerous situation. I see
students crossing the street everywhere and making the street twice as wide
will cause problems of people getting hit!

The spy run creek bridge needs to be redone but that should be it!

SIGNATURE: Jon Slate

From: [Ben Moore](#)
To: [Hope, Briana](#)
Subject: State Blvd. Reconstruction Project, Ft. Wayne
Date: Friday, July 04, 2014 9:48:02 AM

Ms. Hope,

Regarding the State Blvd. Reconstruction Project between Spy Run Ave. and Cass St. in Ft. Wayne:

I fully support the project as outlined in the June 18 presentation at North Side High School. I have lived on Oakridge Road for 16 years. I am proud and lucky to reside in this neighborhood. Initially, my partner and I were renters here, and we moved into our house simply because we could afford the rent and the location was convenient for our jobs. After two years, when our former landlord made the decision to sell the house, we were thrilled to buy it, having fallen in love with the neighborhood. We fully intend to remain here into our retirements.

I believe this reconstruction plan will be a great improvement to traffic that runs through our neighborhood, and do not agree with those who think it will negatively affect property values and the beautiful character of the neighborhood. On the contrary, the planners who have developed this project have taken great pains to simultaneously improve traffic flow while maintaining a park-like feel. For property owners on the south side of East State Blvd. and also on Eastbrook Drive south of State Blvd., they can finally have relief. They have suffered greatly with flooding, and now find themselves stuck with properties that really aren't habitable but that nobody will ever buy. Historic groups' attempts to save those structures, while laudable, doesn't make sense. None of those houses is historically significant or unique, and it's time to let them go, allowing the owners to get out from under the financial and emotional distress of being saddled with a house that they won't be able to sell or use.

The plan proposed by ARCH would be a disaster. A round-about at State Blvd. and Clinton St., which is one of the busiest intersections on the north side, is ludicrous! I believe ARCH's plan will create more traffic back-ups in our neighborhood than what we currently experience.

Please include my comments as part of the public comments related to the hearing process for this project.

Thanks,

Benedict Moore
2335 Oakridge Rd.
Ft. Wayne, IN 46805

From: [Karl](#)
To: [Hope, Briana](#)
Subject: Just another day in the Traffic Calming Curves of State Boulevard
Date: Thursday, July 03, 2014 11:14:51 AM
Attachments: [P1010003.JPG](#)

Briana Hope,

Yesterday (07-02-2014), I witnessed a wreck at State Boulevard and Westbrook. This type of wreck is common due to the lack of visibility to oncoming traffic while in the curve. The majority of the Brookview Neighborhood residents want the new route to be straightened for increased safety at intersections getting on and off. The City's Plan does this by straightening the new route and going 4 lanes instead of 2.

Regards,

Karl Dietsch
2313 Oakridge Road
Fort Wayne IN 46805

(260) 484-1399
kdietsch@comcast.net



From: [Sara Kruger](#)
To: [Hope, Briana](#)
Subject: alternate plan
Date: Wednesday, July 02, 2014 11:43:29 AM

To Whom It May Concern,

After the recent unveiling of the alternate plan for the State Blvd./ Brookview project, there was a very very positive attitude - even joy! - when the SKA representative shared the view, the ideas, the problem solving and overall effect that this alternate plan would have. It is amazing! And anyone can see that this is a better approach. It is a truly thoughtful and beautiful design; properly scaled and one that considers and incorporates safety, flooding, preservation, the environment, history, people, activity, traffic, and the future. Any sensible person can see that it is better in every way, and was highly embraced by the public when unveiled.

Fort Wayne's plan has always been, and continues to be, out of scale, inappropriate, costly, ugly, and a devastation on many many levels. You, as engineers, certainly you are able to recognize that the alternate plan is a refreshing and forward thinking design, and viable in every way.

We understand very clearly that the Federal arm of this project can put a halt on FW's current plan. As a tax payer and area resident, I am requesting - even demanding - that you do just that.

Please, halt the process, halt the money, be professional enough to recognize a better way; and put time, energy, and money into doing it right.....right for those of us who live here, work here, play here, and of course, pay lots of taxes that fund these projects. We want our money spent well; we want our neighborhoods and the people in them respected, and we want the city overall to thrive. Which will only happen when the current plan is stopped, and the alternate plan is put in place.

Most Sincerely,

Sara Kruger Geyman

State Boulevard, Alternate Study Unveiled on 06-30-2014

I attended the State Boulevard **Alternate Study** open showing on 06-30-2014. This study claims to be a result of input from the neighborhood to the project but this is not true. The neighborhood majority want the **City's Plan** as explained and shown for the past 6+ years. The **Alternate Study** has no input from us. It has been done in secret from us. No neighborhood meetings have been held in the past 18 months. Its first exposure to us was the evening of 06-30-2014. This is after the final open hearings on the **City's Plan** held on 06-18-2014. The **Alternate Study** is dated 12-10-2013, more than 6 months before its showing and yet no exposure was made to the neighborhood residents.

A roundabout in the **Alternate Study** at Clinton and State decreases safety slows traffic to stop and go, yields to the left on entry and forces 3 one way Clinton traffic lanes to one lane exit capacity. This roundabout would not handle current and future capacity needs. The longer curvy 2 lane route in the **Alternate Study** decreases safety, capacity and does environmental damage to existing park land. The **City's Plan** removes flood damaged homes in its straighter path staying out of existing park land.

The development of this **Alternate Study** is the work and influence of Councilman John Shoaff involving Arch of Fort Wayne (of which he is a past president of the board of directors) and Indiana Landmarks (of which his wife is a director). The **Alternate Study** is only the latest delaying attempt on the State Boulevard project by a few.

The findings of the **Alternate Study** state many false conclusions. The most obvious is the statement "The CPPA has the support of the affected neighborhoods". The Northwest Quadrant of Fort Wayne Neighborhood Leaders support the **City's Plan** 36 to 6, a ratio of 6:1. The first 9 homes on Oakridge Road and remaining 4 homes on the north side of State after the project is completed all support the **City's Plan**. The 14 homes to be removed (all flood damaged after having basements filled with 1 to 8 feet of water just last year alone) all want to sell out to the City. Residents on Oakridge Road are in favor of the City's plan by a 3:1 ratio. Residents on Terrace Road are 2:1 in favor.

The four goals accomplished in the **City's Plan** are:

- 1) Increase safety with a shorter straighter route and an increase of lanes from 2 to 4
- 2) Increase traffic capacity for current and future needs by increasing lanes from 2 to 4.
- 3) Decrease environment impact with shorter straighter route after removing flood damaged homes.
- 4) Raise the roadbed to an elevation above most creek flash flooding and longer term river floods.

It's time to go with the Win-Win design of the City,

Karl Dietsch, a flood plain resident at 2313 Oakridge Road, Fort Wayne, Indiana

From: [Karen Schwichtenberg](#)
To: [Hope, Briana](#)
Subject: State Blvd Reconstruction
Date: Wednesday, July 02, 2014 10:06:10 AM

Dear Ms. Hope,

I am a resident of the Northside Neighborhood in Fort Wayne, Indiana. I also own a small business that operates on the northeast side of Fort Wayne. I have some very grave concerns about the City of Fort Wayne's project proposal to realign State Boulevard.

I do not want to see a five lane thoroughfare going through our historic neighborhoods of Brookside and Irvington. I believe that this type of avenue in a quiet, historic, park-like residential neighborhood will destroy it! I have great fears that straightening the curve in the road and adding 3 lanes of traffic will greatly increase the amount of traffic and the speed that it travels. This will impact very negatively the adjoining neighborhood of East State Village, where many pedestrians and bicyclists frequent the stores there and the library. There are also several schools and parks in the neighborhood that have a lot of foot traffic. The speed and intensity of the traffic in these areas will increase the danger and potential for accidents.

I attended a public meeting on June 30 where ARCH, Inc. presented an alternate proposal for improving State Boulevard. It involves less than half the amount of roadway, so it will not increase the issue of flooding in the area the way the City proposal does, with its greater roadway surface. The ARCH proposal includes two roundabouts, that will keep traffic moving, but at a much slower, safer speed. Their proposal does not impact the integrity of the historic neighborhood to nearly the degree the City one does. The ARCH design has the bicycle and pedestrian trails off the main roadway to keep this activity separate and safe.

I am in favor of the ARCH, Inc. proposal to revise State Boulevard.

Thank you for your consideration.

Karen Schwichtenberg
President
Purple Blaze Enterprise, LLC

From: [John Kurdziel](#)
To: [Hope, Briana](#)
Subject: State Boulevard Reconstruction Project - INDOT DES #0400587
Date: Tuesday, July 01, 2014 2:07:36 PM

Ms. Hope:

I have reviewed the proposed plan for the reconstruction of State Blvd. in Fort Wayne and find it lacking in some major engineering aspects. The City proposal raises the elevation of State Street over the Spy Run Creek and thereby introduces a major new design problem to this already troubled route, vertical alignment sight distance for the entrances for Westbrook Dr. and the revised Oakridge Rd. entrances. We own property off of Westbrook Dr. and it is already difficult and dangerous to enter onto State Blvd. due to problems with the horizontal alignment and the associated minimal sight distance it provides. Your proposal to raise the road, increase the number of lanes, straighten the roadway defacto increasing the associated speed and introducing an obviously new issue for vertical alignment sight distance that does not currently exist only makes this situation worse. As a minimum, traffic signals need to be included at the intersection of Oakridge Rd. and Westbrook Dr. to allow for the safe entrances to State Blvd. off of these side streets. Failure to acknowledge this obvious design flaw could open the City up to lawsuits in the future which no one wants or can afford.

On the other hand, a proposal was made yesterday, June 30, 2014, at a community meeting hosted by ARCH, Inc. (Storrow, Kinsella Associates & Transportation Solutions, LLC) which provided much more comprehensive approach to this problem. They propose two traffic circles, one at the intersection of Clinton St. and State Blvd. and the other at Westbrook Dr. and State Blvd. that not only corrected the alignment issues with State Blvd. but provided a much more efficient and safe means of moving traffic through this area. This proposal is also much more beneficial to the community as it maintains the integrity of the historic neighborhood, and one could argue, actually enhances it and the ascetics of Fort Wayne as a City.

We all want to increase the desirability of living in the City and be proud of what the City has to offer. The Headwater's Park was an example of what can be done when an engineering need is combined with improvement in the quality of life. We have an opportunity to do the same with this project, let's not waste it.

Thank you for your consideration of my concerns.

Sincerely,

John M Kurdziel

John M. Kurdziel
3450 River Forest Drive
Fort Wayne, IN 46805
260-409-5218

From: [Crites, Scott](#)
To: [Zielinski, Rich](#); [Hope, Briana](#)
Subject: FW: Hearing
Date: Wednesday, June 18, 2014 3:51:29 PM

Scott M. Crites, PE

Project Manager, Road Group

116 E. Berry Street, Suite 1515, Fort Wayne, IN 46802

T 260.373.0600 E scrites@structurepoint.com

F 260.373.0608 W www.structurepoint.com

C 260.402.6271



Follow us on  

From: Shan Gunawardena [<mailto:Shan.Gunawardena@cityoffortwayne.org>]
Sent: Wednesday, June 18, 2014 3:50 PM
To: Dan Avery; Bob Kennedy; Frank Suarez; Crites, Scott
Subject: FW: Hearing

FYI

Shan R. Gunawardena, P.E., PTOE
City Engineer – Fort Wayne
Citizens Square, 200 East Berry Street, Suite 210
Fort Wayne, IN 46802
(260) 427-6169

From: Sara Kruger [<mailto:sarakruger7@gmail.com>]
Sent: Wednesday, June 18, 2014 3:39 PM
To: Tom Smith; Tom Didier; Russ Jehl; Mitch Harper; Martin Bender; John Crawford Councilman; John Shoaff; Glynn Hines; Geoff Paddock; Shan Gunawardena
Subject: Hearing

To all concerned,

I have a family matter that has called me out of town today. I unfortunately am unable to attend tonight's meeting,

With regard to tonight's hearing, I would like you all to know that as a sensible taxpayer, I am in very strong support of the alternate plan for State/Brookview; as it is better in every way. It is more sensible and viable financially, humanly, historically, visually, and environmentally.

My vote is cast for the sensible, viable, alternate plan.

--

Cheers,

*Sara Kruger Geyman - Change Agent
SaraBella Home Staging and Redesign
"Unleash The Potential!"*

c: 260-615-0983

email: sara@sarabellahomestaging.com

web: www.sarabellahomestaging.com

Visit me on Facebook: <http://www.facebook.com/SaraBellaHomeStaging>

Let The Beauty You Love Be The Thing That You Do. - Rumi



From: [Suzanne](#)
To: [Hope, Briana](#)
Cc: [Michael Galbraith](#); joyce.newland@dot.gov; [Patrick A Carpenter](#); CSlider@dnr.IN.gov; [Todd Zeiger](#)
Subject: Consulting Party comments re Fort Wayne State Blvd EA
Date: Friday, July 18, 2014 1:36:11 PM
Attachments: [SSlick StateBlvd EA comments.pdf](#)
[ATT00001_.htm](#)

All,

The City of Fort Wayne is planning to place a multi-lane arterial through a small residential neighborhood in historic Brookview-Irvington Park. A representative from the MPO, NIRCC, told the public (Fort Wayne Northwest Area Partnership meeting, 18 October, 2012) that this project was being done to, “improve the neighborhood”. Now that we all have read the long delayed, and carefully “tweaked” (<http://www.news-sentinel.com/apps/pbcs.dll/article?AID=/20140415/NEWS/140419801/0/SEARCH>) EA, we know that the City’s preferred design will adversely impact the Brookview-Irvington Park Historic District, the Park & Boulevard System, and, of course, the aged bridge. The adverse impact is in the bifurcation of the neighborhood, the removal of the Kessler and Shurcliff design elements, the wide and fast traffic arterial through the residential district, and in the “relocation” of 15+ homes.

The City of Fort Wayne has been rigidly unyielding in the scale of this project and has dismissed pleas from citizens to reduce the massive, intrusive footprint of the planned roadway and elevated bridge. There has not been adequate explanation for the inability to design a road more suitable to the neighborhood -- even though, the project is being done to “improve the neighborhood”. Many citizens feel there is more to the story, that this is a piece of the greater regional transportation plan to increase capacity, and that this project will not be the end of the widening, straightening and enlarging of State Blvd as a main east-west arterial. If it were, indeed, to “improve the neighborhood”, would it not have been scaled down as citizens have requested? And would not the process have been less difficult and contentious?

Mitigation efforts discussed to date will do little to improve the quality of life issues and reduced property values resulting from this massive arterial bifurcating our intimate, historic residential district. The minimal efforts described will do nothing to preserve the segment of the historic Kessler parkway when it is transformed into what Kurt Culbertson, Kessler biographer, referred to as a “traffic sewer” during an outstanding recent presentation in Fort Wayne on George Kessler’s life and work. He expressed great concern that Fort Wayne will do what other cities now regret doing in obliterating their own Kessler legacies by transforming them into massive, bland arterials with the sole purpose of moving goods and people quickly through their environment. Many residents of this district and surrounding districts feel that our Kessler assets and Brookview are being “thrown under the bus” for this “traffic sewer”, and Brookview will not be the last neighborhood sacrificed.

Yet, there is another alternative not included in the City’s proposal -- a prudent and feasible alternative -- that will have a much diminished impact on these threatened historic elements. Fort Wayne ARCH, Indiana Landmarks and the Friends of the Parks of Allen County commissioned [SKA, Storrow Kinsella Associates of Indianapolis](#) (<http://www.storrowkinsella.com>), to design an [alternative concept for State Blvd](#).

The SKA concept embraces and employs many of the ideals espoused in the NIRCC 2035 Plan (<http://www.nircc.com/user/image/2035planfinal.pdf>) while respecting the historic districts and their assets. It minimizes disruption to the residential neighborhood by moving the roadway south of the residential district, in doing so, the homes slated for “relocation” are spared, and the Kessler-designed parkway is improved. The land selected by SKA for the arterial portion of the project utilizes the portion of the historic district from which homes were already “relocated” by the flood removal program that preceded the transportation project.

It incorporates progressive traffic design elements -- roundabouts -- that expedite regulated traffic flow thereby improving air quality by reducing idle emissions, ozone and particulate matter. By employing roundabouts [it also reduces more serious collisions that occur at signalized intersections](#). Roundabouts are now being [celebrated by Fort Wayne City Engineers and Public Works](#) -- one is under construction currently at Ewing/Superior in Fort Wayne!

The Storrow-Kinsella concept:

- Is calculated to provide the required and projected LOS (level of service) for traffic movement,
- Was designed with sensitivity to safety concerns and adverse impacts to residential areas,
- Mitigates air quality problems by reducing vehicle emissions through intersection improvements and constructing roundabouts at appropriate locations,
- Serves to protect our urban area and residential neighborhoods from the adverse impacts associated with truck traffic,
- Reduces vehicle hours of delay,
- Supports energy conservation, protection of the environment and quality of life,
- Scales-down widening projects, as it would add a third lane for left-turning traffic instead of widening to add through-lanes,
- Implements appropriate “complete street” concepts to provide safe bicycle and pedestrian facilities,
- Reduces costs when reductions in funding sources is a reality -- fewer lanes, less concrete, less complex construction,
- Provides a design that will be less costly to maintain,
- Contributes fewer hard surfaces than the City-preferred plan thus offering a superior flood management approach,
- Achieves an efficient and safe transportation system for the movement of people and goods while simultaneously improving the economic and environmental conditions of Brookview-Irvington Park.

The Storrow Kinsella alternative for State Blvd. embraces and supports the core concepts described in the NIRCC “roadmap” for our transportation future. It is superior to the adversely impacting City-preferred design. It would, unlike the City’s approach, actually improve the neighborhood’s cohesion by:

- Providing a context sensitive solution -- the arterial portion of the roadway removed from the historic residential district,
- Preserving the historical assets -- and improving the Kessler-designed portion of State Blvd through Brookview,
- Providing safer, more comfortable travel for pedestrians and bicyclists -- again, removing these elements from the arterial portion of roadway,
- While also calming the traffic in the arterial, itself.

Property values along the historic State Blvd, should, in fact, increase, as the neglected blocks are improved and restored to a quiet, residential street. It is prudent and feasible and must be considered. It is a progressive design that Fort Wayne will point to with pride. It will, however, require real compromise to achieve.

Sincerely,

Suzanne Slick
Irvington Park Neighborhood Consulting Party
3318 Garland Avenue
Fort Wayne, Indiana

From: [Peggy Brady](#)
To: [Hope, Briana](#)
Subject: State Boulevard Reconstruction Project Comments
Date: Wednesday, July 16, 2014 9:48:10 AM

To: Briana Hope @American Structurepoint, Inc, 7260 Shadeland Station, Indianapolis, IN 46256-3957

Fax: 317-543-0270

Email: bhope@structurepoint.com

Re: Comments for State Boulevard Reconstruction Project, Fort Wayne

From: Peggy Brady, 2314 Eastbrook Drive, Fort Wayne, 46805, (260) 483-5880, email: pbbrady@frontier.com

We have lived 3 houses from State Street for 38 years. We have been through the floods of 1978 and 1982. The backup tsunami of June 2012 (when the rivers were low) is the first time we had flood damage in our basement. We have seen the many accidents that occur because of the State Street curve as well as the back flooding when flood water is unable to flow under the State Street bridge due to debris that gets stuck. I have seen the unacceptable conditions of the homes south of State Street that had been promised a buyout.

I have not gotten involved in the Brookview Association because I am already involved in more than I can handle. However, I did attend 1 meeting about the Westbrook rain garden (which looks like a scary weed garden by the way) and 3 meetings over this State Street project. I am ashamed that my association has stalled the city's project for so many years. I am angry that a few plus others who do not even live in the neighborhood have been able to accomplish these stall tactics. I do not agree with any of their objectives. I will be attending my first Brookview meeting tonight. I completed the survey Michelle Wedeman mentioned in her meeting's flyer. Not sure who designed the survey questions; however, some were quite biased toward Michelle's views. If only this much attention, time, & money would have been spent on fixing the problem with flooding. Tonight should be interesting.

I believe the City's plan for the new State Street is great! I wish it could be done immediately and would not take yet another 4 years. I believe the City's plan will increase my property value. State Street needs

to be straight. We already have traffic and four lanes at all intersections nearby. I do not believe semi truck drivers will choose State Street for their travel just because there are 2 more lanes between Cass and Clinton. I do not believe State Street will become Coliseum Blvd. Having a park-like area south of State may help make more space for flood waters. The City's plan will include the buyout of the 14 homes which have been damaged beyond fixing. These 14 homes have been the victims in this project and I feel especially sorry for what they have had to live through.

When attending the City's meeting at Franke Park where we got to see pictures, I was astonished at the number of people complaining about the City's plan. Every time I asked which house was theirs, every single one of them said, "Oh, I live in Lakeside" or "I live in East State Village" or "I live on Forest Park" or "Oh, I don't live in Brookview". I would ask them, "Why are you here?". They would say, "I don't want State Street to become a Coliseum Blvd." or "More semi trucks will start going down State Street" or "It will let people go faster with 4 lanes". One actually asked me, "Don't you believe in the integrity of the neighborhood?". My response was "Are you kidding me? You don't even live here". I am still very angry that all of these people who are not affected and do not live anywhere close are causing the City all of these headaches. I am angry that this has been stalled and the stall tactics continue. Hopefully tonight's meeting will end this insanity.

As for the Alternative Plan, it is ABSURD!

- 1) NO ACCESS to and from the WEST for Eastbrook, Oakridge, Terrace without going way around and out of the way thru 2 incredibly insane roundabouts just to go to the grocery or a drugstore 1 block away!
- 2) Roundabouts on a state highway would be incredibly unsafe.
- 3) Compressing 3 lanes on a state highway down to 1 or 2 to continue south thru a roundabout is ridiculous.
- 4) Keeping a 2 lane road does not solve the problems of traffic congestion and yet another curve for more accidents.
- 5) Placing a 2 lane curved road thru the park-like area south of state defeats the purpose of more park space (especially if it helps with flooding).
- 6) Leaving the 14 homes as is - Oh my! What a shame! These poor folk don't want to stay; their homes are no longer worth fixing. Neighbors don't like seeing run-down homes.
- 7) Walkers to high school going across a roundabout would cause a few more accidents, even if there is a stoplight.

- 8) Stoplight in a roundabout for 2 roads even sounds more confusing.
- 9) A roundabout with 5 entry points at a busy intersection is more than ridiculous.
- 10) Having to buyout all 4 businesses at Clinton and State has to cost a LOT MORE; neighbors probably don't want to see the BP/convenience store go; we would want BP to stay.

So many more objections to this Alternative Plan. I'm angry you have to spend all this time going through these "required" motions to stop the Association from any further blocking of the City's Plan.

A few things I am curious about:

- 1) How many people that attended the meeting at North Side HS actually lived in Brookview.
- 2) How many Brookview residents actually support Michelle's actions.
- 3) How many Brookview residents support the City's Plan.

Just an FYI: When my new neighbors moved in, Michelle was knocking at their door while they were still unpacking. They did not know anything about this project. But they signed Michelle's petition because they felt like she wasn't going to leave until they signed it. Most every neighbor that I know that lives close to State Street supports the City's Plan.

Submitted by,
Peggy Brady

From: [DALE BENDER](#)
To: [Hope, Briana](#)
Cc: [martin.bender@cityoffortwayne.org](#); [jncrawfordmd@gmail.com](#); [jshoaff@proparkwest.com](#); [tsmithdistrict1@gmail.com](#); [russ@russjehl.com](#); [tdidiers5@frontier.com](#); [MitchHarperCouncil@gmail.com](#); [geoffrey paddock@aol.com](#); [glynnhines@aol.com](#)
Subject: ARCH alternative plan for State Boulevard project
Date: Friday, July 18, 2014 1:38:45 PM

Ms. Hope , and , esteemed gentlemen of Fort Wayne City Council

I am writing in response to the June 30th public hearing , at Psi Ote Upper pavilion at Northside Park . At this meeting Michael Galbraith , the executive director of the city's chief historic preservation organization , revealed that ARCH had hired American Structurepoint (an engineering firm) to research an alternative to the City's proposed reconstruction of State Boulevard between Cass Street , and , Clinton Street. The plan was developed by a traffic engineer , and , a landscape specialist .

After having been in attendance at aforementioned hearing , heard the plan proposal , and , studied. the sketch plan of the proposal , I can say that I very much favor this alternative plan over the City's proposed plan through our historic neighborhood .

The reasons for which I favor ARCH'S Alternative Plan :

1. it's inherent beauty , and , simplicity of design .
2. ease of traffic flow.
3. no residential intersections from neighborhood (Eastbrook Dr. , Oakridge , and , TerraceRd.) to the proposed section from Westbrook Dr. to Clinton Street is a good thing . The City's plan would have residents turning west onto State Boulevard across 5 lanes of traffic, from a bottleneck at Oakridge .
4. the 2 lane proposal will have less runoff to Spy run Creek during rain events .
5. Even during electrical black-outs , roundabouts work well .
6. Land under demolished homes , bought out in the worst flood prone areas of Eastbrook Drive , can be utilized as flood control overflow . And , the sooner the better .
7. Historic State Boulevard can still be used by residents for easy access to the new roadway ; and, with enhanced bicycling , walking , running activities .

Amendments to the ARCH Alternative Plan :

1. There are two optional bypass lanes , near the State/Clinton roundabout , shown as #5 on the Sketch Plan . One is north of the new two lane , and , the other is south of the new two lane . I propose the south optional bypass , only , be incorporated into the plan . This would speed flow of easterly , and southerly headed traffic with less backup at the roundabout of vehicles traveling from the west . Eliminating the

optional bypass lane on the north of the new two lane would discourage speeders from traveling through the Brookview Neighborhood (usually along Eastbrook Drive) to travel west on State Boulevard .

2. for For the Clinton Street/State Boulevard Roundabout , I highly recommend the best “ smart “ roundabout signal available for enhancing the flow of traffic in the roundabout , and , for southbound traffic safety on Clinton Street .
3. Design into the plan a way for First Responders to easily pass through traffic waiting their turn through the Clinton Street/State Boulevard roundabout.
4. Raise the new multi-use bike/ped path bridge , over Spy Run Creek , to accommodate flood events ... a helical ramp on the east side of the creek , perhaps .

Thank you for allowing me to register my humble , though much considered , opinions .

Dale A. Bender
2402 Eastbrook Dr.
Fort Wayne , IN 46805
Sent from Windows Mail

Last Minute Brookview Resident Survey

On Wednesday evening, July 16, 2014, at 6:30 pm, the Brookview Neighborhood Association (BNA) held a meeting to discuss both the ARCH and City plan for the State Boulevard Project. After much discussion, we decided that each member of BNA would file separately with American Structurepoint because with so many different views among our members, no single common view was agreeable to all.

Prior to the meeting, the brochure delivered door to door announcing the meeting, requested all BNA members to take the Brookview Resident Survey online at www.surveymonkey.com/s/BQB66XZ by [7/16/2014](http://www.surveymonkey.com/s/BQB66XZ).

Early during the meeting, when minutes were asked to be read for the last BNA meeting, we were told no minutes were available and the last BNA meeting took place more than a year ago. The question was asked, when was the Brookview Resident Survey approved by the BNA? We were told 2 weeks ago. Again no minutes were made available for approving. The BNA meeting of 2 weeks ago had not been announced to all, if any BNA residents. Even more interesting is the survey itself. The BNA president selected the online "basic" free application software online for the survey from Survey Monkey. This survey selection out of 4 choices has a limit of 100 responses per survey.

Since the announcement brochure of the 07-16-2014 BNA meeting asked all BNA residents to fill out the survey by the 07-16-2014 meeting, but failed to do the following:

- 1) Did not disclose that the survey was limited to 100 responses
- 2) Did not have approval of the BNA at an open announced BNA meeting
- 3) No minutes exist of the BNA meeting of 2 weeks ago for member approval at 07-16-2014 meeting

I request that any reporting of the Brookview Resident Survey, for reasons mentioned above, to not be considered a BNA approved or accurate survey of BNA members that reflects the views of most of our 275 household and business members, numbering approximately 800 to 1000 residents.

Karl Dietsch

2313 Oakridge Road

Fort Wayne Indiana 46805

From: [mark handy](#)
To: [Hope, Briana](#)
Subject: State Blvd Reconstruction Project, Fort Wayne, IN
Date: Thursday, July 03, 2014 11:13:56 AM

As a resident of Oakridge Rd and the Brookview-Irvington Park Historic District, I approve of the City's State Blvd reconstruction plan. The City's plan reduces traffic snarls, ensures a safer, quieter neighborhood, and provides a reliable city route for the future.

I find it interesting that the greatest opposition to the City's plan comes from people who live outside the neighborhood, people who do not wait 5-10 minutes every morning to make a turn onto State, only to sit another 5-10 minutes in traffic waiting to move through the intersections, people who refuse to believe that State Blvd has evolved into an east-west traffic artery. They are also people who do not have standing water on their property every time State Blvd floods after more than 5 minutes of heavy rainfall.

At the June 18th meeting at Northside High School to discuss the City plan, Councilman John Shoaff's objections were:

- That the City plan is "a 20th Century dinosaur" dragged into modern times
- That the plan is "over-engineered"
- That the new widened street would drive down property values.
- That no alternatives were presented or outside opinions were sought during the design process

What the Councilman fails to see is that State Blvd has evolved into a major route through the city, and that currently State Blvd is a "dinosaur" carrying more traffic than it was intended to do and needs to be upgraded to deal with the new reality of increasing traffic growth. As an architect, Councilman Shoaff should understand the need to "over-engineer" in order to provide construction stability for the future. His argument about property values seems to defy logic: the City plan moves the roadway a further 190 feet from most residences, providing a safer, quieter neighborhood while preserving much of the original historic neighborhood design, making the existing structures more desirable and valuable. And, during the process, 4 alternative routes were discussed, 51 meetings were held regarding the City's proposal, and a quick perusal of the 894 pages of documentation show that since at least to 2009 Councilman Shoaff, Arch, Indiana Landmarks, and other organizations have been involved in the process, constantly employing delaying tactics to drag the process out as long as possible.

The plan presented by ARCH on June 30th fails to realize the reality of the changing traffic conditions in the city. By adding two roundabouts, the plan ensures continued traffic snarls during commute hours. By replacing the intersection at State Blvd and Clinton St, ARCH's plan effectively cuts three south-bound lanes of traffic into one, as well as the current east- and west-bound lanes, effectively blocking a major city artery. ARCH also seeks to preserve houses along State Blvd which have little to no historic value (all but one of the 13 homeowners signed a petition in favor of the proposed buyout of their properties), and routes the new roadway through a greater area of Vesey Park, eliminating yet more greenspace in the city. A very quick online search also reveals that State Blvd

has been removed from Indiana Landmark's 2014 10 Most Endangered list, and that none of the affected houses are significant structures as listed by Indiana Landmarks.

Brookview-Irvington is an important example of city planning theories from the City Beautiful movement of the 1910s. Walter Hoxie Hillary and Arthur Shurcliff, the designers of the neighborhood, were innovators employing the most current planning techniques of their era. The City plan keeps that tradition of innovation alive. Mr Hillary and Mr Shurcilff would approve of the City's plan.

Mark R Handy
2335 Oakridge Rd.

July 18, 2014

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Fort Wayne – Des. No. 0400587, DHPA No. 5903, Project No. IN20071404

Dear Ms. Hope,

We are submitting herein comments pertaining to the June 4, 2014 updated FHWA Findings and Determinations provided to the consulting parties as part of the Section 106 review and the May 2, 2014 Environmental Assessment for the proposed State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana (Des, No. 0400587, DHPA No. 5903, Project No. IN20071404).

Specifically:

- 1) We concur that a finding of Adverse Effect is in order as the project is currently designed. Under Section 106 and Section 4 (f) the project as designed will have an Adverse Impact to three historic properties – the Bridge carrying State Boulevard over Spy Run Creek, The Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
- 2) As identified in 36 CFR 800.1(a), we concur with the other consulting parties that the Findings and Determinations materials provided on June 5, 2014 adequately identify the historic properties, and assesses effects of the project as currently proposed.
- 3) As identified in 36 CFR 800.1(a) we do not find that the goal of avoiding, minimizing or mitigating adverse effects on historic properties has been met and is wholly inadequate. To address the inadequacy ARCH, Indiana Landmarks and Friends of the Parks along with the Brookview-Irvington Park Neighborhood Association commissioned Storrow Kinsella Associates and Transportation Solutions to examine the background research developed for the project to see if there was an alternative design for the project that better protected the historic assets outlined in #1 above while fulfilling the purposed and need for the project. The design engineers were also tasked to ensure their proposed alternative was prudent and feasible and avoided, minimized or mitigated the adverse effect to the neighborhood. The alternative plan (Alternative) that has been developed better addresses historic preservation concerns while accommodating the purpose and need of the Federal Undertaking. Here is a link to a permanent

DropBox Folder where you can pull up the alternative directly:

https://www.dropbox.com/sh/21xbxj8snangzji/AADLTtOKBej_U5fSLDnhrn9_a

We are aware that the plan was presented to American Structurepoint by ARCH at the June 18, 2014 Public Hearing for the Environmental Assessment. Further ARCH provided the alternative plan to American Structurepoint, FHWA, INDOT and the Indiana Division of Historic Preservation and Archaeology by email on June 24, 2014 and June 26, 2014.

- 4) We are submitting the Alternative as part of our comments under the Section 106 Review and EA comment process. The Alternative must be considered as governed by the guidance in 36CFR 800.1(c). This guidance directs that the Alternative must undergo unrestricted consideration by the agency and further states that the agency official complete the Section 106 process in a manner “that such actions do not restrict the subsequent consideration of alternatives to avoid, minimize or mitigate the undertaking’s adverse effects on historic properties.”
- 5) We are submitting this Alternative as it is in agreement with direction contained in 36 CFR 800.3(b) which directs the agency official to coordinate the Section 106 Review with “...the overall planning schedule for the undertaking and with any reviews required under other authorities...” including NEPA and Section 4 (f) of the Department of Transportation Act. We further believe that the guidance contained in 36 CFR 800.8(a)(1) encouraging agencies to “consider their section 106 responsibilities as early as possible in the NEPA process...” provides further impetus to consider the Alternative plan under both the 4(f) and NEPA process.
- 6) We believe that the Alternative meets the adopted purpose of the project by improving corridor connectivity for both motorists and pedestrians. It also addresses the problems of congestion substandard sight distance, and geometrics. Roadway flooding as well is addressed, although this remains a concern for both the currently proposed project and the Alternative. The nearly adjacent and recently completed US 27 Bridge Reconstruction project (Des. No. 0200914 and 0101527) experienced roadway flooding approximately a year after the project’s completion – demonstrating the need for a thorough investigation and remediation of a larger flooding problem in the area.
- 7) A key for our support of the Alternative is the significant reduction of impact to the identified historic resources noted in #1 above. The Alternative preserves the original curve of State Boulevard and preserves the connectivity of the adjacent residential streets. It does not introduce a foreign and oversized transportation facility adjacent to the historic districts. It preserves the historic homes within the path of the proposed project. The Alternative makes a “special effort to preserve...historic sites” and “minimizes, mitigates or avoids” Adverse Effects to the identified resources.

As outlined in the Adverse Effect findings on pages 206-209 of the Environmental Assessment, The Alternative proposal results in far less damage under both Sections 106 and 4(f), making it a prudent and feasible alternative that causes the least overall harm. In

the Fort Wayne Park and Boulevard System Historic district, and in the Brookview-Irvington Historic District, the Alternative avoids to a much greater degree the removal and disruption of the National Register Listed State Boulevard, minimizes the amount of new land disruption by using land previously altered through the removal of the “flood-buyout” houses, and retains the plan of Arthur Shurcliff-designated plat. In addition, the Alternative avoids the demolition of National Register listed residences entirely. The Alternative mitigates the disruption and bifurcation of the Shurcliff designed plat by the use of a curvilinear “new” State Boulevard replicating the scale of the “original” State Boulevard, providing the driver and pedestrian with a similar feel, design and setting.

- 8) We concur with the other consulting parties that the Draft Memorandum of Agreement (MOA) as presented in the May 2, 2014 Environmental Assessment and the June 5, 2014 Section 106 Review will need to be modified, particularly if the Alternative is adopted as the basis for proceeding forward in this project. We agree that the use of the Context Sensitive Solutions is preferred, but would prefer to see that any such Context Sensitive Solutions be implemented where feasible rather than merely “considered as is suggested in the Draft MOA.

Sincerely,



Todd A. Zeiger
Director, Northern Regional Office

Written Comments

Comment No.	Name / Organization / Comment Date	Comment	Response
1	Robert N. Shoaff 2551 Westbrook Drive City of Fort Wayne, Indiana July 18, 2014 (Fax - Hearing Comment Sheet)	I respectfully request that the City of Fort Wayne and the Fort Wayne City Council allow the residents of the neighborhoods affected to have some meaningful input to the planning process. This would include serious consideration of alternative plans like the Arch Plan. So far the City's attitude has been that its plan is the only plan and the residents can take it and like it.	The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.
2	Michael Galbraith Executive Director of ARCH June 18, 2014 - Provided letter dated December 9, 2013 from Margaret T. Storrow Storrow Kinsella Associates at Public Hearing June 24, 2014 same letter provided by e-mail	Storrow Kinsella Associates, in collaboration with Transportation Solutions, LLC has completed our analysis, evaluation and development of a sketch plan reconstruction alternative for the five-lane roadway widening project currently being proposed by the City of Fort Wayne. We are confident that the resulting report provides a foundation for ARCH, Inc., and the Consulting Parties Team to have a constructive dialog with the City and its consultant. The Consulting Parties Proposed Alternative (CPPA) provides a solution that restores Spy Run Creek Parkway continuity, and maintains the integrity of the Brookview-Irvington historic neighborhood while meeting overall economic development, flood control, connectivity and beautification goals. Please note that this report is a "sketch plan" alternative. We have made a good-faith effort to consider existing conditions and the purpose and need of the project, but must emphasize that additional design and study will be needed before this proposed alternative can be fully incorporated into the City's project. The costs for the CPPA appear to be of similar magnitude or less than the City's Preferred Alternative, based on comparative lane-miles, bridge cross section and length, and probable right-of-way acquisition. In addition, the long term maintenance costs are comparable or less for the CPPA, based on thoroughfare lane length reduction from 5,175 to 2,100 feet. The CPPA, has a two-lane facility, appears to be able to accommodate the 2005 peak hour recorded traffic volume of 750 vehicles per hour per lane. Additional analysis of the corridor as a whole will be needed to assess the future expected performance of the proposed alternative. We remain available to answer questions and assist you and the City with moving an improved State Boulevard Reconstruction Project forward into implementation. Enclosures (see Written Comments Page 4 -13)	The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.

3	<p>Karl Dietsch Fort Wayne Resident</p> <p>June 18, 2014 (written copy of verbal statement)</p>	<p>Talk at Final Hearing on the State Boulevard Project</p> <p>My name is Karl Dietsch, a Fort Wayne Resident.</p> <p>Congratulations to all involved in the final plans of the State Boulevard Project. The well thought out city plan to create a State Boulevard improvement between Wells Street and Spy Run has stood up to the test of time and logic. I appreciate city officials being cooperative in explaining the various design stages of this project in the past years. Many hearings have been held for all to attend and offer constructive comments both verbally and in writing.</p> <p>As a resident of the Brookview Neighborhood and living among the closest homes to and affected by the future State Boulevard Project, I am aware of the strong approval to the city's plan by a majority of my neighbors. Approval ratio of the city's plan is 3:1 on Oakridge Road and 2:1 on Terrace Road. In the Northwest Quadrant, 36 association leaders are in favor of the city's plan for a ratio of 6:1.</p> <p>The city has stated from the beginning that the State Boulevard Project's purpose was to build a safer, higher capacity and elevated street while replacing the Spy Run Creek Bridge. The final drawing presented here tonight is the result of all input that has been made to the designers within the time limits all of us have followed. Not all inputs of mine and others have been included but I accept this as the final decisions are not ours. The city's final design is great and fulfills the project's stated purpose.</p> <p>In the early days of this project, Phase I and II were to be built in 2012 and 2013. Today's schedule is for 2016 and 2017. Why the 4 year delay? Much opposition was formed by a minority of the Brookview Neighborhood when an association was created to fight the State Boulevard Project rather than to represent the views of all members in the association. This association got support from City Councilman John Shoaff. He in turn got support of Arch of Fort Wayne (of which he has been a past president of the board of directors) and the support of Indiana Landmark (of which his wife is a director). Indiana Landmark placed the project on the 2013 Indiana Landmark 10 most critical endangered sites list. This April 2014, the project was removed from their 2014 list.</p> <p>Around the same time, Arch of Fort Wayne placed this project on their "2014 most endangered list". Arch has gone out of its way to discredit this</p>	<p>Due to environmental issues, most significantly the historical nature of the surrounding area, and the need to fully evaluate all potential alternatives associated with the proposed project the time it took to properly prepare the required documents impacted the schedule resulting in the delayed timeline.</p>
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		<p>project in unethical ways. The city sponsored 3 open viewing to educate the public on the city's project plans. Arch encouraged the public at the 3 viewings to fill out Arch's survey slips (not the cities). These survey slips asked for input on the design of 2, 3 and 5 lane project construction but left out a choice of 4lanes as on the city's plan. Arch encouraged people to fill out their comment cards and stated that repetition is a good thing because each card is a vote. No cards said who was responsible for the survey and no signature line was provided on the cards. I consider this as fraudulent activity and a discredit to Arch of Fort Wayne.</p> <p>The environmental impact of the city's project is nil due to the fact that all houses to be removed are already environmentally damaged from flood waters filling their basements. This makes the city's plans as drawn for the State Boulevard Project, a win-win for the home owners, city and all future users of State Boulevard. Let's Build It!</p>	
4	<p>John E. Modezjewski, President of North Highlands Neighborhood Association and President of the North West Urban Development Coalition</p> <p>Bud Mendenhall, President of Bloomingdale Neighborhood and Vice President of the North West Urban Development Coalition</p> <p>John Meinzen Spy Run Neighborhood Association and Founder and Leader of the Core Group</p> <p>(All three individuals comments are represented in this one letter)</p> <p>June 18, 2014 (written copy of verbal comment)</p>	<p>We have asked Karl Dietsch to speak for our neighborhoods at this meeting tonight, as we have a real neighborhood event that is taking place tonight. The Drug and Gang Awareness Night in Hamilton Park is a real neighborhood issue where many of the neighborhoods in the North West area of Fort Wayne have come together as a group of concerned neighborhoods to stand against a real problem, not a manufactured issue that the State Boulevard detractors have been using to stall the progress and construction of a much need improvement to the North West area in the City of Fort Wayne.</p> <p>We need the State Boulevard project to be built as designed by the City of Fort Wayne and approved by the citizens of Fort Wayne at all of the meetings given by the City of Fort Wayne Board of Works and the Traffic Engineering Department.</p> <p>The project needs to be started as soon as possible!! Stop the posturing of a monitory of citizens and the grandstanding of our elected officials and let's get the project moving forward as designed.</p>	No response.

5	<p>Susan R. Haneline City of Fort Wayne, Indiana</p> <p>June 18, 2014 (Hearing Comment Sheet)</p>	<p>As someone whose home is in the actual footprint of the project I am in favor of the project. It will benefit the neighborhood in moving traffic away from the actual homes and provide relief in flooding and increase safety for everyone. The cast majority of us in the actual footprint of the project support this project.</p> <p>As things are currently – our home values have fallen, we are paying out thousands of dollars in flood repair/insurance. Trying to get in and out of our driveways on State Boulevard is a lesson in patience and risk taking. Please do not further delay this project it is something that will benefit so many people.</p>	No response.
6	<p>Phil Miller, President/Owner Darrell Henline, Controller Classic Stereo and Video 2312 Clinton Street Fort Wayne, Indiana 46805</p> <p>June 18, 2014 (letter)</p>	<p>We are very much in favor of the Proposed State Boulevard Reconstruction Project as stated in today's Legal Notice of Public Hearing and as the renderings reflect on the city of fortwayne.org website. Classic Stereo has been located at the North East Corner of Clinton Street and State Boulevard for 48 years; all of which Phil Miller has been with the business. I have been with the company for over 36 years. Over these many years, we have seen how the daily congestion, accidents and flooding adversely affect businesses and homeowners alike.</p> <p>With the addition of extra lanes in each direction and turn lanes as indicated, the design should smoothen the traffic flow to and through the intersections of State Boulevard and Spy Run, Clinton Street, Wells Street and the other adjoining streets in the area. The dreaded morning and late afternoon/evening rushes should have shorter and fewer delay times at the traffic signals. The elimination of the snake curves and the installation of new street lighting should also provide healthier traffic conditions as motorists, bikers and pedestrians are able to view traffic clearer while entering or crossing State Boulevard. The Project also appears to address, in a positive manner, the flooding issues that have constantly bombarded the community. Adding the appeal of the new greeneries beautifies the drive through the area.</p> <p>We might also note that any discussion of cutting lanes, lane restrictions or adding roundabouts would not address the needs and goals that have been predetermined and deemed necessary in previous public hearings, as well as meetings held within pertinent city departments. And, in fact, these ideas may actually be detrimental to and serve as an actual downgrade to current traffic and environmental conditions in the area.</p> <p>In conclusion, we are in desperate need of this Proposal as stated in the</p>	No response.

		<p>Legal Notice of Public Hearing and as the renderings reflect on the cityoffortwayne.org website.</p> <p>I might add that Phil Miller, President and Owner of Classic Stereo and Video, would like to formally apologize for not attending tonight's hearing. He had originally planned to participate in the discussion. But, he has been working at a jobsite out of town the last two days, and won't be able to be here.</p>	
7	<p>Sue Beck 3322 garland Avenue Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (letter)</p>	<p>My name is Sue 'Beck I live in the Irvington Park Neighborhood near the planned projected area and am writing in strong opposition to the city's preferred alternative for State Blvd reconstruction, 3A. Rather than an improvement upon existing conditions, I feel that the city's plan will be highly damaging to overall safety along State Blvd and our surrounding neighborhood, and will negatively impact flash flooding, neighborhood character, our property values, not to mention quality of life.</p> <p>I do support the Storrow Kinsella Associates/Transportation Solutions LLC- prepared conceptual solution presented by Arch/Indiana Landmarks and others. I think (feel) we should keep State Blvd to, two lanes. I think they should remove the remains of the train over pass, and widen the street in that area, that would take most of the curve away, then replace the aging bridge over the Spy Run Creek, Eastbrook and Westbrook are such a very historic neighborhood, why does anyone want to destroy that? not to mention all the kids from North Side High School that walks down that street twice a day every day, when we have heavy snow like we did last year the kids had to walk in the street, because the sidewalks were not safe, so IF there were four lanes how dangerous would that be?</p> <p>Brookview and Irvington Park both want to keep our neighborhood safe, clean, quiet, serene for our children and our grandchildren,. most of U'S have lived here around 25-30 year some even longer, when a house goes up for sale here it is sold BEFORE a sign goes in the yard in a lot of the times that is what Eastbrook and Westbrook means to. us, no we don't want 4 lanes, please keep our neighborhood family safe.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
8	<p>Julie Donnell President, Friends of the Parks of Allen County, Inc.</p> <p>July 16, 2014 (letter)</p>	<p>The Friends of the Parks of Allen County, Inc. appreciates the opportunity to respond to the Environmental Assessment document pertaining to the above referenced project.</p> <p>The first part of the assessment enumerates a number of public hearings and meetings with the public and with private citizens about the project. These meetings took place AFTER the City and NIRCC developed an</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>RFP for a five lane project and AFTER the engineering drawings were underway. (November 29, 2007.) Therefore, they do not correspond to the requirements of Context Sensitive Design, nor are they evidence that the City and NIRCC did due diligence in considering the needs of the neighborhood, and the important historic significance of the landscape that the project will affect. At no time was the public offered an alternative that would address the very strong concerns of neighborhoods up and down the corridor, or respect the newly uncovered understanding of this area a nationally significant landscape.</p> <p>The EA asserts that a de minimus finding is warranted for Vesey Park. This aspect of the Environmental Assessment should be reconsidered, because the EA definition of Vesey Park appears to conflate Vesey Park with a separate Parkway called Brookview Parkway in the National Register Nomination. The EA explains Vesey Park as follows: Vesey Park" is operated by the Fort Wayne Parks and Recreation Department and includes the greenspace between Eastbrook and Westbrook Drive connecting the larger partition of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary's River." This greenspace between Eastbrook and Westbrook Drive is, in fact, Brookview Parkway, and this parkway will be affected significantly by the project. Having it included in the definition of Vesey Park is probably incorrect and the finding of de minimus may apply to Vesey Park, but Vesey Park does not include Brookview Parkway where the project is actually located.</p> <p>The EA asserts that the Fort Wayne Parks and Recreation Department wrote a letter of support for the de minimus finding for Vesey Park. The letter in question does not "support" anything, but simply states that it "appears that there will not be any impact to the recreational activities, features, and attributes of the land currently being used as recreational space." It may be that the Parks and Recreation Department only considered Vesey Park, and not the affected Brookview Parkway. Neither the request from American Structurepoint nor the letter from the Parks and Recreation Department, found in appendix J, reference the Parkway, and the letter from the Parks and Recreation Department refers only to recreational resources, rather than the full set of values that are included in evaluating a significant historic landscape.</p> <p>Our board supports the alternative concept that was commissioned by ARCH and Indiana Landmarks and which was presented at the recent public hearing for the Environmental Assessment in June and later to the</p>	
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		<p>public at a public hearing on June 30 of this year. In 2007 our board fashioned a position that any treatment of State Boulevard and Brookview neighborhood should only go forward under the auspices of a landscape architect who understood the multiple values that are part of a significant American designed landscape. Storrow Kinsella, such a firm in Indianapolis, with the aid of a qualified traffic engineer, developed this concept It addresses the Purpose and Needs Statement as well as the concerns of historic preservationists, neighborhood groups and those affected up and down the State Boulevard corridor.</p> <p>The alternative concept is preferable because it retains the distinctive landscape features of the fine design work of Arthur Schurcliff and will help maintain the character of the neighborhoods up and down the corridor.</p> <p>With all of the advances in knowledge and technology in this century, this community deserves a better solution than the out of date one that NIRCC and the City Administration have put forward.</p>	
9	<p>Michael Galbraith Executive Director, ARCH, Inc.</p> <p>July 18, 2014 (letter/e-mail)</p>	<p>I am writing in reference to the June 5, 2014 updated FHWA Findings and Determinations materials provided as part of the Section 106 Review, and the May 2, 2014 Environmental Assessment for the proposed State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana (Des. No. 0400587, D HPA No. 5903, Project No. IN20071404.) Thank you for the opportunity to comment and the work that went into preparing this and the related Environmental Assessment document for the above referenced project. I have a few specific points that I would like to address concerning the document and proposed project.</p> <p>1. We concur that the project as currently proposed will cause an Adverse Effect under Section 106 and Section 4(1) to the three historic properties (Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge carrying State Boulevard over Spy Run Creek) located within the APE.</p> <p>2. We believe that purpose of any Section 106 Review is as identified in 36 CFR 800.1 (a) to "seek to accommodate historic preservation concerns with the needs of Federal undertakings through consultation among the agency official and other parties with an interest in the effects of the undertaking on historic properties, commencing at the early stages of project planning.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>The goal of consultation is to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties." Pursuant to that purpose, we believe that the Findings and Determinations materials provided on June 5, 2014 adequately identifies historic properties and assesses effects of the project as currently proposed. However, we believe that the third portion of the identified goal to "seek ways to avoid, minimize or mitigate any adverse effects on historic properties" is inadequate. To further the discussion, ARCH, Indiana Landmarks, Friends of the Parks, and Brookview-Irvington Park Neighborhood Association commissioned Storrow Kinsella Associates and Transportation Solutions to examine the background research developed for this project to see if there was an alternative that better protected the neighborhood, fulfilled the purpose and need for the project, was prudent and feasible and avoided, minimized or mitigated the adverse effect to the neighborhood. We believe that the alternative plan (the "Alternative"), presented by ARCH to American Structurepoint at the June 18, 2014 Public Hearing for the Environmental Assessment and provided to American Structurepoint, FHWA, INDOT and Indiana SHPO by e-mail delivery on June 24, 2014 and June 26, 2014, better addresses historic preservation concerns while accommodating the purpose and need of the Federal undertaking.</p> <p>3. We believe that the Alternative is provided during the completion of Section 106 Review and is thus governed by the guidance in 36 CFR 800.1(c). This guidance mandates that the Alternative must undergo unrestricted consideration by the agency official, as indicated in 36 CFR 800.1(c), which directs the agency official to complete the Section 106 process in a manner "that such action do not restrict the subsequent consideration of alternatives to avoid, minimize or mitigate the undertaking's adverse effects on historic properties."</p> <p>4. We believe that the Alternative is presented in agreement with direction contained in 36 CFR 800.3 (b) which directs the agency official to coordinate the Section 106 Review with "... the overall planning schedule for the undertaking and with any reviews required under other authorities..." including NEPA and Section 4(f) of the Department of Transportation Act.. We further believe that guidance contained in 36 CFR 800.8(a)(1) encouraging agencies to "consider their section 106 responsibilities as early as possible in the NEPA process ..." provides further impetus to consider the Alternative plan under both the 4(f) and</p>	
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		<p>NEPA processes.</p> <p>5. We believe that the Alternative meets the identified Purpose and Need for the project. As stated in the May 2, 2014 Environmental Assessment, the Purpose and need for the project is as follows:</p> <p>“The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorist, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary’s River.</p> <p>The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian’s safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.”</p> <p>We believe that the Alternative meets the purpose of the project by improving corridor connectivity for both motorists and pedestrians. The identified problems of congestion, substandard sight distance, and geometrics are all addressed in the Alternative. Roadway flooding as well is addressed, although this remains a concern for both the currently proposed project and the Alternative. The nearly adjacent and recently completed US 27 Bridge Reconstruction project (Des. No. 0200914- and 0101527) experienced roadway flooding a little over a year after the project’s conclusion, demonstrating the need for a thorough investigation and remediation of a larger problem. Regarding the need for the project, we believe that the Alternative addresses the identified congestion, substandard sight distance, and geometrics concerns as well as the pedestrian safety concerns.</p> <p>6. We believe that the Alternative significantly "minimizes, mitigates or avoids "adverse Effect to historic properties as is identified as a purpose of Section 106, and makes "special efforts to preserve ... historic sites" as</p>	
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		<p>is the stated purpose of Section 4(f)).</p> <p>Comparing the enumerated Adverse Effect of the current plan as stated on page 206-209 of the Environmental Assessment, the Alternative proposal results in far less damage under both Sections 106 and 4(f), making it a prudent and feasible alternative that causes the least overall harm. In the Fort Wayne Park and Boulevard System Historic District, and in the Brookview-Irvington Historic District, the Alternative avoids to a much greater degree the removal and disruption of the NRHP listed State Boulevard, minimizes the amount of new land disruption by using land previously altered through the removal of “flood buyout” houses, and retains the plan of the Arthur Shurcliff-designed plat. In addition, the Alternative avoids the demolition of NRHP listed residences entirely. The Alternative mitigates the disruption and bifurcation of the Shurcliff-designed plat by the use of a curvilinear “new” State Boulevard replicating the scale of the “original” State Boulevard, providing the driver and pedestrian alike with a similar feel, design and setting.</p> <p>7. We believe that the Draft Memorandum of Agreement (MOA) AS PRESENTED IN THE May 2, 2014, Environmental Assessment and the June 5, 2014 Section 106 Review will need to be modified, particularly if the Alternative is adopted as the basis for proceeding forward in this project. We agree that the use of the Context Sensitive Solutions (CSS) is preferred, but would like to see that any such CSS solutions be implemented where feasible rather than merely “considered” as is suggested in the Draft MOU.</p>	
10	<p>Kevin Donovan and Lori Donovan 2740 Eastbrook Drive Fort Wayne, Indiana 46805 Brookview Neighborhood</p> <p>July 17, 2014 (letter)</p>	<p>I am writing in brief to express my opposition to the city’s proposed plan (which they have called Alternative 3A) for the State Street Project. I do believe that this proposal is implemented it will not only shun the citizens that live in the Eastbrook and Westbrook and rest of the Brookview Neighborhood area but will also create a greater health risk to our area’s children going to and from school. It will also totally negate the integrity and natural beauty of one of our city’s oldest and characteristic parks, Brookview Park, and of a big part of our neighborhood. For numerous years now the city has been planning a renovation of this area of town. I agree that something can be done here, due to high levels of traffic, and I want to believe a true and genuine concern for the citizens that live here (including myself and my wife at 2740 Eastbrook Drive) plus a hill that keeps the guardrail manufacturers in business year round. These school children are the city’s future, our future council-people and civil servants and possibly even our Mayor, and they will have to maneuver the speedy</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>cacophony of traffic to accomplish their education. The current slow curve that State Street follows was built that way for a purpose; to slow people down to enjoy an oasis of country life while living in the city.</p> <p>During recent years during this planning I sensed a great lack of transparency from the city. My feelings about that were confirmed when in 2008 I learned from a neighbor who had phoned the city for information and shared it with the rest of us living in the area and who invited the City engineer to come and meet with us, not from the City asking, or even telling us of their plans, just three weeks before demolition of Westbrook Drive south of State Street where the city was installing a Rain Garden south of State. This was an attempt to address the flooding issue and I respect that, and appreciate that our neighborhood's intervention in the city plans there informed us and also managed to keep the sidewalk that follows the former Westbrook Drive that takes you to Jacobs and Clinton Street.</p> <p>I am in support of the conceptual 2-lane alternative proposal to the city's plan presented recently by ARCH. I do believe this plan will be all-inclusive just by its simplicity. I get a sense of it being personable and friendly. The route that this alternative plan suggests would lower the number of houses that would need to be demolished. The curve and two round-a-bouts will keep traffic in a safe and controlled environment and slow people down to enjoy an oasis of country life while living in the city. The insertion of bicycle paths will keep the children and others away from the traffic and allow them to grow up and be inspired in their own civil careers. Change is not hard, resistance to change is hard.</p>	
11	<p>James Monroe 3330 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>April 17, 2014 (letter)</p>	<p>Over the past few years, an emphasis has been placed on re-engineering downtown, Fort Wayne. Part of the plan includes encouraging people to live in the City proper.</p> <p>I have resided in Irvington Park for the past twenty-one years with an appreciation for this unique, historical neighborhood. Residents of this area and adjoining Brookview talk frequently about preserving the ambience that makes living here attractive. The widening of State Street will impact this area in a way that concerns me. If it isn't completed with neighborhood preservation as a priority, the project will be serious blow to the area's integrity. Well maintained family homes surrounded by old growth trees, a creek and wetlands should be treasured.</p> <p>Careful planning can and should be used to insure that one of Fort</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>Wayne's best residential areas remains intact.</p> <p>Granted, State Street is in need of some kind of re-design, my hope is that this can be accomplished with minimal impact. Neighborhoods like Brookview and Irvington Park should be valued for their unique character and preserved.</p> <p>P.S. I'm opposed to cities plan 3-A concerning State Street widening. I would support a plan that would incorporate some of the ideas expressed by Storrow Kinsella / Transportation Solutions, LLC.</p>	
12	<p>Barbara A. Brenneman 2526 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (mail/e-mail)</p>	<p>I moved into my house on Eastbrook Drive in the Brookview addition in Fort Wayne, Indiana in November of 1980. The thing that drew me to it was the neighborhood. I was introduced to the neighborhood by a friend that lived in a lovely little white house with a lovely veranda at the curve that follows the creek north of State Boulevard. We bought our house; I loved the neighborhood – it reminded me of New England with curves and hills. About a year after we bought our house, our friends moved to a house two houses toward State because their family was growing and they wanted to stay in the neighborhood with its character and thoughtful design (she was an interior designer) and I bought the little white house from them for my mother to move into; she moved from Massachusetts where I was raised. My house sits five houses to the north of the little white house, making my house one block north of State.</p> <p>So, I have lived here awhile and have a fair amount of emotional investment in addition to whatever financial investment would be figured into that equation.</p> <p>I raised a daughter in this neighborhood. She saved a kitty from crossing State Blvd. when she was nine. The saved kitty went to graduate school with her some years later.</p> <p>That's a little background of why this neighborhood means something and I'd like to see that respected. I didn't find out until 2008 why the neighborhood reminded me of my home in New England. It was designed by Arthur Shurcliff, native of Boston – and I had grown up 13 miles southwest of Boston.</p> <p>What I am writing to you about is to encourage that, for the State Blvd reconstruction, the proposed plan created by Storrow Kinsella Transportation Solutions, LLC, presented by ARCH this past June to be the design, because I think it would have been something that Shurcliff</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>would designed. It respects the historic nature and flavor in a way that the city of Fort Wayne engineers have not in their proposed solution to handle the traffic issues. Progress is here, needs are understood, but let us do the work with respect to the ambience. With no turns on the "Brookview bypass," east/west traffic would flow nicely without interruption. With the roundabouts, the flow would be enhanced.</p> <p>This project issue has gone on way too long. Let's make it a statement; this beautiful merging of design old and new, with sensitivity to the historical nature of the area.</p> <p>Please note: I will be printing this out and sending it US mail as well. I know how busy your inbox is in the cyber world.</p>	
13	<p>James W. Sines 3322 Irvington Drive Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (fax)</p>	<p>As a resident of Irvington Park for over 37 years and a former association president, I am concerned about the State Boulevard Reconstruction Project proposed by the City of Fort Wayne. This plan calls for a 4 lane route straightened from the current State Boulevard route. This plan does not provide for safe or sufficient access to and from other neighborhood streets. Instead, I strongly favor an alternative plan proposed by ARCH and Indiana Landmarks calling for a 2 lane route carving south of the current route with traffic lights and dedicated connections and allowing for safe turns onto these neighborhood streets.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
14	<p>Julie Ratliff 2510 Terrace Road Fort Wayne, Indiana 46805</p> <p>July 16, 2014 (letter)</p>	<p>In Reference to Alternative City Wants/Plans 3A</p> <p>I am for this plan to widen State Street by moving it further South, presented by ARCH. I like that heavy traffic will not directly border my historic neighborhood. My street sees a lot of "cut-through" traffic from Clinton to State, making it dangerous for kids at play. This plan would help present this, I also like that our neighborhood will not be divided by 5-lanes of road as in the city's plan. The city's plan also makes it difficult for me to turn left/east on State. Hoping you really consider this new design.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

15	<p>Ms. Nigel J. Perry 2134 Cass Street Fort Wayne, Indiana 46808</p> <p>July 18, 2014 (Hearing Comment Sheet)</p>	<p>I would like the Spy Run to Cass Street project to be limited to 3-lanes. The 5-lane project will have a greater impact on the environment. Five lanes also mean more money for maintenance. I don't like the median that has to be maintained as it grows. I do not want State Boulevard to become a east west truck route. The reason why there are so many accidents on the south corner of Westbrook Drive and State Boulevard is speed! I did not hear mentioned how wreckers from Kelly wreckers services is too negotiate the new lanes. I bet Cass Street from State to Jacobs Avenue will be used. School buses will also have to use the narrower lanes proposed. In winter this seems to be an added safety concern.</p> <p>This is a 2 issue problem. It will not solve the flooding problem of the Spy Run Creek. The flooding is definitely an environmental problem and raising the bridge to 9 feet will do little to help. Fort Wayne goofed when building started north of the City and no retainer ponds were required.</p> <p>To put a 5-lane expressway in will divide the north from downtown, discouraging citizens from enjoying the progress the City planners has made with the downtown area. We have Coliseum Blvd for trucks and I-469 clear around the city. Who wants to use the Pufferbelly Trail with all the emissions from the trucks to ruin our lungs? Fix the bridge, the sidewalks for safety; enforce the speed limit we now have in force so cars and trucks don't endanger homes along the State Blvd. The City should buy-out the homes of the people that have been promised a buy-out for almost 7-8 years.</p>	<p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
16	<p>Juliana Fleischer 2201 Cass Street Fort Wayne, Indiana</p> <p>July 18, 2014 (Hearing Comment Sheet)</p>	<p>I agree with others at the meeting of the importance of addressing the continual flooding dilemma. It would be a waste of time and money to complete the State Boulevard Reconstruction Project and still have flooding. It's a shame the \$4.4 million earmarked for a "round-a-bout" at Wells and Fairfield can't be used to address and resolve the flooding situation. If there are alternatives to the current proposal can we hear about them first? I don't oppose the project, but I do still fear the access for residents in the 2100 and 2200 block of Cass along with access from Oakridge has not been addressed.</p> <p>We keep being told that with 4 lanes it would make it easier for us to access State Boulevard. I know I represent only a 2 block area on Cass Street, but please don't let us fall into the bureaucratic pit. It is currently difficult to access State Blvd from 2100, 2200 block of Cass and the same is true of we use Jacobs and try to access Wells Street (especially trying</p>	<p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part</p>

		<p>to make a left turn).</p> <p>In ending, please just start the project so we can see an end. The area gets more run down every year because the people do not want to maintain properties the City will end up buying. I hope these people are paid the "fair market value" of their properties back in 2008 before this all started. Hope everything can be resolved soon for everyone.</p>	<p>of the stated purpose of the proposed project.</p> <p>Dedicated left turn lanes will be provided along State Boulevard for residents turning onto Cass Street or Oakridge Road. Oakridge Road will be stop sign controlled with a left and right turn lane for residents turning onto State Blvd. Cass Street will be stop sign controlled as well.</p> <p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
17	<p>John H. Shoaff 4646 West Jefferson Boulevard Fort Wayne, Indiana 46804</p> <p>July 15, 2014 (letter/e-mail)</p>	<p>Because good CSS (context sensitive solutions) practices were not observed, and because the engineers (American Structurepoint, Inc.) were required by the terms of a City RFP of November 29, 2007 to engineer a five lane roadway of city-specified contour, they were not free or well informed enough to pursue an optimal solution to the State Boulevard situation. In fact, they were required to produce a plan that has been determined to have an adverse impact on the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the Bridge over Spy Run.</p> <p>Acting on better information about the historical importance of the Brookview and Irvington Park neighborhoods, and in response to widespread opposition to the city's current proposal voiced repeatedly after the city's intentions as expressed in the 2007 RFP were made known, Indiana Landmarks, ARCH, and the Friends of the Parks of Allen County retained services of well-regarded engineers and planners Storrow Kinsella Associates, whose firm produced a plan that demonstrates a "prudent and feasible alternative" to the AS plan and its multiple adverse impacts. Because it would be good for the city, and because it meets the requirements of federal statute that a prudent and feasible alternative, when available, must be chosen over proposals that have adverse impacts, this plan should be the basis for the final plan.</p> <p>The plan proposed by SKA holds great promise for the directly affected Irvington Park and Brookview neighborhoods and for the city of Fort Wayne at large. Relative to the five-lane plan dictated by the city, the SK plan has only two lanes, and they are gracefully curved and moved well</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>south of the Brookview neighborhood. In scale and character they complement the roads laid out by Arthur Shurcliff. Reduction to two lanes significantly reduces the road's contribution to the flash flooding that is such serious threat to this neighborhood.</p> <p>It also reduces the heat island effect, and avoids the imposition of an overscaled, out-of-character roadway that will overwhelm the modestly scaled, charmed neighborhood. Planners call major traffic carriers of this scale "barriers," and this one will create the appearance of a barrier between this neighborhood and downtown, at the very time we should strengthening the relationship between them.</p> <p>The city's preferred plan is a dinosaur, an anachronism dragged from the 20th Century into the 21st: the concept for the city's plan goes back 30 to 40 years, when migration from the city was in full sway, and traffic volumes were growing everywhere. Now however, things are very different. Traffic volumes nationwide peaked in 2007, fell significantly in the recession, and then leveled off at lower levels. With respect to State Boulevard, Dana is stabilizing its operation at about 1/10 its former work force, and Parkview hospital has largely moved out, significantly lowering present and future traffic needs. Gasoline tax revenues have fallen, and the Federal Highway Funds face bankruptcy. Furthermore, cities everywhere are learning that they must rebuild their federally paid-for projects at their own expense. Cities – including Fort Wayne – are learning that after overbuilding their road systems (a common consensus among urban planners) in the 20th century, they must now repair and rebuild them with declining gas tax revenues in the 21st.</p> <p>And we live with other unpleasant consequences of overbuilding; we have exacerbated urban flash flooding; we increase the heat island effect and unhealthy air. We will intuitively understand that vehicle exhaust is unhealthy, but now studies now have found definitive correlations between living near heavy traffic and heart disease, respiratory problems, and autism; plans like the city-preferred solution that appear to be designed to attract new traffic heavily populated areas should be avoided.</p> <p>The proper solution to the State Boulevard problems should be economical of hard surfaces and as modest in scale as conditions and traffic needs permit, and above all it should support a major and very encouraging trend of our times – the powerful counter-migration back into the city. One of the attractions to the many who now seek the urban experience is the charm, uniqueness, and closeness to downtown of a</p>	
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		<p>city's historic neighborhoods. The best way a city can foster this counter-migration is to protect and enhance the pleasure of living in these neighborhoods. This means, among other things, slower traffic, cleaner air, and roads scaled to a neighborhood's residential scale.</p> <p>SKA has demonstrated that a road fully responsive to the traffic needs of the area can be two lanes wide, a significant distance from the homes, and in character and scale much more like the roads of the Brookview neighborhood: it is the road that Arthur Shurcliff might have designed himself, and it or something like it is the road design that should be part of the historic Brookview neighborhood.</p> <p>Enclosures (see Written Comments Page 4 of 13)</p>	
18	<p>Rene L. Jackson</p> <p>July 18, 2014 (e-mail)</p>	<p>As a former resident of Fort Wayne, I am very concerned about the plan to build a multi-lane arterial through one of the cities quaint historic neighborhoods. I understand that this plan is going to require 12-15 homes being demolished in this historic district. If there is an alternative that does not have this severe impact on the Brookview-Irvington Park neighborhood and can preserve the character of the neighborhood, it should be the preferred approach to the project. Fort Wayne's historic urban core and vibrant downtown are its most appealing qualities, they should be preserved and protected.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
19	<p>Michelle Briggs Wedaman</p> <p>2326 Eastbrook Drive</p> <p>2418 Eastbrook Drive</p> <p>Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (e-mail)</p>	<p>My family and I live in Brookview Neighborhood at 2326 Eastbrook Drive and also own 2418 Eastbrook Drive, both properties in the block of the planned project area. A consulting party on this project, I am writing in strong opposition to the City's preferred alternative (3A) for State Blvd reconstruction. Drawn to Fort Wayne from Seattle by the pleasing center-city tranquility of this City Beautiful neighborhood where my family has owned homes since the 1930s, I can speak to the deep personal attachment so many residents have for this welcoming and appealing historic neighborhood of curving streets and sidewalks and parks, where repair and improved care of public spaces is needed and welcomed.</p> <p>However, rather than an improvement upon existing conditions, I feel that the City's plan 3(A) will be highly damaging to overall safety and accessibility for users traveling along and seeking to cross State Blvd (vehicle traffic, walkers, bikers, and public transit and school buses), safety in our surrounding neighborhood character, quality of life and our property values.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p> <p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street.</p>

		<p>The city points to numerous community meetings, and though they modified their plan to connect Oakridge Road rather than Terrace/Eastbrook to “new State”, have included some appropriate street lighting, landscaping and sidewalk/trail and finally agreed in 2013 to provide drawings of their proposal for the public to see, the most important element of the plan – the new road’s straight route and 5+ lane width – have not changed from the City’s “preliminary route drawing” I saw in July/August 2008 when I contacted the City’s engineer to inquire about State Blvd. project (and learned of the City’s immediate and uncommunicated plans to remove Westbrook Drive south of State and install a Rain Garden at that site) and invited him to walk-around on-site neighborhood meeting, prior to the City hiring American Structurepoint to explore and engineer possible alternatives.</p> <p>The City’s stated need for the project – to improve State Blvd. road way flooding, replace its aging Spy Run Creek bridge and relieve vehicle congestion – can be addressed with solutions not yet publicly considered by the City. Less damaging alternatives for road width and route with the neighborhood have not been presented or discussed, despite my and others’ repeated requests for them over the past six years.</p> <p>I am writing in strong support of the Storrow Kinsella Associates/Transportation Solutions LLC – prepared conceptual solution publicly presented by ARCH/Indiana Landmarks and others in June 2014, and urge that the City be immediately required to pursue that concept as the preferred alternative. Maintaining State Blvd’s two lanes, curving it gently south and allowing dedicate neighborhood street exits/entrances to “new State” at Westbrook Drive and “Old State Blvd at Clinton St/US 27” via roundabouts will allow neighborhood vehicle traffic to safely enter and travel both east and west on State Blvd from both north and south of State Blvd, elevate the bridge over Spy Run Creek, enhance traffic flow on State Blvd and be in keeping with the neighborhood’s character, design and family-friendly residential value. I urge pursuit of this promising and appealing solution, and ask again why the City has been refusing to even discuss it as a possibility with community and professional representatives seeking to do so for the past year, and why the City did not create such a feasible and less impactful alternative themselves, one that meets the project’s states Purpose and Need, incorporates the best practices for traffic calming with its smaller footprint while meeting the needs of improved traffic flow, would be appealing and safe to walkers and bikers, enhance rather than destroy the Districts, and that I and many of my</p>	<p>However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
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		<p>fellow residents will be excited about and welcome.</p> <p>I urge that the City be actively guided and assisted by the Federal Highways Administration in following the FHA recommended Context Sensitive Solutions for achieving the superior design that meaningfully considers, incorporates and better meets the needs of residents like myself who live in the project shadow. The project design must be informed not only by its direct context of the Brookview-Irvington Park Historic District but also of the adjoining neighborhoods, commercial districts, schools, and parks all along the State Blvd corridor, as well as its important gateway to downtown role and potential, rather than the isolated and artificial box of the APE that has been drawn around the immediate project area.</p> <p>I also urge the City to correct area flooding and flash flooding directly by addressing its root causes, along with immediate purchase of any flood-prone homes) on Eastbrook Drive south of State/State Blvd) using flood-related funds, rather than accomplishing any desired flood buy-outs of homes via this transportation project, which has confused the "issues" within our community,</p> <p>Thanks for the opportunity to comment and participate in this important process.</p>	
20	<p>Mike Wedaman 2418 Eastbrook Drive 2326 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (e-mail)</p>	<p>I own properties and live in the Brookview neighborhood at 2326 Eastbrook Drive and 2418 Eastbrook Drive in the block north of the planned State Blvd area and I am writing to express my opposition to the City's preferred alternative (3A) for State Blvd reconstruction.</p> <p>This plan will be damaging to and will devalue my property, and will make this neighborhood much less desirable as a residential area.</p> <p>I am writing in strong support of the Storrow Kinsella Associates/Transportation Solutions LLC – prepared conceptual solution presented by ARCH/Indiana Landmarks and others in June 2014. Maintaining State Blvd two lanes, curving it gently south and allowing dedicate neighborhood street exits/entrances to "new State" at Westbrook Drive and "Old State Blvd at Clinton St/US 27" via roundabouts will allow neighborhood vehicle traffic to safely enter and travel both east and west on State Blvd from both north and south of State Blvd, raise the Spy Run bridge, allow smooth traffic flow on State Blvd away from neighborhood driveways and preserve the neighborhood's character, additional green</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p> <p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street.</p>

		<p>space, and family-friendly residential property value.</p> <p>I further request that the City correct area flooding and flash flooding directly by addressing its root causes, namely the continued massive developments upstream, which has put the neighborhood (as well as the city) in the situation it is in. I also urge the immediate purchase of any flood-prone homes (on Eastbrook Drive south of State/State Blvd) using flood-related funds, rather than accomplishing any desired flood buy-outs of homes via this transportation project.</p>	<p>However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
21	<p>Carol Lawton 3314 Irvington Drive Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (e-mail)</p>	<p>I am writing to ask that further consideration be given to the alternative plan for the State Blvd project in Fort Wayne that has been proposed by Storrow Kinsella Associates. As a resident of the Irvington Park neighborhood, I often travel the current Eastbrook State intersection by car and also by bicycle. The alternative plan seems to have traffic calming characteristics more in keeping with the surrounding neighborhoods and the potential to be more accommodating of pedestrians and bicyclists. For this reason, I hope the alternative plan can be given serious consideration.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
22	<p>Catherine Hill 734 East State Boulevard Fort Wayne, Indiana</p> <p>July 18, 2014 (e-mail)</p>	<p>My family of four lives at 734 E. State across from Northside Park. Although the proposed elevation, widening and straightening project between Spy Run and Cass Street is not in my neighborhood, I am writing to express my concern as it affects my family, my home, and my adjacent neighborhood (Northside) as well. I have friends in the Brookside neighborhood, my family and I travel that corridor frequently (and have never experienced traffic flow issues), by car as well as on foot and by bicycle.</p> <p>While I applaud the adjustments to the plan that have already been made to address foot and bicycle traffic safety, I remain gravely concerned about the current reconstruction plan. It will speed up and attract more traffic – making State Blvd yet another boring, ugly “highway” in the middle of our city. The logical conclusion is that the property values will plummet, responsible homeowners willing to invest in a valuable neighborhood near downtown like me will leave. How sad! This is in complete opposition to all the exciting news and plans I am hearing about Downtown Improvement and the many opportunities for growth and development in neighborhoods adjacent to downtown.</p> <p>I am a big fan of our historic neighborhoods! My own home was added to the National Register of Historic Homes. The primary reason I chose to</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>live on the near north side is because of the character of the homes and neighborhoods. We have a HUGE opportunity with the Storrow Kinsella Associates/Transportation Solutions LLC proposed that is already supported by ARCH, Indiana Landmarks, and many neighbors in the Brookside and Northside Neighborhood Associations. This option addresses all of the issues (flooding, traffic congestion, and bridge improvements) with the added benefit of becoming a major improvement to the aesthetics of the area. I am confident that my neighbors and I, along with the whole city would be proud of a road reconstruction project that takes advantage of best practices in planning and traffic engineering. It seems to me that if we're going to spend the money anyway, let's PLEASE spend it right.</p> <p>Thanks you for your time and consideration.</p>	
23	<p>Karl Dietsch 2313 Oakridge Road Fort Wayne, Indiana 46805</p> <p>July 2014 (e-mail)</p>	<p>Last Minute Brookview Resident Survey</p> <p>On Wednesday evening, July 16, 2014, at 6:30 pm, the Brookview Neighborhood Association (BNA) held a meeting to discuss both the ARCH and City plan for the State Blvd Project. After much discussion, we decided that each member of BNA would file separately with American Structurepoint because with so many different views among our members, no single common view was agreeable to all.</p> <p>Prior to the meeting, the brochure delivered door to door announcing the meeting, requested all BNA members to take the Brookview Resident Survey online at www.surveymonkey.com/s/BQB66XZ by 7/16/2014.</p> <p>Early during the meeting, when minutes were asked to be read for the last BNA meeting, we were told no minute were available and the last BNA meeting took place more than a year ago. The question was asked, when was the Brookview Resident Survey approved by the BNA? We were told 2 weeks ago. Again no minutes were made available for approving. The BNA meeting of 2 weeks ago had not been announced to all, if any BNA residents. Even more interesting is the survey itself. The BNA president selected the online "basic" free application software online for the survey from Survey Monkey. This survey selection out of 4 choices has a limit of 100 responses per survey.</p> <p>Since the announcement brochure of the 07-16-2014 BNA meeting asked all BNA residents to fill out the survey by the 7-16-2014 meeting, but failed to do the following:</p>	No response.

		<ol style="list-style-type: none"> 1. Did not disclose that the survey was limited to 100 questions 2. Did not have approval of the BNA at an open announced BNA meeting 3. No minutes exist of the BNA meeting of 2 weeks ago for member approval of 7-16-2014 meeting <p>I request that any reporting of the Brookview Resident Survey, for reasons mentioned above, to not be considered a BNA approved or accurate survey of BNA members that reflects the views of most of our 275 household and business members, numbering approximately 800 to 1000 residents.</p>	
24	<p>Karen E. Richards 1913 Forest Park Boulevard Fort Wayne, Indiana 46805</p> <p>July 27, 2014 (e-mail)</p>	<p>I am a resident of a neighborhood east of the portion of State Blvd that contains the Brookview/Irvington Park neighborhoods. My neighborhood, like the Brookview/Irvington Park neighborhoods, is a National Register neighborhood, and like many residents who have chosen to live in the city, I value the attributes of this boulevard neighborhood.</p> <p>There are currently two proposals with regard to the reconstruction of State Blvd. I recently attended the public meeting hosted by ARCH and after hearing their alternative proposal, I believe it most closely fits the criteria for choosing which proposal to implement. It is my understanding that the project that is most prudent and feasible should be the project chosen. The City's proposal would build a 4 lane, with a 5th lane as a median, thoroughfare through the neighborhood, demolishing 14-15 homes in the process. It would clearly change the character of this historic neighborhood, would increase the speed of traffic and adversely affect neighborhoods to the east and west. There is also concern about being able to execute a left hand turn out of either side of State Blvd due to the existence of the median.</p> <p>The alternative proposal creates a by-pass around the neighborhood, does not require the demolition of any homes and only requires the demolition of 1 commercial property. This proposal should have no adverse effect on adjoining neighborhoods and leaves the current street virtually intact. This plan clearly does the least harm to this historic neighborhood! That makes this alternative plan the most prudent and feasible. I urge you to adopt it or to at least incorporate the majority of its components.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p> <p>The center median will not be constructed where it is necessary for left turn movements onto a side street. Dedicated left turn lanes will be provided along State Blvd. for residents turning onto Cass Street, Oakridge Road, Westbrook Drive, and Edgehill Avenue.</p>

25	<p>Eileen R. Lee 2402 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>July 17, 2014 (e-mail)</p>	<p>Please consider my comments regarding the State Blvd Reconstruction Project</p> <p>After many years and meetings with city officials, I am displeased with the State Blvd reconstruction project that has been presented to our community. To raise the bridge, straighten the road, expand the number of lanes to 4, plus a 5th turn lane will create a high speed road in the middle of a wonderful, historic community and thereby ultimately create a wasteland where a wonderland once stood.</p> <p>Also this project neglects to address the situation of creek flooding in the area. It merely raises the bridge high enough so that it would not be a problem in moving traffic through. This road is similar in size to Coliseum Blvd which circles the north and east sides of Fort Wayne, and is inappropriate for running through a city and community.</p> <p>Recently our community was presented with a consulting proposed alternative plan (CPPA) to the plan INDOT DES#0400587. This plan was created by Storrow Kinsella Associates – Transportation Solutions LLC at the request of ARCH, Inc. I am much in favor of this CPPA plan as a better solution to replacing the bridge, creating a length of road between Westbrook Drive and Clinton Street free of interchanges, and holding to the historic character of the Brookview community.</p> <p>The CPPA plan allows for open spaces to remain and to be expanded upon that may also serve to hold water during time of creek overflow.</p> <p>The CPPA plan deserves further consideration, study, and development. There are many positions to the use of traffic are considered, and there will be less roadway water runoff during times of heavy rains.</p> <p>Thank you for taking my comments into consideration.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p> <p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
26	<p>Paul Gibson Irvington Park Association</p> <p>July 16, 2014 (e-mail)</p>	<p>I am commenting on a few of the benefits proposed by this alternative concept. I prefer the 2 lane State Blvd proposal opposed to the City of Fort Wayne's 5 lane proposal. Two or three lanes of traffic should handle the flow of traffic sufficiently. Five lanes of traffic proposed by the City of Fort Wayne appear unnecessary. State Blvd is only 2 lanes West of Wells Street. The City has said that there are no plans of expanding lane size or number East or West of this project. I like the proposed Southern curve to State Blvd. This will take traffic away from the neighborhood. There are less houses, properties for the city to purchase and demolish</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>for road construction. The curve preserves the historical design philosophy and intensions of the original designer.</p> <p>Please incorporate/consider some these benefits for the State Blvd road construction project. Keep Fort Wayne a beautiful place to live.</p>	
27	<p>Colin and Jennafer Graham 2401 Oakridge Road Fort Wayne, Indiana</p> <p>July 8, 2014 (e-mail)</p>	<p>I live with my family at 2401 Oakridge Road, part of the Brookview/Irvington Park Historic District, and wish to voice my strong opposition to the proposed State Boulevard Alternative Concept. We will be directly affected by this project and I have seen the City's plan and feel it is much better. The congestion and confusion of the roundabouts will cause issues with traffic flow and convenience for my family and general population. As a former archaeologist and historian with IPFW, I fully understand the 106 process and the importance of historic preservation; however, I strongly disagree with the alternative plan and do not feel it adds any benefit to our historic district and general area. I hope that all parties will accept the original City proposal and move forward quickly with this project.</p>	No response.
28	<p>Jon Slate 215 West State Boulevard Fort Wayne, Indiana 46808</p> <p>July 18, 2014 (e-mail Hearing Comment Sheet)</p>	<p>My first concern is there would be equal access to the businesses as there is currently during construction and that it would be in writing. There should be some form of compensation during construction for any loss of business.</p> <p>My second concern is that the proposed end result that shows a picture viewing west on State toward my property not only blocks my current sign and approximately half of my 35 foot canopy with trees in the small grass medium of which the tree are completely not necessary.</p> <p>My third concern would be the ease of turning left as cars are coming from the east into my parking lot. With this in mind I feel that this is a very dangerous situation for cars waiting to turn left into my parking lot but do not want a median put in the middle.</p> <p>I feel that making this 4 lanes that kids going home from North Side H.S. that cross nearly anywhere is creating a very dangerous situation. I see students crossing the street everywhere and making the street twice as wide will cause problems of people getting hit!</p> <p>The Spy Run Creek bridge needs to be redone but that should be it.</p>	<p>Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One (1) travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.</p> <p>The renderings depicting the completed construction of the State Boulevard project are conceptual. While it is anticipated that the medians will be landscaped the final treatment has not been determined. As the landscaping plan is developed though the final design process the impact vegetation could have on the surrounding area will be considered.</p> <p>Vehicles accessing the property by turning left on State Blvd. from the east would utilize the inside traffic lane to turn left. Through traffic would bypass the stopped vehicle by utilizing the outside travel lane of State Blvd. A center median is not currently designed in this area.</p> <p>Pedestrian facilities including a 5 ft wide sidewalk, 8' wide sidewalk, pedestrian bridge over State Boulevard, and designated cross walks at signalized intersections will provide</p>

			safe crossing locations for pedestrian traffic.
29	<p>Benedict Moore 2335 Oakridge Road Fort Wayne, Indiana 46805</p> <p>July 4, 2014 (e-mail)</p>	<p>I fully support the project as outlined in the June 18 presentation at North Side High School. I have lived on Oakridge Road for 16 years. I am proud and lucky to reside in this neighborhood. Initially, my partner and I were renters here, and we moved into our house simply because we could afford the rent and the location was convenient for our jobs after two years, when our former landlord made the decision to sell the house, we were thrilled to buy it, having fallen in love with the neighborhood. We fully intend to remain here into our retirements.</p> <p>I believe this reconstruction plan will be a great improvement to traffic that runs through our neighborhood, and do not agree with those who think it will negatively affect property values and the beautiful character of the neighborhood. On the contrary, the planners who have developed this project have taken great pains to simultaneously improve traffic flow while maintaining a park-like feel. For property owners on the south side of East State Blvd and also on Eastbrook Drive south of State Blvd., they can finally have relief. They have suffered greatly with flooding, and now find themselves stuck with properties that really aren't habitable but that nobody will ever buy. Historic groups' attempt to save those structures, while laudable, doesn't make sense. None of those houses is historically significant or unique, and it's time to let them go, allowing the owners to get out from under the financial and emotional distress of being saddled with a house that they won't be able to sell or use.</p> <p>The plan proposed by ARCH would be a disaster. A roundabout at State Blvd and Clinton Street, which is one of the busiest intersections on the north side, is ludicrous! I believe ARCH's plan will create more traffic back-ups in our neighborhood than what we currently experience.</p> <p>Please include my comments as part of the public comments related to the hearing process for this project.</p>	No response.
30	<p>Karl Dietsch 2313 Oakridge Road Fort Wayne, Indiana 46805</p> <p>July 3, 2014 (e-mail)</p>	<p>Yesterday (7-2-2014), I witnessed a wreck at State Blvd and Westbrook. The majority of the Brookview Neighborhood residents want the new route to be straightened for increased safety at intersections getting on and off. The City's Plan does this by straightening the new route and going 4 lanes instead of 2.</p> <p>Enclosures (see Written Comments Page 64)</p>	No response.
31	Sara Kruger Geyman	After the recent unveiling of the alternative plan for the State	The Consulting Parties Proposed Alternative

	<p>July 2, 2014 (e-mail)</p>	<p>Blvd./Brookview project, there was a very very positive attitude – even joy! When the SKA representation shared the view, the ideas, the problem solving and overall effect that this alternative plan would have. It is amazing! And anyone can see that this is a better approach. It is truly thoughtful and beautiful design; properly scaled and one that considers and incorporates safety, flooding, preservation, the environment, history, people, activity, traffic, and the future. Any sensible person can see that it is better in every way, and was highly embraced by the public when unveiled.</p> <p>Fort Wayne's plan has always been, and continues to be, out of scale, inappropriate, costly, ugly, and devastation on many many levels. You, as engineers, certainly you are able to recognize that the alternative plan is a refreshing and forward thinking design, and viable in every way. We understand very clearly that the Federal arm of this project can out a halt on FW's current plan. As a tax payer and area resident, I am requesting – even demanding – that you do just that.</p> <p>Please, halt the process, halt the money, be professional enough to recognize a better way; and put time, energy, and money into doing it right . . . right for those of us who live here, play here, and of course, pay lots of taxes that fund these projects. We want our money spent well; we want our neighborhoods and the people in them respected, and we want the city overall to thrive. Which will only happen when the current plan is stopped, and the alternative plan is put in place.</p>	<p>(CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
32	<p>Karen Schwichtenberg Purple Blaze Enterprise, LLC</p> <p>July 2, 2014 (e-mail)</p>	<p>I am a resident of the Northside Neighborhood in Fort Wayne, Indiana. I also own a small business that operates on the northeast side of Fort Wayne. I have some very grave concerns about the City of Fort Wayne's project proposal to realign State Blvd.</p> <p>I do not want to see a five lane thoroughfare going through our historic neighborhoods of Brookside and Irvington. I believe that this type of avenue in a quiet, historic, park-like residential neighborhood will destroy it! I have great fears that straightening the curve in the road and adding 3 lanes of traffic will greatly increase the amount of traffic and the speed that it travels. This will impact very negatively the adjoining neighborhoods of East State Village, where many pedestrians and bicyclists frequent the stores there and the library. There are also several schools and parks in the neighborhood that have a lot of foot traffic. The speed and intensity of the traffic in these areas will increase the danger and potential for accidents.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>I attended a public meeting on June 30 where ARCH, Inc. presented an alternative proposal for improving State Blvd. It involves less than half the amount of roadway, so it will not increase the issue of flooding in the area the way the City proposal does, with its greater roadway surface. The ARCH proposal includes two roundabouts, that will keep traffic moving, but at a much slower, safer speed. Their proposal does not impact the integrity of the historic neighborhood to nearly the degree the City one does. The ARCH design has the bicycle and pedestrian trails off the main roadway to keep this activity separate and safe.</p> <p>I am in favor of the ARCH, Inc., proposal to revise State Boulevard.</p>	
33	<p>John Kurdziel 3450 River Forest Drive Fort Wayne, Indiana 46805</p> <p>July 1, 2014 (e-mail)</p>	<p>I have reviewed the proposed plan for the reconstruction of State Blvd. in Fort Wayne and find it lacking in some major engineering aspects. The City proposal raises the elevation of State Street over the Spy Run Creek and thereby introduces a major new design problem to this already troubled route, vertical alignment sight distance for the entrances for Westbrook Dr. and the revised Oakridge Road entrances. We own property off of Westbrook Dr. and it is already difficult and dangerous to enter onto State Blvd. due to problems with the horizontal alignment and the associated minimal sight distance it provides. Your proposal to raise the road, increase the number of lanes, straighten the roadway defacto increasing the associated speed and introducing an obviously new issue for vertical alignment sight distance that does not currently exist only makes this situation worse. As a minimum, traffic signals need to be included at the intersection of Oakridge Rd. and Westbrook Dr. to allow for the safe entrances to State Blvd. off of these side streets. Failure to acknowledge this obvious design flaw could open the City up to lawsuits in the future which no one wants or can afford.</p> <p>On the other hand, a proposal was made yesterday, June 30, 2014, at a community meeting hosted by ARCH, Inc. (Storrow, Kinsella Associates & Transportation Solutions, LLC) which provided much more comprehensive approach to this problem. They propose two traffic circles, one at the intersection of Clinton St. and State Blvd. and the other at Westbrook Dr. and State Blvd. that not only corrected the alignment issues with State Blvd. but provided a much more efficient and safe means of moving traffic through this area.</p> <p>This proposal is also much more beneficial to the community as it</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p> <p>Vertical sight distance for the project will be designed per AASHTO and Indianan Design manual requirements.</p> <p>It is not currently planned to install traffic signals at Oakridge Road and Westbrook Drive. The intersections will remain stop controlled as the current conditions. If future traffic warrants are met, the installation of signalized intersections would be considered.</p>

		<p>maintains the integrity of the historic neighborhood, and one could argue, actually enhances it and the ascetics of Fort Wayne as a City.</p> <p>We all want to increase the desirability of living in the City and be proud of what the City has to offer. The Headwater's Park was an example of what can be done when an engineering need is combined with improvement in the quality of life. We have an opportunity to do the same with this project, let's not waste it.</p> <p>Thank you for your consideration of my concerns.</p>	
34	<p>Sara Kruger</p> <p>June 18, 2014 (e-mial)</p>	<p>I have a family matter that has called me out of town today. I unfortunately am unable to attend tonight's meeting,</p> <p>With regard to tonight's hearing, I would like you all to know that as a sensible taxpayer, I am in very strong support of the alternate plan for State/Brookview; as it is better in every way. It is more sensible and viable financially, humanly, historically, visually, and environmentally.</p> <p>My vote is cast for the sensible, viable, alternative plan.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
35	<p>Suzanne Slick</p> <p>3318 Garland Avenue</p> <p>Fort Wayne, Indiana</p> <p>July 18, 2014 (e-mail)</p>	<p>The City of Fort Wayne is planning to place a multi-lane arterial through a small residential neighborhood in historic Brookview-Irvington Park. A representative from the MPO, NIRCC, told the public (Fort Wayne Northwest Area Partnership meeting, 18 October, 2012) that this project was being done to, "improve the neighborhood". Now that we all have read the long delayed, and carefully "tweaked" (http://www.news-sentinel.com/apps/pbcs.dll/article?AID=/20140415/NEWS/140419801/0/SEARCH) EA, we know that the City's preferred design will adversely impact the Brookview-Irvington Park Historic District, the Park & Boulevard System, and, of course, the aged bridge. The adverse impact is in the bifurcation of the neighborhood, the removal of the Kessler and Shurcliff design elements, the wide and fast traffic arterial through the residential district, and in the "relocation" of 15+ homes.</p> <p>The City of Fort Wayne has been rigidly unyielding in the scale of this project and has dismissed pleas from citizens to reduce the massive, intrusive footprint of the planned roadway and elevated bridge. There has not been adequate explanation for the inability to design a road more suitable to the neighborhood -- even though, the project is being done to "improve the neighborhood". Many citizens feel there is more to the story, that this is a piece of the greater regional transportation plan to increase</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>capacity, and that this project will not be the end of the widening, straightening and enlarging of State Blvd as a main east-west arterial. If it were, indeed, to “improve the neighborhood”, would it not have been scaled down as citizens have requested? And would not the process have been less difficult and contentious?</p> <p>Mitigation efforts discussed to date will do little to improve the quality of life issues and reduced property values resulting from this massive arterial bifurcating our intimate, historic residential district. The minimal efforts described will do nothing to preserve the segment of the historic Kessler parkway when it is transformed into what Kurt Culbertson, Kessler biographer, referred to as a “traffic sewer” during an outstanding recent presentation in Fort Wayne on George Kessler’s life and work. He expressed great concern that Fort Wayne will do what other cities now regret doing in obliterating their own Kessler legacies by transforming them into massive, bland arterials with the sole purpose of moving goods and people quickly through their environment. Many residents of this district and surrounding districts feel that our Kessler assets and Brookview are being “thrown under the bus” for this “traffic sewer”, and Brookview will not be the last neighborhood sacrificed.</p> <p>Yet, there is another alternative not included in the City’s proposal -- a prudent and feasible alternative -- that will have a much diminished impact on these threatened historic elements. Fort Wayne ARCH, Indiana Landmarks and the Friends of the Parks of Allen County commissioned SKA, Storrow Kinsella Associates of Indianapolis (http://www.storrowkinsella.com), to design an alternative concept for State Blvd.</p> <p>The SKA concept embraces and employs many of the ideals espoused in the NIRCC 2035 Plan (http://www.nircc.com/user/image/2035planfinal.pdf) while respecting the historic districts and their assets. It minimizes disruption to the residential neighborhood by moving the roadway south of the residential district, in doing so, the homes slated for “relocation” are spared, and the Kessler-designed parkway is improved. The land selected by SKA for the arterial portion of the project utilizes the portion of the historic district from which homes were already “relocated” by the flood removal program that preceded the transportation project.</p> <p>It incorporates progressive traffic design elements -- roundabouts -- that</p>	
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		<p>expedite regulated traffic flow thereby improving air quality by reducing idle emissions, ozone and particulate matter. By employing roundabouts it also reduces more serious collisions that occur at signalized intersections. Roundabouts are now being celebrated by Fort Wayne City Engineers and Public Works -- one is under construction currently at Ewing/Superior in Fort Wayne!</p> <p>The Storrow-Kinsella concept:</p> <ul style="list-style-type: none"> • Is calculated to provide the required and projected LOS (level of service) for traffic movement, • Was designed with sensitivity to safety concerns and adverse impacts to residential areas, • Mitigates air quality problems by reducing vehicle emissions through intersection improvements and constructing roundabouts at appropriate locations, • Serves to protect our urban area and residential neighborhoods from the adverse impacts associated with truck traffic, • Reduces vehicle hours of delay, • Supports energy conservation, protection of the environment and quality of life, • Scales-down widening projects, as it would add a third lane for left-turning traffic instead of widening to add through-lanes, • Implements appropriate "complete street" concepts to provide safe bicycle and pedestrian facilities, • Reduces costs when reductions in funding sources is a reality -- fewer lanes, less concrete, less complex construction, • Provides a design that will be less costly to maintain, • Contributes fewer hard surfaces than the City-preferred plan thus offering a superior flood management approach, • Achieves an efficient and safe transportation system for the movement of people and goods while simultaneously improving the economic and environmental conditions of Brookview-Irvington Park. <p>The Storrow Kinsella alternative for State Blvd. embraces and supports the core concepts described in the NIRCC "roadmap" for our transportation future. It is superior to the adversely impacting City preferred design. It would, unlike the City's approach, actually improve the neighborhood's cohesion by:</p> <ul style="list-style-type: none"> • Providing a context sensitive solution -- the arterial portion of the roadway removed from the historic residential district, • Preserving the historical assets -- and improving the Kessler-designed portion of State Blvd through Brookview, 	
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		<ul style="list-style-type: none"> • Providing safer, more comfortable travel for pedestrians and bicyclists -- again, removing these elements from the arterial portion of roadway, • While also calming the traffic in the arterial, itself. <p>Property values along the historic State Blvd, should, in fact, increase, as the neglected blocks are improved and restored to a quiet, residential street. It is prudent and feasible and must be considered. It is a progressive design that Fort Wayne will point to with pride. It will, however, require real compromise to achieve.</p>	
36	<p>Peggy Brady 2314 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>July 16, 2014 (e-mail)</p>	<p>We have lived 3 houses from State Street for 38 years. We have been through the floods of 1978 and 1982. The backup tsunami of June 2012 (when the rivers were low) is the first time we had flood damage in our basement. We have seen the many accidents that occur because of the State Street curve as well as the back flooding when flood water is unable to flow under the State Street bridge due to debris that gets stuck. I have seen the unacceptable conditions of the homes south of State Street that had been promised a buyout.</p> <p>I have not gotten involved in the Brookview Association because I am already involved in more than I can handle. However, I did attend 1 meeting about the Westbrook rain garden (which looks like a scary weed garden by the way) and 3 meetings over this State Street project. I am ashamed that my association has stalled the city's project for so many years. I am angry that a few plus others who do not even live in the neighborhood have been able to accomplish these stall tactics. I do not agree with any of their objectives. I will be attending my first Brookview meeting tonight. I completed the survey Michelle Wedeman mentioned in her meeting's flyer. Not sure who designed the survey questions; however, some were quite biased toward Michelle's views. If only this much attention, time, & money would have been spent on fixing the problem, with flooding. Tonight should be interesting.</p> <p>I believe the City's plan for the new State Street is great! I wish it could be done immediately and would not take yet another 4 years. I believe the City's plan will increase my property value. State Street needs to be straight. We already have traffic and four lanes at all intersections nearby. I do not believe semi truck drivers will choose State Street for their travel just because there are 2 more lanes between Cass and Clinton. I do not believe State Street will become Coliseum Blvd. Having a park-like area south of State may help make more space for flood waters.</p>	No response.

		<p>The City's plan will include the buyout of the 14 homes which have been damaged beyond fixing. These 14 homes have been the victims in this project and I feel especially sorry for what they have had to live through.</p> <p>When attending the City's meeting at Franke Park where we got to see pictures, I was astonished at the number of people complaining about the City's plan. Every time I asked which house was theirs, every single one of them said, "Oh, I live in Lakeside" or "I live in East State Village" or "I live on Forest Park" or "Oh, I don't live in Brookview". I would ask them, "Why are you here?". They would say, "I don't want State Street to become a Coliseum Blvd." or "More semi trucks will start going down State Street" or "It will let people go faster with 4 lanes". One actually asked me, "Don't you believe in the integrity of the neighborhood?". My response was "Are you kidding me? You don't even live here". I am still very angry that all of these people who are not affected and do not live anywhere close are causing the City all of these headaches. I am angry that this has been stalled and the stall tactics continue. Hopefully tonight's meeting will end this insanity.</p> <p>As for the Alternative Plan, it is ABSURD!</p> <ol style="list-style-type: none"> 1) NO ACCESS to and from the WEST for Eastbrook, Oakridge, Terrace without going way around and out of the way thru 2 incredibly insane roundabouts just to go to the grocery or a drugstore 1 block away! 2) Roundabouts on a state highway would be incredibly unsafe. 3) Compressing 3 lanes on a state highway down to 1 or 2 to continue south thru a roundabout is ridiculous. 4) Keeping a 2 lane road does not solve the problems of traffic congestion and yet another curve for more accidents. 5) Placing a 2 lane curved road thru the park-like area south of state defeats the purpose of more park space (especially if it helps with flooding). 6) Leaving the 14 homes as is - Oh my! What a shame! These poor folk don't want to stay; their homes are no longer worth fixing. Neighbors don't like seeing run-down homes. 7) Walkers to high school going across a roundabout would cause a few more accidents, even if there is a stoplight. 8) Stoplight in a roundabout for 2 roads even sounds more confusing. 9) A roundabout with 5 entry points at a busy intersection is more than ridiculous. 10) Having to buyout all 4 businesses at Clinton and State has to cost a LOT MORE; neighbors probably don't want to see the BP/convenience store go; we would want BP to stay. 	
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		<p>So many more objections to this Alternative Plan. I'm angry you have to spend all this time going through these "required" motions to stop the Association from any further blocking of the City's Plan.</p> <p>A few things I am curious about:</p> <ol style="list-style-type: none"> 1) How many people that attended the meeting at North Side HS actually lived in Brookview. 2) How many Brookview residents actually support Michelle's actions. 3) How many Brookview residents support the City's Plan. <p>Just an FYI: When my new neighbors moved in, Michelle was knocking at their door while they were still unpacking. They did not know anything about this project. But they signed Michelle's petition because they felt like she wasn't going to leave until they signed it. Most every neighbor that I know that lives close to State Street supports the City's Plan.</p>	
37	<p>Dale Bender 2402 Eastbrook Drive Fort Wayne, Indiana 46805</p> <p>July 18, 2014 (e-mail)</p>	<p>I am writing in response to the June 30th public hearing, at Psi Ote Upper pavilion at Northside Park. At this meeting Michael Galbraith , the executive director of the city's chief historic preservation organization, revealed that ARCH had hired American Structurepoint (an engineering firm) to research an alternative to the City's proposed reconstruction of State Boulevard between Cass Street , and , Clinton Street. The plan was developed by a traffic engineer, and, a landscape specialist.</p> <p>After having been in attendance at aforementioned hearing, heard the plan proposal, and studied the sketch plan of the proposal, I can say that I very much favor this alternative plan over the City's proposed plan through our historic neighborhood.</p> <p>The reasons for which I favor ARCH'S Alternative Plan :</p> <ol style="list-style-type: none"> 1. it's inherent beauty , and , simplicity of design 2. ease of traffic flow 3. no residential intersections from neighborhood (Eastbrook Dr. , Oakridge , and Terrace Rd.) to the proposed section from Westbrook Dr. to Clinton Street is a good thing. The City's plan would have residents turning west onto State Boulevard across 5 lanes of traffic, from a bottleneck at Oakridge 4. the 2 lane proposal will have less runoff to Spy run Creek during rain events 5. Even during electrical black-outs , roundabouts work well 6. Land under demolished homes, bought out in the worst flood 	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>prone areas of Eastbrook Drive, can be utilized as flood control overflow. And , the sooner the better</p> <p>7. Historic State Boulevard can still be used by residents for easy access to the new roadway; and, with enhanced bicycling, walking, running activities</p> <p>Amendments to the ARCH Alternative Plan</p> <ol style="list-style-type: none"> 1. There are two optional bypass lanes, near the State/Clinton roundabout, shown as #5 on the Sketch Plan. One is north of the new two lane, and, the other is south of the new two lane. I propose the south optional bypass, only, be incorporated into the plan. This would speed flow of easterly, and southerly headed traffic with less backup at the roundabout of vehicles traveling from the west. Eliminating the optional bypass lane on the north of the new two lane would discourage speeders from traveling through the Brookview Neighborhood (usually along Eastbrook Drive) to travel west on State Boulevard. 2. for the Clinton Street/State Boulevard Roundabout , I highly recommend the best “ smart “ roundabout signal available for enhancing the flow of traffic in the roundabout, and, for southbound traffic safety on Clinton Street. 3. Design into the plan a way for First Responders to easily pass through traffic waiting their turn through the Clinton Street/State Boulevard roundabout. 4. Raise the new multi-use bike/ped path bridge, over Spy Run Creek, to accommodate flood events ... a helical ramp on the east side of the creek, perhaps. <p>Thank you for allowing me to register my humble, though much considered, opinions.</p>	
38	<p>Karl Dietsch 2313 Oakridge Road Fort Wayne, Indiana 46805</p> <p>July 2, 2014 (e-mail)</p>	<p>State Boulevard, Alternate Study Unveiled on 06-30-2014</p> <p>I attended the State Boulevard Alternate Study open showing on 06-30-2014. This study claims to be a result of input from the neighborhood to the project but this is not true. The neighborhood majority want the City's Plan as explained and shown for the past 6+ years. The Alternate Study has no input from us. It has been done in secret from us. No neighborhood meetings have been held in the past 18 months. Its first exposure to us was the evening of 06-30-2014. This is after the final open hearings on the City's Plan held on 06-18-2014. The Alternate Study is dated 12-10-2013, more than 6 months before its showing and</p>	No response.

		<p>yet no exposure was made to the neighborhood residents.</p> <p>A roundabout in the Alternate Study at Clinton and State decreases safety slows traffic to stop and go, yields to the left on entry and forces 3 one way Clinton traffic lanes to one lane exit capacity. This roundabout would not handle current and future capacity needs. The longer curvy 2 lane route in the Alternate Study decreases safety, capacity and does environmental damage to existing park land. The City's Plan removes flood damaged homes in its straighter path staying out of existing park land.</p> <p>The development of this Alternate Study is the work and influence of Councilman John Shoaff involving Arch of Fort Wayne (of which he is a past president of the board of directors) and Indiana Landmarks (of which his wife is a director). The Alternate Study is only the latest delaying attempt on the State Boulevard project by a few.</p> <p>The findings of the Alternate Study state many false conclusions. The most obvious is the statement "The CPPA has the support of the affected neighborhoods". The Northwest Quadrant of Fort Wayne Neighborhood Leaders support the City's Plan 36 to 6, a ratio of 6:1. The first 9 homes on Oakridge Road and remaining 4 homes on the north side of State after the project is completed all support the City's Plan. The 14 homes to be removed (all flood damaged after having basements filled with 1 to 8 feet of water just last year alone) all want to sell out to the City. Residents on Oakridge Road are in favor of the City's plan by a 3:1 ratio. Residents on Terrace Road are 2:1 in favor.</p> <p>The four goals accomplished in the City's Plan are:</p> <ol style="list-style-type: none"> 1) Increase safety with a shorter straighter route and an increase of lanes from 2 to 4 2) Increase traffic capacity for current and future needs by increasing lanes from 2 to 4. 3) Decrease environment impact with shorter straighter route after removing flood damaged homes. 4) Raise the roadbed to an elevation above most creek flash flooding and longer term river floods. <p>It's time to go with the Win-Win design of the City.</p>	
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39	<p>Mark Handy 2335 Oakridge Road Fort Wayne, Indiana</p> <p>July 3, 2014 (e-mail)</p>	<p>As a resident of Oakridge Rd and the Brookview-Irvington Park Historic District, I approve of the City's Sate Blvd reconstruction plan. The City's plan reduces traffic snarls, ensures a safer, quieter neighborhood, and provides a reliable city route for the future.</p> <p>I find it interesting that the greatest opposition to the City's plan comes from people who live outside the neighborhood, people who do not wait 5-10 minutes every morning to make a turn onto State, only to sit another 5-10 minutes in traffic waitinbrbreng to move through the intersections, people who refuse to believe that State Blvd has evolved into an east-west traffic artery. They are also people who do not have standing water on their property every time State Blvd floods after more than 5 minutes of heavy rainfall.</p> <p>At the June 18th meeting at Northside High School to discuss the City plan, Councilman John Shoaff's objections were:</p> <ul style="list-style-type: none"> • That the City plan is "a 20th Century dinosaur" dragged into modern times • That the plan is "over-engineered" • That the new widened street would drive down property values. • That no alternatives were presented or outside opinions were sought during the design process <p>What the Councilman fails to see is that State Blvd has evolved into a major route through the city, and that currently State Blvd is a "dinosaur" carrying more traffic than it was intended to do and needs to be upgraded to deal with the new reality of increasing traffic growth. As an architect, Councilman Shoaff should understand the need to "over-engineer" in order to provide construction stability for the future. His argument about property values seems to defy logic: the City plan moves the roadway a further 190 feet from most residences, providing a safer, quieter neighborhood while preserving much of the original historic neighborhood design, making the existing structures more desirable and valuable. And, during the process, 4 alternative routes were discussed, 51 meetings were held regarding the City's proposal, and a quick perusal of the 894 pages of documentation show that since at least to 2009 Councilman Shoaff, Arch, Indiana Landmarks, and other organizations have been involved in the process, constantly employing delaying tactics to drag the process out as long as possible.</p> <p>The plan presented by ARCH on June 30th fails to realize the reality of</p>	No response.
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		<p>the changing traffic conditions in the city. By adding two roundabouts, the plan ensures continued traffic snarls during commute hours. By replacing the intersection at State Blvd and Clinton St, ARCH's plan effectively cuts three south-bound lanes of traffic into one, as well as the current east- and west-bound lanes, effectively blocking a major city artery. ARCH also seeks to preserve houses along State Blvd which have little to no historic value (all but one of the 13 homeowners signed a petition in favor of the proposed buyout of their properties), and routes the new roadway through a greater area of Vesey Park, eliminating yet more greenspace in the city.</p> <p>A very quick online search also reveals that State Blvd has been removed from Indiana Landmark's 2014 10 Most Endangered list, and that none of the affected houses are significant structures as listed by Indiana Landmarks.</p> <p>Brookview-Irvington is an important example of city planning theories from the City Beautiful movement of the 1910s. Walter Hoxie Hillary and Arthur Shurcliff, the designers of the neighborhood, were innovators employing the most current planning techniques of their era. The City plan keeps that tradition of innovation alive. Mr. Hillary and Mr. Shurcliff would approve of the City's plan.</p>	
40	<p>Todd Zeiger Indiana Landmarks July 18, 2014 (letter)</p>	<p>We are submitting herein comments pertaining to the June 4, 2014 updated FHWA Findings and Determinations provided to the consulting parties as part of the Section 106 review and the May 2, 2014 Environmental Assessment for the proposed State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana (Des. No. 0400587, DHPA No. 5903, Project No. IN20071404). Specifically:</p> <ol style="list-style-type: none"> 1) We concur that a finding of Adverse Effect is in order as the project is currently designed. Under Section 106 and Section 4 (f) the project as designed will have an Adverse Impact to three historic properties – the Bridge carrying State Boulevard over Spy Run Creek, The Brookview- Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. 2) As identified in 36 CFR 800.1(a), we concur with the other consulting parties that the Findings and Determinations materials provided on June 5, 2014 adequately identify the historic properties, and assesses effects of the project as currently proposed. 3) As identified in 36 CFR 800.1(a) we do not find that the goal of 	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>avoiding, minimizing or mitigating adverse effects on historic properties has been met and is wholly inadequate. To address the inadequacy ARCH, Indiana Landmarks and Friends of the Parks along with the Brookview-Irvington Park Neighborhood Association commissioned Storrow Kinsella Associates and Transportation Solutions to examine the background research developed for the project to see if there was an alternative design for the project that better protected the historic assets outlined in #1 above while fulfilling the purposed and need for the project. The design engineers were also tasked to ensure their proposed alternative was prudent and feasible and avoided, minimized or mitigated the adverse effect to the neighborhood. The alternative plan (Alternative) that has been developed better addresses historic preservation concerns while accommodating the purpose and need of the Federal Undertaking. Here is a link to a permanent DropBox Folder where you can pull up the alternative directly: https://www.dropbox.com/sh/21xbxj8snangzji/AADLTtOKBej_U5fSLDnhrn9_a</p> <p>We are aware that the plan was presented to American Structurepoint by ARCH at the June 18, 2014 Public Hearing for the Environmental Assessment. Further ARCH provided the alternative plan to American Structurepoint, FHWA, INDOT and the Indiana Division of Historic Preservation and Archaeology by email on June 24, 2014 and June 26, 2014.</p> <p>4) We are submitting the Alternative as part of our comments under the Section 106 Review and EA comment process. The Alternative must be considered as governed by the guidance in 36CFR 800.1(c). This guidance directs that the Alternative must undergo unrestricted consideration by the agency and further states that the agency official complete the Section 106 process in a manner “that such actions do not restrict the subsequent consideration of alternatives to avoid, minimize or mitigate the undertaking’s adverse effects on historic properties.”</p> <p>5) We are submitting this Alternative as it is in agreement with direction contained in 36 CFR 800.3(b) which directs the agency official to coordinate the Section 106 Review with “...the overall planning schedule for the undertaking and with any reviews required under other authorities...” including NEPA and Section 4 (f) of the Department of Transportation Act. We further believe that the guidance contained in 36 CFR 800.8(a)(1) encouraging agencies to “consider their section 106 responsibilities as early as possible in the NEPA process...” provides further impetus to</p>	
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		<p>consider the Alternative plan under both the 4(f) and NEPA process.</p> <p>6) We believe that the Alternative meets the adopted purpose of the project by improving corridor connectivity for both motorists and pedestrians. It also addresses the problems of congestion substandard sight distance, and geometrics. Roadway flooding as well is addressed, although this remains a concern for both the currently proposed project and the Alternative. The nearly adjacent and recently completed US 27 Bridge Reconstruction project (Des. No. 0200914 and 0101527) experienced roadway flooding approximately a year after the project's completion – demonstrating the need for a thorough investigation and remediation of a larger flooding problem in the area.</p> <p>7) A key for our support of the Alternative is the significant reduction of impact to the identified historic resources noted in #1 above. The Alternative preserves the original curve of State Boulevard and preserves the connectivity of the adjacent residential streets. It does not introduce a foreign and oversized transportation facility adjacent to the historic districts. It preserves the historic homes within the path of the proposed project. The Alternative makes a “special effort to preserve....historic sites” and “minimizes, mitigates or avoids” Adverse Effects to the identified resources.</p> <p>As outlined in the Adverse Effect findings on pages 206-209 of the Environmental Assessment, The Alternative proposal results in far less damage under both Sections 106 and 4(f), making it a prudent and feasible alternative that causes the least overall harm. In the Fort Wayne Park and Boulevard System Historic district, and in the Brookview-Irvington Historic District, the Alternative avoids to a much greater degree the removal and disruption of the National Register Listed State Boulevard, minimizes the amount of new land disruption by using land previously altered through the removal of the “floodbuyout” houses, and retains the plan of Arthur Shurcliff-designated plat. In addition, the Alternative avoids the demolition of National Register listed residences entirely. The Alternative mitigates the disruption and bifurcation of the Shurcliff designed plat by the use of a curvilinear “new” State Boulevard replicating the scale of the “original” State Boulevard, providing the driver and pedestrian with a similar feel, design and setting.</p> <p>8) We concur with the other consulting parties that the Draft Memorandum of Agreement (MOA) as presented in the May 2,</p>	
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		2014 Environmental Assessment and the June 5, 2014 Section 106 Review will need to be modified, particularly if the Alternative is adopted as the basis for proceeding forward in this project. We agree that the use of the Context Sensitive Solutions is preferred, but would prefer to see that any such Context Sensitive Solutions be implemented where feasible rather than merely "considered as is suggested in the Draft MOA.	
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Verbal Comments

Comment No.	Name / Organization / Comment Date	Comment	Response
1	John Shoaff	<p>Ok, I want to make sure you hear me clearly without any problems. Okay, can we get one of those things for our City Council? Casual reader of the environmental assessment...inaudible...might be misleading of the impression of public neighborhood meetings cited in the document gave the public ample opportunity for significant input. This is not the case. The primary features...inaudible....and so many of our citizens strenuously object to were dictated by a request for proposal was sent out November 2007. It directed successful respondent to engineer a plan that would widen the existing 2 lanes of State Boulevard between Clayton and Cass Streets to 5 lanes. Structurepoint designed, as instructed and the public has never been shown any other possibilities. Instead the idea has been promoted that there has only been one solution to the various problems and there are some real problems in this area. Part of this fallacy, this is a fallacy is that 5 lanes are essential to alleviate the problems. This plan with 5 lanes to pass the increased beyond reason designed ramp speeds to 35mph, a speed almost certainly fatal to a stuck pedestrian. It is throwback to an earlier time and type of thinking. A 20th century dinosaur is being dragged into the 21st century where conditions are very different than they were thirty or forty years ago. Conditions have changed, our visions have changed, our priorities have changed. Our street system is under severe financial pressure. We have fifty to sixty million dollar backlog on much needed street repairs. Partly, this is because we have been told by experts and we are learning for ourselves how are roads are overbuilt. Revenues and back taxes that pay for these repairs are down because driving is down it has been dropping nationally since the year 2007. So we are in a squeeze. We should be looking for solutions to traffic problems and use the minimum amount possible of paving, not the maximum. What we build has to be</p>	No response.

		<p>repaired or rebuilt in twenty or twenty five years or so for the second time around, we pay for all of it all eventually. The economic consequences are made worse by the impact on property values. Residential values in the area will go down. If you doubt it, ask the 14 neighborhood residents who signed the letter opposing the project in its present form. There is however a most significant partnering change in recent years to which we can respond. It is a counter migration back into the city. This is something we must encourage in every possible way. I spent the morning in a planning session for downtown renewal. There we discussed the importance of protecting the power neighborhoods and the northside neighborhood was specifically named. A project like this one creates a corridor that planners call a barrier, separating the neighborhoods from downtown that weakness and desirability of living in them. It should not be allowed to go forward in the present form. Thank you.</p>	
2	Darrell Henline	<p>To all who are in attendance, we are very much in favor of the proposed State Boulevard new construction project as stated in today's legal notice of public hearing and as the renderings reflect on the CityofFortWayne.org website. Classic Stereo has been located in the northeast corner of Clinton and State for 48 years. All of which the current owner, Phil Miller has been with the business. I've been with the company for over 36 years. Over these many years we have seen how the daily congestion, accidents, and flooding have adversely affect businesses and homeowners alike. With the addition of extra lanes in each direction and turn lanes as indicated, the design should smoothen the traffic flow to and through the intersections of State Boulevard and Spy Run and Clinton Street and other adjoining streets in the area. The dreaded morning and late afternoon-evening rushes should have shorter and fewer delay times at the traffic signals. The elimination of the snake curves and the installation of new street lighting should also provide healthier traffic conditions as motorists, bikers, and pedestrians are able to view the traffic clearer while entering or crossing State Boulevard. The project also appears to address in a positive manner, the flooding issues that have constantly bombarded the community. Adding the appeal of the new greeneries beautifies the driving through the area. We might also note that any discussion of cutting lanes, lane restrictions, or adding round-a-bouts would not address the needs and the goals that have been predetermined and deemed necessary in previous public hearings, as well as means help with the pertinent City departments. In fact, these ideas may actually be detrimental to and serve as an actual downgrade to current traffic and environmental</p>	No response.

		<p>conditions in the area. In conclusion, we are in desperate need of this proposal as stated in the legal notice of public hearing as the renderings reflect on the CityofFortWayne.org website. I might add that Phil Miller, president and owner of Classic Stereo and Video would like to formally apologize for not attending tonight's hearing. He had originally planned to participate in this discussion, but he's been working on a job site out of town the last few days and won't be able to be here tonight. Thank you for this public forum. Sincerely, Phil Miller, President, Owner of Classic Stereo, and Darrell Henline, Controller</p>	
3	<p>Laurie Weier 2526 West Drive Fort Wayne, Indiana 46805</p>	<p>My concern in all of this is for that people who live along State Street all the way west and all the way east. If it becomes so convenient to go so fast and get where you're going in a big hurry, there's going to be a lot more accidents, there will be a lot more children who may be riding bicycles, and all this traffic is going in such fast speeds. I would like to know if you're ever really going to control the traffic? The speed of the traffic? We already have a 35mph on East State near where I live in the East State Village area. It has never ever been monitored. I've never seen a car stopped for speed, so I guess what I'm talking mostly is the safety of the people who use the street and who live along the street. There are more important things than just getting where you want to go in two minutes. The quality of life around this street. State Street is a beautiful street. People live both east and west along the street and have loved it. I hope they will continue to do so, because we will take control of what's going on and those five lanes. Thank you.</p>	<p>No response.</p>
4	<p>Howie Walker 1012 Kenwood Avenue Fort Wayne, Indiana</p>	<p>I live in the northside area, but specifically in the area where some of you are effected by, so for that reason I just want to offer a couple of things as kind of an outsider viewpoint. Something to maybe think about. Maybe it's too late for it, but it seems that one of the main concerns is traffic and getting from point A to point B is always a big concern for everyone. Although as the lady just mentioned, there's more to life than just that too. It would be nice if we could have progress in Fort Wayne and at the same time not forget our history. One of the ideas that I'd like to propose at least if maybe you would consider an overpass that would allow traffic to move and especially emergency vehicles, things like that, and at the same token, allow the neighborhood to remain the neighborhood. Still have its historic routes, friendliness, and charm. And also, it could be tied in, I can't remember the name of the street, but basically goes northwest and the overpass could be tied into Five Points going off Goshen Road and give a lot of access. At the same time it would give a pedestrian and bike route a lot</p>	<p>The design of an elevated overpass in this urban situation would not be feasible due to technical challenges of maintaining access to neighborhoods and City streets.</p>

		<p>more availability in the neighborhood.</p> <p>Also, I see it as an opportunity that Fort Wayne could really tie some other projects together if we could get some kind of a high speed transportation system to help with the traffic flow and maybe tie it into the coliseum for people that work there. Give them access to where they can leave their cars out away from the City. We could turn down town into a nice green area, a lot of bikes, golf carts, and electric vehicles. And this would help create some revenue for the City. Because there could be tolls on those kind of vehicles, or renting bikes, or whatever the case is, it will certainly bring in more businesses downtown, and certainly, hopefully draw some more businesses too. And then also the river development. This is certainly going to affect that. We have some areas that are already pretty wide open, for example the old Kroger facility. Now there's a wide open parking lot there that could really host a lot of activities, art fairs, crafts, different festivals that would tie right in with the river venue. So State boarding tournaments, a lot of different things. I know that's not the issue tonight, but the same token, I hope that we can foresee in the future tying everything together in Fort Wayne and making our City a real City of progress. Change for the sake of change is not necessarily good change, but keeping everything the same does not allow for progress either. Thank you.</p>	
5	<p>Susan Haneline 134 East State Boulevard Fort Wayne, Indiana</p>	<p>I am actually a homeowner involved in the actual footprint of the project. Our house is in the 100 block of State, so this is a very personal topic for me. First of all I want to say that the vast majority of the residents living in that footprint support this project whole heartedly. We have come to every meeting. We have done everything that we can to make sure that everyone understands those of us that have a personal stake in this support it. If you don't believe there's a safety issue, I invite you to come sit on my front porch, hang out there, and you can see what a safety issue is. My daughter just turned 18 and is heading off to college in two months. She never in her life played in our front yard because it wasn't safe. You talk about speed limits, trucks, there's trucks that go down there all the time. It's a truck route for trucks that are making deliveries along State Boulevard. It always has been, It always will be. It's not safe in the matter that it is now. It's been 2-lane streets. The trucks come off up the street, they run over the sidewalks. Again, it's not safe. These are the things that we're trying to get people understand.</p>	No response.

		<p>Flooding – huge issue. I just dumped thousands and thousands and thousands of dollars into my property several months ago due to flooding. This will allow, these improvements are going to allow the water to move better through the area. It's going to make traffic through the area better. It's going to make it safer. You talk about environmental issues. Those of us that live there, that have to pay to have our homes professionally cleaned up after flooding can talk to you about environmental impact. The water is contaminated. We can't live in our homes until they are properly cleaned up. We pay extra money for flood insurance. It's a hardship for those of us living there. This is not a case of big Government coming in and saying we want to take these people's property. This is a case of people saying, please come in and help us. We support this project. We love the idea of more green space in the neighborhood. We love the idea of having our children to be able to play away from the main access of the road. The road itself doesn't particularly change the historic curb remains as what we consider old state or whatever you want to call it. You're backing the traffic away from the neighborhood. You're backing away from where the children play, the people walk, the people bicycle. Again I thank you for your time, and again know that there are many of us out here that attend these every single time, supporting and asking for speedy resolutions to this problem. Thank you.</p>	
6	<p>Mark Hefty – 2702 Florida Drive Fort Wayne, Indiana</p>	<p>I grew up at 2327 Oak Ridge Road. I used to rake leaves for Mr. Sickles as a kid. I believe the bridge project is needed, but I don't want anyone in the room to believe for one minute that it's going to alleviate the flooding problem. I really got upset when the zoo flooded last year. And the reason that happened when they raised the dykes years ago two feet, the one spot they didn't raise was the Lawton Park Dam and put in a pumping station big enough to accommodate all the new development up Highway 3 and the Tributary Ditch going up Bass Road to Thomas Road addition. Without that the homeowners on the north side of Eastbrook and Westbrook can expect continued flooding. Twice in my lifetime did water cross State Street from 1962 when I was born until I left for the service in 1983. I come back and it's a weekly occurrence, which the water it doesn't flow towards the rivers, it's backing up. And we finally flooded out the crown jewel of Fort Wayne, the Zoo that actually had to close last year. I think that's wrong. That problem with the Lawton Park Dam needs to be addressed, and a pumping station needs to be put in. But I do believe the State Street Bridge project needs to go forward. That's all I have to say.</p>	<p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>

7	<p>Mark Andrews 2227 Westbrook Drive Fort Wayne, Indiana</p>	<p>I'm right in the center of this also, and those of you that live in our neighborhood remember that a year ago we had a little rain and that we had quite a flood. The Clinton Street bridge was closed, all of Clinton Street was closed, so we can talk about it all we want but the flood program like Mark Hefty said it does all backup, now we have Pasco and Kelly Chevrolet clear out north, so this project really won't do that much for flood control. We say all we want to do is just make sure State Street doesn't get closed. Although, us in the neighborhood will still have to put up with water. The other thing is they're not talking about in this meeting. I don't know if it doesn't apply to this</p>	<p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
8	<p>Michael Galbrath Executive Director of Architect</p> <p>(Submitted written copy of December 9, 2013 letter from Margaret T. Storrow Storrow Kinsella Associates addressed to Mr. Michael Galbraith – see Written Comments Page 4 -13).</p>	<p>I'd like to begin by acknowledging all of the people that put together this document. I'd like to thank American Structurepoint, the City of Fort Wayne, INDOT SHIPPO, and Federal Highways. It's a truly a massive document. It took hours and hours and hours of time. I'll reserve the bulk of my comments for our official written response, but I'd like to begin by saying a couple of different things.</p> <p>1. We agree that the project as proposed involve substantial controversy. Concerning community and natural resources. All of those viewpoints deserve to be heard. We agree that under Section 106 of the National Historic Preservation Act that there are adverse effects, but we disagree that there's no pertinent feasabel alternative under section 4F. We would like to present this alternative concept here onto American Structurepoint and ask that it be reviewed. Arch, along with Indiana Landmarks and a number of other consulting parties engage Storrow Kinsella, a landscape architecture firm, and Transportation Solutions, a traffic engineering firm, to review the current preferred alternative, and the purpose and need for the projects. We believe that this proposed alternative concept meets that stated purpose and need, is prudent, feasible, and results in an alternative meeting the least overall contest. As this meeting has a tightly scripted agenda, Arch will be holding our own Public Information meeting to further this concept on June 30th, at 7pm in the lower level of the Psi Ote Barn in Northside Park. Thank you.</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>
9	<p>Karl Dietsch 2313 Oakridge Road</p>	<p>Congratulations to all involved in the final plans of the State Boulevard project. This well thought out City plan to create a State Boulevard</p>	<p>No response.</p>

	<p>Fort Wayne, Indiana</p> <p>(written copy of verbal comment submitted at time of comment)</p>	<p>improvement between Wells Street and Spy Run have stood up to the test of time and logic. I appreciate City officials being cooperative and explaining the various design stages of this project in the past six plus years. Many hearings have been held for all to attend and offer constructive comments both verbally and in writing. As a resident of the Brookview neighborhood and living among the closest homes to and affected by the future State Boulevard project, I am aware of the strong approval to the City's plan by a majority of the neighbors. Approval waiting of the City's plan is 3 to 1 on Oakridge Road and 2 to 1 on Terrace Road. In the northwest quadrant of Fort Wayne 36 association leaders are in favor of the City's plan for a ratio of 6 to 1. The City has stated from the beginning that the State Boulevard project's purpose was to build a safer, high capacity, and elevated street, while replacing the Spy Run Creek Bridge. The final drawing presented here tonight is a result of all the input that has been made to the designers within the time limits all of us have followed. Not all inputs of mine and others have been included, but I accept this as the final decisions are not ours. The City's final design is great and fulfills the project's stated purpose. The environmental impact of the City's project is nil, due to the fact that all homes to be removed are already environmentally damaged from flood waters filling their basements. This makes the City's plan as drawn for the State Boulevard project a win-win for the homeowners, City, and all future users of State Boulevard. Let's build it. Thank you for letting me speak.</p>	
10	<p>Karl Dietsch 2313 Oakridge Road Fort Wayne, Indiana</p> <p>(written copy of verbal comment submitted at time of comment)</p>	<p>These are the words of a letter from John Modezjewski, Bud Mendenhall, and John Meinzen. Sirs, we have asked Karl Dietsch to speak for our neighborhoods at this meeting tonight, as we have a real neighborhood event that is taking place tonight. The Drug and Gang Awareness Night in Hamilton Park is a real neighborhood issue for many of the neighborhoods in the northwest area of Fort Wayne have come together as a group of concerned neighborhoods to stand against a real problem, not a manufactured issue that the State Boulevard tractors have been using to solve progress and construction of a much needed improvement to the northwest area in the City of Fort Wayne. We need the State Boulevard project to rebuild as designed by the City of Fort Wayne, and approved by the Citizens of Fort Wayne and all the meetings given by the City of Fort Wayne Board of Works and the Traffic Engineering Department. The project needs to be started as soon as possible. Stop the posturing of a monitory of citizens and the grand stain of our elected officials and let'ss get the project moving forward as designed. Thanks, John Modezjewski, President of North</p>	No response.

		Highlands Neighborhood Association and President of the Northwest Urban Development Coalition, Bud Mendenhall, President of Bloomingdale Neighborhood, Vice President of the Northwest Urban Development Coalition, John Meinzen, Spy Run Neighborhood Association, Founder and Leader of the Core Group. Thank you for your time.	
11	John Slate 215 West State Boulevard Fort Wayne, Indiana	I own X-spot Printing and I guess one of my biggest concerns is this gentleman this evening said that traffic would be open for both on State during the total project as I recall. I guess, I would just like to know a little further in detail on how he plans on doing this. Also, when there is a thing built across State there, how do you plan on having both lanes of traffic going at that time while you're constructing this? Those are a couple questions that I would like to have addressed and I hope that he can do that. Also, to get into the businesses, I guess, I would like that addressed, as to how there will be access to a business along there, such as mine. I've seen a lot of businesses that have nearly gone under because they've had to wait months and months where it took time to construct these things. So, I guess those are some of the issues that I would hope that this gentleman can address to us. Also, is there, can you give us a guarantee that once this is done, is the flooding issue totally going to be taken care of for all of these people out here. I think a lot of these people would like to know that. You know people that's got flooding issues now is there going to be 11.5 million dollars spent and then possibly six months after that there is still flooding going on. I don't think that we want to have that kind of an issue at that time. I just want some sort of figuring out how that's all going to happen and that it can be some sort of a guarantee. So, I guess those are the things that I would like to have addressed before the project goes further. Thank you very much.	<p>Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One (1) travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.</p> <p>During the installation of the prefabricated pedestrian bridge, State Blvd would need to be closed for a short duration while the bridge is set. It is anticipated that the closure would be limited to less than 3 days. Options for limiting the closure time and/or minimizing impacts to local business and residents will be further evaluated.</p> <p>From Clinton Street to Spy Run Avenue, 2-way traffic will be maintained on the existing westbound lanes of existing State Boulevard while the proposed east bound lanes are being constructed. Once the eastbound lanes are built, 2-way traffic will be maintained on the newly constructed eastbound lanes until the proposed west bound lanes are constructed.</p> <p>From Westbrook Drive to Clinton Street, 2-way traffic will be maintained on the existing roadway and bridge structure while the new alignment portions of the eastbound State Boulevard lanes and bridge structure are constructed to the south of the existing alignment. Once the eastbound portion of proposed State Boulevard is constructed, 2-way traffic will be maintained on the proposed eastbound lanes while the westbound lanes and remaining</p>

			<p>bridge structure are constructed.</p> <p>From Cass Street to Westbrook Drive, 2-way traffic will be maintained on the westbound lanes of existing State Boulevard while the eastbound lanes are being constructed. Temporary asphalt pavement widening may be required on the northern side of State Boulevard between Cass Street and Westbrook Drive to accommodate 2-way traffic. Once the proposed eastbound lanes are constructed, 2-way traffic will be maintained on the eastbound lanes while the westbound lanes are being constructed.</p> <p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. . However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
12	Herb Weier 2526 West Drive Fort Wayne, Indiana 46805	My concern not only with this project affecting Spy Run Creek area, I'm worried about what it's going to do to the area of East State on the other side of the St. Joe River. The lanes are not as wide as the lanes that are going to be produced over the new bridge. That's going to create a problem especially when you have big trucks start running down the street. The other thing is that I would like to know the reason why they designed the new road for 35mph but they're going to post it at 30. Well, you know what's going to happen? It's not going to be 30 mph, it's going to be 35 or more. I'm involved with the East State Village attending to the flower pots we have and you sort of take your like in your own hands when you try to maintain those pots with the car traffic, because they do create a 30mph speed limit. So, this was my number one main objection to what is going to happen elsewhere. The other	<p>The proposed project terminates at the western edge of the bridge over the St. Joseph River. The lane widths at the termination point of the project are 11ft, which appears to correlate closely with the lane widths east of the project limits.</p> <p>It is standard engineering practice to meet design criteria of 5 mph greater than the anticipated posted speed.</p> <p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with</p>

		<p>thing is with flood control there are not enough containment ponds up north of the Spy Run Creek. I worked up in that area for 30 years and I remember that whenever the rain would come the parking lot of the company that I worked for would just be pouring that water right into the Spy Run Creek. Therefore, I think that they did not address anything in regard to the, at least it wasn't evident tonight, that they address the containment ponds that should be installed in that area. Thank you.</p>	<p>Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. . However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>
13	Vivian Braun	<p>I've lived here for fifty years. But before that I came from Philadelphia. I know you've heard of it. It's a City of great history and we have two signs in our neighborhood designating us as a historic area. Have you all seen it? So, I have two things to comment on. How would you like it if they decided to put a McDonalds next to Independence Hall and how would you feel if the American flag hanging up there designed in Philadelphia by Betsy Ross suddenly became a house next to a 4-lane highway? You know, we are historic. We should keep it that way. Thank you.</p>	<p>No response.</p>
14	Julie Peebles	<p>I lived at the corner of State and Eastbrook for six years. I moved in September. I had a 2-bedroom house and 3 kids, and it wasn't working, so I had to move. But, I have an opinion on this topic that I would like to share. I met with Shan a couple years ago and I wanted to see pretty much what you've got in your hand tonight (the map). I wanted to know what they were doing, how it was going to be, what it was going to look like, because I've watched my kids play in this area. I live very close to the affected area and it was very personal to me. This was my first home I purchased by myself, so I felt very passionately about all of this. No disrespect to anybody here, but I fail to see how moving the traffic away from my home decreases my property values. I fail to see how moving a road south detracts from the historical value of the neighborhood. People are speeding now, where I'm hearing a lot of concerns about this 30mph. People are going 50 down the hill from Wells towards Clinton. That's happening, that's the reality. The traffic is there. The speeding is there, so I'm having a hard time understanding how my property values are going to go down. I feel like the people who believe that have never sat in their yard and watched a car come flying down State from Wells at 45mph and slam into the bridge. You've never actually sat out in your yard and watch that happen, because it's there</p>	<p>No response.</p>

		<p>people. I cannot tell you how many times that guardrail has been replaced in the six years that I lived there. Those of you that believe that this is going to harm the neighborhood; you've never had twelve motorcycles race each other down the hill at 3 in the morning gunning their engines and waking up your children. That's not your reality. That is mine for six years.</p> <p>You've never watched a car race down Eastbrook, you know, taking a shortcut from Lima to State. That's happening now. You know, my kids played in that yard for six years. My dog, my children walked that dog through the neighborhood, so I didn't like the idea that I had to clench in fear every time my kids wanted to leave the house to go to a friend's worrying who was going to come racing down Eastbrook Drive to get to State 3 seconds sooner. Move the traffic away from the historical neighborhood and you know you're going to see the improvement of life, you're going to see the increase in property values. All of this being said, my last comment that I have down here was those of you that don't see the value, you've never actually have to leave your house for work at 7:30 in the morning and had to wait 20 or more minutes for an opening in traffic just to leave your own home. So, I appreciate your concerns, but this project is going to preserve the historical beauty of the neighborhood and not take away from it. Thanks!</p>	
15	Tom Shoaff 2130 Forestpark Boulevard Fort Wayne, Indiana	<p>I grew up on Westbrook and I went to school and came back I bought a house on Eastbrook and collectively I've been there over 40 years. So, I have very strong feelings about that neighborhood. I loved being there when I was kid, I enjoyed it when I came back and I started raising my children. I now live off of State Street, but further to the east. I have read this poem that we've been given about why this project is going through and throughout this poem they talked about needing an east/west thoroughfare on the north side of Fort Wayne, and this is where they're going to put it. Now I understand your concerns about the traffic and the congestion, but putting in a thoroughfare only means one thing. More lanes, more traffic and the businesses want it and they want it for their trucks. This road is not going to cure the amenities of your neighborhood that you're trying to protect. It's just going to make it bigger. Now what there is, which is available is an alternative that has a southern passage which takes traffic off of State Street and it doesn't speed it up. The idea of connecting the west side of the properties of Fort Wayne with the east side as a thoroughfare only makes sense if you're talking about trucks and big busy intersections. It doesn't take away from what you dislike now. It doesn't save anything that you're</p>	<p>The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.</p>

		<p>trying to save. The alternative is a much easier passage through Fort Wayne without creating congestion. It gets rid of the speed and it doesn't provide for truck passage. I hope you get a chance to look at it. My understanding is the people that have put that together at great expense and with great energy have not been allowed to present that to the City of Fort Wayne. They asked for a meeting, the City of Fort Wayne said we don't want to meet with you. You ought to be asking for it. You ought to be seeing it. I want your neighborhood preserved because I love that neighborhood, and my children, and I want what's best for you. And what the City is providing is not what you want. They tell you they are protecting your neighborhood, but they are turning your neighborhood into a thoroughfare. Just like Ardmore. It's going to be the north side Ardmore of Fort Wayne. I don't think that's good for anybody.</p>	
16	Todd Ernest	<p>I actually built this room that we're all sitting in tonight. Very concerned about this issue as the other years and years previous, they say they're going to reduce flooding by Run Creek and the Army Corps of Engineers, the City all combined have really not fixed the problem. They did the Clinton Street Bridge just the other year heading southbound there. That didn't fix nobodies basement problems in the neighborhood, did it? I don't know if this should be made in to two separate issues as I look at it. One for Highway Department, straightening a road out and alleviating some tension and traffic, and the other issue being the flooding, which I feel is most important to the neighborhood I grew up in. The flooding is documented for years. It comes from northwest, Cook Road and everything. The industrial parks and everything, the water comes down, and then as Mr. Shoaff said, the dam down there is too short. The water just backs up into these neighborhoods. So what are we getting into here is to beautify and straighten out a road or are we going to help our community here where we were born and raised about the flooding? The engineers need to fix the flooding I believe before a new State Street is built right there. It's huge for insurance purposes. These people that live in the neighborhood. If you look at it. I've been in construction 30 years. You build a bridge, fine and dandy. It may straighten the road around your house and make it a nice drive, but that ain't going to help that water below the bridge and keep your basement from flooding. These engineers need to look at that and they could have fixed it years ago, but maybe they could take the extra water run-off and give it to the people in Hometown. Who knows? But I think the flooding issue for our Fort Wayne Zoo and this community needs to be issued before spending millions of Federal dollars straightening that road. That's my</p>	<p>An identified purpose of the State Boulevard project is to address the flooding that causes the roadway to be impassable due to flooding associated with Spy Run Creek and/or the Saint Mary's River. The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's River which will enable State Boulevard to remain open during flood events. As a result of raising the bridge the flash flooding of Spy Run Creek may be alleviated, possibly reducing the flood impacts along Spy Run Creek to Grove Street. However, addressing the overall flooding issues associated with the surrounding area is not part of the stated purpose of the proposed project.</p>

		<p>opinion and I want the people of the neighborhood to say somebody speaking up every time their basements flood. Talk, speak up and talk to right people. That needs fixed before we straighten the road out. Straightening the road out would be a good thing, There's kids and school buses there, but let's fix the initial thing that we haven't fixed in 40 or 50 years prior first. Okay? Thank you.</p>	
17	Danielle Tuck	<p>I feel like this is going to happen and whatever happens, happens quickly. I do have one question that I'd just like answered is making sure that the access from Oakridge for the neighborhood. It didn't look like there was a stop light or a turn lane or any way to get on and off and I have to do the dive off Terrace or Oakridge already to get on and off State going left or onto our street. So, just want to make sure that's addressed or how that's going to be addressed. How are we getting, left turns in and out of neighborhoods?</p>	<p>Dedicated left turn lanes will be provided along State Blvd. for residents turning onto Cass Street, Oakridge Road, Westbrook Drive, and Edgehill Avenue.</p> <p>It is not currently planned to install traffic signals at Oakridge Road and Westbrook Drive. The intersections will remain stop controlled as the current conditions. If future traffic warrants are met, the installation of signalized intersections would be considered.</p>
18	<p>Peggy Grady 2314 Eastbrook Fort Wayne, Indiana</p>	<p>I am one of the four or five of the oldest still left in the neighborhood and I am for the new street and bridge. We need something. I am especially sorry for those people who have been told that their houses were going to be bought out six years ago and the flooding is a problem. It needs to be addressed first and we all agree to that. We did have a backup tsunami last year on June 1st. In 37 years our basement has never had any water. All of our neighbor have, but not us. Why on June 1st did we have roaring in 8-10 inches before we could do a thing about it and because the one gentleman here, he lost the foundation on the south side of his house. The only way that could happen is if there was a backup tsunami, and that does need to be addressed. But, I am sorry for these people that are having to wait out because they because they had to replace their furnace, their water heater, their washers, their dryers, and because this whole project has been stalled and stalled and stalled poor people can't even be bought out to get out of there. And Vivian, she has lived there fifty-some years and she looks out her window everyday and sees all of these broken down properties across the street. No one can put money back into properties when they don't know when they're going to be bought out. It is time we get over and move this forward and get done with all of this other stuff.</p>	<p>No response.</p>

19	Robert Shoaff	I didn't really plan to speak tonight as much as my two brothers already did. But I am concerned that we're confusing this flood issue with a traffic issue here. Two absolutely separate issues. What the City is proposing is a traffic project and has got nothing to do with flooding. Now if people that flood and want to be bought out, should be bought out. They should have been bought out six years ago probably. But it has nothing to do with turning State Street into a 5-lane highway. Now, if I were to boil this issue down to our neighborhood, I would say can our neighborhood survive another Clinton Street? That's what we have, we have a 5-lane highway going through our next door neighborhood and I'm old enough to remember when I could cross Clinton Street on my bicycle and it was a 2-lane highway, my best friend lived across on the other side. I passed across that road several times a day sometimes and if that were a 5-lane highway when I was growing up I might have survived, but my mother wouldn't. She'd have gone to an early grave. Just ask yourself, well okay that's not a concern for us now, but how about the other people who have families with small kids? A lot of them look at houses in our neighborhood and they say we're not going live here. It's not safe for our kids. Turning State Street into a 5-lane highway like Clinton Street is not going to help the safety of our kids. That's all I've got to say.	No response.
20	Julie Donnell	I'm the President of Friends of the Parks Allen County and I just wanted to urge everyone to come to Arch's Public meeting to see the alternative on June 30 th . I think you will admire what the engineers and the landscape architects from Indianapolis have come up with. When the Friends of the Parks learned about this project in 2007 or 2008 the RMP had already been let for 5-lanes, and so no other alternative had been explored publically for the rest of us. We have always hoped that this design should have been held under the....inaudible...of a qualified landscape architect, and Arch will be presenting a plan that is done in that manner so I hope you'll all have a come to see it. Thank you. As I remember what Mike said it's at Psi Ote Barn, lower level, June 30 th , 7 o'clock.	The Consulting Parties Proposed Alternative (CPPA) will be considered and evaluated. The results of the evaluation will be documented in an Additional Information Document to the May 14, 2014 approved Environmental Assessment.
21	Peggy Roy 2121 Skyhawk Drive Fort Wayne, Indiana 46815	I do not live, I do own a home in the affected area, 2248. I am south of State Street. I was one of five homes that was left there after the initial buy out by the City, they bought out all of Westbrook, they bought homes on both ends of my street there in Eastbrook. All part of the flood purchase. In 2007 after my first appraisal, the City called me and told me that INDOT was sending in and it was all a Federally funded INDOT project and that they would not be acquiring my home. That	No response.

		<p>was 2007. Me and my four other neighbors have been living in limbo for 8 years. It is now a rental property for me and if any of you know what it's like to be a landlord all it involves that follow that, it is not a pleasant situation. It is not something I asked for, it's not something I wanted to do. Had I known that the City was going to back out, I would have never had moved out of that house. That property is costing me thousands. June 1st of last year as everybody has stated before flood, 5 ½ inches of rain in a short period of time, was the first time the water ever got up to my front door, let alone broke out my basement windows and flooded all the way up to the first floor. It was an absolute nightmare. With the help of Insurance companies I put \$15,000 on the repairs into that home just so I could rent it temporarily until somebody acquires my property. I don't know whether this whole street straightening thing is going to solve the flooding issue or not. It may help, but I don't think it's going to solve it completely. All I'm asking is that somebody and I thank you...inaudible...for bringing that up and mentioning us on the south side of State Street and you know the hardships that we went through. I don't care when this project starts. I don't care if it happens at all, but I've been told for six, seven, eight years that my property would be acquired and I'm telling you right now it's time, I'm over this. Something needs to happen, something needs to happen soon. I have a home in disrepair. I can't put money into this home because it's going to be torn down. I have to keep fixing it. I want it to be done with. I want it to be over with. Thanks for listening.</p>	
22	John Hartman	<p>I'm John Hartman of Northside Plumbing, 2238 and 2234 North Clinton. In the past my basements have flooded and they didn't on June 1st so the guys that did that must have done something right, so thank you.</p>	No response.

Indiana Department of Transportation

County Allen Route State Boulevard Des. No. 0400587 Project No. _____

FHWA-Indiana Environmental Document

**CATEGORICAL EXCLUSION/ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:

State Boulevard/Allen County

Designation Number:

0400587 (Phase 1, Terrace Road to Spy Run Avenue -1005151, Phase 2, Cass Street to Terrace Road -1005154, Pedestrian Bridge-1005155, and Spy Run Creek Bridge-1005152)

Project Description/Termini:

State Boulevard Reconstruction Project– State Boulevard between Spy Run Avenue and Cass Street including the bridge over Spy Run Creek

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

<input type="checkbox"/>	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
<input type="checkbox"/>	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services).
<input type="checkbox"/>	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA.
<input checked="" type="checkbox"/>	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA.

Note: For documents prepared by or for Environmental Services, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

ESM Signature <u>Jayce E. Newland</u>	Date <u>5.14.14</u>	ES Signature <u>[Signature]</u>	Date <u>8 MAY 14</u>
FHWA Signature	Date		

Release for Public Involvement

ESM Initials _____	Date _____	ES Initials _____	Date _____
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Certification of Public Involvement

Examiner, Public Hearings Signature _____ Date _____

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

Name and organization of CE/EA Preparer: Briana M. Hope – American Structurepoint, Inc.

Attachment 6

Project Commitments – Pages 1-7

**State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No 0400587
Project Commitments**

FIRM COMMITMENTS

USFWS

1. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries.
2. Restrict below-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.
3. Restrict channel work and vegetation clearing to within the width of the normal approach road right-of-way.
4. Minimize the extent of artificial bank stabilization.
5. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
6. Implement temporary erosion and siltation control devices such as placement of straw bales in drainage ways and ditches, covering exposed areas with burlap, jute matting or straw, and grading slopes to retain runoff in basins.
7. Revegetate all disturbed soil areas immediately upon project completion.
8. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season (as applicable).

IDNR

9. Minimize and contain within the project limits in channel disturbance and the clearing of trees and brush.
10. Do not work in the waterway from April 1 through June 30 without prior written approval of the Division of Fish and Wildlife
11. Post "Do Not Mow or Spray" signs along the right-of-way.
12. Seed and protect all disturbed streambanks and slopes that are 3: 1 or steeper with erosion control blankets (follow manufacturer's recommendations for installation); seed and apply mulch on all other disturbed areas.
13. Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion; low endophyte tall fescue may be used in "high maintenance" areas only.[Alternate wording – check your letter - Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes as soon as possible upon completion; low endophyte tall fescue may be used in ditch bottom and side slopes only.]

14. Do not cut any trees suitable for Indiana bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark) from April 1 through September 30.
15. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
16. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
17. Do not work in salmonid waterways from March 15 through June 15 and from July 15 through November 30 without the prior written approval of the Department of Natural Resources, Division of Fish and Wildlife.
18. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
19. The project must not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. This includes maintaining land under the bridge unarmored with riprap to allow for wildlife passage.
20. If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317)232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.
21. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
22. Place all excavated material landward of the floodway.
23. Do not leave felled trees, brush, or other debris in the floodway. Remove all construction debris from the floodway.
24. Keep the bridge waterway opening free of debris and sediment at all times.
25. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree, which is removed that is ten inches or greater in diameter-at-breast height within the regulatory floodway or as required by permit conditions.

IDEM

26. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project.
27. IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff.

28. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. Dirt tracked onto paved roads from unpaved areas should be minimized.
29. All facilities slated for renovation or demolition must be inspected by an Indiana-licensed asbestos inspector prior to renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, demolition, renovation, or asbestos removal activities must be performed in accordance with notification and emission control requirements.
30. In all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition.
31. IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust.
32. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7 percent) oil distillate, is prohibited during the months of April through October.
33. Stabilize all disturbed areas upon completion of land disturbing activities.
34. Sediment-laden water, which otherwise would flow from the project site shall be treated by erosion and sediment control measures appropriate to minimize sedimentation.
35. Wastes and unused building materials shall be managed and disposed of in accordance with all applicable statutes and regulations.
36. A stable construction site access shall be provided at all points of construction traffic ingress and egress to the project site.
37. Public or private roadways shall be kept cleared of accumulated sediment that is a result of run-off or tracking.

MEMORANDUM OF AGREEMENT

38. The City of Fort Wayne shall, where feasible, implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.
39. The City of Fort Wayne shall, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences. The City of Fort Wayne shall provide the Indiana SHPO and consulting parties a dispensation plan for salvaged architectural details. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All

improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.

40. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.
41. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following: The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
 - a. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
 - b. Context sensitive solutions that may include but not be limited to: protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; and maintaining pedestrian connections along the former Eastbrook and Westbrook drives shall be included among the measures considered.
 - c. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
 - d. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), INDOT, the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.
 - e. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be

invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail or US 27) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.

- f. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
- g. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before scheduled meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.
- h. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
- i. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."

42. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges.”
43. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
- a. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - b. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
44. Photo documentation will include black and white prints of digital photographs and a digital video disc (“DVD”) containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards” that are in effect at the time.
- a. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 - b. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
 - c. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
 - d. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.
45. The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George

Kessler's landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps or other WPA era programs in public projects.

- a. The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within thirty (30) days, acceptance will be assumed. If the Indiana SHPO responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

FOR CONSIDERATION

USFWS

1. Shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore we request that trees lost to the project be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail.

INDOT-FORT WAYNE DISTRICT

2. This project will be taking place within the NRHP Eligible Brookview/Irvington Park Historic District. This neighborhood is eligible due to the layout of the streets following Spy Run Creek. Taking of right-of-way in the area will most likely constitute a Section 4(f) impact. Due to these situations, minimization of impacts in this area should be considered by multiple alternatives to show the proposed plan is the most feasible and prudent. Context sensitive design to fit the historic setting of the neighborhood should also be investigated.
- Firm Commitments