

**FEDERAL HIGHWAY ADMINISTRATION'S  
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND  
SECTION 106 FINDINGS AND DETERMINATIONS  
AREA OF POTENTIAL EFFECT  
ELIGIBILITY DETERMINATIONS  
EFFECTS FINDING  
STATE BOULEVARD RECONSTRUCTION  
FROM SPY RUN TO CASS STREET  
FORT WAYNE, ALLEN COUNTY, INDIANA  
DES. NO. 0400587  
FEDERAL PROJECT NUMBER: IN20071404**

**AREA OF POTENTIAL EFFECTS**

**(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) is centered on State Boulevard in Fort Wayne, Wayne Township, Allen County, Indiana. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway.

The archaeological APE is defined as the project footprint.

**ELIGIBILITY DETERMINATIONS**

**(Pursuant to 36 CFR 800.4(c)(2))**

Two historic properties are listed in the National Register of Historic Places (NR): Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District. One historic property has previously been determined eligible for the NR: Bridge over Spy Run Creek.

**Fort Wayne Park and Boulevard System Historic District (NR, 2010).** The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. It encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler. The district includes nearly 2,000 acres of parks, boulevards, and sites. Eight resources (seven of which are contributing) identified as part of the Fort Wayne Park and Boulevard System Historic District are located within the APE for this project. The FWPB is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909 to 1955.

**Brookview-Irvington Park Historic District (NR, 2011).** The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906 to 1965.

**Bridge over Spy Run (NBI No. 0200273).** The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927. The Bridge over Spy Run is also identified as a Contributing resource in the Fort Wayne Park and Boulevard System Historic District and the Brookview-Irvington Park Historic District.

#### **EFFECT FINDING**

**Fort Wayne Park and Boulevard System Historic District (NR, 2010)—Adverse Effect**  
**Brookview-Irvington Park Historic District (NR, 2011)—Adverse Effect**  
**Bridge over Spy Run (NBI No. 0200273)—Adverse Effect**

Federal Highway Administration (FHWA) has determined a finding of Adverse Effect is appropriate for this undertaking.

#### **SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)**

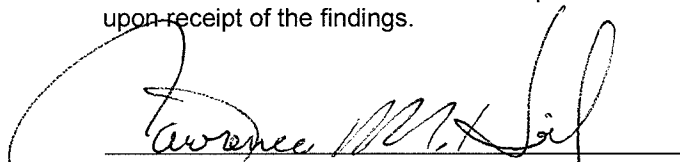
**Fort Wayne Park and Boulevard System Historic District** – This undertaking will convert property from the Fort Wayne Park and Boulevard System Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Fort Wayne Park and Boulevard System Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

**Brookview-Irvington Park Historic District** – This undertaking will convert property from the Brookview-Irvington Park Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Brookview-Irvington Park Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

**Bridge over Spy Run (NBI No. 0200273)** – This resource is used for transportation purposes. This undertaking will have an "Adverse Effect" on the Bridge over Spy Run, a Section 4(f) property; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Bridge over Spy Run. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."



Consulting parties will be provided a copy of the findings and determinations of FHWA, in accordance with FHWA's Section 106 procedures. Comments will be accepted for thirty (30) days upon receipt of the findings.



Lawrence M. Hil

for Karen Bobo  
Acting Division Administrator  
Federal Highway Administration

2-27-2013

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION  
DOCUMENTATION OF SECTION 106 FINDING OF  
ADVERSE EFFECT  
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER  
PURSUANT TO 36 CFR SECTION 800.6(a)(3)  
STATE BOULEVARD RECONSTRUCTION  
FROM SPY RUN TO CASS STREET  
FORT WAYNE, ALLEN COUNTY, INDIANA  
DES. NO.: 0400587  
FEDERAL PROJECT NUMBER: IN20071404**

**1. DESCRIPTION OF THE UNDERTAKING**

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The Preferred Alternative for this project is Alternative 3A, Access Alternate 2. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four ten-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an eight-foot wide raised median and a two-way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment will be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four eleven-foot travel lanes, two in each direction, separated by a twelve-foot two-way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway. Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. The project also includes a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive, which will connect the Pufferbelly Trail.

36 CFR § 800.16(d) defines the Area of Potential Effects (APE) as the “geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.”

The APE is centered on State Boulevard. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway. (See Appendix B, Maps and Site Plans for a map of the APE.) The archaeological APE is defined as the project footprint.

## 2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Pursuant to 36 CFR Section 800.4(b), project consultants conducted efforts to identify historic properties—including consultation—as part of this Section 106 undertaking.

The Westerly Group, Inc. (WGI) initiated efforts to identify historic properties. According to WGI, historians investigated the National Register of Historic Places (NR), Indiana Register of Historic Sites and Structures (SR), *Indiana Historic Bridge Inventory*, *Fort Wayne Interim Report*, Sanborn Fire Insurance maps, as well as other primary and secondary sources. Online resources were also accessed to complete the research. Research included a review of the property listing: “The Civilizing of a Midwestern City: The Park and Boulevard System of Fort Wayne, Indiana--A Plan for the Ideal Development of Transportation, Parks and Residential Subdivision,” the Historic Property Report (HPR) for the US 27 Southbound Bridge Replacement Over Spy Run Creek (Ross Nelson), and a preliminary NR nomination for Wildwood Park Historic District.<sup>1</sup>

On March 18, 2009, archaeologists for Archaeological Consultants of Ossian conducted a field reconnaissance for a project area of approximately 6.43 acres. The reconnaissance included shovel testing, pedestrian walkover, and auger testing.

On March 23, 2009, American Structurepoint, Inc. (Structurepoint) sent a Section 106 Early Coordination Letter, describing the proposed project and inviting the following parties to join Section 106 consultation: Indiana State Historic Preservation Officer (SHPO); Indiana Department of Transportation (INDOT); INDOT—Fort Wayne District, City of Fort Wayne Engineer; Historic Landmarks Foundation of Indiana (now Indiana Landmarks); Allen County Historian; Allen County—Fort Wayne Historical Society; ARCH, Inc.; Fort Wayne Historic Preservation Review Board; and John Shoaff, Fort Wayne city council member. The Allen County historian declined to participate. The City of Fort Wayne, ARCH, Inc., Fort Wayne Historic Preservation Commission, and Indiana Landmarks Northern Regional Office, accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

Archaeological Consultants of Ossian completed an Archeological Field Reconnaissance Report on April 2, 2009. In the report, archaeologists stated, “It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places.” The archaeologist recommended no further work and project clearance. (See Appendix E: Report Summaries.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Both organizations accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

In a letter dated April 23, 2009, SHPO concurred with ARCH, Inc.’s request that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Also, due to potential eligibility of the Bridge over Spy Run Creek, SHPO requested that bridge historian Dr. James Cooper and Historic Spans Taskforce representative Paul Brandenburg be invited to join consultation. Historic Spans Taskforce accepted the invitation to join consultation. The SHPO also requested that the APE be “carefully delineate[d]” to take into account impacts. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On July 2, 2009, Structurepoint transmitted the Archaeological Field Reconnaissance report to the SHPO. (See Appendix F: Correspondence and Meeting Minutes.)

On July 9, 2009, the Irvington Park Neighborhood Association joined consultation. (See Appendix B: Consulting Parties.)

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<sup>1</sup> These are the sources that the Westerly Group lists in the Historic Property Report (2009), page 8.

On November 9, 2009, Structurepoint sent consulting parties and SHPO a copy of the HPR for review and comment. The HPR recommended the Brookview-Irvington Park National Register Historic District, Bridge over Spy Run (NBI No. 0200273), and the House at 315 East State Boulevard eligible for listing in the NR. The HPR also recommended the portion of State Boulevard within the Brookview-Irvington Park district as individually NR eligible and contributing to the district. (See Appendix E: Report Summaries and Appendix F: Correspondence and Meeting Minutes.)

On December 1, 2009, Structurepoint sent an invitation to consulting parties to attend a meeting on December 15, 2009, to discuss the identification of historic properties. On December 8, 2009, Structurepoint provided a revised agenda. On December 15, 2009, a consulting party meeting was held at the City-County Building in Fort Wayne to discuss the identification and evaluation of historic resources per the HPR and future steps in the Section 106 process. Consulting parties requested that the APE be enlarged but offered no additional information regarding the identification and evaluation of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On July 6, 2011, Structurepoint sent an email to consulting parties conveying digital copies of its letters to SHPO. Structurepoint stated, "Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15 day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters." The letter also stated, "if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a 'cc' during this Section 106 process." (See Appendix F: Correspondence and Meeting Minutes.)

On July 13, 2011, an agency meeting was held with FHWA, INDOT, and SHPO to discuss SHPO's comments on the recent Purpose and Need submission; how to address the comments and concerns of consulting parties; Section 4(f); and ACHP involvement. (See Appendix F: Correspondence and Meeting Minutes.)

On August 15, 2011, Structurepoint sent a letter by post to consulting parties informing them of a consulting party meeting scheduled for September 1, 2011. Enclosures included a meeting agenda, agency coordination meeting minutes, a copy of a letter to SHPO, a copy of a letter to FHWA, Historic Bridge Programmatic Agreement Alternatives Analysis, Individual Section 4(f) Alternatives Analysis, Corridor Alternatives Map, and Consulting Party Questions/Comments and Responses. Structurepoint conveyed the same data electronically in an email dated August 16, 2011. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated August 29, 2011, FHWA wrote to the Advisory Council on Historic Preservation (ACHP) asking if they would be involved in consultation for this project. According to FHWA's letter, "FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A - Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), " *... adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*" and (c)(3) for " *... cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council's involvement could help resolve ...* " (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 1, 2011, at Citizens Square in the City of Fort Wayne to discuss project updates; purpose and need update; consulting party comments and responses document; alternatives review; and future steps. Following the meeting, Structurepoint agreed to prepare a three-lane design alternative for review. It was also decided that a another

consulting party meeting would take place after the issuance of the 800.11 documentation and the finding of “Adverse Effect” and at that time the group would discuss mitigation and forming an advisory committee to consult on mitigation measures. (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, an agency meeting with FHWA, INDOT, and Structurepoint was held to follow-up on the consulting party meeting. (See Appendix F: Correspondence and Meeting Minutes.)

On September 29, 2011, Structurepoint transmitted the following material to consulting parties: meeting minutes from the September 1, 2011 consulting party meeting; agency coordination meeting summary from September 2, 2011; revised individual Section 4(f) analysis including an additional alternative; traffic data from NIRCC; and ACHP correspondence. (See Appendix F: Correspondence and Meeting Minutes.)

In February 2012, Structurepoint contracted with Weintraut & Associates, Inc. (W&A) to update the HPR through an Additional Information Report (AI). The purpose of the AI Report was to supplement the HPR following the inclusion of two new NR-listed resources within the APE.

On March 6, 2012, historians from W&A walked and drove the APE, viewed all the resources within the APE, and photographed and recorded survey notes about resources greater than fifty years of age considered or rated Contributing or higher. Historians also field verified the APE at that time.

Historians for W&A published an AI report in April 2012. The report identified two districts that were listed in the NR after publication of the HPR: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). Portions of both districts are within the APE. The Bridge over Spy Run (NBI No. 0200273) was previously determined eligible for listing in the NR. Historians expressed the opinion that proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts and recommended a finding of “Historic Properties Affected—Adverse Effect” for the project. (See Appendix E: Report Summaries.)

On May 22, 2012, Structurepoint conveyed the AI Report to consulting parties, superseding their letter of May 17, 2012. (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an agency meeting was held with FHWA, INDOT, and SHPO to discuss the State Boulevard Project and the eligibility recommendations and findings.

On July 3, 2012, Archaeological Consultants of Ossian conducted an additional records check at the DHPA and on July 5 and 6, 2012 conducted a field reconnaissance for additional project areas. An Archaeological Short Report, completed July 11, 2012, concluded the Phase Ia reconnaissance located no archaeological resources and recommended project clearance. (See Appendix E: Report Summaries.)

On July 16, 2012, Structurepoint conveyed an archaeological short report for the additional area required for the State Boulevard Improvements project. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2012, Structurepoint sent a letter to consulting parties conveying FHWA's Findings and Determinations for the project. Structurepoint invited consulting parties to comment on the letter and to attend a consulting party meeting on September 19, 2012, to discuss the resolution of adverse effects. No comments were received regarding the identification of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On September 18, 2012, Structurepoint sent an email and attachment to consulting parties regarding project impacts to houses at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard—within the Brookview-Irvington Historic District. The letter stated: “It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions.” (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 19, 2012, to discuss the resolution and mitigation of adverse effects. (See Appendix F: Correspondence and Meeting Minutes.)

No further efforts to identify historic properties, including consultation, took place as part of this Section 106 undertaking.

#### **Timeline of Formal Consultation**

(See Appendix F: Correspondence and Meeting Minutes for referenced correspondence.)

<b>March 23, 2009:</b>	Structurepoint initiates consultation by sending a Section 106 Early Coordination Letter
<b>April 23, 2009:</b>	SHPO comments on the Early Coordination Letter
<b>July 2, 2009:</b>	Structurepoint sends archaeological report to SHPO
<b>November 9, 2009:</b>	Structurepoint transmits a copy of the HPR to SHPO and consulting parties
<b>December 1, 2009:</b>	Structurepoint sends invitation to a consulting party meeting
<b>December 14, 2009:</b>	SHPO comments on the HPR and Archaeology Report
<b>December 15, 2009:</b>	Consulting party meeting held in Fort Wayne
<b>December 28, 2009:</b>	Structurepoint sends minutes from consulting party meeting
<b>January 27, 2010:</b>	SHPO comments on minutes of December 15, 2009, consulting party meeting and asks questions regarding Purpose and Need
<b>February 4, 2010:</b>	Structurepoint sends a CD to SHPO containing the City of Fort Wayne 2005 Flood Control Study, traffic data, and revised meeting minutes
<b>March 10, 2010:</b>	SHPO comments on revised minutes from the consulting party meeting and the other informational items sent on February 4, 2010
<b>May 19, 2011:</b>	Structurepoint responds to questions raised in SHPO correspondence comments of January 27, 2010 and March 10, 2010
<b>June 17, 2011:</b>	Structurepoint sends documents missing from May 19, 2011 transmittal and extends comment period for another fifteen days
<b>July 5, 2011:</b>	SHPO responds to Structurepoint’s letters of January 27, 2010 and March 10, 2010

<b>July 6, 2011:</b>	Structurepoint emails consulting parties digital copies of letters to SHPO
<b>July 13, 2011:</b>	Agency meeting with FHWA, INDOT, and SHPO discussing SHPO's comments on recent Purpose and Need submission; how to address consulting parties comments; Section 4(f); and Inviting ACHP involvement
<b>August 15, 2011:</b>	Structurepoint sends an invitation to consulting party meeting scheduled for September 1, 2011
<b>August 15, 2011:</b>	Structurepoint sends a letter to FHWA requesting the agency invite the ACHP to participate
<b>August 16, 2011:</b>	Structurepoint conveys August 15, 2011, letter to FHWA electronically to consulting parties
<b>August 29, 2011:</b>	FHWA asks the ACHP to participate in consultation
<b>September 1, 2011:</b>	Consulting party meeting held in Fort Wayne
<b>September 2, 2011:</b>	Agency meeting with FHWA, INDOT, and Structurepoint to follow-up on the consulting party meeting held the previous day
<b>September 22, 2011:</b>	ACHP requests additional information regarding the project in order to determine if its participation is warranted
<b>September 29, 2011:</b>	Structurepoint transmits meeting minutes and additional information that consulting parties had requested to consulting parties
<b>November 7, 2011:</b>	SHPO responds to the Structurepoint's letters of August 15, 2011, and September 29, 2011
<b>May 22, 2012:</b>	Structurepoint sends a letter conveying the AI Report to consulting parties and replacing a letter sent May 17, 2012
<b>June 20, 2012:</b>	Agency meeting discusses the State Boulevard Project and the eligibility recommendations and findings
<b>June 22, 2012:</b>	SHPO responds to AI Report
<b>July 2, 2012:</b>	Structurepoint sends SHPO a letter answering questions raised in its letter dated November 7, 2011
<b>July 11, 2012:</b>	Archaeological Consultants of Ossian prepares an Archaeological Short Report for additional areas of the project. In the report, archaeologists conclude the Phase Ia reconnaissance located no archaeologists and recommended project clearance
<b>July 16, 2012:</b>	Structurepoint sends an archaeological short report for the additional area required for the State Boulevard Improvements project to SHPO
<b>July 31, 2012:</b>	ACHP declines to participate in consultation

<b>August 1, 2012:</b>	SHPO responds to the June 20, 2012, agency meeting
<b>August 13, 2012:</b>	SHPO concurs with the archaeological short report
<b>August 29, 2012:</b>	Structurepoint sends letter conveying FHWA's Findings and Determination of Adverse Effect and the draft 800.11(e) documentation. The letter includes an invitation to a consulting party meeting
<b>September 18, 2012:</b>	Structurepoint transmits letter regarding project impacts to three parcels on East State Boulevard
<b>September 19, 2012:</b>	Consulting parties meeting
<b>October 5, 2012:</b>	INDOT informs consulting parties of they will have an opportunity to comment on Section 106 materials during the Environmental Assessment review period
<b>November 15, 2012:</b>	SHPO responds to draft MOA
<b>December 18, 2012:</b>	Agency meeting to discuss mitigation

### 3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Three historic properties will be affected by the undertaking: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek.

**Fort Wayne Park and Boulevard System Historic District (NR, 2010).** The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and Bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NR in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized."

**Brookview-Irvington Park Historic District (NR, 2011).** The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street and Jacobs Avenue. The district contains a total of 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965 and represents the construction dates of most buildings within the



historic district and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination.

**Bridge over Spy Run (NBI No. 0200273).** The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

#### **4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES**

The proposed project will include the widening, realignment, and elevation of State Boulevard approximately 190 feet south and the construction of a new access road between the existing and proposed State Boulevard alignment (and the conversion of some existing intersections to cul-de-sacs). The project also proposes modified alignments at Westbrook Drive and Clinton Street. Additionally, the proposed project requires approximately fifteen residential relocations, and the removal of trees and older street amenities. Construction of concrete curb and gutter, raised median, left turn lanes, decorative lighting, modified traffic lights, landscaped utility strip, curb inlets, and storm sewers. A new bridge structure will replace the existing bridge over Spy Run Creek. The project also includes a prefabricated pedestrian trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive. Sidewalk ramps will be extended from the proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. Additionally, removal and relocation has been selected as the most prudent and feasible alternative for the Bridge over Spy Run. The elevation of State Boulevard and the removal of the properties that contribute to the district will result in the creation of new public spaces in a residential setting and the bifurcation of the Brookview-Irvington Historic District. These elements will result in an adverse effect on the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run.

#### **5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT--INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS**

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." Because construction of the Pufferbelly Trail is reasonably foreseeable and because environmental assessment of the Pufferbelly Trail undertaking is currently taking place, effects from that project have been included in this application of the criteria of adverse effects.

**Fort Wayne Park and Boulevard System Historic District.** The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Fort Wayne Park and Boulevard System Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause "[p]hysical destruction of or damage to all or part of the property," particularly through the realignment and elevation of State Boulevard. Further, land from identified Contributing features within the district, including the Bridge over Spy Run and "Sloping Hills and Natural Features" (SR-10), will be taken.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause "[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines."

Per 36 CFR 800.5(a)(2)(iii), the undertaking will cause “[r]emoval of the property from its historic location,” at the location of the State Boulevard realignment.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The Contributing Bridge over Spy Run Creek will be replaced, a prefabricated trail bridge, retaining walls, and ramps (associated with the Pufferbelly Trail) will be constructed over State Boulevard at the abandoned New York Central railroad right-of-way, the Contributing State Boulevard will be realigned, widened, and elevated. The Contributing Eastbrook Drive will be converted to a cul-de-sac.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, widening and elevation of the Contributing State Boulevard, the construction of a cul-de-sac at the Contributing Eastbrook drive, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps and retaining walls (associated with the Pufferbelly Trail) over the Contributing State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the Contributing Bridge over Spy Run Creek.

Per 36 CFR 800.5(a)(2)(vi), the project will not result in “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), there will be no “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

**Brookview-Irvington Park Historic District.** The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Brookview-Irvington Park Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” The proposed undertaking will result in the relocation of Contributing residential resources, the removal of private space, and change the orientation of the Brookview neighborhood plat, a Contributing resource. The elevation of the re-aligned State Boulevard will result in the bifurcation of the district.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the undertaking will result in the “[r]emoval of the property from its historic location.” State Boulevard will be widened, elevated, and realigned south from its historic location; residential relocations will take place within the Brookview-Irvington Park Historic District. The Bridge over Spy Run, a Contributing resource, will be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance” through the realignment of some roadways, conversion of some intersections to cul-de-sacs, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. The landscape of the area will be modified by the realigned and elevated State Boulevard and by the realignment of the original Brookview-Irvington Park plat, both of which

were identified as Contributing to the district. The removal of the Contributing homes will change the character of the plat since the open, public spaces will be much larger than they are presently.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, elevation, and widening of State Boulevard, the realignment of Westbrook Drive and Clinton Street, the construction of cul-de-sacs at some locations, the removal of some streetscape elements and the introduction of other streetscape elements, construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. All of these elements will change the character of the district along State Boulevard.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

**Bridge over Spy Run (NBI No. 0200273).** The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the previously determined NR-eligible Bridge over Spy Run.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” Bridge over Spy Run will either be removed from its present location or demolished as part of this undertaking. The removal or demolition will be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The pending removal or demolition of the bridge is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.” Bridge over Spy Run will be removed or demolished as part of the undertaking, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iii), the property will be removed from its historic location either by demolition or removal, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” Bridge over Spy Run will either be removed from its present location or demolished. The removal or demolition is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The bridge’s setting will change as a result of: the realignment and widening of State Boulevard, the construction of cul-de-sacs near the bridge, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps (associated with the Pufferbelly Trail) over State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the bridge over Spy Run Creek, and the approximately fifteen residential relocations.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .” though the bridge will be removed and demolished.

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

#### **FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS**

Consultation has occurred regarding Purpose and Need, Alternatives, and Traffic Data in order to reduce impacts on historic properties. Consulting parties offered comments regarding the project purpose and need, selection of project alternatives, and traffic data during the Section 106 process for this undertaking and requested changes to the alternatives analysis as a result of that information. At the request of consulting parties, a third alternative—Alternative 3D—was considered. (Consulting party comments on purpose and need, alternatives selection, and traffic data—which will be included as part of the Environmental documentation—are included in Appendix F: Correspondence and Meeting Minutes.)

Consultants have met with consulting parties and with the Indiana SHPO to discuss options to avoid, minimize and mitigate adverse effects on December 15, 2009 (consulting parties and SHPO), July 13, 2011 (SHPO), September 1, 2011 (consulting parties and SHPO), September 2, 2011 (SHPO), June 20, 2012 (SHPO), September 19, 2012 (consulting parties and SHPO), and December 18, 2012 (SHPO). The Indiana SHPO agreed to enter into a Memorandum of Agreement (MOA) at a meeting held on June 20, 2012. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of the Bridge over Spy Run; Restore character of State Boulevard within the district; and Educational mitigation.

A consulting party meeting was held on September 19, 2012, to discuss stipulations for the MOA and to solicit other mitigation ideas. At that meeting, Structurepoint shared the following efforts to minimize impacts: 1. A three-lane road with center turn lane was discarded because it did not provide the required level of service. 2. Reduction of right-of-way impacts to allow three properties to remain in place. (After agency consultation, this was discarded.) 3. Maintaining existing curvature between Eastbrook and Terrace Road. 4. Maintaining existing curb lines of Eastbrook Drive where possible.

#### **The following alternatives have been evaluated for the Bridge over Spy Run:**

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structures Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge structure is five years from the date of the inspection of the report (2011). The existing bridge is currently below the flood elevation of the St. Mary’s River which causes the bridge to be overtopped with backwater from the St. Mary’s River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) “This flooding is caused primarily by backwater from the [St.] Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

According to the recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010. Six out of the seventeen years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed

due to the flooding events. Road closure due flooding events appears to be happening more consistently in recent years, restricting emergency traffic more often.

#### *1. Rehabilitation for Continued Vehicular Use (two-way option)*

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

#### *2. Rehabilitation for Continued Vehicular Use (one-way option)*

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven

feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

### *3. Bypass (Non-vehicular use)*

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the *Fort Wayne Journal-Gazette* and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546.

The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

#### *4. Replacement*

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

#### *5. Relocate and Replacement*

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately seven feet to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure was marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge was listed as available for re-use in the *Fort Wayne Journal-Gazette* on February 2, 2010. The bridge was also listed as available on the INDOT Website and signs were posted on the bridge offering it for re-use. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use.

Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for

vehicular use. While this alternative does meet the project's purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

**The following alternatives have been evaluated for the State Boulevard Reconstruction project:**

*Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)*

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) terminus of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

*Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)*

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the



corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately seventy-five residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility. This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

*Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)*

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

*Access Alternate 1*

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

*Access Alternate 2 (Preferred Access Alternative)*

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State

Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.

#### *Access Alternate 3*

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

#### *Alternative 3B: Widen State Boulevard on Existing Alignment*

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately twenty residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety

concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

#### *Alternative 3C: Shift State Boulevard Alignment South*

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to four-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately four to five-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project’s purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

#### *Alternative 3D: Preferred Alignment with 3-Lane Typical Section*

This alternative is similar to Alternative 3A (Preferred Alternative) but features a three-lane typical section rather than a four-lane typical section. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to three-lanes while correcting the substandard horizontal curve.

By reducing the typical section from four-lanes (Alternative 3A/Preferred Alternative) to three-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four-lanes to three-lanes is only ten-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton

Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

#### *Alternative 4: No Build*

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.

The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

In response to requests for additional review time for the 800.11(e) and findings and determinations distributed on August 29, 2012 INDOT wrote an email on October 5, 2012, to consulting parties regarding comments on the project. INDOT informed consulting parties that the 800.11 documentation would be updated and the finding, draft Memorandum of Agreement [MOA], draft 4(f) evaluation, "will be released with the Draft Environmental Assessment for an additional comment for both consulting parties and the public." Therefore, the comment period for the 800.11 documentation, draft MOA, and draft 4(f) evaluation would not be extended. (See Appendix F: Correspondence.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet with it and its consultants to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain, landscape architect, made the presentation. Cain's plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A: Plans.)

After discussion, the City and its consultants agreed that in addition to the mitigation landscape plan, they would look for other ways to mitigate the adverse effect, such as grants to rehabilitate the facades of existing houses (if practical and legally viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

## **6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS**

During the course of consultation, the following organizations have responded affirmatively to the invitation to join consultation: City of Fort Wayne; Friends of the Parks of Allen County; Allen County Historian; Indiana Landmarks—Northern Regional Office; Fort Wayne Historic Preservation Commission; ARCH, Inc.; Brookview Neighborhood Association; Indiana Historic Spans Taskforce; Irvington Park Neighborhood Association. Additionally, the following individuals or organizations participated in or requested to join consultation: Charley Shirmeyer, Northside Galleries; Albert Cohan, Westbrook 5, LLC; Thomas Niezer, Barret & McNagny, LLP; Ronald Ross, Martin Riley Architects and Engineers; Dan Ernst, Earth Source, Inc.; Jan Dailey, State Boulevard Resident. (See Appendix B: Consulting Parties.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks of Allen County and Brookview Neighborhood Association be invited to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On April 23, 2009, SHPO wrote in response to the notification concerning the reconstruction of State Boulevard and requested a literature review, historic context, research methodology, property descriptions, and NR eligibility evaluations and recommendations to aid analysis of the project. SHPO recommended the Friends of the Parks and Boulevard Neighborhood Association, Indiana Historic Spans Task Force, and bridge historian Dr. James L. Cooper be invited to participate as consulting parties. (See Appendix F: Correspondence and Meeting Minutes.)

On December 7, 2009, Jan Dailey wrote in response to the HPR: "I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effects." In regard to the project, she stated, "While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling

that residents of this neighborhood share.” She also requested that “a separate study be conducted in possible land use of the former Kroger Fuel Center.” (See Appendix F: Correspondence and Meeting Minutes.)

On December 8, 2009, Indiana Landmarks—Northern Regional Office wrote in response to the HPR. Landmarks agreed that Brookview-Irvington Park Historic District is eligible for the NR and suggested modifications to the HPR recommendations in light of NR nominations being composed by ARCH, Inc. Indiana Landmarks also requested more information on the proposed design in order to comment on a preliminary effect finding. Indiana Landmarks disagreed with the APE, asked some preliminary questions regarding the purpose and need in relation to historic properties, questioned the appropriateness of including a “trail bridge” in this Section 106 investigation, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, and expressed the need for a “broad range of alternatives” to be included as part of the project options, and expressed concerns about the impacts of a different project on this Section 106 undertaking. (See Appendix F: Correspondence and Meeting Minutes.)

On December 9, 2009, ARCH, Inc. wrote in response to the HPR. Arch, Inc. agreed with the recommendation of eligibility for the Brookview-Irvington Park Historic District, noting that an NR nomination was being prepared. ARCH, Inc. requested the inclusion of proposed design maps, requested more detailed data regarding the project purpose and need, questioned the inclusion of the “trail bridge” in this Section 106 study, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, disagreed with the APE, stated the importance of consulting “early in the undertaking’s planning,” expressed concerns about the impacts of a different project on this Section 106 undertaking and specifically stated “we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate.” Finally, ARCH, Inc. agreed with statements regarding flooding in the area, but stated they “contend that this is an issue which is recent.”

In a letter dated December 10, 2009, Julie Donnell, president of the Friends of the Parks of Allen County, Inc. wrote in response to the meeting agenda and HPR. Donnell expressed concern over the project’s Section 106 process, including the concern “that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and that after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.” The letter also commented on the Brookview Neighborhood, concurring with other consulting party comments on the resource and posing questions regarding the project’s effects on the landscape, and expressed the integral importance of the landscape in the Brookview neighborhood’s integrity. The letter requested considering the inclusion of the Cultural Landscape Foundation in the Section 106 process. (See Appendix F: Correspondence and Meeting Minutes.)

On December 14, 2009, SHPO wrote in response to the Draft HPR. Regarding the APE, SHPO wrote that “we are not yet prepared to comment on the adequacy of the APE.” SHPO commented on the HPR in the same letter, stating, “[o]ur initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.” SHPO also wrote in response to the archaeological report that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [NR] within the area which was surveyed for this project by Archaeological Consultants of Ossian,” but noted that the final alignment was not yet determined and that further archaeological investigations may be necessary. SHPO asked for more

information on the project alignment and the purpose and need. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held December 15, 2009 in Fort Wayne, consulting parties expressed concern with the APE used in the HPR, noted the importance of the “park-like setting” to the Brookview neighborhood, and questioned the selection of alternatives. (See Appendix F: Correspondence and Meeting Minutes.)

On January 27, 2010, SHPO responded to minutes of the consulting party meeting held December 15, 2009. SHPO requested more information regarding the purpose and need but stated that perhaps their questions would be answered in the forthcoming information packet for consulting parties. SHPO expressed concern about the Purpose and Need of the project. SHPO also asked for “clarification” on “the substandard nature of the roadway curvature on State Boulevard,” especially in light of statements from consulting parties “that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard.” SHPO also stated “[w]e believe it is important for FHWA to evaluate this project’s purpose and need carefully before the Section 106 consultation proceeds much further. . . . Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.” Regarding the APE, SHPO asked some questions given the list of the alternatives provided at the December 15, 2009, consulting party meeting as well as in light of statements from consulting parties. “If . . . diversion of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.” SHPO also stated that “we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.” (See Appendix F: Correspondence and Meeting Minutes.)

SHPO wrote on March 10, 2010, in response to the revised meeting minutes from the December 15, 2009, meeting. In the letter, SHPO stated that the Spy Run Bridge had been finalized as a Non-Select, NR-eligible bridge per the Indiana Statewide Historic Bridge Inventory. SHPO restated the understanding that Arthur Shurcliff intended “that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses. . . that contribute to the Brookview-Irvington Park Historic District are demolished.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 15, 2011, Jill D. Downs, chairperson of the Preservation Committee of ARCH, Inc., wrote to the Deputy SHPO regarding Structurepoint’s May 19, 2011, letter. Downs questioned whether the revised Purpose and Need would “trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.” (See Appendix F: Correspondence.)

On June 16, 2011, John H. Shoaff wrote that as a member of the city council, they “face an unpleasant two-fold task of fighting for a properly democratic, participatory process...” (See Appendix F: Correspondence and Meeting Minutes.)

On June 16, 2012, Todd Zeiger, Indiana Landmarks sent an email asking for clarification of whether consulting parties were to comment on the May 19, 2012, letter and requesting a thirty day extension to the review period. (See Appendix F: Correspondence and Meeting Minutes.)

On June 17, 2011, Julie Donnell of the Friends of the Parks of Allen County sent an email to American Structurepoint conveying her letter dated June 14, 2011, in which she requested an additional thirty days of review. She expressed surprise that changes were made to Purpose and Need without “communicating this.” In the text of the email, Donnell wrote: “In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.” The email also said, “We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 1, 2011, John H. Shoaff wrote to point out discrepancies in traffic numbers presented. (See Appendix F: Correspondence and Meeting Minutes.)

On July 5, 2011, SHPO responded to Structurepoint’s letter of May 19, 2011. In their letter, SHPO wrote that it appeared appropriate to expand the APE “if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project” and stated foreseeable “areas where the character of use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well.” SHPO also requested Structurepoint review previous correspondence and meeting minutes and “make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 10 letter.” SHPO also suggested that Structurepoint share comments “that have been or shortly will be received in response to your May 19 and June 17 letters.” The letter re-stated comments from December 14, 2009, regarding the archaeology report. (See Appendix F: Correspondence and Meeting Minutes.)

Suzanne Slick, of the Irvington Park Neighborhood Association, sent an email on July 6, 2011, expressing disappointment with the project’s evaluation of impacts to neighborhood residents. The letter also stated, “There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 7, 2011, Michelle Briggs Wedaman of the Brookview Neighborhood Association emailed Structurepoint and asked that her email address be updated in the project record and that she would provide comments on behalf of the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

At an Agency Coordination meeting held July 13, 2011, SHPO suggested that Structurepoint coordinate to evaluate if the project would result in a need to change the NR district boundaries. SHPO also suggested that American Structurepoint more specifically address the consulting party issues and comments in coordination. It was also agreed upon that the ACHP should be invited to participate in the State Boulevard project at this stage in the Section 106 process, rather than later. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2011, Suzanne Slick wrote regarding the consulting party comment and response form. Slick wrote regarding the consultation process, “People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive.” Slick expressed concern with the proposed project and provided links to websites associated with



various aspects encountered in this project. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held September 1, 2011, consulting parties questioned the response process and whether all comments had been shared. Consulting parties were encouraged to respond to any Section 106 correspondence, even if the thirty day time period had passed. An effort would be made to post all Section 106 documentation on the City of Fort Wayne's website. Consulting parties suggested that the project include consultation with a professional landscape architect. It was also noted that the State Boulevard curve is included in the Fort Wayne Park and Boulevard System Historic District which is different from the Brookview-Irvington Historic District. SHPO requested the consultant "look at the implications of reduction the width of a new alignment. . .[and]. . . evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements." (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, at the Agency Meeting with FHWA and INDOT, FHWA stated it would follow-up on its invitation to the ACHP, noting that the ACHP's involvement in the process would be beneficial. During the meeting it was agreed that Structurepoint would provide consulting parties with a more elaborate alternatives analysis, would look into developing a Section 106 page for this project on the City of Fort Wayne's website, and that an addendum to the HPR would be prepared. (See Appendix F: Correspondence and Meeting Minutes.)

The ACHP responded to FHWA's invitation to join consultation on September 22, 2011. ACHP requested additional documentation in order to "determine whether our participation in the consultation to resolve adverse effects is warranted." (See Appendix F: Correspondence and Meeting Minutes.)

On November 7, 2011, SHPO responded to the material conveyed August 15, 2011, and September 29, 2011. Regarding the Brookview-Irvington Park Historic District, SHPO stated, "Having considered the marked aerial photograph shown at the last consulting party meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative." However, SHPO added, the proposed realignment of State Boulevard within the district "is not an ideal situation from a [NR] boundary delineation standpoint." Further, SHPO stated, "We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished." SHPO also offered additional comments from the September consulting party meeting that had not been recorded in the meeting minutes regarding the alternatives analysis. SHPO also questioned the feasibility of converting the existing Spy Run Bridge into a pedestrian bridge. SHPO stated they would also recommend, "where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden." SHPO also suggested shifting the proposed alignment somewhat to the east to better reflect Kessler's original plan for connecting State Boulevard. SHPO noted that this change may "result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along State Boulevard. Even if the project costs were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan." (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an Agency meeting was held to discuss the State Boulevard Project. At the meeting, Structurepoint reviewed the responses to the SHPO letter of November 7, 2011, and

agreed to send them in writing. It was decided to hold a meeting with consulting parties in early September to discuss the Additional Information HPR, to present the preferred alternative and to discuss the MOA. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of bridge over Spy Run; Restore character of State Blvd within the district; and Educational mitigation.

On June 22, 2012, SHPO provided comment on the AI Report. In the letter, SHPO stated, “we agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the [APE], for inclusion in the [NR].” SHPO agreed that the house at 315 East State Boulevard “does not appear to possess sufficient historical or architectural significance or integrity to be eligible of inclusion in the [NR].” SHPO also commented on the explanatory note contained in the Fort Wayne Park and Boulevard NR nomination form which stated the portion of State Boulevard within the Brookview-Irvington Historic District was individually eligible for the NR. SHPO stated, “we do not consider that comment . . . to confer individual eligibility on State Boulevard or any part of it.” SHPO further stated, “we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the [APE] is individually eligible” for the NR, but added “[w]e do not disagree, however, with the Fort Wayne Park and Boulevard system nomination identification of the portion of State Boulevard in question as a contributing resource to that historic district.” (See Appendix F: Correspondence and Meeting Minutes.)

Regarding archaeology, SHPO stated, “Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.” SHPO noted that one example of areas that may need archaeological survey included “a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report.” If the entire lot would need to be acquired as part of the project, “then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated July 31, 2012, the ACHP wrote that “[b]ased upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time.” However, the Council did request to be notified in the event of an Adverse Effect finding and at that time the Council would “re-evaluate the undertaking . . . and advise you whether or not we have changed our decision regarding participation in consultation.” (See Appendix F: Correspondence and Meeting Minutes.)

On August 13, 2012, the Indiana SHPO concurred with the archaeology short report (Stilwell, 7/11/12) that “no further investigations appear necessary at these additional portions of the project area” and that the office had not identified any archaeological resources listed or eligible for listing in the NR. (See Appendix F: Correspondence and Meeting Minutes.)

At the consulting party meeting held on September 19, 2012, consulting parties were asked to provide input into mitigation for the proposed undertaking. Most comments focused on purpose and need for the project; some spoke about traffic issues. Michelle Briggs Wedaman (Brookview Neighborhood Association) asked for context sensitive solutions at the beginning of the project rather than the end. Susan Haneline (property owner) asked why the owners of the three residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes. Todd Zeiger (Indiana Landmarks) encouraged the involvement of the ACHP because he feels that there was anticipatory demolition as part of a flood control project. He asked that it be noted in this documentation that there is a bifurcation of the district. Tom Cain (City of Fort Wayne) pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in

those landscape elements needs discussion in the documentation. The visual and special components of the larger landscape need to be understood so they can be addressed in a mitigation discussion. Michael Galbraith (ARCH, Inc.) encouraged ACHP involvement, objected to the change in historic consultant, asserted that the APE is inappropriate, and raised the question of cumulative impacts. Edward Welling (Friends of the Parks of Allen County) said that mitigation is premature since the APE is not appropriate; the MOA should be postponed until Environmental Assessment is complete. Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. Dr. James Glass (Deputy SHPO) expressed reservations that consensus can be developed for this project; he stated that this meeting was the time for consulting parties to put forth mitigation ideas. John Carr (SHPO staff) requested any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion. Mr. Galbraith objected to the timing of the consulting party meeting; Patrick Carpenter, manager of the INDOT-CRO, said that the timing was established so that consulting parties could discuss mitigation and formulate new ideas. Ms. Wedamen said that she did not believe that the public process has been followed. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 14, 2012, Karl Dietsch wrote regarding a safety issue in the proposed project area. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 17, 2012, eleven residents of the Brookview Neighborhood jointly submitted a letter regarding the State Boulevard project. The letter expressed support of the project. The residents stated, "We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated to the south of its current location" and went on to conclude, "We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issue unresolved." (See Appendix F: Correspondence and Meeting Minutes.)

Sara Kruger Geyman, a member of the public, wrote in response to the meeting held September 19, 2012. (Note that the letter conveying responses to the consulting party meetings was dated August 21, 2012, and is likely a typo.) Geyman expressed concern "that residents are not and have not been consulted in this matter" and expressed dissatisfaction with meeting's facilitation. Geyman offered comments to the project in general, objecting to its necessity and, regarding Section 106, stating: "Mitigation is premature in a plan and a process that has been faulty from the beginning. It is a proverbial lollipop stuck in the hands of resident to quiet them down and distract them from the truth." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 1, 2012, Susan R. Haneline, a Brookview neighborhood home owner, expressed support for the project, noting that the current problems with flooding and bridge deterioration "do nothing to showcase what IS historical about the neighborhood." Haneline added, "We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood." Haneline's letter also included photographs showing recent flooding in the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

Susan Haneline submitted an additional letter dated October 2, 2012. Haneline stated the current proposed design, "seems . . . to actually enhance historic vision, not cause it to be destroyed." Haneline offered suggestions to "respect the historic vision," including: 1.) "Installing historically correct lighting in the area"; 2.) "Plantings and green space that gives the area a park like feel, such as period style benches, grouping of trees and flowers, perhaps even brick style side walks"; 3.) "stone or brick entrance pillars for the neighborhood"; 4.) adding trees and flower beds to the bifurcated State Boulevard; 5.) "small monuments" conveying the history of the neighborhood and

Arthur Shurcliff; 6.) “find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 3, 2012, John Shoaff wrote regarding the project, consulting party meeting, and 800.11 material. Shoaff wrote, “I cannot support the current State Boulevard widening plan in anything like its present form. . .” In particular, Shoaff objected to plans to elevate the road as a “perversion of the proper use of the ‘By-pass and Arterial concept’ . . .” Shoaff identified “two legitimate needs” in the Brookview neighborhood: the repair or replacement of the Bridge over Spy Run Creek and the elimination of a “blind spot at the foot of State Boulevard, near the intersection with Westbrook.” Shoaff stated that project plans should address these needs but be “minimally harmful to the historic district.” Shoaff added that discussion of project planning and mitigation discussion “should await the outcome of the Environmental Assessment.” (See Appendix F: Correspondence and Meeting Minutes.)

Shoaff also included comments on the September 19, 2012, consulting party meeting. Shoaff responded to comments received by Michelle Briggs Wedaman from FHWA’s representative. Shoaff objected to the facilitation of the meeting stating “the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.”

Shoaff enclosed letter “signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen.” The letter objected to the State Boulevard project. (See Appendix F: Correspondence and Meeting Minutes.)

Also on October 3, 2012, Suzanne Slick wrote regarding the project and the consulting party meeting of September 19, 2012. Slick stated that not building the project is preferable to mitigation and objected to the facilitation of the consulting party meeting. The letter re-stated some comments offered previously by consulting parties regarding the Purpose and Need and design. Slick objected to the traffic data previously supplied by Structurepoint and offered two examples in which she found low-volume traffic while utilizing the State Boulevard. Slick stated the APE was inappropriate. (See Appendix F: Correspondence and Meeting Minutes.)

Julie Downs, Friends of the Parks of Allen County, submitted comments via a letter dated October 3, 2012. Downs stated the Friends of the Parks of Allen County agreed with the finding of adverse effect for the project but added “any discussion of mitigation is, at best, premature; at worst, the proposed [MOA] is a bad faith attempt to confuse an already complicated and unfair process.” Downs also stated the “APE is not comprehensive enough and should include historic districts along State Boulevard” and “it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.” Finally, on behalf of members of the Friends of the Parks of Allen County who attended the September 19, 2012, consulting party meeting, Downs objected to the facilitation of the meeting and concluded, “Under these circumstances, the public is not being served properly at all.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Jill Downs wrote regarding the 800.11(e) and draft MOA. Downs agreed with the project’s adverse effect finding but noted “the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration” and pointed to the September 19, 2012, consulting party meeting as proof of this breakdown. She stated it was premature to discuss mitigation because the Environmental Assessment had not been completed; the bifurcation of the district, elevation of State Boulevard, and the Pufferbelly Trail project should be added to the list of adverse effects; the Pufferbelly Trail project should be incorporated into the effects discussion; and the project has not fully accounted for the previous removal of several homes by the City of Fort Wayne which creates the impression of less impact

as a result of the project. Downs concluded by stating she did not see the need to reconstruct State Boulevard. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michael Galbraith of ARCH, Inc., wrote formally requesting an extension of the thirty-day comment period for the proposed MOA and mitigation measures. Galbraith stated, "We do not in any form, fashion, or manner concur with the proposed mitigation as present either in the draft supplied with the FHWA 4(f) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintraut." Galbraith also stated that "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact." (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michelle Briggs Wedaman of the Brookview Neighborhood Association, wrote requesting a thirty-day extension of the consulting party comment period to incorporate the material provided on September 18, 2012, into their comments. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Wedaman stated that previous questions from the December 2009 and September 2011 consulting party meetings "have remained unanswered," particularly those dealing "Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the area of impact of this project." Wedaman questioned how an appropriate discussion of mitigation could take place prior to the completion of the environmental assessment. (See Appendix F: Correspondence and Meeting Minutes.)

The SHPO wrote in response to the project in a letter dated October 4, 2012. SHPO concurred with the opinion of the archaeological short report, the Section 106 finding of effect and that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and Bridge on State Boulevard over Spy Run would all be adversely affected as part of this undertaking. SHPO expressed concern "about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District and suggested some minimization measures. In particular, SHPO wondered if "it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive." SHPO expressed sympathy for the preference of some property owners along the south side of State Boulevard who preferred to have their entire property, rather than a smaller portion, purchased, "However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect."

SHPO also offered suggestions for design for minimizing impacts and suggestions for mitigation, including an advisory team, use of context-sensitive designs, photographic documentation of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Todd Zeiger of Indiana Landmarks—Northern Regional Office wrote formally requesting a thirty-day extension on the comment period in light of the material conveyed September 18, 2012. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Zeiger stated "We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(f) compliance document." Zeiger added "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that

information and assess it within the context of the other informant provided in the 4(F) document.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Tom Cain, Fort Wayne urban designer and Creager Smith, Fort Wayne historic preservation planner, wrote regarding the project. Both agreed with the project’s adverse effect finding. The letter listed twenty-one specific adverse effects of the project on the landscape to serve as the “potential basis of mitigation measures.” Cain and Smith also stated “we are available to assist in the development of mitigation design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on a team.” (See Appendix F: Correspondence and Meeting Minutes.)

On October 15, 2012, Tom Cain, City of Fort Wayne, called W&A to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but wanted to make suggestions within the context of SHPO’s assessment of project impacts so that the City may address all adverse effects. Cain also stated that impacts to the Brookview neighborhood should be enumerated. (See Appendix F: Correspondence and Meeting Minutes.)

On October 16, 2012, W&A contacted Tom Cain in response to his phone call the previous day. W&A explained that Structurepoint was very glad to have his input on this project and, at a minimum, would consult with him prior to the agency meeting. Cain spoke about the landscape changes that would take place as a result of the undertaking, particularly the changes from private to public space around the undertaking. He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s. Cain stated he would like for mitigation to deal with changes in scale that will occur; tree planting should occur within three feet of the roadway (and not the standard ten feet required on highways.) Cain stated this would change the scale of the undertaking for the residents. Cain also stated he would convey additional mitigation suggestions via email and stated the importance of achieving the “right feel” for the space. (See Appendix F: Correspondence and Meeting Minutes.)

On November 15, 2012, SHPO wrote in response to Structurepoint’s offer to draft specific language for the MOA. (See Appendix F: Correspondence and Meeting Minutes.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain (landscape architect/City of Fort Wayne) made the presentation. Cain’s plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A, Plans.) Dr. James Glass (SHPO) expressed appreciation for the effort Mr. Cain had put forth for a thoughtful landscape plan. Dr. Glass said that his office needed time to digest but that he understood Mr. Cain’s point that in a Section 106 sense, there was a need to mitigate for the houses and for the loss of historic character. He also understood that there are larger issues of flood control and engineering that make this project difficult. There was discussion of other resources that may be preserved as far as compensation for the lost historic resources (houses and landscaping). It was agreed that SHPO would be given time to digest the landscape design presented at the meeting and that the City and its consultants would look for additional ways to mitigate, such as grants to rehabilitate the facades of existing houses (if practical and legally

viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. Mary Ann Naber (FHWA preservation officer) suggested that the attendees look at the mitigation provided in Tampa. (See Appendix F: Correspondence and Meeting Minutes.)

No other comments were received.

A public notice of Adverse Effect will be posted in a local newspaper and the public afforded thirty (30) days to comment. If appropriate, this document will be revised to reflect those comments.

## **APPENDIX A. Plans**



PROJECT	DESIGNATION
1005151	1005151
CONTRACT	BRIDGE FILE

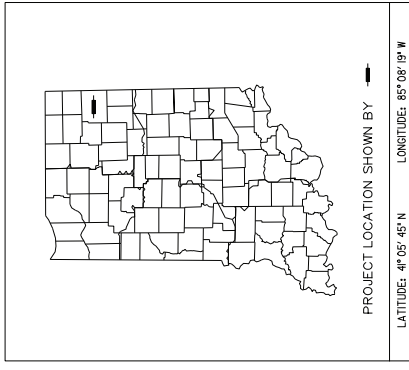
# INDIANA DEPARTMENT OF TRANSPORTATION

## ROAD PLANS

STATE BLVD. RECONSTRUCTION - PART 1  
PROJECT NO. 1005151 P.E.  
PROJECT NO. R/W  
PROJECT NO. CONST.

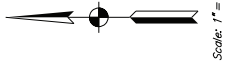
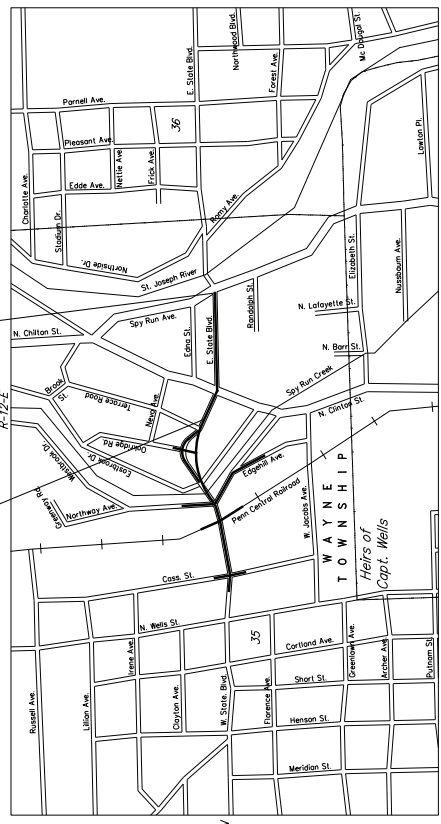
Project Description: Road Reconstruction on State Boulevard Beginning 130.00 Feet East of the Intersection of State Boulevard and Terrace Road, Thence East 999.00 Feet Along Line "PR-A" to the Intersection of Spy Run Ave. and State Boulevard, All in Section 35, Township 31 North, Range 12 East, Wayne Township, Allen County, Indiana

TRAFFIC DATA	STATE BLVD. - LINE "PR-A"
A.A.D.T. (2009)	20,650 V.P.D.
A.A.D.T. (2030)	25,000 V.P.D.
DIRECTIONAL DISTRIBUTION	50% A.P.D.
TRUCKS	60%
MAINLINE EB/AB	2
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Minor Arterial
DESIGN TYPE	Urban (Built-Up)
ACCESS CONTROL	None



BRIDGE LENGTH :	N/A Miles
ROADWAY LENGTH :	0.89 Miles
TOTAL LENGTH :	0.89 Miles
MAX. GRADE :	1.50 %

HYDROLOGIC UNIT CODE: 0410000406060



ALLEN COUNTY

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2010  
TO BE USED WITH THESE PLANS

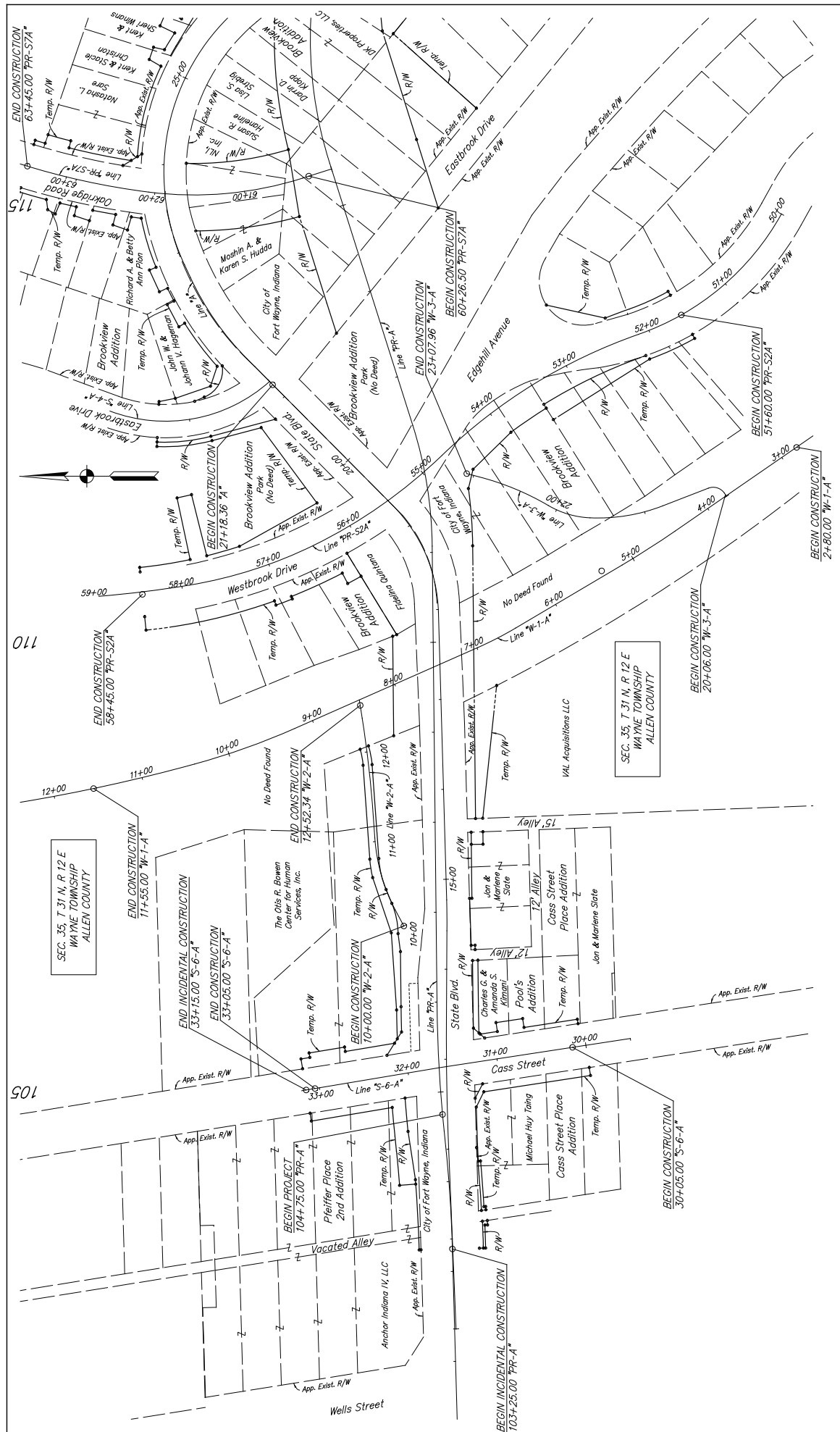
PLANS PREPARED BY: American Structurepoint, Inc.	(317) 547-5580
CERTIFIED BY:	PHONE NUMBER
APPROVED	DATE
FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION
	DATE

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005151
CONTRACT	SHEETS
	1 of 89
	PROJECT NO.
	1005151

2500 SHADYLAND STATION  
INDIANAPOLIS, IN 46226  
TEL 317.547.5580 FAX 317.543.0270  
www.structurepoint.com

**STRUCTUREPOINT**  
S.P.C.





INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGNATION NO.	
DESIGNED: ASU		1005151	
DRAWN: PJT		SURVEY BOOK	
CHECKED: SMC		3	
DATE		PROJECT NO.	
DATE		1005151	
PLAT NO. 1		SHEETS	
		89	
		CONTRACT	
		1005151	

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SEC. 35, T. 31 N., R. 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION  PLAT NO. 1	
DESIGNED: ASU		DRAWN: PJT					
CHECKED: SMC		CHECKED: SMC					
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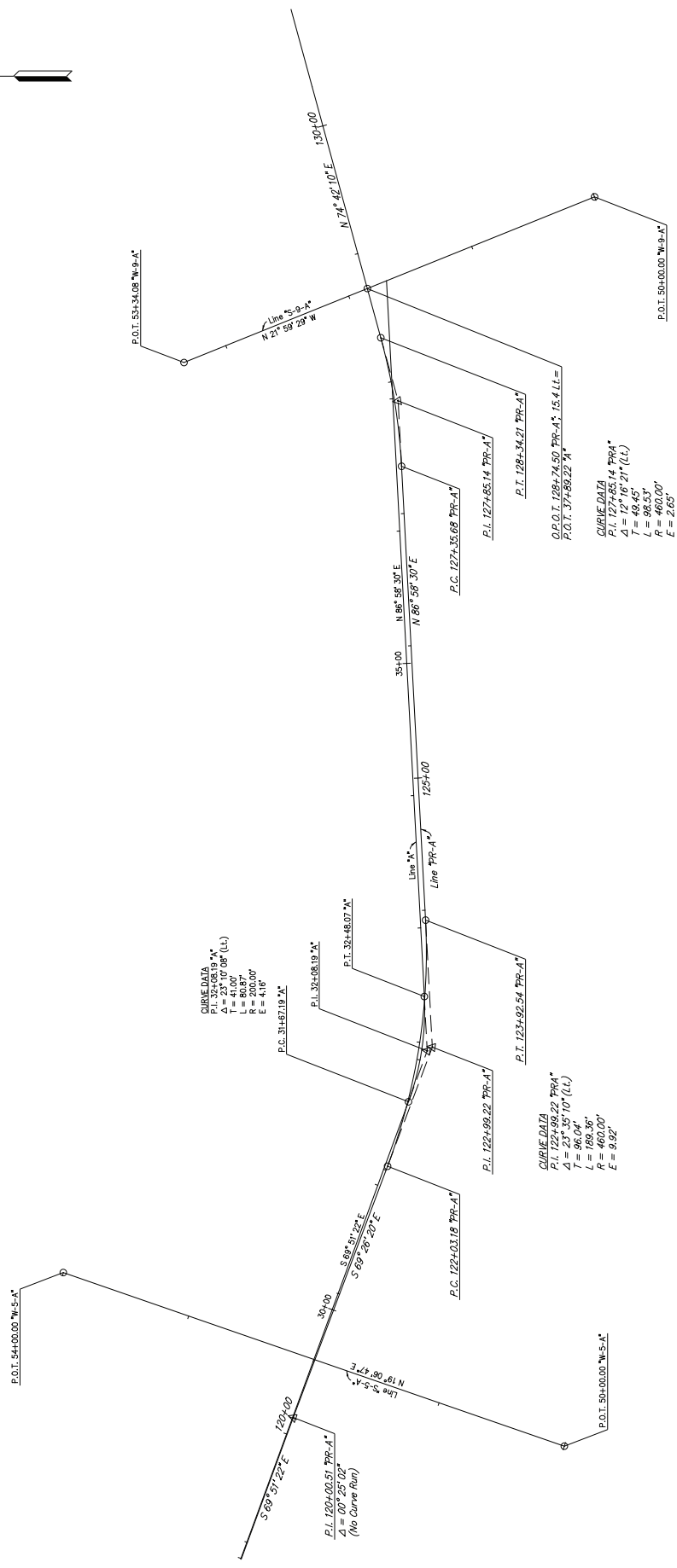




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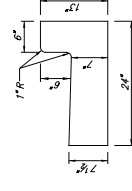
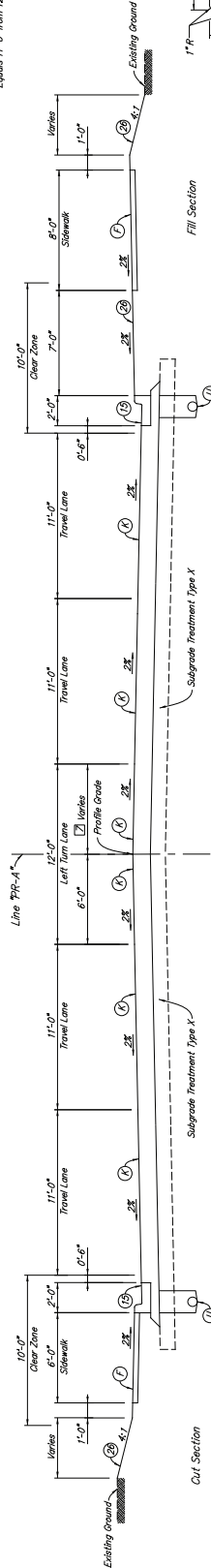
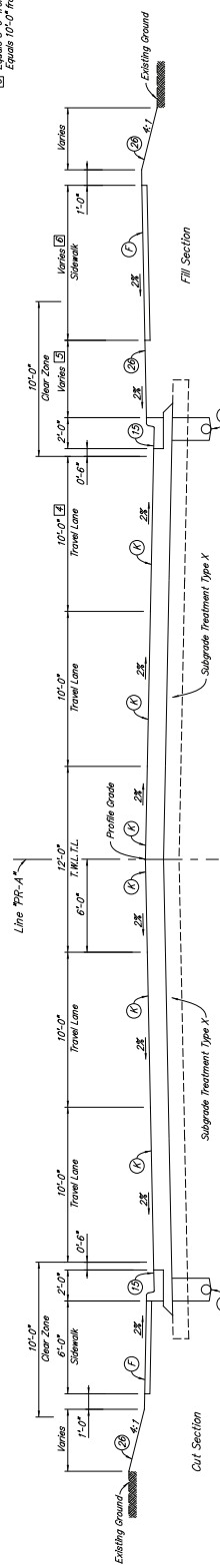
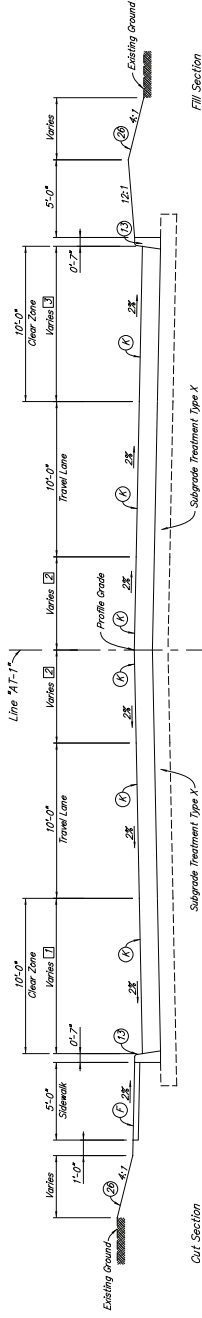
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DATE		SHEETS	
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CHECKED: SMC		PROJECT NO.	
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		7 of 89	
		1005151	



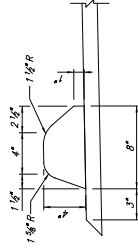
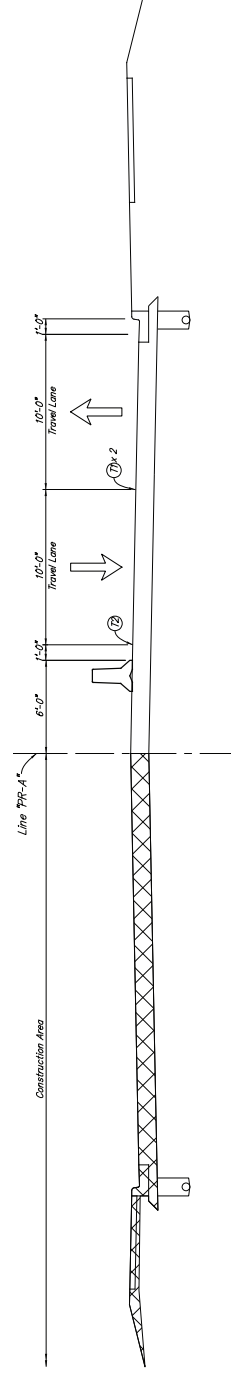
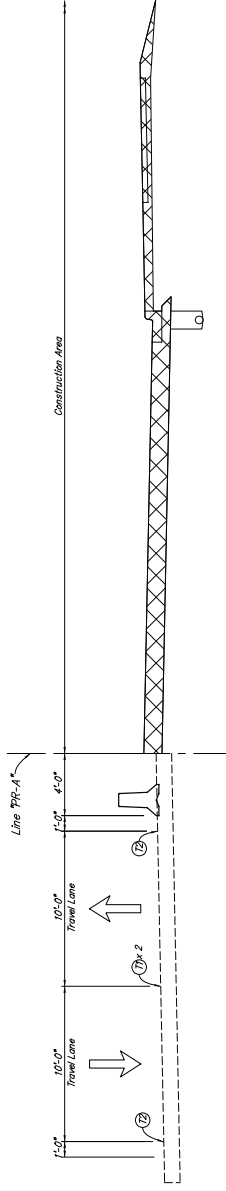
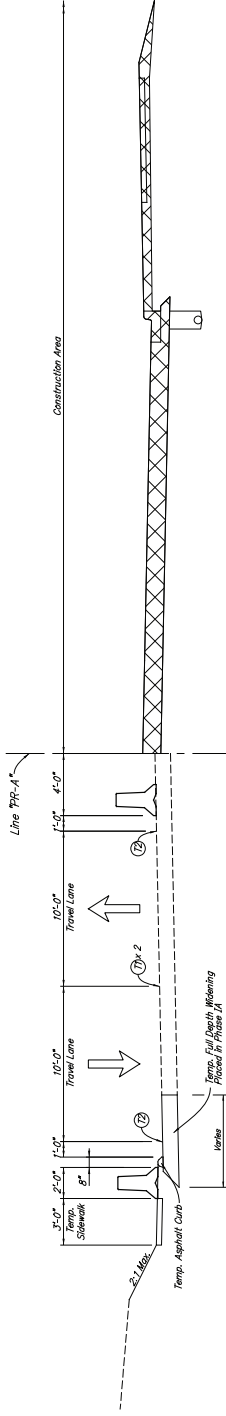








<div>LEGEND</div> <div><div>①</div><div><i>xxx #5/Std. CO/OH-HMA, x, xx, Surface, xx, on</i> <i>xxx #5/Std. CO/OH-HMA, x, xx, Intermediate, xx, on</i> <i>xxx #5/Std. CO/OH-HMA, x, xx, Base, xx, on</i> <i>xxx #5/Std. CO/OH-HMA, x, xx, Intermediate OG, 15mm on</i> <i>xxx #5/Std. CO/OH-HMA, x, xx, Base, xx</i></div></div> <div><div>②</div><div><i>Concrete Slabwork, 4"</i></div></div>		<div>③</div> <div><i>15# #10/Std. HMA Surface, Type "A", on</i> <i>27# #5/Std. HMA Intermediate, Type "A", on</i> <i>6" Compacted Aggregate, No. 5.3 Base</i></div>		<div>④</div> <div><i>Concrete Center Curb, Type "D"</i></div>		<div>⑤</div> <div><i>Sodding Nursery</i></div>		<div>⑥</div> <div><i>Unbound Seeding, Type "B"</i></div>		<div>⑦</div> <div><i>Combined Concrete Curb and Gutter, Modified</i> <i>xxx #5/Std. CO/OH-HMA, x, xx, Base, xx</i></div>	
<div>⑧</div> <div><i>Retaining Wall</i></div>		INDIANA		DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 1'-0"		VERTICAL SCALE 1" = 1'-0"		BRIDGE FILE	
RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____		DATE _____		SURVEY BOOK _____		SHEETS _____		DESIGNATION NO. 100551	
DESIGNED: ASU _____		DRAWN: OAK _____		CHECKED: SMC _____		CHECKED: SMC _____		CONTRACT _____		PROG. NO. 89	
						LINE "PR-A"				100551	







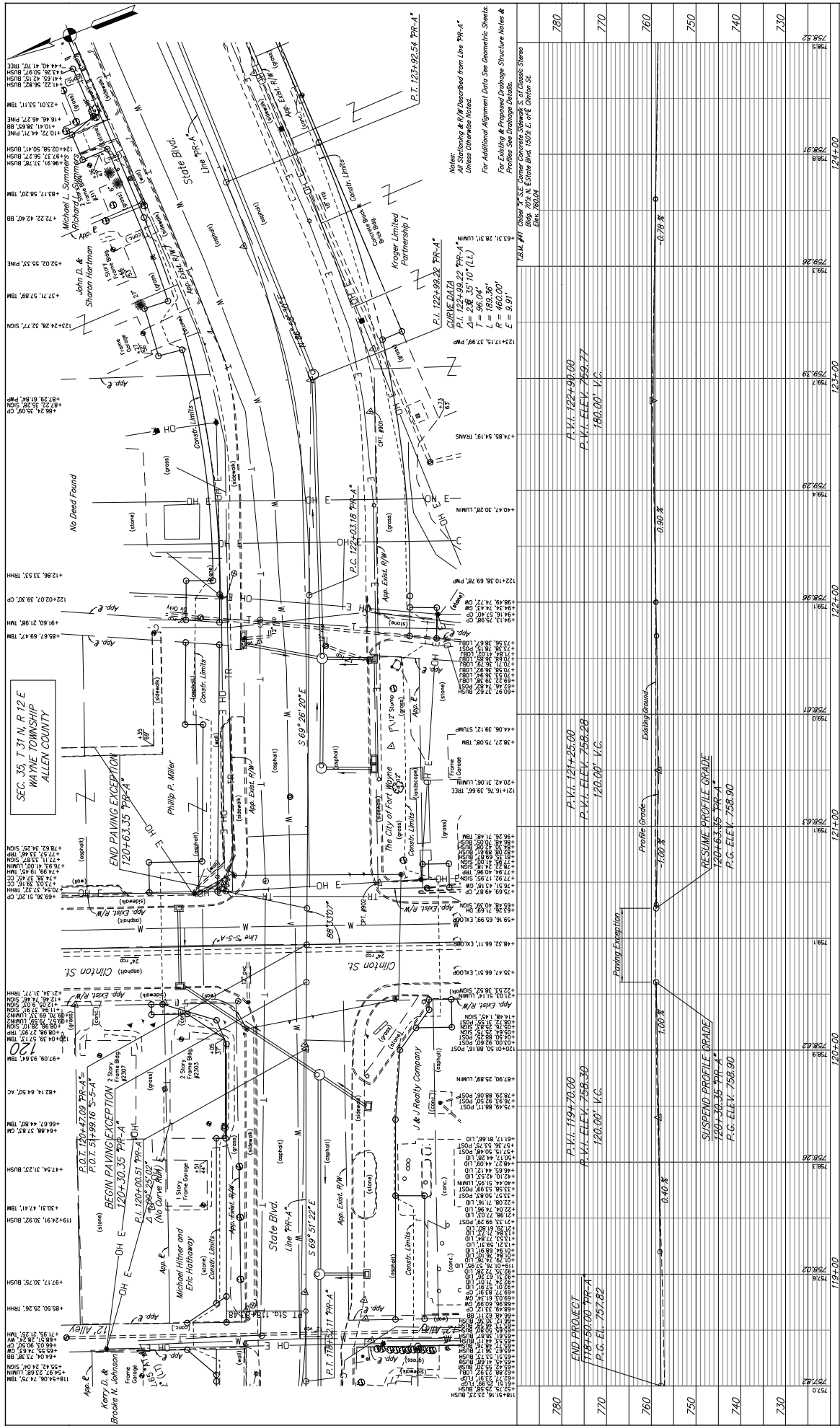
ASPHALT CURB (MOUNTABLE) DETAIL

 Temporary Concrete Barrier		 Standard Drum		 Direction of Traffic		<b>LEGEND</b> ⑦ Temporary Pavement Marking, Solid, Yellow, 4 in. ⑧ Temporary Pavement Marking, Solid, White, 4 in.  Construction Area		<div> <div> <div>RECOMMENDED FOR APPROVAL</div> <div>DESIGN ENGINEER</div> <div>DATE</div> </div> <div> <div>DESIGNED: ASU</div> <div>DRAWN: CMK</div> </div> </div> <div> <div>CHECKED: SMC</div> <div>CHECKED: SMC</div> </div>		<div>INDIANA</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>MAINTENANCE OF TRAFFIC</div> <div>TYPICAL CROSS SECTIONS</div>		<div> <div>HORIZONTAL SCALE</div> <div>VERTICAL SCALE</div> </div> <div> <div>SURVEY BOOK</div> <div>CONTRACT</div> </div> <div> <div>SHEETS 12 of 89</div> <div>PROJECT NO. 1005151</div> </div> <div> <div>BRIDGE FILE</div> <div>DESIGNATION NO. 1005151</div> </div>	
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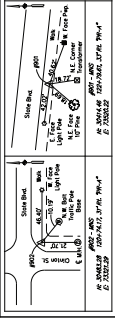
*Notes: HMA Surface to be Placed Following Phase IV Under Live Traffic*

scribes 7/12/2011 12:44:48 PM P:\N2007\1404\0. Drawings\20071404.RD.P1.P1.Y.01.dgn





INDIANA				DEPARTMENT OF TRANSPORTATION				PLAN & PROFILE			
RECOMMENDED FOR APPROVAL				DESIGN ENGINEER				CONTRACT			
DATE				DRAWN				SHEETS			
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SMC				SMC				89			
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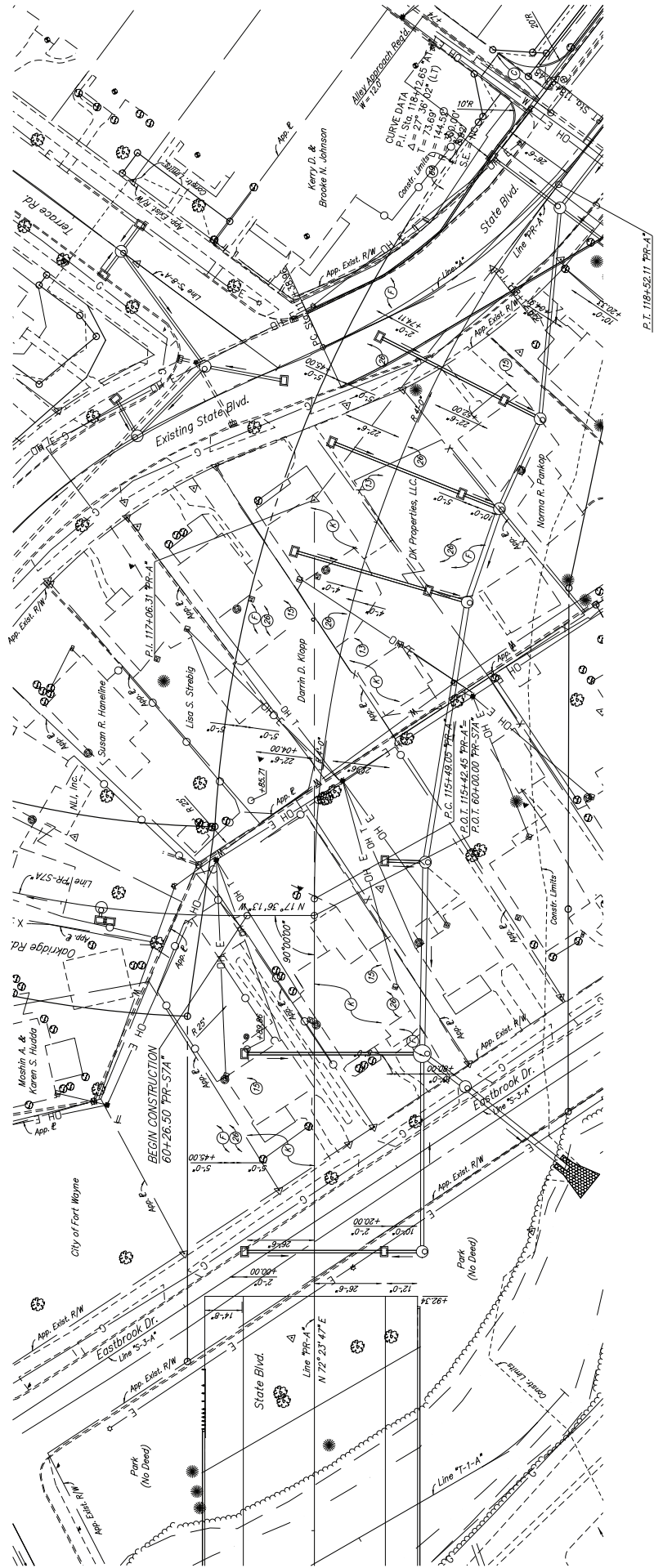








SEC. 35, T 31 N, R 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY

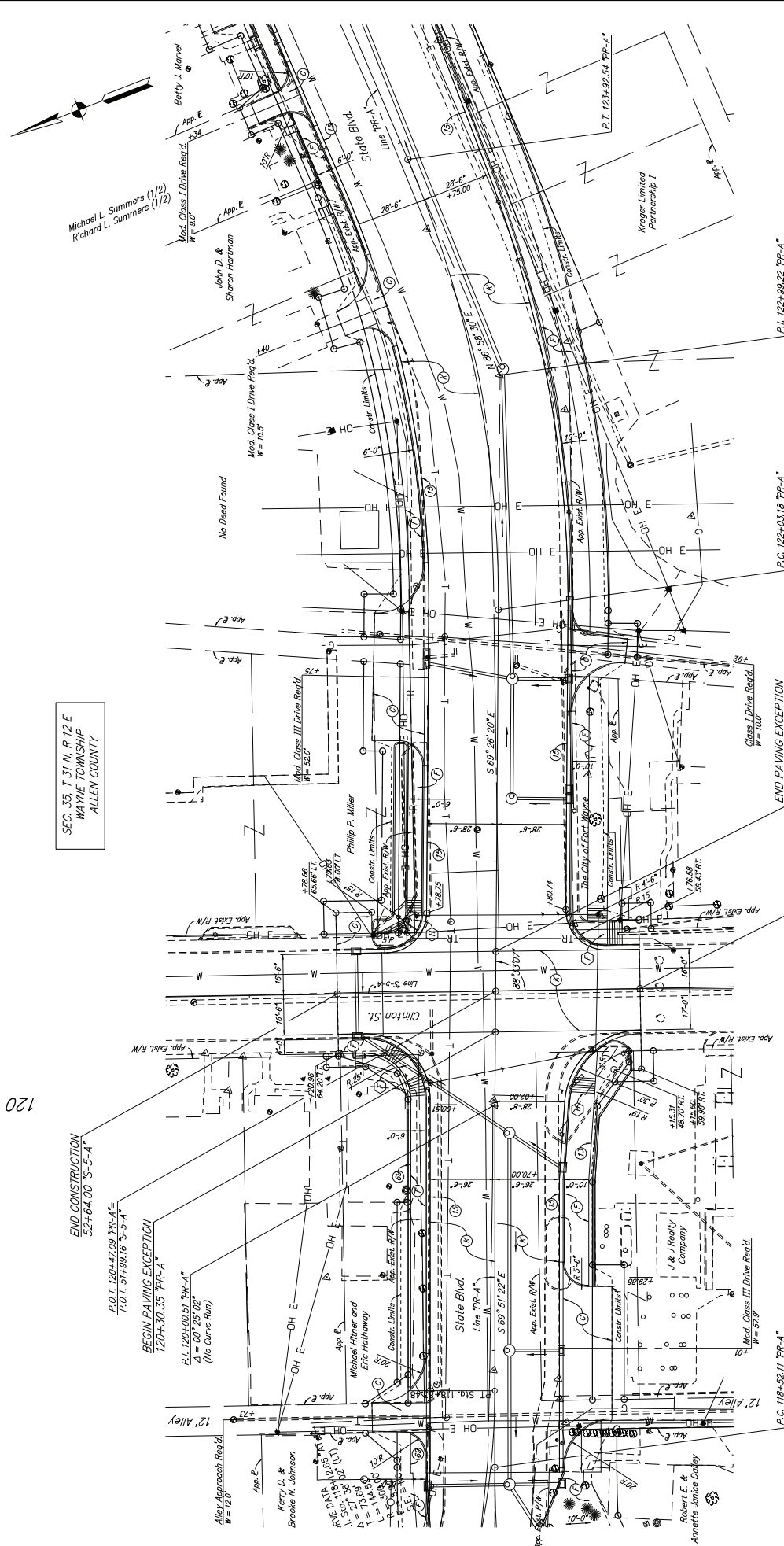


Notes:  
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p><b>LEGEND</b></p> <p>(7) 105 #/Syd. HMA Surface, Type "A" on 275 #/Syd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 53, Base</p> <p>(8) 105 #/Syd. HMA for Approaches Type "A", on 6" Compacted Aggregate, No. 53, Base</p> <p>(9) Concrete Slabways, "A"</p> <p>(10) Concrete Curb</p> <p>(11) Combined Concrete Curb and Gutter, Modified</p> <p>(12) Retaining Wall</p> <p>(13) Concrete Curb Ramp</p> <p>(14) Concrete Center Cuts, Type "D"</p> <p>(15) Siding, Nursery</p> <p>(16) Full Depth Pavement (See Typical Sections)</p> <p>(17) Compacted Aggregate for Shoulder</p> <p>(18) Concrete Slabways, "A"</p> <p>(19) Concrete Curb</p> <p>(20) Combined Concrete Curb and Gutter, Modified</p> <p>(21) Retaining Wall</p> <p>(22) Concrete Curb Ramp</p> <p>(23) Concrete Center Cuts, Type "D"</p> <p>(24) Siding, Nursery</p> <p>(25) Full Depth Pavement (See Typical Sections)</p> <p>(26) Concrete Slabways, "A"</p> <p>(27) Concrete Curb</p> <p>(28) Combined Concrete Curb and Gutter, Modified</p> <p>(29) Retaining Wall</p> <p>(30) Concrete Curb Ramp</p> <p>(31) Concrete Center Cuts, Type "D"</p> <p>(32) Siding, Nursery</p> <p>(33) Full Depth Pavement (See Typical Sections)</p> <p>(34) Concrete Slabways, "A"</p> <p>(35) Concrete Curb</p> <p>(36) Combined Concrete Curb and Gutter, Modified</p> <p>(37) Retaining Wall</p> <p>(38) Concrete Curb Ramp</p> <p>(39) Concrete Center Cuts, Type "D"</p> <p>(40) Siding, Nursery</p> <p>(41) Full Depth Pavement (See Typical Sections)</p> <p>(42) Concrete Slabways, "A"</p> <p>(43) Concrete Curb</p> <p>(44) Combined Concrete Curb and Gutter, Modified</p> <p>(45) Retaining Wall</p> <p>(46) Concrete Curb Ramp</p> <p>(47) Concrete Center Cuts, Type "D"</p> <p>(48) Siding, Nursery</p> <p>(49) Full Depth Pavement (See Typical Sections)</p> <p>(50) Concrete Slabways, "A"</p> <p>(51) Concrete Curb</p> <p>(52) Combined Concrete Curb and Gutter, Modified</p> <p>(53) Retaining Wall</p> <p>(54) Concrete Curb Ramp</p> <p>(55) Concrete Center Cuts, Type "D"</p> <p>(56) Siding, Nursery</p> <p>(57) Full Depth Pavement (See Typical Sections)</p> <p>(58) Concrete Slabways, "A"</p> <p>(59) Concrete Curb</p> <p>(60) 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Ramp</p> <p>(87) Concrete Center Cuts, Type "D"</p> <p>(88) Siding, Nursery</p> <p>(89) Full Depth Pavement (See Typical Sections)</p> <p>(90) Concrete Slabways, "A"</p> <p>(91) Concrete Curb</p> <p>(92) Combined Concrete Curb and Gutter, Modified</p> <p>(93) Retaining Wall</p> <p>(94) Concrete Curb Ramp</p> <p>(95) Concrete Center Cuts, Type "D"</p> <p>(96) Siding, Nursery</p> <p>(97) Full Depth Pavement (See Typical Sections)</p> <p>(98) Concrete Slabways, "A"</p> <p>(99) Concrete Curb</p> <p>(100) Combined Concrete Curb and Gutter, Modified</p> <p>(101) Retaining Wall</p> <p>(102) Concrete Curb Ramp</p> <p>(103) Concrete Center Cuts, Type "D"</p> <p>(104) Siding, Nursery</p> <p>(105) Full Depth Pavement (See Typical Sections)</p> <p>(106) Concrete Slabways, "A"</p> <p>(107) Concrete Curb</p> <p>(108) Combined Concrete Curb and Gutter, Modified</p> <p>(109) Retaining Wall</p> <p>(110) Concrete Curb Ramp</p> <p>(111) Concrete Center Cuts, Type "D"</p> <p>(112) Siding, Nursery</p> 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Curb and Gutter, Modified</p> <p>(165) Retaining Wall</p> <p>(166) Concrete Curb Ramp</p> <p>(167) Concrete Center Cuts, Type "D"</p> <p>(168) Siding, Nursery</p> <p>(169) Full Depth Pavement (See Typical Sections)</p> <p>(170) Concrete Slabways, "A"</p> <p>(171) Concrete Curb</p> <p>(172) Combined Concrete Curb and Gutter, Modified</p> <p>(173) Retaining Wall</p> <p>(174) Concrete Curb Ramp</p> <p>(175) Concrete Center Cuts, Type "D"</p> <p>(176) Siding, Nursery</p> <p>(177) Full Depth Pavement (See Typical Sections)</p> <p>(178) Concrete Slabways, "A"</p> <p>(179) Concrete Curb</p> <p>(180) Combined Concrete Curb and Gutter, Modified</p> <p>(181) Retaining Wall</p> <p>(182) Concrete Curb Ramp</p> <p>(183) Concrete Center Cuts, Type "D"</p> <p>(184) Siding, Nursery</p> <p>(185) Full Depth Pavement (See Typical Sections)</p> <p>(186) Concrete Slabways, "A"</p> <p>(187) Concrete Curb</p> <p>(188) Combined Concrete Curb and Gutter, Modified</p> <p>(189) Retaining Wall</p> <p>(190) Concrete Curb Ramp</p> <p>(191) Concrete Center Cuts, Type "D"</p> <p>(192) Siding, Nursery</p> <p>(193) Full Depth Pavement (See Typical Sections)</p> <p>(194) Concrete Slabways, "A"</p> <p>(195) Concrete Curb</p> <p>(196) Combined Concrete Curb and Gutter, Modified</p> <p>(197) Retaining Wall</p> <p>(198) Concrete Curb Ramp</p> <p>(199) Concrete Center Cuts, Type "D"</p> <p>(200) Siding, Nursery</p> <p>(201) Full Depth Pavement (See Typical Sections)</p> <p>(202) Concrete Slabways, "A"</p> <p>(203) Concrete Curb</p> <p>(204) Combined Concrete Curb and Gutter, Modified</p> <p>(205) Retaining Wall</p> <p>(206) Concrete Curb Ramp</p> <p>(207) Concrete Center Cuts, Type "D"</p> <p>(208) Siding, Nursery</p> <p>(209) Full Depth Pavement (See Typical Sections)</p> <p>(210) Concrete Slabways, "A"</p> <p>(211) Concrete Curb</p> <p>(212) Combined Concrete Curb and Gutter, Modified</p> <p>(213) Retaining Wall</p> <p>(214) Concrete Curb Ramp</p> <p>(215) Concrete Center Cuts, Type "D"</p> <p>(216) Siding, Nursery</p> <p>(217) Full Depth Pavement (See Typical Sections)</p> <p>(218) Concrete Slabways, "A"</p> <p>(219) Concrete Curb</p> <p>(220) Combined Concrete Curb and Gutter, Modified</p> <p>(221) Retaining Wall</p> <p>(222) Concrete Curb Ramp</p> <p>(223) Concrete Center Cuts, Type "D"</p> <p>(224) Siding, Nursery</p> <p>(225) Full Depth Pavement (See Typical Sections)</p> <p>(226) Concrete Slabways, "A"</p> <p>(227) Concrete Curb</p> <p>(228) Combined Concrete Curb and Gutter, Modified</p> <p>(229) Retaining Wall</p> <p>(230) Concrete Curb Ramp</p> <p>(231) Concrete Center Cuts, Type "D"</p> <p>(232) Siding, Nursery</p> <p>(233) Full Depth Pavement (See Typical Sections)</p> <p>(234) Concrete Slabways, "A"</p> <p>(235) Concrete Curb</p> <p>(236) Combined Concrete Curb and Gutter, Modified</p> <p>(237) Retaining Wall</p> <p>(238) Concrete Curb Ramp</p> <p>(239) Concrete Center Cuts, Type "D"</p> <p>(240) Siding, Nursery</p> <p>(241) Full Depth Pavement (See Typical Sections)</p> <p>(242) Concrete Slabways, "A"</p> <p>(243) Concrete Curb</p> <p>(244) Combined Concrete Curb and Gutter, Modified</p> <p>(245) Retaining Wall</p> <p>(246) Concrete Curb Ramp</p> <p>(247) Concrete Center Cuts, Type "D"</p> <p>(248) Siding, Nursery</p> <p>(249) Full Depth Pavement (See Typical Sections)</p> <p>(250) Concrete Slabways, "A"</p> <p>(251) Concrete Curb</p> <p>(252) Combined Concrete Curb and Gutter, Modified</p> <p>(253) Retaining Wall</p> <p>(254) Concrete Curb Ramp</p> <p>(255) Concrete Center Cuts, Type "D"</p> <p>(256) Siding, Nursery</p> <p>(257) Full Depth Pavement (See Typical Sections)</p> <p>(258) Concrete Slabways, "A"</p> <p>(259) Concrete Curb</p> <p>(260) Combined Concrete Curb and Gutter, Modified</p> <p>(261) Retaining Wall</p> <p>(262) Concrete Curb Ramp</p> <p>(263) Concrete Center Cuts, Type "D"</p> <p>(264) Siding, Nursery</p> <p>(265) Full Depth Pavement (See Typical Sections)</p> <p>(266) Concrete Slabways, "A"</p>	
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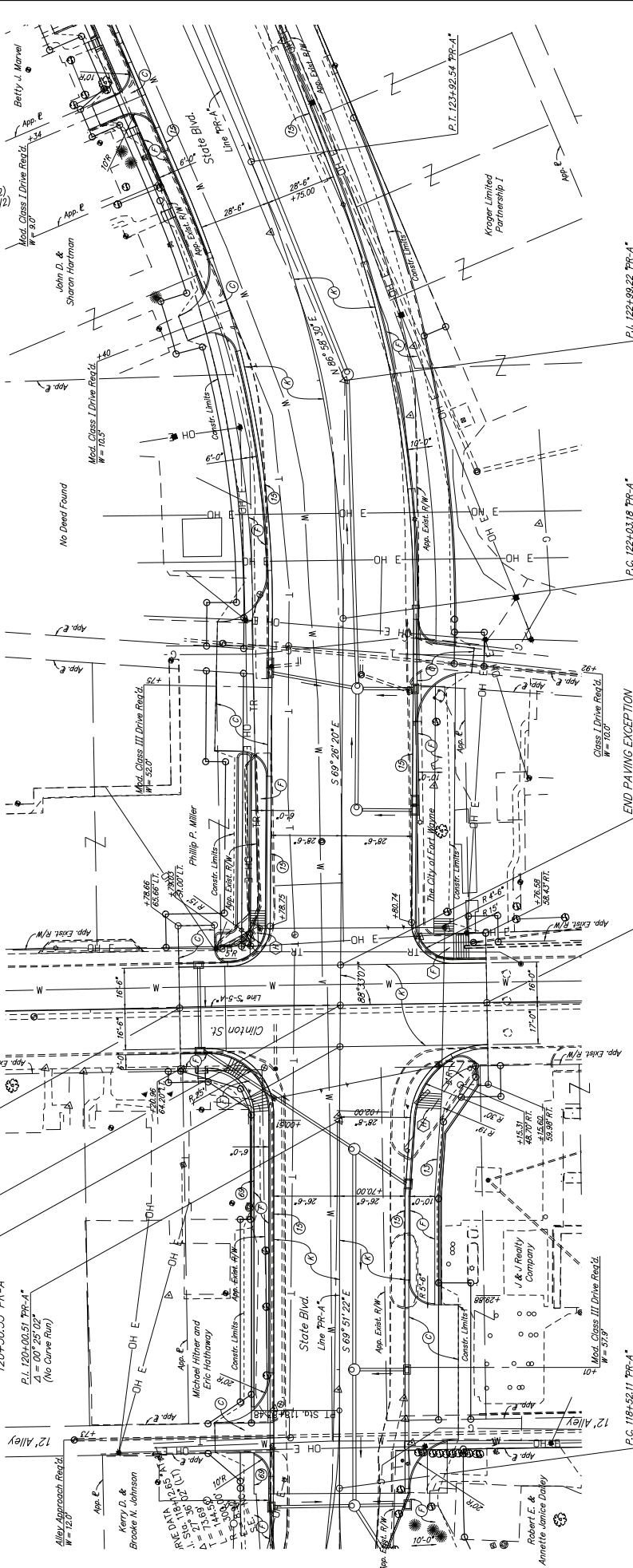
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120



SEC. 35, T. 31 N. R. 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY

Michael L. Summers (1/2)  
Richard L. Summers (1/2)



Notes:  
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Indicated.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

CURVE DATA  
P.L. 120+99.22 PR-A  
T = 65.35' (11' (L))  
L = 96.04'  
R = 183.36'  
E = 460.00'  
E = 9.92'

INDIANA  
DEPARTMENT OF TRANSPORTATION  
CONSTRUCTION DETAILS  
LINE "PR-A"

RECOMMENDED FOR APPROVAL  
DESIGN ENGINEER  
DATE

DESIGNED: ASU  
DRAWN: CAK  
CHECKED: SMC

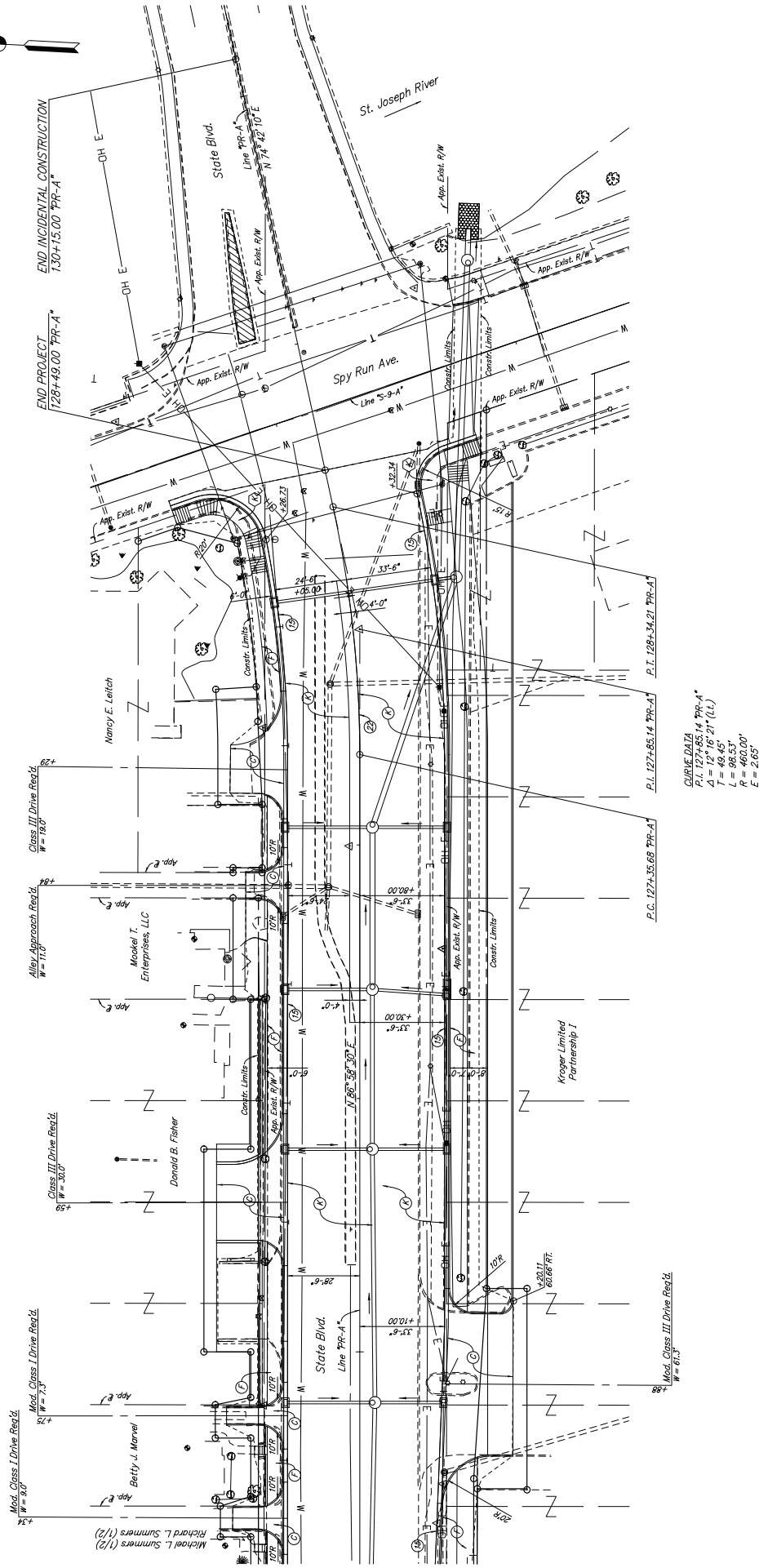
LEGEND

- 1. Cement Concrete Pavement for Driveways
- 2. 275 #/yd. HMA Surface, Type "A", on 6" Compacted Aggregate, No. 53, Base
- 3. 275 #/yd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 53, Base
- 4. 275 #/yd. HMA for Approach Type "A", on 6" Compacted Aggregate, No. 53, Base
- 5. 275 #/yd. HMA for Shoulder Type "A", on 6" Compacted Aggregate, No. 53, Base
- 6. 275 #/yd. HMA for Shoulder Type "A", on 6" Compacted Aggregate, No. 53, Base
- 7. Concrete Shoulder, 4"
- 8. Concrete Curb
- 9. Concrete Curb and Gutter, Modified
- 10. Concrete Curb and Gutter, Modified
- 11. Concrete Curb and Gutter, Modified
- 12. Concrete Curb and Gutter, Modified
- 13. Concrete Curb and Gutter, Modified
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- 97. Concrete Curb and Gutter, Modified
- 98. Concrete Curb and Gutter, Modified
- 99. Concrete Curb and Gutter, Modified
- 100. Concrete Curb and Gutter, Modified

125

130

SEC. 35, T. 31 N. R. 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY



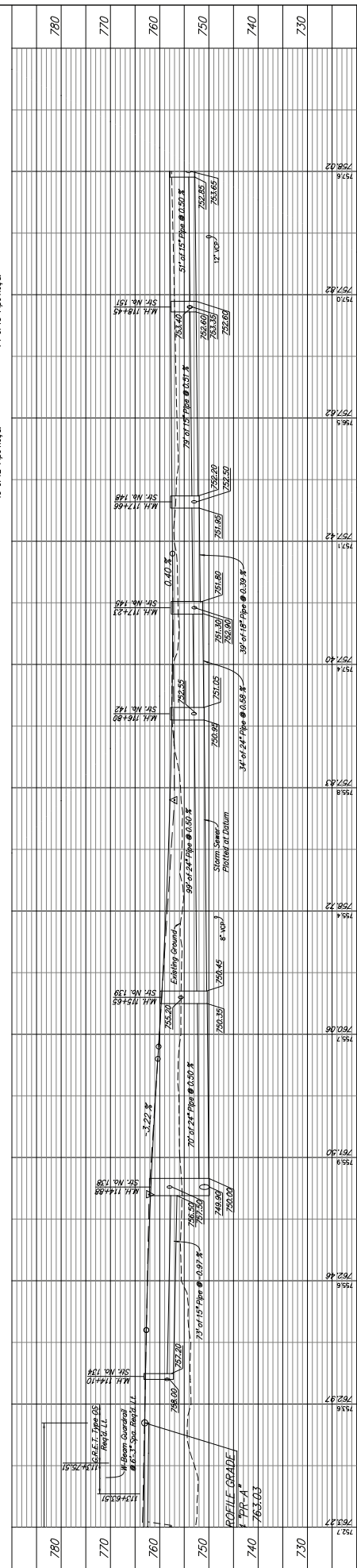
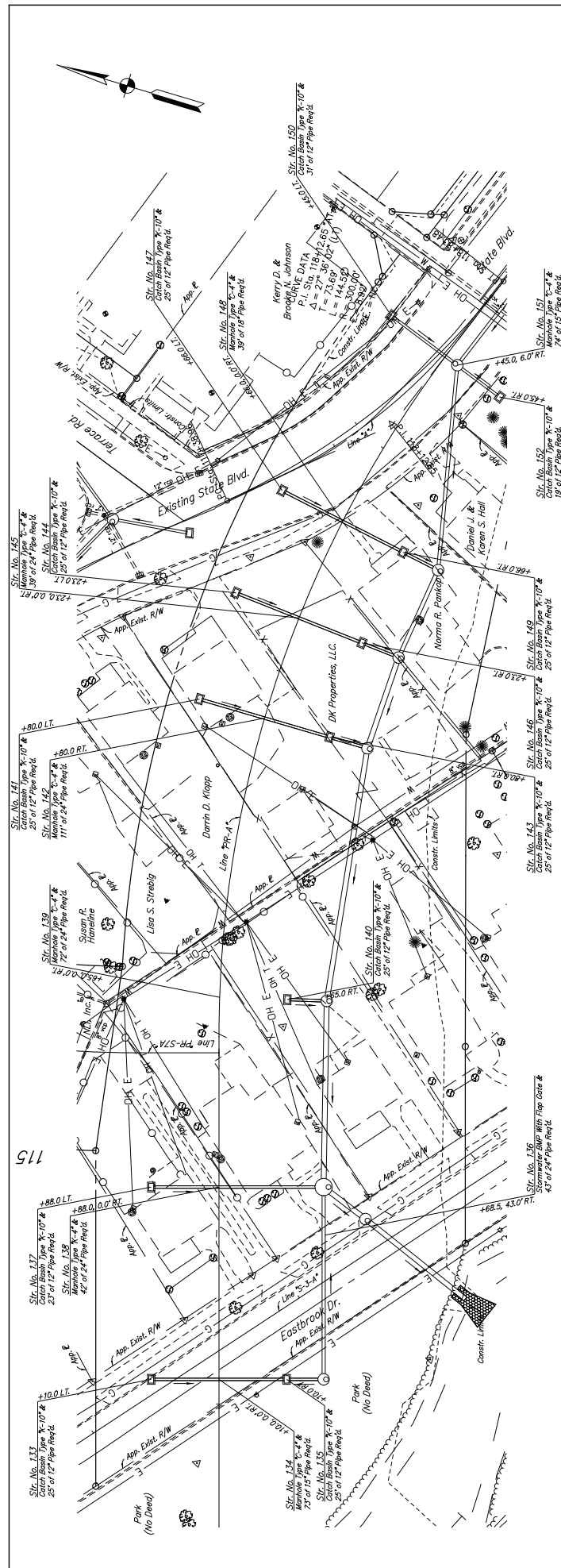
CURVE DATA  
P.I. 127+85.14 'PR-A'  
Δ = 12°16'21" (Lt.)  
L = 68.53'  
R = 460.00'  
E = 2.65'

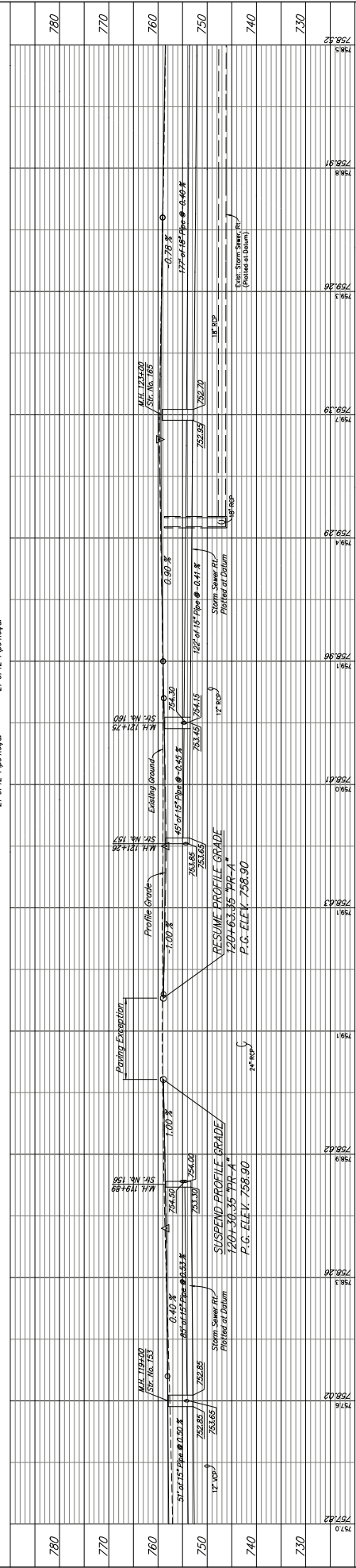
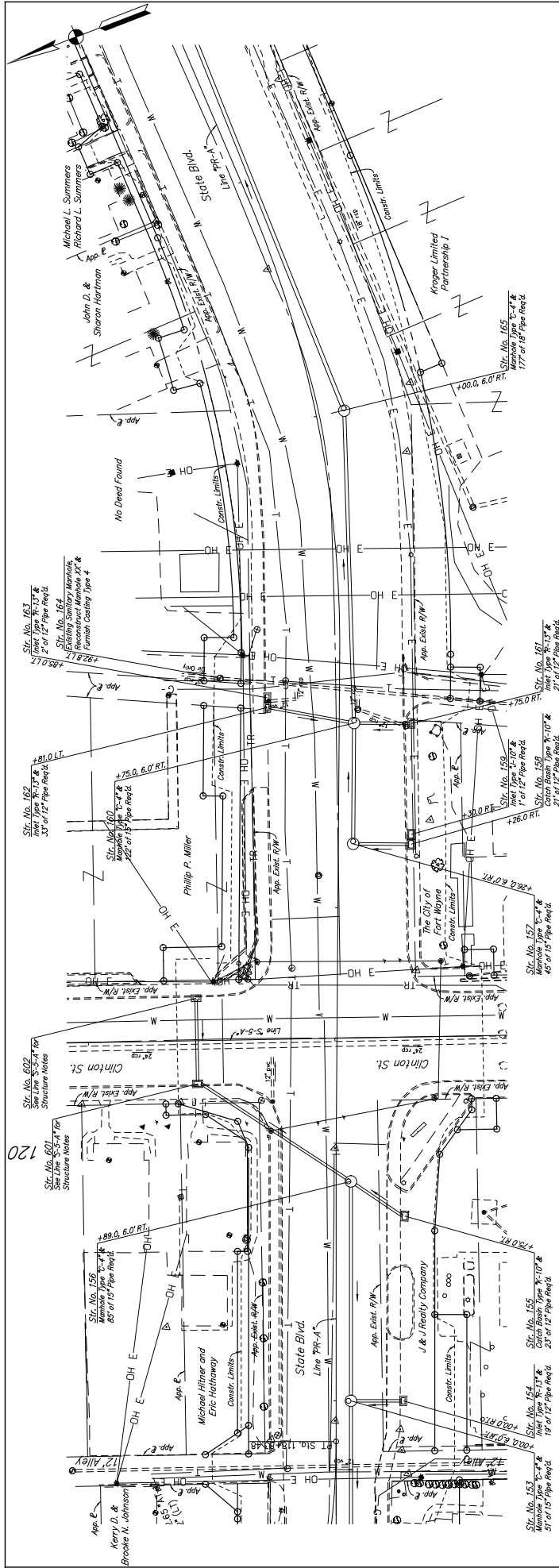
Notes:  
All Stationing & R/W Described from Line 'PR-A' Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

RECOMMENDED FOR APPROVAL				INDIANA DEPARTMENT OF TRANSPORTATION			
DESIGNED:		CONSTRUCTION DETAILS		LINE "PR-A"		BRIDGE FILE	
DESIGNED:		CONTRACT		CONTRACT		DESIGNATION NO.	
CHECKED:		CHECKED:		CHECKED:		SHEETS	
ASU		SMC		SMC		20 of 89	
DATE		DATE		DATE		PROJECT NO.	
12/12/2011		12/12/2011		12/12/2011		10051	

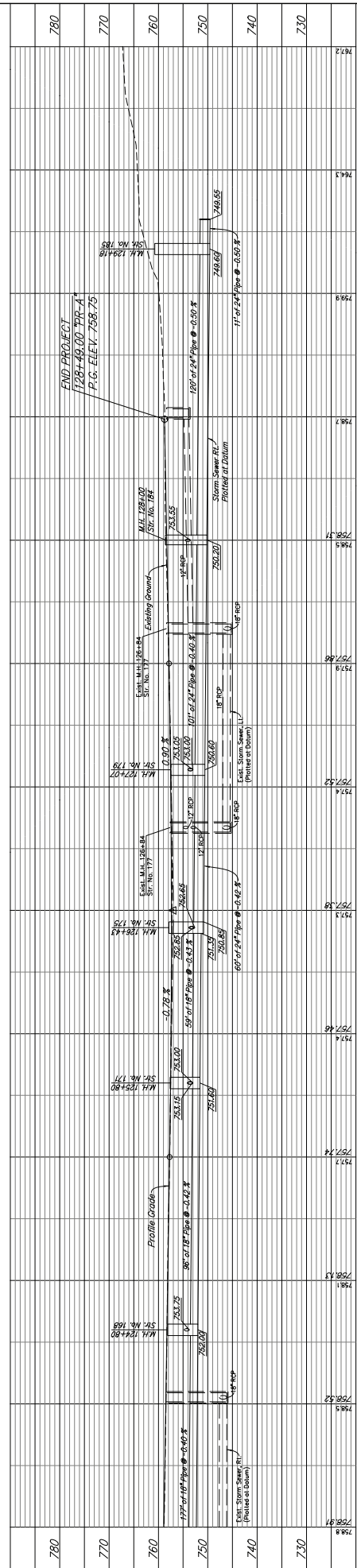
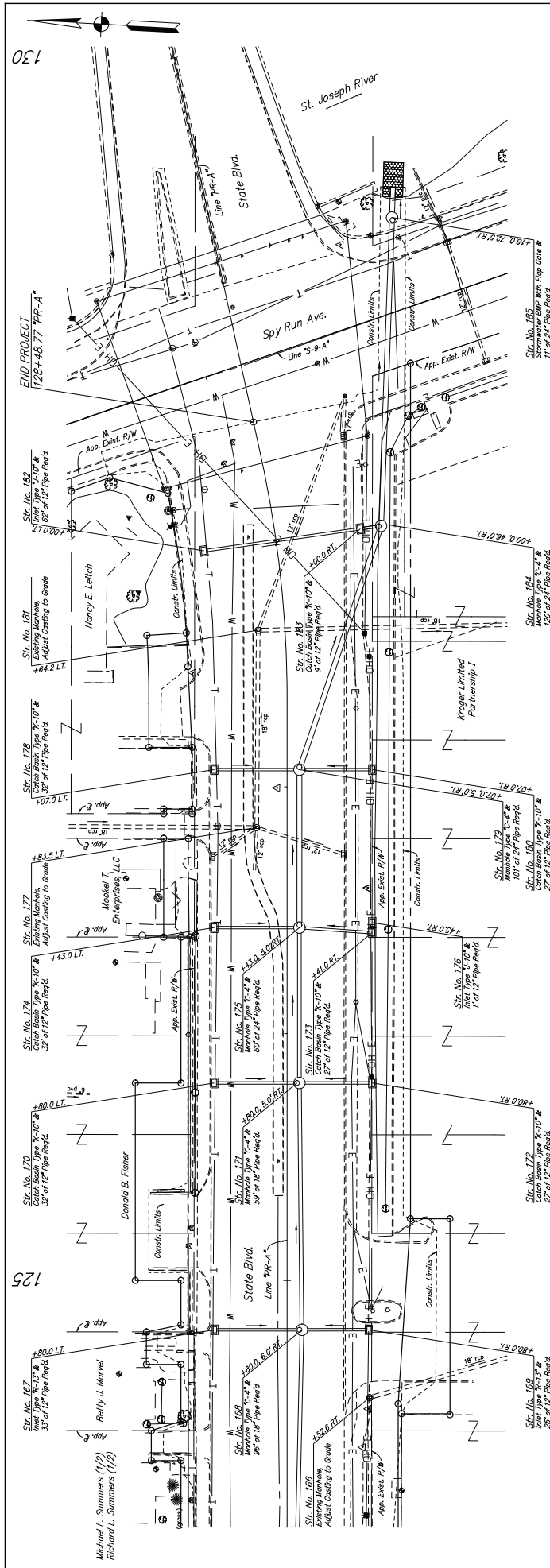
- LEGEND
- 1. Cement Concrete Pavement for Driveways
  - 2. 1/2" Gravel HMA for Approaches Type "A" on
  - 3. 1/2" Gravel HMA Intermediate Type "A" on
  - 4. 1/2" Gravel HMA Intermediate Type "A" on
  - 5. 1/2" Gravel HMA Intermediate Type "A" on
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  - 100. 1/2" Gravel HMA Intermediate Type "A" on



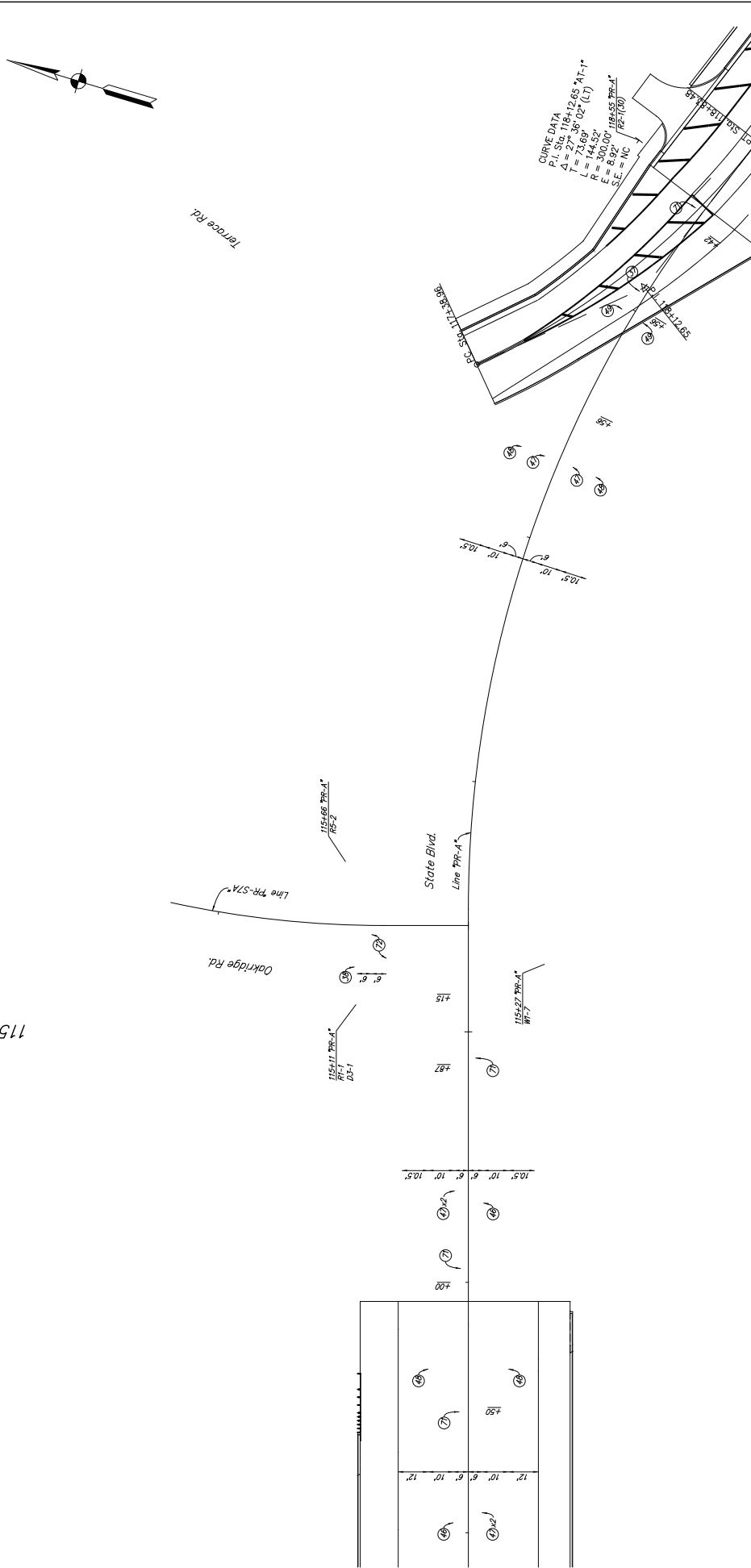
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INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
BRIDGE FILE				DESIGN ENGINEER				DRAWN			
HORIZONTAL SCALE				DATE				CAK			
VERTICAL SCALE				ASU				SNC			
DESIGNATION NO.				SHEET NO.				PROJECT NO.			
1005151				22				1005151			
SURVEY BOOK				CONTRACT				LINE "PR-A"			



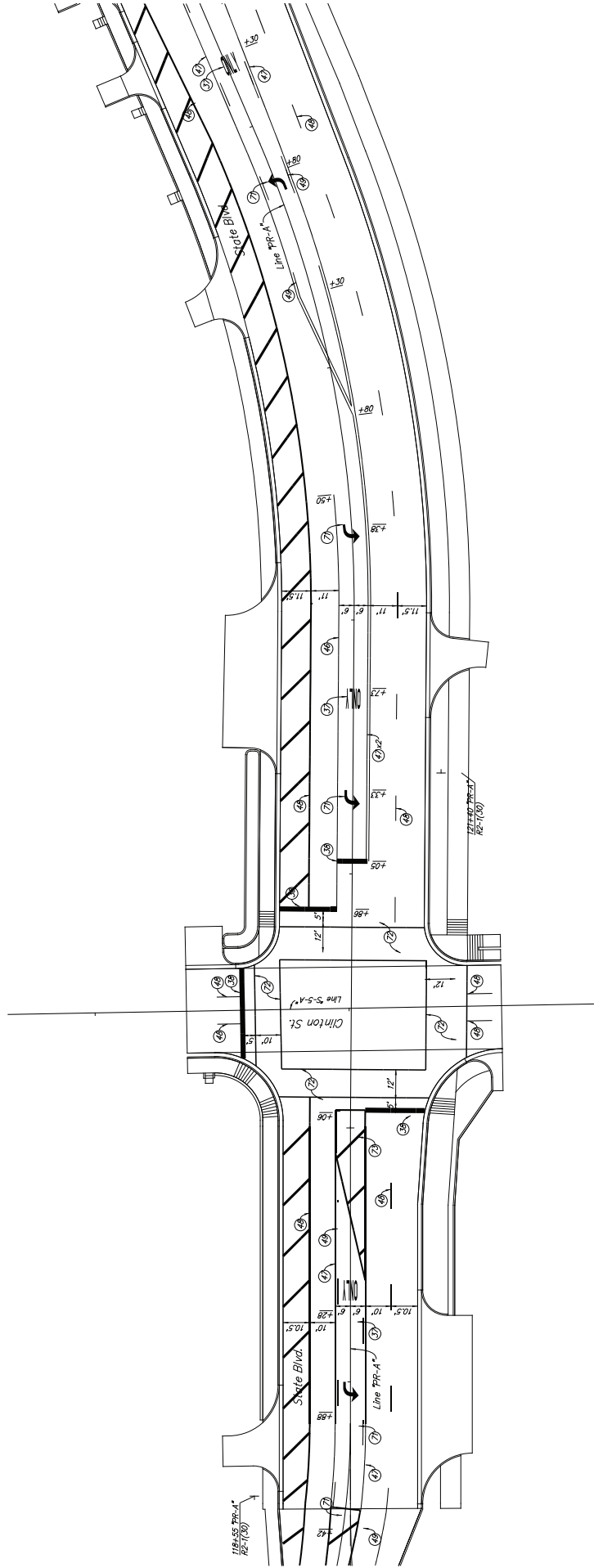
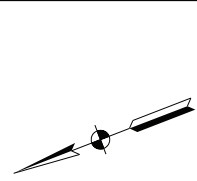
RECOMMENDED FOR APPROVAL				INDIANA			
FOR APPROVAL				DEPARTMENT OF TRANSPORTATION			
DESIGNED:	ASU	CHECKED:	SMC	DRAINAGE DETAILS			
DRAWN:	CAK	CHECKED:	SMC	LINE "PR-A"			
DATE	DATE	DATE	DATE	BRIDGE FILE			
				DESIGNATION NO.			
				1005151			
				SHEETS			
				23 of 89			
				PROJECT NO.			
				1005151			



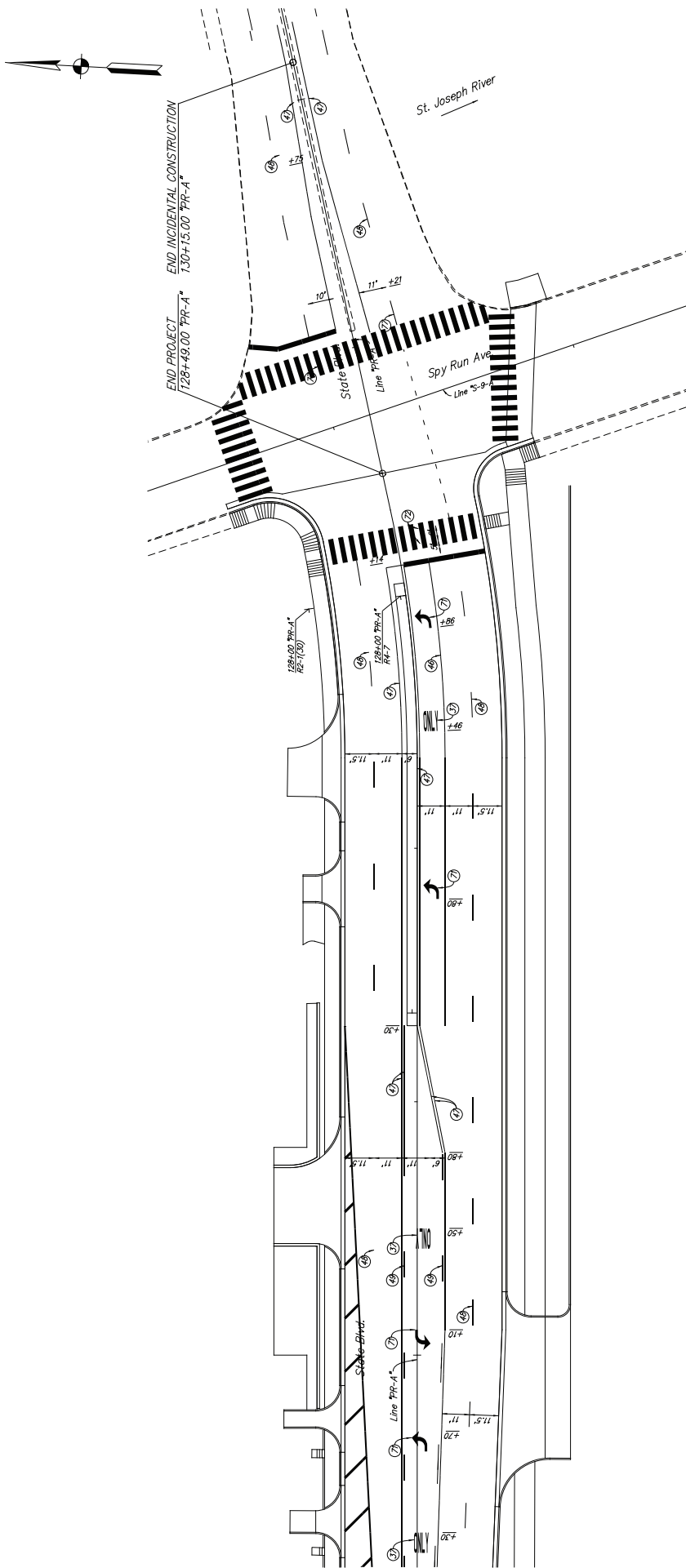
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK		SHEET NO. 24 of 89	
CHECKED: SMC		CHECKED: SMC		PROJECT NO. 10551	
INDIANA		DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE		DESIGNATION NO. 10551	
		SURVEY BOOK		SHEETS	
		CONTRACT		PROJECT NO. 10551	

- LEGEND**
- ① Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.
  - ② Line, Thermoplastic, Broken, White, 4 in.
  - ③ Transverse Markings, Thermoplastic, Stop Line, White, 24 in.
  - ④ Line, Thermoplastic, Dotted, Yellow, 4 in.
  - ⑤ Line, Thermoplastic, Solid, White, 4 in.
  - ⑥ Pavement Message Markings, Thermoplastic, Lane Indication Arrow, White, 4 in.
  - ⑦ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
  - ⑧ Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 12 in.
  - ⑨ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
  - ⑩ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)





INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	SHEETS
		CONTRACT	PROJECT NO.
			1005151
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		CHECKED: SMC	
LEGEND		23 Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.	
24 Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.		24 Line, Thermoplastic, Broken, White, 4 in.	
25 Transverse Markings, Thermoplastic, Stop Line, White, 24 in.		25 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)	
26 Line, Thermoplastic, Solid, White, 4 in.		26 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)	
27 Pavement Message Markings, Thermoplastic, Lane Indication Arrow		27 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	
28 Line, Thermoplastic, Solid, Yellow, 4 in.			



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	1005151
		CONTRACT	SHEETS
			26 of 89
			PROJECT NO.
			1005151

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:	ASU	DRAWN:	CAX
CHECKED:	SMC	CHECKED:	SMC

**LEGEND**

1 Pavement Message Markings, Thermoplastic, Word (message), 4 in. Thermoplastic, Broken, White, 4 in.

2 Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

3 Line, Thermoplastic, Solid, White, 4 in.

4 Line, Thermoplastic, Solid, Yellow, 4 in.

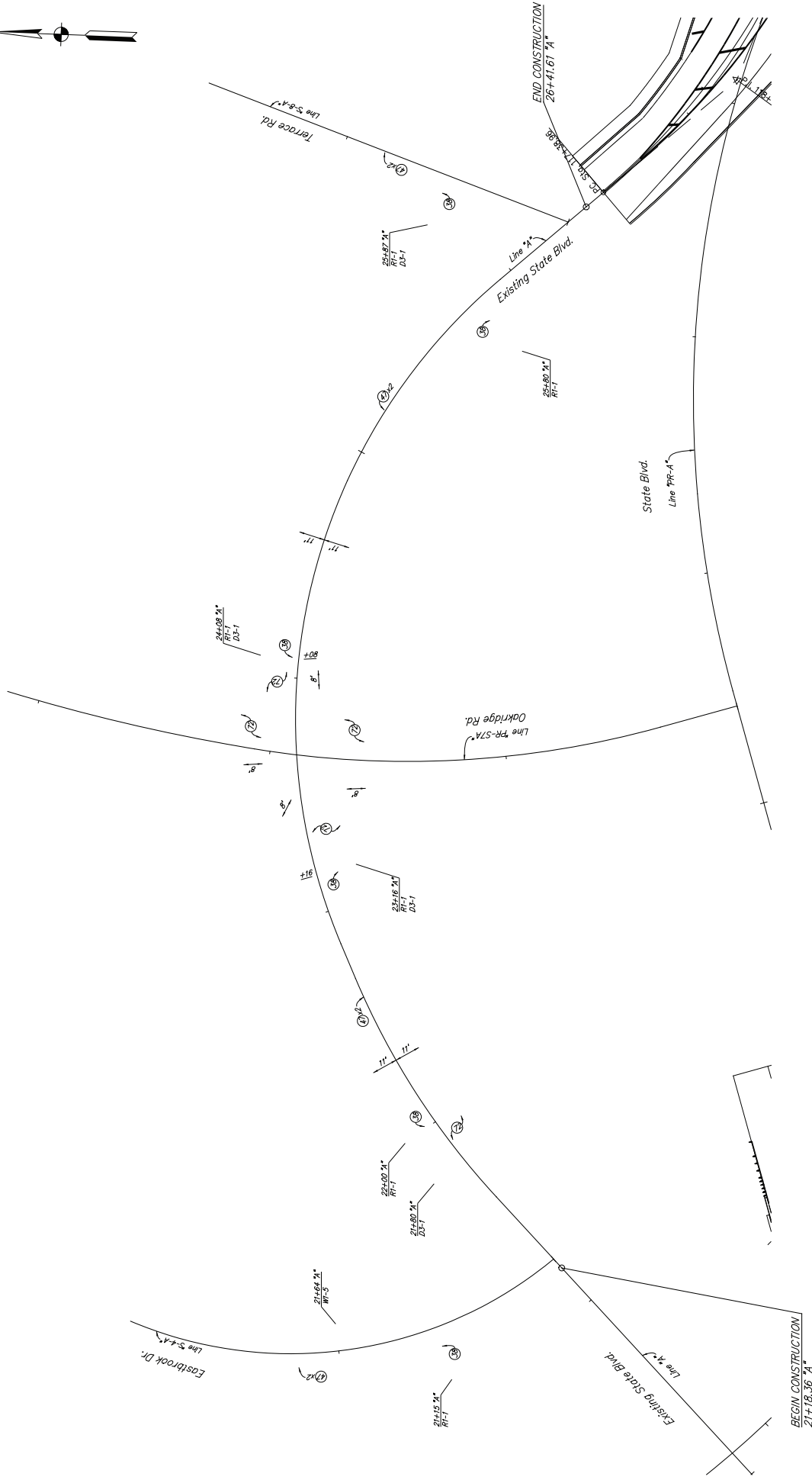
5 Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.

6 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

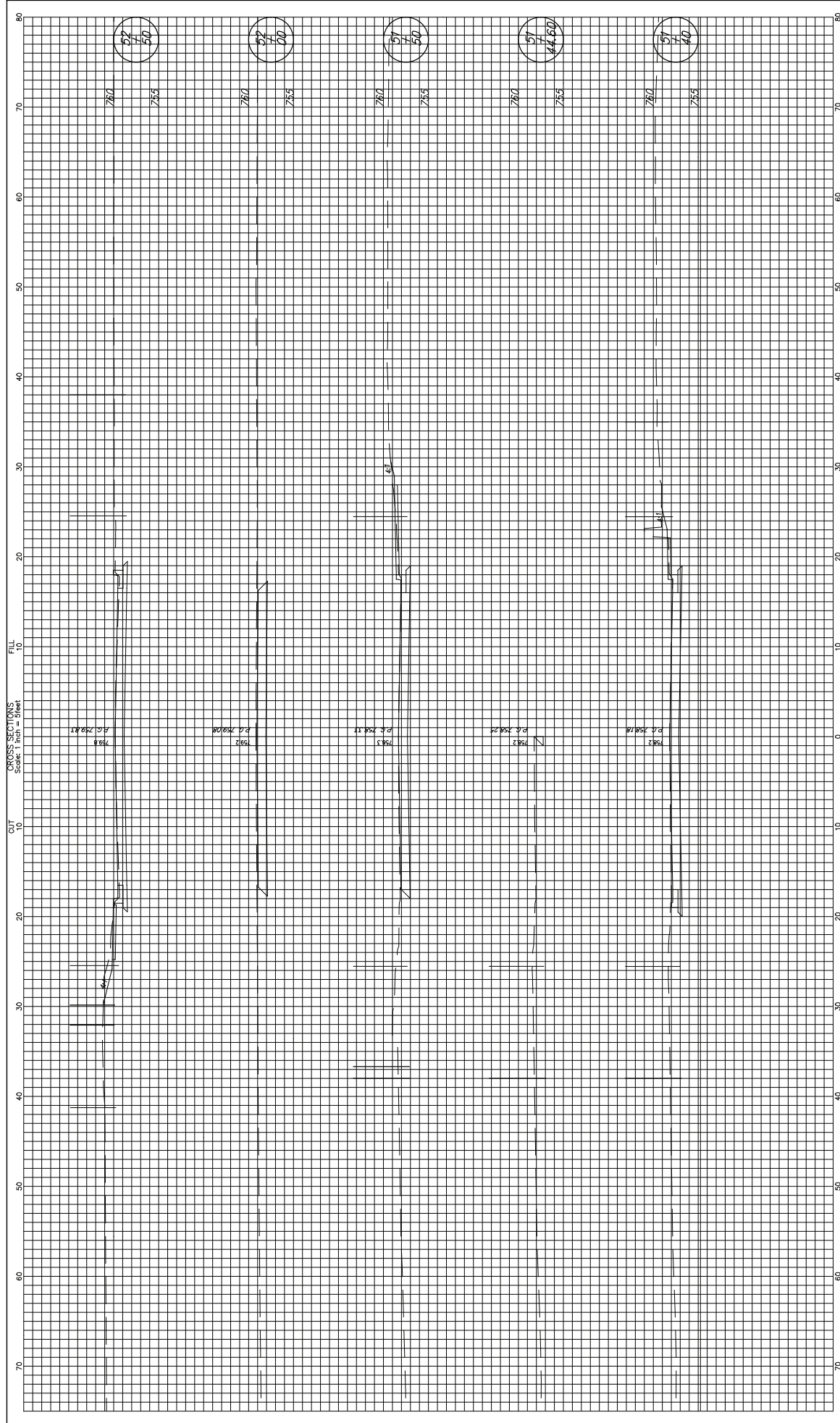
7 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

8 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

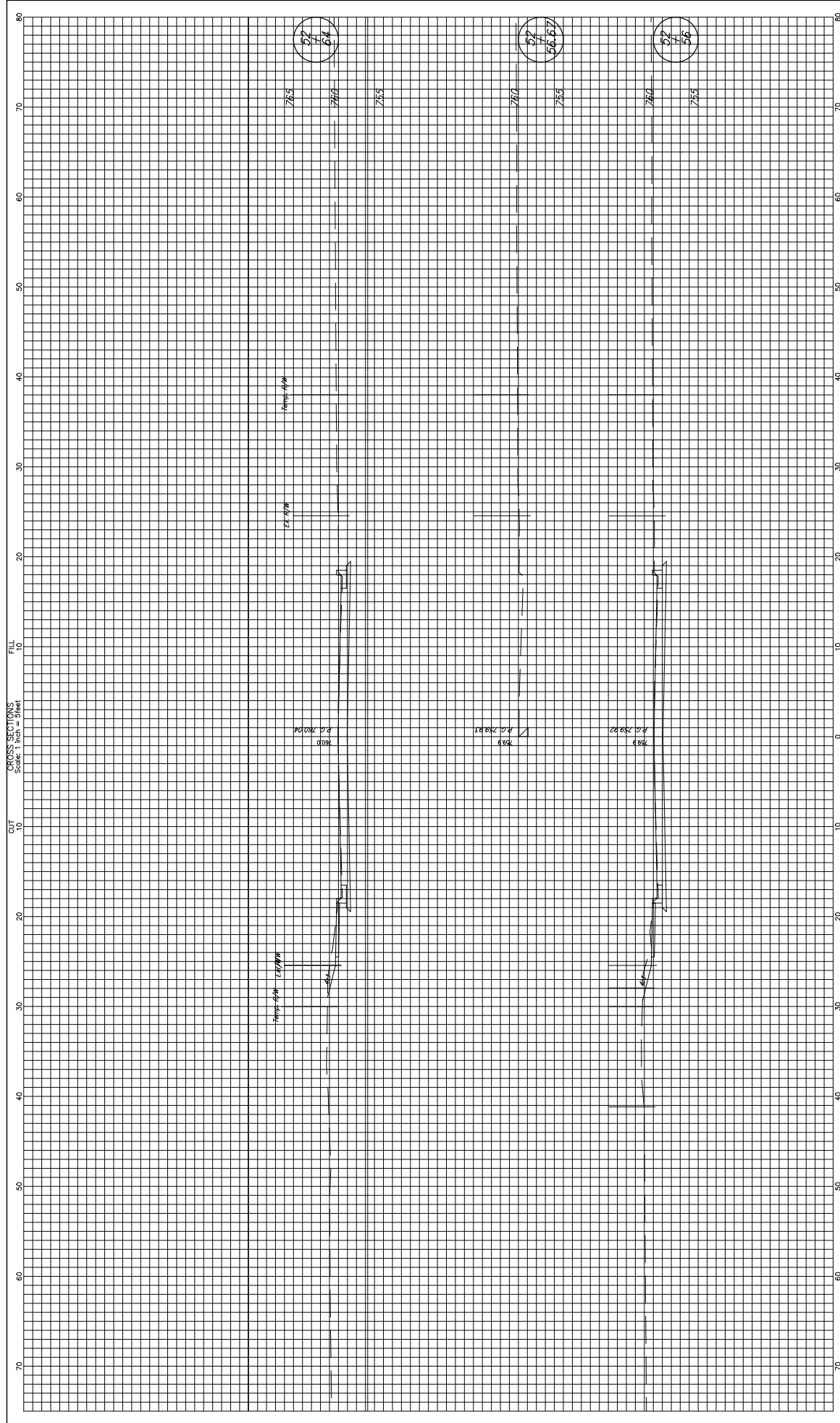
9 Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 12 in.



<p><b>LEGEND</b></p> <p>① Pavement Message Markings, Thermoplastic, Word (message)</p> <p>② Transverse Message Markings, Thermoplastic, Broken, White, 4 in.</p> <p>③ Transverse Message Markings, Thermoplastic, Broken, Yellow, 4 in.</p> <p>④ Line, Thermoplastic, Solid, White, 24 in.</p> <p>⑤ Line, Thermoplastic, Solid, White, 4 in.</p> <p>⑥ Line, Thermoplastic, Solid, Yellow, 4 in.</p> <p>⑦ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p> <p>⑧ Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 12 in.</p> <p>⑨ Line, Thermoplastic, Broken, White, 4 in. (2" Line, 4" Cap)</p> <p>⑩ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)</p> <p>⑪ Pavement Message Markings, Thermoplastic, Lane Induction Arrow</p>		<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGN ENGINEER _____ DATE _____</p> <p>DESIGNED: _____ ASU _____ DRAWN: _____ CAK _____</p> <p>CHECKED: _____ SMC _____ CHECKED: _____ SMC _____</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>PAVEMENT MARKING &amp; SIGNING DETAILS - LINE "A"</p>		<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>SURVEY BOOK _____</p> <p>CONTRACT _____</p> <p>27 of 89</p> <p>DESIGNATION NO. 100551</p> <p>BRIDGE FILE _____</p>
--	--	--	--	--	--	---



INDIANA DEPARTMENT OF TRANSPORTATION				BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE	DESIGNATION NO. 1005151	
DESIGNED:	ASU	DRAWN:	CAK	SURVEY BOOK	
CHECKED:	SMC	CHECKED:	SMC	CONTRACT	
CROSS SECTIONS LINE "S-5-A"				SHEETS 28 of 89	
				PROJECT NO. 1005151	



INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
CROSS SECTIONS LINE "S-5-A"		DESIGNATION NO. 1005151	
RECOMMENDED FOR APPROVAL		SURVEY BOOK CONTRACT	
DESIGNED: ASU		SHEETS 29 of 89	
CHECKED: SMC		PROJECT NO. 1005151	
DATE		DRAWN: CAK	
DESIGN ENGINEER		CHECKED: SMC	

PROJECT	DESIGNATION
1005154	1005154
CONTRACT	BRIDGE FILE

TRAFFIC DATA	WESTBROOK DR. - LINE "PR-S-1-A"
A.A.D.T. (2009)	350 V.P.D.
A.A.D.T. (2030)	592 V.P.D.
D.I.V.	N/A V.P.H.
DIRECTIONAL DISTRIBUTION	N/A %
TRUCKS	N/A % D.H.V.
MAINLINE E.S.A.'S	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
RURAL/URBAN	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	OAKRIDGE RD. - LINE "PR-S-7-A"
A.A.D.T. (2009)	35 V.P.D.
A.A.D.T. (2030)	1545 V.P.D.
D.I.V.	N/A V.P.H.
DIRECTIONAL DISTRIBUTION	N/A %
TRUCKS	N/A % D.H.V.
MAINLINE E.S.A.'S	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
RURAL/URBAN	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	EXIST. STATE BLVD. - LINE "A"
A.A.D.T. (2009)	20,650 V.P.D.
A.A.D.T. (2030)	Minimal V.P.D.
D.I.V.	N/A V.P.H.
DIRECTIONAL DISTRIBUTION	N/A %
TRUCKS	N/A % D.H.V.
MAINLINE E.S.A.'S	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
RURAL/URBAN	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	CASS ST. - LINE "S-6-A"
A.A.D.T. (2009)	Minimal V.P.D.
A.A.D.T. (2030)	Minimal V.P.D.
D.I.V.	N/A V.P.H.
DIRECTIONAL DISTRIBUTION	N/A %
TRUCKS	N/A % D.H.V.
MAINLINE E.S.A.'S	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
RURAL/URBAN	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None



200 SHADLAND STATION  
INDIANAPOLIS, IN 46204  
TEL 317.547.5580 FAX 317.543.0270  
www.structurepoint.com

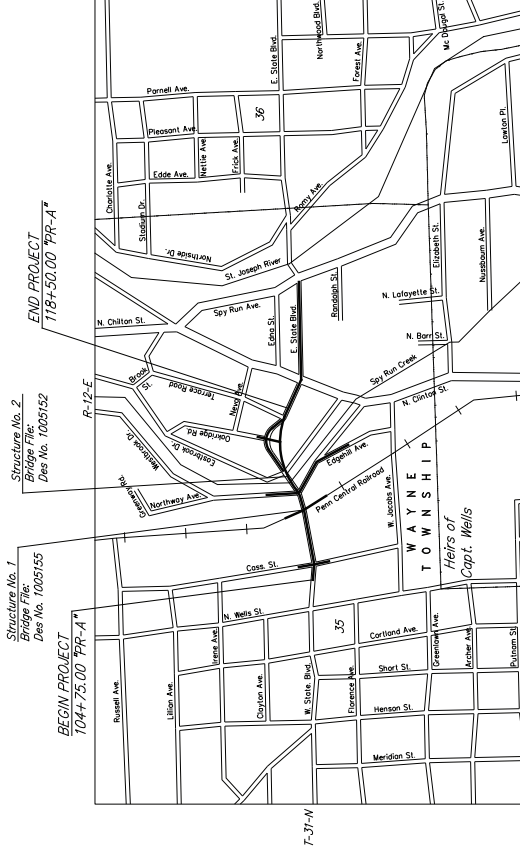
## INDIANA

### DEPARTMENT OF TRANSPORTATION

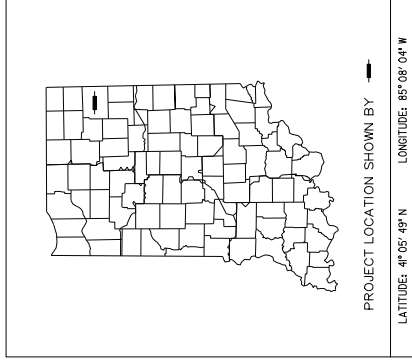
# ROAD PLANS

STATE BLVD. RECONSTRUCTION - PART 2  
PROJECT NO. 1005154 P.E.  
PROJECT NO. R/W  
PROJECT NO. CONST.

Project Description: Road Reconstruction on State Boulevard Beginning 202.00 Feet West of the Intersection of State Boulevard and Cass Street, Thence East 1,325.00 Feet Along Line "PR-A" to approximately 1,30.00 Feet East of the Intersection of Terrace Rd. and State Boulevard, All in Section 35, Township 31 North, Range 12 East, Wayne Township, Allen County, Indiana



TRAFFIC DATA	STATE BLVD. - LINE "PR-A"
A.A.D.T. (2009)	20,650 V.P.D.
A.A.D.T. (2030)	25,000 V.P.D.
D.I.V.	250 V.P.H.
DIRECTIONAL DISTRIBUTION	50 %
TRUCKS	2 % D.H.V.
MAINLINE E.S.A.'S	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Minor Arterial
RURAL/URBAN	Urban (Built-Up)
TERRAIN	Level
ACCESS CONTROL	None



BRIDGE LENGTH :	0.35 Miles
ROADWAY LENGTH :	0.450 Miles
TOTAL LENGTH :	0.45 Miles
MAX. GRADE :	6.47 %

HYDROLOGIC UNIT CODE: 04100004060060



Scale: 1" = 500'

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2010  
TO BE USED WITH THESE PLANS

PLANS PREPARED BY: American Structurepoint, Inc.	(317) 547-5580
CERTIFIED BY:	PHONE NUMBER
APPROVED	DATE
FOR LETTING:	DATE

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	30 of 300
	PROJECT NO.
	1005154

## INDEX TO SHEETS

**\*\* REPRESENTS GENERAL NOTES REQUIRED**

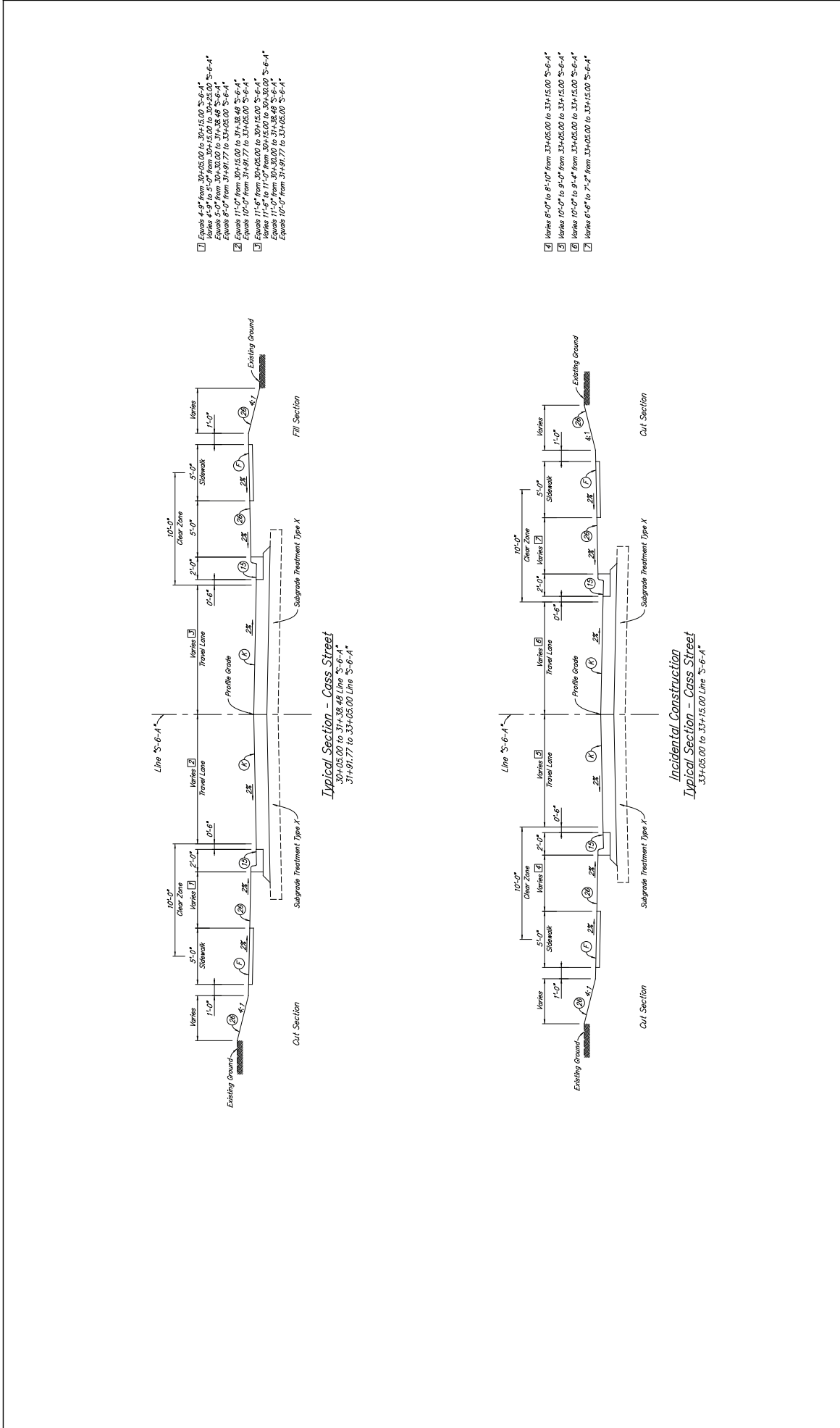
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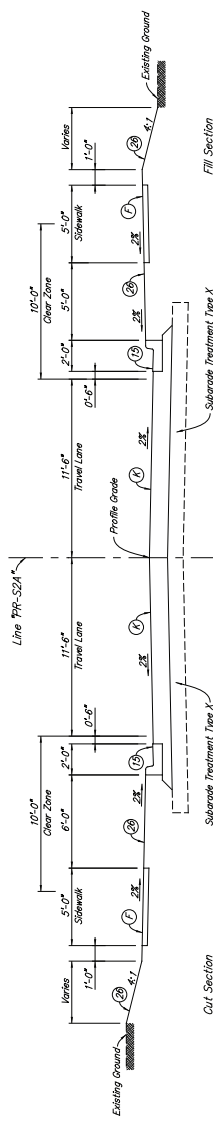




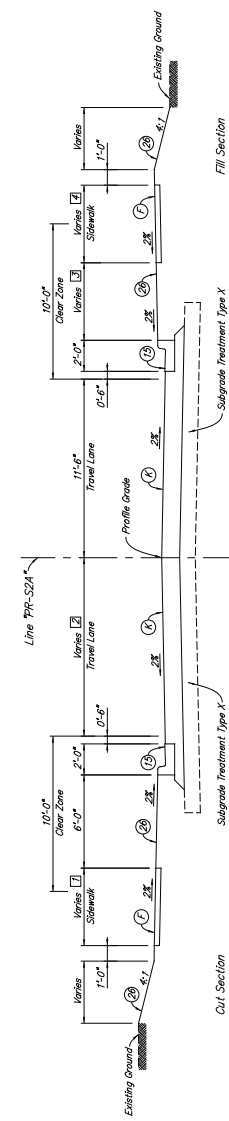




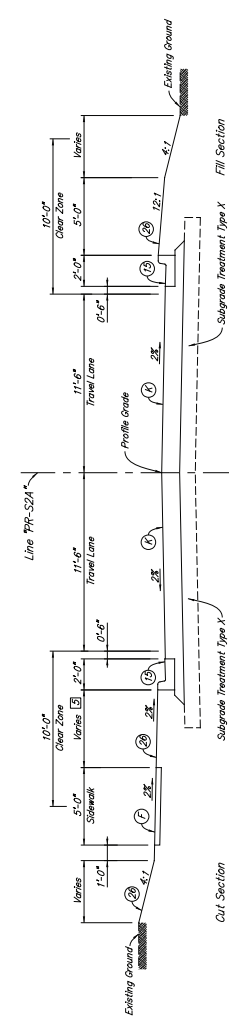
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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER DATE	
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		CHECKED: SMC	
LEGEND		Underdrain	
(1) 165 #/yd. HMA Surface, Type "A", on		(2) Concrete Center Curb, Type "D"	
(2) 275 #/yd. HMA Intermediate, Type "A", on		(3) Sodding, Nursery	
(3) 6" Compacted Aggregate, No. 53, Base		(4) Mulched Seeding, Type "A"	
(4) Concrete Curb		(5) Retaining Wall	
(5) Concrete Sidewalk, 4"		(6) Combined Concrete Curb and Gutter, Modified	



**Typical Section - Edgell Avenue**  
51+60.00 to 52+12.43 Line "PR-S2A"

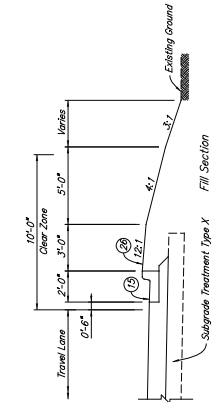


**Typical Section - Edgell Avenue**  
52+12.43 to 54+66.43 Line "PR-S2A"



**Typical Section - Westbrook Drive**  
55+27.76 to 58+45.00 Line "PR-S2A"

- 1. Equals 5'-0" from 52+12.43 to 54+25.00 "PR-S2A"
- 2. Equals 5'-0" to 8'-0" from 54+25.00 to 54+56.00 "PR-S2A"
- 3. Equals 8'-0" from 54+56.00 to 54+66.43 "PR-S2A"
- 4. Equals 11'-6" from 52+12.43 to 53+94.00 "PR-S2A"
- 5. Equals 11'-6" to 15'-6" from 53+94.00 to 54+56.00 "PR-S2A"
- 6. Equals 15'-6" from 54+56.00 to 54+66.43 "PR-S2A"
- 7. Equals 5'-0" from 52+12.43 to 53+94.00 "PR-S2A"
- 8. Equals 5'-0" to 8'-0" from 53+94.00 to 54+56.00 "PR-S2A"
- 9. Equals 8'-0" from 54+56.00 to 54+66.43 "PR-S2A"
- 10. Equals 5'-0" from 52+12.43 to 53+94.00 "PR-S2A"
- 11. Equals 5'-0" to 8'-0" from 53+94.00 to 54+56.00 "PR-S2A"
- 12. Equals 8'-0" from 54+56.00 to 54+66.43 "PR-S2A"
- 13. Equals 5'-0" from 52+12.43 to 53+94.00 "PR-S2A"
- 14. Equals 5'-0" to 8'-0" from 53+94.00 to 54+56.00 "PR-S2A"
- 15. Equals 8'-0" from 54+56.00 to 54+66.43 "PR-S2A"

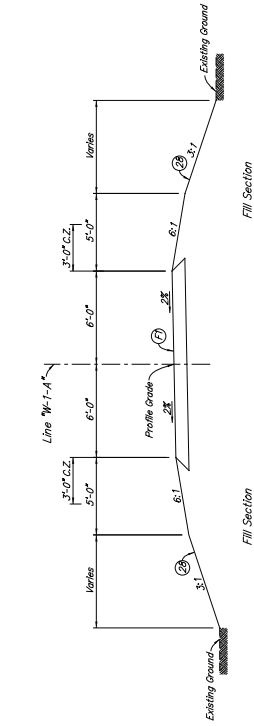


**Fill Section Detail**  
55+27.76 to 56+25.00 RL Line "PR-S2A"

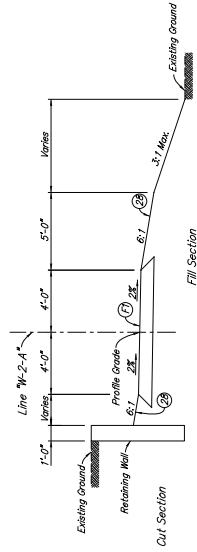
- LEGEND**
- 1. 165 #3/4" HMA Surface, Type "1", on 275 #3/4" HMA Intermediate, Type "1", on 6" Compacted Aggregate, No. 53, Base
  - 2. 165 #3/4" HMA Surface, Type "1", on 275 #3/4" HMA Intermediate, Type "1", on 6" Compacted Aggregate, No. 53, Base
  - 3. Concrete Curb
  - 4. Concrete Sidewalk, 4"
  - 5. Combined Concrete Curb and Gutter, Modified
  - 6. Retaining Wall
  - 7. Mulched Seeding, Type "1"
  - 8. Sodding, Nursery
  - 9. Concrete Center Curb, Type "1"
  - 10. Underdrain

INDIANA	HORIZONTAL SCALE	BRIDGE FILE
DEPARTMENT OF TRANSPORTATION	VERTICAL SCALE	DESIGNATION NO.
TYPICAL CROSS SECTIONS	SURVEY BOOK	SHEETS
LINE "PR-S2A"	CONTRACT	PROJECT NO.
		100514

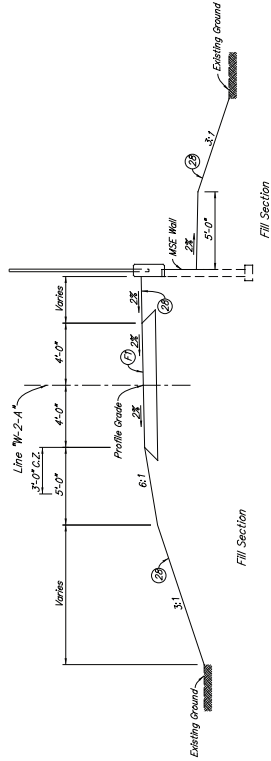




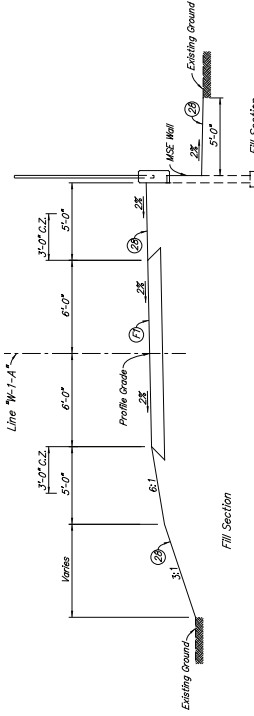
**Typical Section Pufferbelly Trail**  
11+15.00 to 44+00.00 W-1-A  
10+75.00 to 11+55.00 W-1-A



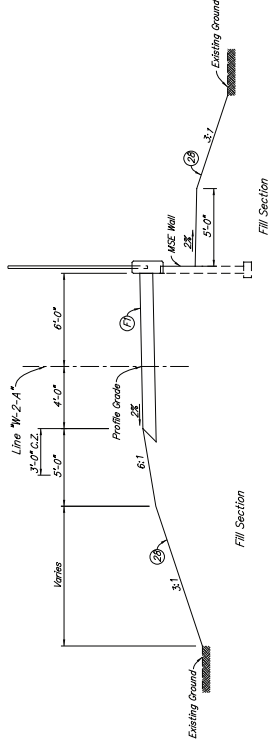
**Typical Section Sidewalk Ramp**  
10+00.00 to 11+25.00 W-2-A



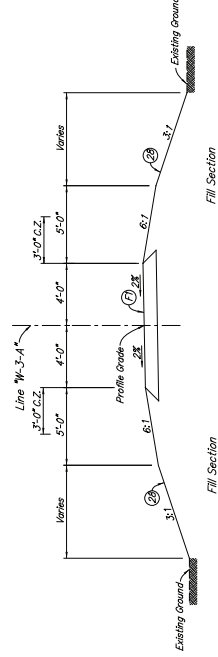
**Typical Section Sidewalk Ramp**  
12+15.29 to 12+52.34 W-2-A



**Typical Section Pufferbelly Trail**  
44+00.00 to 7+02.84 W-1-A  
7+98.16 to 10+75.00 W-1-A



**Typical Section Sidewalk Ramp**  
11+25.00 to 12+15.29 W-2-A



**Typical Section Sidewalk Ramp**  
20+00.00 to 23+01.96 W-3-A

- LEGEND**
- ① 165 #3/4" HMA Surface, Type 1, on 27" #3/4" HMA Intermediate, Type 1, on 8" Compacted Aggregate, No. 53, Base
  - ② 27" #3/4" HMA Intermediate, Type 1, on 8" Compacted Aggregate, No. 53, Base
  - ③ Concrete Curb
  - ④ Concrete Sidewalk, 4"
  - ⑤ Concrete Center Curb, Type 1
  - ⑥ Siding, Nursery
  - ⑦ Mulched Seeding, Type 1
  - ⑧ Retaining Wall

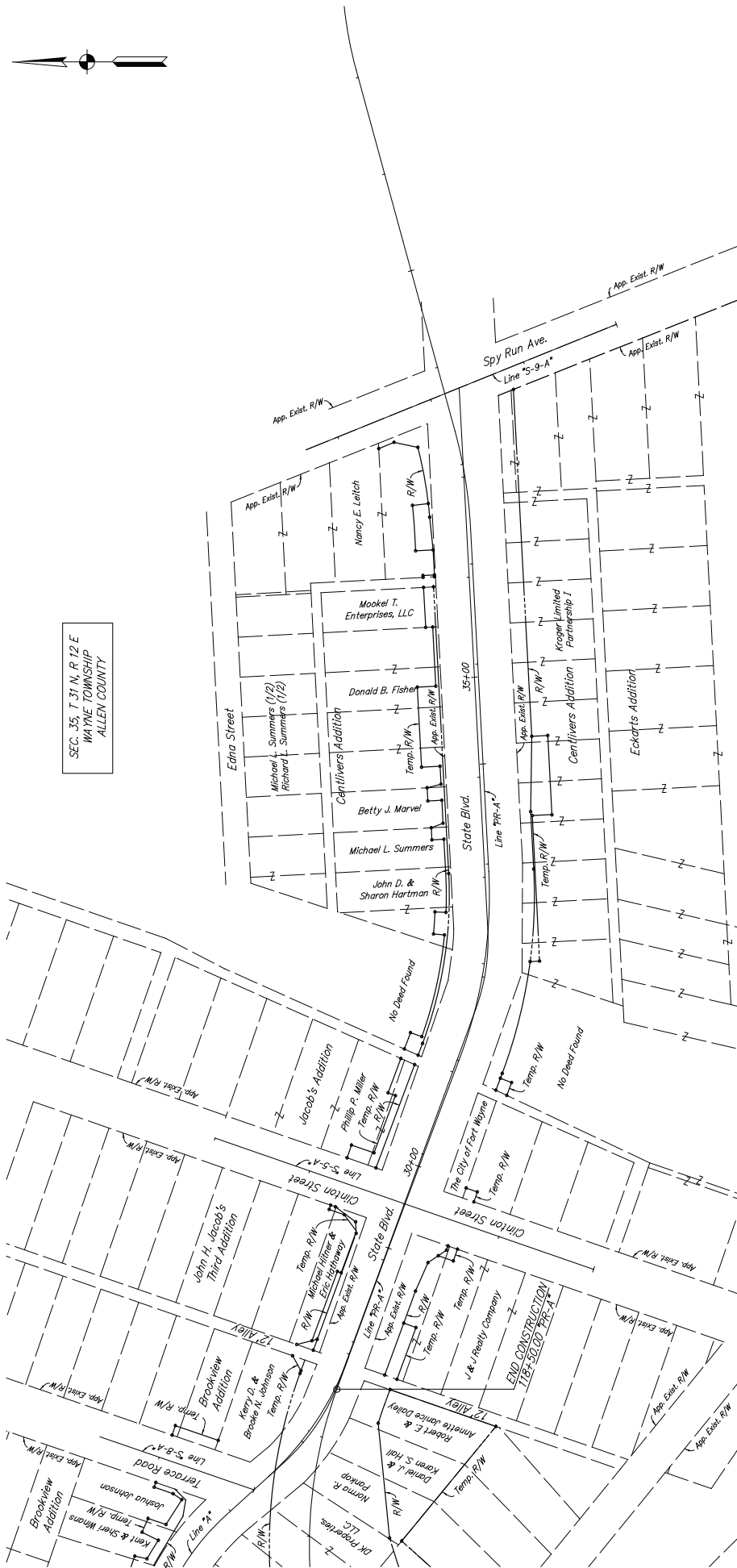
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:		ASU	CAK
CHECKED:		SMC	SMC
INDIANA DEPARTMENT OF TRANSPORTATION		TYPICAL CROSS SECTIONS	
LINE "W-1-A" & "W-2-A" & "W-3-A"		BRIDGE FILE	
		DESIGNATION NO.	
		100514	
		SHEETS	
		37 of 300	
		PROJECT NO.	
		100514	



130

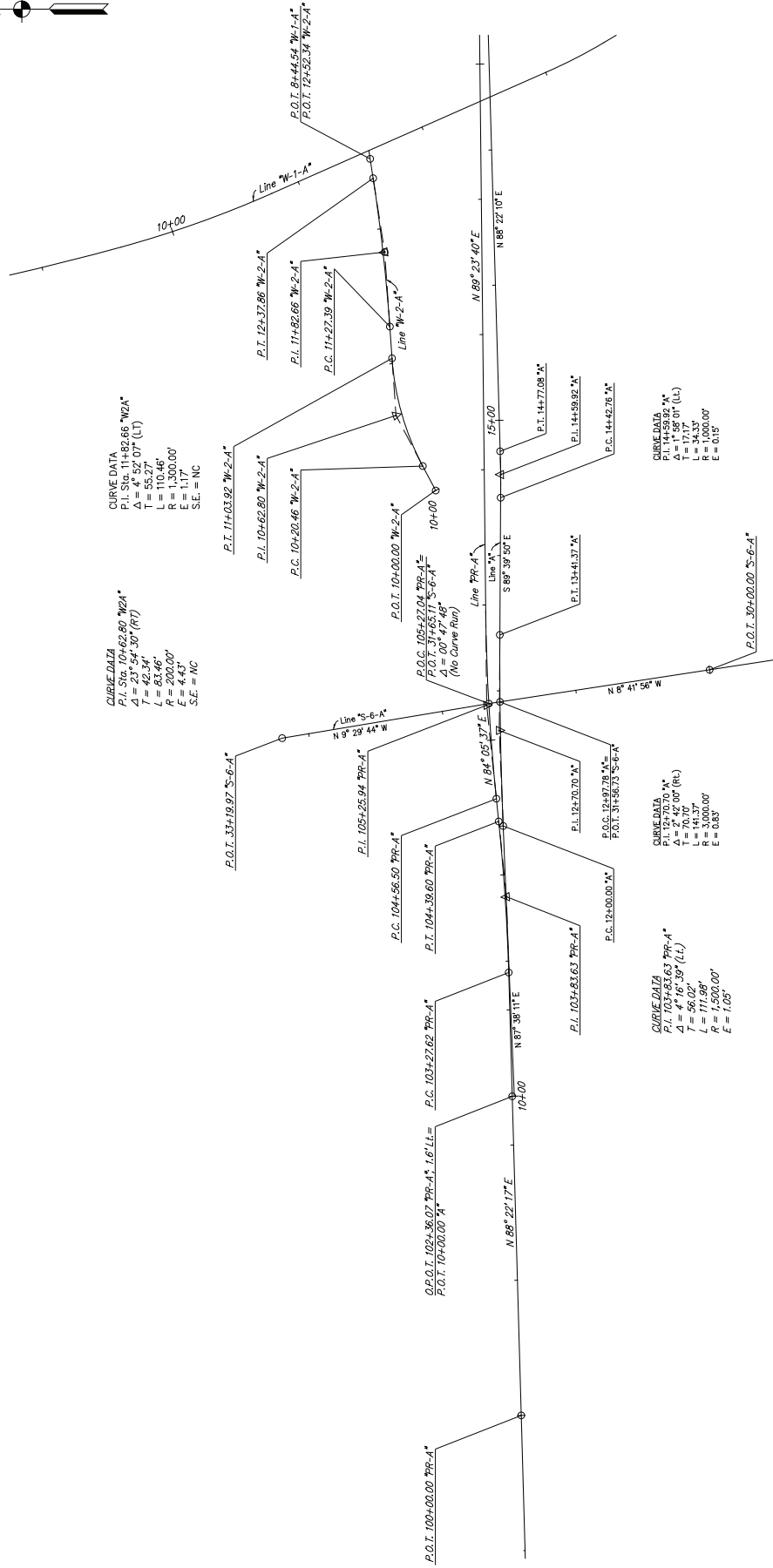
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120



SEC. 35, T. 31 N., R. 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY

				INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		DESIGNATION NO. 10051	
		DESIGNED: ASU		DRAWN: PJT				VERTICAL SCALE	
		CHECKED: SMC		CHECKED: SMC				SURVEY BOOK	
								SHEETS 39 of 89	
								PROJECT NO. 10051	
								CONTRACT	
								PLAT NO. 1	



Notes:  
See Sheet VV for Line W-9-80 Commencement

		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 40'	BRIDGE FILE
					VERTICAL SCALE	DESIGNATION NO. 1005151
		DESIGNED: ASJ      DRAWN: CAC CHECKED: SMC      CHECKED: SMC	GEOMETRIC TIE-UP		SURVEY BOOK	SHEETS 40 of 89
					CONTRACT	PROJECT NO. 1005151

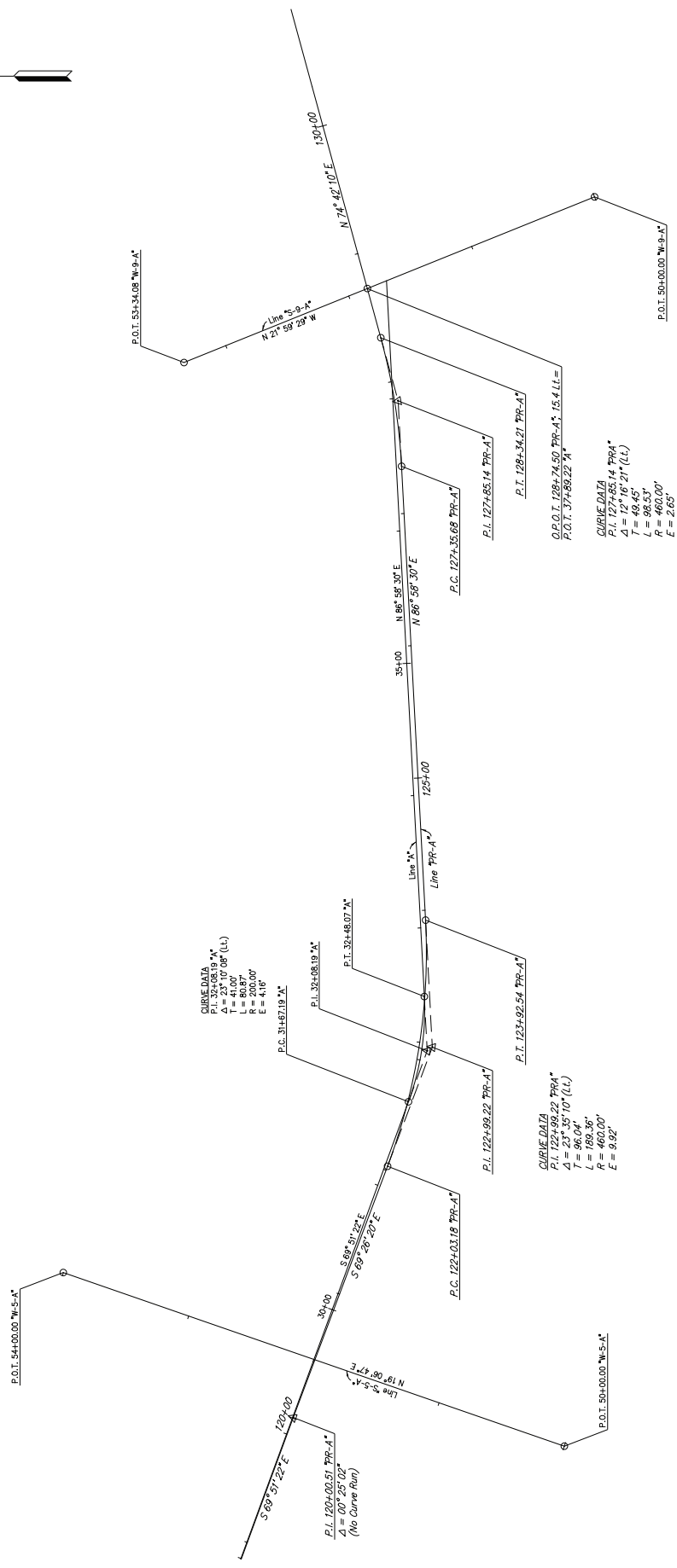




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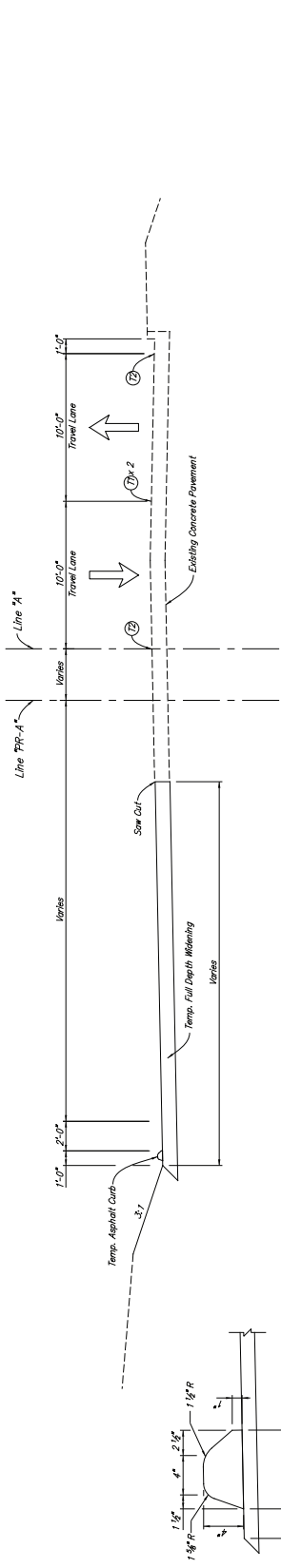
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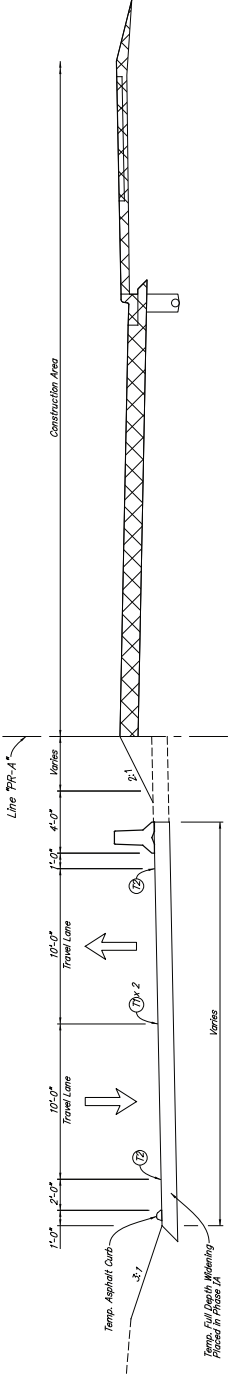
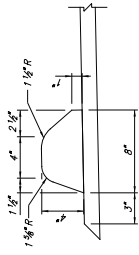


INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
GEOMETRIC TIE-UP		1005151	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
DATE		SHEETS	
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		PROJECT NO.	
		CONTRACT	
		42 of 89	
		1005151	

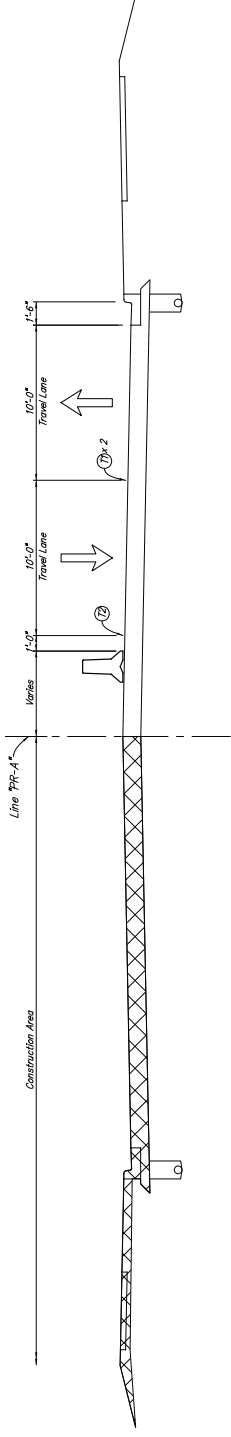




ASPHALT CURB (MOUNTABLE) DETAIL



PHASE I

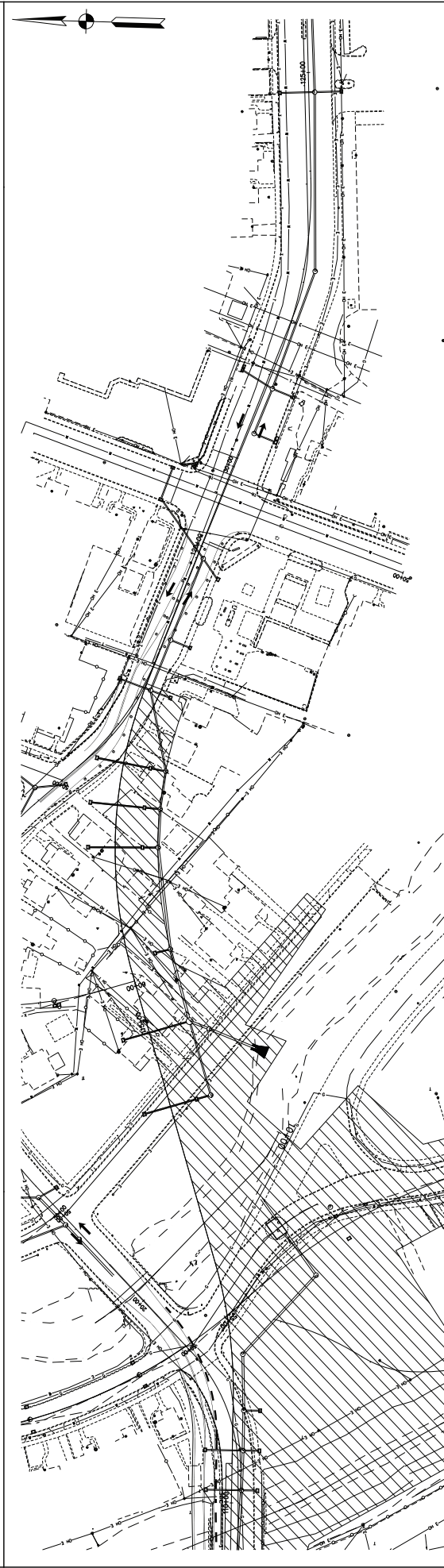
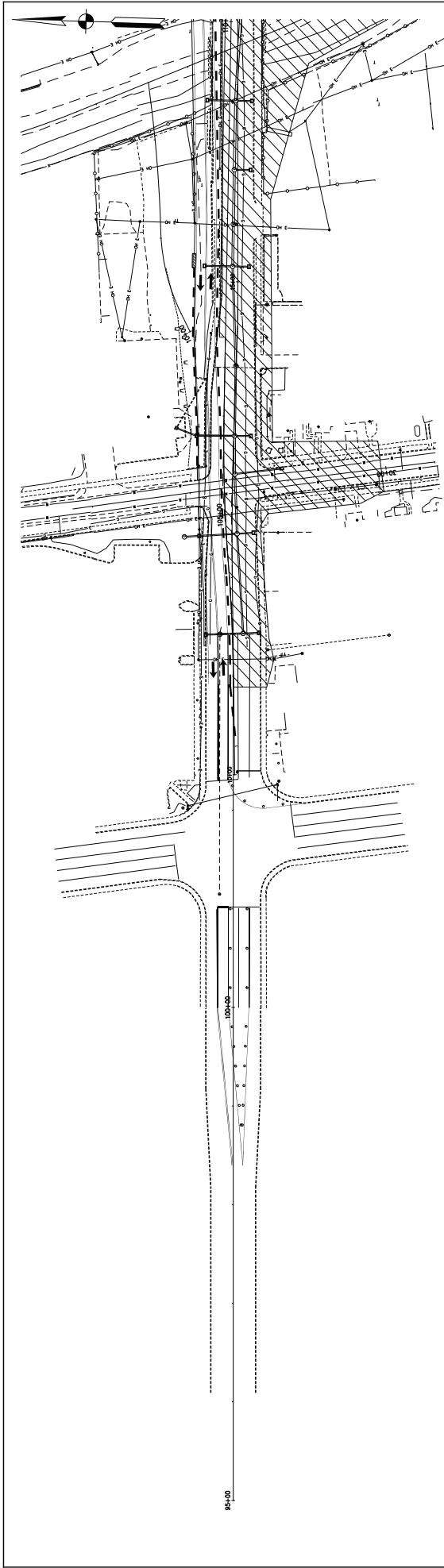


PHASE II

- LEGEND
- Direction of Traffic:
  - Temporary Concrete Barrier:
  - Standard Drum:
  - Construction Area:
  - Temporary Pavement Marking, Solid, Yellow, 4 in.:
  - Temporary Pavement Marking, Solid, White, 4 in.:

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:	ASU	DRAWN:	CAK
CHECKED:	SMC	CHECKED:	SMC
INDIANA DEPARTMENT OF TRANSPORTATION		MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS	
HORIZONTAL SCALE	BRIDGE FILE	DESIGNATION NO.	100514
VERTICAL SCALE		SURVEY BOOK	44
		CONTRACT	XXX
		SHEETS	44 of 100514
		PROJECT NO.	100514

Notes: NMS Surface to be Placed Following Phase II Under Live Traffic



**LEGEND**

-----	Temporary Barrier	↓	Direction of Traffic	▲	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
---	Detour Route	▨	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		▩	Standard Drum	▲	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
		VERTICAL SCALE	DESIGNATION NO.
MAINTENANCE OF TRAFFIC PHASE I		SURVEY BOOK	SHEETS
		CONTRACT	PROJECT NO.

RECOMMENDED FOR APPROVAL



DESIGNED: ASU	DATE
CHECKED: SMC	PIT
SIGNED: SMC	SIC

DESIGNED: ASU	DATE
CHECKED: SMC	PIT
SIGNED: SMC	SIC

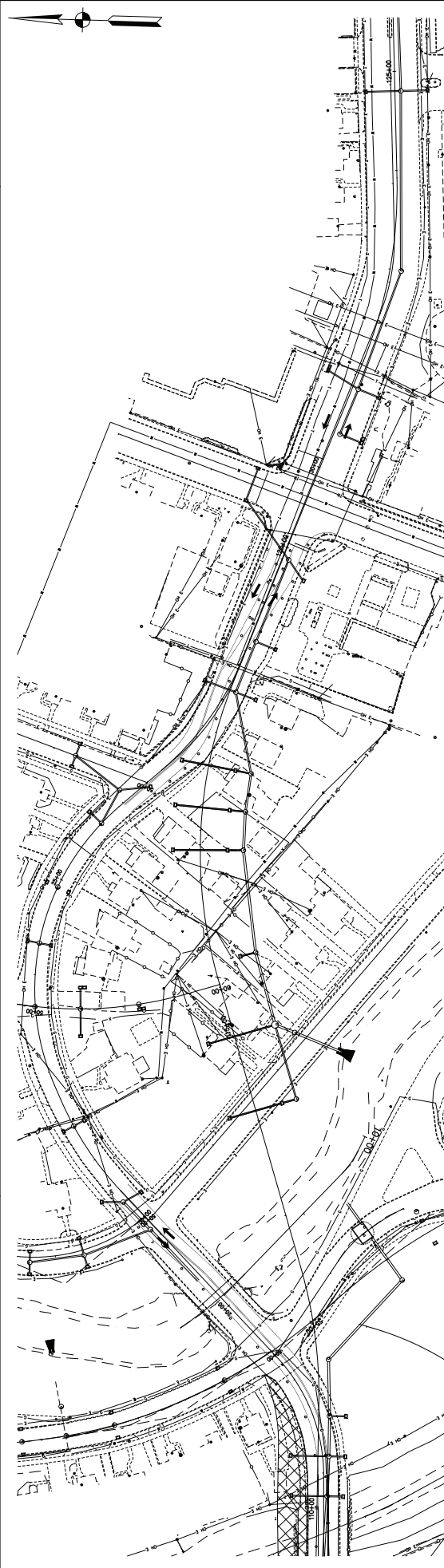
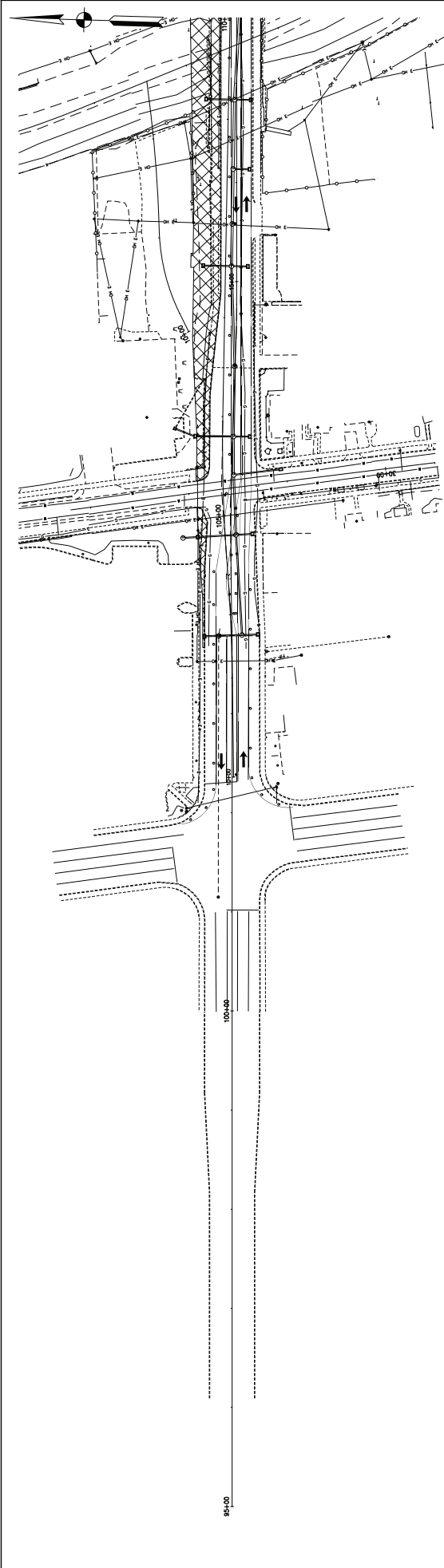
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100+00



<p>----- Temporary Barrier</p> <p>-- Detour Route</p> <p>  Direction of Traffic   Construction Area   Standard Drum </p>	<p><b>LEGEND</b></p> <p><b>A.A.</b>    <i>Type 2<sup>nd</sup> Construction Warning Light</i></p> <p>-----    <i>Type 1<sup>st</sup> Construction Warning Light</i></p> <p><b>B.</b>    <i>Type III Barricade (See Culture for Type)</i></p> <p><b>C.</b>    <i>Construction Sign w/ Light Warning</i></p> <p>①    <i>Temporary Pavement Marking, Solid, Yellow, 4 in.</i></p> <p>②    <i>Temporary Pavement Marking, Solid, White, 4 in.</i></p> <p>③    <i>Temporary Pavement Marking, Solid, White, 24 in.</i></p>	<table border="1"> <tr> <td colspan="2">RECOMMENDED FOR APPROVAL</td> <td>DESIGN ENGINEER</td> <td>DATE</td> </tr> <tr> <td colspan="2"></td> <td></td> <td></td> </tr> <tr> <td>DESIGNED:</td> <td>ASU</td> <td>DRAWN:</td> <td>PJT</td> </tr> <tr> <td>CHECKED:</td> <td>SNC</td> <td>CHECKED:</td> <td>SNC</td> </tr> </table>	RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE					DESIGNED:	ASU	DRAWN:	PJT	CHECKED:	SNC	CHECKED:	SNC	<table border="1"> <tr> <td colspan="2">INDIANA</td> <td>HORIZONTAL SCALE</td> <td>BRIDGE FILE</td> </tr> <tr> <td colspan="2">DEPARTMENT OF TRANSPORTATION</td> <td>VERTICAL SCALE</td> <td>DESIGNATION NO.</td> </tr> <tr> <td colspan="2"></td> <td></td> <td>1005154</td> </tr> <tr> <td colspan="2">MAINTENANCE OF TRAFFIC</td> <td>SURVEY BOOK</td> <td>SHEETS</td> </tr> <tr> <td colspan="2">PHASE I</td> <td>CONTRACT</td> <td>46 of 400</td> </tr> <tr> <td colspan="2"></td> <td></td> <td>PROJECT NO.</td> </tr> <tr> <td colspan="2"></td> <td></td> <td>1005154</td> </tr> </table>	INDIANA		HORIZONTAL SCALE	BRIDGE FILE	DEPARTMENT OF TRANSPORTATION		VERTICAL SCALE	DESIGNATION NO.				1005154	MAINTENANCE OF TRAFFIC		SURVEY BOOK	SHEETS	PHASE I		CONTRACT	46 of 400				PROJECT NO.				1005154
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE																																												
DESIGNED:	ASU	DRAWN:	PJT																																												
CHECKED:	SNC	CHECKED:	SNC																																												
INDIANA		HORIZONTAL SCALE	BRIDGE FILE																																												
DEPARTMENT OF TRANSPORTATION		VERTICAL SCALE	DESIGNATION NO.																																												
			1005154																																												
MAINTENANCE OF TRAFFIC		SURVEY BOOK	SHEETS																																												
PHASE I		CONTRACT	46 of 400																																												
			PROJECT NO.																																												
			1005154																																												





**LEGEND**

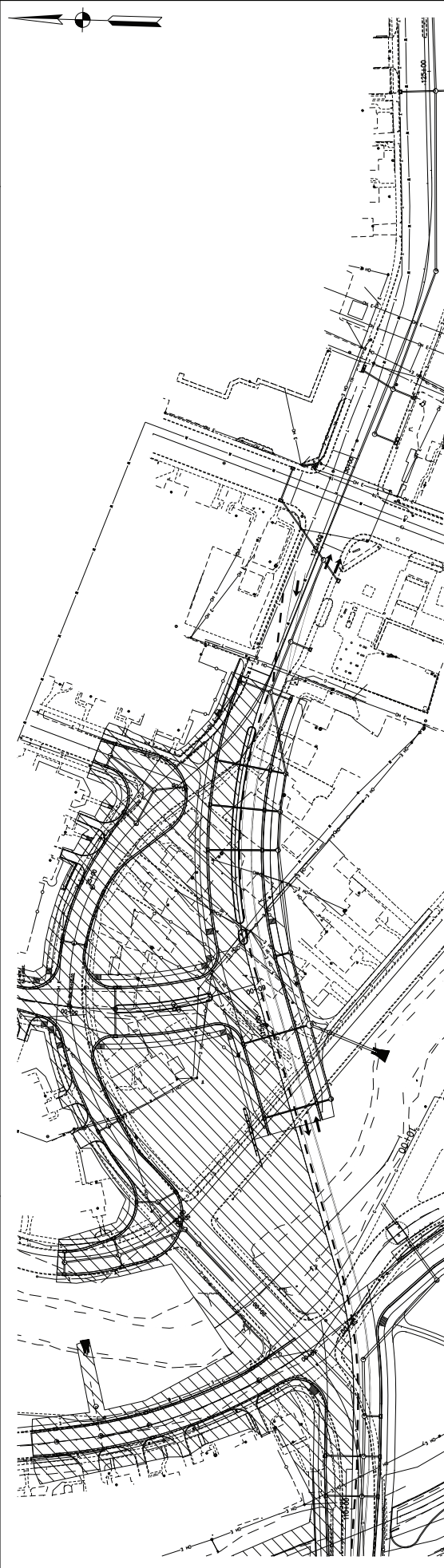
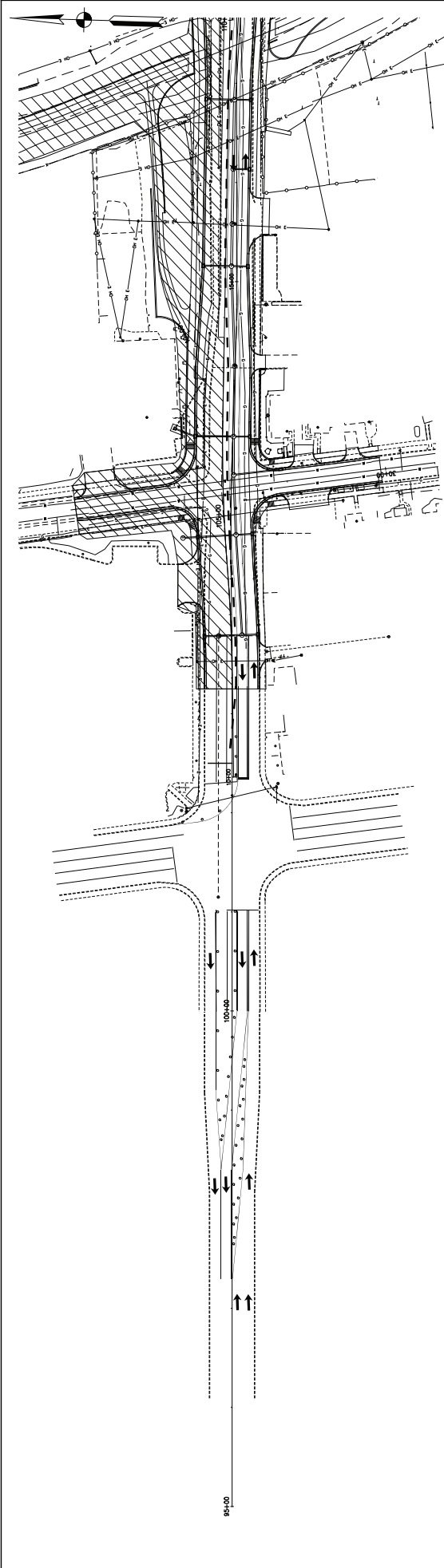
-----	Temporary Barrier	↓	Direction of Traffic	A.A.	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
- - - -	Detour Route	⊠	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		⊞	Standard Drum	⊞	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

**RECOMMENDED FOR APPROVAL**

DESIGNED:	ASU	DRAWN:	PJT
CHECKED:	SMC	CHECKED:	SMC

**INDIANA**  
**DEPARTMENT OF TRANSPORTATION**  
**MAINTENANCE OF TRAFFIC**  
**PHASE I-A**

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	47 of 100
	PROJECT NO.
	1005154



**LEGEND**

-----	Temporary Barrier	↓	Direction of Traffic	--- Type II Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.	
---	Detour Route	▨	Construction Area	--- Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.	
		▩	Standard Drum	③	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

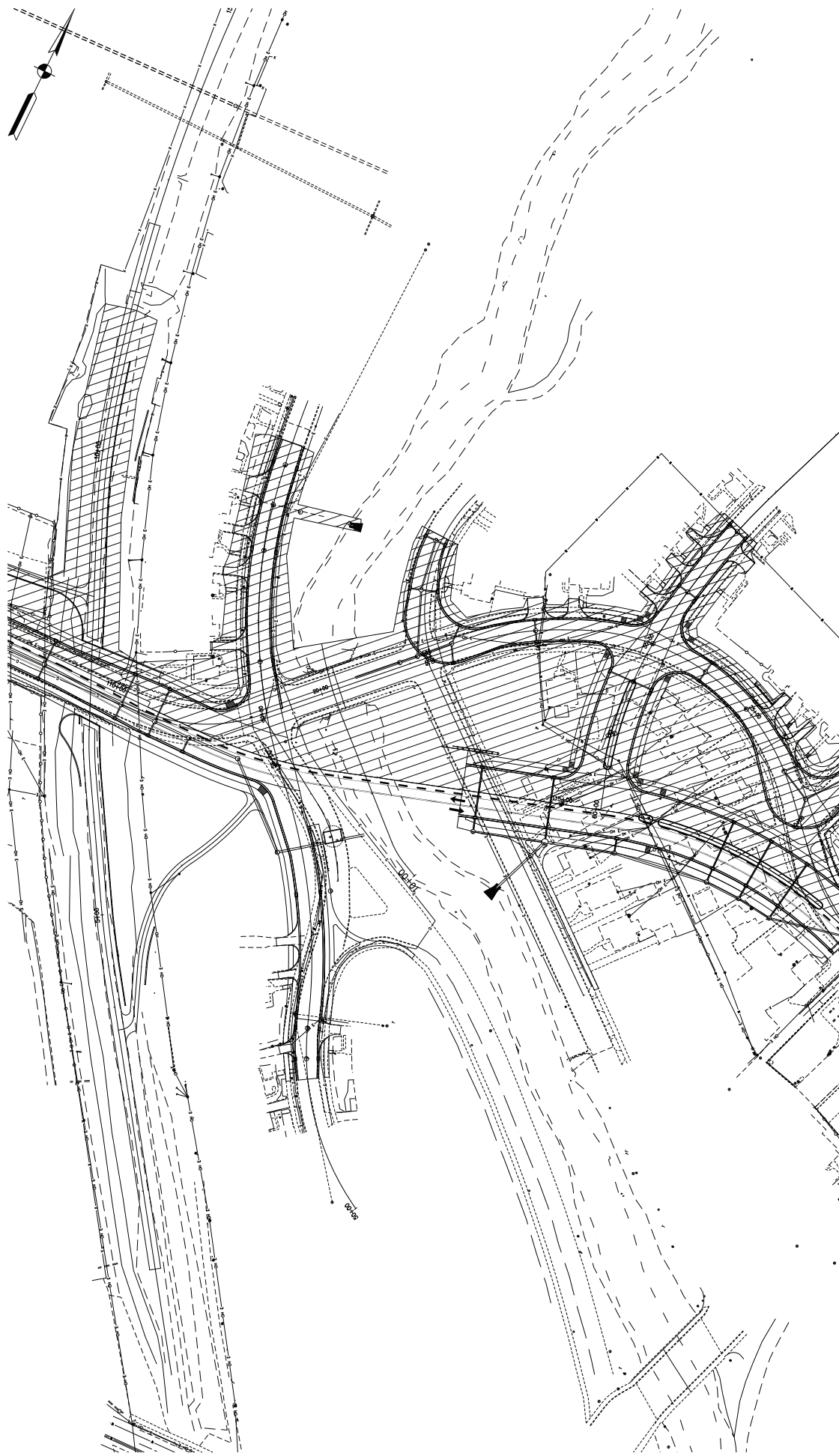
**RECOMMENDED FOR APPROVAL**

DESIGNED: ASU		DRAWN: PJT		DATE:	
CHECKED: SMC		CHECKED: SMC			

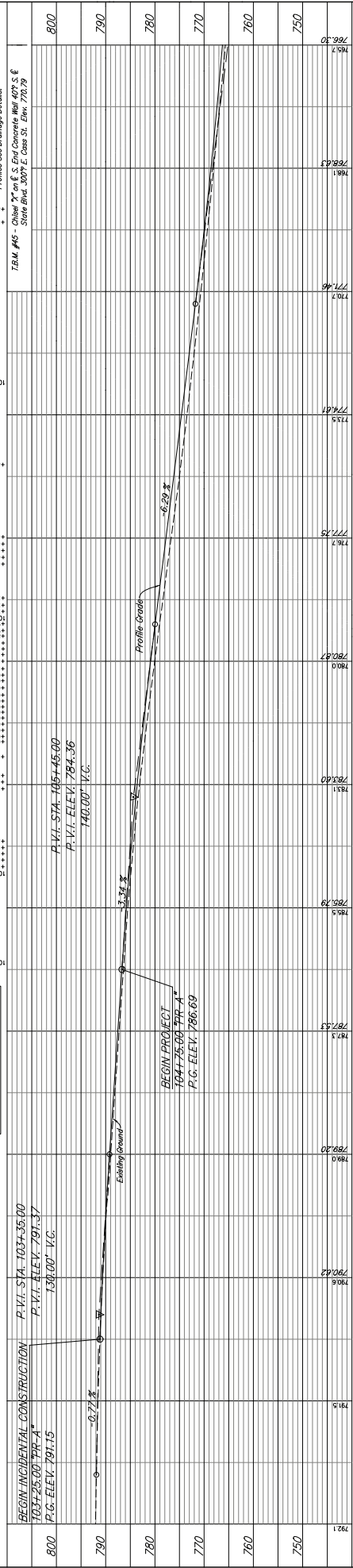
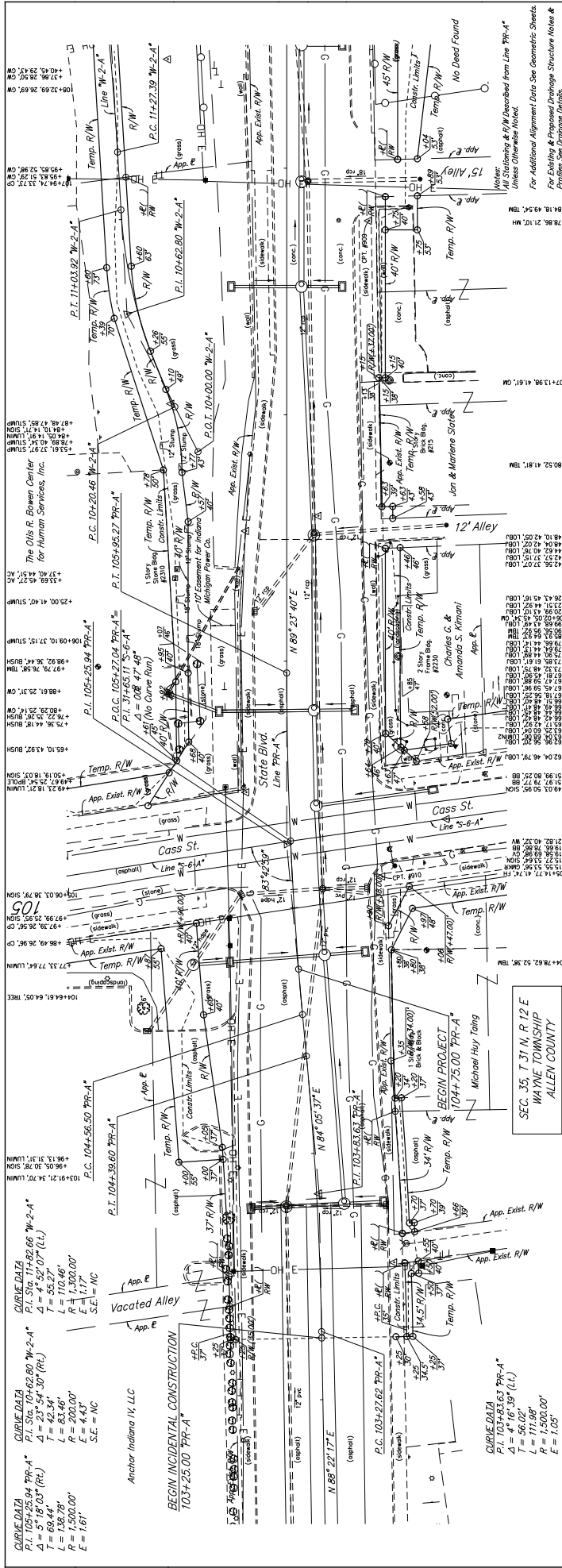
INDIANA  
DEPARTMENT OF TRANSPORTATION  
MAINTENANCE OF TRAFFIC  
PHASE II

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	48 of 100
	PROJECT NO.
	1005154



[illegible]

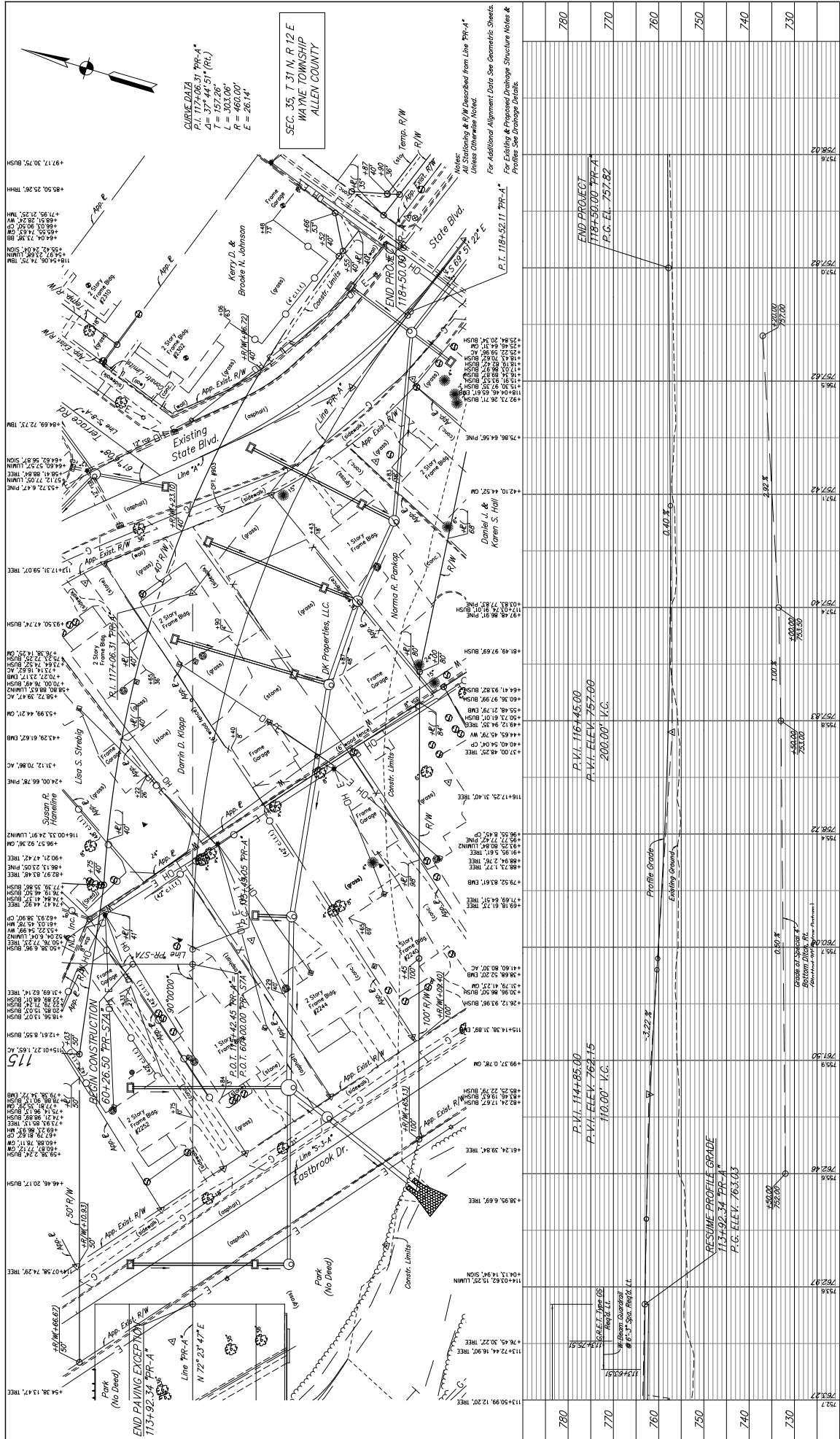
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INDIANA DEPARTMENT OF TRANSPORTATION			
PLAN & PROFILE			
LINE "PR-A"			
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: SMC	CHECKED: SMC	
CHECKED: SMC			
BRIDGE FILE	DESIGNATION NO.	SHEETS	PROJECT NO.
100514	100514	50	100514
SURVEY BOOK	CONTRACT		



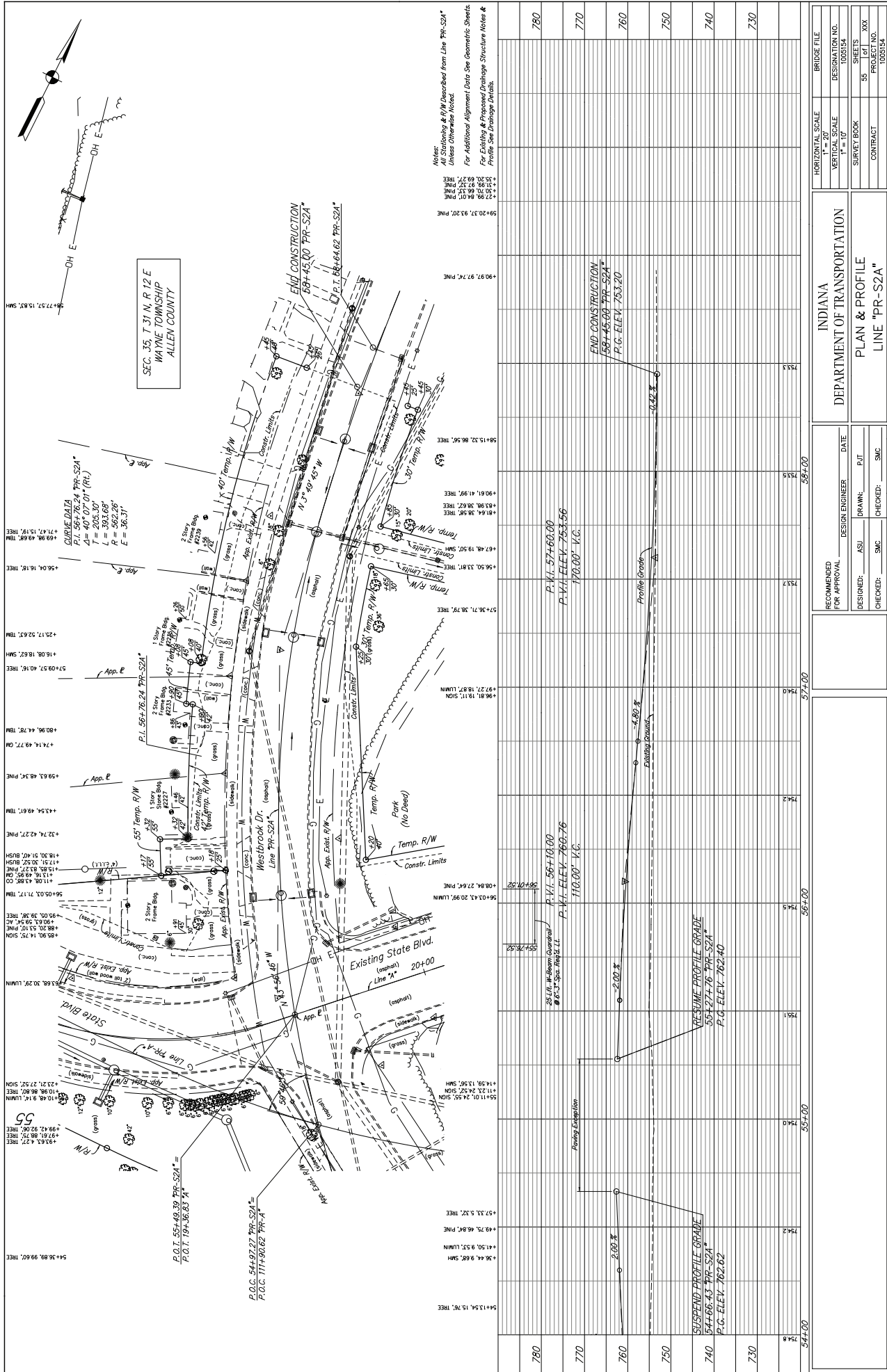




INDIANA		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DEPARTMENT OF TRANSPORTATION		DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC	
PLAN & PROFILE		CHECKED: SMC		PROJECT NO.		100514	
LINE "PR-A"		CONTRACT		SHEETS		52 of 100	
				SURVEY BOOK		100514	
				DESIGNATION NO.		100514	
				BRIDGE FILE			
				HORIZONTAL SCALE		1" = 10'	
				VERTICAL SCALE		1" = 10'	





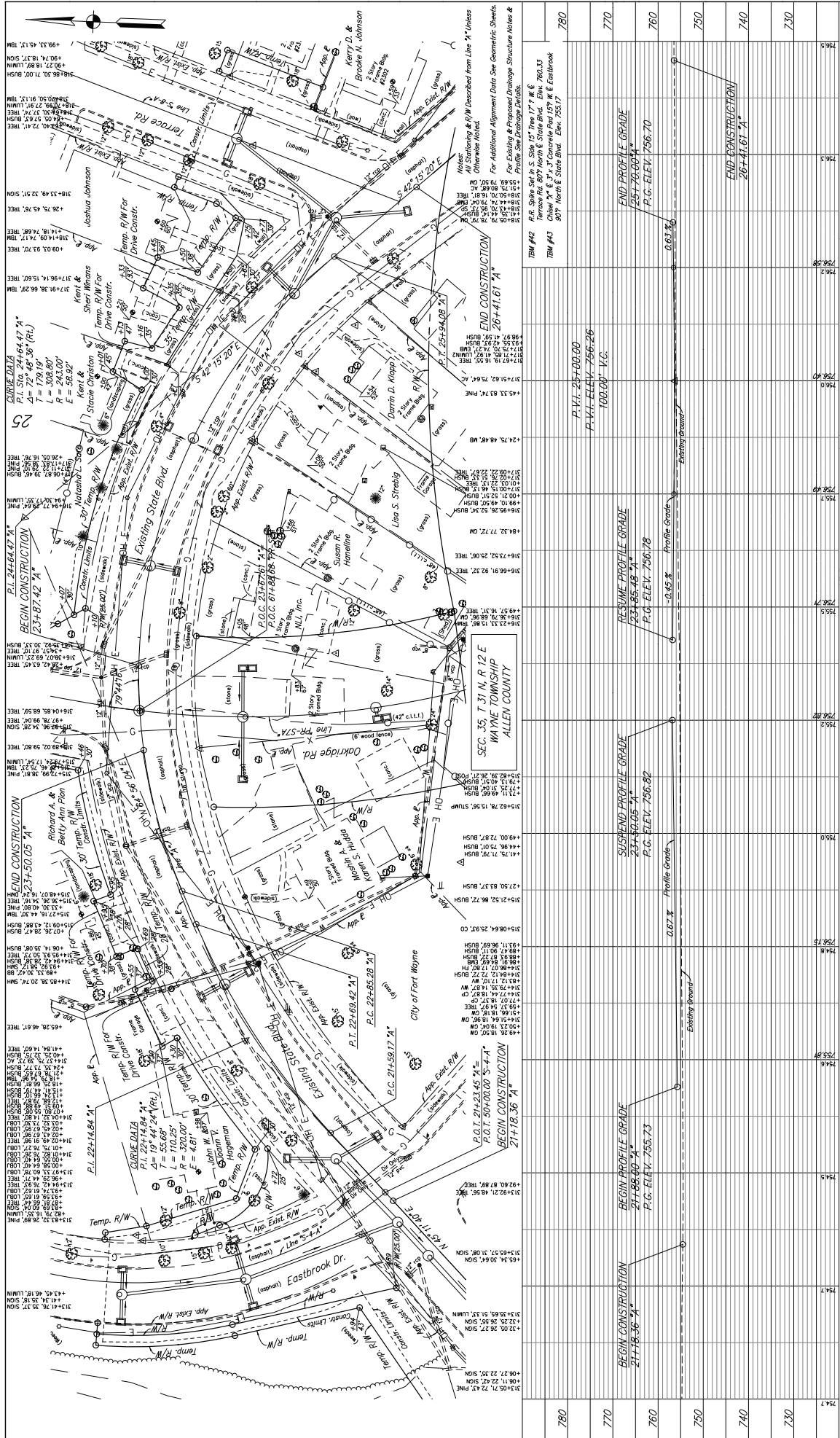


INDIANA		DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DESIGNATION NO.	
DATE		PJT		100514	
DESIGNED: ASU		DRAWN: SMC		SHEETS	
CHECKED: SMC		CHECKED: SMC		55	
				SURVEY BOOK	
				CONTRACT	
				PROJECT NO.	
				100514	

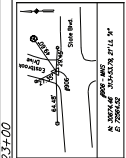








INDIANA				DEPARTMENT OF TRANSPORTATION				PLAN & PROFILE			
RECOMMENDED FOR APPROVAL				DESIGN ENGINEER				CONTRACT			
DATE				DRAWN				PROJECT NO.			
DESIGNED				CHECKED				SHEETS			
ASU				SMC				57			
SMC				SMC				100514			



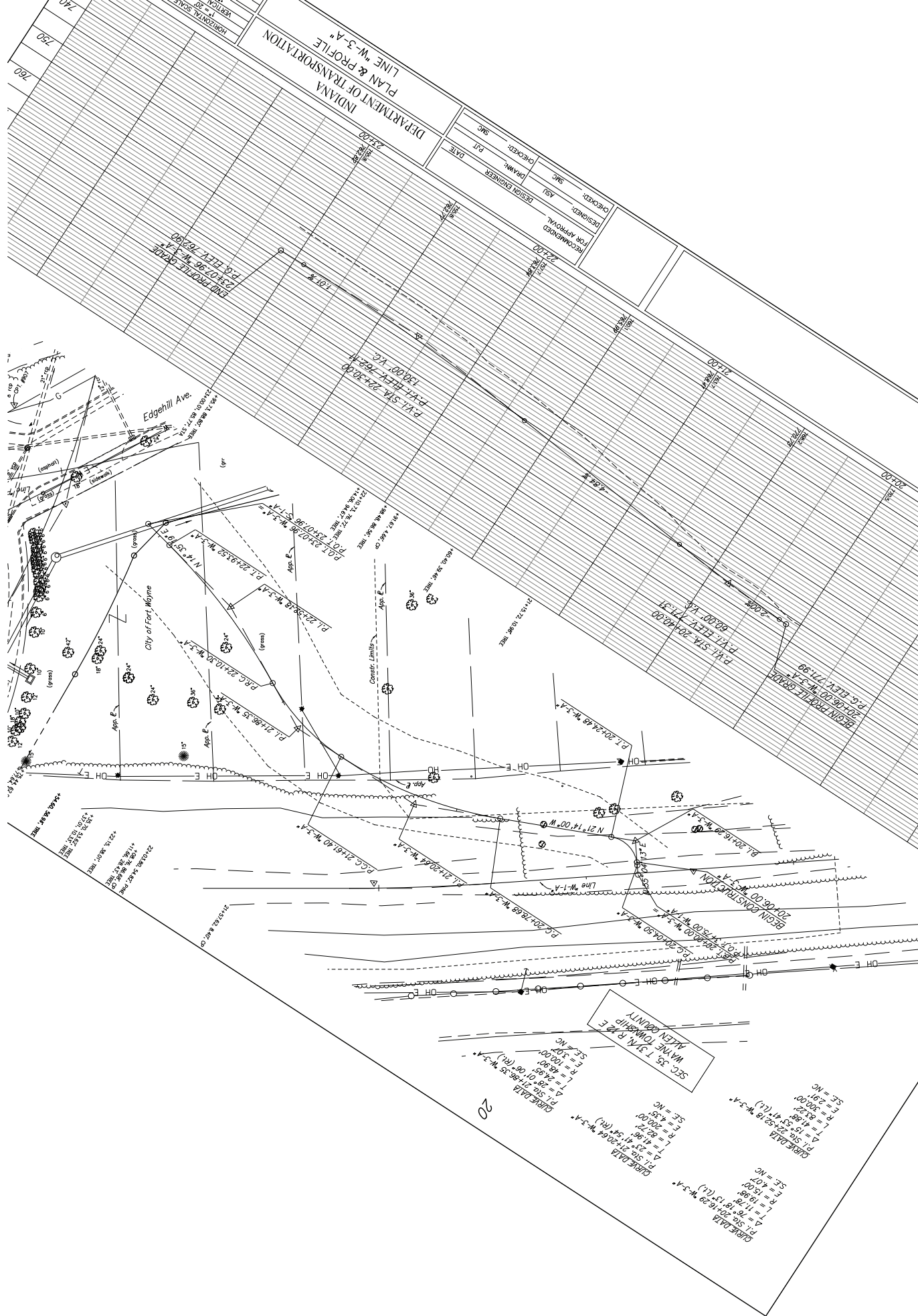
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RECOMMENDED FOR APPROVAL				DESIGN ENGINEER				CONTRACT			
DATE				DRAWN				PROJECT NO.			
DESIGNED				CHECKED				SHEETS			
ASU				SMC				57			
SMC				SMC				100514			

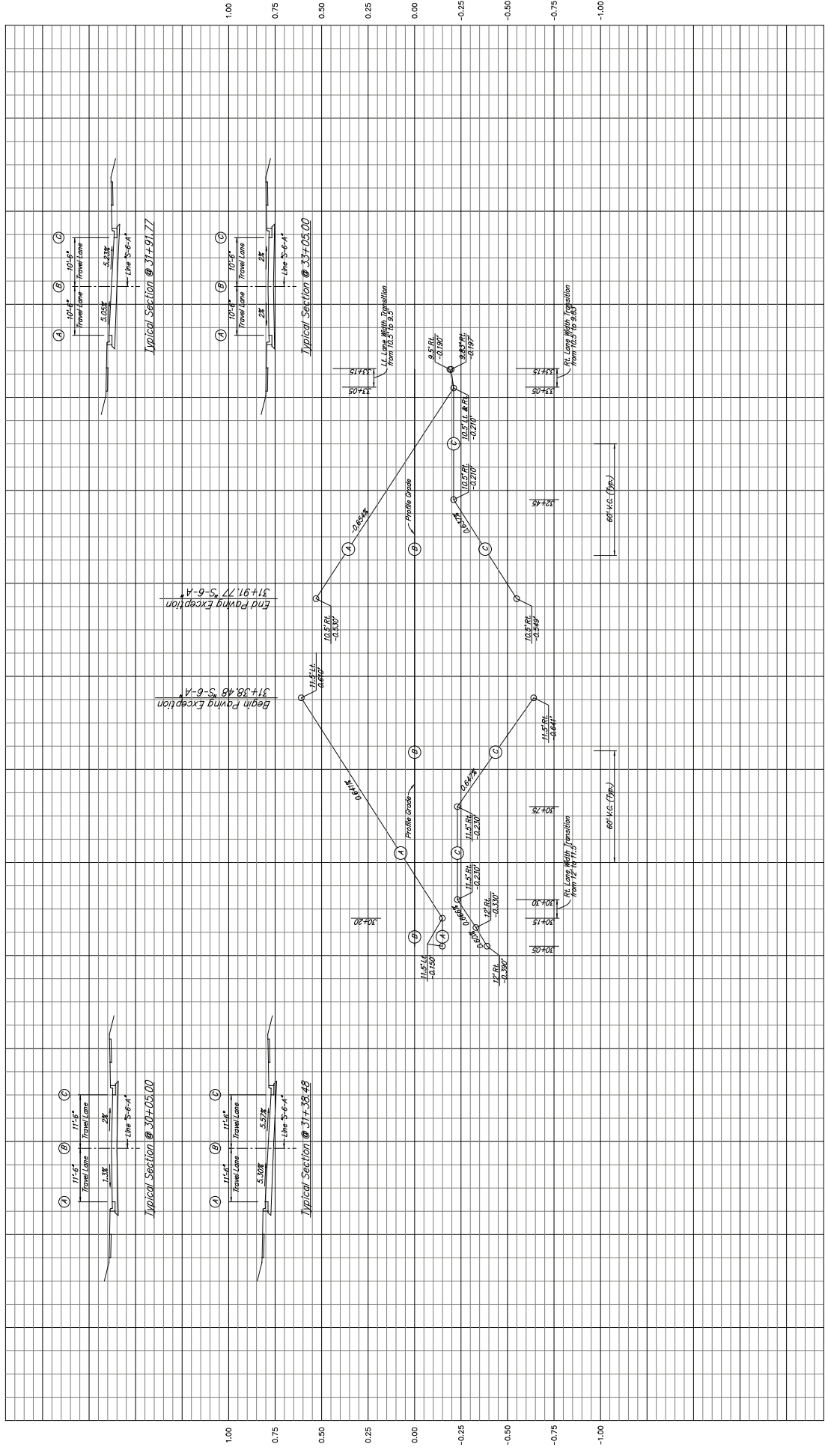






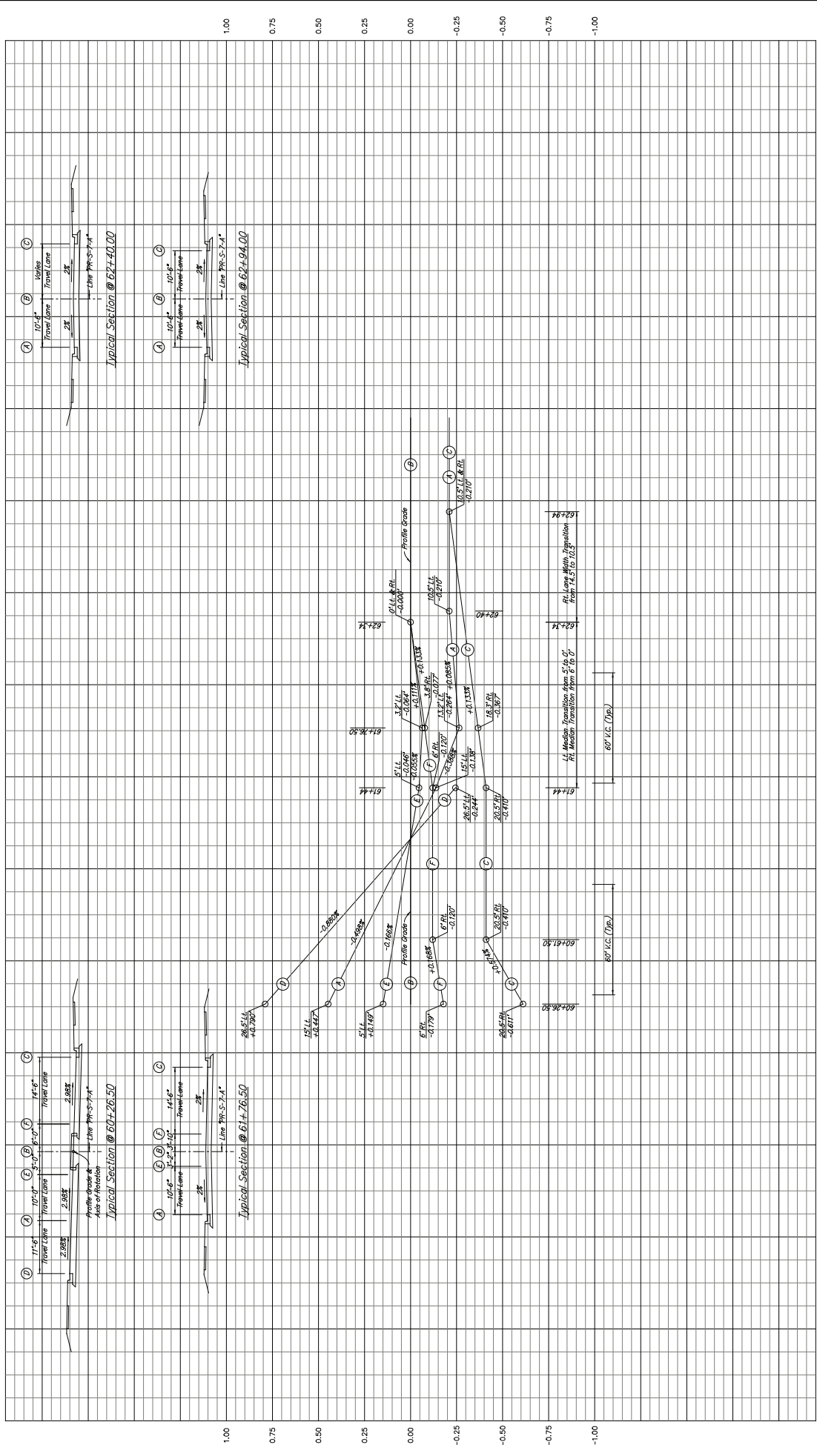






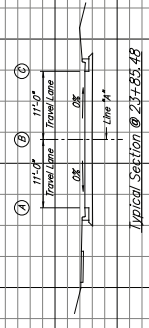
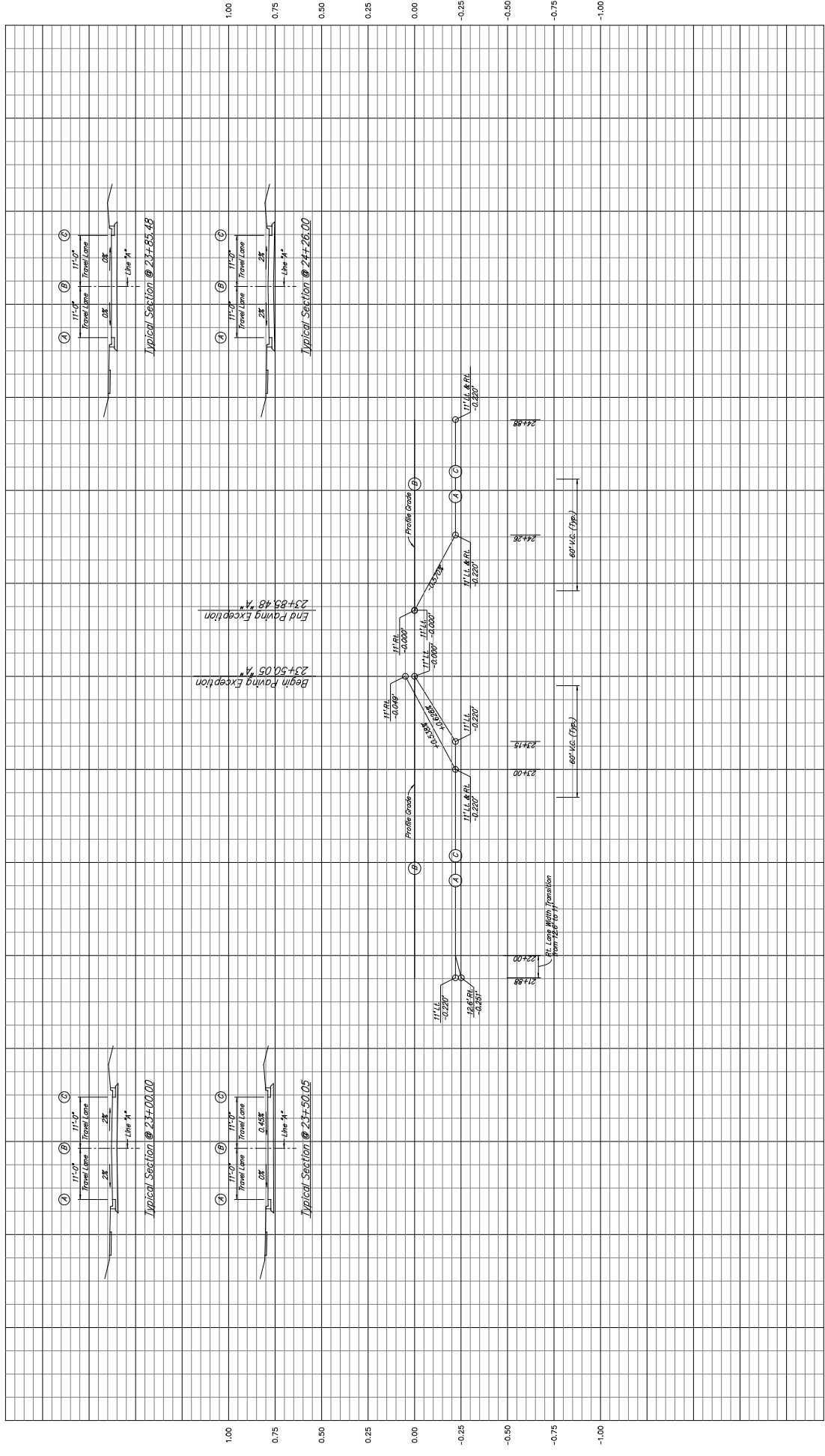
INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGNATION NO. 100514	
DESIGNED: ASU	CHECKED: SMC	SHEETS 62 of 100	
DATE		PROJECT NO. 100514	
DRAWN: ASU		CONTRACT	
CHECKED: SMC		SHEETS 62 of 100	
PAVEMENT EDGE TRANSITION DIAGRAM LINE S-6-A		CONTRACT	



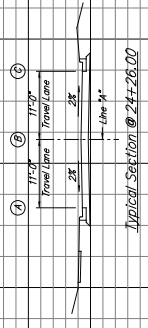


				INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		DESIGNATION NO.	
		DESIGNED:		ASU		DRAWN:		ASU	
		CHECKED:		SMC		CHECKED:		SMC	
								SHEETS	
								64 of 100	
								SURVEY BOOK	
								CONTRACT	
								PROJECT NO.	
								100514	





Typical Section @ 23+00.00



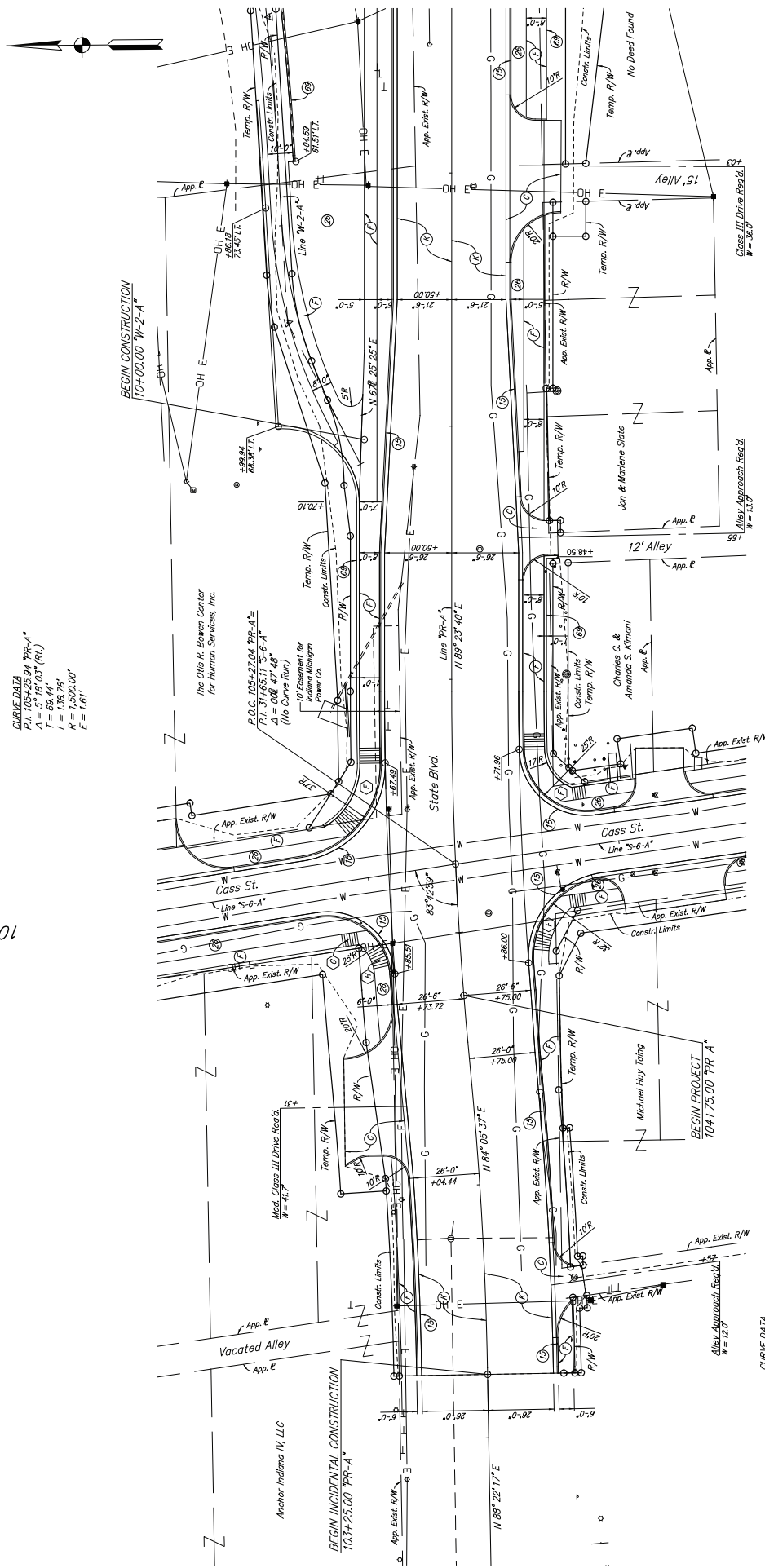
Typical Section @ 23+50.05

Begin Paving Exception  
23+50.05 4'±

End Paving Exception  
23+83.48 4'±

Typical Section @ 24+26.00

INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE VERTICAL SCALE 1" = 0.25'	BRIDGE FILE DESIGNATION NO. 100514
PAVEMENT EDGE TRANSITION DIAGRAM LINE "A"		SURVEY BOOK CONTRACT	SHEETS 65 of 100 PROJECT NO. 100514
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER DATE	
DESIGNED: CHECKED:		ASU SMC	ASU SMC



SEC. 35, T 31 N, R 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY

Notes:  
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

INDIANA		DEPARTMENT OF TRANSPORTATION	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
DATE		DATE	
DESIGNED: ASU		DRAWN: PJT	
CHECKED: SMC		CHECKED: SMC	
SURVEY BOOK		SHEETS	
CONTRACT		68	
PROJECT NO.		XXX	
1005154		1005154	

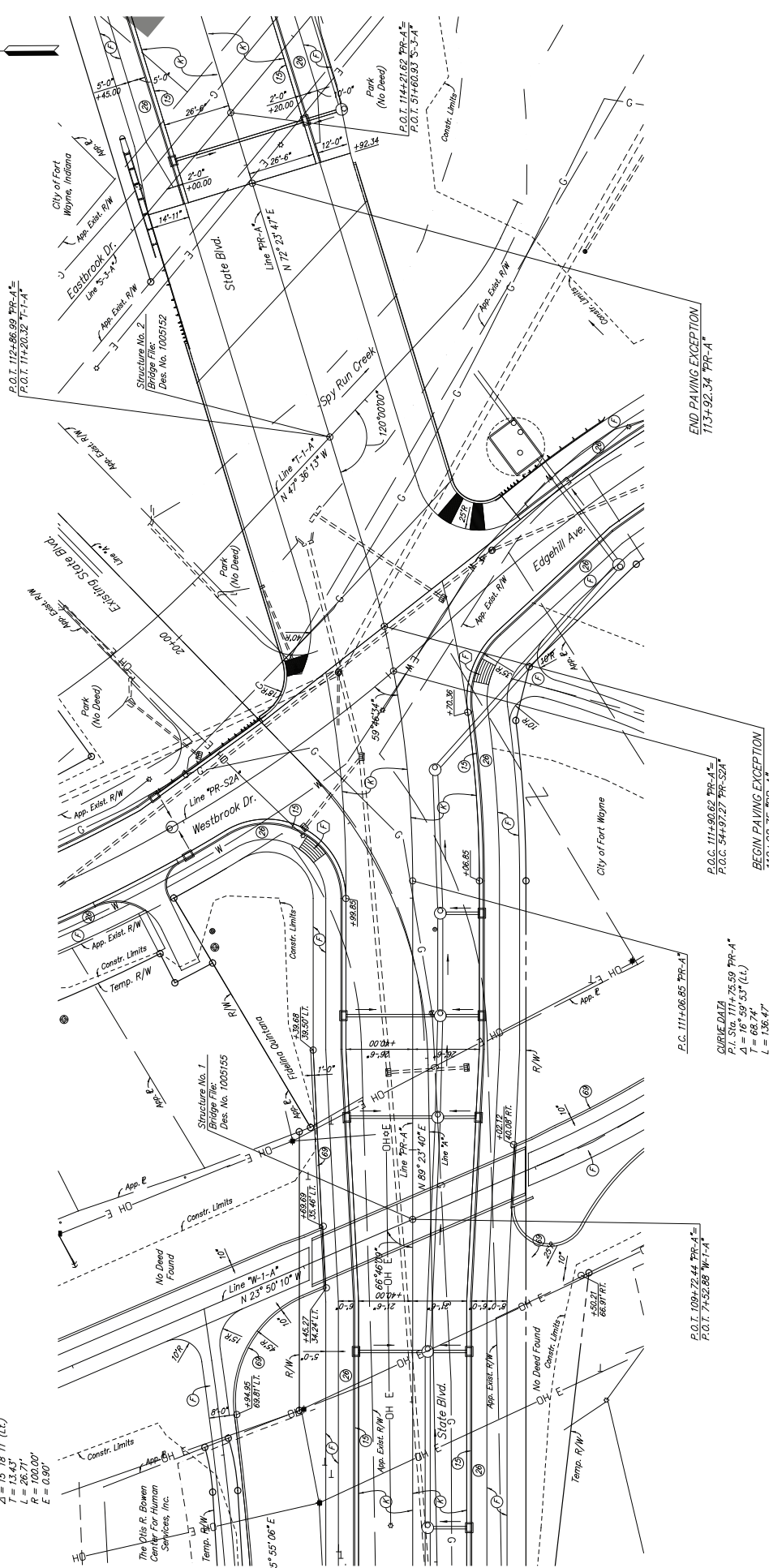
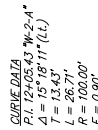
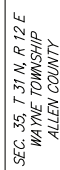
  

<p>⑦ 16S #5yd. HMA Surface, Type 1<sup>st</sup>, on 2<sup>nd</sup> #5yd. HMA Intermediate, Type 1<sup>st</sup>, on 6" Compacted Aggregate, No. 53, Base</p> <p>⑧ x<sup>st</sup> Cement Concrete Pavement for Driveways</p> <p>⑨ x<sup>st</sup> #5yd. HMA for Approaches Type 1<sup>st</sup>, on 6" Compacted Aggregate Base Type 1<sup>st</sup>, No. xx</p> <p>⑩ Concrete Slabwalk, 4"</p>		<p>⑪ Concrete Curb</p> <p>⑫ Combined Concrete Curb and Gutter, Modified</p> <p>⑬ Concrete Curb Ramp</p> <p>⑭ Concrete Curb, Type 2<sup>nd</sup></p> <p>⑮ Siding, Nursery</p>	
<p>⑯ Mulched Siding, Type 1<sup>st</sup></p> <p>⑰ Retaining Wall</p> <p>⑱ Concrete Curb, Type 3<sup>rd</sup></p>		<p>⑲ Center Curb Removal Limits</p>	

<p>① Full Depth Pavement (See Typical Sections)</p> <p>② Siding, Nursery</p> <p>③ Concrete Curb Ramp</p> <p>④ Concrete Curb, Type 2<sup>nd</sup></p> <p>⑤ Combined Concrete Curb and Gutter, Modified</p> <p>⑥ 16S #5yd. HMA Surface, Type 1<sup>st</sup>, on 2<sup>nd</sup> #5yd. HMA Intermediate, Type 1<sup>st</sup>, on 6" Compacted Aggregate, No. 53, Base</p> <p>⑦ x<sup>st</sup> Cement Concrete Pavement for Driveways</p> <p>⑧ x<sup>st</sup> #5yd. HMA for Approaches Type 1<sup>st</sup>, on 6" Compacted Aggregate Base Type 1<sup>st</sup>, No. xx</p> <p>⑨ Concrete Slabwalk, 4"</p>		<p>⑩ Concrete Curb</p> <p>⑪ Concrete Curb, Type 3<sup>rd</sup></p> <p>⑫ Concrete Curb Ramp</p> <p>⑬ Concrete Curb, Type 2<sup>nd</sup></p> <p>⑭ Combined Concrete Curb and Gutter, Modified</p> <p>⑮ 16S #5yd. HMA Surface, Type 1<sup>st</sup>, on 2<sup>nd</sup> #5yd. HMA Intermediate, Type 1<sup>st</sup>, on 6" Compacted Aggregate, No. 53, Base</p> <p>⑯ x<sup>st</sup> Cement Concrete Pavement for Driveways</p> <p>⑰ x<sup>st</sup> #5yd. HMA for Approaches Type 1<sup>st</sup>, on 6" Compacted Aggregate Base Type 1<sup>st</sup>, No. xx</p> <p>⑱ Concrete Slabwalk, 4"</p>	
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Series	7/12/2011	12:46:04 PM	P:\N2007\1404\0. Drawings\20071404.RD.P2.CD.01.dgn
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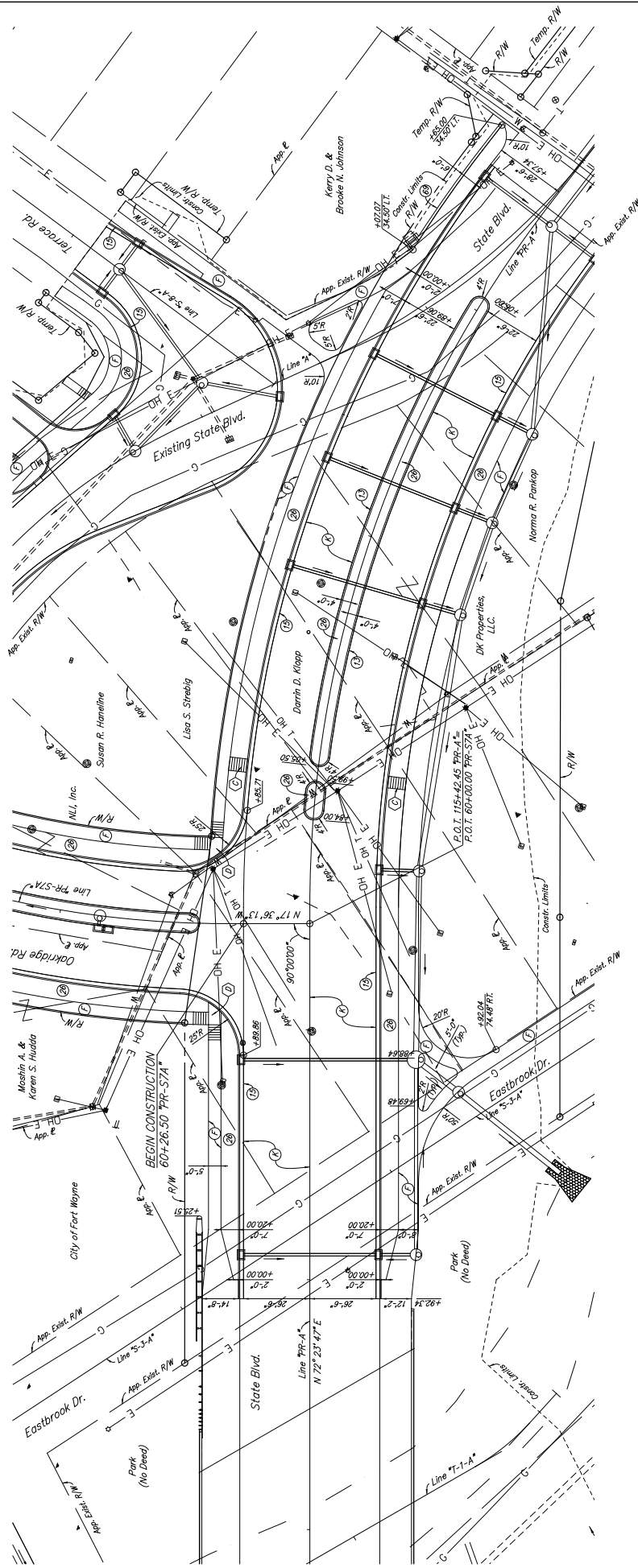


Notes:  
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

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Printed: 7/12/2011 12:46:05 PM P:\N2007\1404\0. Drawings\20071404.RD.P2.CD.02.dgn

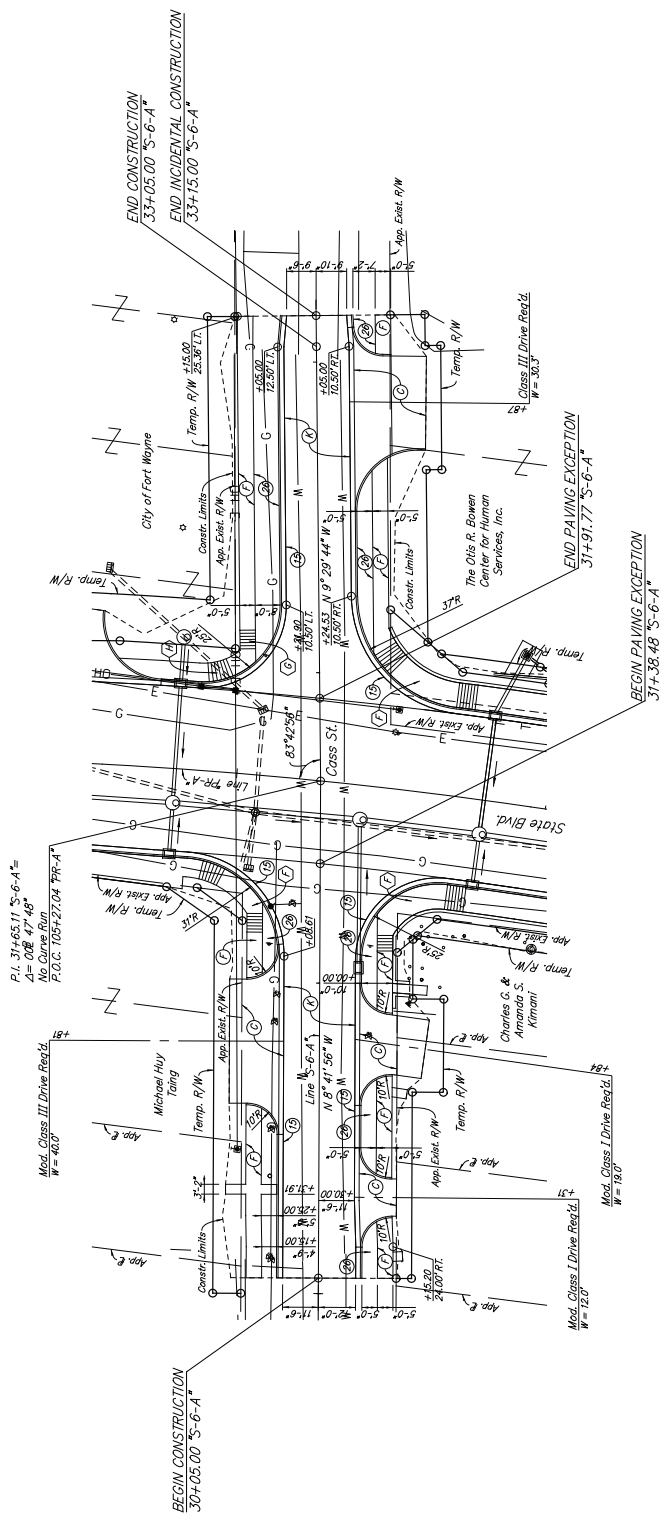
SEC. 35, T 31 N, R 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY



Notes:  
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p><b>LEGEND</b></p> <p>⑦ 16S #5yd. MMA Surface, Type "A" on 27S #5yd. MMA Intermediate, Type "A" on 6" Compacted Aggregate, No. 53. Base</p> <p>⑧ 16S #5yd. MMA for Approaches Type "A" on 6" Compacted Aggregate, No. 53. Base</p> <p>⑨ Concrete Slabwork, 4"</p> <p>⑩ Concrete Curb</p> <p>⑪ Combined Concrete Curb and Culvert, Modified</p> <p>⑫ Concrete Curb Ramp</p> <p>⑬ Concrete Center Cuts, Type "B"</p> <p>⑭ Sodding, Nursery</p> <p>⑮ Full Depth Pavement (See Typical Sections)</p> <p>⑯ Center Curb Removal Limits</p>		<p>⑰ Matched Sodding, Type "B"</p> <p>⑱ Retaining Wall</p> <p>⑲ Concrete Curb</p> <p>⑳ Sodding, Nursery</p>	
<p>① Concrete Curb</p> <p>② Combined Concrete Curb and Culvert, Modified</p> <p>③ Concrete Curb Ramp</p> <p>④ Concrete Center Cuts, Type "B"</p> <p>⑤ Sodding, Nursery</p> <p>⑥ Full Depth Pavement (See Typical Sections)</p> <p>⑦ Center Curb Removal Limits</p>		<p>⑧ Matched Sodding, Type "B"</p> <p>⑨ Retaining Wall</p> <p>⑩ Concrete Curb</p> <p>⑪ Sodding, Nursery</p>	

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INDIANA  
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS  
LINE "S-6-A"

RECOMMENDED FOR APPROVAL

DESIGNED: ASU  
CHECKED: SMC

DESIGN ENGINEER: DATE  
DRAWN: PAT  
CHECKED: SMC

LEGEND

165 #5/Syd. HMA Surface, Type 1A; on 275 #5/Syd. HMA Intermediate, Type 1A; on 6" Compacted Aggregate, No. 33, Base

xx" Compacted Aggregate Base, Type 1A; No. xx

Concrete Sidewalk, 4"

Concrete Curb

Combined Concrete Curb and Gutter, Modified

Concrete Center Curb, Type 3"

Sodding, Nursery

165 #5/Syd. HMA Surface, Type 1A; on 275 #5/Syd. HMA Intermediate, Type 1A; on 6" Compacted Aggregate, No. 33, Base

xx" Compacted Aggregate Base, Type 1A; No. xx

Concrete Sidewalk, 4"

Concrete Curb

Combined Concrete Curb and Gutter, Modified

Concrete Center Curb, Type 3"

Sodding, Nursery

28 Mulched Sanding, Type 1"

28 Retaining Wall

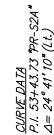
28 Concrete Curb, Type 3"

28 Concrete Curb, Type 3"

28 Concrete Curb, Type 3"

7/12/2011 12:46:07 PM P:\IN0007\10460 - Drawings\0001\10460.DWG 10/25/06 544.dgn



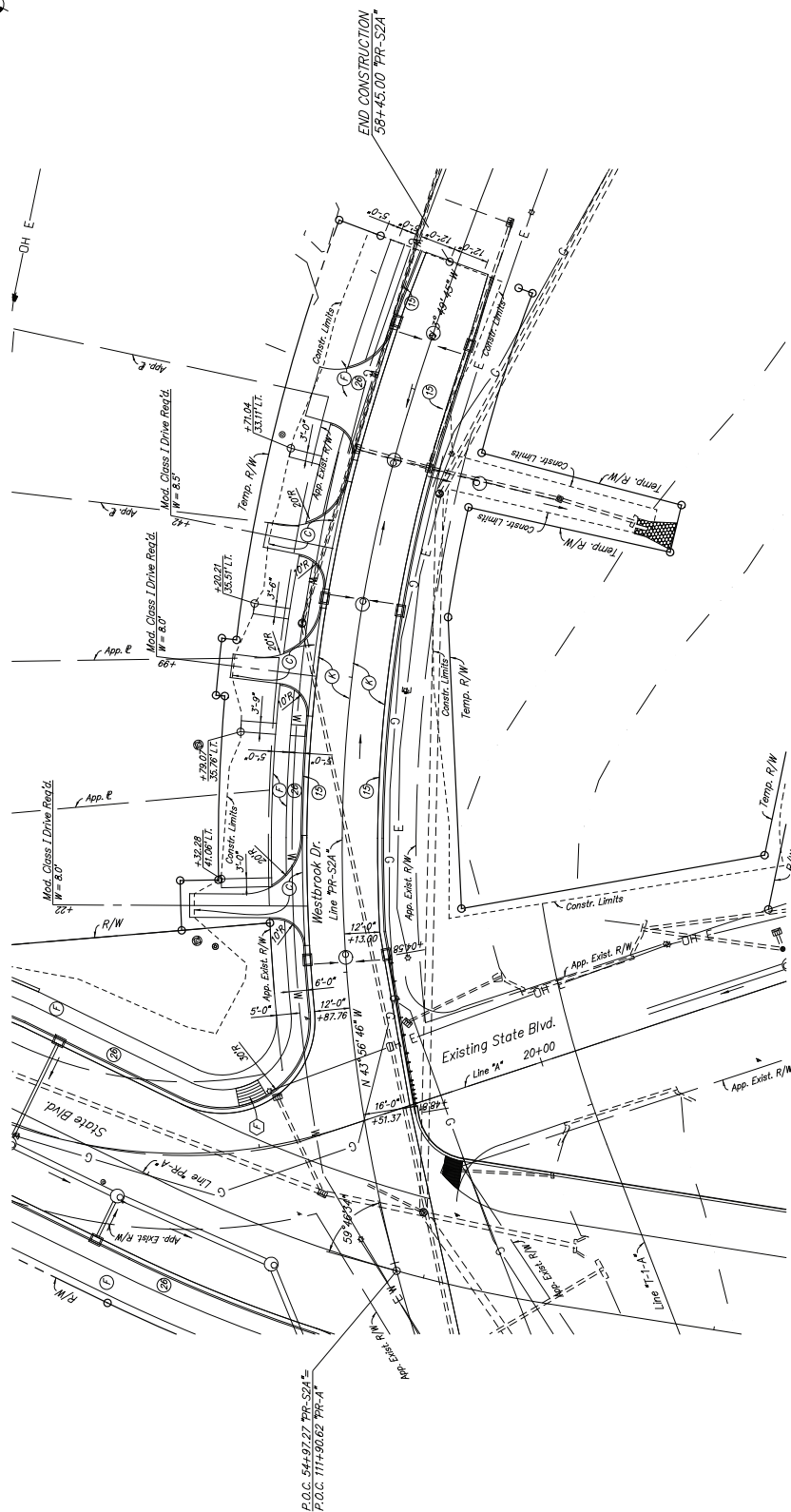


Notes:  
All Stationing & R/W Described from Line P2-S2A\* Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

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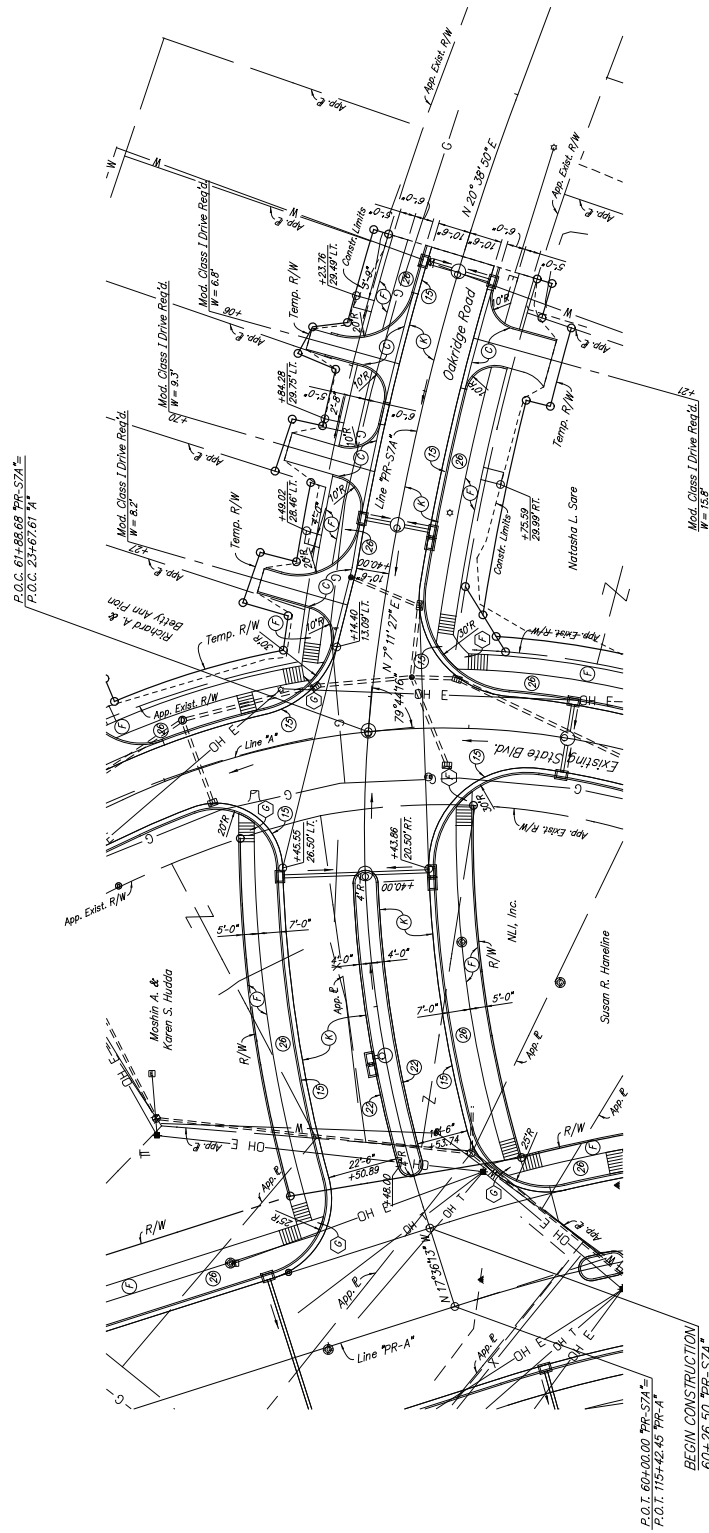
CURVE DATA  
P.I. 56+76.24 "PR-S2A"  
 $\Delta = 40^{\circ} 07' 01"$  (Rt.)  
 $T = 205.30'$   
 $L = 393.68'$   
 $R = 562.26'$   
 $E = 36.31'$



Notes:  
All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>INDIANA DEPARTMENT OF TRANSPORTATION</p>		<p>CONSTRUCTION DETAILS</p>	
<p>RECOMMENDED FOR APPROVAL</p>		<p>DESIGN ENGINEER</p>	
<p>DATE</p>		<p>CAK</p>	
<p>DESIGNED: ASU</p>		<p>DRAWN: SMC</p>	
<p>CHECKED: SMC</p>		<p>CHECKED: SMC</p>	
<p>PROJECT NO. 1005154</p>		<p>CONTRACT 1005154</p>	
<p>BRIDGE FILE</p>		<p>BRIDGE FILE</p>	
<p>VERTICAL SCALE 1" = 20'</p>		<p>VERTICAL SCALE 1" = 20'</p>	
<p>DESIGNATION NO. 1005154</p>		<p>DESIGNATION NO. 1005154</p>	

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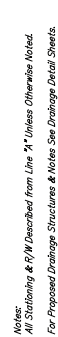
**CURVE DATA**  
P.I. 63+02.90 PR-S7A"  
Δ = 13° 27' 23" (Rt.)  
T = 96.20'  
L = 191.51'  
R = 815.43'  
E = 5.65'

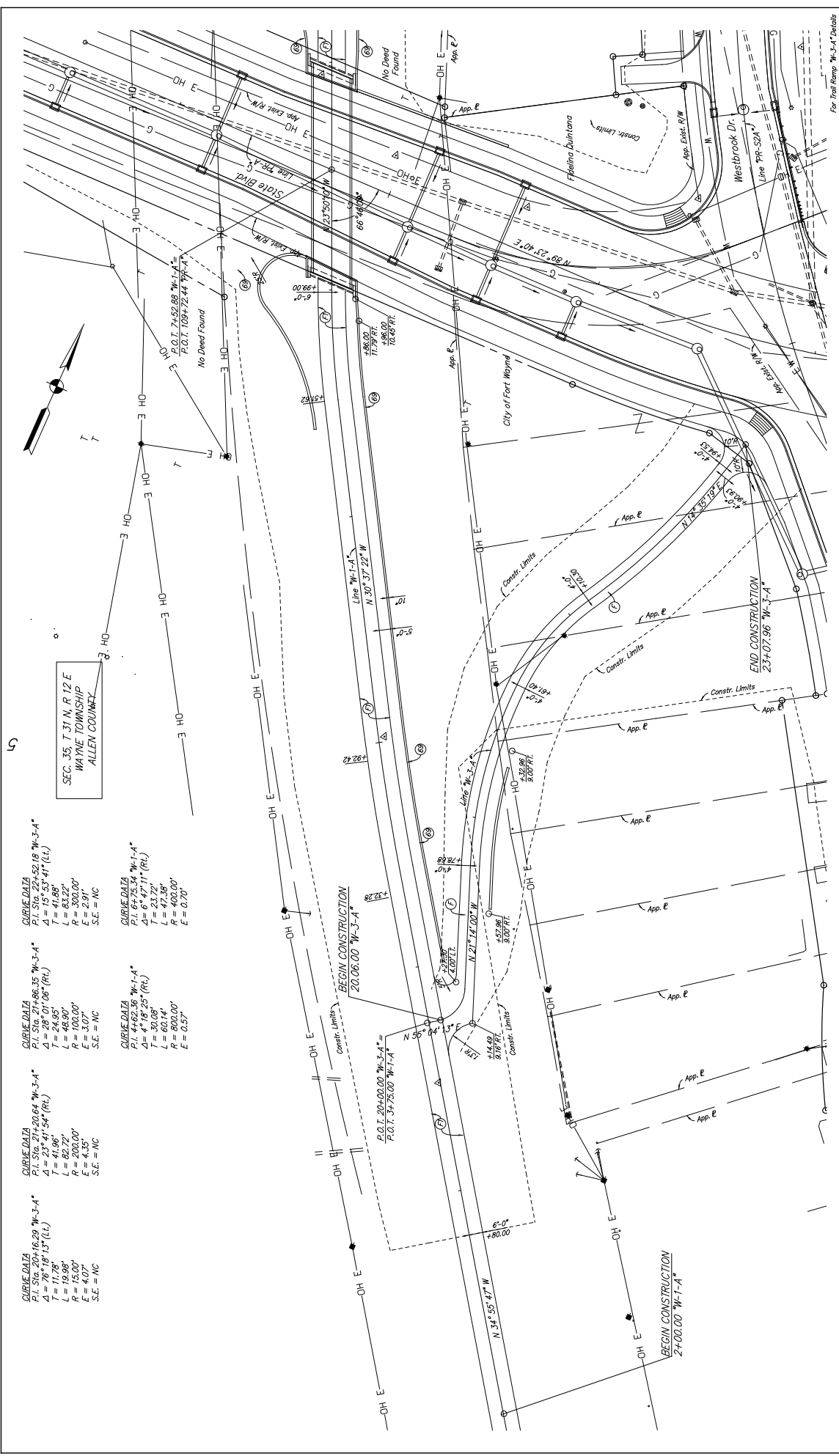
Notes:  
All Stationing & R/W Described from Line "PR-S7A" Unless Otherwise Noted.  
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(c) 4" Cement Concrete Pavement for Driveways	(7) 6S #5yd. HMA Surface, Type "A", on 27S #5yd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 33 Base	(1) Concrete Curb	(4) Mulched Standoff, Type "B"		RECOMMENDED FOR APPROVAL _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE $\frac{1}{8"} = 1'$	BRIDGE FILE _____
(d) 4x6 #5yd. HMA for Approaches Type "A", on 6" Compacted Aggregate, No. 33 Base	(8) 6" Cement Concrete Curb, Type "D"	(2) Combined Concrete Curb and Gutter, Modified	(5) Retaining Wall		DESIGN ENGINEER _____ DATE _____		VERTICAL SCALE _____	DESIGNATION NO. 1005154
(e) Concrete Sidewalk, 4"	(9) Full Depth Pavement (See Typical Sections)	(3) Concrete Curb Ramp (Type is indicated by Letter Inside Heavens)	(6) Sodding, Nursery		DESIGNED: ASU DRAWN: OAK	SURVEY BOOK _____		
				///	CHECKED: SMC CHECKED: SMC	CONTRACT _____		
						LINE "PR-S7A"		PROJECT NO. 1005154

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[illegible]



SEC. 35, T. 31 N. R. 12 E  
WAYNE TOWNSHIP  
ALLEN COUNTY

**CURVE DATA**  
P.I. Sta. 22+52.18 W-3-A  
T = 41.88'  
L = 83.22'  
R = 300.00'  
T = 23.72'  
P.O.T. 109+72.44 W-3-A  
S.E. = NC

**CURVE DATA**  
P.I. Sta. 21+48.35 W-3-A  
T = 24.95'  
L = 48.90'  
R = 100.00'  
T = 3.07'  
S.E. = NC

**CURVE DATA**  
P.I. Sta. 4+62.36 W-1-A  
T = 30.09'  
L = 60.18'  
R = 40.00'  
T = 0.57'  
S.E. = NC

**CURVE DATA**  
P.I. Sta. 6+75.34 W-1-A  
T = 41.88'  
L = 83.22'  
R = 300.00'  
T = 23.72'  
P.O.T. 109+72.44 W-3-A  
S.E. = NC

**RECOMMENDED FOR APPROVAL**

DESIGNED:	ASU	CHECKED:	SMC
DRAWN:	PAT	CHECKED:	SMC

**INDIANA**

**DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS**

**LINE "W-1-A" & "W-3-A"**

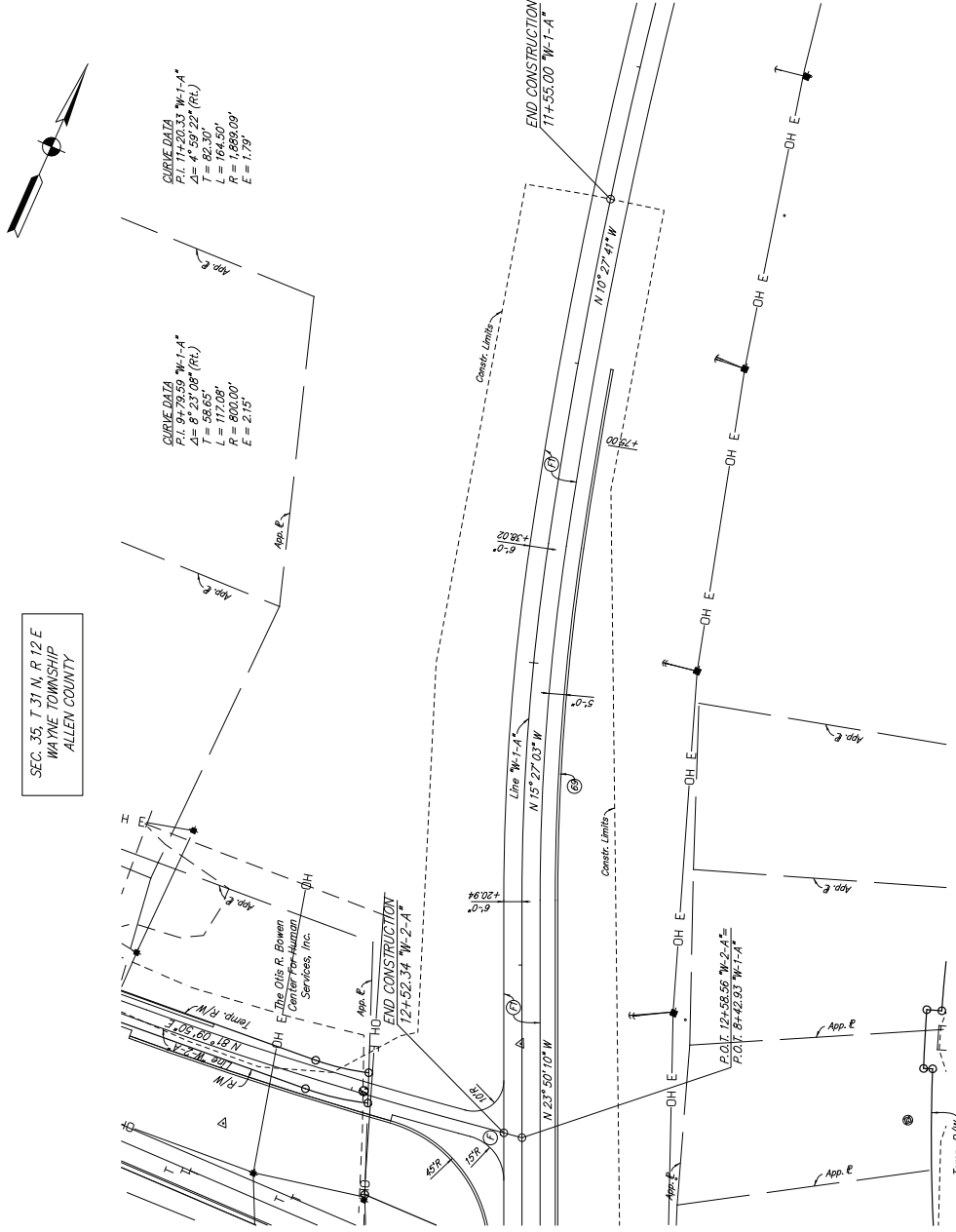
HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	100514
CONTRACT	SHEETS
	74 of 74
	PROJECT NO.
	100514

For Full Plans, See S-14 Details  
See S-14 for Further Details

**LEGEND**

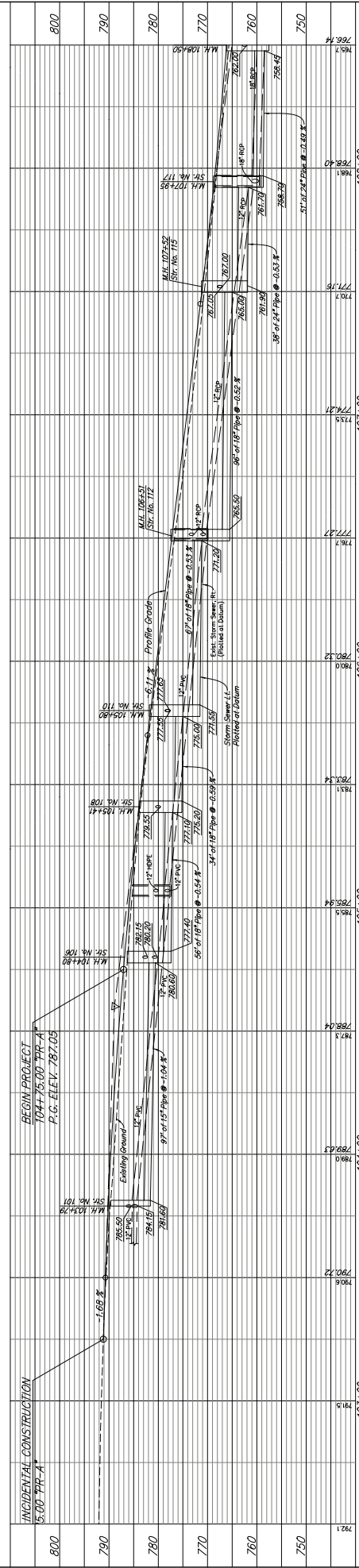
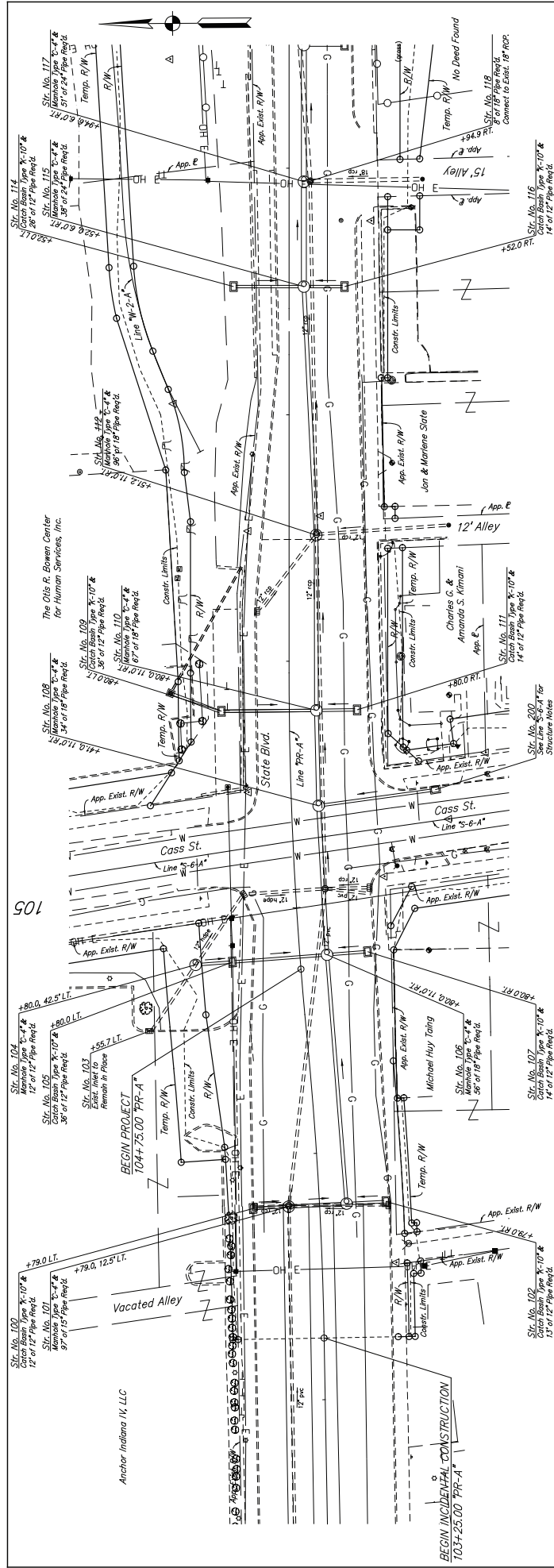
- (1) 165 #5yd. HMA Surface, Type "A", on
- (2) 275 #5yd. HMA Intermediate, Type "A", on
- (3) 6" Compacted Aggregate, No. 53, Base
- (4) 4" Compacted Aggregate, No. 53, Base
- (5) 4" Compacted Aggregate, No. 53, Base
- (6) 4" Compacted Aggregate, No. 53, Base
- (7) 4" Compacted Aggregate, No. 53, Base
- (8) 4" Compacted Aggregate, No. 53, Base
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- (100) 4" Compacted Aggregate, No. 53, Base

SEC. 35, T. 31 N., R. 12 E.  
WAYNE TOWNSHIP  
ALLEN COUNTY



<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER: _____ DATE: _____</p> <p>DRAWN: _____ P.I.T. _____</p> <p>CHECKED: _____ S.M.C. _____</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS</p> <p>LINE "W-1-A"</p>		<p>HORIZONTAL SCALE</p> <p>VERTICAL SCALE</p> <p>SURVEY BOOK</p> <p>CONTRACT</p>	<p>BRIDGE FILE</p> <p>DESIGNATION NO.</p> <p>SHEETS</p> <p>PROJECT NO.</p>
				<p>100514</p> <p>75 of 100</p> <p>100514</p>	<p>100514</p> <p>100514</p> <p>100514</p>

<p>1" = 40' Cement Concrete Pavement for Driveways</p> <p>2" = 80' Cement Concrete Pavement for Driveways</p> <p>3" = 120' Cement Concrete Pavement for Driveways</p> <p>4" = 160' Cement Concrete Pavement for Driveways</p> <p>5" = 200' Cement Concrete Pavement for Driveways</p> <p>6" = 240' Cement Concrete Pavement for Driveways</p> <p>7" = 280' Cement Concrete Pavement for Driveways</p> <p>8" = 320' Cement Concrete Pavement for Driveways</p> <p>9" = 360' Cement Concrete Pavement for Driveways</p> <p>10" = 400' Cement Concrete Pavement for Driveways</p> <p>11" = 440' Cement Concrete Pavement for Driveways</p> <p>12" = 480' Cement Concrete Pavement for Driveways</p> <p>13" = 520' Cement Concrete Pavement for Driveways</p> <p>14" = 560' Cement Concrete Pavement for Driveways</p> <p>15" = 600' Cement Concrete Pavement for Driveways</p> <p>16" = 640' Cement Concrete Pavement for Driveways</p> <p>17" = 680' Cement Concrete Pavement for Driveways</p> <p>18" = 720' Cement Concrete Pavement for Driveways</p> <p>19" = 760' Cement Concrete Pavement for Driveways</p> <p>20" = 800' Cement Concrete Pavement for Driveways</p>	<p>1" = 40' Cement Concrete Pavement for Driveways</p> <p>2" = 80' Cement Concrete Pavement for Driveways</p> <p>3" = 120' Cement Concrete Pavement for Driveways</p> <p>4" = 160' Cement Concrete Pavement for Driveways</p> <p>5" = 200' Cement Concrete Pavement for Driveways</p> <p>6" = 240' Cement Concrete Pavement for Driveways</p> <p>7" = 280' Cement Concrete Pavement for Driveways</p> <p>8" = 320' Cement Concrete Pavement for Driveways</p> <p>9" = 360' Cement Concrete Pavement for Driveways</p> <p>10" = 400' Cement Concrete Pavement for Driveways</p> <p>11" = 440' Cement Concrete Pavement for Driveways</p> <p>12" = 480' Cement Concrete Pavement for Driveways</p> <p>13" = 520' Cement Concrete Pavement for Driveways</p> <p>14" = 560' Cement Concrete Pavement for Driveways</p> <p>15" = 600' Cement Concrete Pavement for Driveways</p> <p>16" = 640' Cement Concrete Pavement for Driveways</p> <p>17" = 680' Cement Concrete Pavement for Driveways</p> <p>18" = 720' Cement Concrete Pavement for Driveways</p> <p>19" = 760' Cement Concrete Pavement for Driveways</p> <p>20" = 800' Cement Concrete Pavement for Driveways</p>	<p>1" = 40' Cement Concrete Pavement for Driveways</p> <p>2" = 80' Cement Concrete Pavement for Driveways</p> <p>3" = 120' Cement Concrete Pavement for Driveways</p> <p>4" = 160' Cement Concrete Pavement for Driveways</p> <p>5" = 200' Cement Concrete Pavement for Driveways</p> <p>6" = 240' Cement Concrete Pavement for Driveways</p> <p>7" = 280' Cement Concrete Pavement for Driveways</p> <p>8" = 320' Cement Concrete Pavement for Driveways</p> <p>9" = 360' Cement Concrete Pavement for Driveways</p> <p>10" = 400' Cement Concrete Pavement for Driveways</p> <p>11" = 440' Cement Concrete Pavement for Driveways</p> <p>12" = 480' Cement Concrete Pavement for Driveways</p> <p>13" = 520' Cement Concrete Pavement for Driveways</p> <p>14" = 560' Cement Concrete Pavement for Driveways</p> <p>15" = 600' Cement Concrete Pavement for Driveways</p> <p>16" = 640' Cement Concrete Pavement for Driveways</p> <p>17" = 680' Cement Concrete Pavement for Driveways</p> <p>18" = 720' Cement Concrete Pavement for Driveways</p> <p>19" = 760' Cement Concrete Pavement for Driveways</p> <p>20" = 800' Cement Concrete Pavement for Driveways</p>	<p>1" = 40' Cement Concrete Pavement for Driveways</p> <p>2" = 80' Cement Concrete Pavement for Driveways</p> <p>3" = 120' Cement Concrete Pavement for Driveways</p> <p>4" = 160' Cement Concrete Pavement for Driveways</p> <p>5" = 200' Cement Concrete Pavement for Driveways</p> <p>6" = 240' Cement Concrete Pavement for Driveways</p> <p>7" = 280' Cement Concrete Pavement for Driveways</p> <p>8" = 320' Cement Concrete Pavement for Driveways</p> <p>9" = 360' Cement Concrete Pavement for Driveways</p> <p>10" = 400' Cement Concrete Pavement for Driveways</p> <p>11" = 440' Cement Concrete Pavement for Driveways</p> <p>12" = 480' Cement Concrete Pavement for Driveways</p> <p>13" = 520' Cement Concrete Pavement for Driveways</p> <p>14" = 560' Cement Concrete Pavement for Driveways</p> <p>15" = 600' Cement Concrete Pavement for Driveways</p> <p>16" = 640' Cement Concrete Pavement for Driveways</p> <p>17" = 680' Cement Concrete Pavement for Driveways</p> <p>18" = 720' Cement Concrete Pavement for Driveways</p> <p>19" = 760' Cement Concrete Pavement for Driveways</p> <p>20" = 800' Cement Concrete Pavement for Driveways</p>
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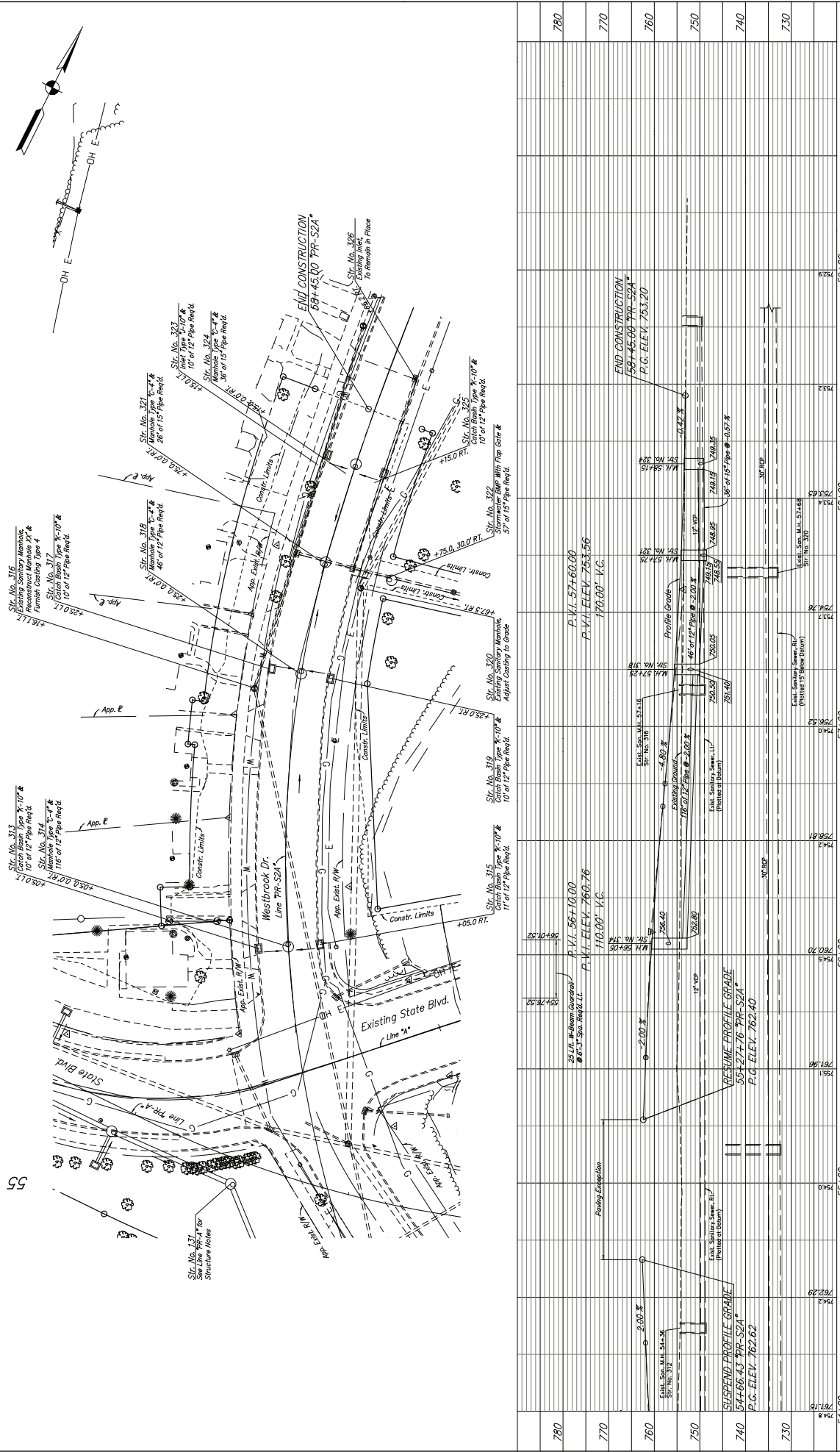
105+00	106+00	107+00	108+00
		RECOMMENDED FOR APPROVAL _____	
		DESIGN ENGINEER _____	DATE _____
		DESIGNED: _____ ASU _____	
		DRAWN: _____ CAK _____	
		CHECKED: _____ SMC _____	
		CHECKED: _____ SMC _____	
INDIANA DEPARTMENT OF TRANSPORTATION			
HORIZONTAL SCALE 1" = 20'			
VERTICAL SCALE 1" = 10'			
DESIGNATION NO. 105154			
SURVEY BOOK _____			
SHEETS 76 of 77			
CONTRACT _____			
PROJECT NO. 00554			
LINE "PR-A"			
BRIDGE FILE _____			







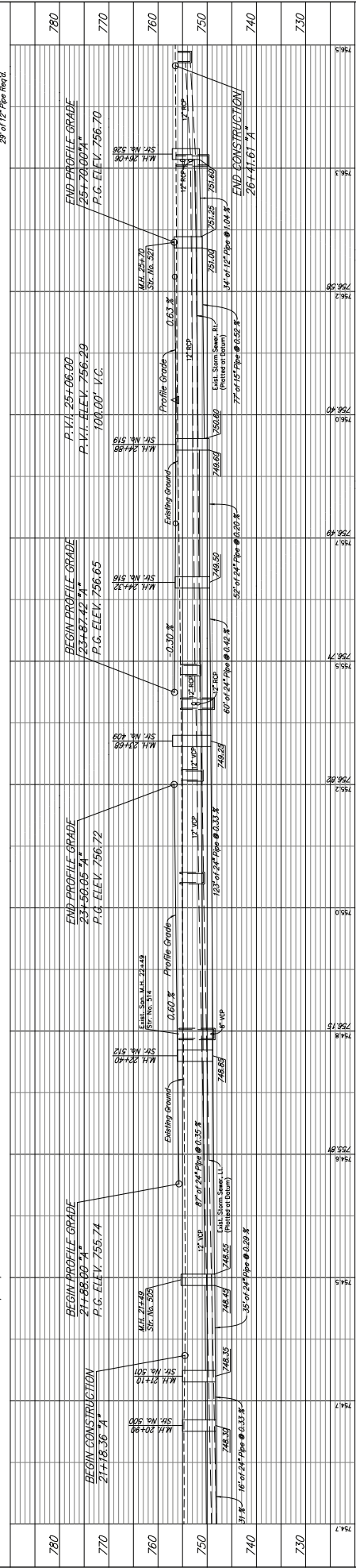
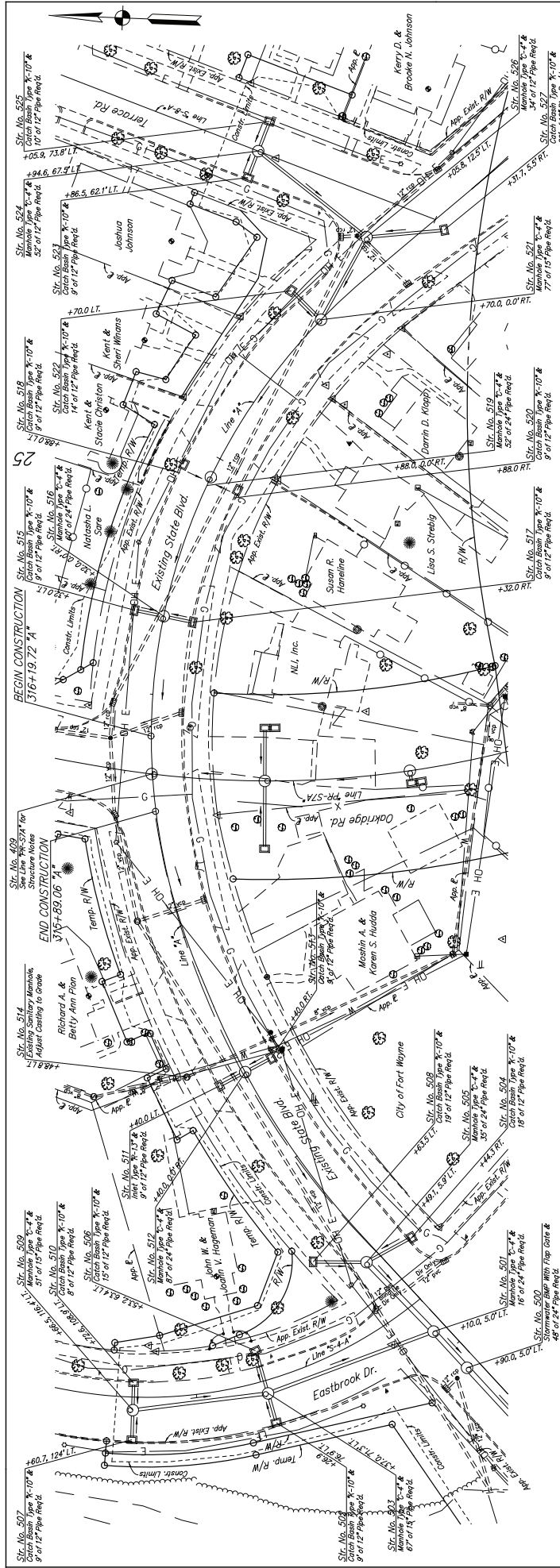




55+00	56+00	57+00	58+00	59+00
<div style="display: flex; justify-content: space-between;"> <div style="width: 20%;"> <p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGN ENGINEER _____</p> <p>DATE _____</p> </div> <div style="width: 60%; text-align: center;"> <p><b>INDIANA</b></p> <p><b>DEPARTMENT OF TRANSPORTATION</b></p> </div> <div style="width: 20%;"> <p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE 1" = 10'</p> <p>DESIGNATION NO. 1005154</p> </div> </div>				
<div style="display: flex; justify-content: space-between;"> <div style="width: 20%;"> <p>DESIGNED: _____</p> <p>CHECKED: _____</p> </div> <div style="width: 60%; text-align: center;"> <p><b>DRAINAGE DETAILS</b></p> <p><b>LINE "PR-S2A"</b></p> </div> <div style="width: 20%;"> <p>SURVEY BOOK</p> <p>CONTRACT</p> </div> </div>				
<div style="display: flex; justify-content: space-between;"> <div style="width: 20%;"> <p>ASU _____</p> <p>SMC _____</p> </div> <div style="width: 60%;"> <p>DRAWN: CAK</p> <p>CHECKED: SMC</p> </div> <div style="width: 20%;"> <p>SHEETS 80 of 100</p> <p>PROJECT NO. 100554</p> </div> </div>				
BRIDGE FILE				

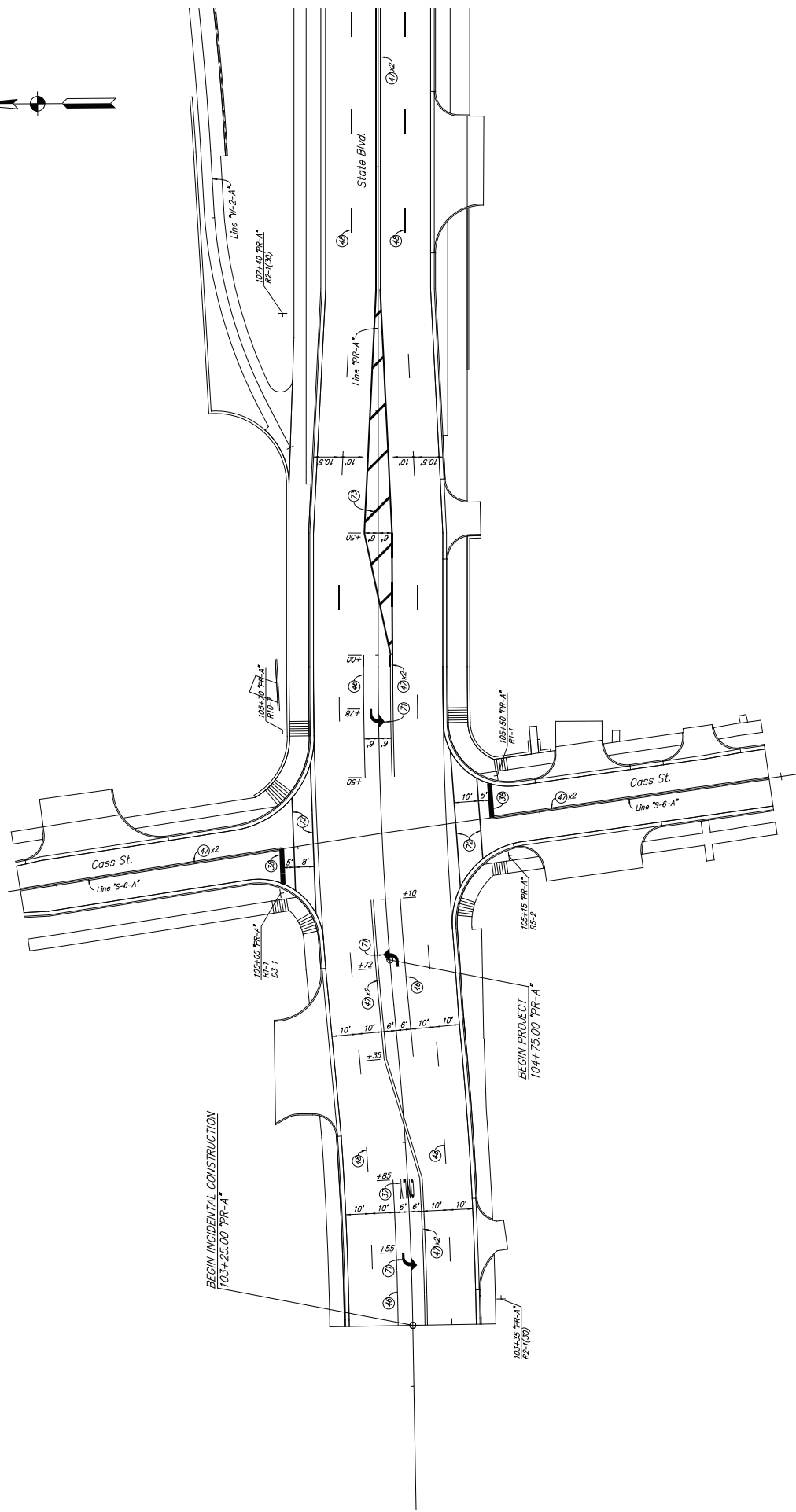






INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
RECOMMENDED FOR APPROVAL				DESIGN ENGINEER				SURVEY BOOK			
DATE				DRAWN				PROJECT NO.			
DESIGNED				CHECKED				CONTRACT			
ASU				SMC				SHEETS			
SMC				SMC				82 of 100			
SMC				SMC				100514			

105

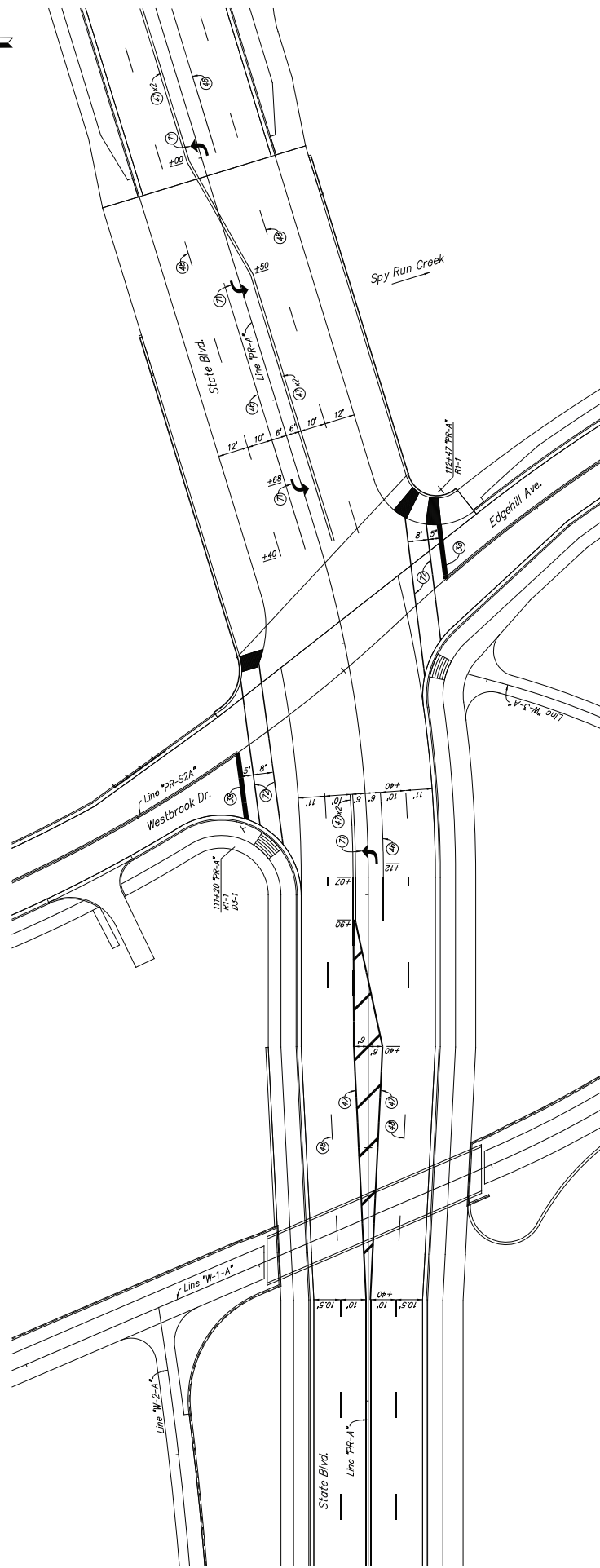


BEGIN INCIDENTAL CONSTRUCTION  
103+25.00 PR-A

BEGIN PROJECT  
104+75.00 PR-A

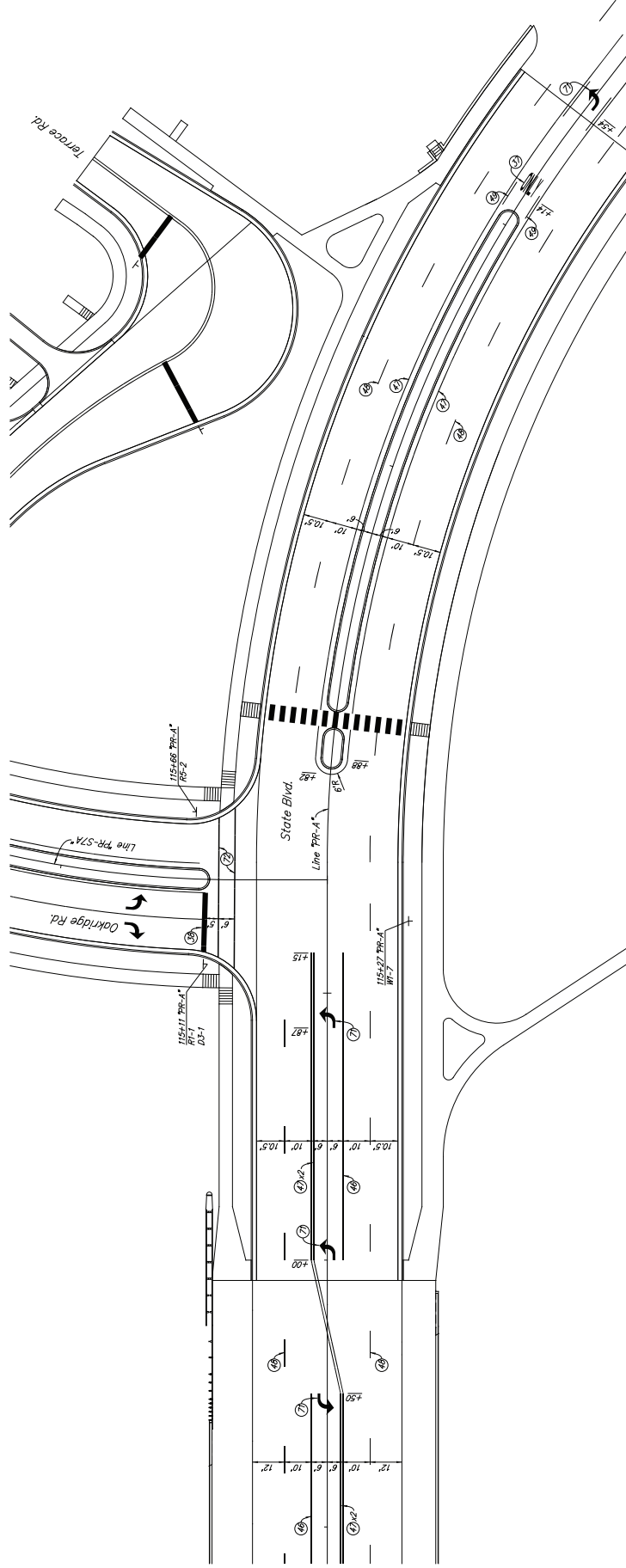
INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		DESIGNATION NO. 100514	
RECOMMENDED FOR APPROVAL		DESIGNER DATE	
DESIGNED: ASU		DRAWN: PJT	
CHECKED: SMC		CHECKED: SMC	
SHEETS 83 of 100		PROJECT NO. 100514	
CONTRACT		SHEETS	
SURVEY BOOK		CONTRACT	
VERTICAL SCALE		HORIZONTAL SCALE	
BRIDGE FILE		DESIGNATION NO.	

- LEGEND**
- ① Pavement Message Markings, Thermoplastic, Word (message), White, 4 in.
  - ② Line, Thermoplastic, Broken, White, 4 in.
  - ③ Line, Thermoplastic, Broken, Yellow, 4 in.
  - ④ Line, Thermoplastic, Solid, White, 4 in.
  - ⑤ Line, Thermoplastic, Solid, Yellow, 4 in.
  - ⑥ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
  - ⑦ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
  - ⑧ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
  - ⑨ Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 6 in.
  - ⑩ Transverse Markings, Thermoplastic, Crosswalk Line, White, 12 in.
  - ⑪ Line, Thermoplastic, Dashed, White, 4 in. (2" Line, 4" Cap)
  - ⑫ Line, Thermoplastic, Dashed, Yellow, 4 in. (2" Line, 4" Cap)



INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
		DESIGNATION NO.	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		SURVEY BOOK	
		CONTRACT	
RECOMMENDED FOR APPROVAL		DESIGNED BY	
		CHECKED BY	
DATE		DRAWN BY	
		CHECKED BY	
DESIGNED BY		PROJECT NO.	
		SHEETS	
ASU		100514	
		84	
SMC		100514	
		84	

- LEGEND**
- (1) Pavement Markings, Thermoplastic, Word (message), White, 4 in.
  - (2) Pavement Markings, Thermoplastic, Stop Line, White, 24 in.
  - (3) Pavement Markings, Thermoplastic, Solid, White, 4 in.
  - (4) Line, Thermoplastic, Solid, Yellow, 4 in.
  - (5) Line, Thermoplastic, Solid, Yellow, 4 in.
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  - (99) Line, Thermoplastic, Solid, Yellow, 4 in.
  - (100) Line, Thermoplastic, Solid, Yellow, 4 in.



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	100514
		CONTRACT	SHEETS
			85 of 100
			PROJECT NO.
			100514

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED:	ASU	DRAWN:	PJT
CHECKED:	SMC	CHECKED:	SMC

**LEGEND**

(1) Pavement Markings, Thermoplastic, Word (message), White, 4 in.

(2) Transverse Markings, Thermoplastic, Broken, White, 4 in.

(3) Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

(4) Line, Thermoplastic, Solid, White, 4 in.

(5) Line, Thermoplastic, Solid, Yellow, 4 in.

(6) Line, Thermoplastic, Dotted, Yellow, 4 in.

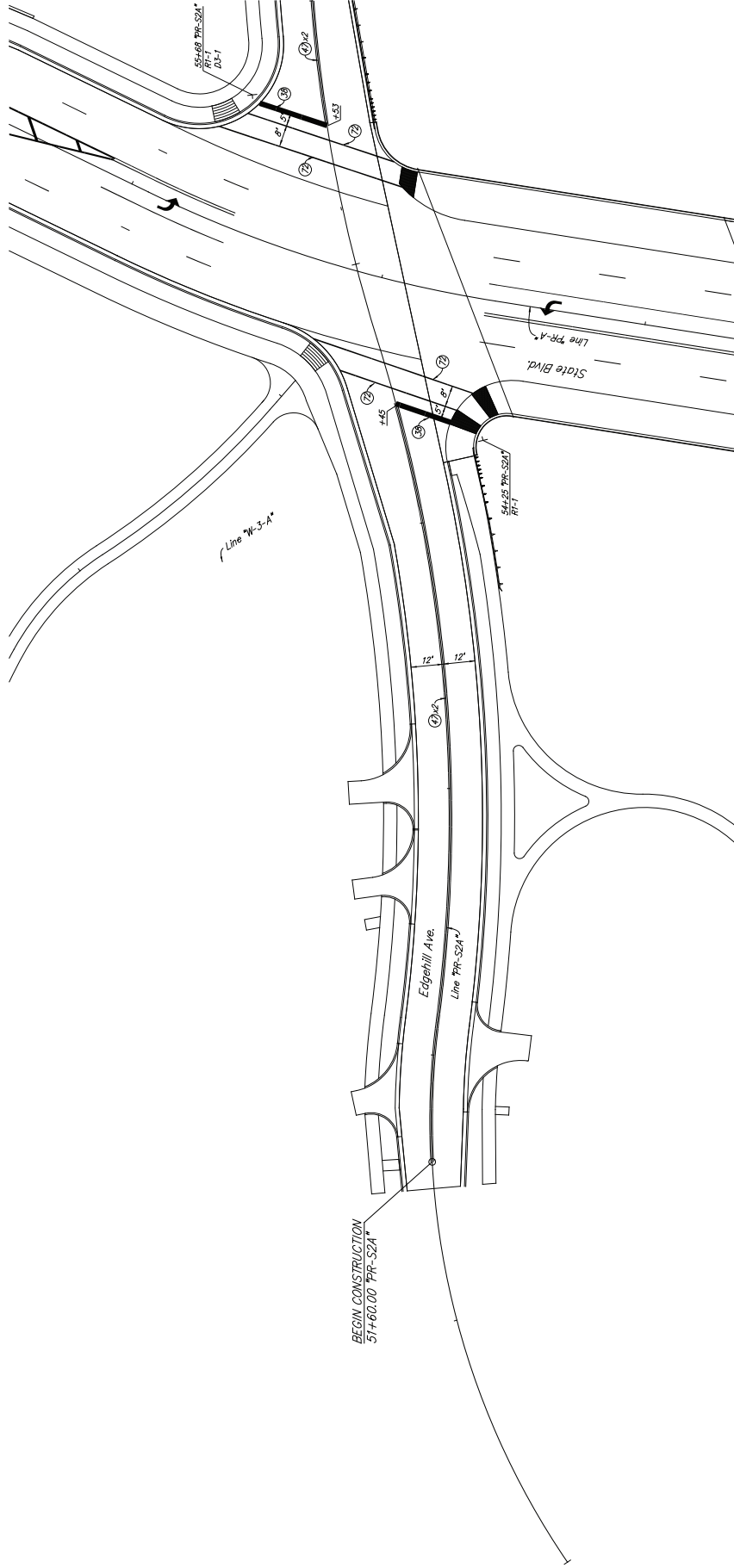
(7) Pavement Markings, Thermoplastic, Lane Indication Arrow

(8) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

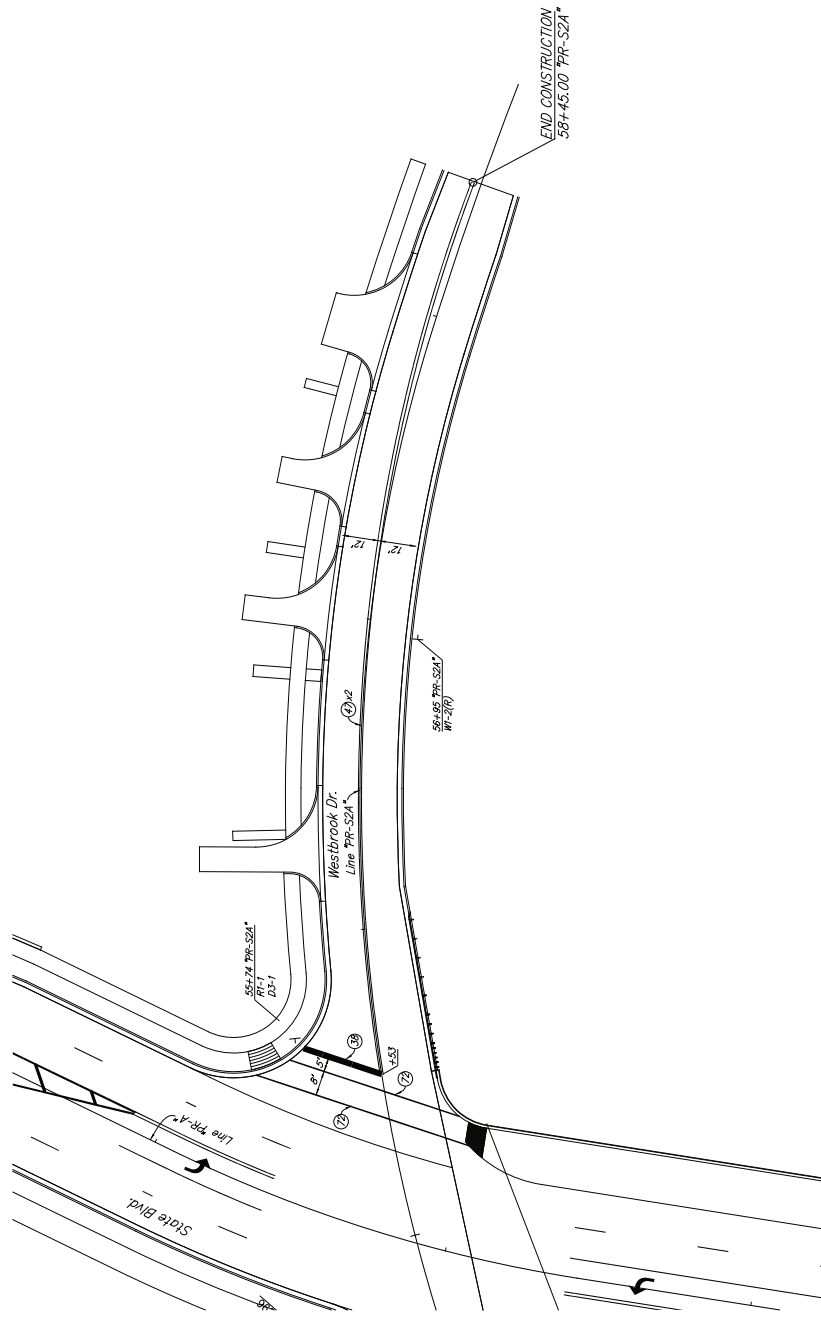
(9) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.

(10) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)

(11) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)

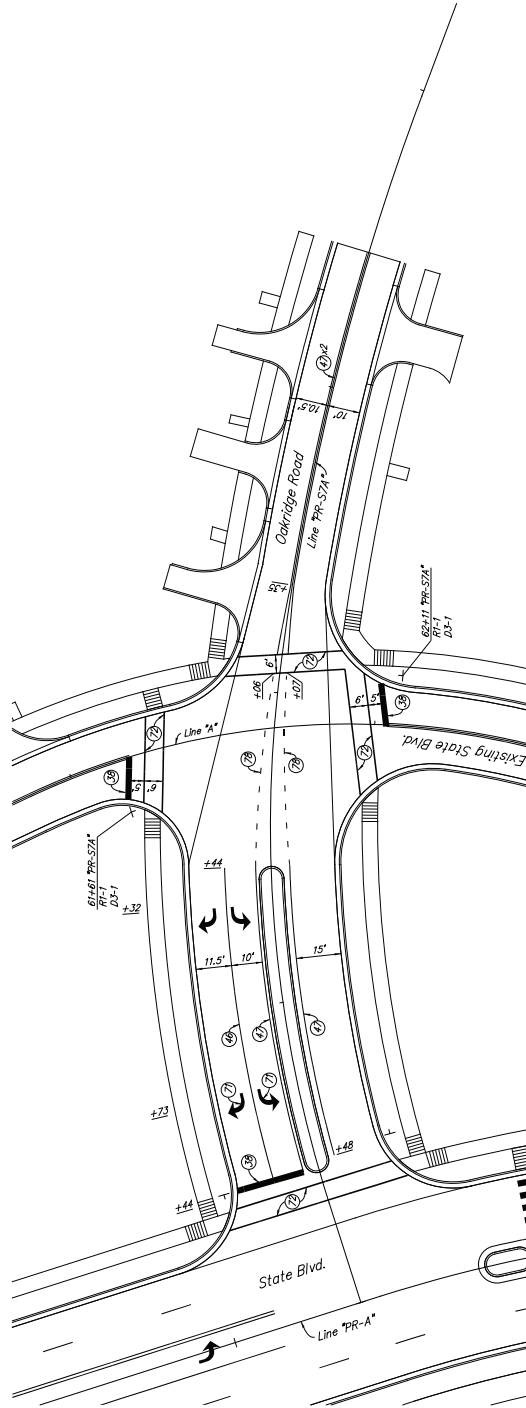


INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
SIGNING DETAILS - LINE "PR-S2A"		DESIGNATION NO.	
PAVEMENT MARKING &		100514	
SIGNING DETAILS - LINE "PR-S2A"		SHEETS	
		86 of 100	
		PROJECT NO.	
		100514	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		CHECKED: SMC	
LEGEND		② Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.	
③ Pavement Message Markings, Thermoplastic, Word (message)		④ Line, Thermoplastic, Broken, White, 4 in.	
⑤ Transverse Markings, Thermoplastic, Stop Line, White, 24 in.		⑥ Line, Thermoplastic, Broken, Yellow, 4 in.	
⑦ Line, Thermoplastic, Solid, White, 4 in.		⑧ Pavement Message Markings, Thermoplastic, Lane Indication Arrow	
⑨ Line, Thermoplastic, Solid, Yellow, 4 in.		⑩ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	

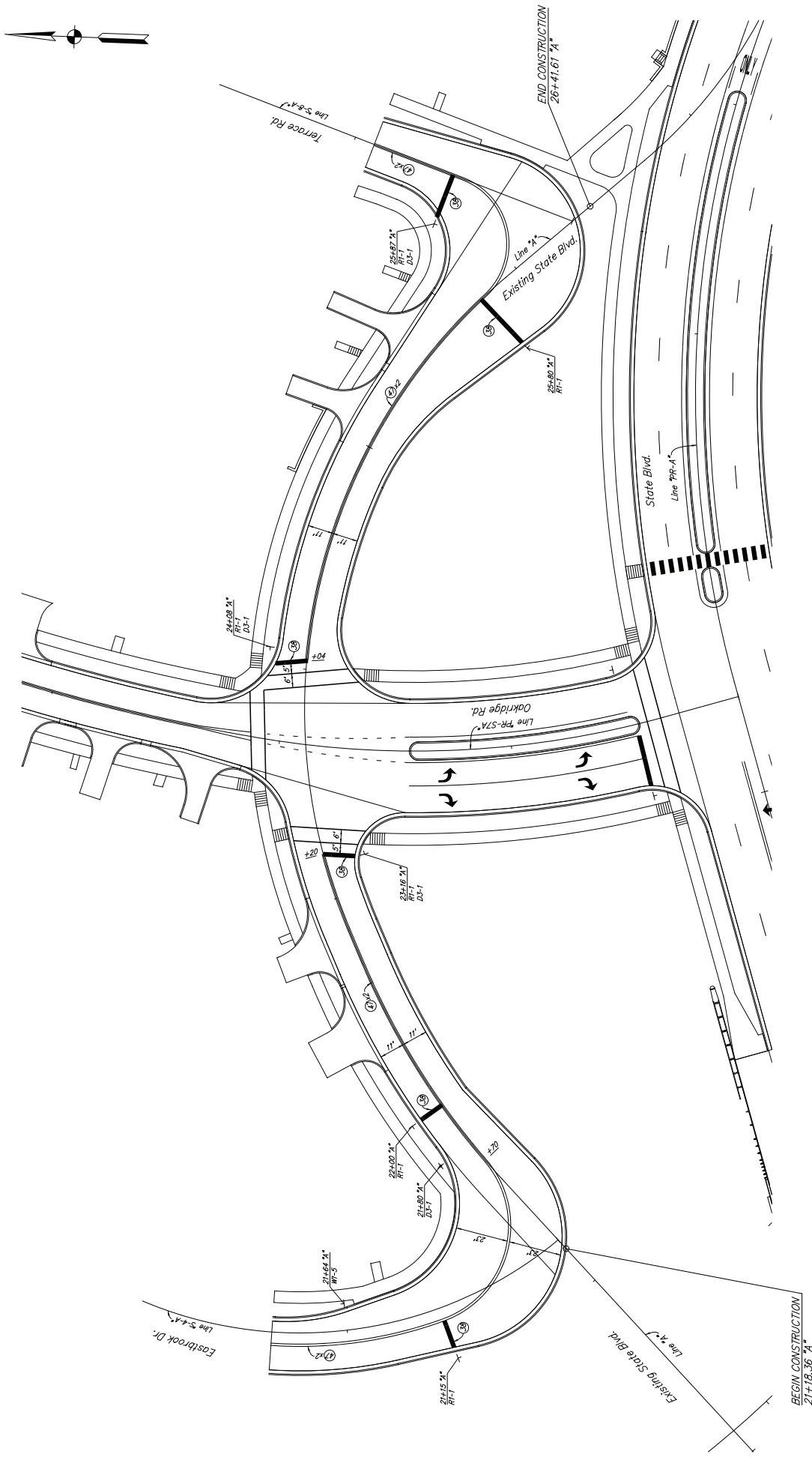


<p>③ Pavement Message Markings, Thermoplastic, Word (message)</p> <p>④ Line, Thermoplastic, Broken, White, 4 in.</p> <p>⑤ Line, Thermoplastic, Broken, White, 4 in.</p> <p>⑥ Line, Thermoplastic, Broken, Yellow, 4 in.</p> <p>⑦ Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.</p> <p>⑧ Line, Thermoplastic, Solid, White, 4 in.</p> <p>⑨ Line, Thermoplastic, Solid, White, 4 in.</p> <p>⑩ Line, Thermoplastic, Solid, Yellow, 4 in.</p> <p>⑪ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p> <p>⑫ Transverse Markings, Thermoplastic, Crosswalk Line, White, 12 in.</p> <p>⑬ Line, Thermoplastic, Dashed, White, 4 in. (2" Line, 4" Gap)</p> <p>⑭ Line, Thermoplastic, Dashed, Yellow, 4 in. (2" Line, 4" Gap)</p> <p>⑮ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p> <p>⑯ Pavement Message Markings, Thermoplastic, Lane Indication Arrow</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>PAVEMENT MARKING &amp;</p> <p>SIGNING DETAILS - LINE "PR-S2A"</p>	<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p>	<p>HORIZONTAL SCALE</p> <p>1" = 20'</p> <p>VERTICAL SCALE</p> <p>1:100</p> <p>SURVEY BOOK</p> <p>87</p> <p>CONTRACT</p> <p>PROJECT NO.</p> <p>100514</p> <p>SHEETS</p> <p>87 of 100</p> <p>BRIDGE FILE</p>
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<p><b>LEGEND</b></p> <p>(17) Pavement Message Markings, Thermoplastic, Word (Message)</p> <p>(18) Transverse Markings, Thermoplastic, Broken, White, 4 in.</p> <p>(19) Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.</p> <p>(20) Line, Thermoplastic, Solid, White, 24 in.</p> <p>(21) Line, Thermoplastic, Solid, White, 4 in.</p> <p>(22) Line, Thermoplastic, Solid, Yellow, 4 in.</p> <p>(23) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.</p> <p>(24) Line, Thermoplastic, Broken, White, 4 in.</p> <p>(25) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>(26) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p> <p>(27) Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.</p>		<p>RECOMMENDED FOR APPROVAL _____</p> <p>DESIGN ENGINEER _____ DATE _____</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p>		<p>PAVEMENT MARKING &amp; SIGNING DETAILS - LINE "PR-S7A"</p>		<p>HORIZONTAL SCALE _____</p> <p>VERTICAL SCALE _____</p> <p>1005154</p>		<p>BRIDGE FILE _____</p>	
<p>DESIGNED: _____</p> <p>CHECKED: _____</p>		<p>ASU _____</p> <p>SNC _____</p>		<p>DRAWN: _____</p> <p>SNC _____</p>		<p>PJT _____</p> <p>SNC _____</p>		<p>SURVEY BOOK _____</p> <p>88 of 88</p>		<p>SHEETS _____</p> <p>PROJECT NO. _____</p> <p>1005154</p>	



INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
		DESIGNATION NO.	
PAVEMENT MARKING & SIGNING DETAILS - LINE "A"		SURVEY BOOK	
		CONTRACT	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED:		DRAWN:	
		PJT	
CHECKED:		CHECKED:	
		SMC	
LEGEND			
(3) Pavement Markings, Thermoplastic, Word (message), White, 4 in.			
(4) Line, Thermoplastic, Broken, White, 4 in.			
(5) Line, Thermoplastic, Broken, Yellow, 4 in.			
(6) Line, Thermoplastic, Solid, White, 24 in.			
(7) Line, Thermoplastic, Solid, White, 4 in.			
(8) Line, Thermoplastic, Solid, Yellow, 4 in.			
(9) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)			
(10) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)			
(11) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.			
(12) Transverse Markings, Thermoplastic, Crosswalk Line, Yellow, 6 in.			
(13) Transverse Markings, Thermoplastic, Crosswalk Line, White, 12 in.			
(14) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)			
(15) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)			





STATE BLVD., EAST VIEW TOWARD SPY RUN AVE.



AMERICAN  
STRUCTUREPOINT  
INC.





STATE BLVD., WEST VIEW TOWARD PROPOSED STATE AND HISTORIC STATE BLVD.



AMERICAN  
STRUCTUREPOINT  
INC.





OAKRIDGE RD., SOUTHWEST VIEW OF THE NEW EXTENSION AND HISTORIC STATE BLVD.



AMERICAN  
STRUCTUREPOINT  
INC.





OAKRIDGE RD., NORTH VIEW TOWARD HISTORIC STATE BLVD.



AMERICAN  
STRUCTUREPOINT  
INC.





STATE BLVD., EAST VIEW TOWARD NEW BRIDGE OVER SPY RUN CREEK



AMERICAN  
STRUCTUREPOINT  
INC.





STATE BLVD., WEST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE

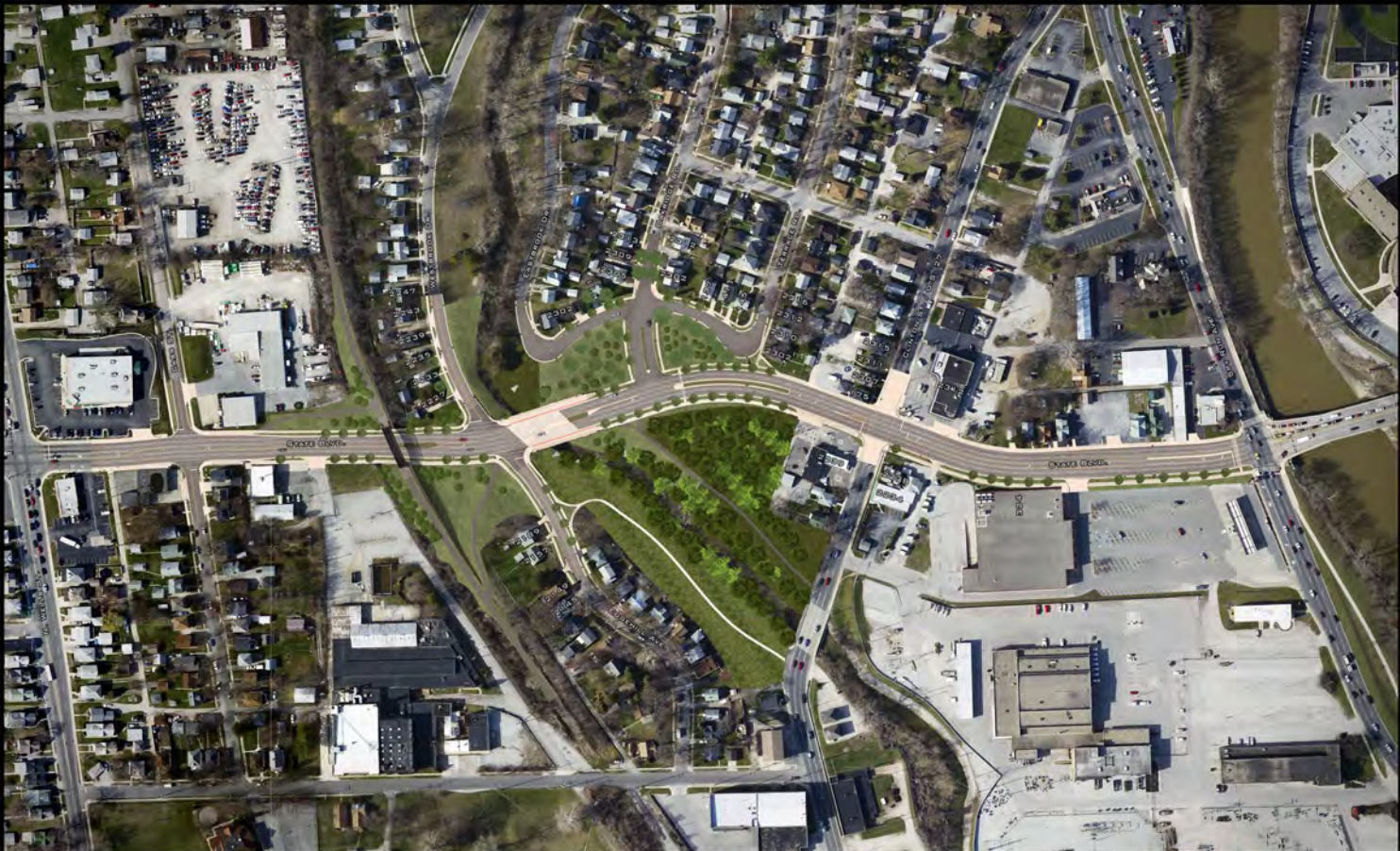




STATE BLVD., EAST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE







AMERICAN  
STRUCTUREPOINT  
INC

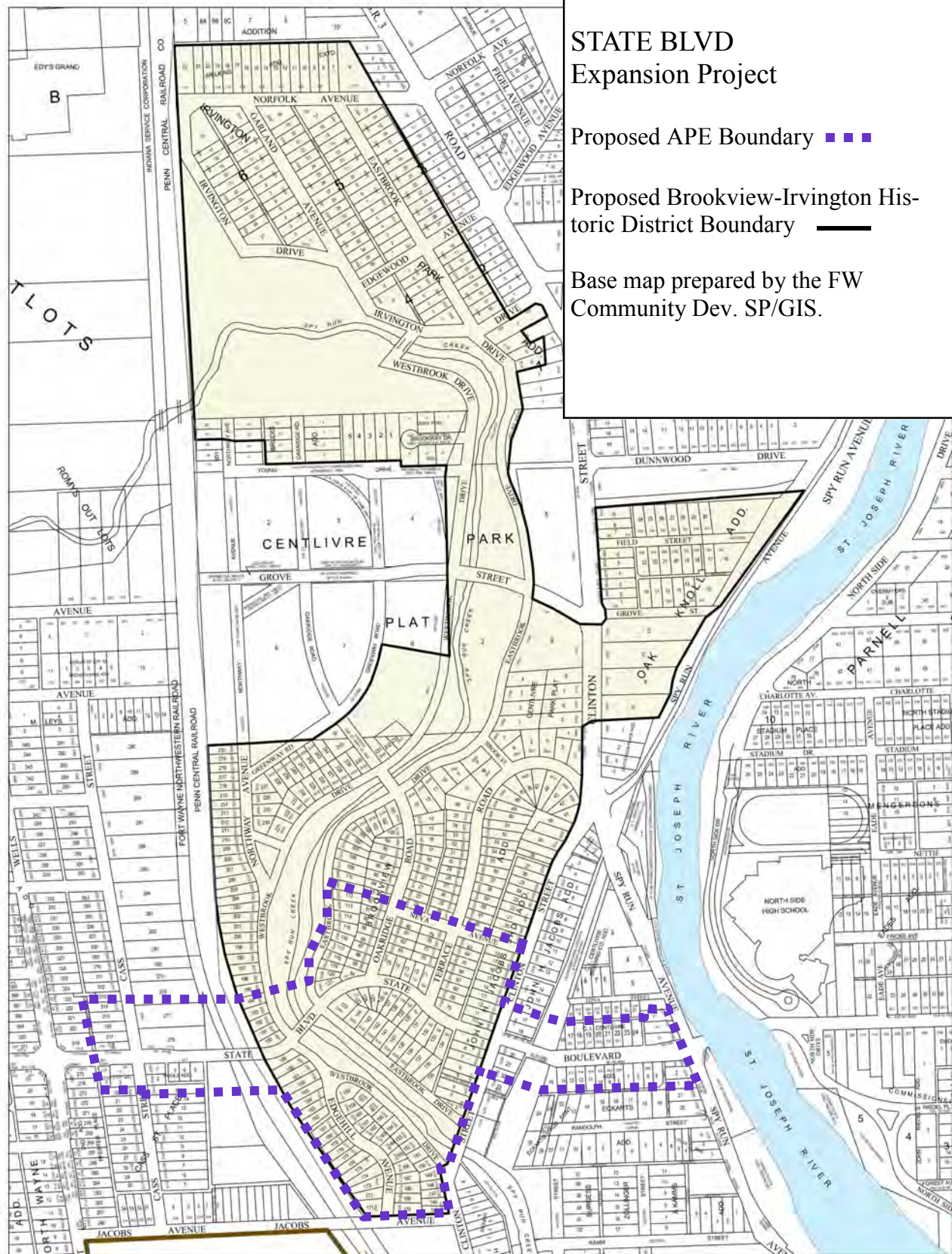
STATE BLVD. OVER SPY RUN CREEK  
PLAN VIEW

STRUCTUREPOINT  
INC



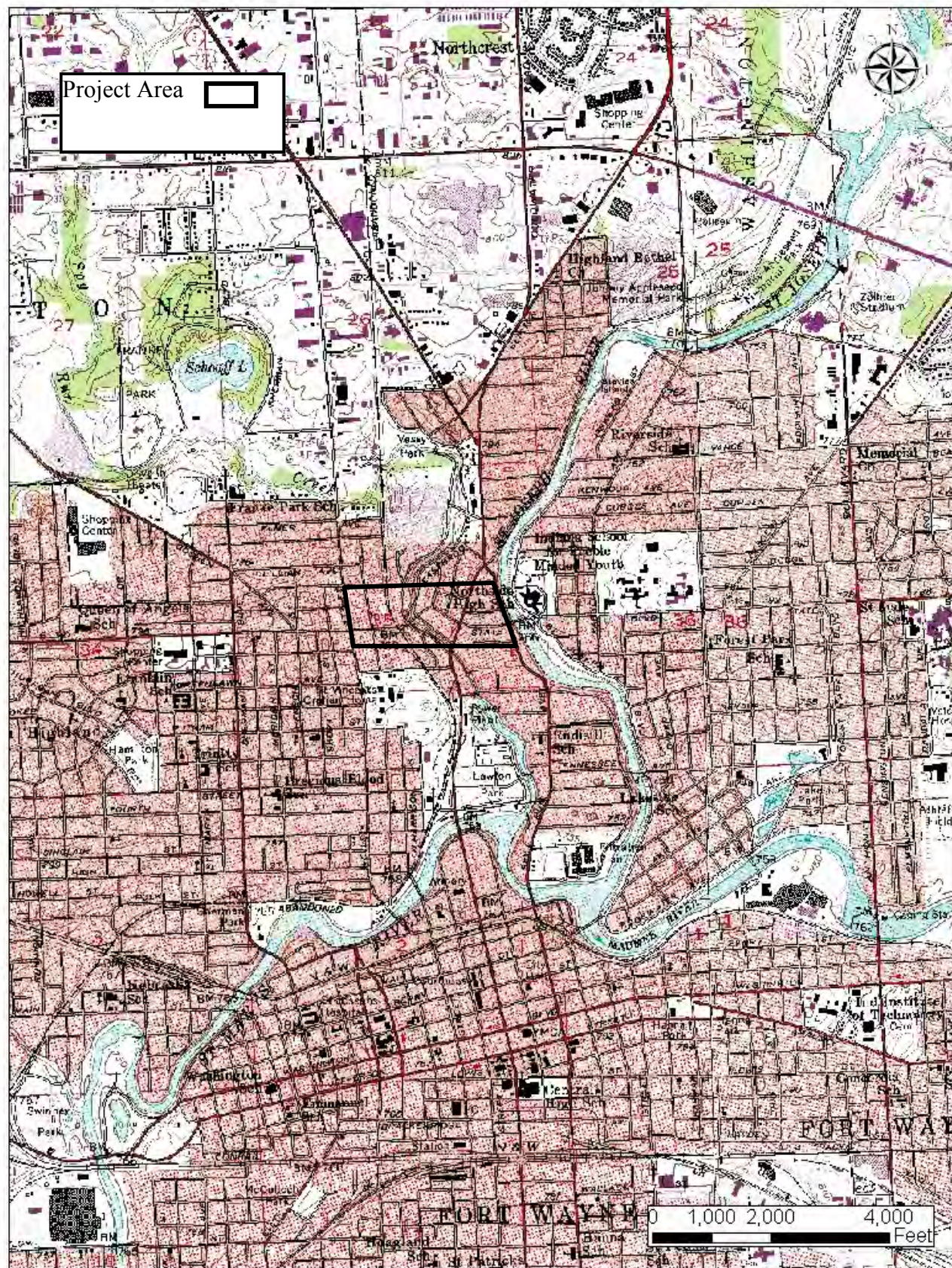


## **APPENDIX B. Maps**

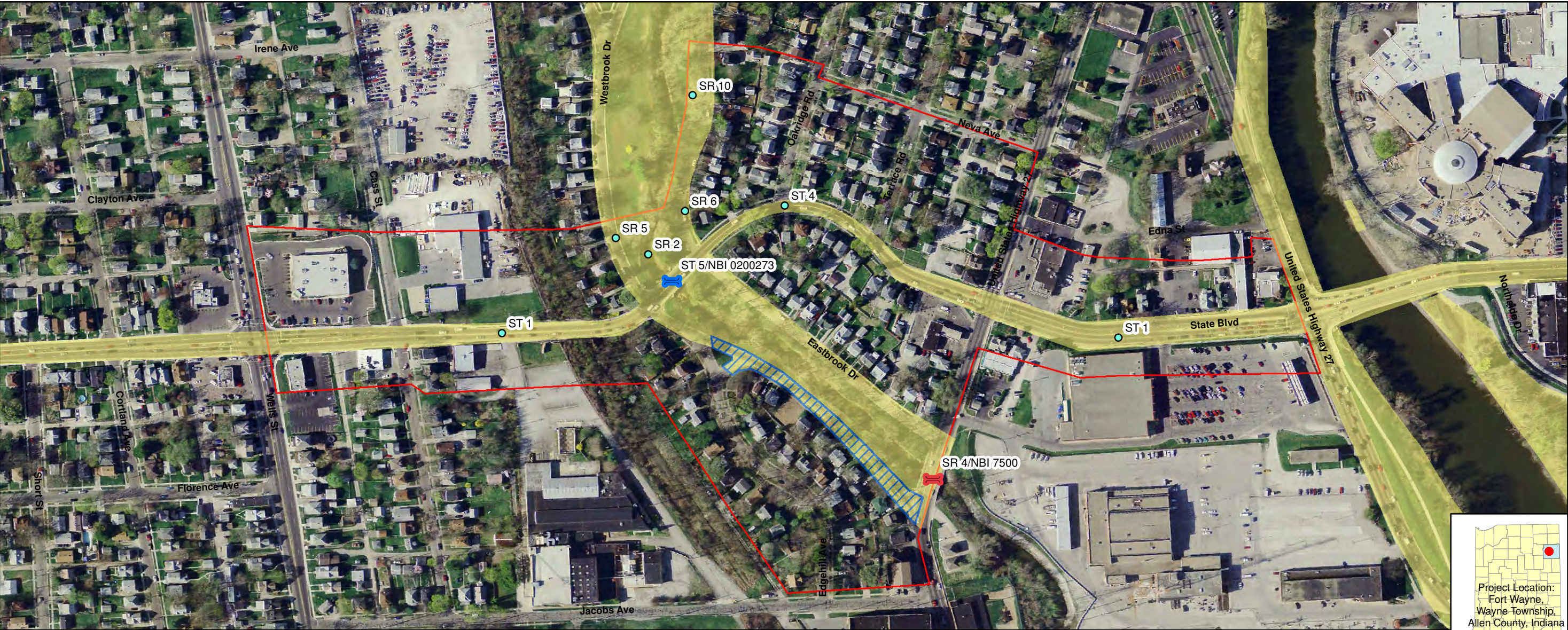




## USGS Map







02505001,000 Feet

Legend

APE

Contributing to FWPB

Non-Contributing Bridge (FWPB)

Determined NR-eligible Bridge

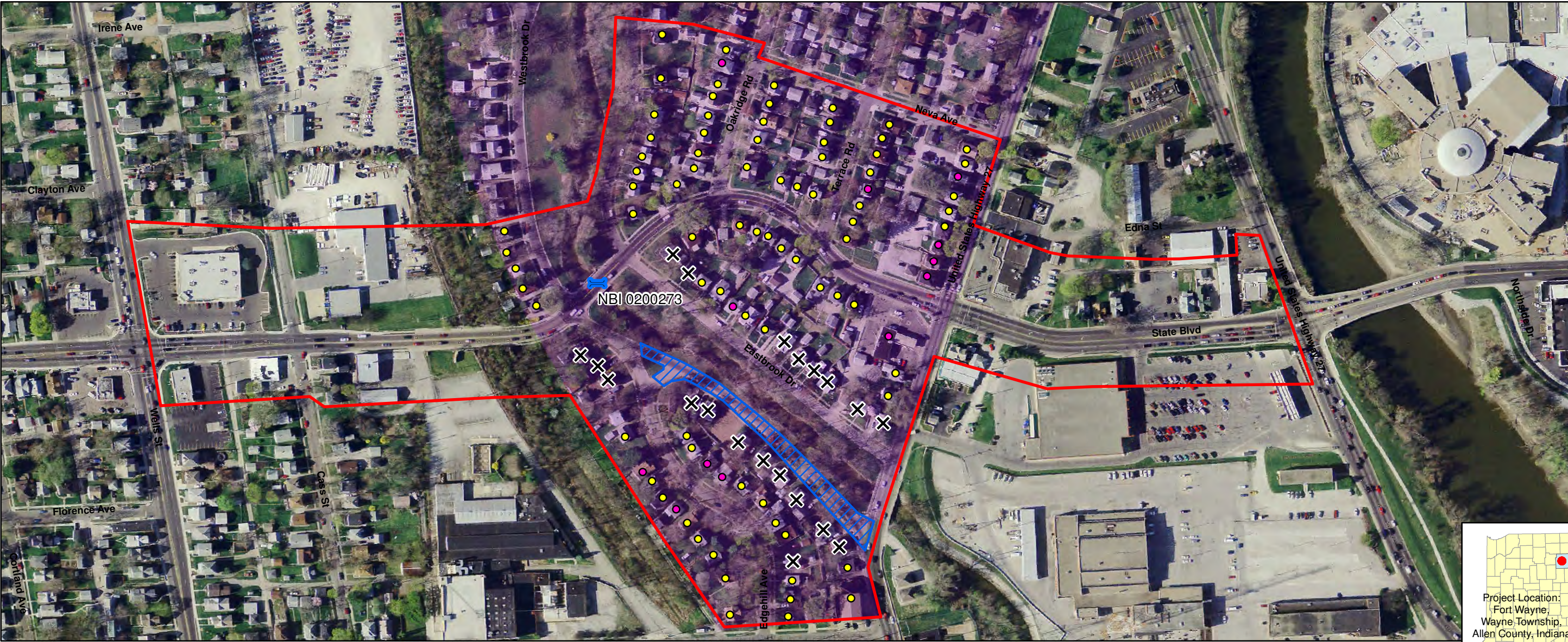
Road Pavement Removed

FWPB Boundary








Project Location:  
Fort Wayne,  
Wayne Township,  
Allen County, Indiana

Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.





**Legend**

 APE	 Road Pavement Removed	 Contributing to BIHD
 BIHD	 Determined NR-eligible Bridge	 Non-Contributing to BIHD
		 No Longer Extant

Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.



## **APPENDIX C. Consulting Parties**



### **Individuals or Groups Invited to Join Section 106 Consultation**

Indiana State Historic Preservation Officer (SHPO)	ARCH, Inc.
Indiana Department of Transportation (INDOT)	Fort Wayne Historic Preservation Review Board
INDOT—Fort Wayne District,	John Shoaff, Fort Wayne city council member
City of Fort Wayne Engineer	Friends of the Parks of Allen County
Historic Landmarks Foundation of Indiana (now Indiana Landmarks)	Brookview Neighborhood Association
Allen County Historian	Dr. James L. Cooper
Allen County—Fort Wayne Historical Society	Paul Brandenburg, Historic Spans Taskforce

### **Individuals or Groups Accepting the Invitation to Join Section 106 Consultation, Requesting Consulting Party Status, or Commenting on Project**

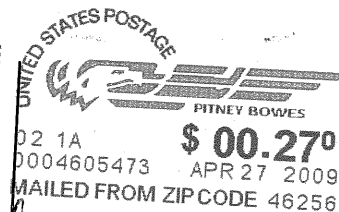
Indiana SHPO	
City of Fort Wayne	Indiana Historic Spans Taskforce
Allen County Historian	Irvington Park Neighborhood Association
Friends of the Parks of Allen County	Charley Shirmeyer, Northside Galleries
Indiana Landmarks—Northern Regional Office	Albert Cohan, Westbrook 5, LLC
Fort Wayne Historic Preservation Commission	Thomas Niezer, Barret & McNagny, LLP
ARCH, Inc.	Ronald Ross, Martin Riley Architects and Engineers
Brookview Neighborhood Association	Dan Ernst, Earth Source, Inc.
	Jan Dailey, State Boulevard Resident

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction  
AGENCY CITY OF FORT WAYNE  
WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐  
CONTACT PERSON SHAN GUNAWARDENA  
ADDRESS ONE EAST MAIN ST Rm. 750  
FORT WAYNE IN 46802  
PHONE (260) 427-6169 FAX (260) 427-1269  
E-MAIL shan.gunawardena@ci.ftwayne.in.us DATE 06/01/09

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction  
AGENCY Friends of the Parks of Allen Co  
WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐  
CONTACT PERSON Julie Donnell  
ADDRESS 3604 South Washington  
Fort Wayne IN  
46802  
PHONE 260-932-7178 FAX \_\_\_\_\_  
E-MAIL juliemarie57e DATE 5/22/09  
earthlink.net



Hayley Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

REC'D MAY 28 2009



### Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY \_\_\_\_\_

WILL PARTICIPATE ☐

WILL NOT PARTICIPATE ☒

CONTACT PERSON Tom Castaldi

ADDRESS 13707 Black Hollow Ct  
FT Wayne  
IND 46814

PHONE \_\_\_\_\_

FAX \_\_\_\_\_

E-MAIL \_\_\_\_\_

DATE 3-27-9

### Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Historic Landmarks Foundation of Indiana

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON Tom Zeiger

ADDRESS 402 W. Washington  
South Bend, IN  
46601

PHONE 574-232-4534

FAX \_\_\_\_\_

574-232-5549

E-MAIL tzeiger@

DATE 4-13-09

historiclandmarks.org

Northern  
office

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Ft. Wayne Historic Preservation Commission

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON DON ORBAN

ADDRESS One E. Main St. Rm 800  
FORT WAYNE, IN 46802

PHONE 260-427-2160 FAX 260-427-1132

E-MAIL don.orban@cityoffortwayne.org DATE 4-2-09

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY ARCH, Inc.

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON Angie Quinn + Michael Galbraith

ADDRESS ARCH, Inc.  
437 E. Berry St. Suite 204  
Fort Wayne, IN 46802

PHONE 260-426-5117 FAX Same

E-MAIL Aquinn@archfw.org DATE 3/26/09

mgalbraith@archfw.org

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Brookview Neighborhood Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Michelle Briggs Wedaman

ADDRESS 2326 Eastbrook Drive  
FT. WAYNE IN

260-480-0120  
PHONE 260-710-4413 FAX 46805

E-MAIL

DATE 5/1/09

mbwedaman@verizon.net

UNITED STATES POSTAGE  
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02 1A \$ 00.27<sup>0</sup>  
0004605473 APR 27 2009  
MAILED FROM ZIP CODE 46256

Hayley Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 462

REC'D MAY 4 2009

## Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY INDIANA HISTORIC SPAS TASCORP

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON PAUL BRANDENBURG

ADDRESS 5868 CROWN CIRCLE  
INDIANAPOLIS IN 46254

PHONE 317-347-1204 FAX 317-347-1206

E-MAIL

DATE 1-11-2009

260-480-0120  
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Hayley Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

REC'D MAY 4 2009



### Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Irvington Park Nbrhd Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Suzanne Slick

ADDRESS 3318 Garland Ave

Fr. Wayne

IN

46805

PHONE 260-482-3991 FAX           

E-MAIL sjslick@mac.com

DATE 7/9/09

Hayley Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

## **APPENDIX D. Photographs**

**STATE BOULEVARD RECONSTRUCTION**  
**PHOTOGRAPHS - Page One    Map A**  
**DHPA - INDOT Des. No. 0400587**



Photo 001



Photo 002



Photo 003



Photo 004

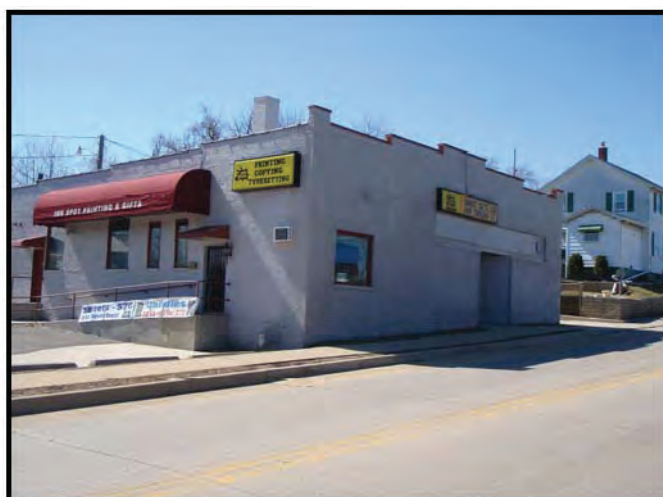


Photo 005

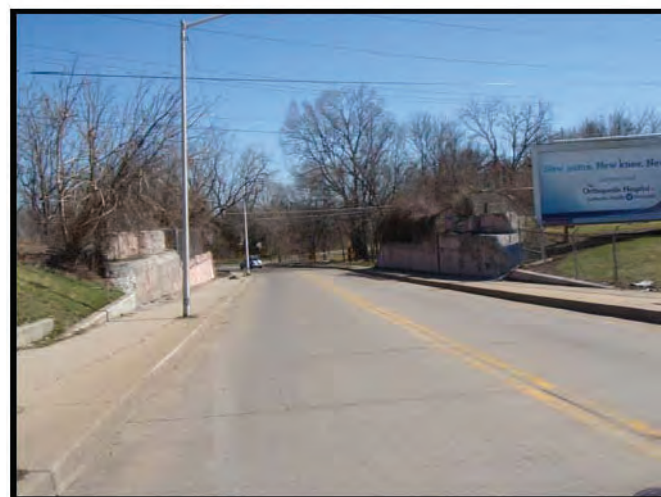


Photo 006





Photo 001A



Photo 001B



Photo 002A



Photo 002B





Photo 007

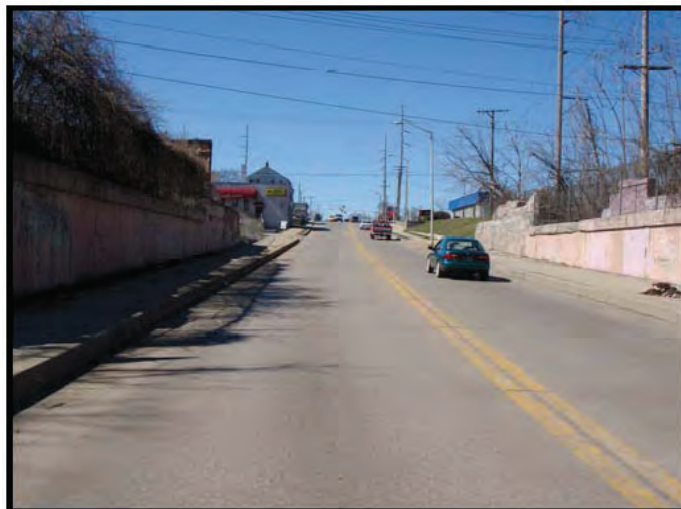


Photo 008



Photo 009



Photo 010



Photo 011



Photo 012





Photo 008A



Photo 008B



Photo 008C



Photo 008D



Photo 008E





Photo 010A



Photo 010B



Photo 010C



Photo 010D



Photo 011A



Photo 011B





Photo 012A



Photo 012B

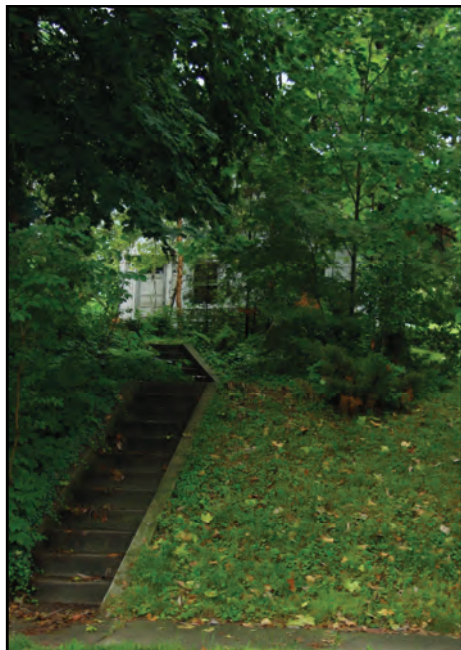


Photo 012C



Photo 012D



Photo 012E





Photo 013



Photo 13A



Photo 13B



Photo 014



Photo 015





Photo 016



Photo 017



Photo 018



Photo 019



Photo 020



Photo 021



Photo 021A

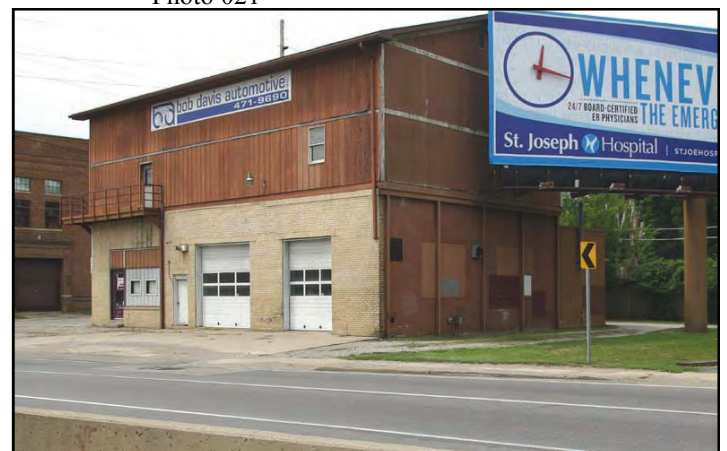


Photo 021B





Photo 022



Photo 023



Photo 024



Photo 025



Photo 026



Photo 027





Photo 025A



Photo 025B



Photo 027A



Photo 027B



Photo 027C



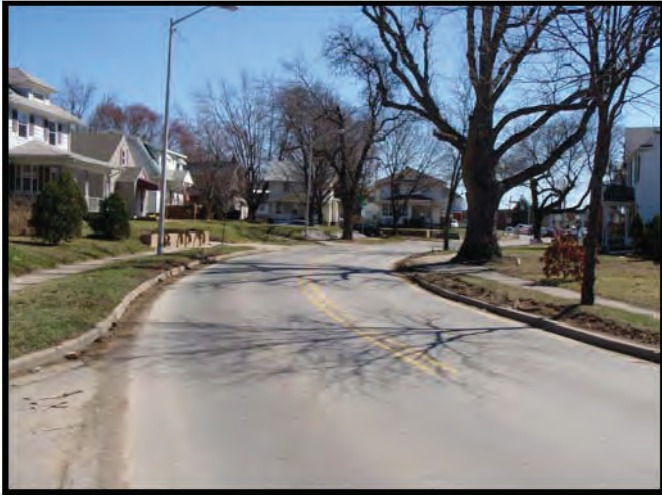


Photo 028



Photo 029



Photo 030



Photo 031



Photo 032



Photo 033





Photo 034



Photo 035



Photo 036



Photo 037



Photo 038



Photo 039





Photo 040



Photo 041



Photo 042



Photo 043



Photo 044



Photo 045





Photo 040A



Photo 040B



Photo 040C



Photo 040D



Photo 040E



Photo041A





Photo 042A



Photo 042B



Photo 042C



Photo 042D



Photo 042E





Photo 044A



Photo 044B



Photo 044C



Photo 044D



Photo 044E



Photo 045A





Photo 046



Photo 047



Photo 048



Photo 049



Photo 050



Photo 051





Photo 052



Photo 053



Photo 054



Photo 055



Photo 056





Photo 057



Photo 058



Photo 059

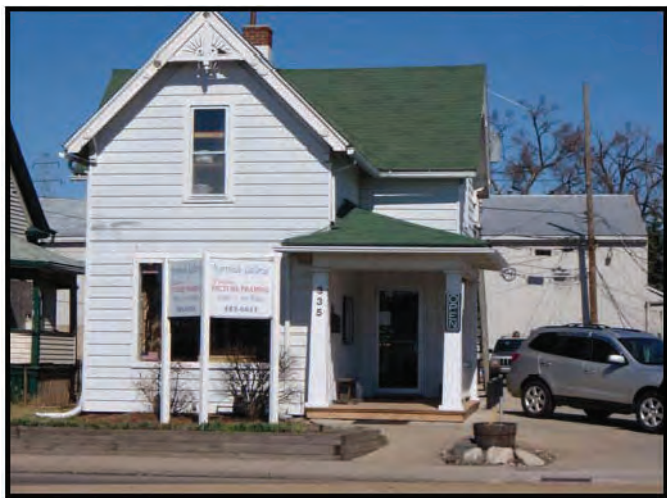


Photo 060



Photo 061



Photo 062





Photo 063



Photo 064



Photo 065



Photo 066



Photo 067



Photo 068





Photo 066A



Photo 067A



Photo 067B



Photo 069



Photo 070



Photo 071



Photo 071A



Photo 071B



Photo 071C





Photo 072



Photo 073



Photo 074



Photo 075



Photo 076



Photo 077





Photo 078



Photo 079



Photo 080



Photo 081



Photo 082



Photo 083





Photo 084



Photo 085



Photo 086



Photo 087



Photo 088





Photo 089



Photo 090



Photo 091



Photo 092





Photo 093



Photo 094



Photo 095



Photo 096





Westbrook looking SE to Edgehill Ave



Westbrook looking NW to State Blvd showing street light



Westbrook looking SE showing filled portion of street and curb



Westbrook looking SE between Edgehill Ave and Spy Run



Looking NE from 2157 Edgehill Ave



Looking SE from 2157 Edgehill Ave





Looking SE from 2232 Eastbrook Dr



SR 4 NC Clinton St Bridge



Looking S to State Blvd from 2320 Oakridge Rd



Looking N to Neva from 2320 Oakridge Rd



Looking SE to State Blvd from Oakridge Rd



Looking SW to State Blvd from Oakridge Rd





Looking SW to Spy Run Bridge from State Blvd



Looking NE to State Blvd from Eastbrook Dr



Looking NE to Eastbrook Dr from State Blvd



Looking SE from 2342 Eastbrook Dr



Terrace Rd looking S to State Blvd



Looking NW from Neva to Oakridge Rd





Looking SW from Neva Ave to Clinton St



Looking W along State Blvd



Looking E across State Blvd



Spy Run Bridge\_ST 5\_N elevation



Spy Run Bridge\_ST 5\_Bridge deck



Looking E to Interurban abutments along State Blvd





Looking W along State Blvd from Cass Street



Looking W along State Blvd from Clinton



Looking NW at State Blvd and Spy Run Avenue



09014\_Looking NW across State Blvd to 315 E State Blvd



Looking E from 315 W State Blv



Looking W along State Blvd from 315 W State State Blvd





State at Wells looking E into the APE



Spy Run Bridge\_ST 5\_rail N elevation



09014\_315 E State Blvd S and E elevation 6

## **APPENDIX E. Report Summaries**

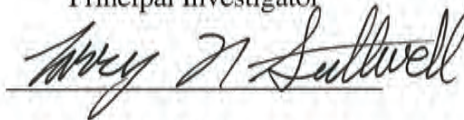


**An Archaeological Field Reconnaissance of the Proposed State Boulevard  
Improvements (Des No. 0400587) in Ft. Wayne, Allen County, Indiana**

By

Larry N. Stillwell

Principal Investigator

A handwritten signature in black ink, reading "Larry N. Stillwell", is written over a horizontal line.

Submitted by:

**Archaeological Consultants of Ossian**

P.O. Box 2374

Muncie, IN 47307

Submitted to:

American Structurepoint, Inc.

Indianapolis, IN

April 2, 2009

### **Abstract**

On March 18, 2009, personnel from Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey of the proposed State Boulevard improvements (Des. No. 0400587) in Ft. Wayne, Allen County, Indiana. The project area totaled approximately 6.43 acres, and was located in an urban setting by the St. Joseph River. Soils within the project area varied from well drained to very poorly drained. Shovel testing and pedestrian walkover survey combined with auger testing was unable to locate any cultural resources within the project limits, and the field reconnaissance determined that the project area was disturbed. It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places. Thus, no further archaeological work is warranted. Project clearance is recommended.



# Historic Property Report State Boulevard Reconstruction From Spy Run to Cass Street Fort Wayne, Allen County



INDOT Des. No. 0400587  
Federal Project No.  
*Prepared for:*  
StructurePoint  
Project No. IN20071404

*Prepared by*  
*The Westerly Group, Inc.*  
*Camille B. Fife, Principal Investigator*  
*225 E. Main St., Madison, IN 47250*  
*(812) 273-8826 (F) (812) 265-9606;*  
*wgimadison@aol.com*  
*September, 2009*

# Executive Summary

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County. Since the project is receiving funding from the Federal Highway Administration (FHWA), it is therefore subject to Section 106 review. More specifically the project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. A new alignment in the section between Westbrook Drive and Terrace Road will provide enhanced roadway safety. The project also includes replacing a bridge over Spy Run Creek and adding a new trail bridge. This report was prepared using information from local and state government repositories, historical societies, local organizations, library, newspapers, and site inspection.

Above-ground resources were identified and evaluated in accordance with Section 106, national Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (Revised January 2001) and Final Rule of Revision of Current Regulations, dated December 12, 2000, and incorporating amendments effective August 4, 2004. As a result of the NHPA, as amended, and CFR Part 800 (Revised January 2001), federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic Properties include buildings, structures, sites, objects, and/or districts. The report follows the Indiana Department of Transportation's (INDOT) Cultural Resource Manual (CRM) guidelines for preparing Historic Property Reports (HPR.)

The project passes through the proposed Brookview-Irvington Park National Register (NR) Historic District. Thirty-eight properties within the proposed historic district were previously surveyed in the Fort Wayne Interim Report( See Table I.) The district boundary encompasses neighborhoods which are significant examples of community planning. The district is eligible for the National Register of Historic Places (NRHP) under criterion C, and criterion A.<sup>1</sup> Taking into account that any effects within part of the district can be considered to effect the entire district, the proposed Area of Potential Effect (APE) for the project has been defined as an area of concentration in the southern portion of the district. This concentrated APE represents the part of the district within which potential alternative alignments for the relocation of State Boulevard might occur. Ninety-four resources were surveyed within this area including two structures. A number of buildings within the APE in the district and one bridge may be impacted by the project, depending upon the preferred alignment. The bridge over Spy Run Creek which is located within the district has been previously determined eligible for listing in the NRHP and contributes to the district.<sup>2</sup>

Twenty buildings and two structures outside the district, but within the APE for this project, were surveyed. Only one of these buildings was recommended eligible for the NRHP. The section of State Boulevard, outside the historic district was reviewed and recommended not eligible for the NRHP.

This photo was probably taken soon after the State Boulevard overpass was completed in 1927. Both the railroad and the interurban ran above the depressed State Boulevard. Source: Berndt, C. *The Toledo & Chicago Interurban Railway Company, Its Predecessor and Successors.* ) **Figure 15**







**Historic Property Report**  
**State Boulevard Reconstruction from Spy Run to Cass Street**  
*Additional Information Report*  
**DES No.: 0400587**

Prepared for  
**Indiana Department of Transportation/  
Federal Highway Administration**

Prepared by  
**WEINTRAUT & ASSOCIATES, INC.**

Principal Investigator: Dr. Linda Weintraut

Author: Bethany Natali, M.A.

P.O. Box 5034

Zionsville, Indiana 46077

317.733.9770

Linda@weintrautinc.com

April 2012

## **Executive Summary:**

### **State Boulevard Reconstruction from Spy Run to Cass Street**

---

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

This report provides additional information to the Historic Property Report (HPR) for the State Boulevard Reconstruction from Spy Run to Cass Street (Westerly Group, Inc., 2009). In 2009, the Westerly Group, Inc. (WGI) identified and evaluated properties more than fifty years of age at that time within the Area of Potential Effect (APE).

In February 2012, American Structurepoint, Inc. contracted with Weintraut & Associates, Inc. (W&A) to prepare an Additional Information Report (AI) to append the HPR. The purpose of the AI is to supplement the

HPR following the inclusion of two new NR-listed resources within the APE. Project historians who meet the Secretary of the Interior's Professional Standards identified and evaluated historic properties within the APE for this project in accordance with Section 106 of the National Historic Preservation Act (1966), as amended and 36 C.F.R. Part 800.

As part of AI investigations for this project, historians identified two districts that were listed in the National Register of Historic Places (NR) after the HPR (2009) was prepared; portions of both districts are contained within the project APE:

- Fort Wayne Park and Boulevard System Historic District (NR, 2010)
- Brookview-Irvington Park Historic District (NR, 2011)

The Bridge over Spy Run (ST-5/NBI No. 0200273) was previously determined eligible for listing in the NR.

Historians believe proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts. Therefore, the recommended finding of effect for this project is: Historic Properties Affected—Adverse Effect.





# INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES  
DIVISION OF HISTORIC PRESERVATION  
AND ARCHAEOLOGY  
402 West Washington Street, Room W274  
Indianapolis, Indiana 46204-2739  
Telephone Number: (317) 232-1646  
Fax Number: (317) 232-0693  
E-mail: [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov)

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Larry Stillwell

Date (month, day, year): 7/11/12

Project Title: An Archaeological Field Reconnaissance of the Proposed Additional Area Required for the State Boulevard Improvements (Des. No. 0400587) in Fort Wayne, Allen County, Indiana

## PROJECT OVERVIEW

Project Description:

The City of Fort Wayne, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map, and in the east ½ of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing two-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the sub-standard horizontal curve. The five lane section will include two new travel lanes in each direction and a center two-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

INDOT Designation Number/ Contract Number: 0400587

Project Number: 12FR66

DHPA Number:

Approved DHPA Plan Number:

Prepared For: American Structurepoint, Inc.

Contact Person: Ms. Briana Hope

Address: 7260 Shadeland Station

City: Indianapolis

State: IN

ZIP Code: 46256-3917

Telephone Number: 317 547 5580

E-mail Address: [bhope@structurepoint.com](mailto:bhope@structurepoint.com)

Principal Investigator: Larry N. Stillwell

Signature:

Company/Institution: Archaeological Consultants of Ossian (ACO)

Address: PO Box 2374

testing methodologies.

Attach photographs documenting disturbances below

Describe Disturbances:

Non-agricultural soil disturbance was noted throughout the project area. In the extreme eastern end of the project commercial development had impacted the few areas of exposed soil that had not been paved (Figure 4). Soils in this area had been graded and filled.

The portion of the project situated between North Clinton Street and the abandoned railroad grade was predominantly residential. Soils had been disturbed in these areas by grading and filling activity associated with home construction, construction of lots and driveways for parking cars, road removal, contouring of the ground for drainage of the neighborhoods, and the removal tracks from a rail line (Figures 4 and 5).

In the western end of the project, soils had been disturbed by residential and commercial development. Most of the project universe was comprised of made land created through commercial development.

No intact naturally occurring soil stratigraphy was encountered during shovel testing of the project.

Comments:

The entire State Boulevard project was 12.8 acres in area. A total of 3.5 acres was originally surveyed by ACO in 2009 (Stillwell 2009). Of the remaining 9.3 acres of additional area examined during the current field reconnaissance, 3.5 acres was covered in existing asphalt pavement. Thus, 5.8 acres of the project was subjected to shovel testing.

## RESULTS

☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.

☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.

☒ Phase Ia reconnaissance has located no archaeological resources in the project area.

☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares:  acres:

Comments:

## RECOMMENDATION

☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.

☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.

☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:



Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

### Attachments

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (*1:24,000scale*).
- ☒ Aerial photograph showing the project area, land use and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (*if available*)

Other Attachments:

References Cited:

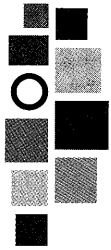
Comments:

### Curation

Curation Facility for Project Documentation:

## **APPENDIX F. Correspondence**





AMERICAN  
**STRUCTUREPOINT**  
INC.

March 23, 2009

James A. Glass, PhD  
State Historic Preservation Officer  
Indiana Department of Natural Resources  
402 West Washington Street, W247  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map and in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. Please see the attached maps and aerial photographs for specific project location.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

The need for the project originates from the substandard horizontal curve along State Boulevard. A new alignment in this section will provide enhanced roadway safety. Curb and gutter, sidewalks, and a new storm sewer system along with "livable" street amenities, such as lighting and landscaping, will also be conducted. The purpose of the project is to improve traffic flow and roadway and pedestrian safety on State Boulevard.

Land use in the vicinity of the project is primarily residential. The project area will be investigated for archaeological and historic resources for compliance with Section 106 of the National Historic Preservation Act. The results of these investigations will be forwarded to the Indiana Department of Transportation (INDOT) and the Indiana State Historic Preservation Officer for their review and concurrence.

IN20071404



James A. Glass, PhD  
March 23, 2009  
Page 2

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you are hereby requested to be a consulting party to participate in the Section 106 process. This process involves efforts to identify historic properties potentially affected by the undertaking, assess the effect of the undertaking on historic properties, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties.

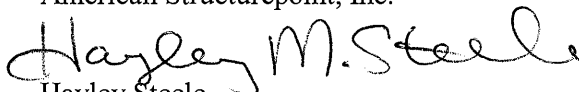
We realize you and/or your organization may want to perform a formal review of properties that may be affected as part of this project and, therefore, may require more detailed information regarding the scope of the project. Please note, preparation of a historical property report is underway and will be sent at a later date to consulting parties.

The following agencies have been invited to be consulting parties:

- State Historic Preservation Officer (SHPO)
- INDOT
- INDOT Fort Wayne District
- City of Fort Wayne Engineer
- Historic Landmarks Foundation of Indiana
- Allen County Historian
- Allen County – Fort Wayne Historical Society
- ARCH, Inc.
- Fort Wayne Historic Preservation Review Board
- John Shoaff, Fort Wayne City Council Member

Per 36 CFR 800.3(f), we hereby request the SHPO notify this office **within 30 days of the receipt of this letter** if the SHPO is aware of any other parties that may be entitled to be consulting parties for this project area.

Very truly yours,  
American Structurepoint, Inc.

  
Hayley Steele  
Environmental Scientist

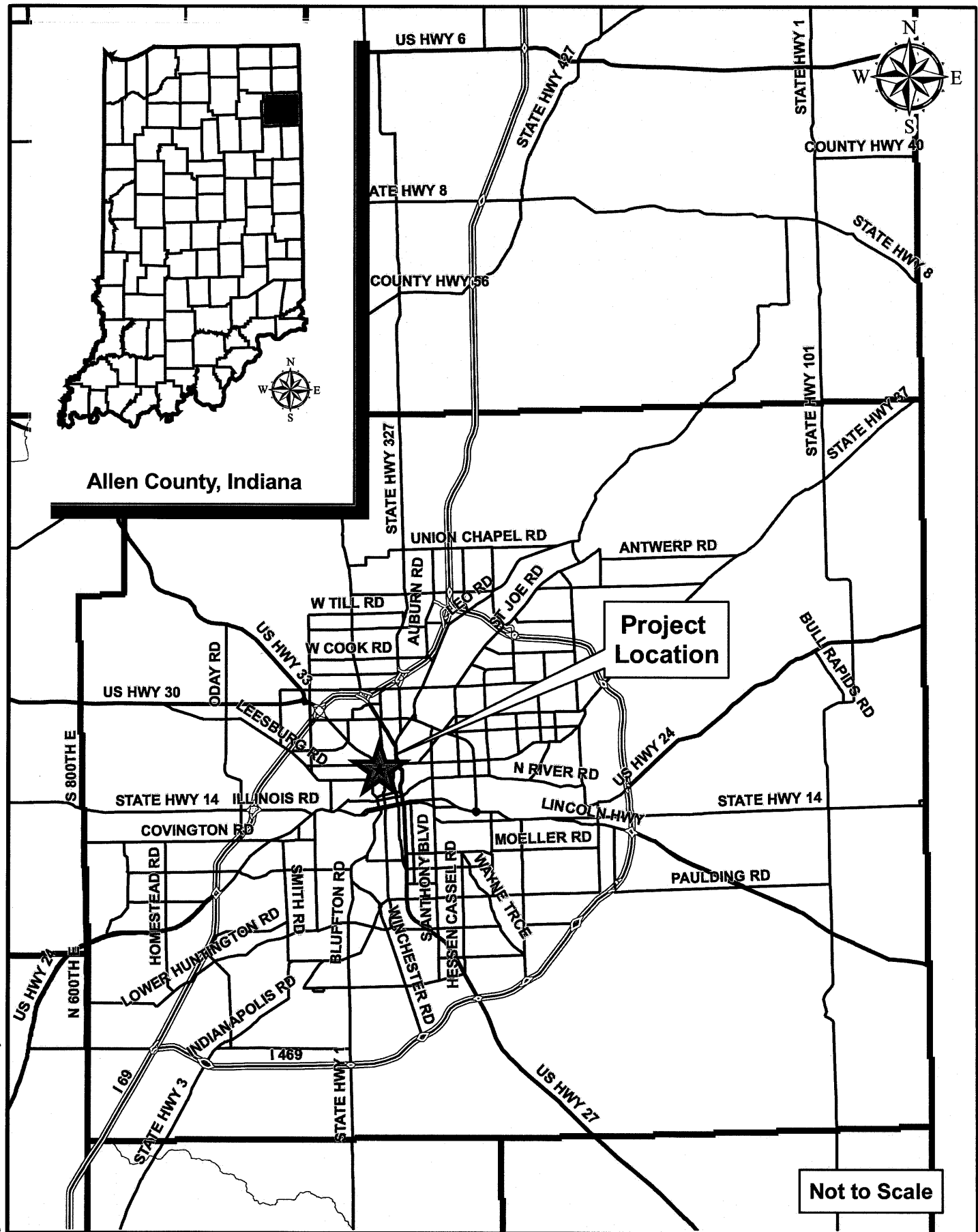
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Enclosures





Allen County, Indiana



Not to Scale

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INC.**  
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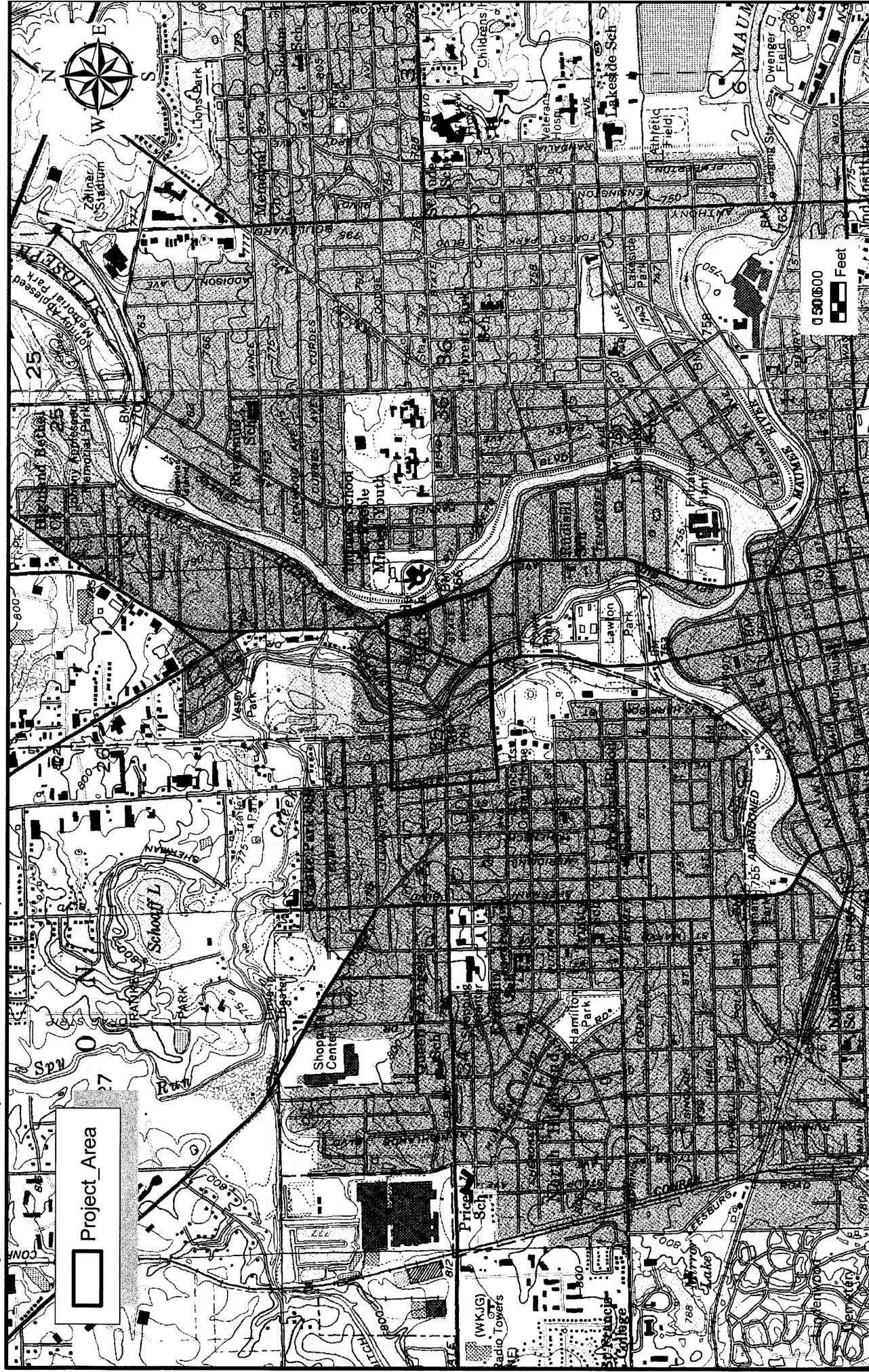
### Project Location Map

Applicant: City of Fort Wayne, Board of Public Works  
420 City-Council Building  
One Main Street  
Fort Wayne, Indiana 46802

### State Boulevard Reconstruction

Location: Fort Wayne  
Township: Wayne  
County: Allen  
State: Indiana

Date: 03/12/2009



<p><b>USGS Topographic Mapping</b> Fort Wayne West and Fort Wayne East Quadrangles</p>	<p><b>Applicant:</b> City of Fort Wayne, Board of Public Works 420 City-Council Building One Main Street Fort Wayne, Indiana 46802</p>	<p><b>State Boulevard Reconstruction</b></p> <p><b>Location:</b> Fort Wayne <b>Township:</b> Wayne <b>County:</b> Allen <b>State:</b> Indiana</p> <p><b>Date:</b> 03/12/2009</p> <p>Sheet 2 of 3</p>
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**Applicant:** City of Fort Wayne, Board of Public Works  
420 City-Council Building  
One Main Street  
Fort Wayne, Indiana 46802

## State Boulevard Reconstruction

Location: Fort Wayne  
Township: Wayne  
County: Allen  
State: Indiana

Date: 03/12/2009



The mission of ARCH is  
to advocate for the protection  
and preservation of historically  
and culturally significant assets  
and historic places in  
Allen County and  
northeast Indiana.

Hayley Steele  
Environmental Scientist  
American Structure Point, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256-3957  
**4/16/09**

Dear Ms. Steele,

Thank you for your invitation to become a Consulting Party in the State Boulevard Reconstruction Project located in Fort Wayne, Allen County, Indiana. (Des. No. 0400587, Project No. IN20071404) We accept your invitation and have returned the enclosed postcard indicating the same. If you have not already done so, I suggest that the following two groups that have a demonstrated interest in the project be added as Consulting Parties.

1. Friends of the Parks, a not-for-profit organization that has a concern for the parks of Allen County has a particular concern for the effects on historic parks and designed landscapes such as those likely to be encountered in this project. Contact person is Julie Donnell, 3604 South Washington Road, Fort Wayne, IN 46802.
2. Brookview Neighborhood Association represents the residents in the historic Brookview neighborhood through which the project will pass. They have a vested legal and economic interest in the project and should be added as Consulting Parties. Contact person is Michelle Briggs-Wedaman, 2326 Eastbrook Drive, Fort Wayne, IN 46805

Sincerely yours,

Michael Galbraith  
Historic Preservation Specialist, ARCH, Inc.  
[mgalbraith@archfw.org](mailto:mgalbraith@archfw.org)

ecc: Michelle Briggs-Wedaman, Brookview Neighborhood Association  
John Carr, DHPA  
Carrie Chapman, Friends of the Parks  
Julie Donnell, Friends of the Parks  
Todd Zeiger, HLF1





**Indiana Department of Natural Resources**

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739  
Phone 317-232-1646 • Fax 317-232-0693 • [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov)

Mitchell E. Daniels, Jr., Governor  
Robert E. Carter, Jr., Director



April 23, 2009

Hayley Steele  
Environmental Scientist  
American Structurepoint, Inc.  
7620 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Notification concerning the Reconstruction of State Boulevard (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA # 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed your letter dated March 23, 2009 and received on March 25, 2009, for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana.

Thank you for notifying our office of the proposed project. As you know, a complete analysis of the project is not possible at this time. Please provide the following information to facilitate the identification and analysis of buildings and structures in the area of potential effects:

- ❖ Literature Review
- ❖ Historic Context
- ❖ Research Methodology
- ❖ Property Descriptions
- ❖ National Register of Historic Places eligibility evaluations and recommendations

For further guidance on the indicated information, please refer to Appendix AA of INDOT's Indiana Cultural Resources Manual (<http://www.in.gov/indot/7287.htm>). Please keep in mind that additional information may be requested in the future.

It is our understanding from the information contained in your letter that the project area will be investigated for archaeological resources. Therefore, we will await the archaeological information which will be provided.

Similarly, it appears that you will be providing us with a historic property report regarding aboveground properties within an area of potential effects ("APE") that is yet to be defined. If so, then it appears likely that the kinds of information outlined above will be provided to us in that format. We do have a few comments to offer at this time.

Regarding consulting parties, we agree with the recommendation by Michael Galbraith of ARCH, Inc., in his April 16, 2009 letter to you, that Friends of the Parks and Brookview Neighborhood Association be invited to participate as consulting parties in this Section 106 review. It appears that the State Boulevard Bridge over Spy Run Creek (also known as Allen County Bridge #546 or the Evard Bridge), may be eligible for inclusion in the National Register. Because it appears that this bridge



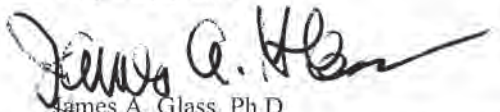
will be affected by the project, we ask that you also invite Paul Brandenburg of the Indiana Historic Spans Task Force (5868 Croton Circle, Indianapolis, Indiana 46254; e-mail: indianabridges@sbcglobal.net) and bridge historian Dr. James L. Cooper (629 East Seminary Street, Greencastle, Indiana 46135; e-mail: jlcooper@ccrtc.com) to participate as consulting parties.

Because this project will physically affect an area at least four city blocks in length, we ask that you carefully delineate the APE to include not only areas where physical effects and obvious visual effects are likely but also areas where foreseeable indirect effects could occur. It appears to us that realigning State Boulevard throughout much of the project area could cause not only a variety of atmospheric effects but also effects such as limiting access to historic properties, either temporarily or permanently, which could alter their uses (*see* 36 C.F.R. § 800.16[d] and [i] and § 800.5[a] and especially [a][2][iv] and [v]).

In a separate undertaking involving INDOT's proposed replacement of the US 27 (Clinton Street) bridge over Spy Run Creek (Des. No. 0200914), we have expressed the opinion that the Brookview-Irvington Park Historic District is eligible for inclusion in the National Register of Historic Places under Criterion C as an example of community planning. ARCH, Inc., has proposed to have an application for nomination of the district to the National Register prepared. Although a formal finding has not yet been made in that project, it is our understanding that FHWA and INDOT preliminarily have agreed with our opinion on the district's eligibility. Presumably, FHWA, INDOT, and we would arrive at a similar conclusion with regard to the undertaking proposed here along State Boulevard, which would cross the district. We have enclosed a map with the preliminary boundary of the district shown in dark lines.

*A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at [www.achp.gov](http://www.achp.gov) for your reference.* If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or [ajohnson@dnr.IN.gov](mailto:ajohnson@dnr.IN.gov). Questions about buildings or structures should be directed to John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). In all future correspondence regarding the above indicated project, please refer to DHPA # 5903.

Very truly yours,



James A. Glass, Ph.D.

Deputy State Historic Preservation Officer

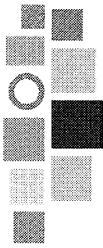
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Enclosure

cc: Robert F. Tally, Jr., P.E., Federal Highway Administration, Indiana Division  
Michelle Allen, Indiana Department of Transportation

enc: Shaun Miller, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Michael Galbraith, ARCH, Inc.  
Julie Donnell, Friends of the Parks  
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.  
Paul Brandenburg, Indiana Historic Spans Task Force  
James L. Cooper, Ph.D., historian





AMERICAN  
**STRUCTUREPOINT**  
INC.

7260 Shadeland Station  
Indianapolis, Indiana 46256  
TEL 317.547.5580  
FAX 317.543.0270

[www.structurepoint.com](http://www.structurepoint.com)


## LETTER OF TRANSMITTAL

<b>To:</b> Dr. James Glass State Historic Preservation Officer DNR - Historic Preservation & Archaeology 402 W. Washington Room W274 Indianapolis, Indiana 46204	<b>Date:</b> July 2, 2009	<b>Billing Group:</b>
	<b>Project Number:</b> IN20071404	<b>Billing Phase:</b>
	<b>Project Description:</b> City of Fort Wayne - State Blvd. between Spy Run and Cass Street Fort Wayne, Indiana (IN) Project Des. 0400587 Purchase Order No. 90640023-000	

<b>Deliver by:</b>	<b>We are sending you:</b>		
Time:	<input checked="" type="checkbox"/> Enclosed	via	<input checked="" type="checkbox"/> Mail
Date:	<input type="checkbox"/> Separately		<input type="checkbox"/> Messenger
			<input type="checkbox"/> Overnight
			<input type="checkbox"/> As Requested
			<input type="checkbox"/> For Your Information
			<input checked="" type="checkbox"/> For Your Review & Comment
			<input type="checkbox"/>

Quantity:	Type:	Description:
1	RPT	An Archaeological Field Reconnaissance of the Proposed State Boulevard Improvements (Des. No. 0400587)

<b>Remarks:</b>
Please review the enclosed report and contact me with any questions or comments you may have. Feel free to contact me by phone, (317)547-5580, or by email at <a href="mailto:hsteele@structurepoint.com">hsteele@structurepoint.com</a> . Thank you.

<b>Copies:</b>	<b>Trans:</b>	<b>Enclosure:</b>	<b>To:</b>	<b>Very truly yours,</b> American Structurepoint, Inc.   Hayley Steele
Please return      copies to this office.				



AMERICAN  
**STRUCTUREPOINT**  
INC.

November 9, 2009

Dr. James A. Glass  
Deputy State Historic Preservation Officer  
Indiana Department of Natural Resources  
Division of Historic Preservation and Archaeology  
402 West Washington Street, Room W274  
Indianapolis, Indiana 46204

Re: Des. No. 0400587, DHPA #5903  
State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. The need for the project originates from the substandard horizontal curve along State Boulevard.

The total project length is approximately 2,300 feet. The existing section of State Boulevard from North Clinton Street to Spy Run will be widened to five lanes along the existing alignment. The existing 2-lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. Project historians from The Westerly Group (WG), who meet or exceed the Secretary of Interior's standards for Section 106 work, identified and evaluated historic properties within the Area of Potential Effect (APE) for this project. Historic properties were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (revised January 2001), Final Rule on Revision of Current Regulations, December 12, 2000, and incorporating amendments effective August 5, 2004. The Historic Properties Report and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on November 6, 2009.

IN20071404





Dr. James A. Glass  
November 9, 2009  
Page 2

The APE of this undertaking is within the Brookview-Irvington Historic District (District). The District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne, especially the planned suburban developments of the Wildwood Companies. In addition, it is eligible as a designed landscape, the work of Arthur A. Shurcliff. The District contains an estimated 315 individual resources. There are 92 individual buildings within the District that were reviewed to determine their contribution to the District. These were located within the APE and could be directly or indirectly affected by the undertaking. All but 12 of these buildings were deemed to contribute to the District. The bridge over Spy Run Creek, which also contributes to the District, and the Brookview-Irvington Historic District are recommended as eligible for the NRHP. State Boulevard itself, both within the District and to the east and west of it, was analyzed. State Boulevard within the District is recommended individually eligible for the NRHP because of its contribution to the District. State Boulevard outside of the District and within the APE is recommended as not individually eligible for the NRHP. Twenty individual buildings within the APE but outside of the District were analyzed. Nineteen of the 20 buildings are not individually eligible for the NRHP, and one is recommended individually eligible for the NRHP. The former railroad and interurban overpass was evaluated and determined not individually eligible for the NRHP.

Due to the proposed realignment of State Boulevard and the replacement of the bridge over Spy Run, impacts to the Brookview-Irvington Historic District are expected. Because of this, a *preliminary* effect finding of Adverse Affect to Historic Properties is anticipated. Additional effects analyses are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting your review and comment on the Historic Properties Report (HPR) and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the HPR by December 11, 2009.

We are also requesting your dates of availability for a consulting parties meeting. The meeting will occur after SHPO and all consulting parties have had time to review the HPR. We would like to have the meeting during either the week of December 7, 2009, or the week of December 14, 2009. I have attached a chart of the possible days for the meeting. Please return this chart indicating either morning or afternoon on any day that you are available for a consulting parties meeting. After receiving availability from SHPO, the Federal Highway Administration, INDOT, and consulting parties, a meeting will be scheduled and an agenda will be set. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at [hsteele@structurepoint.com](mailto:hsteele@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.



Hayley M. Steele  
Environmental Scientist

HMS:mgn

Enclosures

See distribution list on the next page.

Dr. James A. Glass  
November 9, 2009  
Page 3

cc: Patrick Carpenter, Historian, Cultural Resources Section – INDOT (via email)  
Joyce Newland – Federal Highway Administration  
Shan Gunawardena – City of Fort Wayne  
Angie Quinn and Michael Galbraith – ARCH, Inc.  
Don Orban – Fort Wayne Historic Preservation Commission  
Todd Zeiger – Historic Landmarks Foundation of Indiana – Northern Office  
Julie Donnell – Friends of the Parks of Allen County  
Michelle Briggs-Wedaman – Brookview Neighborhood Association  
Dr. James L. Cooper  
Paul Brandenburg – Indiana Historic Spans Task Force  
Charley Shirmeyer – Northside Galleries  
Karl Dietsch – Brookview Neighborhood Association  
Susan Haneline – Brookview Neighborhood Association  
Annette Daily – Brookview Neighborhood Association  
Dan Avery – Northeastern Indiana Regional Coordinating Council  
Suzanne Slick – Irvington Park Neighborhood Association  
Camille Fife – Westerly Group (via email)





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## M E M O R A N D U M

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**DATE:** December 1, 2009

**TO:** Ms. Joyce Newland, Federal Highway Administration  
Mr. Patrick Carpenter, INDOT Cultural Resources  
Mr. Shan Gunawardena, City of Fort Wayne  
Ms. Camille Fife, Westerly Group  
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology  
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.  
Mr. Don Orban, Fort Wayne Historic Preservation Commission  
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana  
Ms. Julie Donnell, Friends of the Parks of Allen County  
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Mr. John H. Shoaff, Fort Wayne City Council  
Dr. James L. Cooper  
Mr. Paul Brandenburg, Indiana Historic Spans Task Force  
Ms. Susan Haneline, Brookview Neighborhood  
Mr. Charley Shirmeyer, Northside Galleries  
Mr. Karl Dietsch, Brookview Neighborhood  
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council  
Ms. Suzanne Slick, Irvington Neighborhood  
Ms. Jan Daily, Brookview Neighborhood

**FROM:** Hayley Steele, American Structurepoint, Inc.

**RE:** State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
Structurepoint No. IN20071404

**CC:** Scott Crites, American Structurepoint, Inc.

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This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for December 15, 2009 at 9:30 am. The meeting will be held in the City County Building, Room 128. Because several of the consulting parties for the State Boulevard Project are also on a list of consulting parties for a nearby project (US 27 over Spy Run) it was requested the meetings for both projects be held on the same day, and the meeting was therefore scheduled for the morning of the 15th.

The City County Building is located at 1 East Main Street in downtown Fort Wayne. This is between South Calhoun and South Clinton Street along Main Street, approximately 1 mile south of the State Boulevard project area. Parking is available in a parking garage attached to the City County Building.

Please see the attached agenda for the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at [hsteele@structurepoint.com](mailto:hsteele@structurepoint.com). If you have any questions or need additional information please feel free to contact me.

**AGENDA**  
**Consulting Parties Meeting**  
**State Boulevard Reconstruction (Des. No. 0400587)**  
**City of Fort Wayne, Allen County, Indiana**

December 15, 2009  
9:30 am  
City County Building- Room 128  
1 East Main Street  
Fort Wayne, Indiana

1. Overview of Proposed Project (American Structurepoint)
  - a. Purpose and Need
  - b. Proposed Improvements
  - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Discussion of Potential Mitigation Measures (Westerly Group/Structurepoint)
4. Next Steps (Westerly Group/Structurepoint)
  - a. Development of Memorandum of Agreement
  - b. Follow-up items



162 E State Blvd

Fort Wayne, In. 46805

12/7/2009

Hayley Steele, Environmental Scientist

American Structurepoint, Inc.

7260 Shadeland Station

Indianapolis, In. 46256

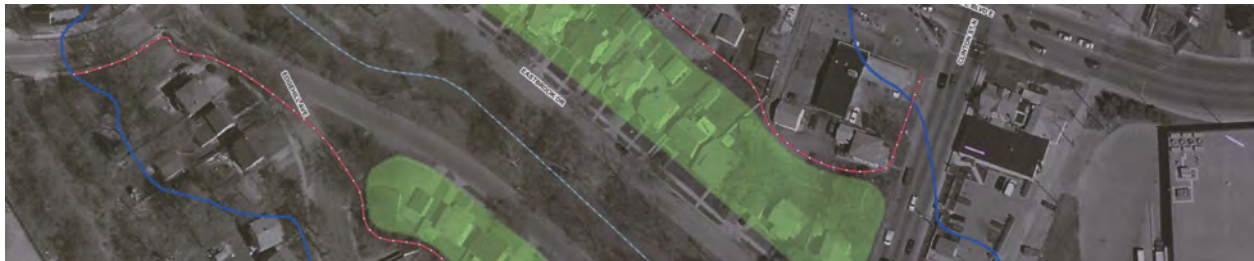
Dear Ms. Steele,

I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effect (APE). The Westerly Group presents the report in a user-friendly format that informs the reader of the project and its anticipated outcome. I appreciate the cautionary notes regarding the preliminary status of the design and the knowledge gained from the report.

One of the recurring themes throughout the report is the mixed-use category that the APE has always had and many residents are acting to preserve its historic nature. As the report notes on page 5, State Boulevard is an urban minor arterial road, (*"Urban minor arterial road" means a route that generally interconnects with and augments an urban principal arterial road and provides service to trips of shorter length and a lower level of travel mobility*) that was designed to slow down traffic by a master landscaper whose examples are limited. The fact that there was a larger design that was never completed makes it likely that the area will become a part of the National Register of Historic Properties under several criterion.

The history of the designers, builders, and the neighborhoods is consistent with the current atmosphere of the APE. The individual descriptions of the properties and the area characteristics allow the reader to understand the primary affected properties and their contribution to the APE. While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling that residents of this neighborhood share.

Furthermore, the recent closing of the Kroger Fuel Center (photo 57) may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek (below).



If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State Boulevard and Spy Run Avenue (as seen in the photo section below) the road would pass behind or around most of the homes in the APE . The designers of the road project could not have foreseen the closing



of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy, it seems only fair that a separate study be conducted factoring in possible land use of the former Kroger Fuel Center.

Sincerely,

Annette "Jan" Dailey





Northern Regional Office  
402 W. Washington  
South Bend, Indiana 46601  
574-232-4534  
574-232-5549 (fax)

December 8, 2009

Ms. Hayley Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

Dear Ms. Steele,

I am writing today to submit comments concerning the Draft Historic Properties report dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). Thank you for your response to my earlier inquiry concerning the scope of our comments at this time. I understand that we are only commenting on the HPR at this time and that we will receive project specific design information for analysis in the future. In partnership with our organizational partner in Fort Wayne, ARCH, we have a number of specific questions and concerns which I will outline below.

1. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne. We believe that the HPR should note that not only is the district eligible but that a National Register nomination is currently being written by ARCH and the Fort Wayne Historic Preservation, with funding from the Indiana Division of Historic Preservation and Archaeology, the boundaries of which will correspond with the proposed boundaries outlined in the draft HPR. The nomination is being drafted to include eligibility not only under Criteria A but also Criterion C as a designated landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. Project Specific Design information: While it would be unusual to combine an HPR with information normally associated with later parts of the Section 106 or 4f process, we wish to note for the record that no maps delineating proposed road changes were included with the narrative description of the project. We note this due to the inclusion at this early stage of the review process language addressing outlining a "preliminary finding of adverse effect" contained in the cover letter as well as the HPR. With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.
3. Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State

Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.

Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated ) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties”. While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.

4. Purpose and Need. We note in the cover letter as well as within the HPR the stated Purpose of the project is “*to improve traffic flow, roadway, and pedestrian safety along State Boulevard.*” The Need for the project is stated as “*substandard horizontal curve along State Boulevard*”. We have a number of significant concerns and questions about how the proposed project will advance that purpose and meet the need. Again, it is unusual to get in to the merits of purpose and need and those metrics within the context of a discussion about the Historic Properties Report. Without specific plans it is difficult to analyze what alternatives under review would not only meet the purpose and need of the project but also avoid adverse impacts to the District and the State Boulevard bridge (noted above as an individually eligible and contributing resource to the district.) We anticipate additional questions once we receive more detailed project documents. In an effort to help keep the review moving we offer the following questions based on the narrative supplied in the HPR. We are offering the following based on the stated purpose as outlined in the cover letter and in the Executive Summary on page 3 of the HPR.
  - a For the purposes of our participation in the Section 106 and anticipated 4f review we will utilize the Purpose and Need as outlined in the Cover Letter and on page 3 of the HPR. Alternatively, if this is not correct please forward the project’s Purpose and Need along with the alternatives so that we can evaluate same in a proper manner.
  - b We have a number of questions and concerns about how the project as described in written form in the HPR will meet the stated purpose and need. Again, it is not possible to appropriately review and comment without project maps and designs of alternatives but absent those we can offer the following comments.
    - 1) How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.
    - 2) Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus.
    - 3) Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.
    - 4) Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.
    - 5) Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.
    - 6) Please provide detailed traffic accident studies or report data pertaining to accidents at the “substandard” curve.

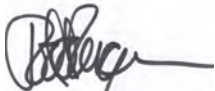


- 7) Please provide detailed pedestrian accident studies or report data pertaining to the "substandard curve."
  - 8) How is the proposal to add a new "trail bridge" over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed "trail bridge".
5. "Trail Bridge": We continue to be concerned about an on-going appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future "trail bridge" should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.
  6. Concerning the "'substandard horizontal curve": Page 4 of the HPR states that " the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive." We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.
  7. Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7, We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).
  8. Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard

to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.

The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.

Sincerely,



Todd A. Zeiger

Director, Northern Region

cc. Indiana Division of Historic Preservation and Archaeology  
Consulting Parties for State Boulevard Project





AMERICAN  
**STRUCTUREPOINT**  
INC.

7260 Shadeland Station, Indianapolis, Indiana 46256  
TEL 317.547.5580 FAX 317.543.0270

[www.structurepoint.com](http://www.structurepoint.com)

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## M E M O R A N D U M

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**DATE:** December 8, 2009

**TO:** Ms. Joyce Newland, Federal Highway Administration  
Mr. Patrick Carpenter, INDOT Cultural Resources  
Mr. Shan Gunawardena, City of Fort Wayne  
Ms. Camille Fife, Westerly Group  
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology  
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.  
Mr. Don Orban, Fort Wayne Historic Preservation Commission  
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana  
Ms. Julie Donnell, Friends of the Parks of Allen County  
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Mr. John H. Shoaff, Fort Wayne City Council  
Dr. James L. Cooper  
Mr. Paul Brandenburg, Indiana Historic Spans Task Force  
Ms. Susan Haneline, Brookview Neighborhood  
Mr. Charley Shirmeyer, Northside Galleries  
Mr. Karl Dietsch, Brookview Neighborhood  
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council  
Ms. Suzanne Slick, Irvington Neighborhood  
Ms. Jan Daily, Graduate Student, IPFW, Sociological Practice  
Mr. Creager Smith, Historic Preservation Planner  
Mr. Albert Cohan, Westbrook 5, LLC  
Mr. Thomas M. Niezer, Barrett & McNagny, LLP  
Mr. Ronald Ross, AIA, Martin Riley Architects and Engineers

**FROM:** Hayley Steele, American Structurepoint, Inc.

**RE:** State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
Structurepoint Project No. IN20071404

**CC:** Scott Crites, American Structurepoint, Inc.

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The agenda for the Consulting Parties Meeting scheduled for December 15, 2009 has been revised. Please feel free to contact me at [hsteele@structurepoint.com](mailto:hsteele@structurepoint.com) or by phone at (317) 547-5580 with any questions or concerns.

We look forward to meeting with all of you on December 15, 2009 at 9:30 am.

Thank you

**AGENDA**  
**Consulting Parties Meeting**  
**State Boulevard Reconstruction (Des. No. 0400587)**  
**City of Fort Wayne, Allen County, Indiana**

December 15, 2009

9:30 am

City County Building- Room 128

1 East Main Street

Fort Wayne, Indiana

(Revised 12/07/09)

1. Overview of Proposed Project (American Structurepoint)
  - a. Purpose and Need
  - b. Proposed Improvements
  - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Future Steps in the Process (Westerly Group/Structurepoint)
  - a. Potential Mitigation Measures
  - b. Development of Memorandum of Agreement
4. Follow-up items





437 East Berry Street

Suite 204

Fort Wayne IN 46802

tel: 260.426.5117

www.archfw.org

The mission of ARCH is  
to advocate for the protection  
and preservation of historically  
and culturally significant assets  
and historic places in  
Allen County and  
northeast Indiana.

Hayley Steele  
Environmental Scientist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

**12/9/09**

Dear Ms. Steele,

I am writing in regard to the Historic Property Report (HPR) dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). ARCH would like to thank you for the opportunity to comment and to commend Ms. Fife for the thorough, well-written Report. I have a few specific questions and concerns regarding the report.

1. 1. Regarding the cover letter. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP). We agree that it is likely eligible under Criterion A for its association with community planning and development in Fort Wayne. We also believe that the greater depth provided in the under progress NRHP Nomination will demonstrate eligibility under Criterion A for Architecture and Landscape Architecture. We also agree with your recommendation that the District be considered eligible under Criterion C as a designed landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. The greater depth provided in the under progress NRHP Nomination will hopefully demonstrate that eligibility. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. 2. Regarding maps. The HPR contained no maps, plans or drawings of the proposed project. We have found it difficult to study, review, and analyze this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review? Particularly helpful will be all maps, plans and drawings that detail possible alternatives that may have been studied.
3. Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary (page 3) of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states

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ARTS  
UNITED  
MEMBER



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to advocate for the protection  
and preservation of historically  
and culturally significant assets  
and historic places in  
Allen County and  
northeast Indiana.

that the "purpose of the project is to improve traffic flow, roadway and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "adding a new trail bridge" as stated on page 3, addresses "traffic flow, roadway and pedestrian safety" along State Boulevard? Need is stated in the cover letter to "originate from the substandard horizontal curve along State Boulevard." I have addressed this in point 5 below.

4. Regarding the "Trail Bridge." On page 3 and page 4 of the HPR a new "trail bridge" is described as being part of the project. Page 4 states that the project includes a "pre-fabricated trail bridge over State Boulevard at the abandoned New York central railroad right-of-way between Cass Street and Westbrook Drive." Besides the safety issues that installation of this bridge raises, there is the issue of project aggregation/co-mingling with which we have been struggling in the US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.
5. Regarding the "substandard horizontal curve." Page 4 of the HPR states that "the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We question the analysis of "substandard" and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower



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environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive."

6. Regarding the Area of Potential Effect (APE). Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State Boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR. Also regarding the APE, were potential changes in traffic use in the Brookview - Irvington Park Historic District studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. 36 CFR 800.16 indicates that the APE shall be established to include "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties" yet page 6 of the HPR indicates that the APE for the State Boulevard reconstruction project was drawn to consider "potential direct impacts on historic districts or properties". We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this District. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.
7. Regarding the Project Description. Plans for this project seem to be quite complete according to the project description on page 7 of the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking."
8. Regarding the homes south of State Boulevard. Page 7 of the HPR states that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. As in point 4 regarding the trail bridge, we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate. The





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northeast Indiana.

City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview - Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.

9. Regarding the seasonal flooding. Page 22 of the HPR states that "At the southern end of the district, between East State Boulevard and North Clinton Street, the creek's seasonal flooding has caused damage to homes." We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview - Irvington Park Historic District has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.

Sincerely yours,

Michael Galbraith  
Historic Preservation Specialist, ARCH, Inc.  
[mgalbraith@archfw.org](mailto:mgalbraith@archfw.org)

emc: Dan Avery, NIRCC  
Paul Brandenburg, SPANS  
Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Karie Brudis, DHPA  
Patrick Carpenter, INDOT-CRS  
John Carr, DHPA  
Dr. James L. Cooper, SPANS  
Julie Donnell, Friends of the Parks  
Camille Fife, Westerly Group  
Joyce Newland, FHWA  
Don Orban  
John Shoaff, Fort Wayne City Council  
Creager Smith  
Todd Zeiger, HLF



**Friends of the Parks of Allen County, Inc.  
3604 South Washington Road  
Fort Wayne, Indiana 46802**

Hayley Steele  
Environmental Scientist  
American StrucutrePoint  
7260 Shadeland Station  
Indianapolis, IN 46256  
12/10/09

Dear Ms. Steele,

Regarding the proposed State Boulevard reconstruction in Allen County (Des. No. 0400587, DHPA# 5903) :

Since, as I told you several weeks ago, I am unavailable to attend the CP meeting on the 15<sup>th</sup>, I am following your advice and putting some of the concerns of the Friends of the Parks in writing. This letter is a response to your agenda, as amended in the e-mail exchange you have had with Mr. Galbraith, and a response to the HPR.

While The Friends of the Parks do not doubt the sincerity of your intentions, we continue to fear that in this case that this procedure is an afterthought rather than a respectful and necessary procedure.

Our concerns begin as a result of a meeting that we had with the staff of the City of Fort Wayne and other subsequent public meetings in the summer of 2008.

At the time, and only because we requested it, the “concept” of widening State Street at the bridge of Spy Run Creek, was presented to our board. The city staff presented the project, and though it was called a “concept” at the time, it bore no relation to what one would typically call a *conceptual* drawing except insofar as it was not a *detailed* drawing. Otherwise, the location, widening and straightening of State Boulevard were not conceptual, they were the FUTURE, and the staff felt very justified about taking the houses down, for this and for the flood control project on East Brook, referring to the “transient” nature of the people who live there, and suggesting that ultimately high rise apartments would be more suitable in the area. In point of fact, this thinking is supported by the traffic ideas included in the future North River Now Plan Final Report (the City does not yet own the property to be developed) prepared in 2007, which assumes the widening and straightening of State Boulevard, and other traffic connections to it. I do not include this information by way of criticism, but to underline that at the time it was already clear that a course of action had been decided upon and justified by more than one line of reasoning.

Thankfully we now have established that this place is important, and nationally so.

Despite the above being a bit of a historical digression, that which is pertinent to the current process and how it is carried out is that, **at that time, the concept was already in place and was unalterable, having over time been linked to future and current plans for the downtown, despite protests by the public, the recognition of the district's importance, and the requests of historic preservationists to reconsider. It also appeared tied to other disturbing projects that were underway.** City staff insisted that they had reviewed all suitable alternatives, and that this was the one that made the most sense. And, they noted, it was in keeping with other with other projects for the good of the community including the removal of homes for a flood control project on Eastbrook, and the changes for the bridge on US 27 which is now also involved in a Section 106 process.

Several months later, a contract to American StructurePoint was approved by City Council for an "Engineering Study". It was a contract for 1 million dollars. An engineering study, as one understands it, is a study about "how" to carry something out structurally, in this case, widen and straighten a road and build a new bridge, rather than a study to present alternatives to solve the identified problems. In other words, our concern is that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and, that, after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.

It is particularly appropriate to call this to your attention because of the nature of the historic property in question. There is a difference between "landscape" and "landscaping", which historic preservationists, planners, and traffic engineers in America are beginning to grapple with.

Brookview Neighborhood is a designed landscape, eligible for the National Register of Historic Places as the work of a Master, as the HPR notes, and therefore changes to the interconnecting landscape components should be thoroughly reviewed before any "concept" is put forward at all. In addition to noting that the bridge and the circulation patterns will be affected, as my colleagues have done, we would ask that the project developers respond to any potential changes to the following aspects of the landscape which can be found in the Federal Guidelines for the Treatment of Historic Landscapes:

1. The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?
2. The vegetation. What are the potential impacts on the vegetation of the designed landscape?



3. The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?
4. Vistas and Views
5. What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.

These have been mentioned, but it would be useful to have them further discussed as part of the HPR, before any sort of amelioration is discussed.

This particular historic resource is more than a series of houses of a certain age, which can be easily moved or walled off. It impossible to assume that one can add “landscaping” in the form of extra vegetation or a grass median to successfully ameliorate the transformation of a 2 lane curving road into a 5 lane straight road which is situated in the middle of a historic district known to be a designed cultural landscape, impossible to respond to the totality of the impact that this enormous change will have. “Landscaping” cannot compensate adequately the potential loss of the topography, vegetation, circulation, and visual character of this place, which is a “landscape.”

Further, in this situation, it is not suitable to offer as alternatives that which simply addresses how many yards the bridge might be moved in a certain direction, or what the ingress and egress options should be. Those are details of an established plan rather than real alternatives that address the question of landscape preservation.

Any plan to alter State Boulevard to solve the problems that this concept is intended to solve, problems which so far have not been set forth clearly, should be planned by a qualified expert in preserving historic landscapes.

We also respectfully request that there be consideration of the involvement of The Cultural Landscape Foundation in the proceedings.

We respectfully submit these comments.

Sincerely,

Julie Donnell  
President  
Friends of the Parks of Allen County, Inc.







Indiana Department of Natural Resources

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Mitchell E. Daniels, Jr., Governor  
Robert E. Carter, Jr., Director



December 14, 2009

Hayley M. Steele  
Environmental Scientist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Draft historic property report (The Westerly Group, September 2009) for the proposed State Boulevard Improvements (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed the materials with your cover letter dated November 9, 2009 and received on November 12 for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana. You had requested comments by December 11, but that would have allowed us and the other consulting parties fewer than 30 days in which to respond.

The federal agency official is to determine the area of potential effects ("APE") in consultation with the State Historic Preservation Officer (36 C.F.R. 800.4[a][a]). Your cover letter and page 4 of the historic property report ("HPR") indicate that a new alignment for State Boulevard to the west of Clinton Street will be proposed but that no final alignment has yet been identified. Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, especially in graphic form, to indicate where the alignments that are under consideration would run.

Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in an avoidance or minimization of the adverse effect.

Our initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.

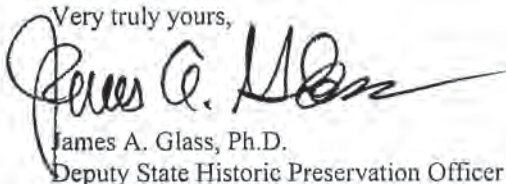
In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places with the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we note that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R.

44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link [http://www.in.gov/dnr/historic/bin/qp/qp\\_archeo.pdf](http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf)).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

*A copy of the revised 36 C.F.R. Part 800 that took effect on August 5, 2004, may be found on the Internet at [www.achp.gov](http://www.achp.gov) for your reference.* If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or [ajohnson@dnr.IN.gov](mailto:ajohnson@dnr.IN.gov). Questions about buildings or structures please should be directed to John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:ALJ-JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation  
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation  
Camille Fife, The Westerly Group, Inc.  
Larry Stillwell, Archaeological Consultants of Ossian  
Michael Galbraith, ARCH, Inc.  
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.





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## MEETING MINUTES

**Location:** City of Fort Wayne, City-County Building, Room 128

**Date:** December 15, 2009 (Minutes revised on February 3, 2010)

**Project Name:** State Boulevard Reconstruction (Des. No. 0400587)

**Project No.:** IN20071404

**Attendees:** Hayley Steele, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)  
Shan Gunawardena, Creager Smith, Ken Nicolet (City of Fort Wayne)  
Camille Fife (The Westerly Group)  
John Carr, Amy Johnson (IDNR, Division of Historic Preservation and Archaeology)  
Patrick Carpenter (INDOT, Cultural Resources)  
Joyce Newland (Federal Highway Administration)  
John Shoaff (Fort Wayne City Council)  
Don Orban (Fort Wayne Historic Preservation Commission)  
Annette "Jan" Dailey (Graduate Student, IPFW, Brookview Neighborhood Resident)  
Suzanne Slick (Irvington Park Neighborhood)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)  
Albert Cohan (Westbrook 5, LLC)  
Angie Quinn, Michael Galbraith (ARCH, Inc.)  
Karl Dietsch (Brookview Neighborhood Resident)  
Michelle Briggs-Wedaman, Scott Simmons (Brookview Neighborhood Association)  
Dan Ernst (Earth Source, Inc.)

1. The meeting was held at 9:30 a.m., December 15, 2009, to discuss the State Boulevard Reconstruction Project (Project), the Historic Properties Report written by the Westerly Group, and future steps in the Section 106 process.
2. Rich Zielinski introduced himself and began the meeting with introductions around the room.
3. Scott Crites gave an overview of the Project, including the purpose and need, proposed improvements, four alternatives, and the project schedule (presentation is attached).
4. Camille Fife gave a presentation (attached), including information regarding:
  - a. Section 106 process to date
  - b. Definition of an Area of Potential Effect (APE)
  - c. State Boulevard project area
  - d. The Brookview-Irvington Historic District
  - e. A preliminary APE
5. John Shoaff (Fort Wayne City Council) expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.

6. Michelle Briggs-Wedaman (Brookview Neighborhood Association) also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.
7. Jan Daily (Brookview Neighborhood Resident) also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecks both down Huguenard and along State Boulevard.
8. Angie Quinn (ARCH) discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.
9. Michelle Briggs-Wedaman agreed with Angie about Shurcliff's park-like design. The park setting is one of the primary reasons people chose to live in the neighborhood and how it attracts residents.
10. ARCH questioned Dan Avery (NIRCC) about the alternatives considered and when they were considered for the State Boulevard corridor. Dan explained during the late 70s or early 80s there were a number of studies done that looked at how to improve east-west and north-south travel throughout the urban area. Based on different alternatives, discussions with the public, and a combination of impacts (neighborhoods, parks, homes, etc), State Boulevard was chosen as a prime corridor for improvements to east-west traffic.
11. ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.
12. John Carr asked about the extent of the alignment of State Boulevard that is within the APE as a resulting product of Kessler's plan and of Shurcliff's design. Discussion took place regarding Kessler's plan lacking specific design details and Shurcliff's final product that created State Boulevard. Patrick Carpenter (INDOT) clarified the Spy Run bridge has been determined eligible and is now going through the select/non-select determination. Patrick also gave a brief definition of select and non-select. Although the list has not been finalized, the State Boulevard over Spy Run bridge is deemed non-select right now. The finalized list will be available in March 2010.
13. Camille Fife and ARCH discussed AW Grosvenor being the bridge designer for the State Boulevard over Spy Run bridge. Camille explained AW Grosvenor was not discussed in depth in the Historic Properties Report (HPR) and ARCH expressed the importance of including this information in the HPR and in the documentation of the bridge.
14. Jan Daily asked whether the select/non-select criteria were based on vehicular use of the bridge. Patrick explained this was all part of the analysis of the bridge, and these issues are difficult with a bridge such as the Spy Run bridge because it is concrete. The bridge can not be disassembled or used in part for other projects.
15. Scott explained the bridge is between five and six feet below the 100-year floodplain elevation, and it is completely submerged during flood events. The flood elevation is actually based on the St. Mary's River, so the new bridge will be constructed at an elevation higher than the flood elevation, but will not affect the current flood elevation.
16. Camille discussed the District's eligibility under criteria A and C. ARCH discussed the US 27 project and its eligibility due to community planning, landscape architecture, and architecture. They feel the same issues will arise with the State Boulevard project regarding eligibility criteria and should be agreed upon before mitigation measures are set forth for the project.
17. John Carr clarified the differences between eligibility requirements for properties within a Multiple Property Listing and for individual properties within a district.



18. Camille discussed we anticipate an adverse effect to the Historic District, to State Boulevard, and to the bridge over Spy Run as a result of this project as it has been designed thus far; within the APE and outside of the district, there may be one property that could be adversely affected. She also discussed an eligible property and a property listed on the NR are evaluated on the same level and are treated as equally important resources.
19. American Structurepoint reiterated the fact State Boulevard is remaining in place and the majority of this Project is along existing alignment. The proposed State Boulevard will be realigned to the south of the existing between Terrace Road and Westbrook, allowing the existing State Boulevard to remain in place instead of being completely eliminated. The southern-most alignment was chosen based on anticipated impacts to the neighborhood. As you go north, the substandard curve and the site distance makes the curve not practical to use. The goal in choosing the current alignment was to limit the impacts to the existing State Boulevard and to impact as few houses as possible.
20. American Structurepoint gave an overview of future steps (more meetings, mitigation, memorandum of agreement) in the Section 106 process and how they will be addressed with consulting parties.

#### **GENERAL CONCERNS**

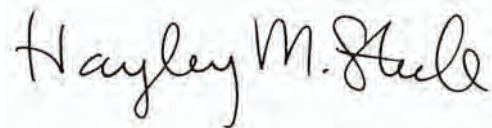
- The purpose and need presented at the meeting are different than those given in the HPR and at previous meetings.
- How will the purpose and need of the project be met with the widening and straightening of State Boulevard? ARCH and the Brookview Neighborhood Association do not feel the purpose and need are strong enough to warrant the project.
- Property values along State Boulevard could possibly decrease as a result of this project.
- Safety of the neighborhood should be a concern in the design of the project.
- The consulting parties would like a time to comment on information being presented as a result of this meeting.

#### **ACTION ITEMS**

- American Structurepoint will compile a packet of information including, but not limited to:
  - Purpose and Need of the Project
  - Traffic data
  - Flood studies
  - Alternatives analysis
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Future meetings will be scheduled as design is finalized and mitigation measures are to be discussed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or [hsteele@structurepoint.com](mailto:hsteele@structurepoint.com) to revise the record.

Very truly yours,  
American Structurepoint, Inc.



Hayley M. Steele

HMS:mgn

Enclosures



Indiana Department of Natural Resources

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Mitchell E. Daniels, Jr., Governor  
Robert E. Carter, Jr., Director



January 27, 2010

Hayley M. Steele  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the meeting minutes that we received on December 28, 2009, for the aforementioned project in Fort Wayne, Allen County, Indiana.

The request we had made in our December 14, 2009 letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to the consulting parties.

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard. It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.



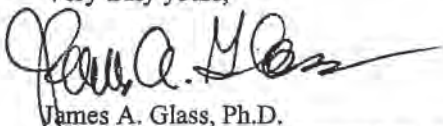
We believe it is important for FHWA to evaluate this project's purpose and need carefully before the Section 106 consultation proceeds much further. Although analysis of purpose and need has not expressly been made a part of the Section 106 review process, it seems to us that it would come into play once an adverse effect has been found formally (see 36 C.F.R. § 800.5), and the consultation turns to the consideration of alternatives that might avoid, minimize, or mitigate an adverse effect (see 36 C.F.R. § 800.6). If purpose and need are not clarified as much as possible now, and the Section 106 process proceeds through the assessment of adverse effects for only the four, very similar alternatives presented to the consulting parties, then we can foresee that by the time the process reaches the point at which it is appropriate to discuss alternatives that might avoid or minimize adverse effects, the applicant and the project planners might be reluctant to invest the time and expense that revisiting purpose and need might entail at that point. Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.

In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or [ajohnson@dnr.IN.gov](mailto:ajohnson@dnr.IN.gov). In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:JLC:jlc





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## M E M O R A N D U M

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**DATE:** February 4, 2010  
**TO:** Consulting Parties and Consulting Parties Meeting Attendees  
**FROM:** Hayley Steele *HS*  
**RE:** Follow-up Information Packet to Consulting Parties Meeting  
**CC:** Scott Crites, Structurepoint

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Per your request, enclosed please find a CD containing the City of Fort Wayne 2005 Flood Control Study as well as the traffic data collected for the State Boulevard project corridor.

Moreover, the request to re-evaluate the Purpose and Need is currently being coordinated with the Indiana Department of Transportation (INDOT), the Federal Highway Administration (FHWA), and the City of Fort Wayne. After coordination is complete the alternatives will once again be evaluated, and a determination will be made as to which alternative best meets the Purpose and Need. Once complete, this information will be sent to all consulting parties and attendees of the consulting parties meeting.

Also enclosed, please find the updated meeting minutes for the Consulting Parties meeting that took place on December 15, 2009. Please keep these final meeting minutes for your records.

Please feel free to contact me by phone at (317) 547-5580 or by email at [hsteele@structurepoint.com](mailto:hsteele@structurepoint.com) if you have any questions or concerns regarding the attached information. We appreciate your cooperation in the development of this project.





Indiana Department of Natural Resources

Mitchell E. Daniels, Jr., Governor  
Robert E. Carter, Jr., Director

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March 10, 2010

Scott Crites  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Final minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903), the notice of the possible availability of the State Boulevard bridge over Spy Run Creek, the March 2005 "Spy Run Creek Flood Protection Study," and crash and traffic volume data, which was collected at various times between 2004 and 2008, inclusive, for various locations within and near the project area.

Dear Mr. Crites:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the aforementioned documents that accompanied your February 4, 2010 cover memorandum, all of which we received on February 8, 2010, for the aforementioned project in Fort Wayne, Allen County, Indiana.

Thank you for providing us with a copy of the final minutes for the December 15 meeting, which your firm modified in response to comments from Section 106 consulting parties.

As you may know, since the notice of the possible availability of the State Boulevard bridge over Spy Run Creek was prepared, the Indiana Department of Transportation has sent notification of the final lists of Select Bridges and Non-Select Bridges, pursuant to the Indiana Historic Bridges Programmatic Agreement. As your firm had anticipated, the State Boulevard bridge is on the list of Non-Select Bridges.

We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the area of potential effects ("APE") of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished by the buyout program and that some of the houses along Eastbrook Drive between State Boulevard and Clinton Street also have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard project, have they been flood-proofed, and what did that flood-proofing entail?

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled



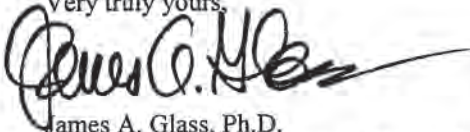
between those points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first blush like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak, weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion we are not sure. Moreover, it appears that a higher volume of traffic—more than 1,700 vehicles during that same hour—traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

The crash data also might yield a useful comparison. In 2008 the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be *realigned* appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned.. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have applied the data correctly. We would appreciate some assistance in that regard.

Another issue about which we are uncertain is whether it might be feasible to *reduce* the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program) that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or [ajohnson@dnr.IN.gov](mailto:ajohnson@dnr.IN.gov). In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Robert F. Tally, Jr., P.E., Indiana Division, Federal Highway Administration  
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation  
Scott Crites, American Structurepoint, Inc.  
Hayley Steele, American Structurepoint, Inc.  
Camille Fife, The Westerly Group, Inc.  
Amy Johnson, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources  
John Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources





AMERICAN  
**STRUCTUREPOINT**  
INC.

May 19, 2011

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer  
Division of Historic Preservation & Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project and included in two separate letters received by our office. The DHPA letters (dated January 27, 2010, and March 10, 2010) were provided following a December 15, 2009, consulting parties meeting. Copies of the DHPA correspondence are included with this letter for your reference. The Section 106 process has been delayed until recently while we have been further developing the project Purpose and Need Statement in cooperation with INDOT and FHWA. On April 11, 2011, FHWA approved the revised Purpose and Need Statement (attached), allowing the NEPA and Section 106 process to proceed.

**DHPA Letter to American Structurepoint, Inc. dated January 27, 2010**

**1) DHPA Comment - Page 1, Paragraph 2:**

The request we had made in our December 14, 2009, letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to consulting parties.

***Response:***

*The project Purpose and Need Statement has been significantly revised and was approved by FHWA and INDOT on April 11, 2011. The Purpose and Need Statement is included with this letter and will be provided to all consulting parties.*

**2) DHPA Comment – Page 1, Paragraph 3:**

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at





Dr. James A. Glass  
May 19, 2011  
Page 2

Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

**Response:**

*Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.*

**3) DHPA Comment – Page 1, Paragraph 4:**

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.

**Response:**

*The existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.*

*According to IDM Chapter 43, Figure 43-3B, the curve radius for a 30 mph roadway is required to be a minimum of 300 ft. Several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.*

*The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.*



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*For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.*

*The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing reverse curve found on State Boulevard between Clinton Street and Cass Street. Specifically, the intersections of Eastbrook Drive and Terrace Road will be replaced and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.*

4) **DHPA Comment – Page 1, Paragraph 4:**

It is our understanding that the section of State Boulevard in question is posted for a 20 mph speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mph limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.

**Response:**

*The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph, which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.*

*Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).*

*State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.*



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*As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.*

*As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.*

**5) DHPA Comment – Page 2, Paragraph 2:**

Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass Streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g. Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

**Response:**

*Pursuant to 36 CFR Section 800.4(a)(1), the APE was drawn to encompass properties on both side of the undertaking and/or with a view shed of it. Generally, properties directly adjacent to the undertaking mask the view from the properties not adjacent. The APE was expanded or contracted based on visibility and the possibility of impact of the undertaking on properties within its view shed. Any indirect or cumulative impacts outside of the APE and Section 106 will be evaluated during the NEPA process.*

**6) DHPA Comment – Page 2, Paragraph 3:**

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near the new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given



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to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

***Response:***

*Consideration will be given to this request at the appropriate time.*

**DHPA Letter to American Structurepoint, Inc., dated March 10, 2010**

**1) DHPA Comment – Page 1, Paragraph 4:**

We appreciate having the “Spy Run Flood Protection Study,” but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgemoor Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne’s flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?

***Response:***

*The flood control study prepared by Christopher B. Burke Engineering, Ltd., for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary’s River.*

*The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.*

*The study identifies necessary additional information regarding the flooding issues of the St. Mary’s river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, “This flooding is caused primarily by backwater from the St. Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”*



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*The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.*

*American Structurepoint is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.*

**2) DHPA Comment – Page 1, Paragraph 5:**

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation; however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006 to 2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 p.m. to 6:00 p.m. That sounds at first like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not, we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

***Response:***

*Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.*

*Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).*

*The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.*



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3) **DHPA Comment – Page 2, Paragraph 2:**

The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clint Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which state Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.

**Response:**

*Your interpretation of the traffic and crash data information mentioned in your letter is correct. It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.*

4) **DHPA Comment – Page 2, Paragraph 3:**

Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

**Response:**

*Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact*



Dr. James A. Glass  
May 19, 2011  
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*residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.*

It is our intent that the information provided in this letter, in combination with the attached FHWA-approved Purpose and Need Statement shall effectively address the comments and questions brought forth following the December 15, 2009, consulting parties meeting. We realize that a significant amount of time has passed since these comments were brought forth and we appreciate the patience of the DHPA and all consulting parties while we worked with INDOT and FHWA to establish a stronger Purpose and Need Statement. Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining to the project's Purpose and Need Statement, we respectfully request that you provide such comments within 30-days from receipt of this letter. Following this 30-day comment period, we will begin coordination to schedule a Consulting Parties meeting as the next step in continuing the Section 106 process. If I can be of assistance in the interim please do not hesitate to contact me at (317) 547-5580, or at [blackey@structurepoint.com](mailto:blackey@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

  
Brett W. Lackey  
Environmental Scientist

BWL:alo

Enclosures

cc: Karie Brudis  
Angie Quinn  
Michael Galbraith  
Don Orban  
Todd Zeiger  
Julie Donnell  
Michelle Briggs-Wedaman  
James L. Cooper  
Paul Brandeburg  
Shan Gunawardena  
Susan Haneline  
Charlie Shirmeyer  
Karl Dietsch  
Dan Avery  
Suzanne Slick  
Annette Dailey  
Joyce Newland  
John H. Shoaff  
Patrick Carpenter  
Camille Fife  
Creager Smith  
Albert Cohen  
Thomas M. Niezer



 AMERICAN STRUCTUREPOINT, INC.

Dr. James A. Glass

May 19, 2011

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Ronald Ross

Dan Ernst

John Carr

Amy Johnson

**Friends of the Parks of Allen County, Inc.  
PO Box 10152  
Fort Wayne, Indiana 46850-0152**

June 14, 2011

Brett Lackey  
Environmental Scientist  
American Structure Point  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903

Dear Mr. Lackey:

The Friends of the Parks appreciate being copied on your recent communication to Dr. James Glass dated May 19, 2011.

However, we are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.

Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communication with Doctor Glass, in a formal way under the auspices of whatever form the Section 106 Review continues.

Sincerely Yours,

Julie Donnell  
President  
Friends of the Parks of Allen County, Inc.

June 14, 2011

Cc: Dr. James Glass  
Michael Galbraith



Todd Zeiger

**Jill D. Downs  
1202 Elmwood Ave.  
Fort Wayne, IN 46805**

June 15, 2011

RECEIVED  
HIST. PRES. & ARCH.

JUN 17 2011

IDNR

Dr. James A. Glass  
Deputy State Historic Preservation Officer  
Division of Historic Preservation and Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, IN 46204

Dear Dr. Glass:

As chairperson of the Preservation Committee of ARCH, Inc., Allen County's historic preservation organization, I am writing in response to the letter dated May 19, 2011 from American Structurepoint to you regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

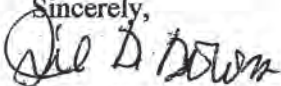
As a nearly lifelong resident of near northeast Fort Wayne, I travel this State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's recent poor attempt to justify its need.

In reading through the May 19 letter, American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than on moving traffic through the area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.

I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.

Thank you for your review of this issue.

Sincerely,



Jill D. Downs

cc: John L. Carr  
Team Leader, Historic Structures Review  
Division of Historic Preservation and Archaeology  
Indiana Department of Natural Resources



JOHN H. SHOAFF, A.I.A.  
ARCHITECT

4646 West Jefferson Boulevard  
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com  
Telephone: 260-459-0231

June 16, 2011

Brett Lackey  
American Structurepoint, Inc.  
7260 Shadeland Station,  
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I'm writing in response to your letter of May 19 to Dr. James Glass of the DHPA addressing questions and concerns posed by Dr. Glass.

First, let me note you said that a Purpose and Need Statement was attached, but I did not find one included with my copy.

In his letter Dr. Glass raises several important concerns, beginning with "it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion." Nor is it clear to me, even after reading your response.

For a three year period, when my mother, who lived on Westbrook Drive, was failing, I made the drive along that stretch almost every day, frequently at the rush hour. On the curve of State Street (as we have always called it), traffic always moved smoothly, but there was often (and always, at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.

Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.

Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the 'Arterial plus Bypass' concept" you refer to on Page 4. This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor", as you are already calling it, as if past errors justify future ones; you argue (bottom of Page 3) that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south - but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelthorn Road - which aligns with and neatly continues the east-west stretch of Coliseum.



Most seriously: while you repeatedly make the case for "continuity" for motorists who wish "to traverse the urban area" (bottom, Page 3), you totally ignore the welfare of the people who live within that area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values of the homes your corridor borders or traverses?

It is in the interest of the whole city to maintain the viability of all these neighborhoods, not only for the sake of the thousands of citizens who live there, but for all our citizens. By driving property values down, as your corridor inevitably will, total city property tax revenues will decline, and this decline will need to be made up elsewhere, or the city will be less able to properly maintain its infrastructure.

Furthermore, one of the prime advantages of living in Fort Wayne, the ability to live in serene, attractive, but affordable neighborhoods close to Downtown, will be diminished by the sum of all the neighborhoods compromised by your efforts to increase cross town traffic. You refer to "The redevelopment of the urban core area" (Page 3, 3<sup>rd</sup> paragraph from the bottom), something we all want; but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor. Housing stock will be lost or diminished in desirability; your project will end up merely making it a little easier drive across the city at the heavy price of making it less desirable to live in.

Your failure to consider any of these issues no doubt stems from the narrowness of the assignment you were given; but your own profession by now should understand the need to be sensitive to the context in which you design. ISTEA was passed in 1991, the Federal Highway Administration's handbook on *Flexibility in Highway Design* was published in 1995, and the AASHTO handbook on design flexibility (*A Guide for Achieving Flexibility in Highway Design*), which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert (Page 2, 3<sup>rd</sup> paragraph from the bottom) that standards appropriate for "highway design elements" apply to a road that abuts and serves a modest residential neighborhood!

I am aware that a majority of my colleagues on city council voted to fund your engineering work; but I think many of them thought they heard you, or the city representatives, say that several alternatives would be explored, after meaningful public input. What we finally saw was quite different: one plan, about which you were totally inflexible, with three trivial alternatives as to which neighborhood street would provide the access point to the newly straightened, raised, and lane-doubled thoroughfare. The choice of alternatives was equivalent to asking a condemned man which tree he wanted to be hanged by, and many of the citizens quite rightly felt patronized and insulted.

We on council now face an unpleasant two-fold task of fighting for a properly democratic, participatory process that was denied those citizens, and of fighting to preserve the quality of life and property values now threatened because of they were denied a proper process.

Times have changed, and the damage your plan in its present form will do is even greater than it would have been when it was conceived, back in the 1980s. We now have property tax caps: this means any deterioration of property values means a deterioration of tax revenue we cannot afford. Furthermore, we must enhance the quality of life in our neighborhoods if we are to successfully sell our community as a good place to live and work. The casualness, indifference, and shortsightedness we have shown in the past must be corrected now, if we are to compete



successfully with the progressive cities that have already changed their ways and avoided or even removed corridors that have undermined urban cores and neighborhoods.

I am aware that I address issues that go beyond those directly relevant to your field, Environmental Science (which should be raising issues I haven't even touched on), but it must say something about the attitudes of your principals that they did not see these issues and address them themselves.

Sincerely yours,  
John H. Shoaff,  
Fort Wayne City Councilman At-large

cc: Dr. James Glass  
Consulting parties to the State Boulevard Reconstruction  
Members of the Fort Wayne City Council

## Lackey, Brett

---

**From:** Carr, John [JCarr@dnr.IN.gov]  
**Sent:** Thursday, June 16, 2011 12:12 PM  
**To:** Lackey, Brett  
**Cc:** Hope, Briana; Tharp, Wade  
**Subject:** RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Brett,

Yes, I think 15 days from the consulting parties' receipt of the P&N document is a sufficient extension of time. When do you anticipate they'll receive it?

John L. Carr  
IDNR-DHPA  
Phone: (317) 233-1949  
Fax: (317) 232-0693  
E-mail: JCARR@DNR.IN.GOV

Take our survey at [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic) to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]  
Sent: Thursday, June 16, 2011 11:31 AM  
To: Carr, John  
Cc: Hope, Briana  
Subject: RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

John,

Thank you for the information. To clarify on your comment below regarding extending the comment period: do you feel that an extension of 15-days (from receipt of the P&N) would be appropriate or should be give another full 30 days? Any thoughts on the matter would be appreciated.

Thanks

Brett Lackey  
American Structurepoint, Inc.

-----Original Message-----

From: Carr, John [mailto:JCarr@dnr.IN.gov]  
Sent: Thursday, June 16, 2011 10:55 AM  
To: Lackey, Brett  
Cc: Slider, Chad; Glass, James  
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903  
Importance: High

Brett,



Page 8 of the 9-page, May 19 letter we received from you refers to the "the attached FHWA-approved Purpose and Need Statement," but the only document in the submission we have is the 9-page letter, itself. So, no, we had not received the P&N document until now.

BTW, I haven't yet had a chance to study that submission closely, but we had intended to comment on it within 30 days of having received it (i.e., no later than June 22). Since we just now received the P&N statement, and since the consulting parties apparently hadn't seen it yet, either, it might be appropriate to extend the comment period somewhat.

John L. Carr  
Team Leader for Historic Structures Review Division of Historic Preservation and Archaeology  
Indiana Department of Natural Resources  
402 W. Washington St., Room W274  
Indianapolis, IN 46204  
Ph. No.: 317-233-1949 Fax No.: 317-232-0693

Take our survey at [www.IN.gov/dnr/historic](http://www.IN.gov/dnr/historic) to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]  
Sent: Thursday, June 16, 2011 9:36 AM  
To: Carr, John  
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903  
Importance: High

John,

Could you let me know whether or not the DHPA received the revised Purpose and Need Statement (attached) which was to be included with the May 19, 2011 letter? We intended for this to be sent to all consulting parties...

Thanks

Brett W. Lackey  
Environmental Scientist  
BLackey@structurepoint.com  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256  
317.547.5580 | office  
317.543.0270 | fax  
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[www.structurepoint.com](http://www.structurepoint.com)

-----Original Message-----

From: Todd Zeiger [mailto:TZeiger@indianalandmarks.org]  
Sent: Thursday, June 16, 2011 9:02 AM  
To: Jim Glass; Lackey, Brett  
Cc: Michael Galbraith; John Carr  
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903  
Importance: High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision. Checking with other consulting parties they did not either. If it is the intention that the consulting parties were supposed to take the receipt of this carbon copy letter as our opportunity to re-engage in the 106 review process on this project (and I would add an unusual way to conduct a 106 review) then receipt of the revised purpose and need statement would be of utmost importance.

I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period. If it was in fact the intention of American Structurepoint that receipt of the copy of the May 19, 2011, letter to the Indiana Division of Historic Preservation and Archaeology serves as our opportunity to provide substantive comments, we request a 30 day extension - following the receipt of the revised purpose and need statement and replies to our other concerns and comments. If it was the intention that receipt of a copy of correspondence between the consulting engineer and the Indiana Division of Historic Preservation and Archaeology serves as consulting parties opportunity to comment we respectfully disagree with that process and request direct and professional engagement with all the parties involved. This is a significant project with the potential for profound change to historic neighborhoods and the utmost care needs to be taken to follow the process appropriately.

Thank you for your attention to this matter.

Todd Zeiger  
Director  
Northern Regional Office  
Indiana Landmarks

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<http://www.emaildisclaimers.com/>

## Lackey, Brett

---

**From:** Todd Zeiger [TZeiger@indianalandmarks.org]  
**Sent:** Thursday, June 16, 2011 9:02 AM  
**To:** Jim Glass; Lackey, Brett  
**Cc:** Michael Galbraith; John Carr  
**Subject:** State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

**Importance:** High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

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Thank you for your attention to this matter.

Todd Zeiger  
Director  
Northern Regional Office  
Indiana Landmarks





AMERICAN  
**STRUCTUREPOINT**  
INC.

June 17, 2011

James A. Glass, PhD  
Deputy State Historic Preservation Officer  
Division of Historic Preservation and Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903

Dear Dr. Glass:

On May 19, 2011, American Structurepoint, Inc., sent a letter to the Division of Historic Preservation and Archaeology regarding the State Boulevard project. It was recently brought to our attention that the revised project Purpose and Need Statement referenced in the letter was inadvertently omitted from that mailing. Please find the Purpose and Need Statement enclosed with this letter. We apologize for this oversight, and at this time we would like to extend the comment period for an additional 15 days from the receipt of this letter.

Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining, we respectfully request that you provide such comments within 15 days from receipt of this letter. If there are any questions or comments, please contact me at (317) 547-5580 or at [blackey@structurepoint.com](mailto:blackey@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

Brett W. Lackey  
Environmental Scientist

BWL:cgh

Enclosures

IN20071404



Dr. James A. Glass

June 17, 2011

Page 2

cc: Karie Brudis  
Angie Quinn  
Michael Galbraith  
Don Orban  
Todd Zeiger  
Julie Donnell  
Michelle Briggs-Wedaman  
James L. Cooper  
Paul Brandeburg  
Shan Gunawardena  
Susan Haneline  
Charlie Shirmeyer  
Karl Dietsch  
Dan Avery  
Suzanne Slick  
Annette Dailey  
Joyce Newland  
John H. Shoaff  
Patrick Carpenter  
Camille Fife  
Creager Smith  
Albert Cohen  
Thomas M. Niezer  
Ronald Ross  
Dan Ernst  
John Carr  
Amy Johnson



## State Boulevard Project DES#0400587

The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

#### **State Street and Spy Run Avenue Intersection**

<b>Morning Peak</b>	<b>LOS Existing</b>
East Bound Left	<b>F</b>
West Bound Through	<b>E</b>

<b>Evening Peak</b>	<b>LOS Existing</b>
East Bound Left	<b>F</b>
East Bound Through	<b>E</b>
West Bound Through	<b>E</b>



### State Street and Clinton Street Intersection

<b>Morning Peak</b>	<b>LOS Existing</b>
South Bound Through	<b>E</b>

<b>Evening Peak</b>	<b>LOS Existing</b>
East Bound Through	<b>E</b>
West Bound Left	<b>F</b>

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element. According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.

Curve Radius Table:

<b>Station Line "A"</b>	<b>Existing Curve Radius</b>	<b>Req'd Radius (30 mph)</b>
18+66.60	175 ft	300 ft
24+64.47	243 ft	300 ft
27+23.73	210 ft	300 ft

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway's safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a critical Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 ft along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 ft along State Boulevard to safely make the right turn maneuver. As noted in the "Intersection Sight Distance Table" below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard. and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11
State Boulevard. and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38
State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16



The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

## Lackey, Brett

---

**From:** Michael Galbraith [mgalbraith@archfw.org]  
**Sent:** Friday, June 17, 2011 10:46 AM  
**To:** Jim Glass; Lackey, Brett  
**Cc:** Michael Galbraith; Todd Zeiger; jcarr@dnr.in.gov; Jill Downs; John Shoaff; Julie Donnell; Michelle Briggs-Wedaman  
**Subject:** State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Dear Dr. Glass and Mr. Lackey-

I am writing today concerning the May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587; DHPA No. 5903) in Fort Wayne, Allen County, Indiana. I have a number of concerns and questions.

1. I am unsure how this letter fits into the Section 106 process and the ability of Consulting Parties and the public to provide meaningful comment and criticism. Although I contacted Mr. Lackey by phone asking whether comment would be accepted from the Consulting Parties in regard to this letter, my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as mere footnote. Our Dec. 8, 2009, comments regarding the HPR and our comments from the initial Consulting Parties (Dec. 15, 2009) remain unaddressed. I request that our concerns and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed, and that we be given time in which to respond to those answers.
2. It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project Purpose and Need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in Purpose and Need.

Michael Galbraith  
Preservation Specialist, ARCH, Inc.  
818 Lafayette Street, Fort Wayne, IN 46802  
[mgalbraith@archfw.org](mailto:mgalbraith@archfw.org)  
260.4265117



## Lackey, Brett

---

**From:** Julie Donnell [juliemarie57@earthlink.net]  
**Sent:** Friday, June 17, 2011 11:15 AM  
**To:** Lackey, Brett; James Glass  
**Cc:** Mike Galbraith; Todd Zeiger; Jill Downs; Michelle Briggs Wedaman  
**Subject:** State Boulevard Reconstruction Project (Des. No. 0400587; Project No: IN20071404  
**Attachments:** Structurepoint response 6.13.11.doc

Dear Dr. Glass and Mr. Lackey;

Attached is a letter which outlines the Friends of the Parks' concerns about the the recent communication between DHPA and Structurepoint, which I recieved a copy of earlier in the month.

In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.

We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.

My formal letter is attached.

Sincerely,  
Julie Donnell  
President, Friends of the Parks of Allen County, Inc.

Julie Donnell  
[juliemarie57@earthlink.net](mailto:juliemarie57@earthlink.net)  
EarthLink Revolves Around You.

JOHN H. SHOAFF, A.I.A.  
ARCHITECT

4646 West Jefferson Boulevard  
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com  
telephone: 260-459-0221

July 1, 2011

Brett Lackey  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I am now in receipt of the Purpose and Need (P and N) statement which you intended but failed to include in your letter of May 19, 2011 to Dr. James Glass. It presents a problem: there are there material discrepancies between the accident counts in your new P and N statement and those sent by Hayley Steele of your firm to the Historic Preservation Specialist (now executive director) of ARCH on February 4, 2010 and also sent by the City of Fort Wayne to the Brookview Neighborhood Association president. Discrepancies occur in every one of the accident totals for the intersections of State Boulevard at Spy Run, Clinton Street, Eastbrook Dr., and Westbrook Dr. in the years 2007 and 2008.

In all but 2 of the 8 instances, your figures, relative to the Feb. 4, 2010 and City accident reports, raise the RMV (Rate per Million entering Vehicles) from 2.0 or below (typically well below) - the threshold above which there is a safety problem - to a number above 2.0. The tables you sent in February 2010 and the table that is part of the new P and N statement are included. A summary of the discrepancies:

	<u>Feb. 2010/City Reports</u>	<u>New P &amp; N Figures</u>
State and Westbrook: 2007	7 accidents;	16 accidents
2008	11	17
State and Eastbrook; 2007	3	17
2008	6	17
State and Clinton: 2007	28	41
2008	33	49
State and Spy Run; 2007	36	34
2008	33	35

Feb. 4, 2010/

Your discrepancies are remarkably consistent in providing support for the argument that you are clearly so eager to make but that the 'city figures will not support, namely that, based on RMVs, there are serious safety problems at the four intersections. Only at the Spy Run intersection do the RMVs remain similar. Calculating from your data, the RMVs, when corrected for the Feb.4, 2010/City figures, work out to (in the order listed above, 2007 listed before 2008): State and Westbrook, 1.3, 1.5.; State and Eastbrook, .43, .92; State and Clinton, 1.9, 2.2; State at Spy Run, 2.16, 2.0.

Two points are noteworthy: the RMVs generated by your figures are much higher than the city's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning of and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.



With respect to the Spy Run and Clinton intersections, it strains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part; namely, to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.

The discrepancies in the accident figures raise questions that are both obvious and serious. I await your response.

Sincerely yours,

  
John H. Shoaff,  
Fort Wayne City Councilman At-large

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard. and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11
State Boulevard. and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38
State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16



Intersection of State Blvd. & Cass St.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1	1		3				1	
Rear End	2			1			2			2		
Left-Turn	1									1		
Right-Turn							2					
Side-Swipe										1	1	
Head On												
Off Road				1								
Pedestrian/Bicycle												
Total	4	0	0	3	1	0	7	0	0	4	2	0
Total per Year	4			4			7			6		

Crash Rate (Crashes per MEV)	0.74
Number of years	4
Entering Vehicles	19,500
Total Accidents	21

Intersection of State Blvd. & Westbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1			3			1		
Rear End	1			2	1		2	1		1	1	
Left-Turn												
Right-Turn												
Side-Swipe	2						1			2		
Head On		1								4	2	
Off Road	3			1			2					
Pedestrian/Bicycle												
Total	7	1	0	4	1	0	8	1	0	8	3	0
Total per Year	8			5			9			11		

Crash Rate (Crashes per MEV)	1.25
Number of years	4
Entering Vehicles	18,070
Total Accidents	33

Intersection of State Blvd. & Eastbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	2			2						1	1	
Rear End							1			1		
Left-Turn										1		
Right-Turn												
Side-Swipe												
Head On	1							1				
Off Road	1						1			3		
Pedestrian/Bicycle												
Total	4	0	0	2	0	0	2	1	0	5	1	0
Total per Year	4			2			3			6		

Crash Rate (Crashes per MEV) 0.36
Number of years 4
Entering Vehicles 17,350
Total Accidents 9

Intersection of State Blvd. & Oakridge Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End	2			3			2					
Left-Turn												
Right-Turn												
Side-Swipe				1								
Head On												
Off Road												
Pedestrian/Bicycle												
Total	2	0	0	3	1	0	2	0	0	0	0	0
Total per Year	2			4			2			0		

Crash Rate (Crashes per MEV) 0.32
Number of years 4
Entering Vehicles 17,250
Total Accidents 8



Intersection of State Blvd. & Terrace Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End										1		
Left-Turn												
Right-Turn												
Side-Swipe												
Head On	1											
Off Road												
Pedestrian/Bicycle												
Total	1	0	0	0	0	0	0	0	0	1	0	0
Total per Year	1			0			0			1		

Crash Rate (Crashes per MEV)	0.08
Number of years	4
Entering Vehicles	17,250
Total Accidents	2

Intersection of State Blvd. & Clinton St. (SB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	4	3		8	1		7	1		3	3	
Rear End	15	2		10	1		10	1		13	2	
Left-Turn	3			2	1		2			2	1	
Right-Turn	1									1		
Side-Swipe	5			2			3	2		6		
Head On	1							1				
Off Road							1			2		
Pedestrian/Bicycle												
Total	29	5	0	22	3	0	23	5	0	27	6	0
Total per Year	34			25			28			33		

Crash Rate (Crashes per MEV)	2.00
Number of years	4
Entering Vehicles	41,000
Total Accidents	120

Intersection of State Blvd. & Spy Run (NB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	3	2		3			3			1	4	
Rear End	13			13	1		17	2		15	3	
Left-Turn	3	2		2	2		6	1		2		
Right-Turn				2								
Side-Swipe	10	1		6			6			6		
Head On										1		
Off Road	1						0	1		1		
Pedestrian/Bicycle		1		1	2							
Total	30	6	0	27	5	0	32	4	0	25	8	0
Total per Year	36			32			36			33		

Crash Rate (Crashes per MEV)	1.91
Number of years	4
Entering Vehicles	49,250
Total Accidents	137

West 200 block of State Blvd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PI	F	
Right Angle												
Rear End	1						1					
Left-Turn												
Right-Turn												
Side-Swipe							2					
Head On										1		
Off Road	2	1					1					
Pedestrian/Bicycle												
Total	3	1	0	0	0	0	4	0	0	0	1	0
Total per Year	4			0			4			1		

Crash Rate (Crashes per 100 Million VM)	3.14.33
Length of Segment	0.12
Number of years	4
ADT	16,500
Total Accidents	9



Division of Historic Preservation & Archaeology 402 W. Washington Street, W274-Indianapolis, IN 46204-2739  
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



July 5, 2011

Brett W. Lackey  
Environmental Specialist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Response to SHPO's January 27, 2010, and March 10, 2010, letters and revised purpose and need statement concerning the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated May 19, 2011 and received on May 23 and the revised purpose and need statement submitted with your cover letter dated June 17, 2011 and received on June 20, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Thank you for responding the questions we had raised in our earlier letters. We continue to think, however, that it might be appropriate to expand the Section 106 area of potential effects ("APE"), if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. You indicated on page 4 of your May 19 letter that the proposed APE was based on view sheds of State Boulevard from nearby properties and that indirect impacts outside the APE will be evaluated during the NEPA process. We believe that any reasonably foreseeable direct or indirect effect should be taken into consideration in determining the APE (36 C.F.R. § 800.16[d]). Certainly, view sheds of areas where demolition or construction will take place are appropriate considerations (see 36 C.F.R. § 800.5[a][2][v]), but we think that areas where the character or use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well (see 36 C.F.R. § 800.5[a][2][iv]).

A case in point may be found in one of the rationales given in the revised purpose and need statement in support of the need for this project: "As the Transportation Plan [for the Fort Wayne Urbanized Area] has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor" (page 4). Improvements listed there include widening the St. Joseph River bridge east of Spy Run Avenue, and widening State Boulevard between Spy Run and Cass Street, between Goshen Avenue and Cass, and between Coliseum Boulevard and Maplecrest Road. In retrospect, it seems to us that, given those improvements to the west and east of the project area under review here—especially those closest to this project area—it might have been foreseeable that there eventually would be a perceived need to improve the two-lane stretch of State Boulevard between Cass Street and Spy Run. In a similar way, it seems to us that it now may be foreseeable that traffic patterns on side streets to the north and south of State Boulevard would change and that those changes could affect historic properties. Consequently, we again request that consideration be given to expanding the Section 106 APE, even if such changes in traffic patterns also would be evaluated in the NEPA process.

We have found no basis to challenge the conclusions of the purpose and need statement. We assume that the facts stated there are accurate, and we have no knowledge of any other information that would contradict the conclusions drawn on them. Because you have indicated that the purpose and need statement has been developed with FHWA and Indiana



Department of Transportation involvement and has received FHWA's approval, we assume, therefore, that those agencies believe that the revised purpose and need statement is persuasive. We have been copied on at least some of the consulting parties' responses to your May 19 letter, and they have identified a number of historic preservation, economic, pedestrian safety, and neighborhood livability values that they believe should be promoted. The revised purpose and need statement identifies the transportation mobility, connectivity, and accessibility and traffic safety values that the City of Fort Wayne wishes to promote. The Section 106 process is not designed to choose among those values; rather, it is intended to ensure that a reasonable opportunity is provided for consulting parties and the public to express their views on effects on historic properties and that the federal agency responsible for the project takes those effects into consideration and gives the Advisory Council on Historic Preservation an reasonable opportunity to comment.

The revised purpose and need statement indicates that two other, possible east-west corridors also were considered, but the State Boulevard Corridor was considered the most practical. It goes on to indicate that these three alternatives will be discussed in the NEPA process. We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevant to the Section 106 process, because 36 C.F.R. § 800.6(a) requires that there be consultation about alternatives that might avoid, minimize, or mitigate adverse effects on historic properties. The alternatives presented to the consulting parties so far appear to be minor variations on the State Boulevard Corridor alternative. It is unclear to us how the alternatives analysis for NEPA and Section 106 purposes for this project as a whole would dovetail with Stipulation III.B. of the Historic Bridges PA, which would seem to apply specifically, and perhaps exclusively, to the Non-Select State Boulevard Bridge over Spy Run. Perhaps FHWA can clarify how the alternatives analysis should be handled.

Some of the consulting parties have expressed concerns that although you have responded to the Indiana SHPO's previous questions, not all of theirs have been addressed. In light of these comments, you may want to review previous correspondence from the consulting parties and the minutes of the December 15, 2009 consulting parties meeting and make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 19 letter. Sometimes in Section 106 consultations, there is a tendency to give greater weight to the Indiana SHPO's concerns than to those submitted by other consulting parties. While the 36 C.F.R. Part 800 regulations assign a somewhat different role to the SHPO than to the other consulting parties, the regulations also provide for the federal agency to consider issues and concerns raised by other consulting parties (see 36 C.F.R. §§ 800.5[c][2][i] and 800.6[a]).

It occurs to us that not all of the consulting parties may be aware of comments being offered by other consulting parties. We are aware that the usual practice is to provide copies of consulting party comments in the documentation that is provided with the finding. However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in response to your May 19 and June 17 letters.

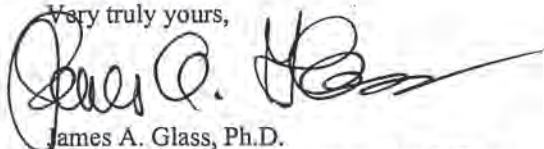
As previously stated in our December 14, 2009, letter to Hayley M. Steele, Environmental Specialist at American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link [http://www.in.gov/dnr/historic/bin/qp/qp\\_archeo.pdf](http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf)).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.



A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at [www.achp.gov](http://www.achp.gov) for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). In all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Staffan Peterson, Ph.D., Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Patrick Carpenter, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Brett W. Lackey, American Structurepoint, Inc.

## Lackey, Brett

---

**From:** Lackey, Brett  
**Sent:** Wednesday, July 06, 2011 3:17 PM  
**To:** 'aquinn@archfw.org'; 'mgalbraith@archfw.org'; 'tmn@barrettllaw.com'; 'mbwedaman@verizon.net'; 'shan.gunawardena@ci.ft-wayne.in.us'; 'Creager Smith'; 'joyce.newland@dot.gov'; 'jshoaff@proparkwest.com'; 'don.orban@cityoffortwayne.org'; 'juliemarie57@earthlink.net'; 'jandailey59@msn.com'; 'tzeiger@indianalandmarks.org'; 'indianabridges@sbcglobal.net'; 'Carpenter, Patrick A'; Kennedy, Mary; 'sjslick@mac.com'; 'ross@martin-riley.com'; 'danavery@co.allen.in.us'; 'albertcohan@aol.com'; 'jcooper@ccrtc.com'; 'dan@earthsouceinc.net'; 'jcarr@dnr.in.gov'; 'wtharpe1@dnr.in.gov'  
**Cc:** Hope, Briana  
**Subject:** State Boulevard - Section 106 Comments  
**Attachments:** IN20071404.EV.2011-04-21.LTR.Response to SHPO Letters.bwl.pdf; State Blvd P&N Statement to CPs.pdf

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who still intend to comment, please provide comments by next Tuesday July 12<sup>th</sup>. Please let me know if there are any questions.

Thanks!

Brett W. Lackey  
Environmental Scientist  
[BLackey@structurepoint.com](mailto:BLackey@structurepoint.com)  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256  
317.547.5580 | office  
317.543.0270 | fax  
317.850.0257 | cell  
[www.structurepoint.com](http://www.structurepoint.com)



**From:** Suzanne [sjslick@me.com]  
**Sent:** Wednesday, July 06, 2011 4:18 PM  
**To:** Lackey, Brett; mayor@ci.ft-wayne.in.us  
**Cc:** aquinn@archfw.org; mgalbraith@archfw.org; tmn@barrettlaw.com; mbwedaman@verizon.net; shan.gunawardena@ci.ft-wayne.in.us; Creager Smith; joyce.newland@dot.gov; jshoaff@proparkwest.com; don.orban@cityoffortwayne.org; juliemarie57@earthlink.net; jandailey59@msn.com; tzeiger@indianalandmarks.org; indianabridges@sbcglobal.net; Carpenter, Patrick A; Kennedy, Mary; rross@martin-riley.com; danavery@co.allen.in.us; albertcohan@aol.com; jcooper@ccrtc.com; dan@earthsouceinc.net; jcarr@dnr.in.gov; wtharpe1@dnr.in.gov; Hope, Briana  
**Subject:** Re: State Boulevard - Section 106 Comments

Dear Brett,

I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State Boulevard correspondence. It seems we are missing a wonderful opportunity to create a beautiful and useful roadway system in this project.

What we will have is a massive concrete thoroughfare that will be unfriendly to pedestrians and bicyclists and probably to drivers, as well. There is no attempt at traffic calming, but a great emphasis on traffic rushing. There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability. Other cities are making great strides in building roadways that are user friendly, and safe for everyone -- not just car and truck drivers -- and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.

Very sincerely,  
 Suzanne Slick  
 Irvington Park Consulting Party  
 Fort Wayne

On Jul 6, 2011, at 3:16 PM, Lackey, Brett wrote:

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who

still intend to comment, please provide comments by next Tuesday July 12<sup>th</sup>. Please let me know if there are any questions.

Thanks!

Brett W. Lackey  
Environmental Scientist  
[BLackey@structurepoint.com](mailto:BLackey@structurepoint.com)  
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7260 Shadeland Station  
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317.547.5580 | office  
317.543.0270 | fax  
317.850.0257 | cell  
[www.structurepoint.com](http://www.structurepoint.com)

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## Lackey, Brett

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**From:** Michelle Briggs Wedaman [mbwedaman@frontier.com]  
**Sent:** Thursday, July 07, 2011 9:05 AM  
**To:** Lackey, Brett  
**Subject:** State Blvd Project Ft Wayne contact info update Brookview Neighborhood  
**Importance:** High

7/7/2011

Hi Brett,  
Your 7/6/11 email was forwarded to me by another consulting party member.  
Could you update my email from the Verizon to:  
[mbwedaman@frontier.com](mailto:mbwedaman@frontier.com)

I plan on submitting comment on behalf of the neighborhood.

Thanks,  
Michelle Briggs Wedaman  
Brookview Neighborhood president, Ft Wayne  
260.710.4413



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**State Boulevard Section 106 Agency Coordination Meeting  
MEETING MINUTES**

**Location:** INDOT Central Office Room N642  
**Date:** 7/13/2011  
**Project Name:** State Boulevard Reconstruction  
**Project No.:** IN20071404  
**Owner:** City of Fort Wayne  
**Attendees:** John Carr – IDNR DHPA  
Scott Crites – Structurepoint  
Jim Glass – IDNR DHPA  
Briana Hope – Structurepoint  
Jason Kaiser – INDOT Fort Wayne District  
Mary Kennedy – INDOT Cultural Resources Office  
Brett Lackey – Structurepoint  
Ben Lawrence – INDOT Office of Environmental Services  
Joyce Newland – FHWA  
Amanda Ricketts – IDNR DHPA  
Greg Smith – INDOT Fort Wayne District  
Wade Tharp – IDNR DHPA  
**Minutes By:** Brett Lackey

The following notes reflect our understanding of the discussions and decisions made at this meeting.  
If you have any questions, additions, or comments, please contact the issuer of these minutes.

**ITEMS DISCUSSED:**

- ❖ The meeting opened with introductions and an overall project update by American Structurepoint. Structurepoint also explained that the purpose of the agency coordination meeting was to discuss the following items:
  - SHPO's comments on recent Purpose and Need submission
  - How to address Consulting Parties comments
  - Section 4(f)
  - Inviting ACHP involvement
  
- ❖ As requested in the July 5, 2011 DHPA letter the re-evaluation of extending the APE to the north to accommodate the potential for added traffic through that neighborhood was discussed.
  - American Structurepoint explained that the project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. American Structurepoint also stated that it is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project.
  - DHPA requested that this be stated in a formal response to the July 5, 2011 letter and copied to all consulting parties.



- ❖ DHPA also asked about consulting parties request to extend the APE east and west.
  - American Structurepoint explained that while some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE.
  - INDOT Fort Wayne District added that the travel patterns in the Fort Wayne area are well established and that it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.
- ❖ DHPA asked about the other alternatives discussed in the revised Purpose and Need.
  - American Structurepoint explained that two additional corridors (Butler Rd-Vance Rd and Spring St-Tennessee Ave) were considered and discarded due to the need for new roadway alignments, relocations, historical impacts, park impacts, and potential hazardous waste impacts.
  - All agencies agreed that more details were needed for the other two east-west corridors studied.
  - Strong discussion of alternatives will be included in both the NEPA document and 4(f) document. American Structurepoint pointed out that consulting parties will have the opportunity to review the alternatives analysis as part of the 4(f) process. American Structurepoint will discuss the alternative analysis in more detail as part of the next consulting parties meeting.
- ❖ DHPA suggested that American Structurepoint coordinate with their National Register experts to determine if the project would result in a need to change the district boundaries.
- ❖ DHPA asked if all consulting party comments had been addressed.
  - American Structurepoint commented that the significant comments related to the Section 106 Process were addressed in the May 19, 2011 DHPA response letter.
  - DHPA suggested that American Structurepoint more specifically address the consulting parties issues and comments in coordination specifically addressed to the consulting parties.
  - Structurepoint suggested creating a spreadsheet identifying each consulting party and their specific comment with a response to the comment. Structurepoint committed to sending this document to consulting parties with the invitation to the next consulting parties meeting.
- ❖ American Structurepoint suggested FHWA invite the Advisory Council on Historic Preservation (ACHP) to participate at this time since the project seems to be controversial.
  - All agencies agreed ACHP should be invited to participate in the State Boulevard project now, rather than later in the Section 106 process.

#### **ACTION ITEMS:**

- ❖ Structurepoint will respond to the idea of extending the APE in writing via letter to DHPA.
- ❖ Structurepoint will formally request, via letter, FHWA coordinate with ACHP and request their involvement on the project at this time.
- ❖ Structurepoint will produce a chart with questions/answers that addresses all consulting party and agency comments received to-date – this will be provided to all consulting parties prior to next consulting party meeting.

**NEXT MEETING:**

Consulting Party Meeting (Date TBD)

cc: Attendees  
Consulting Parties

Very truly yours,  
American Structurepoint, Inc.

A handwritten signature in black ink, appearing to read 'B. Lackey', with a stylized flourish extending to the right.

Brett Lackey  
Environmental Scientist





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## M E M O R A N D U M

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**DATE:** August 15, 2011

**TO:** Ms. Angie Quinn, ARCH Inc.  
Ms. Jill Downs, ARCH Inc.  
Mr. Michael Galbraith, ARCH Inc.  
Mr. Don Orban, Fort Wayne Historic Preservation Commission  
Mr. Todd Zeiger, Indiana Landmarks  
Ms. Julie Donnell, Friends of the Parks of Allen County  
Ms. Michelle Briggs-Wedaman, Brookview Civic Neighborhood Association  
Dr. James Cooper  
Mr. Paul Bandenburg, Indiana Historic Spans Task Force  
Mr. Shan Gunawardena, City of Fort Wayne  
Ms. Susan Haneline, Brookview Civic Neighborhood Association  
Mr. Dan Avery, Northeastern Indiana Regional Coordination Council  
Ms. Suzanne Slick, Irvington Park Neighborhood Association  
Ms. Jan Dailey, Brookview Civic Neighborhood Association  
Ms. Joyce Newland, Federal Highway Association  
Mr. John Shoaff, Fort Wayne City Council  
Mr. Jason Kaiser, INDOT Fort Wayne District  
Mr. Patrick Carpenter, INDOT Cultural Resources  
Ms. Mary Kennedy, INDOT Cultural Resources  
Ms. Camille Fife, Westerly Group  
Mr. Creager Smith, Fort Wayne Office of Planning and Policy  
Mr. Albert Cohan, Westbrook 5, LLC  
Mr. Thomas Niezer, Barrett & McNagney, LLP  
Mr. Ronald Ross, Martin Riley Architects and Engineers  
Mr. Dan Ernst, Earth Source Inc.  
Dr. James Glass, IDNR DHPA  
Mr. John Carr, IDNR DHPA  
Ms. Amy Johnson, IDNR DHPA  
Ms. Amanda Rickets, IDNR DHPA  
Mr. Wade Tharp, IDNR DHPA  
Mr. Tom Cain, Fort Wayne Redevelopment

**FROM:** Brett W. Lackey, American Structurepoint, Inc.

**RE:** State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
Structurepoint No. IN20071404

**CC:** Scott Crites, American Structurepoint, Inc.

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This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for Thursday, September 1 at 9:30 am. The meeting will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030 located in the Garden Level of Citizens Square.

Please review the enclosed materials prior to the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at [blackey@structurepoint.com](mailto:blackey@structurepoint.com). If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Agenda  
Agency Coordination Meeting Minutes (7/13/2011)  
Letter to IDNR DHPA  
Letter to FHWA  
Historic Bridge Programmatic Agreement Alternatives Analysis  
Individual Section 4(f) Alternatives Analysis  
Corridor Alternatives Map  
Consulting Party Questions/Comments and Responses



**AGENDA**  
**Consulting Parties Meeting**  
**State Boulevard Reconstruction (Des. No. 0400587)**  
**City of Fort Wayne, Allen County, Indiana**

Thursday, September 1, 2011  
9:30 AM  
Room 030 (Garden Level)  
Citizens Square  
200 East Berry Street  
Fort Wayne, IN 46802

1. Project Update
  - a. Purpose and Need
  - b. Consulting Party Comments/Responses
2. Project Alternatives Review
  - a. Minimization Measures
3. Future Steps in the Process
  - a. Potential Mitigation Measures
  - b. Development of Memorandum of Agreement
4. Follow-up items



AMERICAN  
**STRUCTUREPOINT**  
INC.

August 15, 2011

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer  
Division of Historic Preservation & Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN200701404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated July 5, 2011.

The letter states that the DHPA believes that it may be appropriate to expand the APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. The proposed project will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access, but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut-through traffic. The project would likely draw current cut-through traffic out of the neighborhoods, because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established, and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor. The traffic pattern alteration is not anticipated to result in any adverse impacts.

Your letter also indicates that a discussion of the other two possible east-west corridor alternatives mentioned in the project's purpose and need statement would be relevant to the Section 106 process. Alternatives have continued to be developed throughout the Section 106 process and an alternatives analysis will be provided to all consulting parties prior to the next (second) consulting parties meeting. Please see the following descriptions of the two possible east-west corridor alternatives:





**Alternative 1: Butler Road – Vance Road Corridor**

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard. This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land. This alternative avoids impacts to historic properties identified within the APE of the project; however, the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District, as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

**Alternative 2: Spring Street – Tennessee Avenue**

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue Bridge over the St. Joseph River, a select historic bridge which has been determined eligible for the National Register of Historic Places.



Dr. James A. Glass  
August 15, 2011  
Page 3

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park. This alternative avoids impacts to historic properties identified within the APE of this project; however, the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Your July 5, 2011, letter also expresses concern that not all consulting parties feel that their comments have been adequately addressed. The letter also indicates that not all consulting parties are aware of comments being provided by other consulting parties. In an effort to both address individual comments and distribute comments amongst consulting parties, we have created a table with all comments received thus far, as well as individual responses to all comments. This document shall be distributed to all consulting parties for review prior to the next consulting parties meeting.

Concern regarding how the project may affect the proposed boundaries of the Brookview – Irvington Park National Register District has also been expressed. As such, at this time we respectfully request the DHPA's assistance in soliciting the opinion of the appropriate National Register authority regarding the potential for the project to impact the currently proposed boundaries of the Brookview – Irvington Park National Register District.

We hope that the information in this letter adequately addresses the concerns expressed in the July 5, 2011, DHPA letter. The information herein will be provided to all consulting parties prior to the next consulting parties meeting and will also be discussed during that meeting. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at [blackey@structurepoint.com](mailto:blackey@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

Brett W. Lackey  
Environmental Scientist

BWL:alo

Enclosures

cc: John Carr  
Wade Tharp  
Amanda Ricketts  
Mary Kennedy  
Patrick Carpenter



Dr. James A. Glass

August 15, 2011

Page 4

Staffan Peterson, Ph.D.  
Jason Kaiser  
Greg Smith  
Joyce Newland  
Dan Avery  
Creager Smith  
Shan Gunawardena  
Camille Fife  
Angie Quinn  
Michael Galbraith  
Jill Downs  
Todd Zeiger  
Don Orban  
Julie Donnell  
Michelle Briggs-Wedaman  
James L. Cooper  
Paul Brandeburg  
Susan Haneline  
Charlie Shirmeyer  
Karl Dietsch  
Suzanne Slick  
Annette Dailey  
John Shoaff  
Albert Cohen  
Thomas M. Niezer  
Ronald Ross  
Dan Ernst



AMERICAN  
**STRUCTUREPOINT**  
INC.

August 15, 2011

Ms. Joyce Newland  
Planning/Environmental Specialist  
Indiana Division - Federal Highway Administration  
575 North Pennsylvania Street, Room 254  
Indianapolis, Indiana 46204

Re: Request for ACHP Involvement  
State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Ms. Newland:

The purpose of this letter is to initiate the coordination necessary for involvement of the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana. As you know, ACHP involvement in the Section 106 process of an individual project must be solicited by the project's lead Federal agency. Therefore, at this time we respectfully ask that the Federal Highway Administration (FHWA) requests the involvement of the ACHP in this project.

We believe that ACHP involvement is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. Specifically, the State Boulevard Project meets the criteria of **Part (c)(1)** for “...adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district” and **Part (c)(3)** for “...cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...”

If any additional information or supplemental documentation is needed in order for FHWA to request ACHP involvement, please do not hesitate to contact me at (317) 547-5580, or at [blackey@structurepoint.com](mailto:blackey@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

Brett W. Lackey  
Environmental Scientist

BWL:alo





Ms. Joyce Newland

August 15, 2011

Page 2

cc: James Glass, Ph.D  
John Carr  
Wade Tharp  
Amanda Ricketts  
Mary Kennedy  
Patrick Carpenter  
Staffan Peterson, Ph.D.  
Jason Kaiser  
Greg Smith  
Joyce Newland  
Dan Avery  
Creager Smith  
Shan Gunawardena  
Camille Fife  
Angie Quinn  
Michael Galbraith  
Jill Downs  
Todd Zeiger  
Don Orban  
Julie Donnell  
Michelle Briggs-Wedaman  
James L. Cooper  
Paul Brandeburg  
Susan Haneline  
Charlie Shirmeyer  
Karl Dietsch  
Suzanne Slick  
Annette Dailey  
John Shoaff  
Albert Cohen  
Thomas M. Niezer  
Ronald Ross  
Dan Ernst

**Alternatives Analysis – Historic Bridge PA**  
**State Boulevard Bridge No. 546 over Spy Run Creek**  
**State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)**

1. Rehabilitation for Continued Vehicular Use (two-way option)

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The



State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

2. Rehabilitation for Continued Vehicular Use (one-way option)

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

3. Bypass (Non-vehicular use)

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The



State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

4. Replacement

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

5. Relocate and Replacement

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use. Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for vehicular use. While this alternative does meet the project’s purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

**Alternatives Analysis – Individual Section 4(f)**  
**State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)**

**Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)**

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.



**Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)**

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

### **Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)**

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly



Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

### **Alternative 3B: Widen State Boulevard on Existing Alignment**

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 16 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

### **Alternative 3C: Shift State Boulevard Alignment South**

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 14 residential relocation from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

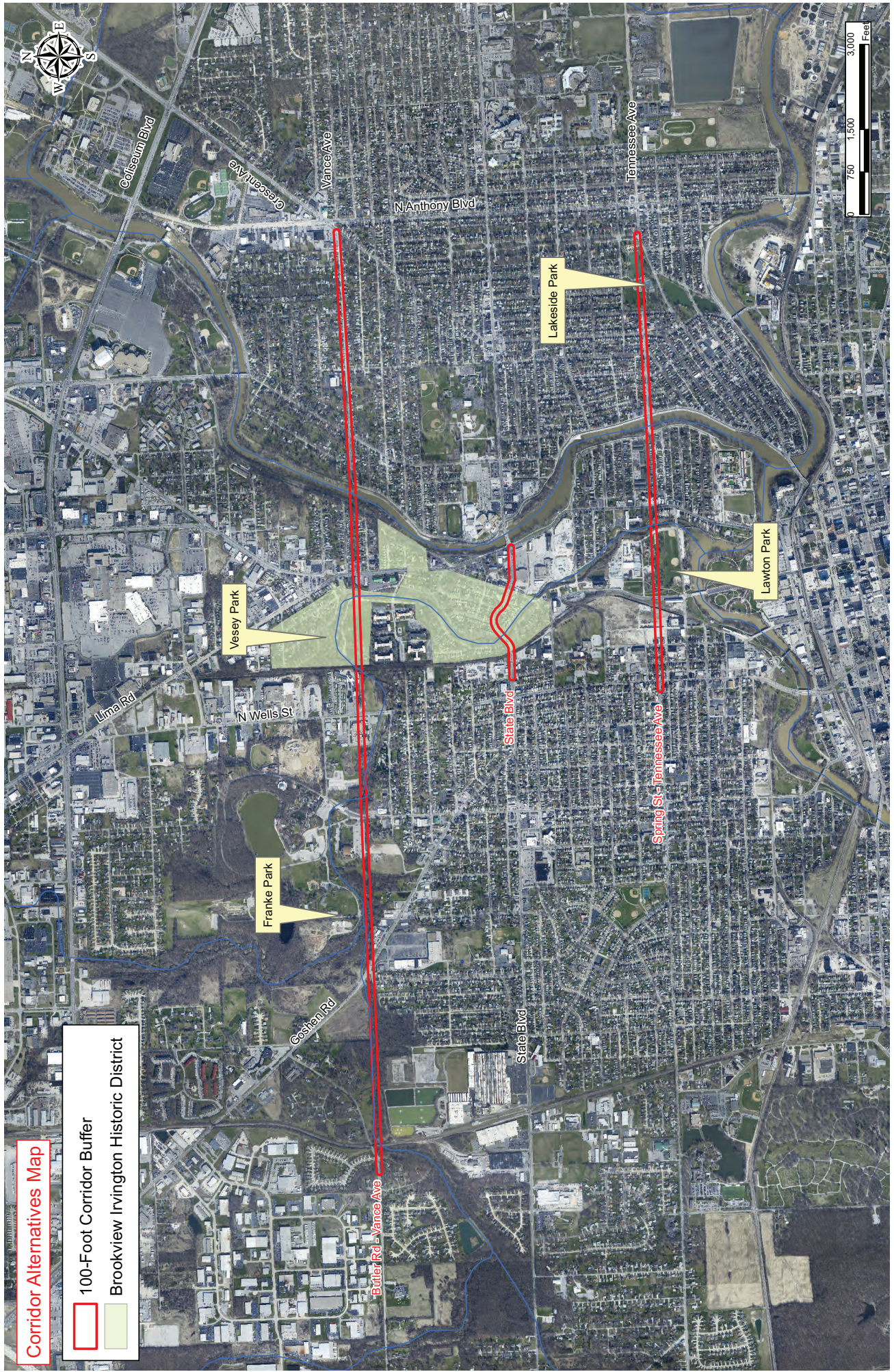
### **Alternative 4: No Build**

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage.

This alternative would avoid impacts to historic properties.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.







Comment No.	Name / Organization / Comment Date	Comment	Response
1	Jan Dailey – Brookview Neighborhood Resident 12/7/2009 Letter	The recent closing of the Kroger Fuel Center may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek. If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State boulevard and Spy Run Avenue the road would pass behind or around most of the homes in the APE. The designers of the road project could not have foreseen the closing of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy it seems only fair that separate study be conducted factoring in possible land use of the former Kroger Fuel Center.	<p>The realignment of State Blvd to follow the bank of the Spy Run Creek would require extensive work in the channel of Spy Run Creek and the skew of the crossing would create the need for a longer bridge which would significantly increase the cost of the bridge. This alignment would also increase cost due to significant increase in the amount of required right of way and the demolition of a major commercial structure.</p> <p>A new intersection with Clinton Street would need to be constructed in close proximity to the existing State Blvd/Clinton Street intersection which would create safety concerns. This alignment would also likely cause economic impacts to the existing businesses located along existing State Blvd due to decreased visibility and accessibility</p>
2	Michael Galbraith – ARCH 12/9/2009 Letter	The HPR contained no maps, plans, or drawings of the proposed project. We have found it difficult to study, review, and analyses this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review?	<p>Maps, plans, and drawings have been developed since this comment was received. These maps, plans, and drawings were made available for review at the first CP meeting and additional drawings will be provided at the next CP meeting.</p>
3		Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states that the "Purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "Adding a new trail bridge" as stated on Page 3, addresses "Traffic Flow, Roadway, and Pedestrian Safety" along State boulevard need is stated in the cover letter to "originate from the substandard horizontal curve along State boulevard."	<p>The Purpose and Need for the project has been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.</p>

4		<p>It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid Project, possibly with the intention of avoiding future section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.</p>	<p>Pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
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5	<p>Regarding the substandard horizontal curve: we question the analysis of “substandard” and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff’s plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; “Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects o the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive.”</p>	<p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway’s safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table><tr><th>Station Line “A”</th><th>Existing Curve Radius</th><th>Req’d Radius (30 mph)</th></tr><tr><td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr><tr><td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr><tr><td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr></table>	Station Line “A”	Existing Curve Radius	Req’d Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
Station Line “A”	Existing Curve Radius	Req’d Radius (30 mph)												
18+66.60	175 ft	300 ft												
24+64.47	243 ft	300 ft												
27+23.73	210 ft	300 ft												
6	<p>Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR.</p>	<p>The Indiana Service Corporation Historic District is not included in the Project’s APE and therefore was not studied for potential effect from this project. No impacts to the Indiana Service Corporation Historic District are anticipated to result from this project.</p>												

7		<p>Also regarding the APE, were potential changes in traffic use in the Brookview – Irvington Park Historic District Studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this district. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut through traffic and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
8		<p>Plans for this project seem to be quite complete according to the project description in the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for this undertaking."</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>



	<p>We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. We believe that these projects must be aggregated for Section 106 review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 review that investigation into possible violation of Section 110(k) of the NHPA would be appropriate. The City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur: through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview – Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially</p>
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			<p>reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
9		<p>We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview – Irvington Park Historic district has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.</p>	<p>The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.</p>



10	Todd Zeiger – Indiana Landmarks 12/8/2009 Letter	<p>With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis will be provided to all CPs prior to the next (second) CPs Meeting.</p>
11		<p>Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.</p> <p>Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated ) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include "the geographic area or areas within which an undertaking may <u>directly or indirectly</u> cause alterations in the character or use of historic properties". While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

12		<p>How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.</p>	<p>The purpose and need for this project has been revised since this comment was provided. The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p>
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13		<p>Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus</p>	<p>Detailed accident data compiled by the City was included in the revised purpose and need and provided to all consulting parties.</p> <p>Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As provided in the purpose and need statement, the RMV exceeds 2.0 which indicates that a safety problem exists.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
14		<p>Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
15		<p>Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
16		<p>Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>



<b>17</b>		Please provide detailed traffic accident studies or report data pertaining to accidents at the "substandard" curve.	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
<b>18</b>		Please provide detailed pedestrian accident studies or report data pertaining to the "substandard curve."	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
<b>19</b>		How is the proposal to add a new "trail bridge" over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed "trail bridge".	<p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>

20		<p>"Trail Bridge": We continue to be concerned about an ongoing appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future "trail bridge" should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.</p>	<p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
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<p>21</p>	<p>Concerning the “substandard horizontal curve”: Page 4 of the HPR states that “ the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve.” We believe that this curve, a designed element of Arthur Shurcliff’s plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, “Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive.” We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.</p>	<p>In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway’s safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table border="1"> <thead> <tr> <th>Station Line “A”</th><th>Existing Curve Radius</th><th>Req’d Radius (30 mph)</th></tr> </thead> <tbody> <tr> <td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr> <tr> <td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr> <tr> <td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr> </tbody> </table>	Station Line “A”	Existing Curve Radius	Req’d Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
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<p>22</p>	<p>Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7. We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity “early in the undertaking’s planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis with will be provided to all CPs prior to the next (second) CPs Meeting.</p>												



23	<div data-bbox="185 132 690 787"> <p>Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgemoor Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.</p> <p>The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.</p> </div> <div data-bbox="185 787 690 1442"> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program or associated voluntary buyout program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p> </div>
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<b>24</b>	<b>Julie Donnell – Friends of the Parks 12/10/2009 Letter</b>	The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?	Yes – there is a proposed grade change of approximately 7 feet in the area of the new bridge.
<b>25</b>		The vegetation. What are the potential impacts on the vegetation of the designed landscape?	Development of final plans will include appropriate measures to mitigate and/or enhance landscape features within the project area.
<b>26</b>		The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?	With the exception of the new bridge over Spy Run Creek and channel grading immediately adjacent to the proposed bridge, no impacts to the waterway are anticipated from this project.
<b>27</b>		What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.	The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.
<b>28</b>	<b>Dr. James Glass - SHPO 12/14/2009 Letter</b>	Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, specifically in graphic form, to indicate where the alignments that are under consideration would run.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
<b>29</b>		Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in avoidance or minimization of the adverse effect.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.

30		<p>In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we not that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources.</p>	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
31	<p><b>John Shoaff – City Council 12/15/2009 CP Meeting</b></p>	<p>John Shoaff expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>



32	<p><b>Michelle Briggs – Wedaman – Brookview Neighborhood Association 12/15/2009 CP Meeting</b></p>	<p>Michelle Briggs-Wedaman also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
33	<p><b>Jan Daily – Brookview Neighborhood Resident 12/15/2009 CP Meeting</b></p>	<p>Jan Daily also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecking both down Huguenard and along State Boulevard.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

<b>34</b>	<b>Angie Quinn – ARCH 12/15/2009 CP Meeting</b>	Angie Quinn discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.	It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.
<b>35</b>	<b>ARCH 12/15/2009 CP Meeting</b>	ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
<b>36</b>	<b>Dr. James Glass - SHPO 1/27/2010 Letter</b>	One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.	Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.

37	<div data-bbox="191 793 738 1442" data-label="Text"> <p>Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.</p> </div> <div data-bbox="191 132 495 793" data-label="Text"> <p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> </div> <div data-bbox="521 132 738 793" data-label="Text"> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> </div> <div data-bbox="764 132 1042 793" data-label="Text"> <p>The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.</p> </div> <div data-bbox="1068 132 1261 793" data-label="Text"> <p>For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> </div> <div data-bbox="1287 132 1425 793" data-label="Text"> <p>The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing curve found on State Boulevard between Clinton Street and Cass Street.</p> </div> <div data-bbox="1451 510 1479 793" data-label="Text"> <p>(continued on next page)</p> </div>
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			<p>Specifically, the intersections of Eastbrook Drive and Terrace Road will be reconstructed and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south utilizing the Oakridge Rd extension. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.</p>
38		<p>It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum</p> <p>(continued on next page)</p>

		<p>Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
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39		<p>In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
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40		<p>Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred-and others that may yet occur-as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.</p>	<p>Consideration will be given to this request at the appropriate time.</p>
41	<p><b>Dr. James Glass – SHPO 3/10/2010 Letter</b></p>	<p>We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended – flood-proofing of some homes- has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State</p> <p>(continued on next page)</p>

		<p>Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
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42	<div data-bbox="191 1444 1229 1803"></div> <div data-bbox="191 793 1229 1444"> <p>We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first bluish like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.</p> </div> <div data-bbox="191 132 1229 793"> <p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the table included in the purpose and need statement..</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p> </div>
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43		<p>The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
44		<p>Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?</p>	<p>Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.</p>

45	Jill Downs - ARCH 6/15/2011 Letter	<p>With the Exception of a relatively short window of time during peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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46	<div data-bbox="191 793 1484 1444" data-label="Text"> <p>My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and “traffic-calming” solutions are being devised as ways of protecting neighborhood integrity and pedestrians safety.</p> </div> <div data-bbox="191 132 1484 793" data-label="Text"> <p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p> </div>
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			<p>Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
47		<p>The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's poor attempt to justify its need.</p>	<p>Please see the project's purpose and need; approved by INDOT and FHWA.</p>
48		<p>...American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than moving traffic through the area.</p>	<p>Original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

49		<p>It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
50		<p>Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. These congested conditions lead to increased air pollution in the form of emissions, travel time delays for motorists, and loss of productivity. Intersection congestion also causes motorists to attempt to cut through neighboring streets in order to avoid congestion and delays at the intersections. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p>

<b>51</b>		I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.	American Structurepoint is dedicated to following the Section 106 process in its entirety. No deviations from the Section 106 process which would warrant a "New Section 106 Review" have occurred.
<b>52</b>	<b>Todd Zeiger – Indiana Landmarks 6/16/2011 Email</b>	I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.	Additional clarification regarding the opportunity for Consulting Parties to comment was provided. As was intended and as is always the case, Consulting Parties may comment on any information received during the Section 106 process. Every attempt to address each consulting parties comment is being taken: as is evident by this document.
<b>53</b>		Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.
<b>54</b>		I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period.	An additional comment period was provided for the Purpose and Need. Requesting and considering consulting parties concerns is an ongoing effort and a central purpose of the Section 106 process. We are continually working to address your concerns, however this should not preclude additional consulting parties meetings which provide the opportunity for other comments to be provided.
<b>55</b>	<b>John Shoaft – City Council 6/16/2011 Letter</b>	First, let me note you said that a purpose and need statement was attached, but I did not find one included with my copy.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.



56		<p>In his letter Dr. Glass raises several important concerns, beginning with “It is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion”. Nor is it clear to me, even after reading your response.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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57		<p>On the curve of State Street, traffic always moved smoothly; but there was often (and always at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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58	<p>Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p>
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		<p>Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
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59	<div data-bbox="191 793 602 1442" data-label="Text"> <p>Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the arterial plus bypass concept". This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor," as you are already calling it, as if past errors justify future ones; you argue that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south – but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelhorn Road – which aligns with and neatly continues the east-west stretch of Coliseum.</p> </div> <div data-bbox="191 132 412 793" data-label="Text"> <p>The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.</p> </div> <div data-bbox="440 132 821 793" data-label="Text"> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> </div> <div data-bbox="849 132 1235 793" data-label="Text"> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northeast Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> </div> <div data-bbox="1263 132 1430 793" data-label="Text"> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy</p> </div> <div data-bbox="1458 510 1479 793" data-label="Text"> <p>(continued on next page)</p> </div>
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		<p>Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p> <p>The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.</p> <p>State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stielhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.</p>
60	<p>You totally ignore the welfare of the people who live within the area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values to the homes your corridor borders or traverses?</p>	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document. This is not appropriate for Section 106.</p>



<b>61</b>		<p>You refer to “The redevelopment of the urban core area” something we all want, but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor.</p>	<p>The mention of “redevelopment of the urban core area” is taken out of context in this comment. As stated in the purpose and need: “The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes.” This project is not being presented in order to redevelop the urban core, it is being presented in order to accommodate that redevelopment and the traffic demands it is currently placing on State Boulevard, which will continue to grow over time.</p>
<b>62</b>		<p>ISTEA was passed in 1991, the FHWA handbook on Flexibility in Highway Design was published in 195, and the AASHTO handbook on design flexibility which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert that standards appropriate for “highway design elements” apply to a road that abuts and serves a modest residential neighborhood!</p>	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p>
<b>63</b>	<b>Michael Galbraith - ARCH 6/17/2011 Letter</b>	<p>...my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as a mere footnote. Our Dec 8 2009 comments regarding the HPR and our comments from the initial Consulting Parties (Dec 15 2009) remain unaddressed. I request that our concern and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed and that we be given time in which to respond to those answers.</p>	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
<b>64</b>		<p>It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project purpose and need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in purpose and need.</p>	<p>The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment. The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

<b>65</b>	<b>Julie Donnell – Friends of the Parks 6 /14/2011 - Letter</b>	...We are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.  The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.
<b>66</b>		Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communications with Dr. Glass, in a formal way under the auspices of whatever form the Section 106 review continues.	An additional comment period was provided following distribution of the purpose and need.
<b>67</b>	<b>John Shoauff - 7/1/2011 Letter</b>	There are material discrepancies between the accident counts in your new P and N Statement and those sent by Hayley Steele of your firm to ARCH on 2/4/2010.	The discrepancy lies in the methodology used to identify potentially hazardous locations for system wide planning purposes in comparison to method that compiles crashes based on a conservative distance, usually from an intersection. The information utilized for both methods is based on the same data, which is derived from crash reports filed by state and local law enforcement agencies.  The "planning method" is used to assist in the screening and allocation of crashes to specific areas, to identify "hot-spots" where actual crashes exceed expected thresholds. This method compiles crashes within a 250' radius from a single point, generally an intersection. When the distance between intersections is less than 500', this method will compile crashes that are common to both intersection.  A micro-analysis, that examines each crash report, is the only way to ascertain the precise type, location and cause of each crash. We have conducted a micro-analysis for the State Boulevard corridor, Cass Street to Spy Run Avenue, that identifies all crashes for the years 2007-2010.
<b>68</b>		The RMVs generated by your figures are much higher than the City's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.	Both sets of crash data were provided to American Structurepoint by NIRCC.  See above response to question number 67.

69		<p>With respect to the Spy Run and Clinton intersections, it stains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part: namely to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels.</p> <p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
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70	Dr. James Glass – SHPO 7/5/2011	<p>We continue to think, however, that it may be appropriate to expand the Section 106 APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
71		<p>We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevance to the Section 106 process. The alternatives presented to the consulting parties so far appear to be minor variations in the State Boulevard Corridor alternative.</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>
72		<p>However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in responses to your May 19 and June 17 letters.</p>	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
73		<p>Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.</p>	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
74	Suzanna Slick – Irvington Park 7/6/2011 - Email	<p>I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State boulevard Correspondence.</p>	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document.</p>

75		There is no attempt at traffic calming, but a great emphasis on traffic rushing.	Alleviation of unnecessary congestion is not intended to translate to "traffic rushing." The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.
76		There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability.	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p> <p>It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.</p>
77		Other cities are making great strides in building roadways that are user friendly, and safe for everyone – not just car and truck drivers – and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.	The proposed project is being designed with the central goals of providing a safe roadway for both pedestrians and motorists, while impacting the historic area as little as possible.

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1<sup>st</sup>, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne.

We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1<sup>st</sup>, until then please let me know if there are any questions.

Thanks

**Brett W. Lackey**

Environmental Specialist, Environmental Sciences Group

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U.S. Department  
of Transportation  
**Federal Highway  
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254  
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard  
FHWA Liaison  
Advisory Council on Historic Preservation  
1100 Pennsylvania Avenue, NW  
Washington, DC 20004

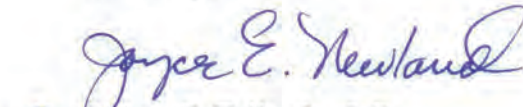
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at [joyce.newland@dot.gov](mailto:joyce.newland@dot.gov).

Sincerely,

  
Robert F. Tally, Jr., P.E.  
Division Administrator

Enclosure



## Lackey, Brett

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**From:** Suzanne [sjslick@mac.com]  
**Sent:** Monday, August 29, 2011 5:06 PM  
**To:** Lackey, Brett; mayor@ci.ft-wayne.in.us  
**Cc:** Glass, James; Carr, John; Tharp, Wade; aricketts@dnr.in.gov; Kaiser, Jason; Carpenter, Patrick A; Kennedy, Mary; Newland, Joyce; aquinn@archfw.org; Michael Galbraith; don.orban@cityoffortwayne.org; tzeiger@indianalandmarks.org; juliemarie57@earthlink.net; Michelle Briggs Wedaman; jcooper@ccrtc.com; jandailey59@msn.com; indianabridges@sbcglobal.net; shan.gunawardena@cityoffortwayne.org; danavery@co.allen.in.us; jshoaff@proparkwest.com; creager.smith@cityoffortwayne.org; albertcohan@aol.com; tmn@barrettllaw.com; rross@martin-riley.com; tom.cain@cityoffortwayne.org  
**Subject:** Re: State Boulevard Consulting Party Meeting  
**Attachments:** image001.jpg; ATT00001..htm; image002.jpg; ATT00002..htm; image003.jpg; ATT00003..htm; IN20071404.EV.2011-08-12.Consulting Parties Meeting Memo.pdf; ATT00004..htm

All,

After reading the Consulting Party comments and rebuttals from American Structurepoint I'm not very optimistic about the upcoming meeting -- either Structurepoint is being deliberately obtuse or they refuse to acknowledge our very real concerns about the State Blvd project's impact on our neighborhood and our City. People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive. Neighbors who are intimately familiar with the streets and traffic in the area -- much more familiar than anyone else involved in this discussion -- have weighed in in opposition to this massive alteration of our neighborhood, yet the responders continue to insist that this will improve safety and the level of service delivered to the users. The responses repeat the mantra that safety is of utmost importance and the primary goal, yet language regarding traffic calming seems to be deliberately avoided in the answers. While many cities are moving away from the trend to rush traffic quickly through urban areas and toward a complete streets approach to integrated roadways that encourage and expedite usability by non-motorized "traffic", State Blvd's future seems to be the opposite -- an artery of speeding cars and trucks racing in a straight line at high speed bisecting our quiet, quaint neighborhood, in effect cutting neighbors off from anything on the "other" side of State Blvd. In the list of alternatives, one would expect to find some discussion of the use of standard calming devices like reduced speed, raised crosswalks, chicanes, lateral shifts and roundabouts, for example. The "road diet" approach is not mentioned either. There is nothing remotely related to these approaches in any of the responses, just lots of rhetoric about "lengthy delays and congestion". Look, I drive the Cass to Clinton stretch daily -- there are no major delays and no lost productivity for motorists. Accidents in this stretch are primarily caused by speeding motorists which means as speed increases, as it surely will with a multi-lane straightaway, danger of accidents will increase. Certainly, risk to nonmotorized users will increase greatly. And passing off pedestrian needs to the Pufferbelly Trail project seems like an inadequate solution -- more an afterthought than a priority. Are the experts making these decisions and designing this roadway "improvement" that out of step with my neighborhood and with current best practices in street design? Let me point you to some information that will inform the conversation:

Here is a quote from the Kansas City Walkability Plan - <http://ww4.kcmo.org/planning/walkplan/Aappendix.pdf> :

Traffic calming is a way to design streets using engineering principles to encourage people to drive more slowly. It creates physical and visual cues that induce drivers to travel at appropriate speeds. Traffic calming is self-enforcing. The design of the roadway results in the desired effect without reliance on enforcement or voluntary compliance. Traffic control devices such as signals and signs rely on compliance. While elements such as landscaping and lighting do not force a change in driver behavior, they do provide the visual cues that encourage people to drive more slowly.

The reason traffic calming is such a powerful and compelling tool is that it has proven to be so effective. Some goals of traffic calming are clearly measurable such as increasing safety through fewer and less severe crashes. Others, such as supporting community and livability, are less tangible but equally important.

National Complete Streets Coalition -- <http://www.completestreets.org/>

More at these sites:

<http://ww4.kcmo.org/planning.nsf/plnpres/walkability?opendocument>

<http://www.fhwa.dot.gov/environment/sidewalk2/sidewalks209.htm>

<http://www.ite.org/traffic/tcstate.asp>

[http://safety.fhwa.dot.gov/ped\\_bike/](http://safety.fhwa.dot.gov/ped_bike/)

<http://www.virginiadot.org/business/resources/TrafficCalmingGuideOct2002.pdf>

<http://www.pps.org/articles/livememtraffic/>

<http://cityofsparks.us/sites/default/files/assets/documents/traffic/Traffic%20Calming.pdf>

Fort Wayne is a smart, vibrant city that could achieve so much more in improving livability and healthy neighborhoods, the State Blvd project could be an opportunity to do this. I'm afraid what we will get is a noisy, frenetic, dangerous megastructure that citizens will avoid unless they are speeding through in a car or truck on their way to somewhere else. It is not an appealing image to those of us who will have to tolerate its unavoidable presence in our neighborhood. And we have already lost so very much in the last few years to the flood control buy-out, the blighting of Centlivre and even in the loss of ash trees in our green spaces. If we must have this new roadway, can't it be crafted in a forward-thinking, people-friendly, neighborhood-sustaining fashion?

Sincerely,  
Suzanne Slick  
Consulting Party for Irvington Park

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1<sup>st</sup>, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1<sup>st</sup>, until then please let me know if there are any questions.

Thanks

**Brett W. Lackey**  
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## MEETING MINUTES

**Location:** City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

**Date:** September 1, 2011

**Project Name:** State Boulevard Reconstruction (Des. No. 0400587)

**Project No.:** IN20071404

**Attendees:** Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)  
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross  
(City of Fort Wayne)  
Camille Fife (The Westerly Group)  
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and  
Archaeology)  
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)  
Jason Kaiser (INDOT Fort Wayne District)  
Joyce Newland (Federal Highway Administration)  
John Shoaff (Fort Wayne City Council)  
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)  
Suzanne Slick (Irvington Park Neighborhood)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)  
Michael Galbraith, Jill Downs (ARCH, Inc.)  
Michelle Briggs-Wedaman (Brookview Neighborhood Association)  
Charlotte Weybright (Friends of the Parks of Allen County)  
Susan Haneline (Brookview Neighborhood Resident)  
Charley Shirmeyer (Northside Galleries)  
Mike Thomson (Allen County Highway Department)  
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
  - 1) Project Update
  - 2) Purpose and Need Update
  - 3) Consulting Party Comments and Responses document
  - 4) Alternatives Review
  - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.



18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.



50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.



## ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or [blackey@structurepoint.com](mailto:blackey@structurepoint.com) to revise the record.

Very truly yours,  
American Structurepoint, Inc.

A handwritten signature in dark ink, appearing to be "BWL", with a long horizontal line extending to the right.

Brett W. Lackey

BWL:mgn

Enclosures

## Lackey, Brett

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**From:** Lackey, Brett  
**Sent:** Thursday, September 08, 2011 12:18 PM  
**To:** Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich  
**Cc:** 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott  
**Subject:** State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
  - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
  - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
  - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
  - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
  - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
  - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
  - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
  - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
  - Either city website or [www.structurepoint.com](http://www.structurepoint.com)
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
  - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

**Brett W. Lackey**

Environmental Specialist, Environmental Sciences Group

7260 Shadeland Station

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*Preserving America's Heritage*

September 22, 2011

Robert F. Tally, Jr., P.E.  
Division Administrator  
FHWA – Indiana Division  
575 North Pennsylvania Street, Room 254  
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project  
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, “Protection of Historic Properties” (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP’s regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking’s effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004  
Phone: 202-606-8503 • Fax: 202-606-8647 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at [ngabriel@achp.gov](mailto:ngabriel@achp.gov).

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson  
Historic Preservation Technician  
Office of Federal Agency Programs



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## M E M O R A N D U M

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**DATE:** September 29, 2011

**TO:** Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross (City of Fort Wayne)  
Camille Fife (The Westerly Group)  
Dr. James Glass, John Carr, Wade Tharp, Amy Johnson, Amanda Ricketts (IDNR, Division of Historic Preservation and Arch.)  
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)  
Jason Kaiser (INDOT Fort Wayne District)  
Joyce Newland (Federal Highway Administration)  
John Shoaff (Fort Wayne City Council)  
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)  
Suzanne Slick (Irvington Park Neighborhood)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)  
Michael Galbraith, Jill Downs, Angie Quinn (ARCH, Inc.)  
Michelle Briggs-Wedaman, Karl Dietsch (Brookview Neighborhood Association)  
Julie Donnell, Charlotte Weybright (Friends of the Parks of Allen County)  
Susan Haneline (Brookview Neighborhood Resident)  
Charley Shirmeyer (Northside Galleries)  
Mike Thornson (Allen County Highway Department)  
Todd Zeiger (Indiana Landmarks)  
Dr. James Cooper, Paul Brandenburg (Indiana Historic Spans Task Force)  
Albert Cohan (Westbrook 5, LLC)  
Thomas Neizer (Barrett & McNagney, LLP)  
Ronald Ross (Martin Riley Architects and Engineers)  
Dan Ernst (Earth Source, Inc.)

**FROM:** Brett W. Lackey (American Structurepoint, Inc.)

**RE:** State Boulevard Reconstruction  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
Structurepoint No. IN20071404

**CC:** Scott Crites, Briana Hope, Rich Zielinski (American Structurepoint, Inc.)

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Enclosed, please find the following items:

- 1) Consulting Party Meeting Minutes (9/01/2011)  
Meeting minutes were prepared for the September 1, 2011 Consulting Parties Meeting. The meeting minutes were prepared based on a digital recording of the meeting.
- 2) Agency Coordination Meeting Minutes (9/02/2011)  
A meeting was held on Friday, September 2, 2011, at the American Structurepoint office to discuss the State Boulevard Consulting Party Meeting on September 1, 2011. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The meeting minutes summarize the discussion.



3) Individual Section 4(f) Alternative Analysis (Revised)

Following the September 1, 2011 Consulting Parties Meeting, American Structurepoint evaluated an additional State Boulevard Alternative which includes a 3-lane design. This Alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3D*.

American Structurepoint has also re-evaluated an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south. Additional information regarding this alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3C*.

American Structurepoint has added a discussion of three additional configurations for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway. A discussion of these access alternates (*Access Alternates 1-3*) is included as a subset of *Alternate 3A*.

American Structurepoint has added additional information to *Alternate 4 (No-Build)*.

4) Traffic Data from NIRCC

As requested by Consulting Parties during the September 1, 2011 meeting, additional traffic information regarding the intersection Level of Service has been compiled by NIRCC and is enclosed for your information.

Upon further review of the State Boulevard intersection level of service information, the purpose and need statement has been updated by removing the evening peak Spy Run Avenue eastbound through movement from the deficient category of the purpose and need statement. Although the overall level of service for this intersection approach is deficient (LOS E), the LOS associated with the eastbound through movement is LOS D which is considered acceptable. This revision is located on page 2 of 5 of the purpose and need statement. A copy of page 2 with the revision highlighted is included for your review.

5) ACHP Correspondence

As indicated during the September 1, 2011 Consulting Parties Meeting, the FHWA has initiated coordination with the Advisory Council on Historic Preservation (ACHP). The FHWA requested ACHP involvement in a letter dated August 29, 2011. On September 22, 2011, the ACHP provided a response letter to the FHWA which indicated that additional information will need to be evaluated by the ACHP prior to deciding whether or not the ACHP will choose to be involved in the project's Section 106 process. The requested additional information is currently being prepared and will be submitted to the ACHP in the near future. Copies of the two coordination letters are included for your review.

As requested during the September 1, 2011 Consulting Parties Meeting, the City of Fort Wayne has created an online archive for the project's Section 106 correspondence and documents. This data can be accessed online at <http://www.cityoffortwayne.org/publicworks/west-state-blvd-realignment.html>

At this time we are requesting that all consulting parties review the enclosed materials and provide any comments within 30 days of receipt of this mailing. I can be reached by phone at (317) 547-5580 or by e-mail at [blackey@structurepoint.com](mailto:blackey@structurepoint.com). If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Minutes (9/01/2011)  
Agency Coordination Meeting Minutes (9/02/2011)  
Individual Section 4(f) Alternatives Analysis (Revised)  
Traffic Data from NIRCC  
ACHP Correspondence  
Purpose and Need Statement Revision (Page 2 of 5)



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## MEETING MINUTES

**Location:** City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

**Date:** September 1, 2011

**Project Name:** State Boulevard Reconstruction (Des. No. 0400587)

**Project No.:** IN20071404

**Attendees:** Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)  
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross  
(City of Fort Wayne)  
Camille Fife (The Westerly Group)  
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and  
Archaeology)  
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)  
Jason Kaiser (INDOT Fort Wayne District)  
Joyce Newland (Federal Highway Administration)  
John Shoaff (Fort Wayne City Council)  
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)  
Suzanne Slick (Irvington Park Neighborhood)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)  
Michael Galbraith, Jill Downs (ARCH, Inc.)  
Michelle Briggs-Wedaman (Brookview Neighborhood Association)  
Charlotte Weybright (Friends of the Parks of Allen County)  
Susan Haneline (Brookview Neighborhood Resident)  
Charley Shirmeyer (Northside Galleries)  
Mike Thornson (Allen County Highway Department)  
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
  - 1) Project Update
  - 2) Purpose and Need Update
  - 3) Consulting Party Comments and Responses document
  - 4) Alternatives Review
  - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.



18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.



50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.



## ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or [blackey@structurepoint.com](mailto:blackey@structurepoint.com) to revise the record.

Very truly yours,  
American Structurepoint, Inc.



Brett W. Lackey

BWL:mgn

Enclosures

## Lackey, Brett

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**From:** Lackey, Brett  
**Sent:** Thursday, September 08, 2011 12:18 PM  
**To:** Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich  
**Cc:** 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott  
**Subject:** State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
  - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
  - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
  - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
  - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
  - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
  - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
  - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
  - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
  - Either city website or [www.structurepoint.com](http://www.structurepoint.com)
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
  - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

**Brett W. Lackey**

Environmental Specialist, Environmental Sciences Group

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### **Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)**

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

## **Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)**

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

### **Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)**

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

#### **Access Alternate 1**

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

#### **Access Alternate 2 (Preferred Access Alternative)**

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.



### Access Alternate 3

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

### **Alternative 3B: Widen State Boulevard on Existing Alignment**

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 20 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

### **Alternative 3C: Shift State Boulevard Alignment South**

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and "mirror" or "flip" the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately 4 to 5-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and

State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project's purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

### **Alternative 3D: Preferred Alignment with 3-Lane Typical Section**

This alternative is similar to Alternative 3A (Preferred Alternative) but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes while correcting the substandard horizontal curve.

By reducing the typical section from 4-lanes (Alternative 3A/Preferred Alternative) to 3-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from 4-lanes to 3-lanes is only 10-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.



A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

#### **Alternative 4: No Build**

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.


















The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

# HCM Signalized Intersection Capacity Analysis

## 1: State Blvd & Clinton St



















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	543	46	200	437	0	0	0	0	126	1707	47
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.99		1.00	1.00						1.00	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3427		1687	1792						4999	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3427		1687	1792						4999	
Peak-hour factor, PHF	0.92	0.84	0.72	0.88	0.85	0.92	0.92	0.92	0.92	0.73	0.84	0.78
Adj. Flow (vph)	0	646	64	227	514	0	0	0	0	173	2032	60
RTOR Reduction (vph)	0	9	0	0	0	0	0	0	0	0	3	0
Lane Group Flow (vph)	0	701	0	227	514	0	0	0	0	0	2262	0
Heavy Vehicles (%)	0%	3%	13%	7%	6%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3						1	
Permitted Phases										1		
Actuated Green, G (s)		18.5		13.5	36.5						34.4	
Effective Green, g (s)		18.5		13.5	36.5						34.4	
Actuated g/C Ratio		0.23		0.17	0.46						0.43	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		792		285	818						2150	
v/s Ratio Prot		c0.20		c0.13	0.29							
v/s Ratio Perm											0.45	
v/c Ratio		0.88		0.80	0.63						1.05	
Uniform Delay, d1		29.7		31.9	16.6						22.8	
Progression Factor		1.00		0.58	1.15						1.00	
Incremental Delay, d2		13.8		11.9	2.0						34.8	
Delay (s)		43.5		30.4	21.0						57.6	
Level of Service		D		C	C						E	
Approach Delay (s)		43.5			23.9			0.0			57.6	
Approach LOS		D			C			A			E	
<b>Intersection Summary</b>												
HCM Average Control Delay			48.2			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.95									
Actuated Cycle Length (s)			80.0			Sum of lost time (s)				13.6		
Intersection Capacity Utilization			75.5%			ICU Level of Service				D		
Analysis Period (min)			15									
c Critical Lane Group												

## HCM Signalized Intersection Capacity Analysis

### 2: State Blvd & Spy Run Ave

9/29/2011


















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	93	526	0	0	470	211	131	1166	230	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.97				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1612	3505			1827	1568	1612	4672				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1612	3505			1827	1568	1612	4672				
Peak-hour factor, PHF	0.75	0.90	0.92	0.92	0.85	0.70	0.91	0.93	0.77	0.92	0.92	0.92
Adj. Flow (vph)	124	584	0	0	553	301	144	1254	299	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	29	0	50	0	0	0	0
Lane Group Flow (vph)	124	584	0	0	553	272	144	1503	0	0	0	0
Heavy Vehicles (%)	12%	3%	0%	0%	4%	3%	12%	8%	7%	0%	0%	0%
Turn Type	Prot				Perm				Perm			
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Effective Green, g (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Actuated g/C Ratio	0.08	0.44			0.29	0.29	0.43	0.43				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	131	1542			530	455	695	2015				
v/s Ratio Prot	c0.08	0.17			c0.30			c0.32				
v/s Ratio Perm						0.17	0.09					
v/c Ratio	0.95	0.38			1.04	0.60	0.21	0.75				
Uniform Delay, d1	36.6	15.1			28.4	24.4	14.2	19.1				
Progression Factor	1.56	0.41			1.00	1.00	1.00	1.00				
Incremental Delay, d2	36.4	0.3			50.9	5.7	0.7	2.6				
Delay (s)	93.4	6.5			79.3	30.1	14.9	21.6				
Level of Service	F	A			E	C	B	C				
Approach Delay (s)		21.7			61.9			21.1			0.0	
Approach LOS		C			E			C			A	
<b>Intersection Summary</b>												
HCM Average Control Delay			31.9		HCM Level of Service				C			
HCM Volume to Capacity ratio			0.87									
Actuated Cycle Length (s)			80.0		Sum of lost time (s)				15.8			
Intersection Capacity Utilization			75.5%		ICU Level of Service				D			
Analysis Period (min)			15									
c Critical Lane Group												



# HCM Signalized Intersection Capacity Analysis

## 1: State Blvd & Clinton St



















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	629	67	192	539	0	0	0	0	178	1593	117
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.98		1.00	1.00						0.99	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3445		1687	1810						4955	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3445		1687	1810						4955	
Peak-hour factor, PHF	0.92	0.95	0.83	0.89	0.89	0.92	0.92	0.92	0.92	0.93	0.95	0.75
Adj. Flow (vph)	0	662	81	216	606	0	0	0	0	191	1677	156
RTOR Reduction (vph)	0	8	0	0	0	0	0	0	0	0	8	0
Lane Group Flow (vph)	0	735	0	216	606	0	0	0	0	0	2016	0
Heavy Vehicles (%)	0%	2%	12%	7%	5%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3						1	
Permitted Phases										1		
Actuated Green, G (s)		29.5		17.5	51.5						59.4	
Effective Green, g (s)		29.5		17.5	51.5						59.4	
Actuated g/C Ratio		0.25		0.15	0.43						0.49	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		847		246	777						2453	
v/s Ratio Prot		c0.21		c0.13	0.33							
v/s Ratio Perm											0.41	
v/c Ratio		0.87		0.88	0.78						0.82	
Uniform Delay, d1		43.4		50.2	29.4						25.8	
Progression Factor		1.00		1.51	0.46						1.00	
Incremental Delay, d2		11.7		26.0	5.7						3.3	
Delay (s)		55.1		101.6	19.1						29.0	
Level of Service		E		F	B						C	
Approach Delay (s)		55.1			40.8			0.0			29.0	
Approach LOS		E			D			A			C	
<b>Intersection Summary</b>												
HCM Average Control Delay			37.1			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.84									
Actuated Cycle Length (s)			120.0			Sum of lost time (s)			13.6			
Intersection Capacity Utilization			109.3%			ICU Level of Service			H			
Analysis Period (min)			15									
c Critical Lane Group												

## HCM Signalized Intersection Capacity Analysis

### 2: State Blvd & Spy Run Ave

9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	134	606	0	0	508	275	130	1862	243	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.98				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1770	3539			1863	1615	1719	5036				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1770	3539			1863	1615	1719	5036				
Peak-hour factor, PHF	0.76	0.92	0.92	0.92	0.92	0.82	0.86	0.97	0.92	0.92	0.92	0.92
Adj. Flow (vph)	176	659	0	0	552	335	151	1920	264	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	10	0	15	0	0	0	0
Lane Group Flow (vph)	176	659	0	0	552	325	151	2169	0	0	0	0
Heavy Vehicles (%)	2%	2%	0%	0%	2%	0%	5%	1%	2%	0%	0%	0%
Turn Type	Prot				Perm				Perm			
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Effective Green, g (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Actuated g/C Ratio	0.09	0.44			0.31	0.31	0.47	0.47				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	155	1569			578	501	809	2371				
v/s Ratio Prot	c0.10	0.19			c0.30			c0.43				
v/s Ratio Perm						0.20	0.09					
v/c Ratio	1.14	0.42			0.96	0.65	0.19	0.91				
Uniform Delay, d1	54.8	22.8			40.6	35.7	18.4	29.5				
Progression Factor	0.89	2.00			1.00	1.00	1.00	1.00				
Incremental Delay, d2	92.1	0.4			27.8	6.4	0.5	6.9				
Delay (s)	140.6	46.0			68.4	42.1	18.9	36.4				
Level of Service	F	D			E	D	B	D				
Approach Delay (s)		65.9			58.5			35.3			0.0	
Approach LOS		E			E			D			A	
<b>Intersection Summary</b>												
HCM Average Control Delay		46.7			HCM Level of Service			D				
HCM Volume to Capacity ratio		0.95										
Actuated Cycle Length (s)		120.0			Sum of lost time (s)			15.8				
Intersection Capacity Utilization		109.3%			ICU Level of Service			H				
Analysis Period (min)		15										
c Critical Lane Group												



U.S. Department  
of Transportation  
**Federal Highway  
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254  
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard  
FHWA Liaison  
Advisory Council on Historic Preservation  
1100 Pennsylvania Avenue, NW  
Washington, DC 20004

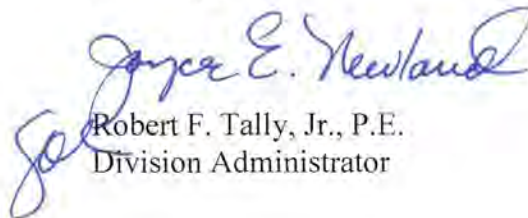
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at [joyce.newland@dot.gov](mailto:joyce.newland@dot.gov).

Sincerely,

  
Robert F. Tally, Jr., P.E.  
Division Administrator

Enclosure







*Preserving America's Heritage*

September 22, 2011

Robert F. Tally, Jr., P.E.  
Division Administrator  
FHWA – Indiana Division  
575 North Pennsylvania Street, Room 254  
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project  
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, "Protection of Historic Properties" (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP's regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking's effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004  
Phone: 202-606-8503 • Fax: 202-606-8647 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at [ngabriel@achp.gov](mailto:ngabriel@achp.gov).

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson  
Historic Preservation Technician  
Office of Federal Agency Programs

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

#### State Street and Spy Run Avenue Intersection

<b>Morning Peak</b>	<b>LOS Existing</b>
East Bound Left	<b>F</b>
West Bound Through	<b>E</b>

<b>Evening Peak</b>	<b>LOS Existing</b>
East Bound Left	<b>F</b>
<del>East Bound Through</del>	<del>E</del>
West Bound Through	<b>E</b>

Removed





Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739  
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



November 7, 2011

Brett W. Lackey  
Environmental Specialist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Effects of the State Boulevard Reconstruction and consideration of alternatives (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated August 15, 2011 and your memorandum dated September 29, 2011 and received which were received on August 16 and September 30, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Your August 15, 2011 letter asked us to offer an opinion on how the National Register boundaries of the listed Brookview-Irvington Park Historic District might be affected by the preferred alternative (now called Alternative 3A in the revised Section 4[f] alternatives analysis). Having considered the marked aerial photograph shown at the last consulting parties meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative. We also think that if the preferred alternative were to be implemented as shown on the marked aerial photograph, one might still be justified in considering the part of the historic district that lies to the southwest of Spy Run (i.e., along Edgehill Avenue, Jacobs Avenue, and Clinton Street south of Westbrook Drive) and south of State Boulevard) to be sufficiently connected to the rest of the district at the Westbrook Avenue-State Boulevard intersection that the buildings and landscapes in that southern end of the district would still contribute to the district's significance. However, that is not an ideal situation from a National Register boundary delineation standpoint. We would anticipate that, even if the right-of-way along the preferred alignment were appropriately landscaped, the eye would perceive be a rather broad and obvious expanse of non-contributing land and mostly straight, new roadway between the current alignment of State Boulevard and the outline that has been left Westbrook, south of Spy Run (as a result of the flood buy-outs and pavement removal). We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished.

The minutes of the September 1, 2011 consulting parties meeting reflect some, but not all, of the comments I offered regarding a couple of other, potential alternatives. One was the widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B). We recall, however, that this alternative was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.

I recall also having asked that an alternative on roughly the same alignment as the preferred alternative (i.e., running south of the existing State Boulevard between Westbrook Drive and a point to the east of Terrace Road) be considered that would have three lanes, instead of four lanes with a left turn lane or landscaped median (captured in part in points 50, 56, and 81 in the minutes). This second alternative would have one through lane in each direction and a center left turn lane. I had asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of

fewer residences than the preferred alternative. It appears that this alternative is essentially what is now called Alternative 3D in the revised Section 4(f) alternatives analysis.

We would like to elaborate on that second alternative, 3D, in light of our observations during a site visit to the project area and the Brookview-Irvington Park Historic District that Wade Tharp, John Carr, and I made following the September 1 meeting. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State. Even if a T-intersection would be practicable only at Eastbrook, perhaps a single, exit-only lane from new State to existing State could be left in place near where the bypass turns southwestward, which would retain westbound access to the bypassed part of State. Narrowing the roadway to three lanes could eliminate the need to take the contributing house at the northwest corner of State Boulevard and Westbrook Drive and leave somewhat more of the back yards of the houses along the south side of State, possibly saving as many as three more houses on State. Narrowing the roadway to three lanes also could reduce the cost of the new bridge over Spy Run and pull the new bridge away from the historic bridge. Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work, and with the installation of bollards both the Eastbrook and Westbrook ends. As a pedestrian bridge, its closure would not be a major impediment to transportation when floodwaters overtop it. We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.

The revised alternatives analysis says that Alternative 3D "is not considered prudent as it does not address the project's entire purpose and need." We wonder, however, whether a project is worthwhile and fundable by FHWA only if it fully meets all the needs and purposes that are identified. We wonder, also, whether there is not room for compromise between purpose and need and other values, such as historic preservation and neighborhood livability and preservation.

In the meeting minutes, points 39 and 40 refer to questions raised about the possibility of a new alignment (now called Alternative 3C) farther to the south than the preferred alternative (Alternative 3A). We do not recall that part of the discussion *verbatim*, but we think that the reference in the point 39 to "reversing the existing alignment/flipping the existing curb to the south" probably refers to a suggestion that the *curve* of the new alignment west of Clinton Street be reversed or flipped, so that most of the new alignment would be farther to the south than the preferred alternative.

The revised alternatives analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection? That curve now is proposed to begin west of the alley between Clinton and Terrace Road. However, if the beginning of the curve could be moved perhaps 100 feet farther east, without changing the radius significantly and without changing significantly the angle of the State-Clinton intersection, most of the new State Boulevard would shift to the east and away from several of the houses on the south side of State between Terrace and Eastbrook Drive. If that shift were coupled with the elimination of the proposed, southward extension of Oakridge that we suggested above for Alternative 3D, then several contributing houses on the south side of State could be avoided by the project. More of the character of the curve of State Boulevard that Arthur Shurcliff designed in 1917 could be preserved if houses were left on both sides of State. Furthermore, we noticed, in the historic properties report and in the National Register nomination for the Fort Wayne Parks and Boulevard System Historic District, that George Kessler's 1912 plan for State Boulevard (or Pfeiffer Boulevard, as he called it) roughly would have aligned State, running westward from its intersection with Spy Run Avenue, with that part of State lying to the west of Spy Run (the stream). It appears to us that shifting the curve of the new State Boulevard toward the east would result in an alignment somewhat closer to that which Kessler had intended (but which was not implemented) between Clinton and Spy Run (the stream), while retaining much of the existing State Boulevard's Shurcliff-designed curve through the Brookview-Irvington Park Historic District.

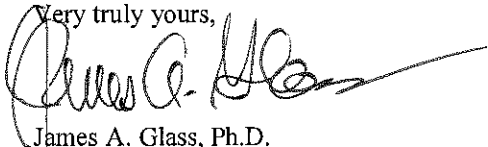
The eastward shifting of the alignment of either Alternative 3A or Alternative 3D probably would result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along existing State Boulevard. Even if the project cost were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan.

As we earlier had stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link [http://www.in.gov/dnr/historic/bin/qp/qp\\_archo.pdf](http://www.in.gov/dnr/historic/bin/qp/qp_archo.pdf)).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at [www.achp.gov](http://www.achp.gov) for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Staffan Peterson, Ph.D., Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Patrick Carpenter, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Brett W. Lackey, American Structurepoint, Inc.





AMERICAN  
**STRUCTUREPOINT**  
INC.

May 17, 2012

Linda Weintraut, Ph.D.  
Weintraut & Associates, Inc  
4649 Northwestern Drive  
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Dr. Weintraut:

As discussed at the September 1, 2011, Consulting Parties Meeting, resources previously identified in the Historic Properties Report (HPR) (Westerly Group, Inc, 2009) as eligible for listing in the National Register (NR) have since been listed on the NR, requiring an update to the HPR. Weintraut & Associates, Inc., has prepared an Additional Information (AI) Report to append the HPR. The AI is a supplement to the HPR following the inclusion of two new NR listed resources within the Area of Potential Effect (APE). Historians from Weintraut & Associates identified portions of both the Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011) as contained within the project APE. As previously stated, it is anticipated that the proposed project activities will have an adverse affect on these properties; therefore, an "Adverse Effect" finding continues to be anticipated.

The AI and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. Additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the AI within 30 days of receipt of this letter.

Please also beware that Weintraut & Associates will act as the Qualified Professional associated with the Section 106 process on this project as we continue to move forward. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at [bhope@structurepoint.com](mailto:bhope@structurepoint.com). The information herein will be provided to all consulting parties.

Very truly yours,  
American Structurepoint, Inc.

Briana M. Hope  
Environmental Project Manager

BMH:mgn

Enclosures

IN20071404





AMERICAN STRUCTUREPOINT, INC.

Linda Weintraut, Ph.D.

May 17, 2012

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Distribution List

Dr. James A. Glass, IDNR  
Angie Quinn, ARCH  
Jill Downs, ARCH  
Michael Galbraith, ARCH  
Don Orban, Fort Wayne Historic Preservation Commission  
Todd Zeiger, Indiana Landmarks  
Julie Donnell, Friends of the Parks of Allen County  
Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Dr. James L. Cooper  
Paul Brandenburg, Indiana Historic Spans Task Force  
Shan Gunawardena, City of Fort Wayne  
Susan Haneline, Brookview Civic Neighborhood Association  
Charlie Shirmeyer, Northside Galleries  
Karl Dietsch, Brookview Civic Neighborhood Association  
Dan Avery, NIRCC  
Suzanne Slick, Irvington park Neighborhood Association  
Annette "Jan" Dailey, adjacent property owner  
Joyce Newland, FHWA  
John Shoaff, Fort Wayne City Council  
Jason Kaiser, INDOT  
Patrick Carpenter, INDOT  
Mary Kennedy, INDOT  
Creager Smith, City of Fort Wayne  
Albert Cohen, Westbrook 5, LLC  
Thomas M. Niezer, Barrett & McNagny, LLP  
Ronald Ross, Marin Riley Architects and Engineers  
Dan Ernst, Earth Source, Inc.  
John Carr, IDNR  
Amy Johnson, IDNR  
Amanda Ricketts, IDNR  
Wade Tharp, IDNR  
Tom Cain, City of Fort Wayne  
Linda Weintraut, Weintraut & Associates  
Najah Duvall-Gabriel, ACHP  
Greg Smith, INDOT



AMERICAN  
**STRUCTUREPOINT**  
INC.

May 22, 2012

Linda Weintraut, Ph.D.  
Weintraut & Associates, Inc  
4649 Northwestern Drive  
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Dr. Weintraut:

You should have recently received a letter dated May 17, 2012, discussing the Additional Information (AI) Report, prepared by Weintraut & Associates, appending the Historic Properties Report (HPR) prepared for the State Boulevard Project. The report included with the May 17 letter was the original HPR and did not include the AI. The AI was accidentally omitted from the coordination.

Please find enclosed the AI and eligibility recommendations approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. As previously stated, additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. We are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, we have extended the comment period and ask that you reply with comments on the AI within 30 days of receipt of this letter.

We appreciate your cooperation in the development of this project and apologize for any confusion our omission may have caused. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at [bhope@structurepoint.com](mailto:bhope@structurepoint.com). The information herein will be provided to all consulting parties.

Very truly yours,  
American Structurepoint, Inc.

Briana M. Hope  
Environmental Project Manager

BMH:ead

Enclosures

IN20071404





Dr. Weintraut  
May 22, 2012  
Page 2

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Dr. James A. Glass, IDNR  
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Thomas M. Niezer, Barrett & McNagny, LLP  
Ronald Ross, Marin Riley Architects and Engineers  
Dan Ernst, Earth Source, Inc.  
John Carr, IDNR  
Amy Johnson, IDNR  
Amanda Ricketts, IDNR  
Wade Tharp, IDNR  
Tom Cain, City of Fort Wayne  
Linda Weintraut, Weintraut & Associates  
Najah Duvall-Gabriel, ACHP  
Greg Smith, INDOT

Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739  
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



June 22, 2012

Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: "Historic Property Report: State Boulevard Reconstruction from Spy Run to Cass Street, Additional Information Report" ("AI Report"; Natali, 4/2012) to append the historic properties report (Fife, 9/2009) for the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials submitted with your cover letters dated May 17 and 22, 2012, and received on May 21 and 24, respectively for the above-indicated project in the City of Fort Wayne, Allen County, Indiana.

We agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the area of potential effects, for inclusion in the National Register of Historic Places

We were asked to address specifically the eligibility of two, particular properties. First, we agree with the AI Report that the ca. 1895, two-story gabled-ell house at 315 East State Boulevard does not appear to possess sufficient historical or architectural significance or integrity to be eligible for inclusion in the National Register. Second, the AI Report notes that the Fort Wayne Park and Boulevard System Historic District National Register nomination characterized the portion of State Boulevard that lies within the Brookview-Irvington Park Historic District as being "individually eligible for the NR" (page 75 of the nomination). We do not consider that comment, which was included in an explanatory note about features of the district under "Section 7. Description" within the National Register nomination form, to confer individual eligibility on State Boulevard or any part of it. Furthermore, based on the information provided in the AI Report, we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the area of potential effects of this project is individually eligible for the National Register. We do not disagree, however, with the Fort Wayne Park and Boulevard System nomination's identification of the portion of State Boulevard in question as a contributing resource to that historic district.

As we stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we note that the historic property report (Fife 9/09) stated, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation

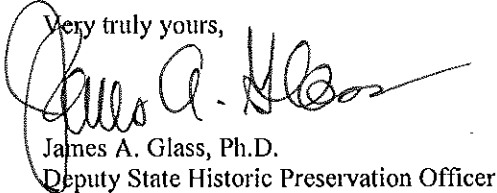
and Archaeology for review before we can comment further (see the list of qualified professional archaeologists at the Division of Historic Preservation and Archaeology website, [http://www.in.gov/dnr/historic/bin/qp/qp\\_archeo.pdf](http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf)).

An example of an area that might still need to be surveyed would be the portion of a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report (Stillwell 4/2/09). If an entire residential lot, rather than just the portion of it that was surveyed, will now have to be acquired for the State Boulevard Reconstruction, then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration  
Patrick A. Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Briana Hope, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.





AMERICAN  
**STRUCTUREPOINT**  
INC.

July 2, 2012

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer  
Division of Historic Preservation & Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated November 7, 2011, and items further discussed in an agency meeting held June 20, 2012. In attendance at the June 20, 2012, agency meeting were Dr. James Glass and John Carr - IDNR DHPA, Shan Gunawardena - City of Fort Wayne, Larry Heil - FHWA, Patrick Carpenter and Mary Kennedy - INDOT CRO, Jason Kaiser - INDOT Fort Wayne District (conference line), Dan Avery - NIRCC (conference line), Briana Hope and Scott Crites - American Structurepoint, and Linda Weintraut - Weintraut and Associates.

The letter states that the DHPA believes the minutes of the September 1, 2011, consulting parties meeting reflect some, but not all, of the comments SHPO offered regarding a couple of other, potential alternatives. We are providing responses to these additional comments.

- ❖ **The widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B) ... was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.**

Alternative 3B includes widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four lanes. By reducing the typical section from four lanes to three lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this Alternative would result in impacts to 20 residential properties within the Brookview-Irvington Historical District, the same number of relocations as the 4-lane typical section on existing alignment. This alternative would also still require a new bridge with additional travel lanes over Spy Run Creek. The widening of two lanes to three lanes as suggested by your office would ultimately result in a feasible alternative. However, the alternative is not considered prudent as it does not address the project purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.



- ❖ [DHPA had] asked that an alternative on roughly the same alignment as the preferred alternative be considered that would have three lanes, instead of four lanes, with a left-turn lane or landscaped median. It appears that this alternative is essentially what is called Alternative 3D in the revised Section 4(f) alternative analysis. [DHPA] asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of fewer residences than the preferred alternative. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State.

As part of the alternatives analysis, Alternative 3D was developed to explore the impacts of reducing the proposed State Boulevard to a 3-lane section with one through lane in each direction and a center median or left-turn lane where required. This option would reduce project costs by reducing pavement costs and allowing a narrower bridge to be constructed over Spy Run Creek. In addition to the reasons Alternative 3D was determined to be unfeasible in the 4(f) alternative analysis, the reduction in lanes would also result in traffic congestion and level of service at the intersections remaining the same as the existing conditions. Northeastern Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently and in the 3-lane section scenario, both intersections exhibit intersection movements having service levels of E or F. The following table (as included and discussed in the purpose and need statement) summarizes the levels of service at the Clinton Street intersection and Spy Run Avenue intersection, with the lane configurations remaining the same as the existing condition due to constructing State Boulevard as a 3-lane section:

**State Boulevard and Spy Run Avenue  
 Intersection – Levels of Service**

<b>Morning Peak</b>	<b>LOS Existing</b>
Eastbound Left	<b>F</b>
Westbound Through	<b>E</b>

<b>Evening Peak</b>	<b>LOS Existing</b>
Eastbound Left	<b>F</b>
Eastbound Through	<b>E</b>
Westbound Through	<b>E</b>

**State Street and Clinton Street  
 Intersection – Levels of Service**

<b>Morning Peak</b>	<b>LOS Existing</b>
Southbound Through	<b>E</b>

<b>Evening Peak</b>	<b>LOS Existing</b>
Eastbound Through	<b>E</b>
Westbound Left	<b>F</b>

It was also found in our analysis that by reducing the typical section from four lanes to three lanes, the limits of construction are reduced by approximately ten feet in width along both the north and south sides of the proposed State Boulevard. This reduction in construction limit width is not sufficient to significantly reduce impacts along the roadway. This configuration would result in impacts to approximately 15 residential properties within the Brookview-Irvington Historic District, the same as the preferred alternative.

As requested in your November 7, 2011, letter and per subsequent discussions in our June 20, 2012, meeting, alternate configurations for the connection into the Brookview neighborhood were explored.

One possible alternate connection was to extend Eastbrook Avenue to the proposed State Boulevard alignment. The skew of Spy Run Creek in conjunction with the length of the proposed bridge required to span the creek obstruct Eastbrook Avenue from connecting to the proposed State Boulevard roadway. The elevation of proposed State Boulevard near Eastbrook Drive would require the connection between existing State Boulevard and proposed State Boulevard to be constructed with a grade steeper than the 10 percent maximum grade allowed for a low speed urban street per the Indiana Design Manual (IDM). This design would be considered substandard and would contribute to safety concerns by not providing adequate sight distance to safely enter the proposed State Boulevard intersection.

Another possible configuration would be to extend Terrace Road to intersect the proposed State Boulevard alignment. The extension of this intersection would require construction near the midpoint of the State Boulevard mainline curve. In order to minimize impacts, the State Boulevard mainline curve at this location utilizes the maximum curvature allowed per IDM for low speed urban street criteria (460 feet). Constructing an intersection on the curve would increase potential safety concerns due to the geometry of State Boulevard at this location and inadequate sight distance that would not allow a motorist to safely negotiate the proposed intersection with State Boulevard. This scenario would also locate the Terrace Road intersection approximately 250 feet west of the Clinton Street intersection. The minimum intersection spacing per the IDM is 400 feet in order to facilitate traffic and avoid impeding traffic operations and efficiency at both intersections. Locating the intersection at Terrace Road would violate the IDM spacing requirement from the Clinton Street intersection and should be avoided.

After further analyzing and discussing alternate configurations to maintain a connection for the Brookview neighborhood to proposed State Boulevard, it is recommended that the proposed Oakridge Road extension remain as the preferred connection to State Boulevard.

During further discussion at our June 20, 2012, meeting, the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard was also explored. It was agreed that the designer would evaluate potential adjustments where possible to minimize impacts to these houses. The results of this effort will likely minimize impacts, allowing three houses once assumed to be total acquisitions to remain in place.

The following design modifications will be evaluated through the design process to minimize impacts to houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard by reducing the Oakridge Road typical section width:

- Eliminate boulevard roadway section for Oakridge Road
- Eliminate center curbed median and landscaping
- Shorten right-turn lane for vehicles exiting Oakridge Road west on State Boulevard

As agreed at the June 20, 2012, agency meeting, if after the design process is complete it is determined that the houses in question can remain in place, a firm commitment will be made to avoid complete acquisition of these parcels.

- ❖ **Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work.**

The existing State Boulevard bridge is hydraulically inadequate to accommodate a 100-year flood scenario and is an obstruction to water flowing through Spy Run Creek. Allowing this bridge to stay in place would also impede the hydraulic adequacy of the proposed State Boulevard bridge located immediately downstream. The existing concrete girder bridge is in poor condition and would continue to rapidly deteriorate due to continued Spy Run Creek flood events created from the backwater of the St. Mary's River. The close proximity of the western abutment of the existing bridge and the western abutment of the proposed bridge



would also make construction difficult. Due to the poor condition and required bridge hydraulics, it is recommended that the existing State Boulevard bridge be removed.

- ❖ **We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Schurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.**

Existing State Boulevard and Eastbrook Drive curbs and sidewalk will be left place where practicable as a mitigation effort for historic preservation. In addition, other potential mitigation measures were also discussed as part of the June 20, 2012, meeting, including:

- Photo documentation of the neighborhood and structures
  - Research and restore existing State Boulevard streetscape to replicate its look during the early 20<sup>th</sup> Century
  - Historical markers
  - Distribution locally of literature regarding the historical significance of the area
- ❖ **The revised alternative analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection?**

This suggestion was also discussed in detail at the June 20, 2012, meeting. If the mainline curve is shifted toward the Clinton Street intersection, the mainline State Boulevard roadway would shift slightly to the southeast from the proposed location. Currently, this proposed curve utilizes the maximum curvature allowed per the IDM for low speed urban street criteria and is located approximately 150 feet from the Clinton Street intersection. The proposed curve was designed at its current location in order to provide 150 feet of tangent roadway prior to the Clinton Street intersection, allowing for adequate sight distance to see both the traffic signals and also vehicles that may be stopped at the traffic signal. Relocating or shifting the roadway curve immediately adjacent to the intersection will likely result in increased safety concerns and increased accidents associated with the Clinton Street signal. Shifting the curve to the east would also require the proposed bridge over Spy Run Creek to be constructed at a greater skew, thus lengthening the bridge span and increasing project cost and stream impacts. Due to the associated safety and cost concerns, it is recommended that the mainline State Boulevard curve remain located per the current preferred alternative.

- ❖ **As we earlier stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultant of Ossian. Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, than an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.**

In reviewing the area previously surveyed by Archaeological Consultants of Ossian, it has been determined that there are in fact areas within the limits of the preferred alternative that have not been surveyed.

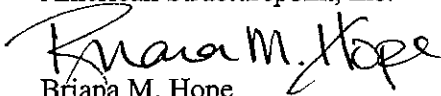
Archaeological Consultants of Ossian is currently performing an archaeological reconnaissance on the additional area and an archaeological report will be provided to your office as soon as it is available.

In addition to the items discussed at the agency meeting regarding the November 7, 2011, DHPA letter and the above responses, it was also determined that the next consulting parties meeting would likely be held in early September once FHWA felt it appropriate to issue a signed finding of adverse effect. The main purpose of the meeting will be to present potential mitigation measures to the consulting parties, as well as solicit additional potential mitigation concepts. The consulting parties will be notified of the date of the next consulting parties meeting concurrently with the distribution and request for comment on FHWA signed finding of adverse effect.

On behalf of Joyce Newland, Larry Heil (FHWA) also reported that the Advisory Council of Historic Preservation had not notified FHWA with a final decision in response to FHWA request for participation in the Section 106 process associated with the State Boulevard Project.

We hope that the information in this letter adequately addresses the concerns expressed in the November 7, 2011, DHPA letter. A copy of this response letter and a copy of the November 7, 2011, DHPA letter will be sent to all consulting parties concurrently with this letter. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at [bhope@structurepoint.com](mailto:bhope@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

  
Briana M. Hope  
Environmental Project Manager

BMH:mgn

Enclosures

Distribution List

Dr. James A. Glass, IDNR  
Angie Quinn, ARCH  
Jill Downs, ARCH  
Michael Galbraith, ARCH  
Don Orban, Fort Wayne Historic Preservation Commission  
Todd Zeiger, Indiana Landmarks  
Julie Donnell, Friends of the Parks of Allen County  
Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Dr. James L. Cooper  
Paul Brandenburg, Indiana Historic Spans Task Force  
Shan Gunawardena, City of Fort Wayne  
Susan Haneline, adjacent property owner  
Charlie Shirmeyer, Northside Galleries  
Karl Dietsch, Brookview Civic Neighborhood Association  
Dan Avery, NIRCC  
Suzanne Slick, Irvington park Neighborhood Association  
Annette "Jan" Dailey, adjacent property owner  
Joyce Newland, FHWA  
John Shoaff, Fort Wayne City Council  
Jason Kaiser, INDOT  
Patrick Carpenter, INDOT  
Mary Kennedy, INDOT  
Creager Smith, City of Fort Wayne  
Albert Cohen, Westbrook 5, LLC  
Thomas M. Niezer, Barrett & McNagny, LLP  
Ronald Ross, Marin Riley Architects and Engineers  
Dan Ernst, Earth Source, Inc.  
John Carr, IDNR  
Amy Johnson, IDNR  
Amanda Ricketts, IDNR  
Wade Tharp, IDNR  
Tom Cain, City of Fort Wayne  
Linda Weintraut, Weintraut & Associates  
Najah Duvall-Gabriel, ACHP  
Greg Smith, INDOT





AMERICAN  
**STRUCTUREPOINT**  
INC.

July 16, 2012

James A. Glass, Ph.D.  
State Historic Preservation Officer  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: Indiana Archaeological Short Report  
State Boulevard Improvements Project – Additional Area  
Fort Wayne, Allen County, Indiana  
DHPA No. 5903  
Des. No. 0400587  
Project No. 20071404

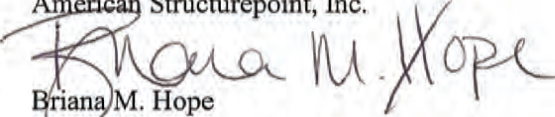
Dear Dr. Glass:

In reviewing the area previously surveyed by Archaeological Consultants of Ossian and brought to our attention in your November 7, 2011 letter, it has been determined that there were in fact areas within the limits of the preferred alternative for the proposed State Boulevard Improvements Project that had not been surveyed. Please find included with this correspondence the Indiana Archaeological Short Report, prepared by Archaeological Consultants of Ossian, for the additional area required for the State Boulevard Improvements project. The short report was reviewed and approved by the Indiana Department of Transportation, Cultural Resources on July 16, 2012.

The Phase I survey detected no properties that are eligible for nomination to the National Register of Historic Places. Since no historically or archaeologically significant sites will be impacted by the proposed undertaking, the Archaeological Consultants of Ossian recommended that project clearance be granted.

To facilitate the development of this project, you are asked to provide any comments you may have regarding this report within 30 days of the receipt of this letter. Please contact me at (317) 547-5580 or by email at [bhope@structurepoint.com](mailto:bhope@structurepoint.com) if you have any questions or require further information.

Very truly yours,  
American Structurepoint, Inc.

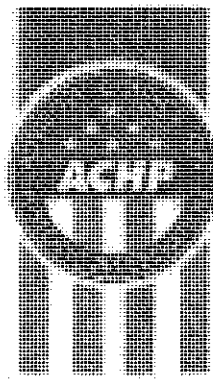
  
Briana M. Hope  
Project Manager

AMB:alo

Enclosures

cc: Shaun Miller, INDOT Cultural Resources





*Preserving America's Heritage*

July 31, 2012

Mr. Robert F. Tally, Jr.  
Administrator  
Federal Highway Administration  
Indiana Division  
575 N. Pennsylvania Street, Rm 254  
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Transportation Project  
Indiana*

Dear Mr. Tally:

On August 29, 2011, the Advisory Council on Historic Preservation (ACHP) received your letter requesting our participation in the Section 106 consultation for the reference undertaking. On September 22, 2011, we responded to your letter indicating that additional background information was needed in order to complete our review of the undertaking and determine our participation status. On May 10, 2012, the Federal Highway Administration (FHWA) provided the requested information to us and requested, again.

Since receiving the latest request from Indiana FHWA, we have spoken with FHWA staff and the State Historic Preservation Office (SHPO) regarding the timing and basis of your request that we participate in the Section 106 consultation. Based upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time. However, we are available to assist consulting parties regarding compliance with the Section 106 review process in accordance with Section 800.2(b)(2) of our regulations, "Protection of Historic Properties" (36 CFR Part 800).

Once the Indiana FHWA makes its effect determination for this undertaking, please notify the ACHP if you conclude that there is a finding of adverse effect. We will re-evaluate the undertaking once we receive your notification and advise you whether or not we have changed our decision regarding participation in consultation.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004

Phone: 202-606-8503 • Fax: 202-606-8647 • [achp@achp.gov](mailto:achp@achp.gov) • [www.achp.gov](http://www.achp.gov)

Thank you for contacting the ACHP regarding the consultation. Please don't hesitate to contact Najah Duvall-Gabriel at (202) 696-8585 or via email at [ngabriel@achp.gov](mailto:ngabriel@achp.gov) if you have any questions or concerns.

Sincerely,

A handwritten signature in cursive script, reading "Charlene Dwin Vaughn". The signature is written in dark ink and is positioned above the printed name.

Charlene Dwin Vaughn, AICP

Assistant Director

Office of Federal Agency Programs

Federal Permitting, Licensing and Assistance Section





Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739  
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



August 1, 2012

Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Your July 2, 2012 letter responding to our November 7, 2011 letter and to issues discussed at the June 20, 2012 agency meeting in Indianapolis regarding the State Boulevard Reconstruction Project (Des. No. 0400587; Project No. IN20071404; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your July 2 letter, which we received on July 3, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

Thank you for responding formally to the issues we had raised previously. We especially appreciate the FHWA's encouragement to the City of Fort Wayne and American Structurepoint to investigate the feasibility of preserving some of the houses on the south side of State Boulevard between Terrace Road and Eastbrook Drive and your firm's resulting proposal, on behalf of the City, to attempt to leave in place the houses at 112, 134, and 138 East State Boulevard. We think that if those houses can be avoided by this project, then the remaining segment of the original State Boulevard, as it passes through the Brookview-Irvington Park Historic District, will retain somewhat more of its historic character than it would if all houses on the south side of State Boulevard in that area were to be removed. We have similar hopes for the proposal to leave bypassed curbs and sidewalks in place.

We have no further comments or recommendations at this time for avoidance, minimization, or mitigation of this project's likely effects.

We have received your July 16, 2012 cover letter transmitting the archaeological short report (Stillwell, 7/11/12), which you advised us in your July 2 letter would be forthcoming. We will reply to your July 16 submission in a separate letter.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration  
Patrick Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Briana Hope, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.  
Larry Stillwell, Archaeological Consultants of Ossian, Inc.



Division of Historic Preservation & Archaeology 402 W. Washington Street, W274-Indianapolis, IN 46204-2739  
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



August 13, 2012

Briana M. Hope  
Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana archaeological short report (Stillwell, 7/11/12) of additional areas for the State Boulevard Reconstruction Project (Designation No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("SHPO") has conducted an analysis of the materials dated July 16, 2012, and received on July 16, 2012, for the above indicated project in the City of Fort Wayne, Allen County, Indiana.

Based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

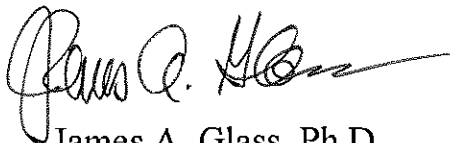
If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of



Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). If you have questions about buildings or structures please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,

A handwritten signature in black ink, appearing to read "James A. Glass". The signature is fluid and cursive, with a long horizontal stroke extending to the right.

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:WTT:wt

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Lawrence Heil, P.E., Indiana District, Federal Highway Administration  
Patrick A. Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Briana Hope, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.  
Larry Stillwell, Archaeological Consultants of Ossian, Inc.  
John L. Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources  
Wade T. Tharp, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources



AMERICAN  
**STRUCTUREPOINT**  
INC.

August 29, 2012

Linda Weintraut, Ph.D.  
Weintraut & Associates, Inc  
4649 Northwestern Drive  
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Dr. Weintraut:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street in Fort Wayne, Allen County, Indiana. The project is intended to improve corridor connectivity along State Boulevard for both motorists and pedestrians. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you were invited to be a consulting party to participate in the Section 106 process via mail with a letter dated March 23, 2009. In response to this invitation or as requested, we have recorded your wish to participate as a consulting party to assess the potential impact of this project on historic properties within or adjacent to the project area.

A Historic Properties Report (HPR) was completed in September 2009 by The Westerly Group, Inc. An Additional Information Report was completed in April 2012 by Weintraut & Associates, Inc. Two historic districts listed in the National Register of Historic Places (NR) were identified within the Area of Potential Effect: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). In addition, one historic property was also previously determined eligible for the NR within the Area of Potential Effect: Bridge over Spy Run (NBI No. 0200273).

Enclosed, please find a copy of the August 27, 2012, Federal Highway Administration (FHWA) Findings and Determinations required under procedures established for Section 106 consultation. The approval indicates FHWA concurs with the Area of Potential Effect, the Determination of Properties listed or Eligible for listing in the National Register of Historic Places, and the Determination of Adverse Effect for the proposed State Boulevard Reconstruction Project (DES No. 0400587). Included with this documentation is all correspondence regarding the proposed project and the Section 106 process, including all correspondence since the September 2011 Consulting Parties Meeting.

IN20071404



Linda Weintraut, Ph.D.

August 29, 2012

Page 2

Under the provisions of 36 CFR 800.5(c), the state historic preservation officer and all consulting parties have 30 days from the receipt of an adequately documented FHWA determination to take exception to that determination. You are asked as a consulting party to provide any comments or concerns you have regarding the FHWA Finding of Adverse Effect.

Please provide all comments to:

Ms. Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

Comments must be submitted no later than October 4, 2012. If you do not comment on the Adverse Affect finding by October 4, 2012, your concurrence will be assumed.

In order to initiate the Resolution of Adverse Effects process, we are inviting you to participate in a Consulting Parties Meeting scheduled for:

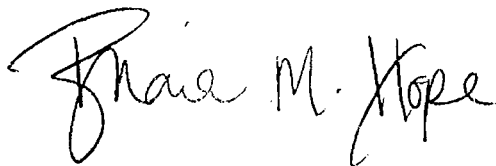
**Wednesday, September 19, 2012, at 10 a.m.**  
**Allen County Public Library, Meeting Room A**  
**900 Library Plaza, Fort Wayne, Indiana**

**The purpose of this meeting will be to discuss Section 106 and potential mitigation measures to offset the Section 106 finding of Adverse Effect.** An agenda for the Consulting Parties Meeting will be provided prior to the meeting date. Also enclosed is a copy of the draft Memorandum of Agreement (MOA) to be discussed at the Consulting Parties Meeting. Once the Consulting Parties Meeting is complete and all comments and suggestions have been compiled and taken into consideration, a revised MOA will be distributed to the MOA signatories, as well as provided to the consulting parties for informational purposes.

As you may recall from previous coordination and discussions, FHWA was considering elevating the required National Environmental Policy Act (NEPA) documentation from a Categorical Exclusion Level 4 (CE4) to and Environmental Assessment (EA). Due to the level of public interest associated with this project, FHWA has requested that the environmental document be elevated to an EA. By elevating the proposed project to an EA, a Finding of No Significant Impact (FONSI) will be required to conclude the NEPA process.

We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at [bhope@structurepoint.com](mailto:bhope@structurepoint.com). We look forward to meeting with you on Wednesday, September 19, 2012, at 10 a.m.

Very truly yours,  
American Structurepoint, Inc.



Briana M. Hope  
Environmental Project Manager

BMH:mgn

Enclosures



Page 3

Dr. James A. Glass, IDNR  
Angie Quinn, ARCH  
Jill Downs, ARCH  
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Todd Zeiger, Indiana Landmarks  
Julie Donnell, Friends of the Parks of Allen County  
Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Dr. James L. Cooper  
Paul Brandeburg, Indiana Historic Spans Task Force  
Shan Gunawardena, City of Fort Wayne  
Susan HanGine, Brookview Civic Neighborhood Association  
Charlie Shirmeyer, Northside Galleries  
Karl Dietsch, Brookview Civic Neighborhood Association  
Dan Avery, NIRCC  
Suzanne Slick, Irvington park Neighborhood Association  
Annette "Jan" Dailey, adjacent property owner  
Joyce Newland, FHWA  
Larry Heil, FHWA  
John Shoaff, Fort Wayne City Council  
Jason Kaiser, INDOT  
Patrick Carpenter, INDOT  
Mary Kennedy, INDOT  
Creager Smith, City of Fort Wayne  
Albert Cohen, Westbrook 5, LLC  
Thomas M. Niezer, Barrett & McNagny, LLP  
Ronald Ross, Marin Riley Architects and Engineers  
Dan Ernst, Earth Source, Inc.  
John Carr, IDNR  
Amy Johnson, IDNR  
Amanda Ricketts, IDNR  
Wade Tharp, IDNR  
Tom Cain, City of Fort Wayne  
Linda Weintraut, Weintraut & Associates  
Najah Duvall-Gabriel, ACHP  
Greg Smith, INDOT

September 14, 2012

Ms. Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

**Re:** State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

**To:** Ms. Briana M. Hope

**Fm:** Karl Dietsch, Resident of Brookview Neighborhood

**Cc:** Shan Gunawardena, City of Fort Wayne

**Subject:** Safety issue regarding lack of westbound turnoff lane on State Boulevard for Oakridge Road

Thank you for a copy of the memorandum of agreement received recently for the Spy Run to Cass Street, State Boulevard Reconstruction.

Based on the detailed street blueprints enclosed, I have an issue as follows:

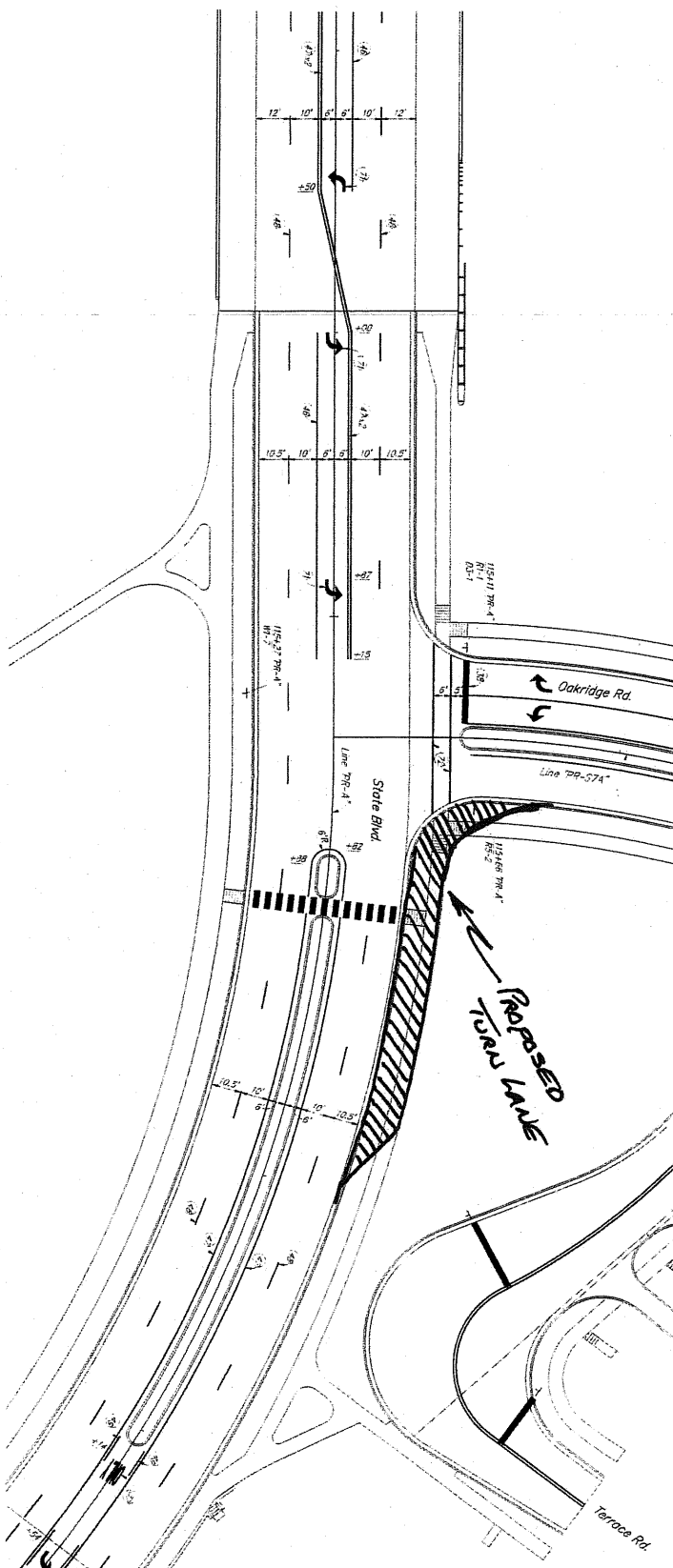
1. Westbound traffic on State Boulevard is lacking a separate turn lane for turning right (north) onto Oakridge Road. This is of concern because westbound traffic on State Boulevard is accelerating from the Clinton Street intersection at the same time some traffic is slowing to make a right turn into Oakridge Road.
2. This safety issue can easily be resolved by having a short turn lane as per my attached print.

Regards,

Karl Dietsch, Resident @ 2313 Oakridge Road  
Phone: (260) 484-1399

ATTACHMENT #1  
KARL DIETSCH

115



LEGEND			
1) Pavement Markings: Thermoplastic, Broad (overcast)	148	Line, Thermoplastic, Broken White, 4 in	17) Inverse Markings: Thermoplastic, Roundish Line, (Yellow) 12 in
158 Inverse Markings: Thermoplastic, Stop Line, White, 24 in	149	Line, Thermoplastic, Broken White, 4 in	178 Line, Thermoplastic, Round White, 4 in, 12" Line, 4" (Top)
166 Line, Thermoplastic, Solid White, 4 in	171	Forward Markings: Thermoplastic, Lane Reduction Arrow	179 Line, Thermoplastic, Round White, 4 in, 12" Line, 4" (Top)
171 Line, Thermoplastic, Solid Yellow, 4 in	172	Inverse Markings: Thermoplastic, Crosswalk Line, White, 6 in	
INDIANA DEPARTMENT OF TRANSPORTATION			
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"			
DESIGNED BY	DATE	CHECKED BY	DATE
ASU	DRANK	PD	
CHECKED	SAC	CHECKED	SAC



September 17, 2012

To Whom It May Concern,

We as the below listed residents of the Brookview Neighborhood currently reside within the proposed footprint of the E. State Blvd. widening project. We want to take this opportunity to make our voices heard. It seems the only people truly getting an opportunity to voice their opinions are those opposed to the project. We would like to be sure our voices in SUPPORT of the project are not being lost among the more vocal opposition by special interest groups, politicians and Brookview residents whose homes are NOT slated for removal.

Many of us listed on this letter have lived in our residences for many years. We have a very personal interest in this proposed project. Over the years we have seen a substantial increase in traffic along State Blvd. Multiple times throughout the day traffic comes to a standstill from the 300 block of West State Blvd to the 300 block of East State Blvd. This bottleneck makes it nearly impossible to exit our driveways or to enter onto State Blvd from intersecting streets within that area. As the city continues to grow and progress, we feel this issue will only continue to get worse. Those of us on the south side of the 100 block of E. State Blvd also have to contend with large trucks running up off of the curb and driving along our sidewalk / park strips. Because the road is so narrow and the curve is so sharp they do not stay on the paved surface. Listening to the tractor trailers bounce back down onto the pavement has become a constant sound at all hours of the day and night. Add to this the sound of squealing tires as cars try to take the curve at to high a rate of speed and it makes it difficult to enjoy ones time at home.

As residents of this area of State Blvd. and Eastbrook Dr, our quality of life has been greatly affected by the multiple flooding instances that occur yearly in this area. We have to depend on the City of Fort Wayne to place 18"- 36" clay dikes in front of our houses to prevent flood waters from overtaking our residences. These dikes, although necessary... prevent us access to our driveways and property. This leaves us trying to find an alternate location for our vehicles along side streets. Often times our vehicles are stranded in our garages and driveways because we were unlucky enough not get them moved before the thousands of pounds of clay were trucked in. Once the flooding has subsided, we must contend with the mess left behind while the dikes sit and await removal. The issues however are not resolved, even when the clay is taken away. After weeks of the clay sitting in flood water, once removed we are left with massive amounts of mud and debris in the street, our yards and driveways. This flooding halts traffic along one of the cities major east / west streets for days and weeks at a time. This causes an inability of people who live in the area to get to work and take their children to school, not to mention public transportation, school busses and emergency vehicles must also find ways to function when this major artery ceases to be passable. These issues with flooding have, we believe, caused a decrease in our property values and undue financial hardship. Frankly, no one should have to live like this year after year. The fact that the city is trying to address this issue with the State Blvd. widening project gave us hope that a solution was forthcoming. Now... years later it seems this project has become more about personal agendas and political attention then it has about solving these problems. You cannot imagine the stress these delays continue to cause those of us who are most intimately affected. We believe allowing our houses to remain under the guise of historic preservation and community support only makes our problem worse.

The roadway has already been raised for new bridge on Clinton St. south of State Blvd. so it seems counterproductive not to raise the bridge at Spy Run Creek. By delaying or even stopping the State Blvd. project, the lower roadway/bridge on State Blvd. between Westbrook and Eastbrook will only cause flooding in the area to worsen.

It simply does not make sense to not move the road to the south and install a new bridge with the same surface height as the new bridge on Clinton Street. Again, special interest groups, residents outside the footprint for property buyouts and local politicians seems to have gotten so caught up in their own agendas that many of us feel our concerns and support are being drowned out.

We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated and widened to the south of its current location. We also support the removal of the old bridge at Spy Run Creek and the constructing of a new bridge on the relocated State Blvd. We believe that the city / engineers have taken great care with the proposed widening project plans and have the best interest of not only the City of Fort Wayne, but we as the residents who live within the actual footprint of the project. Please do not let those of us desperate for a resolution to these problems continue to be unheard, ignored or worse yet, used as pawns in some sort of political grandstanding.

Please note that of the existing 13 homeowners on Eastbrook and along the south side of E. State Blvd. (between Eastbrook and Clinton), the vast majority want to be listed on this letter as supporting the project and speedy property buyouts. We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issues unresolved.

Again, those of us in favor of the project and the buy out of our homes do not believe that our interests and the interests of the City Of Fort Wayne are being served by delaying or trying to stop this project. We respectfully ask that this project proceed in a timely manner.



Sincerely,

Susan R. Haneline 134 E. State Blvd.

Moshin & Karen Hudda 112 E. State Blvd.

Rex Well, NLI Inc. 128 E. State Blvd.

Lisa Strebig 138 E. State Blvd.

Darrin Klopp 142 and 146 E. State Blvd.

Michael McKinley 2252 Eastbrook Dr.

Peggy S. Roy 2248 Eastbrook Dr.

Joe and Amy Sale 2244 Eastbrook Dr.

Stephanie Mays 2240 Eastbrook Dr.

**\*Attachments: 3**

- 1. - A photo map with numbers designating homes of persons mentioned in or signing this letter.**
- 2. - 3 pages with signatures or verbal requests to be listed in favor of property buyouts.**
- 3. - Summary and 2 typed pages with property owners information - this corresponds with the above signatures and photo map**



2

© 2012 Google

Google earth



**In summary, 9 out of the 13 property owners are in favor of being bought out. (There are 14 properties shown in this document, but 1 owner has two properties)**

**Of the 4 remaining properties:**

1 property is vacant - no owner information available

1 property is a rental, no landlord contact info. available

1 homeowner does not wish to be bought out

1 home owner was unreachable therefore we are unable to formally list whether they are for or against buyouts.

## **EASTBROOK DRIVE**

### **2236 Eastbrook Drive**

Owner: **KRISTIN A CREAGER & LOIS JEAN LOCKHART**

**House 1 on the Map**

\* Spoke with a male at the residence, he stated he does not wish to sign paper in favor of buyout

### **2240 Eastbrook Drive**

Owner: **ERIKA E MAYS MERRICK & STEPHANIE G MAYS**

260-471-1367 or 260-341-3988

**House 2 on the Map**

\* Signed letter in favor of buyout.

### **2244 Eastbrook Drive**

Owner: **Resource Network Inc Indiana - Joe Sale**

**House 3 on the Map**

\* Signed letter in favor of buyout.

### **2248 Eastbrook Drive**

Owner: **PEGGY S ROY**

260-493-1495

**House 4 on the Map**

\* Signed letter in favor of buyout.

### **2252 Eastbrook Drive**

Owner: **MICHAEL GARY MCKINLEY**

**House 5 on the Map**

\* Spoke with Michael McKinley via telephone on 9/18/12, he made a verbal request to be included in these documents of owners supporting buyouts.



## **East State Blvd**

### **112 E State Boulevard**

Owner: **MOSHIN A HUDDA & KAREN S HUDDA**

260-484-5244

#### **House 6 on Map**

\* Signed letter in favor of buyout.

### **128 E State Boulevard**

Owner: **NLI INC**

**Rex Wells - Verbal signature given on 9/18/2012 via phone**

NLI INC / Alliance Property Management

1616 N Harrison St

Fort Wayne, IN 46808 (260) 420-1428

#### **House 7 on Map**

\*\* Rental Property,

\* Spoke with rep. of NLI INC, owner Rex Wells made a verbal request (via telephone) to be included in these documents of owners supporting buyouts.

### **134 E State Boulevard**

Owner: **SUSAN R HANELINE**

260-482-2200 or 580-2447

#### **House 8 on Map**

\* Signed letter in favor of buyout.

### **138 E State Boulevard**

Owner: **LISA S STREBIG**

260-760-1956

#### **House 9 on Map**

\* Signed letter in favor of buyout.

### **142 E State Boulevard**

Owner: **DARRIN D KLOPP**

#### **House 10 on Map**

\* Verbal request on letter in favor of buyout, contact phone number listed.

### **146 E State Boulevard**

Owner: **DK PROPERTIES LLC**

#### **House 11 on Map**

\* Verbal request on letter in favor of buyout, contact phone number listed.

**154 E State Boulevard**

Owner: **NORMA R L/EST PANKOP & PANKOP TRS**

**House 12 on Map**

This house is a vacant property - no information available on owner

**158 E State Boulevard**

Owner: **DANIEL J HALL & KAREN S HALL**

**\*\* House 13 on Map**

This is a rental, no landlord contact information available.

**162 E State Boulevard**

Owner: **ROBERT E DAILEY & JANICE ANNETTE DAILEY**

(260) 483-1303 or 483-6316

**\*\* HOUSE 14 on Map**

This homeowner is a member of the Consulting Parties. Attempts to contact were unsuccessful, so no opinion for or against the buyout is given.

Name of Resident: Moshin & Karen Hudda  
Address of Residence: 112 E. State Blvd  
Number of people residing in the home: 2 Number on Attached Map 6  
Signature: Karen Hudda, Moshin A Hudda  
Contact Phone Number: 260-484-5244  
Additional Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Name of Resident: Joe & Amy Solo  
Address of Residence: 2244 East Brook Drive  
Number of people residing in the home: 4 Number on Attached Map 3  
Signature: Joe Solo  
Contact Phone Number: \_\_\_\_\_  
Additional Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Name of Resident: Stephanie Mays  
Address of Residence: 2240 Eastbrook Drive  
Number of people residing in the home: 2 Number on Attached Map 2  
Signature: Stephanie Mays  
Contact Phone Number: 260 471-1367 341-3988  
Additional Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_



Name of Resident: Peggy S. Roy  
Address of Residence: 2248 Eastbrook Dr.  
Number of people residing in the home: 0 Number on Attached Map 4  
Signature: Peggy S. Roy  
Additional Comments: \_\_\_\_\_  
Contact Phone Number: 493-1495

\* Moved due to city's promise of property acquirement,  
Have been renting (at a loss) for 6 years. House  
has issues at much expense and finding tenants.  
is nearly impossible. I consider this to be a hardship.

Name of Resident: Lisa Strebig  
Address of Residence: 138 E. State  
Number of people residing in the home: \_\_\_\_\_ Number on Attached Map 9  
Signature: Lisa Strebig  
Contact Phone Number: 260-760-1956  
Additional Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Name of Resident: Susan R. Haneline  
Address of Residence: 134 E. State Blvd  
Number of people residing in the home: 2 Number on Attached Map 8  
Signature: Susan R. Haneline  
Contact Phone Number: \_\_\_\_\_  
Additional Comments: Author of Attached Letter  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

Name of Resident: NLI INC.  
Address of Residence: 128 E. State Blvd  
Number of people residing in the home: \_\_\_\_\_ Number on Attached Map \_\_\_\_\_  
Signature: Verbal Request By Rex Wells of NLI INC to be added  
Additional Comments: \_\_\_\_\_

Contact Phone Number: 420-1428

Contacted NLI 1616 N Lincoln FFWAYE IN.  
Rep Rex Wells wants to be added to this document.  
Request via telephone @ 9/18/2012 - may call him to verify if needed

Name of Resident: Michael McKinley  
Address of Residence: 2252 Eastbrook  
Number of people residing in the home: \_\_\_\_\_ Number on Attached Map 5  
Signature: Verbal request by phone to show supporting buyout  
Contact Phone Number: 443-3219  
Additional Comments: I spoke with Mr McKinley he wished  
to be listed on the residents in favor of a buyout  
confereration on 9/18/12 @ 9pm

Name of Resident: DARIN KLOPP  
Address of Residence: 142 E - STATE BLVD.  
Number of people residing in the home: \_\_\_\_\_ Number on Attached Map 11 + 10  
Signature: Verbal Request by phone to show supporting Buy out  
Contact Phone Number: 249-7606  
Additional Comments: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_



bethany w &lt;bethany@weintrautinc.com&gt;

## Fwd: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

1 message

Linda Weintraut &lt;linda@weintrautinc.com&gt;

Fri, Sep 21, 2012 at 8:42 AM

To: bethany w &lt;bethany@weintrautinc.com&gt;

----- Forwarded message -----

From: **Hope, Briana** <bhope@structurepoint.com>

Date: Tue, Sep 18, 2012 at 4:12 PM

Subject: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

To: "Hope, Briana" <bhope@structurepoint.com>, "mgalbraith@archfw.org" <mgalbraith@archfw.org>, "aquinn@archfw.org" <aquinn@archfw.org>, "don.orban@cityoffortwayne.org" <don.orban@cityoffortwayne.org>, "tzeiger@indianalandmarks.org" <tzeiger@indianalandmarks.org>, "mbwedaman@frontier.com" <mbwedaman@frontier.com>, "juliemarie57@earthlink.net" <juliemarie57@earthlink.net>, "jlcooper@ccrtc.com" <jlcooper@ccrtc.com>, "indianabridges@sbcglobal.net" <indianabridges@sbcglobal.net>, "shan.gunawardena@cityoffortwayne.org" <shan.gunawardena@cityoffortwayne.org>, "dan.avery@co.allen.in.us" <dan.avery@co.allen.in.us>, "sjslick@mac.com" <sjslick@mac.com>, "jandailey59@msn.com" <jandailey59@msn.com>, "joyce.newland@fhwa.dot.gov" <joyce.newland@fhwa.dot.gov>, "larry.heil@fhwa.dot.gov" <larry.heil@fhwa.dot.gov>, "jshoaff@proparkwest.com" <jshoaff@proparkwest.com>, "jasonkaiser@indot.in.gov" <jasonkaiser@indot.in.gov>, "pacarpenter@indot.in.gov" <pacarpenter@indot.in.gov>, "mkennedy@indot.in.gov" <mkennedy@indot.in.gov>, "linda@weintrautinc.com" <linda@weintrautinc.com>, "creager.smith@cityoffortwayne.org" <creager.smith@cityoffortwayne.org>, "albertcohan@aol.com" <albertcohan@aol.com>, "tmn@barrettllaw.com" <tmn@barrettllaw.com>, "rross@martin-riley.com" <rross@martin-riley.com>, "dan@earthsourceinc.net" <dan@earthsourceinc.net>, "jglass@dnr.in.gov" <jglass@dnr.in.gov>, "jcarr@dnr.in.gov" <jcarr@dnr.in.gov>, "kdietsch@comcast.net" <kdietsch@comcast.net>, "alec.johnson@ci.ft-wayne.in.us" <alec.johnson@ci.ft-wayne.in.us>, "Crites, Scott" <SCrites@structurepoint.com>, "Zielinski, Rich" <RZielinski@structurepoint.com>, "gsmith2@indot.in.gov" <gsmith2@indot.in.gov>, "aricketts@dnr.in.gov" <aricketts@dnr.in.gov>, "wtharp1@dnr.in.gov" <wtharp1@dnr.in.gov>, "tom.cain@cityoffortwayne.org" <tom.cain@cityoffortwayne.org>

Consulting Party Members –

Attached to this e-mail is a copy of the letter sent to the SHPO's Office documenting the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

A hardcopy of the this letter has been placed in the mail. In addition, the information presented in the letter will be discussed as part of tomorrows (Sept. 19<sup>th</sup>) Consulting Parties Meeting.

Thank you,

Briana



---

**Briana M. Hope**

Project Manager, Environmental Sciences Group

7260 Shadeland Station, Indianapolis, Indiana 46256

T 317.547.5580 E [bhope@structurepoint.com](mailto:bhope@structurepoint.com)

F 317.543.0270 W [www.structurepoint.com](http://www.structurepoint.com)



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**IN20071404.EV.2012-09-18.LTR.ROW Minimization.bmh - Copy.pdf**

400K



AMERICAN  
**STRUCTUREPOINT**  
INC.

September 18, 2012

James A. Glass, PhD  
Deputy State Historic Preservation Officer  
Division of Historic Preservation & Archaeology  
Indiana Department of Natural Resources  
402 West Washington Street, W274  
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to document the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

After completion of the proposed project, the three existing structures would be located between the existing and proposed State Boulevard roadways. Due to the location of the structures, investigations were needed to assess the impacts to the properties and feasibility of maintaining the existing structures between the existing and proposed roadways.

**Minimization of Property Acquisition**

In order to minimize acquisition of property associated with these homes, American Structurepoint, Inc., investigated options that evaluated modifications to the Oakridge Road extension to proposed State Boulevard. The first included shortening the right-turn lane and eliminating the landscaped median. This modification provided a reduction in anticipated right-of-way of approximately six feet. Constructing sidewalks adjacent to the curb with retaining wall placed at the back of sidewalks also reduced the anticipated right-of-way by an additional 16 feet.

The use of guardrail was also evaluated in an effort to minimize potential right-of-way acquisition. Currently, the proposed design utilizes the required 4:1 side slope from the proposed State Boulevard roadway, encroaching on the back yards of the homes in question. The roadway through





this area is elevated due to the need of the proposed roadway to tie into a larger bridge required to span the floodplain of the Saint Mary's River. To utilize guardrail would allow for the construction of a 3:1 side slope, resulting in a minor reduction of required right-of-way, but would ultimately be offset by the requirement of a flat shelf needed for installation of the guardrail at the top of the slope immediately adjacent to the roadway. In addition, utilization of guardrail would pose a safety concern for vehicles making a left-hand movement from Oakridge Drive to proposed State Boulevard by introducing a visual obstruction to the west.

### **Stormwater Drainage**

In order to facilitate stormwater drainage adjacent to the homes, a drainage swale will be necessary in all options. The drainage swale will be constructed at the bottom of the proposed roadway fill slope just south of the houses in question. The drainage swale will collect stormwater runoff from both the proposed roadway and the adjacent properties located to the north. The flow will be conveyed west outletting into Spy Run. Currently, the stormwater drainage system is proposed as a vegetated drainage swale.

The option of an enclosed drainage system utilizing inlets was evaluated in this area, but would still require a vegetated swale above the enclosed system to direct water to the inlets. The enclosed drainage system did not provide a reduction of anticipated right-of-way.

Please see attached exhibits showing the anticipated impacts resulting from the proposed construction/right-of-way limits. The distance between each residence and the proposed construction limits/right-of-way is marked, as well as the anticipated elevation differences. The most significant elevation difference of seven feet exists at 112 East State Boulevard and decreases to three feet near 138 East State Boulevard.

### **Recommendation**

The evaluation found that the properties would be significantly impacted by construction of the proposed roadway and drainage swale.

- 112 East State Boulevard – The property would be located approximately 7.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately eight feet from the existing residence. Approximately 62 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.
- 134 East State Boulevard – The property would be located approximately 3.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately ten feet from the existing residence. Approximately 55 percent of the existing backyard/greenspace between the house and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building and significant portion of the existing yard/greenspace would be included in the area to be acquired.

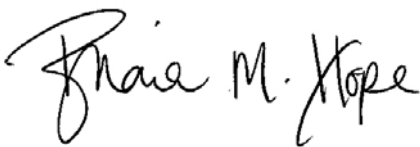
- 138 East State Boulevard – The property would be located approximately three feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately three feet from the existing residence. Approximately 77 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalk, and drainage swale. The existing garage, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.

It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions. The significant reduction in greenspace between the existing residence and proposed roadway, impacts to existing drives, and removal of non-residential structures located on the properties is appropriate justification for the complete acquisition of the parcels in question.

In additional coordination with the Indiana Department of Transportation, the Right-of-Way and Land Acquisition Section advised American Structurepoint representatives that as part of state and federal law, land cannot be purchased from one property owner and given to another to offset the amount of land being acquired. Therefore, the impacts to one parcel cannot be offset by acquiring an adjacent property and giving or selling that acquired property to an adjacent owner. The land acquired from the parcels in question would result in a significant permanent reduction of property and green space.

We hope that the information in this letter adequately resolves the requested investigation of options to minimize impacts at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. This letter is intended for informational purposes only. A copy will be sent to all consulting parties concurrently with this correspondence. If you have any additional questions, please do not hesitate to contact me at (317) 547-5580, or at [bhope@structurepoint.com](mailto:bhope@structurepoint.com).

Very truly yours,  
American Structurepoint, Inc.

A handwritten signature in black ink that reads "Briana M. Hope". The signature is written in a cursive, flowing style.

Briana M. Hope  
Environmental Project Manager

BMH:mgn

Enclosures

Distribution List

Dr. James A. Glass, IDNR  
Angie Quinn, ARCH  
Jill Downs, ARCH  
Michael Galbraith, ARCH  
Don Orban, Fort Wayne Historic Preservation Commission  
Todd Zeiger, Indiana Landmarks  
Julie Donnell, Friends of the Parks of Allen County  
Michelle Briggs-Wedaman, Brookview Neighborhood Association  
Dr. James L. Cooper  
Paul Brandenburg, Indiana Historic Spans Task Force  
Shan Gunawardena, City of Fort Wayne  
Susan Haneline, adjacent property owner  
Charlie Shirmeyer, Northside Galleries  
Karl Dietsch, adjacent property owner  
Dan Avery, NIRCC  
Suzanne Slick, Irvington park Neighborhood Association  
Annette "Jan" Dailey, adjacent property owner  
Joyce Newland, FHWA  
John Shoaff, Fort Wayne City Council  
Jason Kaiser, INDOT  
Patrick Carpenter, INDOT  
Mary Kennedy, INDOT  
Creager Smith, City of Fort Wayne  
Albert Cohen, Westbrook 5, LLC  
Thomas M. Niezer, Barrett & McNagny, LLP  
Ronald Ross, Marin Riley Architects and Engineers  
Dan Ernst, Earth Source, Inc.  
John Carr, IDNR  
Amanda Ricketts, IDNR  
Wade Tharp, IDNR  
Tom Cain, City of Fort Wayne  
Linda Weintraut, Weintraut & Associates  
Najah Duvall-Gabriel, ACHP  
Greg Smith, INDOT





Eastbrook Dr.

Carriage Rd

Existing State Blvd.

Terrace Rd.

Elev. 756

Elev. 757

Elev. 755

Elev. 755.5

Elev. 762.5

Elev. 759

State Blvd.

Construction Limits/  
Right-of-Way

Construction Limits/  
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Construction  
Limits/Right-of-Way

Construction  
Limits/Right-of-Way





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## MEETING MINUTES

**Location:** Allen County Public Library, 900 Library Plaza, Meeting Room A, Fort Wayne, Indiana

**Date:** September 19, 2012

**Project Name:** State Boulevard Reconstruction (Des. No. 0400587)

**Project No.:** IN20071404

**Consulting Party** Rich Zielinski, Scott Crites, Briana Hope, Chris Meador (American Structurepoint, Inc.)

**Attendees:** Shan Gunawardena, Creager Smith, Tom Cain (City of Fort Wayne)  
Dr. Linda Weintraut (Weintraut & Associates)  
Dr. James Glass, John Carr (IDNR, Division of Historic Preservation and Archaeology)  
Patrick Carpenter (INDOT, Cultural Resources)  
Jason Kaiser, Greg Smith (INDOT Fort Wayne District)  
Joyce Newland, Larry Heil (Federal Highway Administration)  
John Shoaff (Fort Wayne City Council)  
Suzanne Slick (Irvington Park Neighborhood)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)  
Michael Galbraith, Jill Downs (ARCH, Inc.)  
Michelle Briggs-Wedaman (Brookview Neighborhood Association)  
Susan Haneline, Karl Dietsch (Adjacent Property Owner)  
Todd Zeiger, Catherine Wright (Indiana Landmarks)  
Edward Welling (Friends of the Parks of Allen County)  
Dan Ernst (Earth Source, Inc.)

**Conference Line:** Najah Duvall-Gabriel (Advisory Council on Historic Preservation)

**Attendees** Frank Saurez (City Public Works), Marty Bender (FWPD/City Council), Shawna  
**Observing** Nicelley, Larraine Weier, Herb Weier, Thomas Roach III, Sarah Krugen Geyman  
**Meeting:**

1. The meeting was held at 10:00 a.m., September 19, 2012, to discuss the following agenda items:
  - 1) Section 106 Update
  - 2) Section 106 Action Items regarding Adverse Effect Finding
  - 3) Additional Mitigation Measures
2. Briana Hope introduced herself and began the meeting with introductions around the room and by stepping through the first item on the agenda, including an update on project progress since the last consulting party meeting (09/2011).
3. Dr. Linda Weintraut presented a PowerPoint presentation briefly recapping the Section 106 process, including identified properties, minimization and avoidance measures, effects, and potential mitigation measures.

4. Susan Haneline asked during the avoidance and minimization portion of the PowerPoint presentation why the owners of the 3 residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes, as they do not want to remain. She thought the property owners should have a say as to whether or not the impact to their property is significant enough to justify leaving the house in place.
  - Larry Heil responded that Section 4(f) requires evaluation of measures to avoid and minimize impacts to the historic district. The evaluation is focused on the historic resources protected by the law. Any time right-of-way is acquired the property owner is reimbursed for the impact to the property, but FHWA is required by law to minimize impacts to the historic district. If there is a way to preserve contributing resources, which the three homes in questions are, FHWA is required to preserve these resources.
5. Following the PowerPoint presentation, Briana Hope opened up the meeting for general discussion regarding the potential mitigation measures proposed and any additional ideas regarding potential mitigation.
6. Michelle Briggs Wedaman stated that she will be providing extensive written comments related to the materials sent with the invitation to the consulting parties meeting, but generally had the following comments:
  - Related to the agenda, feels it is inappropriate to have moved beyond the discussion of purpose and need.
  - Understand timelines and agency requirements but feels the larger issues of real time and real impact on the community and are not guided by the timelines that dictate quick movement on the project but by the guiding principles of the Plan-It Allen report, the comprehensive plan for Allen County. She will be commenting on the relevance of the document in the submitted comments.
  - Context Sensitive Solutions (CSS) – CSS should be applied to the larger issue of the road footprint itself and should occur at the beginning of the project not towards the end of the project or the last stage of a project.
  - The neighborhood supports Sue Haneline and the most directly impacted residents. The neighborhood was not invited to the agency meeting in June and not included in the decisions that were made at the meeting potentially impacting the Oakridge extension and the effort being made to preserve the homes.
  - Encouraged agencies and project sponsors to consider what the final project will look like and to reconsider the outcome of the project and forcing residents to stay.
7. Todd Zeiger had the following comments:
  - Encourages the Advisory Council of Historic Preservation (ACHP) to be involved now and moving forward.
  - Concurs with “adverse effect” but feels it is not complete. Concerned about bifurcation of the district by creating a visual dissection of the neighborhood and district both height-wise and width-wise. The bifurcation needs to be discussed in the effects.
    - Dr. Linda Weintraut stated that the effects document will be updated to include the bifurcation of the historic district.
  - Feels that impacts to individual resources have been minimized by the City of Fort Wayne in what he believes is anticipatory demolition as part of a flood control project. The individual demolitions are directly related to the project because one of the purposes of the project is stated as flood control issues. This is not discussed in official findings and should be.



- Will be responding in writing by October 4<sup>th</sup> and encourages that before ACHP makes its formal decision that the written comments are included and considered and not just the conversation from the consulting parties meeting.
  - In the letter announcing the consulting parties meeting it states that FHWA is elevating the project to full Environmental Assessment (EA). Questioned why mitigation was being discussed when the full impacts that are going to be discussed in the EA are unknown.
    - Larry Heil responded that when a project has some sensitivity to it FHWA prefers to issue a definitive decision and a Categorical Exclusion (CE) by definition is a type of project excluded from the requirement of NEPA documentation. Because of the sensitivity and public concerns, FHWA wanted to have an official FHWA NEPA decision. The next steps in the EA process include finalizing a Draft 4(f) Analysis, revising the Draft MOA, and finalizing data collection to compile a Draft EA Document to present to the public. Once the Draft EA is released for public involvement, a public hearing will be held and the public will be encouraged to comment and present facts or clarify that facts are inaccurate from a technical standpoint.
8. Karl Dietsch pointed out a safety concern regarding traffic traveling west on new State Boulevard. Traffic will be picking up speed at Oakridge as it is heading west at the same time traffic making a right turn on Oakridge will be slowing down, thus increasing the risk of rear-end accidents. A short right turn lane was suggested for westbound traffic along State at Oakridge Road.
  9. Tom Cain pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in those landscape elements in the documentation need to be discussed. The visual and spatial components of the larger landscape need to be understood so they can be addressed in a mitigation discussion.
  10. Michael Galbraith also stated that he would be providing detailed comments by October 4<sup>th</sup> and had the following comments:
    - Discussion of mitigation and minimization is a red herring to avoid discussing the issues that have been brought-up and not discussed by the consulting parties.
    - Minimization efforts documented and sent out via e-mail late the evening prior to the consulting parties meeting did not give consulting party members adequate time for review.
      - Larry Heil stated that the letter did not represent FHWA's position.
    - Raised concern about a new Section 106 consultant with brand new information and being able to adjust to Dr. Weintraut as a consultant.
    - Based on the PowerPoint presentation and the May 19<sup>th</sup> letter from American Structurepoint to Dr. Glass, Mr. Galbraith feels the APE is still too small for the project.
    - Encourages ACHP to consider that this project separates and segregates projects happening in the same geographical location and same time period impacting the same neighborhood and separates them based on agencies. They should be considered amalgamated for review due to their cumulative impacts on the neighborhood. A total of eight federal aid projects are happening in the same area at the same time.
  11. Briana Hope reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed that the consulting parties meeting was an opportunity to verbally express their mitigation comments and ideas.
  12. Michelle Briggs Wedaman expressed the following additional comments:
    - Has not received a traffic study for the area that has been requested since the beginning of the project. How is the need for this project (safety and flooding) documented without a traffic

study? Have issues with the safety and congestion part of the Purpose and Need, specifically related to accidents at intersections, been studied? How will this project calm traffic?

- Larry Heil indicated that FHWA provides funds to help Major Metropolitan Planning Organizations (MPOs) develop reliable traffic models. FHWA reviews the traffic models every four years to evaluate the models. FHWA has a high degree of confidence in the models and utilizes these models around the country to aid in project development.
- 2005 Flood Study is at odds with the project and in no way points to this project as a solution to real life flash flooding.

13. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed her concern that the consulting party members were losing the opportunity to have input related to mitigation.

14. John Shoaff commented that he believes the purpose of the proposed project is to make a major arterial out of a street that runs through a number of neighborhoods. Arterials do not have a good safety record related to pedestrians. Mr. Shoaff also made the following comments:

- Presented a letter signed by 15 neighborhoods opposing the project and stated that the letter would be included as part of his comments submitted by October 4<sup>th</sup>.
- Stated that the majority of the public does not agree with the Purpose and Need Statement.
- The historical aspects of the neighborhood are great and will be destroyed by this project.
- Concerned about the project having a negative impact on property values.

15. Michael Galbraith reiterated his concern regarding the multiple federal projects involved in the neighborhood that are not be looked at and evaluated cumulatively.

16. Edward Welling agrees with the adverse effect but feels that the discussion of mitigation is premature and that the proposed MOA is an attempt to confuse the process, especially since the FHWA elevated the project to an EA. Mr. Welling also had the following comments:

- Asserted that the APE is not appropriate.
- Turning the roadway into a major arterial and the addition of traffic will impact the quality of life along the corridor.
- Suggested that the discussion of mitigation should be postponed until EA is complete.

17. Susan Haneline commented that 14 houses in the initial footprint were under the impression that the project was not just about traffic flow but also related to flooding. She also had the following comments:

- Every house in the footprint of the project has been impacted by flooding.
- Presented a letter documenting the vast majority of owners in the footprint are requesting a buyout due to loss of property, traffic flow issues, inability to access property, flooding, and financial hardships.
- Feels that being listed in a historic district is making it harder for the city and state to address the concerns of the people in the footprint of the proposed project.
- The majority of the home owners in the footprint of the project were grateful for the opportunity presented by this project to vacate their properties, open up a green space, and retain the original footprint of State Boulevard, but dealing with an elevated roadway just for the city to protect contributing properties is not a long-term solution.
- Does not oppose project and looks at it as an opportunity to not continue to lose value in properties.

- Requested agencies and project sponsors to not forget to put a face with the people that are in the footprint and intimately dealing with the flooding issues and not being able to get out of their driveway because of traffic.
18. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation.
19. Tom Cain commented that preservation of historic buildings depends of the viability of the properties to remain invested in, in the future. He had the following supporting comments:
- Homes that are in a floodplain have limits on how much can be invested in them, their future preservation may not be ensured.
  - The Section 106 process may preserve buildings but it may also create a condition unattainable for preservation and economic life and existence as a structure.
  - The process of preserving specific structures may impact the need to provide mitigation for the larger landscape and planning characteristics of the neighborhood.
  - The preservation of several specific structures may not ensure long term preservation and limit opportunities to mitigate larger landscape design issues which are the more significant components of the neighborhood based on the Arthur Shurcliff plan for the area.
  - Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. The two designers involved, George Kessler and Shurcliff, had differing approaches to composition of roadways, one was a more formal straightforward boulevard with setbacks, and tree lined streets and the other a more curvilinear pattern.
20. Dr. Linda Weintraut commented that the National Historic Preservation Act was established so that historic properties and modern undertakings could exist in harmony; one was not to be at the expense of the other. We are trying to come up with a compromise that allows both to move forward. Dr. Weintraut again encouraged the consulting parties to think about mitigation and how that might offset the adverse impacts of the undertaking.
21. Michael Galbraith commented that he agrees that the Section 106 process was designed to allow historic properties and transportation projects to live in harmony, but what is happening to the neighborhood is not harmonious. This neighborhood is being destroyed by a combination of federal projects that are being executed piecemeal that have destroyed dozens of houses. If the project proceeds it will destroy dozens more, property values, and the historic resource in the process.
22. Dr. James Glass commented that he recalled at previous consulting party meetings some very strong opinions were voiced similar to the ones heard today and that there still does not seem to be a degree of consensus from the community about the project. He stated that presumably the City of Fort Wayne very strongly supports the project, but also heard John Shoaff (City Council Member) express a very different point of view, as well as differing points of view from the neighborhood. He also had the following comments:
- One issue the community needs to consider is: can a consensus be developed on this project. From a section 106 perspective, the State Historic Preservation Office (SHPO) depends on the project sponsor and the community to develop some kind of consensus on the need of the project and whether all the issues in terms of feasibility have been worked out. SHPO then takes that as a starting point for considering effects on historic properties and ways to mitigate and lessen adverse effects.
  - In June, SHPO had a very preliminary meeting with the agencies to consider what the anticipated project design was based on the safety factors and consideration of alternatives. The meeting



was to provide a basis for a starting point at the consulting parties meeting to try to begin a discussion on mitigation to offset some major adverse effects.

- The meeting today is an invitation for consulting party members to provide additional mitigation ideas or alternative ideas.
- The letter yesterday was sort of a wildcard that no one anticipated. We were waiting for this discussion to bring forth the idea on the terms of the 3 houses and the many significant adverse effects. We would welcome any additional ideas for mitigation.

23. Larry Heil commented that it is critical to point out how federal funds are spent in this area. The decisions are made by the Northeastern Indiana Regional Coordinating Council (NIRCC) Policy Board. There is a reason that MPOs are established and required by federal law. These are regional issues and it is within that context that the regional plan that they identify what the arterials are in the region. State Boulevard is and has historically been an arterial. The plan that is developed and adopted by all the officials from the entire region is what guides all investment in the area. None of the decisions are made purely by City of Fort Wayne staff or one or two elected officials. They are made by the policy board which is made up of a group of elected officials so there is a regional perspective.

24. Dr. Linda Weintraut again asked for mitigation measures that consulting party members may wish to put forward during this opportunity of the Section 106 process. She stated that this is the chance to offer mitigation. Dr. Weintraut explained that the agencies and project sponsors are looking for ideas to offset the impacts such as educational programming, CSS, or any other ideas that the community could put forward. This is the consulting party's opportunity in the process to be heard on this issue.

- Dr. John Carr added the request for any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion.

25. Tom Cain commented that the discussion that has occurred for most the meeting, has taken away from the opportunity to talk about anything based on what is in front of us. He stated that he has a fairly lengthy list of observations and suggestions that he didn't feel he had enough time to discuss.

- Larry Heil requested the Tom Cain provide the comments and suggestions in writing and ensured him that FHWA would review them and take them into consideration in developing the MOA. He also reiterated that the purpose of the meeting was to talk about potential mitigation features and the historical elements that can be preserved.

26. Susan Haneline suggested that the State Boulevard curve be considered. The curve will remain and is not being lost, but as a resident of that particular footprint feels nothing is being done to showcase the feature. By allowing homes to remain in the footprint that are being vacated due to flooding and traffic problems, the curve will not be showcased. The homes that are not retaining value or are specific structures of historic significance should be removed. Ms. Haneline said that there are twenty other houses similar in style to the ones in the footprint found within an equal area [of the historic district]. The significance is simply the footprint of the area. By retaining the existing structures, nothing is being done to showcase the beauty of the Brookview Neighborhood or the feeling that the people that live there would like to have. Removing the negative aspect of leaving property owners that do not wish to remain and finding a way to showcase the features that everyone is feels are so important should be a focus of mitigation.

27. Michael Galbraith commented that the call for the five-minute mitigation measure is inappropriate. He feels the scheduling of the meeting in such a hurried fashion before the consulting parties are allowed a full opportunity to comment on the proposed mitigation measures in writing is inappropriate. People deserve an opportunity to review what has been presented to them and an opportunity to comment in writing if they do not feel comfortable arguing in front of 20 to 30 people.

- Jason Kaiser commented that what was presented were ideas for minimization efforts and that the agencies, project sponsors, and representatives are soliciting additional ideas. An effort was

made to give some ideas for minimization and mitigation to help spur further discussion among the consulting party members.

- Patrick Carpenter stated that the rationale for having the current meeting was to have it in the middle of the comment period. He stated that this does not foreclose comments or discussion but provides an opportunity for consulting parties to hear what other people have to say. This opportunity was provided to avoid comments going into a vacuum and allow consulting party members to get an idea of what everyone is thinking. The meeting was also intended to allow consulting party members to hear comments and ideas and be encouraged to formulate new ideas and put those into your comments that are due October 4<sup>th</sup>. This is an opportunity for people to provide input and not foreclose anything it helps encourage people to get involved.

28. Dr. Linda Weintraut stated that the consulting party members have until October 4, 2012 to submit written comments and encouraged everyone to comment looking at the minimization efforts, mitigation ideas, and thus consider how to creatively mitigate for the adverse effects. Your role as a consulting party member is to offer up ideas regarding mitigation regarding historical properties.

29. Michelle Briggs Wedaman commented that she is here as a representative of the neighborhood but also carrying a letter representing over 11,000 households and businesses. A lot of the comments that have been heard today speak to the public process. Ms. Wedaman said that she feels the public process has not been followed. Commented that for the last 4 plus years we have been involved in this process and the community has been asking the same questions in looking at this complex project. No roadmap has been provided since August 6, 2008 when we started working with the department of public works. The CSS approach process has been designed for sorting out these types of complex issues. We know that both the State and FHWA join us in wanting to spend the \$11 million dollars in a meaningful way that will benefit the community, but we ask you again to really listen to the questions we have, including why other public offices are not represented.

- Where are our policy officers, economic development officers, and historic preservation officer? Why have these and other officers not been allowed at the central planning table for this project?
- Why is the City going ahead with a project that is clearly out of line with the goals of this community, except just a few residents?

We stand behind the residents of State Boulevard in their plea not to be left next to a highway. To do this in the name of historic preservation makes it even more disturbing to us. Please join us in understanding why we are asking these questions.

30. Karl Dietsch commented in regards to the adverse effects that he sees many more positive aspects to this alternative than adverse effects. He will be able to turn onto State Boulevard safely as compared to now due to visual obstructions. He will also be able to always travel west where before he could not due to road closures as a result of flooding.

31. Patrick Carpenter requested that any comments received be included on the City's website.

- Michael Galbraith requested that the link be re-emailed to the all the consulting parties.

32. Briana Hope asked for any closing statement from FHWA and SHPO, thanked everyone for his or her participation, and adjourned the meeting.

## **ACTION ITEMS**

- American Structurepoint will update the online archive for the project's Section 106 correspondence and documents and provide the e-mail address to all consulting parties via e-mail.
- Weintraut and Associates and American Structurepoint will update the effects documentation as needed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or [bhope@structurepoint.com](mailto:bhope@structurepoint.com) to revise the record.

Very truly yours,  
American Structurepoint, Inc.

Briana M. Hope  
Environmental Project Manager

BMH:

Enclosures



August 21, 2012

Ms. Brianna M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

Sara Kruger Geyman  
1827 Forest Park Blvd.  
Fort Wayne, IN 46805

Re: Consulting Party/Mitigation Meeting, August 19, 2012

Ms. Hope,

I am so pleased that I was able to attend this meeting; it was quite informative. It was interesting to see first hand how our government works.


Before stating my comments, I would like to first point out that as a resident, I, along with others were uninvited to this meeting; a meeting of *consulting parties*. My first concern is that residents are not and have not been consulted in this matter. And the refused entry of said residents at this meeting only adds to the concern that plans are made behind closed doors – even against the wishes of the taxpaying citizens. Thankfully, due to our councilman, some members of this meeting acquiesced and we did attend.

Secondly, the grossly unprofessional behavior of Dr. Weintraut of Weintraut and Associates was appalling. The sighs and eye rolls that Dr. Weintraut displayed clearly showed her disdain for community feedback, and was very disrespectful.

Comments, as requested, regarding the issues of this plan are as follows:

- Residents were never, and are still not being included, making this faulty from the start.
- The Purpose and Need for this project has been decided and laid out in an **undemocratic way**. The purpose and need is still in question. The city has failed to demonstrate how this project will resolve issues. Residents concerns have had no bearing on Public Works, as they adhere to their plan and plow through historical homes, neighborhoods, and lives.

flooding has yet to be addressed. Failing to understand that and continuing with this plan is equal to giving a patient an aspirin to cure their cancer.

- **Mitigation is premature** in a plan and a process that has been faulty from the beginning. It is a **proverbial lollipop** stuck in the hands of residents to quiet them down and distract them from the truth.
- Albeit premature, the suggested mitigation measures were offensive:
  - “Save” three houses (but add an elevated 5 lane highway.) – Lollipop
  - “Take a picture of the bridge” (but destroy the land and neighborhood in which it once stood) – Lollipop
  - “Educational Measures” – (Is this where we tell future generations of the beautiful thriving neighborhoods of Brookview, Northside and Forest Park Blvd. that once upon a time existed but now is a highway?) – Poison lollipop
- Per the Federal Department Representative attending.. *“Addressing speed is not related to this project.”* It should be, because that is what is being invited. It seems the right hand knows not what the left hand is doing.
- This plan does not solve problems, it creates more of them. Destruction, speeding, urban blight, and the need for more of the same as it invites more of the same.
- This project is not a small isolated proposal, but a disastrous massive plan with immeasurable impact on property (both physical and fiscal), lives, entire neighborhoods, ultimately the city itself. Fort Wayne Public Works seem bent on cutting off its nose to spite its face.
- The stated “safety and congestion” solution that this plan claims, are two entirely different things. ‘Safe’ is slow and minimal. To add an element of safety one should consider ADDING curves, reducing the number of vehicles. This plan will add DANGER not safety. 
- Congestion should be relieved by sending traffic to the outlying arteries – Coliseum Blvd., Hwy 469 –for which they were created. Inner city streets should be meant for inner city living and working, and playing.
- Arterials DO NOT offer safety (per a city study), but rather an increased rate and higher number of accidents.
- It's been pointed out that many offices have NOT been included in the making of plans (i.e., The Housing Dept.) Where is the ;

Traffic study? – This has been requested, but not heeded

Environmental impact study?

Cultural impact study?

Housing study?


*Death Study* Long term effects of highways <sup>and heart attacks</sup> through neighborhoods study? *Per Harvard*

Design professional?

Urban Planning?

My suggestions:

- This massive and poorly managed project should be **halted immediately** and any plans to modify State Boulevard should begin anew, with a publicly transparent planning process which involves residents and resident neighborhood leaders, elected representatives, urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.
- Keep the well designed traffic calming curves and force commercial and lateral traffic to the arteries north – for which they were created.
- Think progressively! Look to the success of other downtown areas. Invite people back in to the urban areas by **CREATING MORE** desirable areas. Discourage speed and urban sprawl, encourage density in our urban core. (Per Pam Holocher, Ft Wayne deputy director of community development.) Narrow the streets, concentrate use, beautify, slow down. Invite a quality of life, and you invite quality people to populate downtown.
- Finally, I would suggest that Dr. Weintraut not be present in future meetings and be replaced by a respectful professional.

*Most Sincerely*  




Susan R. Haneline  
134 E. State Blvd  
Fort Wayne, IN 46805  
Brookview Neighborhood Home Owner

10/01/2012

To Whom It May Concern,

There has been a lot of talk about the historic significance of the Brookview Neighborhood. Its winding roads, park like settings and flowing creek certainly make it sound like the ideal place to live. I have seen this reflected in pictures of the neighborhood. Taken with such care, on a perfect Spring day... the area seems to shine with fairytale beauty. What those pictures do not reflect is many of the real issues in the neighborhood, the reality of the OTHER part of the neighborhood. I have attached 19 pictures of what MY part of the neighborhood looks like when it floods. This is the part of the neighborhood in the actual footprint of the E. State Blvd. road/bridge project. Please remember, as you look at these pictures, the flooding incidents you are seeing are not the exception any more, they are the norm. Some years we have had multiple instances of flooding lasting weeks at a time. As you can see, 3 - 4 foot clay dikes and dirty, contaminated flood water does not exactly project fairytale beauty.

In my part of the neighborhood, the increase in rental properties and vacant homes tell the story of just how many people are leaving the area because of the issues with traffic and flooding. The cries to maintain the neighborhoods historic significance seem to have lost sight of whether it is actually prudent to save something just for the sake of history. The crumbling bridge between Eastbrook and Westbrook Drives is a case in point. It is too narrow, it sits too low and it is in terrible disrepair. Each time the water in the creek rises to several feet below street level, the bridge acts like a damn, preventing the water from flowing underneath. It forces massive amounts of contaminated water to spill out onto our properties and close our driveways and roads. This is a case where practical is getting lost in the desire to maintain or retain something that is simply broken and outdated.

These problems do nothing to showcase what IS historical about the neighborhood. It is time to honestly address these problems and move forward with the road/bridge project. We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood. We certainly do not showcase our humanity when we force homeowners to keep properties that are neither safe, desirable or retain their monetary value. This is not a case of big government trying to run rough shod over the

little guy. This is a case of the little guy, begging government to step in and solve a problem, to do the right thing. Please allow us to be gracefully be removed from the burden of properties that have become undesirable places to live. I again ask you to take a hard and honest look at the attached photos. Can you, in good conscious, really retain our homes in the footprint of this project knowing that the issue of flooding and traffic will not be resolved? If you owned property here, would you not be begging for someone to step in and solve the problem? Brookview Neighborhood was designed to have a park life feel, why not allow the homeowners to be bought out and then, as proposed, turn all that land into green space? Give the neighborhood a true entrance, showcase its unique feel through a thoughtful use of lighting, plaques, hardscape and monuments. I am not saying we should not honor our history, but there is a way to honor history without sacrificing homeowners quality of life.

Respectfully,

A handwritten signature in dark ink, appearing to read 'Susan R. Haneline', with a stylized, flowing script.

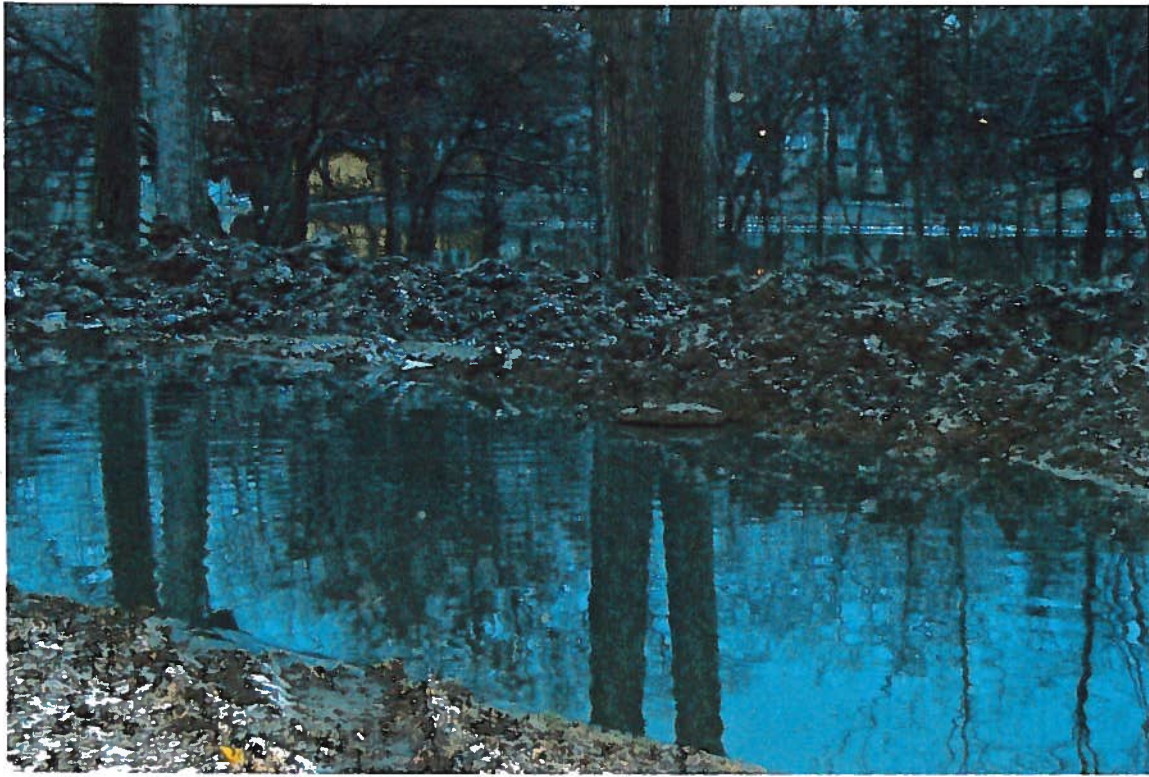
Susan R. Haneline









































Susan R. Haneline  
134 E. State Blvd.  
Fort Wayne, IN 46805  
Homeowner, Brookview Neighborhood

10/02/2012

To Whom It May Concern,

This letter is in response to questions about how we might best preserve and honor the history of Brookview Neighborhood, while proceeding with the State Boulevard project.

I have done a lot of thinking about the vision of the neighborhood, what it was meant to convey about the city, about progress and the importance of beautiful and aesthetically pleasing urban green spaces.

It is my understanding that the current proposed plan does not remove the historic footprint of the neighborhood. The bridge that is being proposed for removal is, in all honesty, an eyesore and a flooding issue. It has been in a state of disrepair for years. Its removal would improve the overall look of the neighborhood. I do not agree with attempts to fix the bridge in its current location. Even repairing the bridge seems counterproductive. At its current height and size, it would continue to back up the water in the creek which plays a significant part in the flooding of homes in that area. As for the moving of State Blvd, it is my understanding that the old roadway will, for the most part remain intact. It will simply become a part of the residential neighborhood. Perhaps it could simply be renamed something like "Historic State Boulevard". This plan seems to me, to actually enhance historic vision, not cause it to be destroyed. The winding street would still exist, but it would exist as a quiet tree lined boulevard set within a urban neighborhood green space.

As I have stated in previous letters, the homes located in the footprint of this project have been suffering for years due to flooding and traffic flow. The majority of these homeowners would like to be bought out for this project. I honestly believe that these buyouts and the subsequent open green space around the new State Boulevard would improve then neighborhood. To further respect the historic vision, I would suggest the following:

1. Installing historically correct lighting in the area. Perhaps something that reflected lighting styles from the early part of the 1900's. I think reproductions of the old gas style lights would be a wonderful addition to the neighborhood.

2. Plantings and green space that gives the area a park like feel, such as period style benches, groupings of trees and flowers, perhaps even brick style side walks. All these kinds of details and features pay homage to the areas history.
3. Beautiful stone or brick entrance pillars for the neighborhood. Perhaps something similar to the Forrest Park Boulevard area, a feature that would define the neighborhood.
4. I like the idea of a divided new State Blvd, one that is tree lined, perhaps with some perennial cottage or old English type flower beds.
5. I would love to see some small monuments that would tell the story of Arthur Shurcliff and his vision of the neighborhood. Perhaps even pointing out examples in the project that pay homage to his vision.
6. I think the best way to crown the area would be to find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces. Again, I think nod to an old fashion early 1900's park setting would be beautiful. Maybe the addition of an appropriately styled fountain or gazebo could add to the historic feel.

I believe there are many ways to honor our history without halting the progress of a project that I truly believe would benefit the neighborhood, the public and the city.

Respectfully,

A handwritten signature in black ink, appearing to read 'Susan R. Haneline', with a stylized, flowing script.

Susan R. Haneline

JOHN H. SHOAFF, A.I.A.  
ARCHITECT

4646 West Jefferson Boulevard  
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com  
telephone: 260-459-0221

Ms. Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256

October 3, 2012

Re: State Boulevard Reconstruction Project  
Des. No. 0400587  
Project No. IN20071404

Dear Ms. Hope:

As a Councilman elected to represent the welfare and best interests of the citizens of our city, I cannot support the current State Boulevard widening plan in anything like its present form, nor can I support the "concept" as described in the purpose and need statement on which it is based. The attempt to "elevate" (all words in quotation marks are taken from either the P and N statement or the 2030 II Transportation Plan) State Street to a major east-west arterial for regional as well as local traffic is nothing less than a perversion of the proper use of the "By-pass and Arterial concept", a concept which should be used to divert through traffic from residentially populated areas, not to them, as this plan does.

There are two legitimate needs in the Brook View area that must be addressed: the bridge over Spy Run Creek needs serious repair or replacement, and a blind spot at the foot of State Boulevard, near the intersection with Westbrook, needs to be eliminated. Plans responsive to these needs but, unlike the present plan, minimally harmful to the historic district it divides and effectively discouraging of faster traffic and truck traffic should be developed following AASHTO and Federal Highway Administrations Guidelines for public participation in context sensitive traffic project design. I will be pleased to participate in such a process, if properly conducted, and in the development of effective mitigation measures which would integral to it. In any event, discussion of further planning along any lines, mitigation or otherwise, should await the outcome of the Environmental Assessment which I understand is currently under way.

I call your attention to the enclosed letter, signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen. As a councilman at-large I represent them; but not just they will be harmed: the legitimate complaints and distress they convey translate into decreased property values. In turn this translates into a decreased property tax base and decreased tax revenues, which hurts the whole city. In fact this project in its current form will exacerbate the trend of recent decades toward the hollowing out of the city – that is, the migration of homeowners from the city to outlying areas - and it will do so precisely



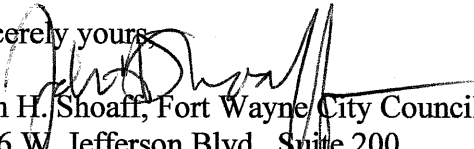
at the time we are trying to revitalize our downtown and at a time when we should be taking advantage of a national countertrend, led by seniors and young people, toward moving back into the city.

Reactions to the last mitigation meeting: Just prior to it Mr. DuMontelle, in a response to an inquiry from Michelle Wedaman, wrote that alternative plans were presented at a public meeting and a preference chosen. He was misinformed. Attendees were presented with just one plan, the only plan the planners have ever shown and about which they have been totally inflexible. The only alternatives offered were three minor ones that had to do with which local street would access the new four and five lane corridor. This fell far short of verbally made promises to explore and offer real alternatives; and, insofar as it was a pretense of doing otherwise, was an insult to the intelligence of my constituents. As I've said elsewhere, it was akin to asking the condemned man which tree he wished to be hanged from.

Mr. DuMontelle might wish to refute my implication that he did not show a proper grasp of the democratic process when he said that he liked the P and N statement because "it was decided by a regional planning body". He could reasonably say that he meant only that NIRCC decides only what recommendation will be sent to the elected body, and that he knows full well that the elected body makes the decision.

Finally, I also suggest that Ms. Weintraut consider excusing herself from any further moderating duties. Her rude and highly unprofessional reactions to speakers she disagreed with made it clear that the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.

Sincerely yours,



John H. Shoaff, Fort Wayne City Councilman At-large  
4646 W. Jefferson Blvd., Suite 200  
Fort Wayne, IN 46804  
Phone: 260 459 0221

September 1, 2012

**To:** Fort Wayne Common Council members and Mayor Tom Henry

**RE:** City's State Boulevard reconstruction project Wells/Cass Streets to Spy Run Avenue  
State Blvd Reconstruction - Des. No. 0400587

Dear representative of the people of Fort Wayne:

The City of Fort Wayne wants to expand State Boulevard into a primary east-west arterial corridor like Coliseum Boulevard, and is currently preparing to widen State Boulevard's two lanes between Wells Street and Spy Run Avenue to five lanes, straightening the carefully designed traffic-speed-calming curves over Spy Run Creek and through the celebrated National Register of Historic Places Brookview Neighborhood. The City plans to elevate the roadway 7'-9'; turning this appealing residential district into yet another wide, fast urban highway. State Boulevard is an integral part of Fort Wayne's acclaimed Parks and Boulevard System, recognized in its entirety on the National Register of Historic Places as the Park and Boulevard System Historic District.

This plan displays a failure of the public process. The City claims that this project will address "safety and flooding" but has failed to demonstrate that either will be accomplished, at a cost of more than \$11 million public dollars. The decision-making used to arrive at this unsatisfactory proposal has been unclear, has alienated many of us who have sought input as engaged residents and has failed to address even basic issues about the project's real impacts on our property values (and consequent impact on property tax revenue), land use, safety and flood control, and the character of our neighborhoods. The City's Public Works Department has responded to our concerns only by holding public information meetings where we've been shown again and again the same "preliminary design" engineering drawings with no alternatives offered. The department has not altered its initial plan.

Our concern extends far beyond the additional dozen or so homes which would be razed, adding to the 26 or so recently demolished in the historic Brookview Neighborhood. Roadway design in this area affects the entire length of State Boulevard and the surrounding city. The current proposal will damage all of our property values and overall livability of the area. The decrease in property values will decrease property tax revenue, which will harm the entire city. We predict a migration from the well-tended and well-loved center city historic core, fewer owner-occupied homes, decreased care of properties, blight, and a decreased tax base for the city overall. Any road

projects here must be carefully considered from a comprehensive perspective and understood for what they mean to the people of the community. We are gravely concerned about our property values; quality of life; city attractiveness; safety and transportation accessibility for uses that include local vehicles, biking, walking and public transit; coherence with unique historical character and distinctive natural features; air, water, sound and light quality; and incorporation of meaningful flood control by identification of root causes.

This massive and poorly managed project should be halted immediately, and any plans to modify State Boulevard should begin anew, this time with a publicly transparent planning process which involves us - a working group of resident neighborhood leaders - along with elected representatives, and urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.

Thank you,

Michelle Briggs Wedaman, president, Brookview Neighborhood, 275 households and businesses  
Paul Gibson, president, Irvington Park, 80 households  
Marcia Simmons, president, Park Place Condominium Association, 175 households  
John Meinzen, vice president, Spy Run Neighborhood Association, 500 households  
Cynthia Keller, president, Northside Neighborhood Association, 4,000 households and businesses  
Shawna Nicelley, president, Forest Park Boulevard, 66 households  
Steve Morehead, president, North Anthony Area Association, 1,100 households and businesses  
Helene Evans, president, Frances Slocum Association, 1,200 households and businesses  
Evelyn Bickel, president, Brentwood Park Association, 290 households  
Gretchen Johnson, president, Bloomingdale Neighborhood Association, 1,850 households and businesses  
Judi Wire, president, The Wells Corridor Business Association, 50 businesses  
Mike Reuille, president, North Franke Park Association, 450 households and businesses  
Rick Stoeckley, president, Lincoln Park, 680 households  
Linda Spurrier, president, Fall Creek Neighborhood, 107 households  
Tom Tiernon, president, Historic Oakdale Neighborhood Association, 700 households

**CONTACT:**

You can reach us all on this matter via Michelle Briggs Wedaman  
260-710-4413  
[mbwedaman@frontier.com](mailto:mbwedaman@frontier.com)  
mailing address: 2326 Eastbrook Drive, Fort Wayne, IN 46805



**From:** [John Shoaff](#)  
**To:** [Hope, Briana](#)  
**Subject:** Copy of Comments letter on State Boulevard widening project  
**Date:** Wednesday, October 03, 2012 3:37:51 PM  
**Attachments:** [NORTH ANTHONY.doc](#)  
**Importance:** High

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Dear Ms. Hope,

I've attached a copy of my letter of comments re: State Boulevard Project Des. No. 0400587 which was mailed today, Oct. 3.

John Shoaff

JOHN H. SHOAFF, A.I.A.  
ARCHITECT

4646 West Jefferson Boulevard  
Fort Wayne, IN 46802

e-mail: [jshoaff@proparkwest.com](mailto:jshoaff@proparkwest.com)  
telephone: 260-459-0221

October 3, 2012

Steve Kennedy  
Grants Section  
Indiana Division of Historic Preservation and Archeology  
402 West Washington Street, Room w274  
Indianapolis, IN 46204

Dear Mr. Kennedy,

I am very pleased to join the chorus of those urging that the potential North Anthony Boulevard Historic District be listed on the National Register.

This is an area I have known since childhood, when I began making lifelong friends at the nearby Forest Park School. It has held up well over the decades, and has remained a good place to live, close to our downtown. It represents one stretch of the Kessler Park and Boulevard system, appreciated nationally as one of the most intact of George Kessler's urban plans, and recognition on the National Register would further encourage prideful home ownership.

Thank you for consideration of this excellent candidate for the Register.

Sincerely yours,

John H. Shoaff  
Fort Wayne City Councilman At-large

Friends of the Parks of Allen County, Inc.  
Po Box 10152  
Fort Wayne, Indiana 468050

Ms. Briana Hope  
Environmental Project Manager  
American Structure Point, Inc.  
7620 Shadeland Station  
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project  
Des. No. 0400587  
Project No. IN20071404

The Friends of the Parks of Allen County, Inc. agree that there will be an adverse effect of the Fort Wayne Park and Boulevard System Historic District and the Brookview - Irvington Park Historic District, as well as to the Bridge over Spy Run (NBI No. 0200273).

However we feel that any discussion of mitigation is, at best, premature; at worst, the proposed Memorandum Agreement is a bad faith attempt to confuse an already complicated and unfair process, especially in light of the fact that the Federal Highway Authority has elevated the National Environmental Policy Act (NEPA) Documentation to Environmental Assessment (EA).

We continue to assert that the APE is not comprehensive enough and should include those historic districts along State Boulevard. This project is not just a construction project. It is a project which will continue the ongoing process of turning the historic boulevard into a major (as opposed to "minor") traffic arterial according to the plans that the Northeast Regional Coordinating Council has been following for years. The additional traffic that these plans are intended to generate will degrade the quality of life substantially along the State Boulevard corridor and will negatively impact a number of historic neighborhoods, schools, and other properties. Connectivity (safety), air quality, and visual relationships will be harmed along the entire corridor. We respectfully request that the APE be expanded to include all the properties along State Boulevard.

Further, it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.

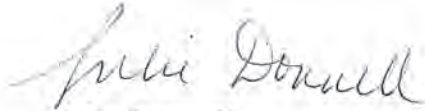
In conclusion, although I, myself, did not attend the most recent meeting, Ed Welling, Vice President of the Friends of the Parks was there. The Friends are appalled at the continuing disrespect shown to the citizens of Fort Wayne through this process, in



particular, the very unprofessional behavior of Ms. Weintraut and the assertions of Mr. Dumontelle that alternative plans have been presented to the public.

Under these circumstances, the public is not being served properly at all.

Sincerely,

A handwritten signature in cursive script that reads "Julie Donnell".

Julie Donnell  
October 3, 2012

Julie Donnell  
President,  
Friends of the Parks of Allen County, Inc.  
September 19, 2012

260-432-7178  
[juliemarie57@earthlink.net](mailto:juliemarie57@earthlink.net)

**From:** [Suzanne](#)  
**To:** [Hope, Briana](#)  
**Subject:** Fort Wayne State Blvd project - Mitigation response following meeting of 19 September, 2012  
**Date:** Wednesday, October 03, 2012 11:53:56 PM

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All,

On finding ways to mitigate the adverse effects of the massive, intrusive State Blvd project on the impacted area, here's a solution: don't build it.

Fact: It is not needed.

Fact: The proposed plan is flawed.

Fact: The APE is inadequate.

Fact: The process has been unfair and undemocratic, and now we can also say -- abusive.

Regarding the meeting of 19 September, 2012, the disrespectful, contemptuous tone and the conduct of our meeting hosts were appalling. The attempts to deny concerned citizens entrance to the meeting was transparently obstructionist. The scoldings and rude interruptions by our hosts and government agency reps were extremely unprofessional and insulting. The facilitator's dramatic eye-rolling, grimacing, smirking, and toe-tapping while those in opposition were trying to speak were quite a stunning display of dramatics. I hope those of you who attempt to operate with integrity were embarrassed by these ridiculous stunts.

At this so-called mitigation meeting we were admonished to provide only facts and simple ideas for mitigation. Some mitigation ideas offered us by the facilitator included taking a couple of photographs of the existing bridge and coming up with a few educational signs and materials about the historic elements that will be destroyed by this colossal project -- as if such trivial efforts would smooth over the loss of a dozen homes and the eruption of a highway in the midst of our quaint historic district.

Here are more facts that have been offered by the consulting parties and repeatedly rejected by Stucturepoint and our government agency reps:

- The P&N rationale is flawed and has shifted and changed depending on the agenda of the moment. It's the curves, no, it's flooding, no, it's congestion, no, it's . . . fill in the blank with some other fabricated reason, but, never with the NIRCC plan goals to push arterial traffic through the north side of Fort Wayne.
- Safety has been given lip service, but substantive traffic calming is never, *has never been*, included in the design plan in *any way*, even though traffic calming

elements are being designed into other Fort Wayne urban roadways. So *clearly* safety is not a priority here. If it were, accurate data would have been provided to the public. Instead we have vague references to crashes that might have occurred somewhere in the *general* area that might or might not have been caused by congestion, or speeding, or, we don't know what, because that information was never provided!

- It is a fact that the proposed roadway will withstand higher speeds; this does not play nicely with traffic calming.
- The current plan is intrusive, and massive yet no substantive design elements have ever been revealed in renderings that the public can easily evaluate, even though, the bifurcation of the historic district is a **devastating** aspect of the plan. Where are street-level renderings of the proposed design that show the elevated bridge and vacated lots where homes once sat?
- The APE is not adequate. The plan must be rejected until it appropriately addresses the entire impacted area in all of its invasive, destructive aspects. How can a project this devastating and monstrous not impact the entire near north side of the City?
- Here's another fact, I pulled onto State (southbound from Eastbrook, turning west onto State) at 8:05 a.m. on 2 October, 2012, a Tuesday morning, and my car was the SOLE vehicle in either lane of the roadway at that time for the entire stretch of the relevant curved area. In fact, my car was the only vehicle in sight at that time. Your purpose and needs congestion argument is not tenable! And, when I came home about 11:00 a.m., there were widely spaced lines of "traffic" -- about six cars -- moving at a steady speed in both directions. I proceeded to Eastbrook made my left turn safely and without having to wait.
- And one more factoid for you to ponder: on my bicycle today at 5:02 p.m. -- afternoon rush-hour -- I had to wait perhaps 30 seconds to safely ride across State Blvd at Cass from south to north. There was a wide gap, with no traffic in either direction **at 5:00 p.m.** So plug that into your expensive traffic model and explain why we need to squander 11+ million dollars on this disastrous project!

With all *due* respect,



Suzanne Slick  
Irvington Park Consulting Party

**From:** [Jill and Andrew Downs](#)  
**To:** [Hope, Briana](#)  
**Subject:** State Boulevard - Des. No. 0400587/DHPA No. 5903  
**Date:** Thursday, October 04, 2012 9:33:45 AM  
**Attachments:** [DesNo0400587Oct2012.doc](#)

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Dear Ms. Hope,

I have attached a letter with comments regarding the State Boulevard Reconstruction Project (Des. No. 0400587/DHPA No. 5903). I will put a signed copy in the mail today.

Thank you.

Jill Downs

**Jill D. Downs  
1202 Elmwood Ave.  
Fort Wayne, IN 46805**

October 4, 2012

Briana Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

Dear Ms. Hope:

I am writing to provide my comments to the most recent findings regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

In short, I would agree that this project creates an adverse effect in the area. However, it is clear that the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration. Although American Structurepoint and the Federal Highway Administration feel the project is at a point where mitigation discussions can be held, the majority of those opposed to the project remain unconvinced about its perceived necessity. Because of this, it is difficult to move forward. I think this was painfully obvious at the September 19, 2012, meeting of the consulting parties.

Those opposed to the project were not interested in discussing mitigation for a variety of reasons. These include the fact that the Environmental Assessment has not been completed, the resultant bi-furcation of the historic district and consequences of raising of the road should be added to the list of adverse effects, and that because houses in the area had already been removed by the City of Fort Wayne, there appears to be a lesser impact to the district. Also, it was noted by Linda Weintraut in the meeting that there is another project proposed within the area, this being the Pufferbelly Trail, that is creating an impact on the design of the State Boulevard Reconstruction Project. It would seem as those information about the specifics of the Pufferbelly Trail project should be incorporated into the State Boulevard project. It is premature to discuss mitigation.

Overall, I do not believe there is not a need to reconstruct State Boulevard. As a nearly lifelong resident of near northeast Fort Wayne, I travel the State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. Such projects are even being undertaken within Fort Wayne, specifically the downtown area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area. A recent "Travel Time Delay Study" for fiscal year 2012 indicated that during peak travel times, there is only a 5.7 delay in traveling from Sherman Boulevard to Beacon Street. Just from a fiscal standpoint alone, I do not believe spending millions of dollars to save 5.7 minutes is worthwhile. The State Boulevard Reconstruction Project is not warranted, and this is evident by the poor attempts by the City of Fort Wayne, American Structurepoint, and the Federal Highway Administration to justify its need.

Sincerely,

Jill D. Downs



**From:** [Michael Galbraith](#)  
**To:** [Hope, Briana](#)  
**Cc:** [Michael Galbraith](#); [John Carr](#); [Joyce Newland](#); [Patrick A Carpenter](#); [Todd Zeiger](#); [Cathy Wright](#); [Jill Downs](#); [Michelle Briggs-Wedaman](#); [John Shoaff](#); [Julie Donnell](#)  
**Subject:** Proposed State Street Reconstruction (Des. No. 0400587, DHPA No. 5903)  
**Date:** Thursday, October 04, 2012 2:22:52 PM  
**Attachments:** [doc00147120121004140115.pdf](#)  
[ATT00001..htm](#)

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Briana-

I've sent the following letter in hard copy form to you today via US Mail. I thought you might like to have an electronic version for your files.

Mike

Michael Galbraith  
Executive Director, ARCH  
[mgalbraith@archfw.org](mailto:mgalbraith@archfw.org)  
260.426.5117



The mission of ARCH is to advocate for the protection and preservation of historically and culturally significant assets and historic places in Allen County and northeast Indiana.

Briana M. Hope  
Environmental Project Manager  
American Structurepoint  
7260 Shadeland Station  
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter dated August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form, fashion, or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintrout.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. Although the material had literally been sent via e-mail at the close of business hours the day before the meeting, it still played an integral part in the American Structurepoint presentation. At the meeting it was concurred by FHWA and the DHPA that furnishing information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other information provided in the 4(F) document.

Given the breadth and scope of the project, and associated substantial adverse impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,

Michael Galbraith  
Executive Director, ARCH, Inc.

Ecc: DHPA, FHWA

**From:** [Michelle Briggs Wedaman](#)  
**To:** [Hope, Briana](#)  
**Cc:** ["John Carr"](#); ["Joyce Newland"](#); ["Patrick A Carpenter"](#); ["Todd Zeiger"](#); ["Cathy Wright"](#); ["Jill Downs"](#); ["John Shoaff"](#); ["Julie Donnell"](#); ["Michael Galbraith"](#); ["Michelle Briggs Wedaman"](#)  
**Subject:** Proposed State Street Reconstruction (Des. No. 0400587, DHPA No. 5903)  
**Date:** Thursday, October 04, 2012 10:44:30 PM  
**Attachments:** [10 04 2012 Brookview Neighborhood on State Blvd Reconstruction August 29 2012 packet.pdf](#)

---

10/4/2012

Hello,

The attached letter (pdf) has been mailed to American Structurepoint via U.S. Mail as well.

Thanks,

Michelle

Michelle Briggs Wedaman  
Brookview Neighborhood  
260.710.4413  
[mbwedaman@frontier.com](mailto:mbwedaman@frontier.com)



# **BROOKVIEW**

## **NEIGHBORHOOD ASSOCIATION**

Briana M. Hope  
Environmental Project Manager  
American Structurepoint  
7260 Shadeland Station  
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to request an additional 30 days to comment on the American Structurepoint letter and packet dated August 29, 2012, about proposed mitigation for State Boulevard Reconstruction in Fort Wayne, IN. We require additional time to incorporate our comments related to the August 29 packet, as additional materials were sent to us from American Structurepoint via email the night before the September 19, 2012 consulting parties meeting. Those materials were then referred to by American Structurepoint during their September 19, 2012 presentation.

Since the last consulting parties meeting notes in 2011, we have received only the May 2012 updates related to historic properties and no indication of or updates on other project alternatives under exploration, and no answers to the very real questions we have about this project. The many questions we asked at both the December 2009 and the September 2011 consulting parties meetings have remained unanswered: questions about the project's Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the real area of impact of this project.

We understand that an environmental assessment is still being completed for this project. How can we discuss mitigation before being enlightened by information contained in that report? How can this group discuss mitigation without being enlightened by information we have all requested about this project?

This project is for us, not a theoretical one. These are our streets and our neighborhood, and we have a right and a responsibility to request and receive not only substantive answers to questions we and others within the consulting party circle have been asking, but ample time to review and comment in writing upon information related to this exploration.

Sincerely,  
Michelle Briggs Wedaman  
President, Brookview Neighborhood  
2326 Eastbrook Drive, Fort Wayne, IN 46805  
260.710.4413 [mbwedaman@frontier.com](mailto:mbwedaman@frontier.com)

**Brookview Beautiful.**  
**An historic garden neighborhood in the heart of the city. This place matters.**



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739  
Phone 317-232-1646 • Fax 317-232-0693 • [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov)



October 4, 2012

Richard J. Marquis  
Acting Division Administrator  
Indiana Division  
Federal Highway Administration  
575 North Pennsylvania Street, Room 254  
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft memorandum of agreement for the State Boulevard Reconstruction Project (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Mr. Marquis:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed American Structurepoint's letters of August 29, 2012 (with enclosures) and September 18, 2012 (with enclosures), and has taken into consideration the discussion at the September 19, 2012 consulting parties meeting, regarding the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

As we had said in our August 13, 2012 letter, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

We concur with FHWA's August 27, 2012 Section 106 finding of Adverse Effect for this undertaking.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected:

- Fort Wayne Park and Boulevard System;
- Brookview-Irvington Park Historic District; and
- Bridge on State Boulevard over Spy Run (NBI. No. 0200273).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

Furthermore, we wish to offer some comments and suggestions about the draft memorandum of agreement, Version 8/24/2012 ("Draft MOA").

#### SUGGESTIONS FOR FURTHER MINIMIZATION

We think we understand the issues described in your September 18 letter that would make preservation of the houses at 112, 134, and 138 East State Boulevard problematic. We remain concerned about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District. Having reflected further upon your September 18 letter, we wonder whether, as a minimization measure, it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive. It is our impression that most of the existing sidewalk along the south side of State Boulevard could remain in place, and it seems to us that the existing sidewalk could serve pedestrians who would be walking along the north side of the new alignment, even though the northward bow in the existing State Boulevard would make one's walk slightly farther than if a sidewalk immediately paralleled the new alignment along its north side. We are sympathetic to the concerns of property owners at the September 19 meeting who expressed a preference to have their entire properties along the south side of the current alignment of State Boulevard, rather than to sell only large portions of their yards and have the new proposed right-of-way come within only several feet from their houses. However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect.

It appears to us that if the sidewalk and the grass buffer between the sidewalk and the curb were eliminated from the plans along the north side of the new alignment from Terrace to Eastbrook, and if a railing of some kind were constructed adjacent to the curb, then at least ten feet less right-of way would be needed along that north side of the new alignment. Furthermore, if a retaining wall were constructed near that railing, instead of a sloped embankment and a drainage swale, it appears to us that even less right-of-way would be needed along the north side of the proposed alignment. Also, if there were no sidewalk immediately adjacent to the north side of the new alignment, it appears to us that it might be feasible to eliminate one or both of the new sidewalks that are proposed along the Oakridge Road extension. If there were no sidewalk along the north side of the new alignment, then there would seem to be no need to provide new sidewalks extending southward along the Oakridge extension from the existing State Boulevard to the new alignment of State Boulevard.

We also wonder whether the reconstruction of State Boulevard, which would elevate the roadway above the existing grade as it runs west from Terrace, could be designed to serve to some extent as a levee to prevent most Spy Run floodwaters from reaching the three houses in question on the south side of the existing State Boulevard.

If some or all of the suggestions above prove to be feasible and prudent, then we think they should be incorporated into the memorandum of agreement, in an effort to avoid confusion in the future about the design parameters upon which agreement has been reached.

#### SUGGESTIONS FOR MITIGATION

Stipulation I. of the Draft MOA appropriately directs that context sensitive solutions be incorporated into the new construction and related landscaping and streetscape design. That stipulation also would establish an advisory team to review and comment on the specifics of that design work, in keeping with the directive contained in Stipulation I.B.ix. of the 2009 "Memorandum of Agreement. . . Regarding the US 27 Southbound Realignment and Bridge Replacement over Spy Run Creek in Fort Wayne, Allen County, Indiana" for future federal projects in the area. The advisory team that was established under that US 27 memorandum of agreement provided useful recommendations for context sensitive solutions for that project. However, we do not believe that there is a need for the Indiana SHPO to be directly involved in all of the meetings and activities of future advisory teams in the area. We believe that the most important input will arrive in the form of the Advisory Team members' recommendations, based on their perceptions of what is best for their community, and of the guidance from FHWA and the Indiana Department of Transportation regarding the feasibility of those recommendations. Consequently, we ask that the Indiana SHPO *not* be given a role in convening advisory team meetings, as is currently proposed in Stipulation I. B. and I.B.vi. of the Draft MOA, and that the Indiana SHPO's participation in meetings of the advisory team be left to the Indiana SHPO's discretion. It would be appropriate, however, for the Indiana SHPO to remain involved in the kind of consultative role that is prescribed in the final sentence of both I.B.vi. and of I.B.viii.



We anticipate that at least one consulting party will be making recommendations for crafting context sensitive solutions in keeping with the natural landscape of the project area and the landscape design philosophy of George Kessler or Arthur Shurcliff. This is an intriguing idea, and we would ask that serious consideration be given to any consulting party recommendations along those lines. It appears that regardless of the particulars of the final design of the reconstructed State Boulevard, a considerable amount of green space will be opened by this project, and how that green space is designed could play an important mitigative role.

We agree that the current State Boulevard bridge over Spy Run (NBI. No. 0200273) should be documented photographically, as provided for generally in the Historic Bridges PA and specifically in Stipulation II. of the Draft MOA. We would ask that such photo-documentation be performed in accordance with the version in effect, at that time, of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards." It recently has been brought to our attention that the State Archives, rather than the State Library's Memory Project, is the legally-authorized repository of all state government records that are required to be preserved.

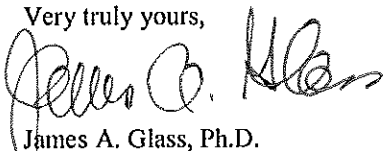
We also request that it be stipulated in the MOA that the portion of the Brookview-Irvington Park Historic District within and immediately adjacent to the proposed project area should be photographically documented. We recommend that streetscape and broad views of the setting of that part of the neighborhood be emphasized, but we think that at least a couple of photographs of each house that is to be demolished also should be included in the documentation. The photographs should be taken from oblique angles so as to document all four elevations of each house.

For both the State Boulevard bridge photographs and the streetscape and district photographs, we request that a set of the photographic images in both print and digital form, saved on a compact disc, and following, as closely as possible the guidance of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards," be provided to our office for ultimate transfer to the State Archives. We also think that a duplicate set of the electronic and print photo-documentation be prepared for and delivered to a local public library or not-for-profit institution that would be capable of and willing to retain the documentation on a permanent basis, so that it would be readily accessible to local researchers.

If you or American Structurepoint, Inc. would find it helpful, we could draft specific MOA stipulation language or modifications to language in Version 8/24/2012 to show how our recommendations might be incorporated into the MOA. If you wish to receive such suggestions of specific language, then, in order to facilitate our drafting efforts, we would appreciate receiving an electronic copy of the MOA in a format that would allow us to show changes and make explanatory comments.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). In all future correspondence regarding the State Boulevard Reconstruction Project, please continue refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Briana Hope, American Structurepoint, Inc.

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration  
Patrick Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Briana Hope, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

**From:** [Todd Zeiger](#)  
**To:** [Hope, Brian](#)  
**Cc:** [Jim Glass](#); [Michael Galbraith](#)  
**Subject:** State Boulevard Reconstruction Project Des. No. 0400587  
**Date:** Thursday, October 04, 2012 3:00:02 PM  
**Attachments:** [State boulevard october request.docx](#)

---

Good afternoon. Please see attached letter concerning the consulting party process for the State Boulevard Reconstruction project. Thank you.



Todd Zeiger  
Director, Northern Regional Office

.....  
**Indiana Landmarks**  
402 W. Washington  
South Bend, IN 46601  
Ph: 574-232-4534  
Fax: 574-232-5549  
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October 4, 2012

Briana M. Hope  
Environmental Project Manager  
American Structurepoint  
7260 Shadeland Station  
Indianapolis, IN 46256-3957

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter of August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. In fact, I was unaware of the additional documentation and information therein until attending the meeting itself as my travel time required I leave early that morning and did not have an opportunity to see or review that email. At the meeting it was concurred by FHWA and the DHPA that sending pertinent information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other informant provided in the 4(F) document.

Given the breadth and scope and associated substantial impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,

Todd A. Zeiger  
Director, Northern Regional Office

Ecc: Dr. Jim Glass, Indiana Division of Historic Preservation and Archaeology  
Mike Galbraith, ARCH



**From:** [Creager Smith](#)  
**To:** [Hope, Briana](#)  
**Cc:** [Tom Cain](#); [Shan Gunawardena](#); [Pam Holocher](#); [Sherese Fortriede](#); [Don Orban](#); [John Urbahns](#); [John Wallace](#)  
**Subject:** State Blvd. Reconstruction Project - CP Comments from FW Community Development  
**Date:** Thursday, October 04, 2012 4:11:40 PM  
**Attachments:** [MOA and mitigation comments letter on letterhead 10-4-2012.pdf](#)  
**Importance:** High

---

October 4, 2012

Briana M. Hope  
Project Manager  
American Structurepoint  
Environmental Sciences Group

Ms. Hope:

I have attached a letter with Consulting Party comments RE the State Blvd Reconstruction Project from Thomas Cain and Creager Smith of Fort Wayne Community Development. The comments focus on the draft MOA and potential mitigation issues.

A copy of the attached letter, dated October 4, 2012, is also in the mail.

Thank you—

CS

Creager Smith  
CD Specialist-Planner-Historic Preservation  
City of Fort Wayne, Indiana  
260.427.2161

Citizens Square  
200 E. Berry St., Suite 320  
Fort Wayne, IN 46802



# CITY OF FORT WAYNE

THOMAS C. HENRY, MAYOR

October 4, 2012

Ms. Briana M. Hope  
Environmental Project Manager  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, IN 46256

RE: State Boulevard Reconstruction Project  
Fort Wayne, Allen County, Indiana  
Des. No. 0400587  
DHPA No. 5903  
Project No. IN20071404

Dear Ms. Hope:

This letter provides comments on the above-referenced project from both Tom Cain, Fort Wayne Urban Designer, and from Creager Smith, Fort Wayne Historic Preservation Planner. We both agree with the determination that the proposed project would have an adverse effect on three historic properties within the APE.

The bullet points that follow are a list of adverse effects on landscape and planning features, in both the Brookview-Irvington Park Historic District and in the Fort Wayne Park and Boulevard System Historic District, caused by the State Boulevard Reconstruction Project. The list was assembled by Tom Cain, and edited by Creager Smith. It is a suggested starting point for more detailed discussion during any consideration of mitigation of adverse effects. These items are in no particular order; they are the potential basis of mitigation measures, although there may be additional adverse effects that do not appear on the following list.

- Roadway and sidewalk layout changes from historic alignments to new alignments; alteration of the locations, sizes, and shapes of the carefully designed public and private spaces provided by the historic Brookview plat.

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Hope

October 4, 2012

Page 2

- Roadway elevation changes and topographic relationship changes between linear public roadway space and the private linear yards.
- Stream corridor vegetation loss from new road and gap in streamside vegetation left by bridge removal.
- New open space fragments that are not part of the historic Brookview plat.
- Disintegration of public space definition on south side of old State Blvd. alignment by demolition of homes and loss of the private space.
- Alteration of historic relationship of road elevation to former rail bridge embankment and bridge abutments.
- Alteration of road width through former rail bridge abutments.
- Substantially increased width of roadway through neighborhood.
- Changes in pedestrian patterns.
- Alteration of neighborhood street connections to State Blvd, concentrating them into a new extension of Oakridge Rd.
- Removal of later street lighting which is of similar scale to historical lighting for the area.
- Removal of historic rail and Spy Run Creek bridge architecture.
- Loss of street trees.
- Loss of historic streetscape patterns of Eastbrook Dr.
- Alterations of Westbrook Dr. and other segments of neighborhood streets, especially at ends of blocks and at driveways.
- Insertion of access ramps from State Blvd. to the new greenway located on rail embankment.
- Increased height of rail embankments to access new greenway bridge over State Blvd.
- New greenway bridge architecture, and new bridge abutments, over State Blvd.
- Loss of historic neighborhood development pattern of the Brookview plat; the plat is of significance to the National Register listing.



Hope

October 4, 2012

Page 3

- Loss of houses and garages that are contributing resources in the Brookview-Irvington Park Historic District is an adverse effect; however if the historic context of the relationship between public spaces and private buildings is lost due to alterations and destruction of the Brookview plat, then carefully chosen demolitions should be considered as part of a broader mitigation strategy .
- Loss of spatial definition along linear public ways with removal of houses and the street removal along State Blvd. and Eastbrook.
- Medians placed within the roadway are not a character-defining feature of the Fort Wayne Park and Boulevard System as designed by George Kessler, nor are they a feature of the Brookview plat; if proposed they must be very carefully considered.

As this Section 106 process moves forward we are available to assist in the development of mitigating design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on the team.

Sincerely,



Thomas Cain, RLA, ASLA  
Senior Urban Designer



Creager Smith  
Historic Preservation Planner

**From:** "Carpenter, Patrick A" <[PA\\_Carpenter@indot.IN.gov](mailto:PA_Carpenter@indot.IN.gov)>

**Date:** October 5, 2012 4:17:47 PM EDT

**To:** "Hope, Briana" <[bhope@structurepoint.com](mailto:bhope@structurepoint.com)>, <[mgalbraith@archfw.org](mailto:mgalbraith@archfw.org)>, <[aquinn@archfw.org](mailto:aquinn@archfw.org)>, <[don.orban@cityoffortwayne.org](mailto:don.orban@cityoffortwayne.org)>, <[lzeiger@indianalandmarks.org](mailto:lzeiger@indianalandmarks.org)>, <[mbwedaman@frontier.com](mailto:mbwedaman@frontier.com)>, "Donnell, Julie" <[juliemarie57@earthlink.net](mailto:juliemarie57@earthlink.net)>, <[jlcooper@ccrtc.com](mailto:jlcooper@ccrtc.com)>, <[indianabridges@sbcglobal.net](mailto:indianabridges@sbcglobal.net)>, <[shan.gunawardena@cityoffortwayne.org](mailto:shan.gunawardena@cityoffortwayne.org)>, <[dan.avery@co.allen.in.us](mailto:dan.avery@co.allen.in.us)>, <[sislick@mac.com](mailto:sislick@mac.com)>, <[jandailey59@msn.com](mailto:jandailey59@msn.com)>, <[joyce.newland@fhwa.dot.gov](mailto:joyce.newland@fhwa.dot.gov)>, "Heil, Larry" <[larry.heil@fhwa.dot.gov](mailto:larry.heil@fhwa.dot.gov)>, <[jshoaff@proparkwest.com](mailto:jshoaff@proparkwest.com)>, "Kaiser, Jason" <[JASONKAISER@indot.IN.gov](mailto:JASONKAISER@indot.IN.gov)>, "Kennedy, Mary" <[MKENNEDY@indot.IN.gov](mailto:MKENNEDY@indot.IN.gov)>, <[linda@weintrautinc.com](mailto:linda@weintrautinc.com)>, <[creager.smith@cityoffortwayne.org](mailto:creager.smith@cityoffortwayne.org)>, <[albertcohan@aol.com](mailto:albertcohan@aol.com)>, <[tmn@barrettllaw.com](mailto:tmn@barrettllaw.com)>, <[rross@martin-riley.com](mailto:rross@martin-riley.com)>, <[dan@earthsourceinc.net](mailto:dan@earthsourceinc.net)>, "Glass, James" <[JGlass@dnr.IN.gov](mailto:JGlass@dnr.IN.gov)>, "Carr, John" <[JCarr@dnr.IN.gov](mailto:JCarr@dnr.IN.gov)>, <[kdietsch@comcast.net](mailto:kdietsch@comcast.net)>, <[alec.johnson@ci.ft-wayne.in.us](mailto:alec.johnson@ci.ft-wayne.in.us)>, "Crites, Scott" <[SCrites@structurepoint.com](mailto:SCrites@structurepoint.com)>, "Zielinski, Rich" <[BZielinski@structurepoint.com](mailto:BZielinski@structurepoint.com)>, "Smith, Gregory" <[GSmith2@indot.IN.gov](mailto:GSmith2@indot.IN.gov)>, "Johnson, Amy \ (DNR\)" <[AJohnson@dnr.IN.gov](mailto:AJohnson@dnr.IN.gov)>, "Ricketts, Amanda" <[ARicketts@dnr.IN.gov](mailto:ARicketts@dnr.IN.gov)>, "Tharp, Wade" <[WTharp1@dnr.IN.gov](mailto:WTharp1@dnr.IN.gov)>, <[tom.cain@cityoffortwayne.org](mailto:tom.cain@cityoffortwayne.org)>, "Hilden, Laura" <[lhilden@indot.IN.gov](mailto:lhilden@indot.IN.gov)>, "Mcmullen, Kenneth B." <[KMCMULLEN@indot.IN.gov](mailto:KMCMULLEN@indot.IN.gov)>

**Subject:** Section 106 Consulting Party Comments-State Blvd. (Des. #0400587)

Dear Consulting Parties,

Thank you for your comments . In light of the comments received during the most recent comment period and at the September 19, 2012 Consulting Parties meeting, the finding and 800.11 documentation will be updated. The updated finding, draft MOA and draft 4(f) Evaluation will be released with the Draft Environmental Assessment for an additional 30-day comment period. As such, we are not extending the current comment period, but providing for an additional 30-day comment period for both consulting parties and the public . This comment period will be initiated once the draft Environmental Assessment (EA) is released for public involvement by FHWA. The draft EA is expected to be released by the end of this year. The City of Fort Wayne and their consultants, Structurepoint, will continue to consult with FHWA and SHPO concerning comments received and measures to minimize adverse effects.

Thank you all for your continued participation.

Patrick Carpenter  
Manager, Cultural Resources Office  
Environmental Services  
Indiana Department of Transportation  
100 N Senate Ave., IGCN-Rm. N-642  
Indianapolis, IN 46204-2216  
[317-233-2061](tel:317-233-2061)

Phone Log

October 15, 2012

Conversation between Tom Cain & Linda Weintraut

At approximately 4:30 pm Tom Cain called to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but want to make them within the context of SHPO's assessment of project impacts and that the City addresses all adverse effects.

Cain also stated that impacts to the Brookview neighborhood should be enumerated.

Linda Weintraut said she would consult with Structurepoint and return Cain's call.



Phone Log

October 16, 2012

Conversation between Tom Cain & Linda Weintraut

I returned Tom's call from yesterday. I told him that Structurepoint was very glad to have his input on this project; at the very minimum, we would consult with him prior to the agency meeting and Briana was checking to see if it would be OK if he attends.

Tom spoke about the landscape changes that would be wrought as a result of the undertaking, particularly the changes from private to public space around the undertaking.

He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s.

He would like for mitigation to deal with changes in scale that will occur; tree planting should occur within 3 feet of the roadway (and not the standard 10 feet required on highways.) Tom believes that this would change the scale of the undertaking for the residents. He has other ideas that he will type up and send to Briana and I.

He said that it is important to achieve the "right feel" for the space.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739  
Phone 317-232-1646 • Fax 317-232-0693 • [dhpa@dnr.IN.gov](mailto:dhpa@dnr.IN.gov)



November 15, 2012

Linda Weintraut, Ph.D.  
Weintraut & Associates, Inc.  
Post Office Box 5034  
Zionsville, Indiana 46077

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana State Historic Preservation Officer's recommended changes to the October 17, 2012 draft of the "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No. )

Dear Dr. Weintraut:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the draft memorandum of agreement submitted with your October 16, 2012 e-mail message, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

In response to your acceptance of our offer to draft specific language for the memorandum of agreement ("MOA") to illustrate the recommendations we made in our October 4, 2012 letter, we have enclosed a marked-up copy of the draft MOA.

The most significant changes to the MOA reflect the extensive comments and recommendations we had offered in our October 4 letter, so we have not explained those changes here. We think that the reasoning behind the other changes is self-evident.

One issue that we noticed in the draft MOA but which we have not attempted to change is what appears to us to be the interchangeable use of "shall" and "will." We are not sure that it matters which word is used, but to the extent that they are being used synonymously, we would suggest that one or the other be used consistently.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 230953 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). If you have questions about issues pertaining to buildings or structures, please contact John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). Additionally, in all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.  
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

Enclosure

Linda Weintraut, Ph.D.  
November 15, 2012  
Page 2

emc: Joyce Newland, Indiana Division, Federal Highway Administration  
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration  
Patrick Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Briana Hope, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.





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## MEETING MINUTES

**Location:** INDOT Central Office, Room N601, Indianapolis, Indiana

**Date:** December 18, 2012

**Project Name:** State Boulevard Reconstruction (Des. No. 0400587)

**Project No.:** IN20071404

**Attendees:** Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)  
Shan Gunawardena, Tom Cain (City of Fort Wayne)  
Dr. Linda Weintraut (Weintraut & Associates)  
Dr. James Glass, John Carr, Chad Slider (IDNR, Division of Historic Preservation and Archaeology)  
Patrick Carpenter (INDOT, Cultural Resources)  
Larry Heil (Federal Highway Administration)  
Dan Avery (Northeastern Indiana Regional Coordinating Council)

**Conference Line:** MaryAnn Naber (Federal Highway Association)  
Jason Kaiser, Greg Smith (INDOT Fort Wayne District)

1. The meeting was held at 10:00 a.m., December 18, 2012, to discuss the following agenda items:
  - Section 106 Update
  - Mitigation Proposal
  - Draft MOA Stipulations/Mitigation
  - Next Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room and went through the agenda and explained that the primary purpose of the agency meeting was to discuss and work toward a consensus regarding the memorandum of agreement (MOA) and signatories. More specifically, the fate of the three residential homes located south of existing State Boulevard near Oakridge Drive.
3. Larry Heil, FHWA provided a recap of the Section 106 process as it related to the State Boulevard Project.
4. Larry Heil, FHWA requested that an updated Finding/800 document and MOA be provided to FHWA, INDOT, and SHPO.
5. MaryAnn Naber, FHWA asked when the consulting party (CP) members would see documentation. Briana Hope explained that the CP members had seen a signed finding/800 document, all previous correspondence, and a draft memorandum of agreement (MOA). Larry Heil explained that the next opportunity for CP members to review and provide comment on the Section 106 process (updated

Finding/800 Document and Draft MOA) would be when the Draft Environmental Assessment (EA) is released for public review. Review of the Draft 4(f) would also be available at this time.

6. Tom Cain explained that he believed that the primary impact to historical resources located in the project area is to the community plan and that impacts should be looked at as a whole package. He further explained the need to retain spatial quality of the streets and restore and recreate these spatial qualities by borrowing a model from elsewhere in the project. He explained that in order to save the three homes in question other potential and more meaningful historic mitigation opportunities are being compromised. See attached memo further supporting Tom Cain's assessment of impacts to the overall community plan and suggestions to offset those impacts.
7. Dr. James Glass, DHPA requested that a submittal of Tom Cain's discussion and mitigation proposals be submitted to SHPOs office to further review and digest the large amount of information presented. Dr. Glass also stated that he understood the impact to the community plan and the historic fabric, but that compensation for the loss of architectural features also needed to be provided for as part of the mitigation plan. He suggested researching other historically significant areas within the historic district that could be restored or preserved. Ideas also suggested included potential grant programs for façade restoration for residents within the historic district, potential relocation of homes to other lots within the historic district, restoration of structures within the historic district especially those along the boundaries of the district.
8. Larry Heil stated that FHWA would prefer to spend public funds on public lands. MaryAnn Naber explained that if a commitment was made in the MOA as part of a federal aid project that federal funds could be utilized to accomplish or complete the commitment.
9. Larry Heil, FHWA stated that he believes that the net benefit of leaving or enhancing the landscape along new State Boulevard is greater than allowing the contributing architectural structures in the floodway to remain. He also suggested that the 4(f) documentation needed to discuss that the houses are minimally contributing especially when left in a fragmented condition, and that this may be doing more harm or causing a greater historical impact by leaving them in place.

#### **ACTION ITEMS**

- American Structurepoint to submit revised MOA and finding/800 documentation to INDOT and FHWA for review and new signature.
- American Structurepoint to submit supplemental a summary of the potential mitigation measures presented to agencies for review.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or [bhope@structurepoint.com](mailto:bhope@structurepoint.com) to revise the record.

Very truly yours,  
American Structurepoint, Inc.

Briana M. Hope  
Environmental Project Manager

Enclosures



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739  
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



April 1, 2013

Karen Bobo  
Acting Division Administrator  
Federal Highway Administration, Indiana Division  
575 North Pennsylvania Street, Room 254  
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Version 2/20/2013) (Des. No. 0400587; Federal Project No. IN20071404; DHPA No. 5903)

Dear Ms. Bobo:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the documents submitted with American Structurepoint's March 1, 2013, letter, which we received on March 4, for the aforementioned project in Allen County, Indiana.

We concur with FHWA's February 27, 2013, Section 106 finding of Adverse Effect for the State Boulevard Reconstruction Project.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected by this project: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and State Boulevard Bridge over Spy Run (NBI No. 0200273).

Thank you for taking in to consideration our comments on an earlier draft of the draft memorandum of agreement ("MOA"). We have some specific suggestions regarding Version 2/20/2013 of the draft MOA.

In the past, the staff of the Advisory Council on Historic Preservation has recommended that in an MOA for a project where the design is important in determining effect, a reference be included in the MOA to the date of the project plans. Doing so clarifies which elements comprised the project at the time the adverse effect finding was made and serves to reduce the likelihood of later disagreement among the signatories to the MOA about the nature of changes to historic properties that the project may have. Although the supporting documentation provide here includes plan drawings, it occurs to us that it could still be early enough in the design process that it might be necessary for the City of Fort Wayne to make revisions to one or more of those drawings before construction begins. At the very least, plans showing how the project will incorporate context sensitive solutions, such as those recommended in stipulations I.A. and I.D.2., will have to be drawn. Consequently, it does not seem feasible to tie the finding and the MOA commitments for this project to any currently existing set of plans.

We think, however, that aside from new construction design issues, the subject most likely to result in future controversy pertains to the identities of the principal buildings that will be demolished by this project (if they are not demolished for other reasons beforehand). Various alternatives and variations on alternatives have been proposed, either by the City or by other consulting parties, during the Section 106 consultation on this project, and some of those proposals would have left more buildings standing than the preferred alternative now would. Instead of citing in this MOA the date of any



existing construction plans, one measure that could be incorporated into the MOA to avoid a misunderstanding about the extent of demolition that would be allowed in this project would be a listing of the street addresses of all principal buildings that will be demolished, including those that are not considered to be individually eligible for the National Register of Historic Places or to contribute to the significance of an eligible historic district. That way, all consulting parties will be able to visualize, in a general way, the geographic boundaries of the demolition that will be necessary to implement the as-yet-unfinished designs of the new State Boulevard alignment and of the related context sensitive solutions. A "whereas" clause in the preamble to the MOA might be the appropriate place to list those street addresses.

In the last line of Stipulation I.D.2., "the rights-of-way" appears to need a gerund in front of it to describe the kind of context sensitive solution that might be applied to rights-of-way within the project footprint.

The second paragraph of Stipulation III. requires that the content and design of the four interpretive plaques be provided to the Indiana SHPO and the (other) consulting parties for review and comment. However, that paragraph seemingly requires the City to attempt to accommodate only the recommendations of the Indiana SHPO. We suggest that the third sentence of that paragraph be revised to read as follows: "If the Indiana SHPO or any other consulting party responds with recommendations, a good faith effort to accommodate the recommendations will be made." We often have found, not surprisingly, that local consulting parties are better able than we are to evaluate the accuracy of the content and the appropriateness of the design of interpretive markers.

In regard to archaeology, we recommend the following revisions to the Stipulation V. ("Post Review Discovery") section of the Draft MOA. We recommend that the second sentence be revised to state, "Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1, 312 IAC 21, and 312 IAC 22, and the most current Guidebook for Indiana Historic Sites and Structures Inventory—Archaeological Sites."

Following your receipt of these comments and those of other consulting parties, and before you circulate a final version of the MOA for signature, please send the revised draft to all consulting parties for review and comment.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 232-1650 or [wtharp1@dnr.IN.gov](mailto:wtharp1@dnr.IN.gov). Questions about issues pertaining to buildings or structures should be directed to John Carr at (317) 233-1949 or [jcarr@dnr.IN.gov](mailto:jcarr@dnr.IN.gov). In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903 and address it to Chad W. Slider, Assistant Director for Environmental Review, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources, 402 West Washington Street, Room W274, Indianapolis, Indiana 46204.

Very truly yours,



Ron McAhron  
Deputy Director  
Indiana Department of Natural Resources

RM:WTT:JLC:jlc

cc: Christine Meador, American Structurepoint, Inc.

emc: Karen Bobo, Federal Highway Administration, Indiana Division  
Joyce Newland, Federal Highway Administration, Indiana Division  
Lawrence Heil, P.E., Federal Highway Administration, Indiana Division  
Patrick Carpenter, Indiana Department of Transportation  
Mary Kennedy, Indiana Department of Transportation  
Shaun Miller, Indiana Department of Transportation  
Melany Prather, Indiana Department of Transportation  
Christine Meador, American Structurepoint, Inc.  
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

**MEMORANDUM OF AGREEMENT**  
**BETWEEN THE FEDERAL HIGHWAY ADMINISTRATION AND**  
**THE INDIANA STATE HISTORIC PRESERVATION OFFICER**  
**SUBMITTED TO THE ADVISORY COUNCIL ON HISTORIC PRESERVATION**  
**PURSUANT TO 36 C.F.R. Section 800.6(b)(iv)**  
**REGARDING THE STATE BOULEVARD RECONSTRUCTION FROM SPY RUN TO CASS**  
**STREET**  
**IN FORT WAYNE, WAYNE TOWNSHIP, ALLEN COUNTY, INDIANA**

**WHEREAS** the Federal Highway Administration ("FHWA") proposes to widen and realign a portion of State Boulevard for the State Boulevard Reconstruction from Spy Run to Cass Street (Des No.: 0400587) in, Fort Wayne, Wayne Township, Allen County, Indiana ("Project"); and

**WHEREAS** the FHWA, in consultation with the Indiana State Historic Preservation Officer ("SHPO"), has defined this State Boulevard Reconstruction from Spy Run to Cass Street's area of potential effects, as the term defined in 36 C.F.R. Section 800.16(d), to be the area extending 250 feet from the alley west of Cass Street to the abandoned New York Central Railroad, encompassing the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway; and

**WHEREAS** the FHWA, in consultation with the Indiana SHPO, has found that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and the State Boulevard Bridge over Spy Run are within the area of potential effects; and

**WHEREAS** the FHWA and the Indiana SHPO both recognize that the Fort Wayne Park and Boulevard System and Brookview-Irvington Park Historic District are listed in the National Register of Historic Places; and

**WHEREAS** the FHWA and the Indiana SHPO both recognize that the State Boulevard Bridge over Spy Run has been determined eligible for listing in the National Register of Historic Places per the Indiana Statewide Historic Bridge Inventory; and

**WHEREAS** the FHWA, in consultation with the Indiana SHPO, has determined pursuant to 36 C.F.R. Section 800.5(a) that the State Boulevard Reconstruction from Spy Run to Cass Street will have an adverse effect on the Fort Wayne Park and Boulevard System, Brookview-Irvington Historic District, and the Bridge over Spy Run; and

**WHEREAS** the FHWA, Indiana Department of Transportation ("INDOT"), the Indiana SHPO, and the Advisory Council on Historic Preservation ("Council") have executed in 2006 a Programmatic Agreement on the Management and Preservation of Indiana's Historic Bridges and the FHWA has determined that the State Boulevard Bridge over Spy Run is "Non-Select" (not considered an excellent example of a given type or not suitable candidate for preservation); and

**WHEREAS** the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Section 800) to resolve the adverse effect on the Fort Wayne Park and Boulevard System and Brookview-Irvington Historic District; and

**WHEREAS** the FHWA has consulted with the Indiana SHPO and both recognize that 2244 Eastbrook Drive is a non-contributing resource within the Brookview-Irvington Historic District and will be demolished as part of the project; and

**WHEREAS** the FHWA has consulted with the Indiana SHPO and both recognize that following properties are contributing resources within the Brookview-Irvington Historic District and will be demolished as part of this undertaking: 2221 Westbrook Drive; 112 East State Boulevard; 128 East State Boulevard; 134 East State Boulevard; 138 East State Boulevard; 142 East State Boulevard; 146 East State Boulevard; 154 East State Blvd; 158 East State Boulevard; 162 East State Boulevard; 2252 Eastbrook Drive; 2248 Eastbrook Drive; 2240 Eastbrook Drive; and 2236 Eastbrook Drive; and

**WHEREAS** the public was given an opportunity to comment on the undertaking's adverse effect in a notice published on **\*(give date(s) of publication)\*** in the **\*(give name of publication)\***; and

**WHEREAS** the FHWA has notified the Council of the adverse effect and invited the Council's participation in the project, pursuant to 36 CFR Section 800.6(a)(1), in a letter dated August 29, 2011 and in additional material conveyed on May 10, 2012; and

**WHEREAS** the Council declined to participate in consultation in a letter dated July 31, 2012; and

**WHEREAS** the FHWA, in consultation with the Indiana SHPO, has invited the INDOT and the City of Fort Wayne to participate in the consultation and to become a signatory/signatories to this memorandum of agreement; and

**WHEREAS** the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Part 800) concerning the scope of work on March 23, 2009; July 2, 2009; November 9, 2009; December 1, 2009; December 15, 2009; December 28, 2009; February 4, 2010; May 19, 2011; June 17, 2011; July 6, 2011; July 13, 2011; August 15, 2011; August 16, 2011; September 1, 2011; September 2, 2011; September 29, 2011; May 22, 2012; June 20, 2012; July 2, 2012; July 16, 2012, December 18, 2012, and March 1, 2013; and agreed to proceed with the project as proposed April 23, 2009; December 14, 2009; July 5, 2011; November 7, 2011; June 22, 2012; August 13, 2012 and April 1, 2013; and

**NOW, THEREFORE**, the FHWA and the Indiana SHPO agree that, upon the submission of a copy of this executed memorandum of agreement, as well as the documentation specified in 36 C.F.R. Section 800.11(e) and (f) to the Council pursuant to 36 C.F.R. Section 800.6[b][1][iv]) and upon the FHWA's approval of the State Boulevard Reconstruction from Spy Run to Cass Street, the FHWA shall ensure that the following stipulations are implemented in order to take into account the effect of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

### **Stipulations**

FHWA will ensure that the following measures are implemented:

#### **I. CONTEXT SENSITIVE SOLUTIONS**



A. The City of Fort Wayne shall consider and, where feasible, shall implement context sensitive solutions for this undertaking, including but not limited to: the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.

B. The City of Fort Wayne shall consider and, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences.

C. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:

1. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
2. Context sensitive solutions that may include but not be limited to: protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; and maintaining pedestrian connections along the former Eastbrook and Westbrook drives shall be included among the measures considered.
3. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
4. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.

5. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail or SR 27) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.

6. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

7. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before scheduled meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.

8. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

9. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.

## II. PHOTOGRAPHIC DOCUMENTATION

- A. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory

Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges.”

- B. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
  - 1. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
  - 2. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
- C. This documentation will include black and white prints of digital photographs and a digital video disc (“DVD”) containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards” that are in effect at the time.
  - 1. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
  - 2. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
  - 3. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
  - 4. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.

### III. EDUCATIONAL PROGRAMMING

The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler’s landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps in public projects.

The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within fifteen (15) days, acceptance will be assumed. If the Indiana SHPO or any other consulting party responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.



#### IV. OBJECTION RESOLUTION PROVISION

Disagreement and misunderstanding about how this memorandum of agreement is or is not being implemented shall be resolved in the following manner:

A. If the Indiana SHPO or any invited signatory to this memorandum of agreement should object in writing to the FHWA regarding any action carried out or proposed with respect to the State Boulevard Reconstruction from Spy Run to Cass Street or implementation of this memorandum of agreement, then the FHWA shall consult with the objecting party to resolve this objection. If after such consultation the FHWA determines that the objection cannot be resolved through consultation, then the FHWA shall forward all documentation relevant to the objection to the Council, including the FHWA's proposed response to the objection. Within 45 days after receipt of all pertinent documentation, the Council shall exercise one of the following options:

1. Provide the FHWA with a staff-level recommendation, which the FHWA shall take into account in reaching a final decision regarding its response to the objection; or
2. Notify the FHWA that the objection will be referred for formal comment pursuant to 36 C.F.R. Section 800.7(c), and proceed to refer the objection and comment. The FHWA shall take into account the Council's comments in reaching a final decision regarding its response to the objection.

B. If comments or recommendations from the Council are provided in accordance with this stipulation, then the FHWA shall take into account any Council comment or recommendations provided in accordance with this stipulation with reference only to the subject of the objection. The FHWA's responsibility to carry out all actions under the memorandum of agreement that are not the subjects of the objection shall remain unchanged.

#### V. POST REVIEW DISCOVERY

In the event that archaeological artifacts (sites), human remains, or one or more historic aboveground properties—other than Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run—are discovered or that unanticipated effects on historic properties are found during the implementation of this memorandum of agreement, the FHWA shall follow the procedure specified in 36 C.F.R. Section 800.13, as well as and IC 14-21-1-27 and IC 14-21-1-29, by stopping work in the immediate area and informing the Indiana SHPO and the INDOT Cultural Resources Section of such unanticipated discoveries or effects within two (2) business days. Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1, 312 IAC 21, and 312 IAC 22, and the most current Guidebook for Indiana Historic Sites and Structures Inventory – Archaeological Sites.

#### IV. AMENDMENT

Any signatory to this memorandum of agreement may request that it be amended, whereupon the parties shall consult to consider the proposed amendment. 36 C.F.R. 800.6(c)(7) shall govern the execution of any such amendment.

#### V. TERMINATION

A. If the terms of this memorandum of agreement have not been implemented within five years of the onset of construction, then this memorandum of agreement shall be considered null and void. In such an event, the FHWA shall so notify the parties to this

memorandum of agreement and, if it chooses to continue with the State Boulevard Reconstruction from Spy Run to Cass Street, then it shall reinitiate review of the State Boulevard Reconstruction from Spy Run to Cass Street in accordance with 36 C.F.R. Sections 800.3 through 800.7.

B. Any signatory to the memorandum of agreement may terminate it by providing thirty (30) days notice to the other parties, provided that the parties shall consult during the period prior to termination to seek agreement on amendments or other actions that would avoid termination. In the event of termination, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

C. In the event that the FHWA does not carry out the terms of this memorandum of agreement, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

The execution of this memorandum of agreement by the FHWA, the City of Fort Wayne, and the Indiana SHPO, the submission of it to the Council with the appropriate documentation specified in 36 C.F.R. Section 800.11(e) and (f), and the implementation of its terms evidence that the FHWA has afforded the Council an opportunity to comment on the State Boulevard Reconstruction from Spy Run to Cass Street and its effect on historic properties and that the FHWA has taken into account the effects of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

**SIGNATORIES (required):**

FEDERAL HIGHWAY ADMINISTRATION

Signed by: \_\_\_\_\_ Date: \_\_\_\_\_

Name and Title: \_\_\_\_\_

(Typed or printed)

DRAFT



INDIANA STATE HISTORIC PRESERVATION OFFICER

Signed by: \_\_\_\_\_ Date: \_\_\_\_\_

Name and Title: \_\_\_\_\_

(Typed or printed)

DRAFT

**INVITED SIGNATORIES**

INDIANA DEPARTMENT OF TRANSPORTATION

Signed by: \_\_\_\_\_ Date: \_\_\_\_\_

Name and Title: \_\_\_\_\_

DRAFT

THE CITY OF FORT WAYNE

Signed by: \_\_\_\_\_ Date: \_\_\_\_\_

Name and Title: \_\_\_\_\_

(Typed or printed)

(If an entity has responsibilities under the MOA, include that entity's name here)

DRAFT



## Public Notice

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

The existing reinforced concrete girder, T-beam bridge over Spy Run Creek was constructed in 1927. It is listed as 40.3 feet long, 48.2 feet wide, with a height of 9.5 feet. A.W. Grosvenor and O. Darling are credited as the designers. The July 17, 2006, Structure Inventory and Appraisal Report listed the structure as in poor condition, with a Sufficiency Rating of 27.9 (structurally deficient). The bridge, which is listed as contributing to the proposed Brookview-Irvington Park National Register Historic District, has also been determined eligible for listing in the National Register of Historic Places, according to information in Volume 2 of the Indiana Historic Bridge Inventory. This bridge has been rated as non-select in M&H Architecture, *Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges (draft)*, based on a report by HNTB.

In an effort to preserve this historic bridge, the City of Fort Wayne is notifying interested parties of the availability of the bridge over Spy Run Creek for reuse or salvage of elements that may be stored and used for future repair of similar historic bridges. This notice is being published in accordance with the Programmatic Agreement (PA) among Federal Highway Administration (FHWA), Indiana Department of Transportation (INDOT), State Historic Preservation Officer (SHPO), and Advisory Council on Historic Preservation (ACHP) for the Management and Preservation of Indiana's Historic Bridges, stipulation III.B.2. As required, this notice is being posted a minimum of six months in advance of the public hearing.

Because the *Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges*, is still a draft report, it is possible the bridge can be listed as Select in the final report. If the bridge is listed as Select, demolition will not be an option and alternative preservation options must be evaluated in accordance with the PA.

The recipient agency, organization, or individual will be responsible for all costs associated with relocation or reuse of any elements associated with the bridge, including but not limited to site preparation, reassembly of the bridge, any structural work required for the proposed use, liability and long-term maintenance, and any required permits.

Before submitting a Letter of Interest (LOI), any interested agency, organization or individuals are encouraged to visit INDOT's Historic Bridges Marketing Program (<http://www.in.gov/indot/2967.htm>) or contact American Structurepoint, Inc. (contact information below), in order to obtain a copy of the Structure Inventory and Appraisal Report. LOIs for this bridge will be accepted prior to and up to 15 days following the public hearing. If no recipient is identified or selected, the bridge will be demolished following the PA among FHWA, INDOT, SHPO, and ACHP for the Management and Preservation of Indiana's Historic Bridges, Attachment B, Standard Treatment Approach for Historic Bridges, Demolition.

For more information, or to submit an LOI, please contact:

Hayley Steele, Environmental Scientist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256  
Telephone: (317) 547-5580  
[hsteele@structurepoint.com](mailto:hsteele@structurepoint.com)

# The Journal Gazette

Allen County, Indiana

Account # 1064183 - 794026  
American Structurepoint Inc

## PUBLISHER'S CLAIM

### LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall  
total more than four solid lines of the type in which the body of the  
advertisement is set) -- number of equivalent lines \_\_\_\_\_

Head -- number of lines \_\_\_\_\_

Body -- number of lines \_\_\_\_\_

Tail -- number of lines \_\_\_\_\_

Total number of lines in notice

118

### COMPUTATION OF CHARGES

118 lines, 1 column(s) wide equals

118 equivalent lines at \$ 0.371 cents per line

\$ 43.78

Additional charges for notices containing rule or tabular work  
(50 per cent of above amount) -

Charge for extra proofs of publication  
(\$1.00 for each proof in excess of two) -

TOTAL AMOUNT OF CLAIM

\$ 43.78

### DATA FOR COMPUTING COST

Width of single column in picas . . . . 9.8 Size of type . . . . 7point.

Number of Insertions . . . . 1

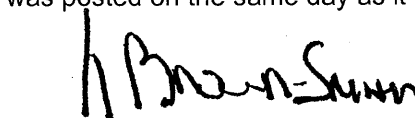
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times.

The dates of publication being as follows:

2/5/2010

Additionally, Newspaper has a Web site and this public notice was posted on the same day as it was published in The Journal Gazette.



T. Brown-Smith  
Legal Clerk

Date: February 5, 2010

## PUBLIC NOTICE

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

The existing reinforced concrete girder, T-beam bridge over Spy Run Creek was constructed in 1927. It is listed as 40.3 feet long, 48.2 feet wide, with a height of 9.5 feet. A.W. Grosvenor and O. Darling are credited as the designers. The July 17, 2006, Structure Inventory and Appraisal Report listed the structure as in poor condition, with a Sufficiency Rating of 27.9 (structurally deficient). The bridge, which is listed as contributing to the proposed Brookview-Irvington Park National Register Historic District, has also been determined eligible for listing in the National Register of Historic Places, according to information in Volume 2 of the Indiana Historic Bridge Inventory. This bridge has been rated as non-select in M&H Architecture, Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges (draft), based on a report by HNTB.

In an effort to preserve this historic bridge, the City of Fort Wayne is notifying interested parties of the availability of the bridge over Spy Run Creek for reuse or salvage of elements that may be stored and used for future repair of similar historic bridges. This notice is being published in accordance with the Programmatic Agreement (PA) among Federal Highway Administration (FHWA), Indiana Department of Transportation (INDOT), State Historic Preservation Officer (SHPO), and Advisory Council on Historic Preservation (ACHP) for the Management and Preservation of Indiana's Historic Bridges, stipulation III.B.2. As required, this notice is being posted a minimum of six months in advance of the public hearing.

Because the Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges, is still a draft report, it is possible the bridge can be listed as Select in the final report. If the bridge is listed as Select, demolition will not be an option and alternative preservation options must be evaluated in accordance with the PA.

The recipient agency, organization, or individual will be responsible for all costs associated with relocation or reuse of any elements associated with the bridge, including but not limited to site preparation, reassembly of the bridge, any structural work required for the proposed use, liability and long-term maintenance, and any required permits.

Before submitting a Letter of Interest (LOI), any interested agency, organization or individuals are encouraged to visit INDOT's Historic Bridges Marketing Program (<http://www.in.gov/indot/2967.htm>) or contact American Structurepoint, Inc.



(contact information below), in order to obtain a copy of the Structure Inventory and Appraisal Report. LOIs for this bridge will be accepted prior to and up to 15 days following the public hearing. If no recipient is identified or selected, the bridge will be demolished following the PA among FHWA, INDOT, SHPO, and ACHP for the Management and Preservation of Indiana's Historic Bridges, Attachment B, Standard Treatment Approach for Historic Bridges, Demolition.

For more information, or to submit an LOI, please contact:

Hayley Steele,  
Environmental Scientist  
American Structurepoint, Inc.  
7260 Shadeland Station  
Indianapolis, Indiana 46256  
Telephone: (317) 547-5580  
hsteele@structurepoint.com

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**The News-Sentinel**

Allen County, Indiana

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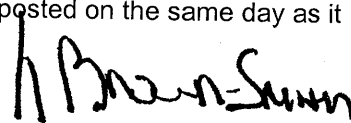
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ATTACH COPY OF ADVERTISEMENT HERE

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