



AMERICAN
STRUCTUREPOINT
INC.

ENVIRONMENTAL ASSESSMENT

Des. No. 0400587

State Boulevard Reconstruction Project
State Boulevard between Spy Run Avenue and Cass Street
including the bridge over Spy Run Creek

City of Fort Wayne
Allen County, Indiana

Prepared for

City of Fort Wayne, Board of Public Works
Citizen Square
200 East Berry Street, Suite 240
Fort Wayne, Indiana 46802

Prepared by

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May 2, 2014

Indiana Department of Transportation

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FHWA-Indiana Environmental Document

**CATEGORICAL EXCLUSION/ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:

State Boulevard/Allen County

Designation Number:

0400587 (Phase 1, Terrace Road to Spy Run Avenue -1005151, Phase 2, Cass Street to Terrace Road -1005154, Pedestrian Bridge-1005155, and Spy Run Creek Bridge-1005152)

Project Description/Termini:

State Boulevard Reconstruction Project– State Boulevard between Spy Run Avenue and Cass Street including the bridge over Spy Run Creek

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

<input type="checkbox"/>	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
<input type="checkbox"/>	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services).
<input type="checkbox"/>	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA.
<input checked="" type="checkbox"/>	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA.

Note: For documents prepared by or for Environmental Services, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

ESM Signature 	Date <u>5.14.14</u>	ES Signature 	Date <u>8 MAY 14</u>
FHWA Signature	Date		

Release for Public Involvement

ESM Initials _____	Date _____	ES Initials _____	Date _____
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Certification of Public Involvement

Examiner, Public Hearings Signature _____ Date _____

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

Name and organization of CE/EA Preparer: Briana M. Hope – American Structurepoint, Inc.

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Discuss what public involvement activities (legal notices, letters to affected property owners and residents, meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

Survey notice letters were sent to adjacent property owners on March 18, 2009, informing them of the proposed project. Copies of the survey notice letters are included in Appendix F pages F-2 to F-3.

Public Information Meetings (5 Meetings)

A total of five public meetings have been held throughout the development of this project. Public information meetings were initiated by the City in 2008 and 2009 (September 11, 2008; October 27, 2008; November 17, 2008; May 28, 2009; and September 30, 2009) to solicit input from the public during the early design stages of the proposed project. The public meetings consisted of formal presentations and opportunities for public questions and comments. In addition, a design charrette was utilized as part of the September 30, 2009, public meeting to further aid the City of Fort Wayne in collecting information regarding how the connecting streets should intersect with the new State Boulevard. Information gathered from the charrette was evaluated and taken into consideration and incorporated into the proposed design. The public meetings were held in varying locations to allow the public to attend meetings that would be most convenient and easily accessible to them. Meetings were held at the City Building, Northside High School, and the Allen County Public Library.

Neighborhood Association Meetings (13 Meetings)

The City of Fort Wayne attended multiple neighborhood meetings to present project information and address project questions and concerns. In most cases, the City attended a regularly scheduled meeting held in the neighborhoods, but also met with individual representatives of associations when requested. As the Brookview Civic Neighborhood is located within the proposed project limits, the majority of the meetings involved this neighborhood association or individual representatives from the association. In an effort to help adjacent property owners better understand the proposed project, a representative from the project team met twice in the field to walk the proposed project with interested individuals from the Brookview neighborhood. In addition to the Brookview Civic Neighborhood, the City also met with neighborhood associations outside the limits of the project. The purpose of these meetings was to answer questions and concerns expressed about the project and discuss how they would be affected as they travel through the area whether by motorized vehicles or other modes of transportation. The additional neighborhood associates consisted of Northside Neighborhood Association, Historic Oakwood Neighborhood Association, West Central Neighborhood Association, Bloomingdale Neighborhood Association, and Forest Park Neighborhood Association.

Open House Events (3 Events)

The City of Fort Wayne conducted a series of three open house events to present preliminary renderings of the preferred alignment to the public. These meetings were held on February 25, 2013, from 5:00 PM to 7:00 PM at the Franke Pond Pavilion located at 3411 Sherman Boulevard, Franke Parke, Fort Wayne; on March 1, 2013, from 11:00 AM to 1:00 PM at the Main Branch Allen County Public Library, Meeting Room A, 900 Library Plaza, Fort Wayne; and on March 7, 2013, from 5:00 PM to 7:00 PM at the Psi Ote Barn - Lower Level, Bob Arnold Northside Park, located at East State Boulevard and Parnell Avenue, Fort Wayne. Renderings were also available for comment on the City of Fort Wayne website. Comments were accepted at the open house, on-line, via email, and US Postal Service. For reference to renderings presented, see Appendix F pages F-25 to F-32.

Other Group and Individual Meetings (27 Meetings)

When requested, the City of Fort Wayne met with individuals, including representatives of interested groups, business owners, and adjacent property owners. The City met with these individuals to help explain the project, provide project updates, and address comments and concerns. Meeting with these individuals and representatives further helped the City ensure information regarding the project was reaching the public. Representatives from the varying groups brought comments and concerns to the City and distributed project information to their groups.

See Appendix F page F-4 for a list of all meetings, dates, and locations.

Section 106 (3 Consulting Party Meetings)

The Section 106 Area of Potential Effect (APE) determination (36 CFR 800.4(a)(1)) and the Adverse Effect determination (36 CFR 800.11(e)) were approved by FHWA on February 27, 2013, and distributed to the State Historic Preservation Officer (SHPO) on March 1, 2013. Upon release for public involvement for this document, copies of both

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this document and the approved Adverse Effect determination will be submitted to Consulting Parties for review. A public notice describing the project and the Section 106 finding of "Adverse Effect" will be published in local media in conjunction with the Legal Notice of Public Hearing.

The bridge over Spy Run Creek was advertised for reuse, per the Historic Bridges Programmatic Agreement (HBPA). A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-490 to C-496. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

In addition, three consulting party meetings were also held to discuss the findings of Historical Properties Report, effect findings, and options to avoid, minimize and mitigate adverse effects to the surrounding cultural resources. Meetings were held on December 15, 2009; September 1, 2011; and September 19, 2012. A total of 35 individuals, representing the FHWA, State, City, neighborhood associations, historic preservation groups, and adjacent property owners were invited to participate in the consulting party meetings.

For reference to consulting party consultation see Appendix C pages C-2 to D-476.

Public Hearing

The Indiana Department of Transportation (INDOT) Public Involvement Procedures Policy requires a public hearing be scheduled and held for projects classified as EAs. A Legal Notice of Public Hearing will be published twice in local media, and may be mailed via First Class US Mail to adjacent property owners and local or state officials whom may have an interest in the proposed project, and may be posted on the City of Fort Wayne website. The EA will be made available for public review. Comments will be accepted for 30 days following the hearing. The public hearing will include an informal open house, formal presentation, and comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

A public notice describing the project and the Section 4(f) *de minimis* finding associated with Vesey Park will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FONSI) request document submitted to the FHWA.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes

☒ X

No

☐

Remarks:

During the preliminary project development, multiple citizens and consulting parties have expressed their opposition to the proposed project and the proposed impacts associated with the identified cultural resources and the overall footprint of the project.

As part of the Section 106 process, multiple consulting parties have expressed their concern associated with the project purpose and need as well as the magnitude of potential impact the preferred alternative would have on the identified historic resources within the project area.

Opportunity for a Public Hearing Required

Yes

☒ X

No

☐

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Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: City of Fort Wayne INDOT District: Fort Wayne
Local Name of the Facility: State Boulevard

Funding Source: ☒ Federal ☐ State ☒ Local ☐ Private

PURPOSE AND NEED:

Describe the problem that the project will address.

The purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe traveling environment for motorists, bicyclists, or pedestrians, as the existing roadway is congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run Creek and/or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity, and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (1-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run Avenue and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one westbound lane. East of Clinton Street, State Boulevard is a 2-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stellhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

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Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E
Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E
Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements, which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 feet. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards. For further reference to the IDM see http://www.in.gov/indot/design_manual/design_manual_2013.htm.

Curve Radius Table:

Station Line "A"	Existing Curve Radius	Required Radius (30 mph)
18+66.60	175 feet	300 feet
24+64.47	243 feet	300 feet
27+23.73	210 feet	300 feet

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway's safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 feet along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 feet along State Boulevard to safely make the right turn maneuver. As noted in the "Intersection Sight Distance Table" below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approximate Existing Sight Distance (feet)	Required Sight Distance (feet)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Drive (South)	LT	150	420
Westbrook Drive (North)	LT	210	420
Eastbrook Drive (South)	LT	270	420
Eastbrook Drive (South)	RT	210	375
Eastbrook Drive (North)	LT	250	420
Terrace Road (North)	RT	160	375

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Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top 20 high crash locations in Allen County for the time period 2007-2011. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0, which indicates that a safety problem exists.

Crash Location	2 0 0 7				2 0 0 8				2 0 0 9				2 0 1 0				2 0 1 1			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard and Eastbrook Drive	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11	9	1	0	1.26	12	3	0	1.69
State Boulevard and Clinton Street	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38	30	3	0	2.04	36	8	0	2.45
State Boulevard And Spy Run Avenue	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48	27	7	0	1.63	43	11	0	2.60
State Boulevard and Westbrook Drive	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16	9	1	0	1.26	12	3	0	1.69

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System, which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9, which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is five years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Creek Flood Control Study (Christopher B. Burke, 2005), "this flooding is caused primarily by backwater from the St. Mary's River, which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Seven out of the 17 years (1978, 1982, 2003, 2004, 2005, 2008, and 2009), State Boulevard was closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

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PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Allen
Municipality: Fort Wayne
Limits of Proposed Work: State Boulevard between Spy Run Avenue and Cass Street in Fort Wayne
Total Work Length / Area: 0.45 Miles

Is an Interchange Modification Study/Interchange Justification Study (IMS/IJS) required?
If yes, when did the FHWA grant a conditional approval for this project?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the Remarks box below, describe in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The current preferred alternative is Alternative 3A. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four (4) lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have four (4) 10-foot travel lanes, two (2) in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot-wide raised median. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven (7) feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four (4) 11-foot travel lanes, two (2) in each direction, separated by a 12 foot 2-way left turn lane. The overall alternative length is 2,370 feet. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway.

Access to existing State Boulevard would be via a new access road, which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing State Boulevard intersections with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from five (5) feet to ten 10 feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five (5) feet wide, would be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary. New curb inlets and storm sewer would be constructed throughout the project limits. A new bridge structure would replace the existing bridge over Spy Run Creek. The proposed bridge would be elevated approximately seven (7) feet to eliminate roadway flooding along State Boulevard. As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would extend from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail, which would be constructed by others.

For the entire proposed project, a total of approximately 3.80 acres of new permanent and 2.50 acres of temporary right-of-way would be required. Based on 2015 costs, the estimated cost of the project is \$10,372,000.

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OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Alternative 1: Butler Road – Vance Road Corridor: This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts to the Franke Parke Elementary School and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project; however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

Alternative 1 results in the use of the Brookview-Irvington Historic District (northern extents), Vesey Park, and Franke Park, all Section 4(f) resources.

Alternative 1 is not reasonable as it does not address any of the Project's purpose and need. Alternative 1 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential and commercial relocations for construction and approximately 2.0 miles of new roadway through existing forested land. For these reasons, Alternative 1 has been eliminated from further consideration.

Alternative 2: Spring Street – Tennessee Avenue: This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places (NRHP).

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central museum, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project; however, the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative would result in the use of 4(f) resources including Lakeside Park, Lawton Park, and the NRHP eligible bridge over the St. Joseph River.

The alternative is not reasonable as it does not address any part of the Project's purpose and need. Alternative 2 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential, commercial, and recreational property impacts/relocations for construction. For these reasons, Alternative 2 has been eliminated from further consideration.

Alternative 3B: Widen State Boulevard on Existing Alignment: This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four lanes. This alternative would require a new bridge with additional travel

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lanes over Spy Run Creek. The overall alternative length is 2,700 feet.

This alternative would require approximately 18 residential relocations (contributing properties) from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. Furthermore, this alternative requires a higher number of residential and historic property relocations for construction as compared to other alternatives.

Alternative 3C: Shift State Boulevard Alignment South: This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4 lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. The existing intersection of State Boulevard with Eastbrook Drive would be eliminated and converted to a cul-de-sac. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Terrace Road and State Boulevard. The Terrace Road extension would be required to provide access to the neighborhood north of existing State Boulevard as a result of access restrictions due to Clinton Street being a one-way south roadway. This alternative would also require a new bridge over Spy Run Creek at an elevation seven feet above the existing bridge elevation.

Similar to Alternative 3A, the realignment of State Boulevard and change in elevation would result in the bifurcation of the Brookview-Irvington Park Historic District. Contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the existing bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of Eastbrook Drive (north of State Boulevard) to a cul-de-sac, the replacement of the bridge over Spy Run Creek, and the removal of five contributing properties, the landscape of the area would be modified altering the character and setting of the district. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad will also change the character of the district along State Boulevard. Furthermore, the realignment of State Boulevard would require the acquisition of right-of-way from the Fort Wayne Park and Boulevard System Historic District, again altering the historic location of State Boulevard. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). The prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over the contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing new visual element to the Fort Wayne Park and Boulevard System Historic District.

While this alternative would reduce the number of contributing property relocations on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Boulevard would cross Spy Run Creek; impacts to the creek would be increased by approximately 330 linear feet for the purposes of re-grading. The new bridge length would be approximately 250 feet longer than the bridge design included in Alternatives 3A or 3D. This alternative would also require construction of a new intersection of State Boulevard with Clinton Street. The new intersection would be built in close proximity to the new Terrace Road intersection which would significantly impede traffic operations and efficiency as well as increase project costs due to additional traffic signal work. The increased length of the proposed bridge combined with relocating the roadway south would also require the intersection of State Boulevard and Clinton Street to be raised two to three feet, thus causing additional reconstruction along Clinton Street (approximately 500 feet) and further increasing project costs. In addition to the nine residential relocations that are also considered contributing resources, this alternative would result in the relocation of four commercial businesses, including the gas station at the southwest corner of Clinton Street and State Boulevard, a plumbing business on the southeast corner, a dog grooming business located just south of the gas station, and a storage unit business located on the southwest corner of Spy Run Avenue and State Boulevard.

Alternative 3C addresses the project’s congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street which would create new operational and safety issues due to its close proximity to the new Terrace Road intersection. Project costs associated with Alternative 3C are an estimated five million dollars more than any other alternative due to increased impacts to commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street.

Alternative 3D: Substandard Horizontal Curve Correction with a 3-Lane Typical Section: This alternative is similar to Alternative 3A but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes and correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have two ten foot travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes would be separated by a twelve-foot wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes would be separated by a twelve foot two way left turn lane. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four eleven foot travel lanes, two in each direction, separated by a twelve foot two way left turn lane. As appropriate, left turn lanes would be

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installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway. As a part of this project, the new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

By reducing the typical section from 4-lanes (Alternative 3A) to 3-lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this alternative would continue to result in the same 4(f) use as Alternative 3A to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run Creek.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3D does not fully address corridor connectivity or traffic congestion concerns along the corridor. This alternative would not address the congestion concerns at the intersection of State Boulevard and Clinton Street. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within an urban area. This intersection currently functions at a low Level of Service. Alternative 3D would not address the poor Level of Service (E/F) at State Boulevard and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion along the corridor, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain. Furthermore, this alternative would result in the same use of 4(f) resources as compared to Alternative 3A.

Alternative 4: No Build: With the No Build Alternative, there would be no use of resources subject to Section 4(f) provisions. This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate. The existing roadway would continue to flood causing continued problems with accessibility and pavement deterioration. Traffic accidents would most likely continue to increase as the current congestion issues would not be addressed. The existing bridge over Spy Run Creek is currently rated structurally deficient and the estimated remaining life of the superstructure is five years. This structure is in immediate need of replacement due to the condition. East-west connectivity would continue to be a problem for the overall transportation network. The no build alternative would likely result in the complete failure of the structure over Spy Run Creek.

The No Build Alternative would not meet any of the needs of the project; therefore, is not considered a feasible and prudent alternative.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems, or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe) _____

X
X
X
X

ROADWAY CHARACTER:

Functional Classification:	<u>Minor Arterial</u>	
Current ADT:	<u>20,650 VPD 2009</u>	Design Year ADT: <u>26,200 VPD 2030</u>
Current Year DHV	<u>1,730 VPH</u>	Design Year DHV <u>2,620 VPH</u>
Trucks (%)	<u>2</u>	Trucks (%) <u>2</u>
Designed Speed (mph):	<u>35</u>	Legal Speed (mph): <u>30</u>

	Existing	Proposed
Number of Lanes:	<u>2</u>	<u>5</u>
Type of Lanes:	<u>Through Travel Lanes</u>	<u>4 through travel lanes and 1 left turn lane when required</u>
Pavement Width:	<u>10</u> ft.	<u>10-11</u> ft.
Shoulder Width:	<u>NA</u> ft.	<u>NA</u> ft.
Median Width:	<u>NA</u> ft.	<u>8</u> ft.
Sidewalk Width:	<u>5</u> ft.	<u>6 - 10</u> ft.

Setting:	<input checked="" type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input type="checkbox"/> Rural
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES:

Structure Number(s): Allen County Bridge No. 00546 Sufficiency Rating: 27.9 (2006 Allen County Structure Inventory and Appraisal Report)

	Existing	Proposed
Bridge Type:	Concrete Girder	Continuous Composite Prestressed Concrete Box Beam
Number of Spans:	<u>1</u>	<u>3</u>
Weight Restrictions:	<u>NA</u> ton	<u>NA</u> ton
Height Restrictions:	<u>NA</u> ft.	<u>NA</u> ft.
Curb to Curb Width:	<u>24</u> ft.	<u>56</u> ft.
Outside to Outside Width:	<u>26</u> ft.	<u>85.83</u> ft.
Shoulder Width:	<u>1</u> ft.	<u>2</u> ft.
Length of Channel Work:	<u>NA</u> ft.	<u>270</u> ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks:

The existing bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010). The Bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

The proposed bridge over Spy Run Creek would be a three span, continuous, composite, prestressed concrete box beam structure. The proposed span lengths are 28 feet, 58 feet, and 28 feet. The structure would have a total bridge width of 85 feet and 10 inches, and would be comprised of four 10-foot travel lanes, a 12-foot left turn lane, with 2-foot shoulders. In addition, a 16-foot, 2-inch wide sidewalk on the north side and a 12-foot, 8-inch wide sidewalk on the south side are also proposed. The clear roadway width is 56 feet and the proposed structure would be skewed 30-degrees to the left.

Will the structure be rehabilitated or replaced as part of the project? Yes ☒ No ☐ N/A ☐
If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One (1) travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.

From Clinton Street to Spy Run Avenue, 2-way traffic will be maintained on the existing westbound lanes of existing State Boulevard while the proposed east bound lanes are being constructed. Once the eastbound lanes are built, 2-way traffic will be maintained on the newly constructed eastbound lanes until the proposed west bound lanes are constructed.

From Westbrook Drive to Clinton Street, 2-way traffic will be maintained on the existing roadway and bridge structure while the new alignment portions of the eastbound State Boulevard lanes and bridge structure are constructed to the south of the existing alignment. Once the eastbound portion of proposed State Boulevard is constructed, 2-way traffic will be maintained on the proposed eastbound lanes while the westbound lanes and remaining bridge structure are constructed.

From Cass Street to Westbrook Drive, 2-way traffic will be maintained on the westbound lanes of existing State Boulevard while the eastbound lanes are being constructed. Temporary asphalt pavement widening may be required on the northern side of State Boulevard between Cass Street and Westbrook Drive to accommodate 2-way traffic. Once the proposed eastbound lanes are constructed, 2-way traffic will be maintained on the eastbound lanes while the westbound lanes are being constructed.

MOT plans were included as part of the plan sets made available for public review at the three open house events hosted by the City (February 25, 2013, March 1, 2013, and March 7, 2013). No comments or concerns have been received regarding the MOT plan.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 1,062,295 Right-of-Way: \$ 2,300,000 Construction: \$ 1,500,000/6,572,000
(FY 2015) (FY 2017/2018)

Anticipated Start Date of Construction: April 1, 2015

Date project incorporated into STIP July 11, 2013

If in an MPO area, location of project in TIP on pages 42, 43, and 51* which was incorporated by reference into The STIP on July 11, 2013

*Administrative modification processed for project to account for the change in Year of Expenditure for Right-of-Way and Construction costs.

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Agricultural	0.00	0.00
Commercial	1.06	0.57
Forest	0.00	0.00
Industrial	0.00	0.00
Other	0.00	0.00
Other: Park	0.55	0.12
Residential	2.19	1.81
Wetlands	0.00	0.00
TOTAL	3.80	2.50

Remarks: Approximately 3.80 acres of additional permanent right-of-way will be acquired for the construction of the proposed project. Existing right-of-way currently extends approximately 25 feet from the centerline on both sides of State Boulevard. The right-of-way to be acquired will be primarily residential; however, some right-of-way will also be acquired from commercial areas. Acquisition of 15 whole parcels is anticipated as part of the proposed project. Acquisition of 15 residential structures is anticipated.

Approximately 2.50 acres of temporary right-of-way will be acquired for grading, driveway construction, and tie-ins. Project plans, including existing and proposed right-of-way limits, are included in Appendix A pages A-11 to A-129 of

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this document.

All right-of-way will be acquired in accordance with applicable federal and state procedures. Those procedures include specific requirements for appraisals, review appraisals, negotiations, and relocation benefits. Compliance with these procedures will assure the fair and equitable treatment of affected residents and businesses. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 as amended.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Streams, Rivers, Watercourses & Jurisdictional Ditches	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
State Wild, Scenic or Recreational River	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

There is one stream located within the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. One stream, Spy Run Creek, was identified as potential "waters of the US". Defined bed and bank were observed to be associated with Spy Run Creek. An ordinary high water mark (OHWM) was estimated at a depth of 1.5 feet. Spy Run Creek flows south through the project area under existing State Boulevard eventually outletting into the Saint Mary's River.

This stream is not a state natural, scenic, or recreational river. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E pages E-2 to E-12.

Based on the preliminary project design, avoidance of all waterways is not possible. The bridge carrying State Boulevard over Spy Run Creek will completely span the ordinary high water mark; however, impacts as a result of storm water outfalls, existing bridge removal, and channel grading are unavoidable. The total permanent impacts to waterways associated with the project are 292 linear feet and include a temporary crossing for construction, storm water outfalls, and stream bank stabilization for erosion control purposes.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Other Surface Waters				
Reservoirs	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

There are no other surface waters located in the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E, pages E-2 to E-12.

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Presence

Yes

No

Impacts

Yes

No

Wetlands

☐
☒
☐
☐

Total wetland area: 0 acre(s) Total wetland area impacted: 0 acre(s)
(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Impacted Acres – Permanent	Impacted Acres - Temporary	Total Impacted Acres	Comments
Totals:					

Wetlands

Wetland Determination

Wetland Delineation Report

USACE Isolated Waters Determination

Mitigation Plan

Documentation

Yes

No

☒
☒
☐
☐
☐
☐
☒
☒

ES Approval Dates

LPA Project/Red Flag

LPA Project

Jurisdiction for all waterways will be given to the USACE

Individual

Wetland

Finding

Yes

No

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

☐
☐
☐
☐
☐
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☐

Measures to avoid, minimize and mitigate wetland impacts need to be discussed in the remarks section

Remarks:

There are no wetlands located in the project corridor. This was initially determined by referencing aerial photography and USGS Topographic Mapping and field verified by American Structurepoint personnel during the August 14, 2009, field visit to conduct a wetland delineation and waters investigation. For reference, see the Ecological Evaluation Form and attachments prepared for the project corridor, which is located in Appendix E pages, E-2 to E-12.

Presence

Yes

No

Impacts

Yes

No

Terrestrial Habitat

☒
☐
☒
☐

Use the remarks table to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks:

Terrestrial habitat within the project corridor includes residential yard and grassed passive park along Spy Run Creek. Approximately 2.19 acres of residential property and 0.55 acre of grassed passive park are located within the project study area and will be impacted by the proposed project. None of these areas are considered significant or sensitive habitat.

The US Fish and Wildlife Service (USFWS), in their April 20, 2009, early coordination response letter provided comments relative to impacts to wetlands, streams, and forested areas. USFWS indicated they felt shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore, trees lost to the project should be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail. The USFWS letter also indicated there is no known habitat for any endangered species within the project area and stated the project is not likely to adversely affect endangered species. For reference to this coordination see Appendix B, page B-15 TO B-16.

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Appropriate stormwater best management practices will be implemented as part of the project and stormwater collection system. In addition, a landscaping plan is proposed as part of this project. The landscaping plan will help address the replacement of trees removed from residential yards and along the Spy Run Creek corridor. Trees will be planted along the proposed roadway and remaining green spaces in an effort to mitigate for the anticipated loss of trees as well as to help preserve the park like appearance currently associated with this segment of State Boulevard..

Coordination with the IDNR on November 18, 2009, recommended appropriate sediment and erosion control measures and restrictions to minimize impacts to fish, wildlife, and botanical resources. IDNR stated the Natural Heritage Program's data indicated no plant or animal species listed as state or federally threatened, endangered, or rare have been reported in the project vicinity. For reference to this coordination see Appendix B, page B-19.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Are karst features located within or adjacent to the footprint of the proposed project?

If yes, will the project impact any of these karst features?

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Use the remarks table to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

The project is located outside of the designated karst area of the state as identified in the October 13, 1993, Memorandum of Understanding (MOU). No karst features were observed or are known to exist within or adjacent to the proposed project area. The 1993 Karst MOU is not applicable to this project, and a karst assessment is not required. Project location mapping is included in Appendix A, page A-2. No karst features were noted on the Red Flag Investigation Mapping included in Appendix D pages D-2 to D-14.

Threatened or Endangered Species

Within the known range of any federal species?

Any critical habitat identified within project area?

Federal species found in project area (based upon informal consultation)?

State species found in project area (based upon consultation with IDNR)?

Is Section 7 formal consultation required for this action?

Presence		Impacts	
Yes	No	Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>		
<input type="checkbox"/>	<input checked="" type="checkbox"/>		
<input type="checkbox"/>	<input checked="" type="checkbox"/>		

Remarks:

Coordination with the IDNR on November 18, 2009, confirmed the Natural Heritage Database has been checked and to date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project's vicinity. See Appendix B, page B-19 for reference to the IDNR coordination letter.

Coordination with the USFWS on April 20, 2009, indicated the proposed project area is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the candidate eastern massasauga rattlesnake (*Sistrurus catenatus*). Re-coordination with USFWS on March 19, 2014, indicated that the endangered species in All County, Indiana had been revised. In addition to the previously identified species, Allen County is now within the range of the Federally endangered rayed bean mussel (*Villosa fabalis*) and the proposed endangered northern long-eared bat (*Myotis septentrionalis*). There is no known habitat for any of these species within the proposed project area; therefore, the proposed project is not likely to adversely affect these endangered, proposed endangered, and candidate species. In addition, both the April 20, 2009 and March 19, 2014 USFWS coordination stated "this precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation." See Appendix B, page B-15 to B-16 and B-25 to B-26 for reference to the USFWS coordination letters.

SECTION B – OTHER RESOURCES

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	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Drinking Water Resources				
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input checked="" type="checkbox"/>		
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>		
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>		
Source Water Protection Area(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Wellhead Protection Area	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: The proposed project is located in Allen County; therefore, the project is not located within the area of the St. Joseph Sole Source Aquifer the only legally designated sole source aquifer in Indiana. The FHWA/EPA Sole Source Aquifer MOA is not applicable to this project, and a groundwater assessment is not required.

Review of the Wellhead Proximity Locator (<http://idemmaps.idem.in.gov/whpa/>) on March 15, 2013, indicated the proposed project area is not located in a wellhead protection area.

Drinking water is provided by the City of Fort Wayne within the project area. Existing water mains will be replaced as necessary throughout the project corridor.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Flood Plains				
Longitudinal Encroachment	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the project located in a FEMA designated floodplain?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks: Per the INDOT Categorical Exclusion manual, the proposed project includes a new bridge on new alignment, and is therefore considered a Category 5 project.

A hydraulic design study has been performed by American Structurepoint, and concluded that the project will meet all requirements of the Indiana Design Manual, and may therefore be considered to have no adverse impact on the floodplain. This hydraulic study was approved on May 13, 2010, by INDOT Hydraulics Section. A summary of this study is included in Appendix E, pages E-13 to E-17.

There will be no substantial impacts on natural and beneficial floodplain values; there will be no substantial change in flood risks; and there will be no substantial increase in potential for interruption or termination of emergency service or emergency evaluation routes; therefore it has been determined that this encroachment is not substantial. A map depicting the mapped DFIRM flood plain boundaries is included in Appendix E, pages E-11 to E-12.

Formal permit approval of the IDNR under the Flood Control Act (IC 14-28) will be obtained for this project.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Farmland				
Agricultural Lands	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
NRCS Form AD-1006/CPA-106 scored \geq 160?	<input type="checkbox"/>	<input checked="" type="checkbox"/>		

Provide the NRCS Form AD-1006/CPA-106 score and state whether there is a significant loss of farmland as a result of the project in the remarks section. See CE Manual for guidance to determine which NRCS form is appropriate for your project.

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Remarks:

As is required by the Farmland Protection Policy Act (FPPA), the NRCS has been coordinated with (March 10, 2009). The NRCS indicated that the project will not cause a conversion of prime farmland, Appendix B, page B-8. Since there will not be a conversion of prime farmland, the requirements of the FPPA are not applicable and the completion of the CPA-106 is not required. No other alternatives other than those already discussed in this document will be considered without a reevaluation of the project's potential impacts upon farmland. This project will not have a significant impact to farmland.

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates
Minor Projects PA Clearance			

Eligible and/or Listed Resource Present

Results of Research

	Yes	No
Archaeology		X
History/Architecture	X	
NRHP Buildings/Site(s)	X	
NRHP District(s)	X	
NRHP Bridge(s)	X	

Project Effect

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
No Historic Properties Affected		X	
No Adverse Effect		X	
Adverse Effect	X		FHWA: 02/27/2013 SHPO: 04/01/2013

Documentation Prepared

Documentation

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
Historic Properties Short Report		X	
Historic Property Report	X		ES: 07/16/2012 SHPO 08/13/2012
Archaeological Records Check/ Review	X		
Archaeological Phase Ia Survey Report	X		ES: 07/16/2012 SHPO 08/13/2012
Archaeological Phase Ic Survey Report		X	
Archaeological Phase II Investigation Report		X	
Archaeological Phase III Data Recovery		X	
APE, Eligibility and Effect Determination	X		FHWA: 02/27/2013 SHPO 04/01/2013
800.11 Documentation	X		FHWA: 02/27/2013 SHPO 04/01/2013
Memorandum of Agreement	X		Approval date to be documented in FONSI request to FHWA

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work, which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

Area of Potential Effect (APE): The APE is centered on State Boulevard in Fort Wayne, Wayne Township, Allen County, Indiana. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway. Maps depicting the APE are included in Appendix C, pages C-134 to C-137.

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The archaeological APE is defined as the project footprint.

Coordination with Consulting Parties: An invitation to consulting parties and a request for participation in the Section 106 process was provided to federal, state, and local agencies initially on March 23, 2009. Additional requests (multiple dates) for participation in the process was provided as individuals or groups expressed interest. Those agencies were invited to be consulting parties and participate in the development of the project in accordance with provisions of Section 106 of the National Historic Preservation Act.

The following is a list of organizations and individuals that were invited or requested to be consulting parties. If no response was received to the consulting party invitation after 30 days, it was assumed the parties involved did not wish to act as consulting parties. FHWA, INDOT, and SHPO are considered automatic consulting parties.

Organization/Name	Response
Indiana Historical Society	No response
Fort Wayne City Council	Added 03/23/2009
ARCH, Inc.	Participant 03/26/2009
Allen County Historian	Participate 03/27/2009
Fort Wayne Historic Preservation Review Board	Participant 04/02/2009
Indiana Landmarks (formerly known as Historic Landmarks Foundation), Northern Regional Office	Participate 04/13/2009
Brookview Neighborhood Association	Participate 05/01/2009
Indiana Historic Spans Task Force	Participate 05/01/2009
Friends of the Parks of Allen County	Participate 05/22/2009
City of Fort Wayne	Participate 06/01/2009
Allen County Historical Society	No response
Irvington Park Neighborhood Association	Participate 07/09/2009
Historic Bridge Expert, James L. Cooper	No response
Adjacent Property Owner, Susan Haneline	Added 12/01/2009
Northside Galleries	Added 11/07/2009
Adjacent Property Owner, Karl Dietsch	Added 12/01/2009
Northeastern Indiana Regional Coordinating Council	Added 12/01/2009
Adjacent Property Owner, Annette "Jan" Dailey	Added 12/01/2009
Westbrook 5, LLC	Added 12/06/2009
Barrett & McNagney, LLP	Added 12/06/2009
Martin Riley Architects and Engineers	Added 12/06/2009
Earth Source, Inc.	Added 12/15/2009
Spy Run Neighborhood Association	No response
Five Points Neighborhood Association	No response
Bloomingtondale Neighborhood Association	No response
Advisory Council on Historic Preservation	Declined 07/31/2012

Archaeology: Archaeological Consultants of Ossian completed an Archaeological Field Reconnaissance of the proposed State Boulevard Reconstruction Project on April 2, 2009. No archaeological sites were located during the field reconnaissance. The Archaeological Field Reconnaissance Report concluded no properties on or eligible for listing on the NRHP will be affected by the proposed project. In reviewing the area previously surveyed by Archaeological Consultants of Ossian it was determined that there were areas within the limits of the preferred alternative for the proposed State Boulevard Improvements Project that had not been surveyed. On July 11, 2012, Archaeological Consultants of Ossian completed the Indiana Archaeological Short Report, for the additional area required for the State Boulevard Improvements project. The short report was reviewed and approved by the Indiana Department of Transportation, Cultural Resources (INDOT-CR) on July 16, 2012, and the State Historic Preservation Officer (SHPO) on August 13, 2012.

Historic Properties: A Historic Properties Report (HPR) was prepared by The Westerly Group, Inc. in September 2009, for the proposed State Boulevard Reconstruction Project. Historic properties were identified and evaluated in accordance with current Section 106 federal regulations. Four properties were recommended as eligible for listing on the NRHP including 315 East State Boulevard, the proposed Brookview-Irvington Park Historic District, the bridge carrying East State Boulevard over Spy Run Creek, and State Boulevard (within the historic district). In February 2012, Weintraut & Associates, Inc. prepared an Additional Information Report (AI) to append the HPR. The AI was prepared to supplement the HPR following the inclusion of two new NRHP-listed resources within the APE. As part of the AI investigation two districts were identified that were listed in the NRHP after the HPR (2009) was prepared. Portions of both the Fort Wayne Park and Boulevard System Historic District (NRHP, 2010) and Brookview-Irvington Park Historic District

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(NRHP, 2011) are contained within the project APE. The AI further recommended that the portion of State Boulevard within the Brookview-Irvington Park Historic District is best represented as a contributing component of the NRHP historic districts and would not be recommended eligible as an individual resource, and that 315 East State Boulevard does not meet the criteria to be eligible for listing in the NRHP. The AI was reviewed and approved by INDOT-CR on May 10, 2012, and SHPO on June 22, 2012.

Documentation, Findings: Two historic properties are listed in the NRHP: Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District. One historic property has previously been determined eligible for the NRHP: Bridge over Spy Run Creek.

- Fort Wayne Park and Boulevard System Historic District (NRHP, 2010)—Adverse Effect
- Brookview-Irvington Park Historic District (NRHP, 2011)—Adverse Effect
- Bridge over Spy Run Creek (NBI No. 0200273)—Adverse Effect

The Section 106 APE Determination (36 CFR 800.4(a)(1)), and the Finding of Adverse Effect (36 CFR 800.6(a)(3), was approved by Federal Highway on February 27, 2013 and concurred with by the SHPO on April 1, 2013. The Section 800 Determination and Finding Documentation, signed by FHWA will be sent to all consulting parties at the same time the Environmental Assessment is released for public involvement. A Draft Memorandum of Agreement (MOA) was prepared to outline the proposed 'Adverse Effect' the project will have on the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District and the proposed mitigation for those adverse impacts. The Bridge over Spy Run Creek falls within the scope of the HBPA; and therefore, does not require an MOA for the adverse effect the project will have on the resource. The Draft MOA will be distributed to the IDNR-DHPA and consulting parties at the same time the Environmental Assessment is released for public involvement. Once the MOA is finalized and signed it will be forwarded to the ACHP for their information and record.

Public Involvement: Three consulting party meetings were held to discuss the findings of Historical Properties Report, effect findings, and options to avoid, minimize and mitigate adverse effects to the surrounding cultural resources. A total of 35 individuals, representing the FHWA, State, City, neighborhood associations, historic preservation groups, and adjacent property owners were invited to participate in the consulting party meetings. Meetings were held on December 15, 2009, September 1, 2011, and September 19, 2012. Meeting minutes can be found in the Section 106 Documentation in Appendix C, pages C-222 to C-224, C-340 to C-348, and C-427 to C-434.

A multitude of comments were received from consulting parties during the Section 106 process. Most comments received were to express concern with the scope and magnitude of the project and the significant impact it will have on the Brookview-Irvington Parks Historical District. Copies of all Section 106 consulting party comments can be found in Appendix C, pages C-189 to C-485.

The bridge over Spy Run Creek was advertised for reuse, per the HBPA. A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-491 to C-493. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

A public notice describing the project and the Section 106 finding of "Adverse Effect" will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. This will also be the final chance for a responsible party to come forward to fund perseveration of the bridge. Should no party come forward within 30 days and the draft MOA be approved the 106 process will be concluded. A summary of any comments received and the disposition of those comments will be included in the FONSI request packet to be reviewed by FHWA prior to their issuance of a FONSI.

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SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES Approval/dates</u>
	Yes	No	Yes	No	
Parks & Other Recreational Land					
Publicly owned park	X		X		
Publicly owned recreation area		X			
Other (school, state/national forest, bikeway, etc.)		X			
Programmatic Section 4(f)		X			
Individual Section 4(f) Evaluation		X			
“De minimis” Impact	X				Pending FONSI

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES Approval/dates</u>
	Yes	No	Yes	No	
Wildlife & Waterfowl Refuges					
National Wildlife Refuge		X			
State Fish and Wildlife Area – recreation or refuge areas only		X			
Programmatic Section 4(f)		X			
Individual Section 4(f) Evaluation		X			
“De minimis” Impact		X			

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES approval/dates</u>
	Yes	No	Yes	No	
Historic Properties					
Sites eligible and/or listed on the NRHP	X		X		
Programmatic Section 4(f)	X				Historic Bridge PA Pending FONSI
Individual Section 4(f) Evaluation	X				Pending FONSI
“De minimis” Impact		X			

Discuss Programmatic Section 4 (f) and De minimis Section 4(f) impacts in the remarks section below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, De minimis and Individual Section 4(f) documents please refer to the “Procedural Manual for the Preparation of Environmental Studies.” Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Parks and other Recreational Land

“De minimis” Impact – Vesey Park: One property, Vesey Park was noted in the project limits as a Section 4(f) resource. This park is operated by the City of Fort Wayne Parks Department and includes the green space along Spy Run Creek between Eastbrook Drive and Westbrook Drive connecting the larger portion of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary’s River. The park features open space among the trees with areas for picnicking and views to Spy Run Creek. This undertaking would convert approximately 0.55-acre of permanent right-of-way to a transportation use for the installation of a new bridge over Spy Run Creek and State Boulevard. Avoidance of this resource is not feasible as the existing roadway crosses Spy Run Creek and Vesey Park and one purpose of the project is to replace the existing bridge. Coordination with the City of Fort Wayne Parks Department regarding the proposed project was undertaken. The City of Fort Wayne Parks Department provided a letter in support of this project on January 23, 2013. The project will have a *de minimis* effect on Vesey Park, a Section 4(f) property, as it will not adversely affect the activities, features, and attributes that qualify Vesey Park for protection under Section 4(f). For reference to the communication see Appendix J page J-2 to J-8.

A public notice describing the project and the Section 4(f) *de minimis* finding associated with Vesey Park will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. Comments or concerns brought forth by the public during this process will be addressed in the Finding of No Significant Impact (FNOSI) request document submitted to the FHWA.

Historical Properties

It has been determined two historic districts and a historic bridge eligible for listing in the NRHP exist within the APE of this project. The undertaking will affect the Fort Wayne Park and Boulevard System Historic District, the Brookview-

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Irvington Park Historic District, and the Bridge over Spy Run Creek.

Programmatic Section 4(f) – Bridge over Spy Run Creek: The Bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010) and is thus considered a Section 4(f) resource based upon 23 CFR 774.11(e). The Bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. As part of the project, the bridge will be removed and replaced on new alignment.

The project falls within the stipulations for the Historic Bridges Programmatic Section 4(f). Per the Programmatic Section 4(f) Evaluation and Approval for FHWA Projects that Necessitate the Use of Historic Bridges, three specific alternatives must be evaluated prior to the use of a historic bridge. The following are these alternatives, along with findings that are supported through consultation with consulting parties:

1. Do Nothing. The do nothing alternative has been studied. The do nothing alternative ignores the basic transportation need. For the following reasons this alternative is not feasible and prudent:
 - a. Maintenance - The do nothing alternative does not correct existing deficiencies that cause the bridge to be considered structurally deficient or deteriorated. These deficiencies can lead to sudden collapse and potential injury or loss of life. Normal maintenance is not considered adequate to cope with the situation.
 - b. Safety - The do nothing alternative does not correct the situation that causes the bridge to be considered deficient.

Because of these deficiencies the bridge poses serious and unacceptable safety hazards to the traveling public and places intolerable restriction on transport and travel.

2. Build on New Location Without Using the Old Bridge. Investigations have been conducted to construct a new bridge on a new location or parallel to the old bridge (allowing for a 1-way couplet).
 - a. Preservation of Old Bridge - It is not feasible and prudent to preserve the existing bridge, even if a new bridge were to be built at a new location. The existing bridge carrying State Boulevard over Spy Run Creek provides an insufficient waterway opening and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. *State Boulevard Reconstruction From Spy Run Creek to Cass Street, Version February 20, 2013, Fort Wayne, Allen County, Indiana Des. No.: 0400587 Federal Project Number: IN20071404* 17. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River frequently, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Creek Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River, which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure. This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides an insufficient waterway opening and is quickly deteriorating.

3. Rehabilitation without Affecting the Historic Integrity of the Bridge. Studies have been conducted of rehabilitation measures, but, for the following reason, this alternative is not feasible and prudent:
 - a. The bridge is so structurally deficient that it cannot be rehabilitated to meet minimum acceptable load requirements without affecting the historic integrity of the bridge.

The project's alternatives were developed using the July 17, 2006, Programmatic Agreement (PA) on Indiana's Historic Bridges, as well as guidance provided on this PA by INDOT subsequent to its enactment. According to the Indiana Historic Bridge Inventory report dated December 2010, the Bridge over Spy Run Creek (NBI No. 0200273) is considered

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a non-select candidate for inclusion on the NRHP. As such, the project was evaluated utilizing guidance from this PA for non-select bridges.

Initial Section 4(f) alternatives were sent out with the HPR to consulting parties and SHPO on August 15, 2011, along with the invitation to the September 1, 2011 Consulting Party Meeting. The initial alternatives were discussed and further developed as a result of input received during Consulting Party meetings which were held throughout the development of the project. The final Section 4(f) alternatives were included in the Section 800 documentation presented by INDOT to FHWA for their review and comment and approved on February 27, 2013. The Section 800 documentation was then submitted to SHPO for review on March 1, 2013 and concurred with on April 1, 2013.

The Alternatives Analysis resulted in the identification of a preferred alternative (described previously in this document in the Project Description Section) that includes replacement of the existing bridge, thus resulting in an "Adverse Effect". FHWA signed the finding of "Adverse Effect" on February 27, 2013. By signature of this document, the FHWA has concluded that there is no feasible and prudent alternative to the Section 4(f) use of the Bridge over Spy Run Creek (NBI No. 0200273) through replacement.

The Bridge over Spy Run Creek (NBI No. 0200273) was advertised for reuse, per the HBPA. A notice was published in the *Fort Wayne Journal Gazette*, indicating a six month period during which interested parties could submit proposals for reuse of the bridge. Affidavits are found in Appendix C, pages C-490 to C-496. The bridge was advertised on the INDOT website, and signs were also placed at each end of the bridge, indicating the same six month response period. No responses were received regarding the notices.

A public notice describing the project and the Programmatic Section 4(f) will be advertised concurrently with the EA release for public involvement in local media. The public notice will solicit comments regarding the project for a 30-day comment period. This will also be the final chance for a responsible party to come forward to fund preservation of the bridge. If a responsible party does not take ownership of the bridge it will be demolished.

Individual Section 4(f) Evaluation – Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Historic District: The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of 11 parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and Bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NRHP in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized." Approximately 0.60 acres of permanent right-of-way will be acquired from this district as part of the proposed project.

The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total of 424 contributing resources including houses, garages, and the combined plats of the district, as well as the previously determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965, represents the construction dates of most buildings within the historic district, and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination. Approximately 2.60 acres of permanent right-of-way and 15 residential relocations will be required from this district as part of the proposed project.

This undertaking will convert property from two historic districts and an historic bridge, all NRHP eligible properties, to a transportation use. The FHWA has determined the appropriate Section 106 finding is Adverse Effect for both districts and therefore, an Individual Section 4(f) evaluation was undertaken. An Individual 4(f) Document has been prepared, which discusses project use of the Section 4(f) resources. The Individual 4(f) Document evaluated and summarized the proposed project's purpose and need, reasonable alternatives, Section 4(f) resources, and all possible planning to minimize harm to those resources. The report identified Alternative 3A as the alternative which would cause the least over all harm in light of the statute's preservation purpose. This alternative includes widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4 lanes while correcting the substandard horizontal curve. For reference to the Section 4(f) evaluation, see Appendix J pages J-9 to J-51.

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Mitigation measures have been detailed in a Memorandum of Agreement (MOA) to be executed by consulting parties. As mitigation for unavoidable impacts to each of the NRHP listed historic districts, the City of Fort Wayne shall implement context sensitive design solutions for this undertaking, salvage architectural details from homes to be demolished, explore funding opportunities for neighborhood improvements, and convene an Advisory Team to ensure the project is developed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. The Bridge over Spy Run Creek falls within the scope of the HBPA; and therefore, does not require an MOA for the adverse effect the project will have on the resource.

In compliance with Section 4(f), pursuant to 23 CFR Part 774.5, the draft Section 4(f) documentation was provided to the US Department of Interior (DOI) for review and comment on May 24, 2013. The DOI provided comments on July 8, 2013 (Appendix J, pages J-52 to J-53). The DOI indicated they would tend to concur with the FHWA and INDOT that there are no feasible and prudent alternatives to the preferred alternative, if built as proposed, which would result in impacts to Section 4(f) properties. Constrained linear features such as State Boulevard offer few good alternatives when 4(f) resources have grown up on either side of the corridor and the functionality of the feature becomes compromised by growing populations. DOI also states that as recently as this last December, there was still considerable disagreement over the project and its mitigation. The Department cannot concur with the INDOT and FHWA because there is no evidence that all parties, including the SHPO, have agreed to the mitigation measures, or is there evidence in the evaluation that the MOA has been signed. DOI reserves their concurrence with the hope that the final 4(f) will present the necessary agreements. For reference to the Section 4(f) documentation see Appendix J, pages J-9 to J-51.

A public notice describing the project and the Individual Section 4(f) Evaluation for impacts to the Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Historic District will be advertised concurrently with the EA release for public involvement in the local media. The public notice will solicit comments regarding the project for a 30-day comment period. After the conclusion of the comment period efforts will be made to finalize the MOA and obtain concurrence from all necessary signatories. Once the MOA has been signed and the Section 4(f) has been finalized it will be submitted to DOI for final concurrence. The Individual Section 4(f) document will then be reviewed by FHWA for legal sufficiency. Comments or concerns brought forth during this process will be addressed in the FONSI request document submitted to the FHWA.

Section 6(f) Involvement

Section 6(f) Property

<u>Presence</u>		<u>Use</u>	
Yes	No	Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The project will not involve any properties acquired by or improved with the Land and Water Conservation Fund (LWCF). The US Department of the Interior, National Park Service LWCF Detailed Listing of Grants Grouped by County was reviewed for Allen County. Twenty-four sites were noted in Allen County, all of which are outside of the project area. Therefore, there is no Section 6(f) involvement and there will be no taking of LWCF property. DNR's Division of Outdoor Recreation early coordination response (April 7, 2009) also confirmed that no LWCF properties are within the project area. See Appendix B, pages B-27 to B-28 for a copy of the Allen County 6(f) property listings.

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SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes

No

☒

☐

If YES, then:

Is the project in the most current MPO TIP?

☒

☐

Is the project exempt from conformity?

☐

☒

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

☒

☐

Is a hot spot analysis required (CO/PM)?

☐

☒

Is an MSAT level 1a Analysis required?

☐

☒

Is an MSAT level 1b Analysis required?

☒

☐

Is an MSAT level 2 Analysis required?

☐

☒

Is an MSAT level 3 Analysis required?

☐

☒

Is an MSAT level 4 Analysis required?

☐

☒

Is an MSAT level 5 Analysis required?

☐

☒

Remarks:

The project area is located within the air quality maintenance area of ozone and attainment for particulate matter. Copies of the air quality maps are included in Appendix G pages G-16 to G-18. The FY 2014 to 2017 Transportation Improvement Program for the Northeastern Indiana Regional Coordinating Council was found to conform to air quality regulations and incorporated by reference into the FY 2014 to 2017 State Transportation Improvement Program on July 11, 2013. The proposed project is regionally significant and non-exempt. For reference to the planning documents see Appendix G, pages G-8 to G-15.

The purpose of this project is to improve vehicular and pedestrian/bicycle safety along State Boulevard. This project has been determined to generate minimal air quality impacts for CAAA criteria pollutants and has not been linked with any special MSAT concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause an increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES model forecasts a combined reduction of over 80 percent in the total annual emission rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 100 percent. This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

SECTION F - NOISE

Noise

Is a noise analysis required in accordance with FHWA regulations and INDOT's noise policy?

Yes

No

☒

☐

No

Yes/ Date

ES Approval of Noise Analysis

☐

10/18/2011 (Technical Sufficiency)

Remarks:

The proposed State Boulevard Reconstruction Project proposes road improvements on new alignment utilizing federal funds. Under the provisions of 23 CFR, part 772, the project is considered a "Type I" noise project requiring an analysis of potential noise impacts and, if so, whether there are feasible and reasonable ways to mitigate those impacts.

A noise analysis was prepared by the Corradino Group following the guidance in the Federal Highway Administration's (FHWA's) *Highway Traffic Noise: Analysis and Abatement Guidance* (July 2010) and the Indiana Department of Transportation's (INDOT's) *Procedural Manual for Preparing Environmental Documents* and its *Traffic Noise Policy* (July 2011).

Noise measurements were made in conformance with Federal Highway Administration (FHWA) guidance at six locations that represent 63 residential receivers present within 500 feet of the proposed improvement (the analysis distance criterion set in INDOT's *Traffic Noise Policy*). The noise measurement locations represent worst case

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locations for all homes in what are considered noise sensitive areas. An additional measurement was made at another noise sensitive receiver, North Side High School, beyond the east construction limit of the proposed project. Land use at the west project end is commercial, as it is in the east, with the exception of the school. The residential receivers fall into land use category B in terms of FHWA's Noise Abatement Criteria (NAC) (Table 1). The applicable noise criterion for this land use is 67 dBA in terms of the one-hour equivalent noise level, expressed as L_{eq} (1h). Because Part 772 defines potential impacts in terms of noise levels approaching or exceeding the NAC and INDOT's *Noise Policy* defines approaching as one decibel, the effective value for impact analysis in Indiana for land use category B is 66 dBA, rather than 67 dBA. The school falls into NAC land use category C, which is subject to the same NAC dBA criterion.

Existing measured noise levels did not approach or exceed the NAC at any receiver, with the exception noted below. Analysis using the Traffic Noise Model (TNM2.5) validated the noise measurements obtained in the field. TNM2.5 modeling also finds no receivers will experience future project noise levels that approach or exceed the NAC, with the same exception. And, no modeled receiver will experience predicted noise levels that substantially exceed existing noise levels (INDOT's *Noise Policy* defines this as 15 dBA). So, except for measurement site 2S, there are no noise impacts and no mitigation is needed.

Measurement site 2S represents a home on the south side of State Boulevard, where the new alignment joins the existing alignment west of Clinton Street, plus the home across State Boulevard on the east side of Terrace Street. These homes are 22 feet and 16 feet, respectively, from existing State Boulevard. The home on the south side of State Boulevard was a measurement site because early engineering did not call for its acquisition. More detailed design found it was necessary to acquire this home for the project. The house on the north side will remain and will be approximately 50 feet from the future roadway edge. It will experience noise levels exceeding the NAC. However, there is no feasible or reasonable mitigation that could protect this home.

Based upon preliminary design costs and design criteria, no locations have been identified where noise abatement is likely. Noise abatement has not been found to be feasible because effective noise barriers require long, uninterrupted segments of barrier to be feasible. As such, because of the existing cross streets, access points, alleyways and driveways located throughout the project area, it is not feasible to construct effective noise barriers for the roadway. Noise walls would not be reasonable because the cost of providing a wall for an individual home would exceed INDOT cost-effectiveness guidelines. Therefore, there is no feasible or reasonable noise mitigation proposed.

A reevaluation of the noise analysis will occur during final design. If during final design it has been determined that conditions have changed such that noise abatement is feasible and reasonable, the abatement measurements might be provided. The final decision on the installation of any abatement measure(s) will be made upon the completion of the project's final design and the public involvement processes.

For reference, the complete Noise Study Report is provided in Appendix I, pages I-2 to I-53. A copy of the approval of the technical sufficiency of the Noise Analysis (from INDOT Environmental Services) was received on October 18, 2011, and is included in Appendix I, page I-54.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
 Will the proposed action result in substantial impacts to community cohesion?
 Will the proposed action result in substantial impacts to local tax base or property values?
 Will construction activities impact community events (festivals, fairs, etc.)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The proposed project will improve public safety, improve roadway capacity at intersections, improve traffic flow along the project corridor, and improve the infrastructure along State Boulevard. No substantial adverse community impacts are anticipated to result from this project. The project will require a total of 15 residential relocations. The project will not affect community cohesion because it will not substantially change access or travel patterns within the community.

Currently, the State Boulevard project corridor does not provide an adequate and safe link between the two Greenway Trail Systems located in the project area. The proposed project will provide this link between the Pufferbelly Trail and the St. Joseph Pathway. New sidewalks, varying in width from five feet to ten feet, will be constructed on both sides of the roadway.

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The preferred alternative is anticipated to improve neighborhood/community cohesion, as one of the identified needs addressed by the proposed project is pedestrian safety. The proposed project will address the limited north/south pedestrian connectivity caused by traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street. As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail.

The project is not anticipated to affect any public facilities during construction. Traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. One travel lane is expected to remain open at all times and access shall be maintained to all residences and businesses during construction.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes

☐

No

☒

Remarks:

This project will improve public safety, traffic flow, and infrastructure along State Boulevard. The project will improve existing conditions and will not result in any substantial indirect or cumulative impacts. The project will reconstruct an existing road in an already fully developed area.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public utilities, fire, police, emergency services, religious institutions, public transportation or pedestrian and bicycle facilities? Discuss the maintenance of traffic, and how that will affect public facilities and services.

Yes

☐

No

☒

Remarks:

Based on the Maintenance-of-Traffic Plan, traffic is expected to be maintained along the existing roadway during construction, through the use of phased construction. Access to residential, commercial, and public properties will be maintained throughout construction.

Early coordination describing the project was sent to public agencies, including the highway department, sheriff's department, fire department, public schools, and other local public agencies. No other responses were received from local agencies. See Appendix B, page B-7 for reference to the early coordination list.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes

☒

No

☐

Are any EJ populations located within the project area?

X

☒☐

Will the project result in adversely high or disproportionate impacts to the EJ population?

☐

X

☒

Remarks:

An EJ concern is considered any impact that would have a disproportionately high and adverse effect on an environmental justice population. For EJ analysis, the reference community is typically a county, city, or town that contains the project and is called the community of comparison (COC). The community that overlaps the project limits is called the affected community (AC). Affected communities, which are more than 50 percent minority or low-income are automatically EJ populations. For all other affected communities, an EJ population exists if the low-income population or minority population is 25 percent higher than the population in the COC. A low-income population is a population with a median income that is below the federal poverty guidelines. A minority population consists of individuals who belong to one or more minority groups.

The project area is comprised of two Census Tracts, as determined by a review of the 2010 US Census data. These Census Tracts are considered to be the ACs. For this analysis, Allen County was analyzed as the COC. Within Allen County, 16.3 percent of the population was considered low-income and 28.8 percent were considered minority populations. An EJ population would exist if the population exceeds 20.4 percent low income or 36.0 percent minority respectively.

Within the project limits, Census Tract 00500 includes the eastern portion of the proposed project. According to the 2010 US Census, 33.3 percent of this population is low income and 34.8 percent is minority. Census Tract 00701 includes the western portion of the proposed project. According to the 2010 US Census, 27.9 percent of this population is low income and 25.4 percent is minority. As such, a potential environmental justice low income population exists within the Affected Community as compared to Allen County. For reference see the table below and Appendix H pages H-2 to H-10.

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Analysis of Potential EJ Populations	COC	AC	
	Allen County, Indiana	Census Tract 00500	Census Tract 00701
LOW-INCOME			
Total Population for whom poverty status is determined (estimated)	248,772	2,766	3,342
Total Population Below Poverty Level (estimated)	40,534	922	931
Percent Low-income	16.3%	33.3%	27.9%
125 Percent of COC	20.4%	AC>125% COC	AC>125% COC
Potential Low-income EJ Impact?		Yes	Yes
MINORITY			
Total population (all races)	254,228	2,939	3,343
White alone or in combination	181,101	1,915	2,493
Number Non-white/Minority	73,127	1,024	850
Percent Non-white/Minority	28.8%	34.8%	25.4%
125 Percent of COC	36.0%	AC>125% COC	AC>125% COC
Potential Minority EJ Impact?		No	No

The 15 residential properties are anticipated to be acquired as part of the proposed project. Avoidance of these acquisitions is not possible due to the proximity of the existing structures to the roadway and due to re-alignment of the proposed roadway. Impacts have been minimized to the greatest extent possible. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Relocation resources are available to all residential and business relocates without discrimination. No person displaced by this project will be required to move from a displaced dwelling unless comparable replacement housing is available to that person.

The project is intended to improve safety along State Boulevard by widening and realigning the roadway. The widening and realignment is required to help correct sight distance issues and substandard intersections, as well as provide turn lanes as appropriate. The project will also increase pedestrian safety by the addition of sidewalks varying in width from five feet to ten feet along both sides of the roadway.

The proposed project is expected to benefit the immediate project area including those Census Tracts with environmental justice concerns, through addition of pedestrian facilities, correction of drainage issues associated with the roadway, and improvement of the existing roadway. The existing bridge is currently below the flood elevation of the St. Mary's River, which causes the bridge to be overtopped with backwater from the Saint Mary's River with frequently, therefore affecting roadway safety by flooding State Boulevard and requiring the closure of the roadway. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often. The proposed project will address this issue by raising the vertical alignment of the roadway approximately seven feet at the proposed bridge over Spy Run Creek. This will significantly reduce the amount of road closures due to flooding events and allow emergency vehicles and local residents access during times when they may not have in the past. Noted negative effects include up to 15 residential relocations and the impact those will have on the existing neighborhood.

Significant efforts were made to engage and involve the public in the project planning process. Early coordination was initiated with representatives of the community. On multiple occasions the City of Fort Wayne met with neighborhood associations, business owners, adjacent property owners, and interested groups. The City met with these individuals to help explain the project, provide project updates, and address comments and concerns. Meeting with these groups, individuals, and representatives further helped the City ensure the public was involved in the planning process. In addition five public information meetings and three open-house style public information meetings were conducted to further attempt to engage the public. Significant efforts were made to encourage participation in the meetings, including public notices and press releases published in the *Fort Wayne Journal Gazette*. For additional information see the public involvement documents associated with this project see Appendix F pages F-12 to F-24.

The positive effects of the project outweigh the noted negative effects; the project would be a benefit to those in the area. The State Boulevard Reconstruction Project would not cause a disproportionate impact on the known EJ community. Significant efforts were made to encourage full and fair participation by all potentially affected communities in the project planning process, and suggestions and comments received from community participants are being considered in the final project design. As a result of this analysis and public involvement process, the requirements of Executive Order 12898 and the policy principles of the US DOT have been addressed, and no further evaluation is warranted.

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Relocation of People, Businesses or Farms:

Will the proposed action result in the relocation people, businesses or farms?

Is a Business Information Survey (BIS) required?

Is a Conceptual Stage Relocation Study (CSRS) required?

Yes	No
X	
	X
	X

Number of relocations: Residences: 15 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the Remarks section.

Remarks:

Relocations have been minimized to the extent practical. Existing structures to be relocated are generally within zero to thirty feet of the proposed edge of pavement. Significant property acquisition cannot be avoided due to the roadway alignment and profile. For reference to the parcels anticipated to be relocated see plans included in Appendix A pages A-11 to A-129.

There are no other relocations anticipated from this project; however, during property acquisition, it is possible additional structures may be acquired. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. Relocation resources are available to all residential and business relocatees without discrimination. No person displaced by this project will be required to move from a displaced dwelling unless comparable replacement housing is available to that person.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

	Documentation	
	Yes	No
Red Flag Investigation	X	
Hazardous Materials Site Assessment Form	X	
Phase I Initial Site Assessment (ISA)	X	
Phase II Preliminary Site Investigation (PSI)		X
Design/Specifications for Remediation required?		X

	No	Yes/ Date
ES Review of Investigations	X	

Include a summary of findings for each investigation.

Remarks:

A Red Flag Investigation (RFI) was initiated by American Structurepoint, Inc., in 2007. The investigation included a search of nationwide and local database resources provided by IndianaMap and FirstSearch. A total of 46 hazardous material concern records were identified within a 0.5-mile of the project radius. Results of the 2007 preliminary investigation recommended a Phase I Initial Site Assessment (ISA). Prior to completion of the RFI on April 26, 2013 a search of nationwide and local databases was again performed to review updated information. No additional hazardous material concern records were identified in the 2013 search.

A Hazardous Material Site Visit Form was also completed for the project area. The Hazardous Materials Site Visit Form did not identify any additional hazardous materials concerns.

An ISA was prepared by American Structurepoint, Inc on November 11, 2011. A total of five sites were assessed, with no sites identified as having a Recognized Environmental Condition (REC). Because no RECs were identified, no additional investigations are necessary. The following are those properties addressed as part of the ISA.

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Site ID	Address	Site Name	REC	Additional Investigation Recommended
1	215 West State Boulevard	Ink Spot Printing	None	No
2	324 East State Boulevard	Kroger	None	No
3	310 West State Boulevard	Townsend and Pratt Auto Sales	None	No
4	2230 North Clinton Street	Lassus Brothers Oil Handy Dandy	None	No
5	2522 Cass Street	Superior Collision	None	No

A copy of the RFI, Hazardous Material Site Visit Form, and ISA Executive Summary are included in Appendix D, pages D-2 to D-14.

SECTION I – PERMITS CHECKLIST

	<u>Required</u>	<u>Not Required</u>
Army Corps of Engineers (404/Section 10 Permit)		
Individual Permit (IP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDEM		
Section 401 WQC	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDNR		
Construction in a Floodway	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mitigation Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
US Coast Guard Section 9 Bridge Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Others (Please discuss in the Remarks section below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The project will require a Section 404 from the USACE and a Section 401 Water Quality Certification from IDEM for impacts to regulated wetlands or waterways. The project will require a Construction in a Floodway permit from IDNR for the crossings of Spy Run Creek. The project will require a Rule 5 Erosion Control Permit from IDEM if at least one acre of land is disturbed.

The local project sponsor is responsible for obtaining all required permits.

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SECTION J- ENVIRONMENTAL COMMITMENTS

Information below must be included on Commitments Summary Form. List all commitments, indicating which are firm and which are optional.

Remarks:

Firm Commitments

USFWS

1. Post DO NOT DISTURB signs at the construction zone boundaries and do not clear trees or understory vegetation outside the boundaries.
2. Restrict below-water work to placement of piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.
3. Restrict channel work and vegetation clearing to within the width of the normal approach road right-of-way.
4. Minimize the extent of artificial bank stabilization.
5. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
6. Implement temporary erosion and siltation control devices such as placement of straw bales in drainage ways and ditches, covering exposed areas with burlap, jute matting or straw, and grading slopes to retain runoff in basins.
7. Revegetate all disturbed soil areas immediately upon project completion.
8. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season (as applicable).

IDNR

9. Minimize and contain within the project limits in channel disturbance and the clearing of trees and brush.
10. Do not work in the waterway from April 1 through June 30 without prior written approval of the Division of Fish and Wildlife
11. Post "Do Not Mow or Spray" signs along the right-of-way.
12. Seed and protect all disturbed streambanks and slopes that are 3: 1 or steeper with erosion control blankets (follow manufacturer's recommendations for installation); seed and apply mulch on all other disturbed areas.
13. Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion; low endophyte tall fescue may be used in "high maintenance" areas only.[Alternate wording – check your letter - Revegetate "low maintenance" areas with a mixture of grasses (excluding all varieties of tall fescue), legumes as soon as possible upon completion; low endophyte tall fescue may be used in ditch bottom and side slopes only.]
14. Do not cut any trees suitable for Indiana bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark) from April 1 through September 30.
15. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
16. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
17. Do not work in salmonid waterways from March 15 through June 15 and from July 15 through November 30 without the prior written approval of the Department of Natural Resources, Division of Fish and Wildlife.
18. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
19. The project must not create conditions that are less favorable for wildlife passage under the structure compared to current conditions. This includes maintaining land under the bridge unarmored with riprap to allow for wildlife passage.
20. If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317)232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.
21. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
22. Place all excavated material landward of the floodway.
23. Do not leave felled trees, brush, or other debris in the floodway. Remove all construction debris from the floodway.
24. Keep the bridge waterway opening free of debris and sediment at all times.
25. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree, which is removed that is ten inches or greater in diameter-at-breast height within the regulatory floodway or as required by permit conditions.

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IDEM

26. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project.
27. IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff.
28. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. Dirt tracked onto paved roads from unpaved areas should be minimized.
29. All facilities slated for renovation or demolition must be inspected by an Indiana-licensed asbestos inspector prior to renovation or demolition activities. If regulated asbestos- containing material (RACM) that may become airborne is found, demolition, renovation, or asbestos removal activities must be performed in accordance with notification and emission control requirements.
30. In all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition.
31. IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust.
32. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7 percent) oil distillate, is prohibited during the months of April through October.
33. Stabilize all disturbed areas upon completion of land disturbing activities.
34. Sediment-laden water, which otherwise would flow from the project site shall be treated by erosion and sediment control measures appropriate to minimize sedimentation.
35. Wastes and unused building materials shall be managed and disposed of in accordance with all applicable statutes and regulations.
36. A stable construction site access shall be provided at all points of construction traffic ingress and egress to the project site.
37. Public or private roadways shall be kept cleared of accumulated sediment that is a result of run-off or tracking.

MOA (commitments are considered firm pending the MOA approval)

38. The City of Fort Wayne shall consider and, where feasible, shall implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.
39. The City of Fort Wayne shall consider and, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences.
40. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.
41. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:
 - a. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
 - b. Context sensitive solutions, such as protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; maintaining pedestrian connections along the former Eastbrook and Westbrook drives; the rights-of-way, shall be included among the measures considered.
 - c. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
 - d. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic

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- preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.
- e. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail Des. No. 0710990 or US 27 Nos. 0101527 and 0200914) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.
 - f. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
 - g. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before schedule meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.
 - h. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.
 - i. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.
42. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."
43. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
- a. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - b. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
44. Photo documentation will include black and white prints of digital photographs and a digital video disc ("DVD") containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" that are in effect at the time.
- a. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 - b. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
 - c. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic

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District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and

- d. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.
45. The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler's landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps or other WPA era programs in public projects.
46. The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within thirty (30) days, acceptance will be assumed. If the Indiana SHPO responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

For Consideration

USFWS

1. Shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore we request that trees lost to the project be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail.

INDOT-Fort Wayne District

2. This project will be taking place within the NRHP Eligible Brookview/Irvington Park Historic District. This neighborhood is eligible due to the layout of the streets following Spy Run Creek. Taking of right-of-way in the area will most likely constitute a Section 4(f) impact. Due to these situations, minimization of impacts in this area should be considered by multiple alternatives to show the proposed plan is the most feasible and prudent. Context sensitive design to fit the historic setting of the neighborhood should also be investigated.

Indiana Department of Transportation

County Allen Route State Boulevard Des. No. 0400587 Project No. _____

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Remarks:

Agency	Date Mailed	Response Received	Appendix Location
US Fish and Wildlife Service	March 16, 2009 March 19, 2014	April 20, 2009 March 19, 2014	B-15 to B-16 B-25 to B-26
US Natural Resources Conservation Service	March 16, 2009	March 20, 2009	B-8
US Army Corps of Engineers	March 16, 2009	May 11, 2009	B-17 to B-18
Indiana Geological Survey	March 16, 2009	April 06, 2009	B-13
Aeronautics Section – INDOT	March 16, 2009	No Response	
Indiana Department of Environmental Management	April 24, 2013 (electronic submittal)	April 24, 2013	B-21 to B-24
IDNR, Division of Fish and Wildlife	March 16, 2009	November 18, 2009	B-19
Ninth Coast Guard Unit	March 16, 2009	March 30, 2009	B-9
Fort Wayne District – INDOT	March 16, 2009	March 30, 2009	B-10
Allen County Sheriff's Department	March 16, 2009	No Response	
City of Fort Wayne	March 16, 2009	April 3, 2009	B-11 to B-12
Allen County Surveyor	March 16, 2009	No Response	
Allen County Highway Department	March 16, 2009	No Response	
City of Fort Wayne Office of Mayor	March 16, 2009	No Response	
Fort Wayne Community School Board	March 16, 2009	No Response	
Allen County Executive Board of Health	March 16, 2009	No Response	
Northside High School	March 16, 2009	No Response	
Allen County Parks and Recreation	March 16, 2009	No Response	
Imagine Master Academy	March 16, 2009	No Response	
Forest Park Elementary School	March 16, 2009	No Response	
Department of Planning Services	March 16, 2009	No Response	
IDNR – Division of Outdoor Recreation	March 16, 2009	April 07, 2009	B-14
Allen County Engineer	March 16, 2009	No Response	
Allen County Board of Commissioners	March 16, 2009	No Response	

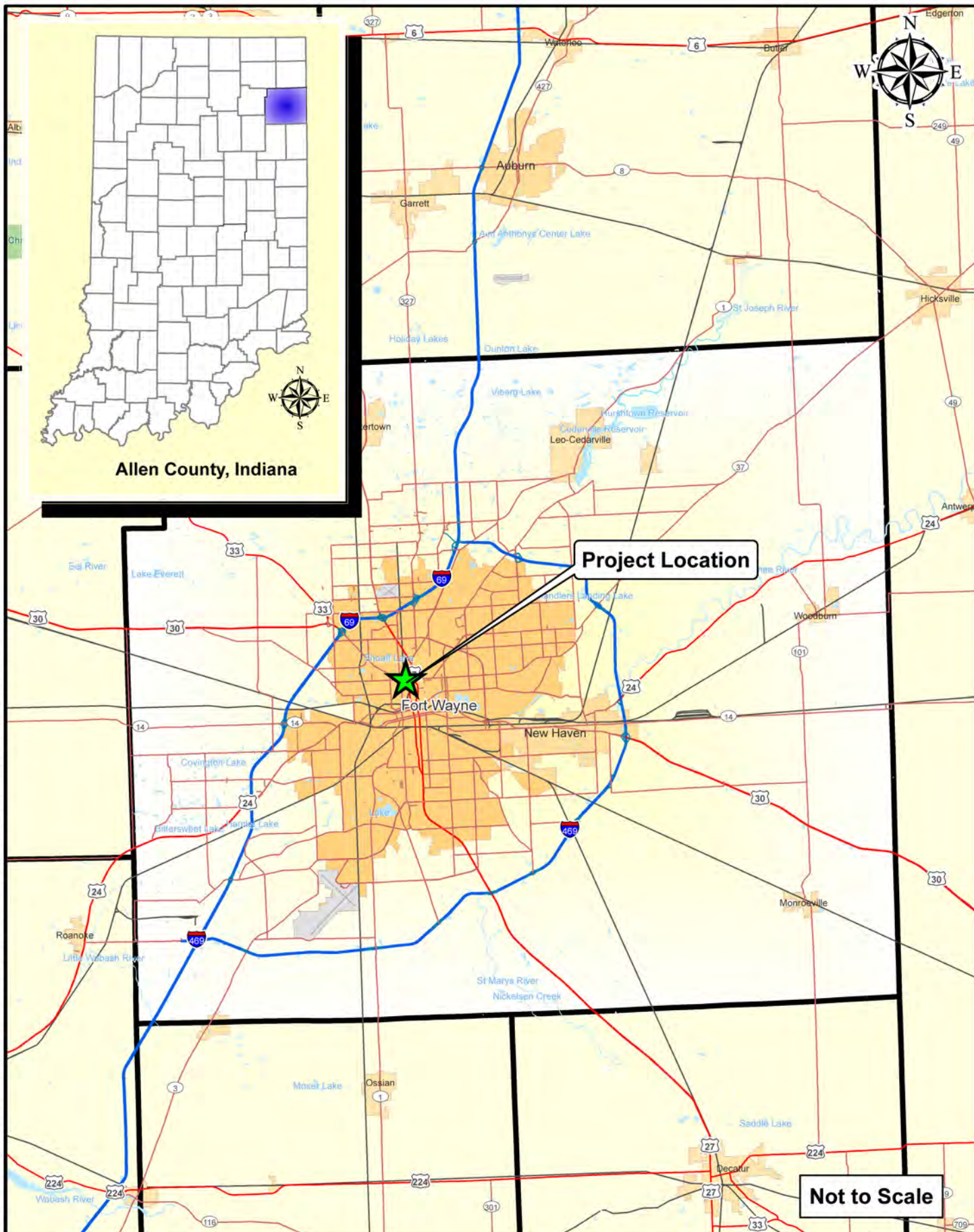
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• Project Plans – Pufferbelly Trail over State Boulevard (Des. No. 1005155)	A-21 to A-29
• Project Plans – Phase I (Terrace Road to Spy Run Avenue, Des. No. 1005151)	A-30 to A-60
• Project Plans – Phase II (Cass Street to Terrace Road, Des. No. 1005154)	A-61 to A-129
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• Early Coordination Mailing List	B-7
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• City of Fort Wayne – April 03, 2009	B-11 to B-12
• Indiana Geological Survey – April 06, 2009	B-13
• Indiana Department of Natural Resources , Division of Outdoor Recreation – April 07, 2009	B-14
• US Fish and Wildlife Service – April 20, 2009	B-15 to B-16
• US Army Corps of Engineers – May 11, 2009	B-17 to B-18
• Indiana Department of Natural Resources , Division of Water – March 18, 2009	B-19
• Indiana Department of Environmental Management Environmental Review Letter – April 24, 2013	B-21 to B-24
• US Fish and Wildlife Service – March 19, 2014	B-25 to B-26
• Land and Water Conservation Fund listing for Allen County	B-27 to B-28
Appendix C: Section 106 of the NHPA	
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• Executive Summary Initial Site Assessment	D-17

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• INDOT Hydraulics Approval (May 13, 2010)	E-18 to E-19
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Appendix A: Graphics

- Project Location – State Map
- Project Location – USGS Topographic Map
- Project Location – 2012 Aerial Photography
- Project Photographs
- Project Plans – State Boulevard over Spy Run Creek (Des. No. 1005152)
- Project Plans – Pufferbelly Trail over State Boulevard (Des. No. 1005155)
- Project Plans – Phase I (Terrace Road to Spy Run Avenue, Des. No. 1005151)
- Project Plans – Phase II (Cass Street to Terrace Road, Des. No. 1005154)



Project Location Map

City of Fort Wayne, Board of Public Works
Citizens Square Building
200 east Berry Street, Suite 240
Fort Wayne, Indiana 46802

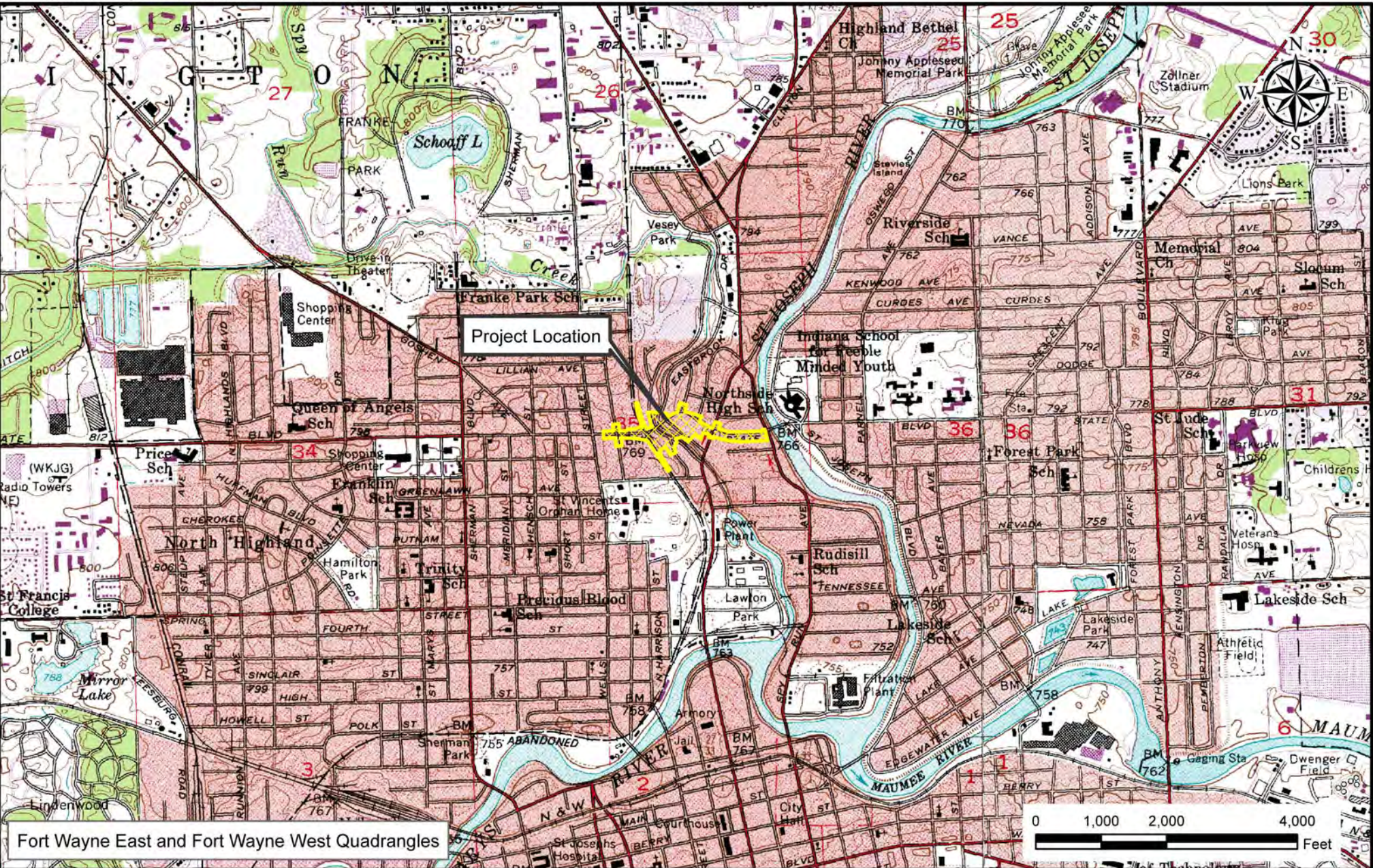
State Boulevard Reconstruction

Des. No. 0400587

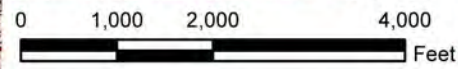
Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 04/15/2013

Appendix A
Page 2 of 129



Fort Wayne East and Fort Wayne West Quadrangles



Project Location - USGS Topographic Map

Applicant: City of Fort Wayne, Board of Public Works
Citizens Square
200 east Berry Street, Suite 240
Fort Wayne, Indiana 46802

State Boulevard Reconstruction
Des. No. 0400587

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 04/24/2013

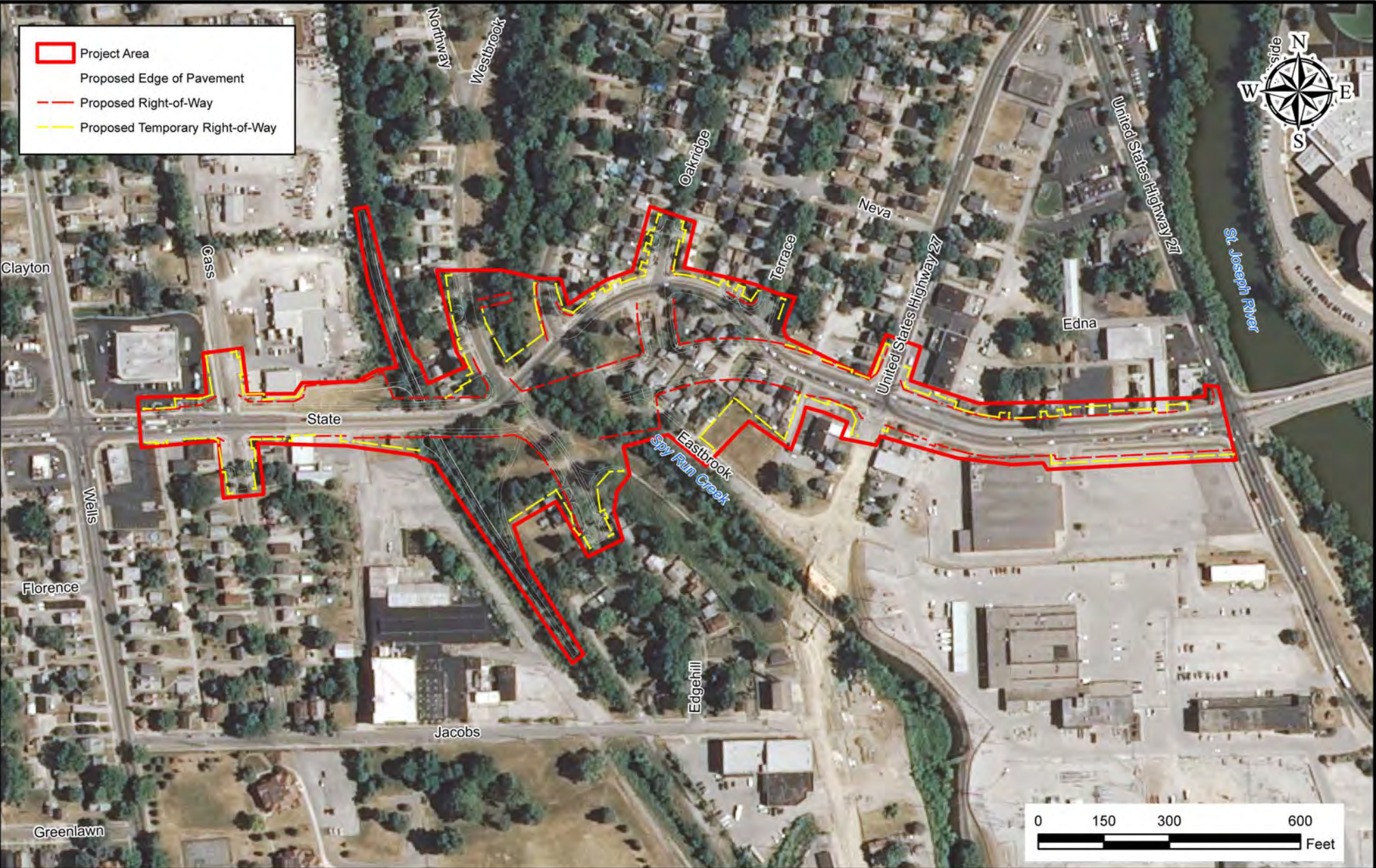
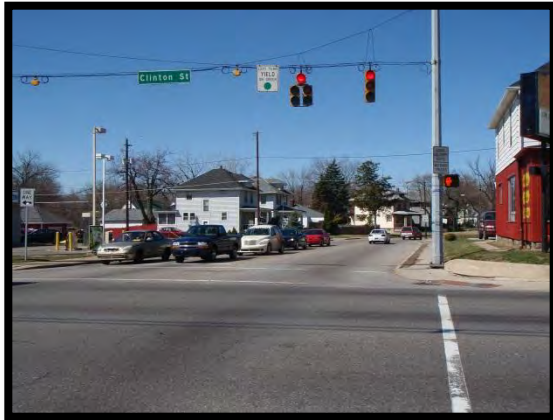


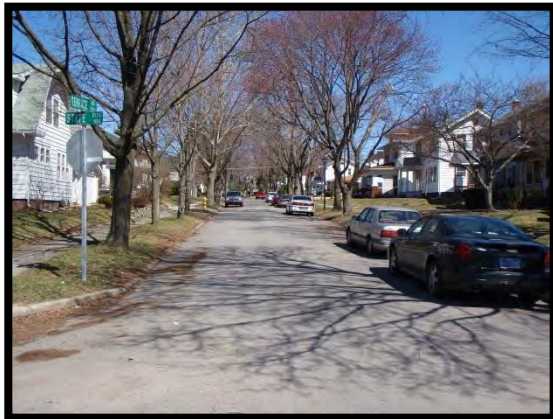
Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



1. Looking east along State Boulevard from Clinton Street.



2. Looking south along Clinton Street from State Boulevard.

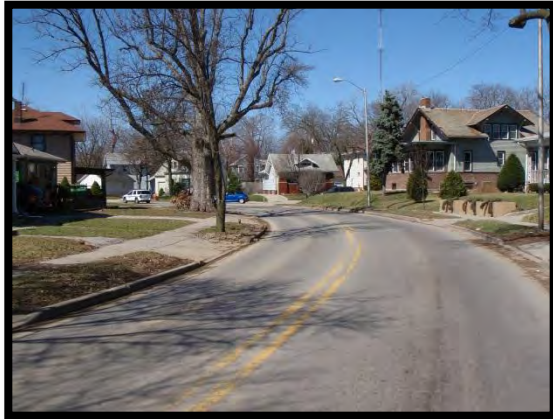


3. Looking north along Terrace Road from State Boulevard.



4. Looking east along State Boulevard from Terrace Road.

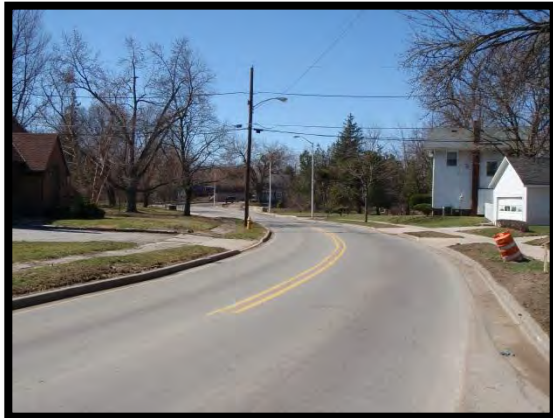
Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



5. Looking west along State Boulevard from Terrace Road.



6. Looking north along Oakridge Road from State Boulevard.



7. Looking west along State Boulevard from Oakridge Road.



8. Looking east along State Boulevard from Oakridge Road.

Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



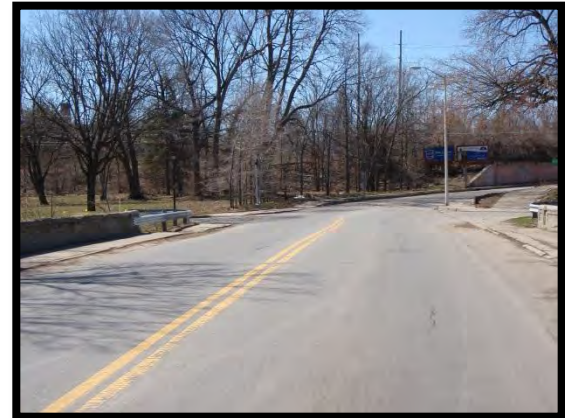
9. Looking north along Eastbrook Drive from State Boulevard.



10. Looking east along State Boulevard from Eastbrook Drive.



11. Looking south along Eastbrook Drive from State Boulevard.



12. Looking west along State Boulevard from Eastbrook Drive.

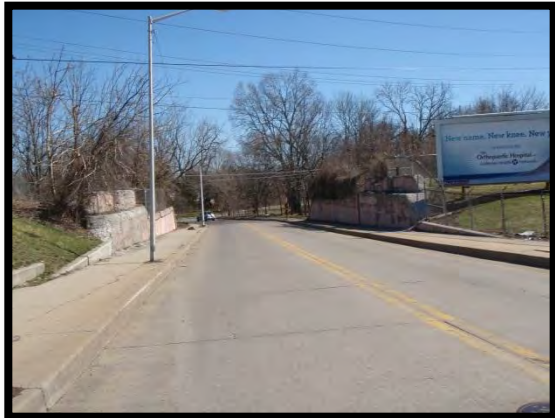
Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



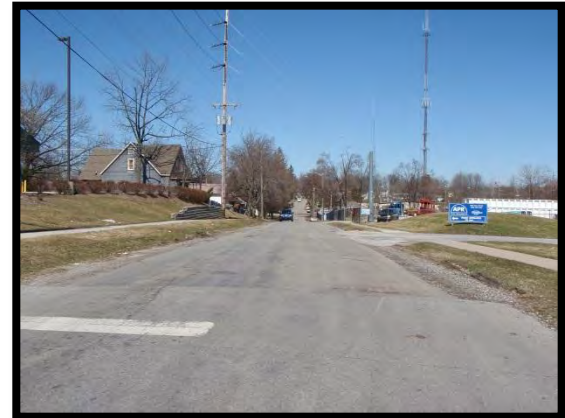
13. Looking south along Spy Run Creek.



14. Looking north along Spy Run Creek
toward State Boulevard Bridge.

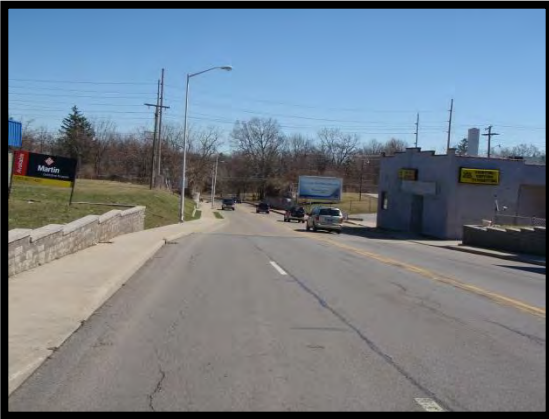


15. Looking east along State Boulevard
toward old railroad crossing.

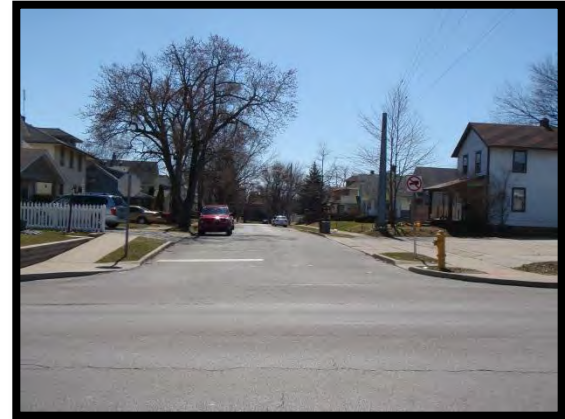


16. Looking north along Cass Street from
State Boulevard.

Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



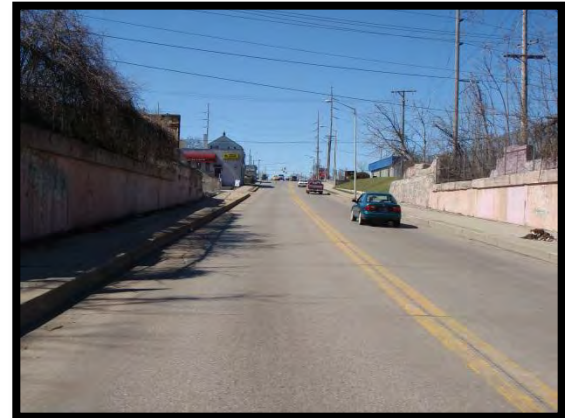
17. Looking east along State Boulevard from Cass Street.



18. Looking south along Cass Street from State Boulevard.



19. Looking west along State Boulevard from Cass Street.



20. Looking west along State Boulevard from old railroad crossing.

Photo Log (March 19, 2009)
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587



21. Looking north along old Westbrook drive from State Boulevard.



22. Looking north along Spy Run Creek toward State Boulevard Bridge.



23. Looking north along Eastbrook Drive toward State Boulevard.



24. Looking south along Eastbrook Drive from State Boulevard.

Electric

*American Electric Power
Distribution
PO Box 60
Ft. Wayne, IN 46801
Contact: Tom Kanning
(260) 421-1766*

*American Electric Power
Transmission
700 Morrison Rd.
Gahanna, OH 43230
Contact: Tina Hairston
(614) 552-1801*

Television

Comcast Cablevision
720 Taylor St.
Ft. Wayne, IN 46802
Contact: John Gayday
(260) 456-9000

Gas

NIPSCO
801 E. 86th. Ave.
Merrillville, IN 46410
Contact: Jill Boganwright
(419) 647-6502

Sewers

City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

Telephone

Frontier Communications
8001 W. Jefferson Blvd, INIFAON
Fort Wayne, IN 46804
Contact: Jean-Paul Kalonji
(260) 461-3650

Water

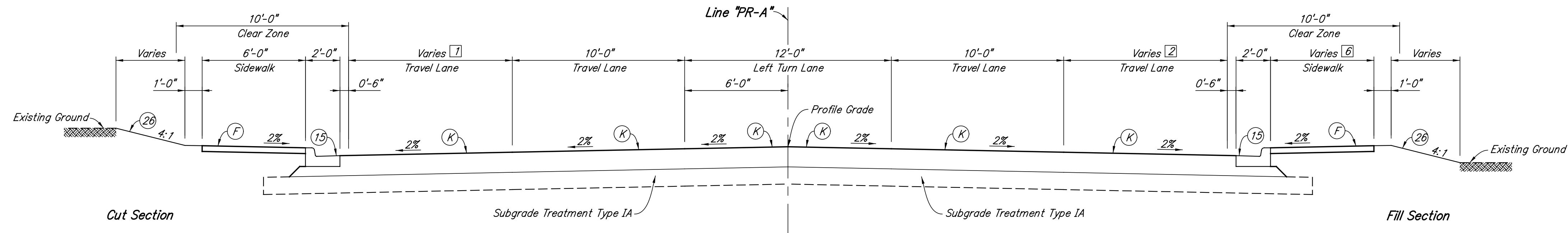
City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

**	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction.
	Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
**	All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.

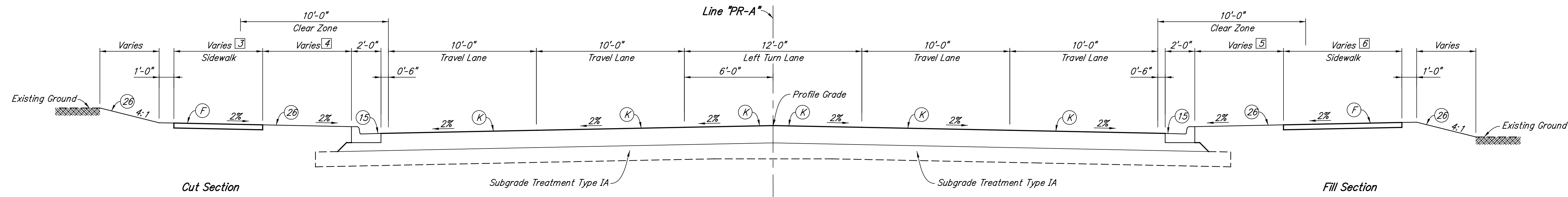
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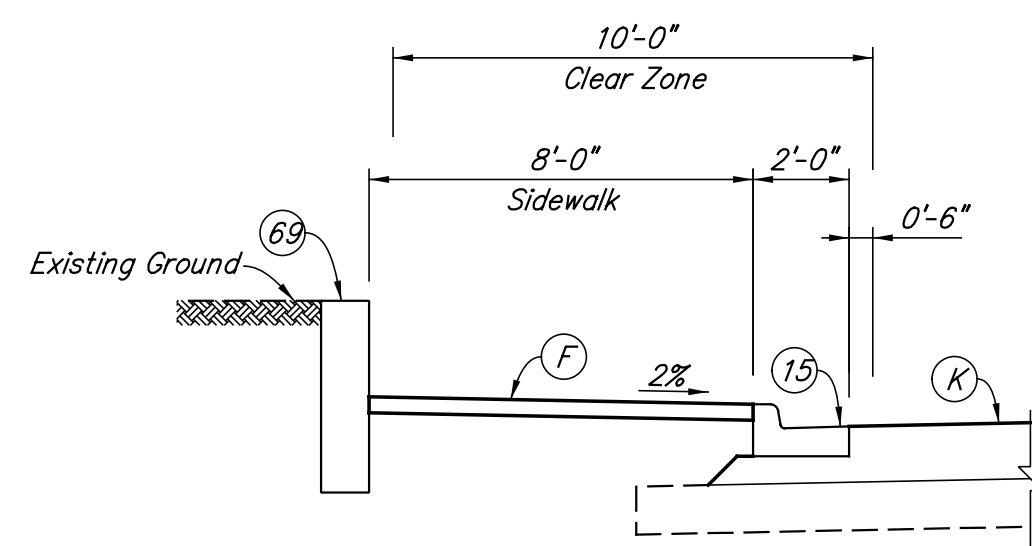
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		DESIGN ENGINEER _____				VERTICAL SCALE N/A		DESIGNATION NO. 1005152	
		DATE _____							
		DESIGNED: _____ BMP		DRAWN: _____ BMP		SURVEY BOOK		SHEETS	
		CHECKED: _____ JPD		CHECKED: _____ JPD		CONTRACT R-33130		PROJECT NO. 1005152	
						INDEX & GENERAL NOTES			



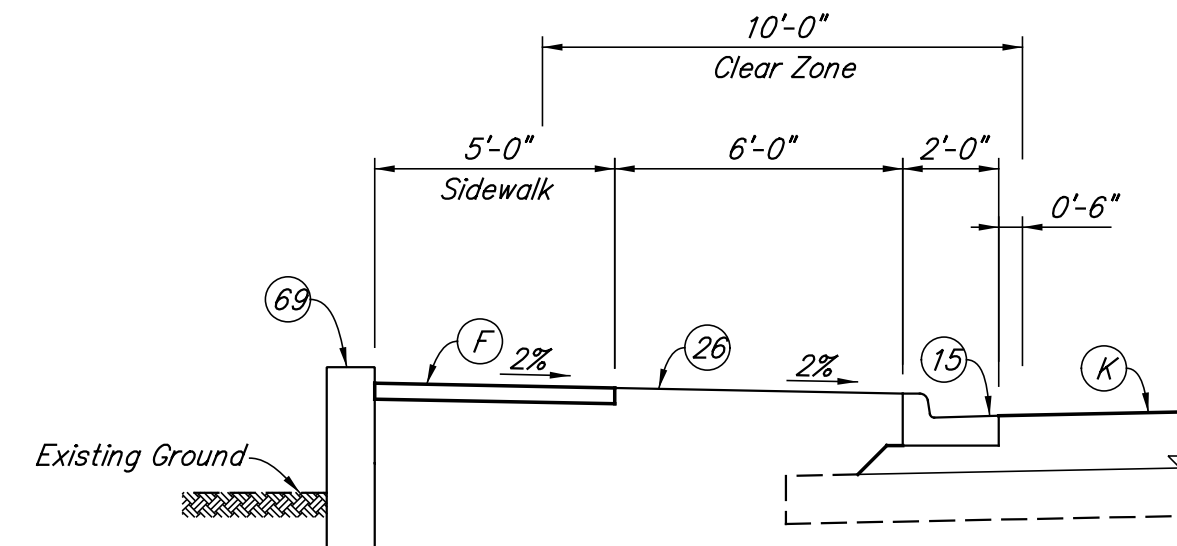
Typical Section - Incidental Construction
103+25.00 to 104+75.00 "PR-A"



Typical Section - State Boulevard
104+75.00 to 106+50.00 "PR-A"
110+40+00 to 112+07.58 "PR-A"

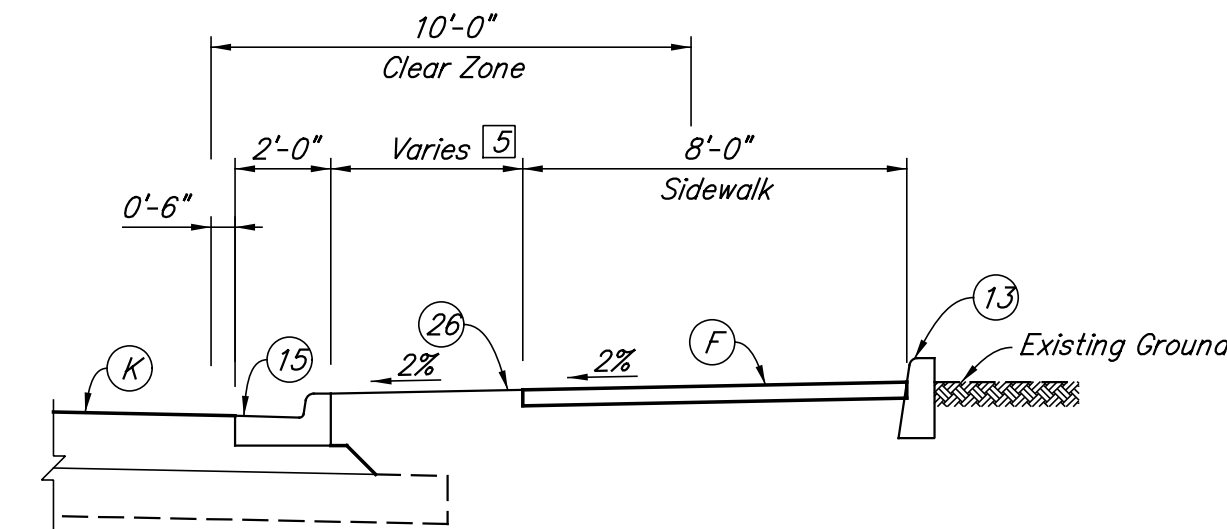


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Cut Section
109+85.00 to 110+39.68 Lt. "PR-A"

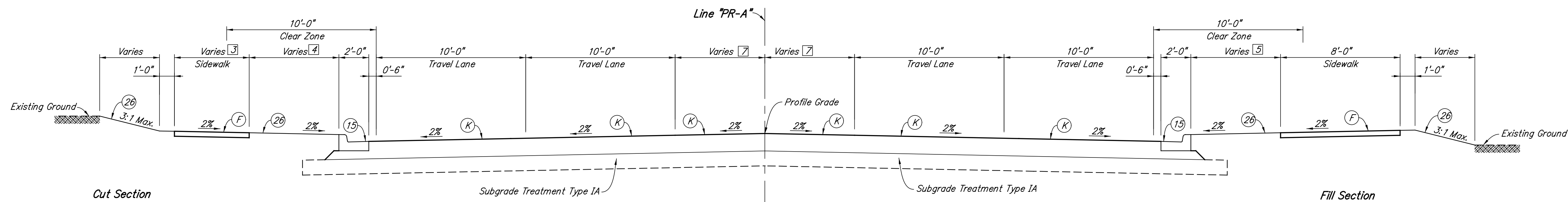
Paving Exception
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- 2 Equals 9'-6" from 103+25.00 to 104+50.00 "PR-A"
Varies 9'-6" to 10'-0" from 104+50.00 to 104+75.00 "PR-A"

- 3 Equals 6'-0" from 104+75.00 to 105+27.04 "PR-A"
Equals 8'-0" from 105+27.04 to 106+50.00 "PR-A"
Varies 8'-0" to 9'-0" from 106+50.00 to 106+70.00 "PR-A"
Varies 7'-0" to 5'-0" from 106+70.00 to 107+30.00 "PR-A"
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- 4 Equals 0'-0" from 104+75.00 to 106+70.00 "PR-A"
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- 5 Equals 0'-0" from 104+75.00 to 106+90.00 "PR-A"
Varies 2'-0" to 6'-0" from 106+90.00 to 107+50.00 "PR-A"
Equals 6'-0" from 107+50.00 to 112+07.58 "PR-A"
- 6 Equals 6'-0" from 103+25.00 to 104+11.27 "PR-A"
Varies 6'-0" to 10'-5" from 104+11.27 to 104+85.03 "PR-A"
Equals 8'-0" from 104+85.03 to 112+07.58 "PR-A"
- 7 Varies 6'-0" to 1'-0" from 106+50.00 to 107+50.00 "PR-A"
Equals 1'-0" from 107+50.00 to 110+40.00 "PR-A"
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Typical Section - State Boulevard
106+50.00 to 110+40.00 "PR-A"

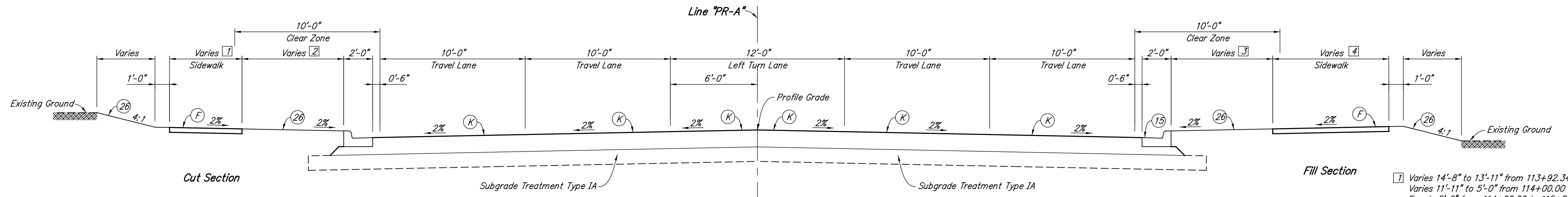
FOR INFORMATION ONLY

- LEGEND**
- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"
- (F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx
- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete
- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

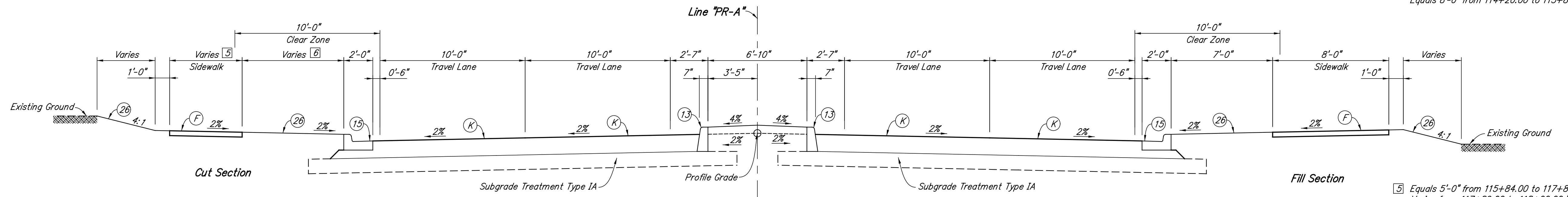
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "PR-A"	

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE ALLEN CO. BR. #545
VERTICAL SCALE	DESIGNATION NO. 1005152
SURVEY BOOK	SHEETS 3 of 15
CONTRACT R-33130	PROJECT NO. 1005152



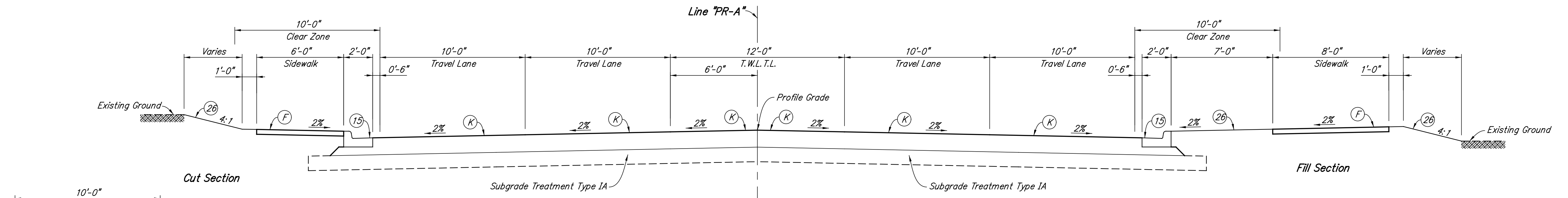
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- 2 Equals 0'-0" from 113+92.34 to 114+00.00 "PR-A"
Varies 2'-0" to 7'-0" from 114+00.00 to 114+20.00 "PR-A"
Equals 7'-0" from 114+20.00 to 115+84.00 "PR-A"
- 3 Equals 0'-0" from 113+92.34 to 114+00.00 "PR-A"
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Equals 7'-0" from 114+20.00 to 115+84.00 "PR-A"
- 4 Varies 12'-2" to 13'-0" from 113+92.34 to 114+00.00 "PR-A"
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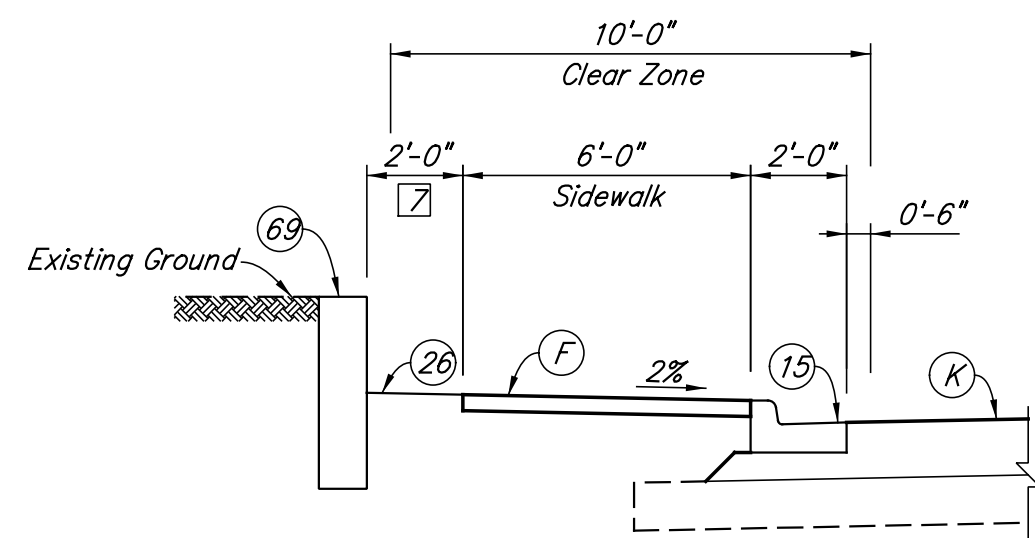


Typical Section - State Boulevard
115+84.00 to 118+06.00 "PR-A"

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Varies from 117+89.00 to 118+00.00 "PR-A"
Equals 6'-0" from 118+00.00 to 118+06.00 "PR-A"
- 6 Equals 7'-0" from 116+00.00 to 117+89.00 "PR-A"
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Equals 0'-0" from 118+00.00 to 118+06.00 "PR-A"

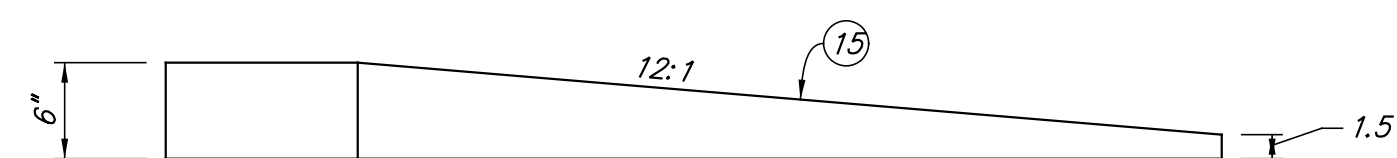


Typical Section - State Boulevard
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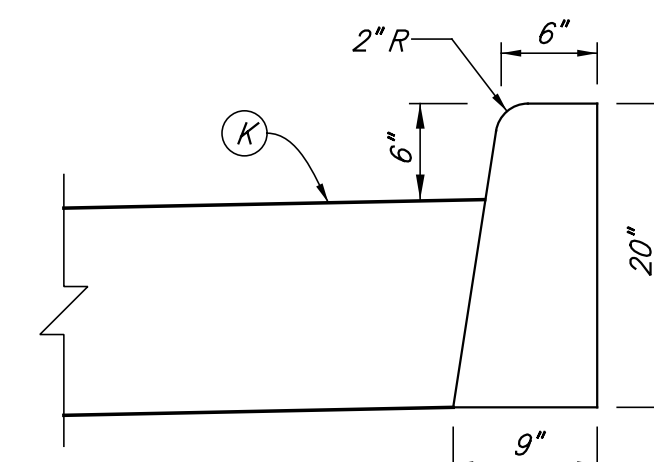


Cut Section
118+00.00 to 118+65.00 Lt. "PR-A"

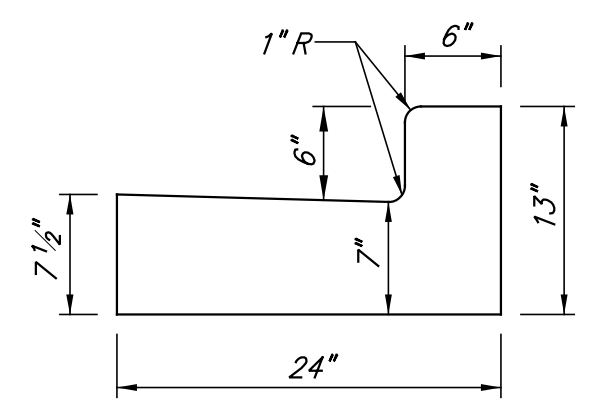
- 7 Equals 0'-0" from 118+00.00 to 118+02.11 "PR-A"
Varies 0'-0" to 2'-0" from 118+02.11 to 118+07.57 "PR-A"
Equals 2'-0" from 118+07.57 to 118+65.00 "PR-A"



Curb Transition Detail
Not to Scale



Concrete Curb Detail
Not to Scale



Curb & Gutter, Concrete, Modified Detail
Not to Scale

FOR INFORMATION ONLY

- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"

- LEGEND**
- (F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

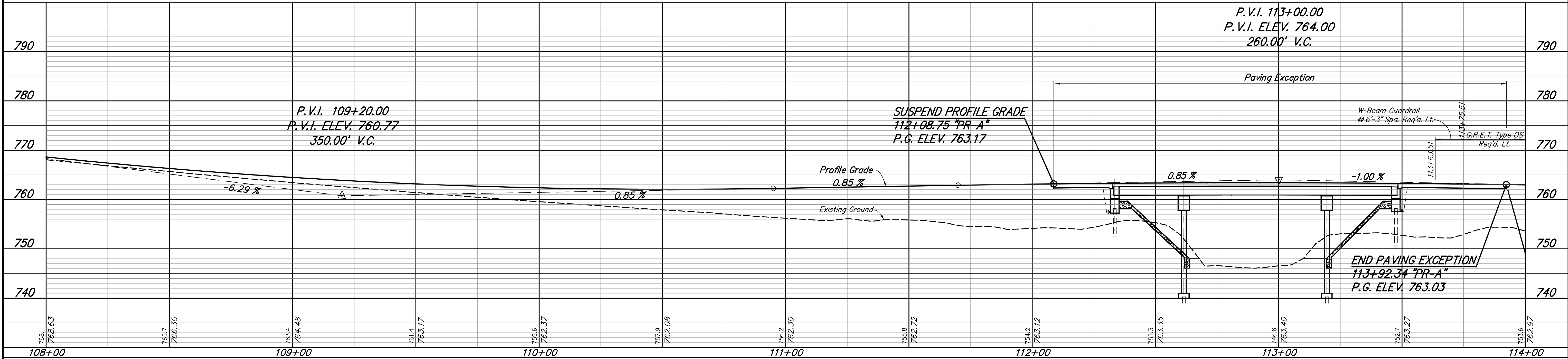
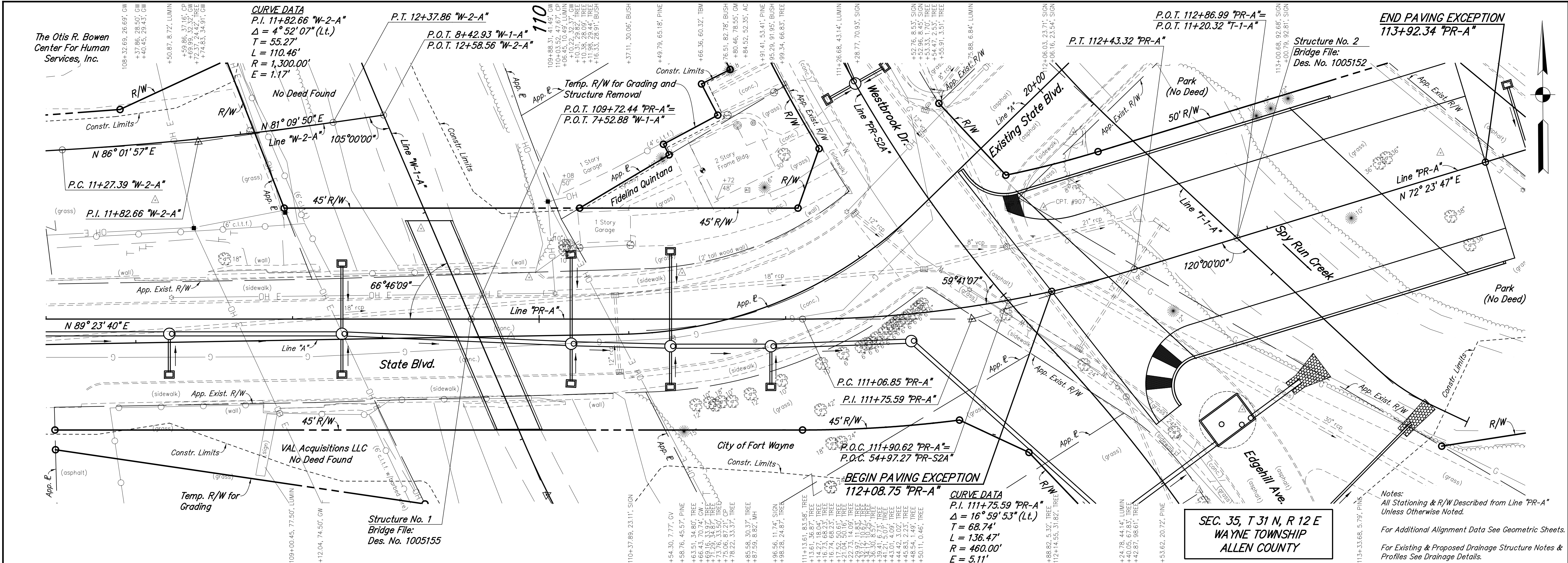
- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete

- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

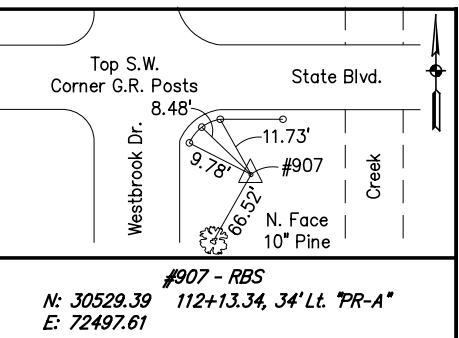
RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: CAK
CHECKED: SMC	CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "PR-A"	

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE ALLEN CO. BR. #545
VERTICAL SCALE	DESIGNATION NO. 1005152
SURVEY BOOK	SHEETS 4 of 15
CONTRACT R-33130	PROJECT NO. 1005152



FOR INFORMATION ONLY



RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED:	ASU	DRAWN: PJT
CHECKED:	SMC	CHECKED: SMC

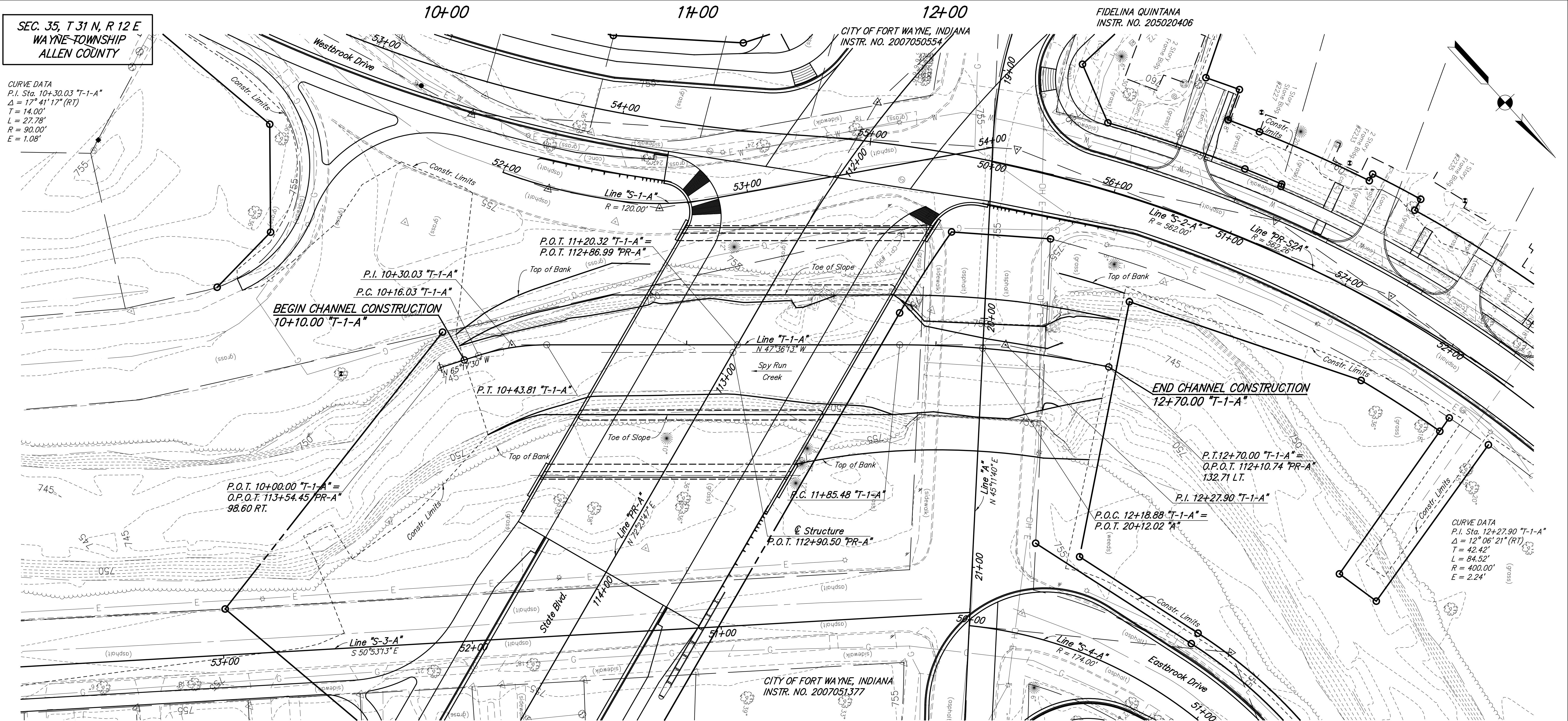
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-A"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		ALLEN CO. BR. #545	
VERTICAL SCALE		DESIGNATION NO.	
1" = 10'		1005152	
SURVEY BOOK		SHEETS	
CONTRACT		5 of 15	
R-33130		PROJECT NO.	
		1005152	

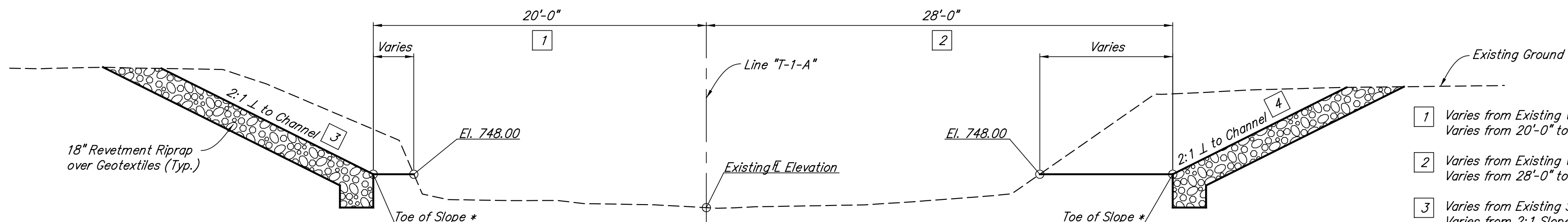
SEC. 35, T 31 N, R 12 E
WAYNE-TOWNSHIP
ALLEN COUNTY

CURVE DATA
P.I. Sta. 10+30.03 "T-1-A"
 $\Delta = 17^{\circ} 41' 17''$ (RT)
T = 14.00'
L = 27.78'
R = 90.00'
E = 1.08'



SITUATION PLAN

Scale: 1" = 20'-0", Contour Interval 1'-0"



TYPICAL CHANNEL SECTION
10+10.00 "T-1-A" to 12+70.00 "T-1-A"

Scale: 1/4" = 1'-0"

- Varies from Existing Width to 20'-0" 10+10.00 "T-1-A" to 10+70.00 "T-1-A"
Varies from 20'-0" to Existing Width 12+30.00 "T-1-A" to 12+70.00 "T-1-A"
- Varies from Existing Width to 28'-0" 10+10.00 "T-1-A" to 10+40.00 "T-1-A"
Varies from 28'-0" to Existing Width 12+30.00 "T-1-A" to 12+70.00 "T-1-A"
- Varies from Existing Slope to 2:1 Slope from 10+10.00 "T-1-A" to 10+70.00 "T-1-A"
Varies from 2:1 Slope to Existing Slope 12+30.00 "T-1-A" to 12+70.00 "T-1-A"
- Varies from Existing Slope to 2:1 Slope 10+10.00 "T-1-A" to 10+40.00 "T-1-A"
Varies from 2:1 Slope to Existing Slope 12+30.00 "T-1-A" to 12+70.00 "T-1-A"

NOTES:

For Additional Road Alignment Data, see Roadway Geometrics Sheets in the Road Plans.

Cross-Hatched Areas Indicate Limits of Class 18" Revetment Riprap over Geotextiles (For Quantities, See Sheet No. 6.)

* Toe of Slope to Continue Below El. 748.00 in Area of Existing Bridge Abutment to provide Riprap Keyway at Existing Ground.

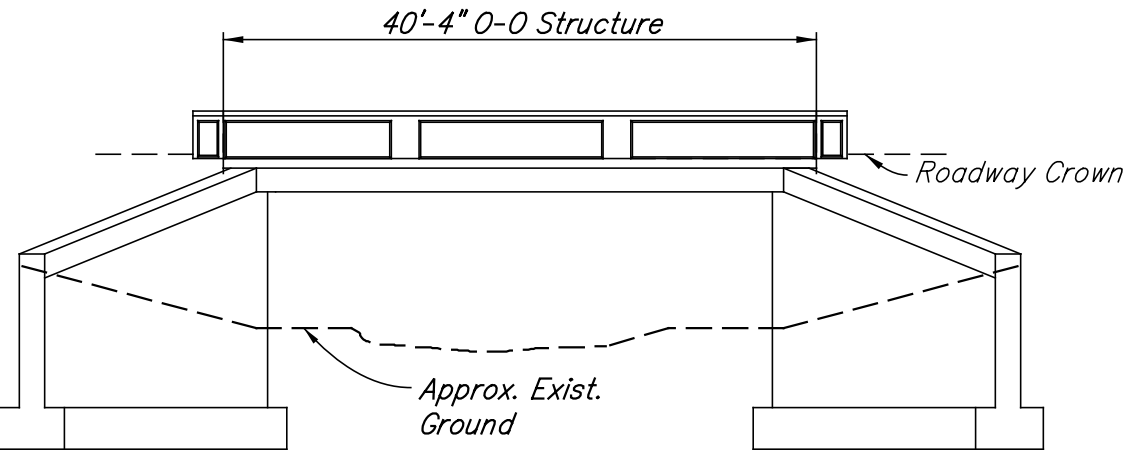
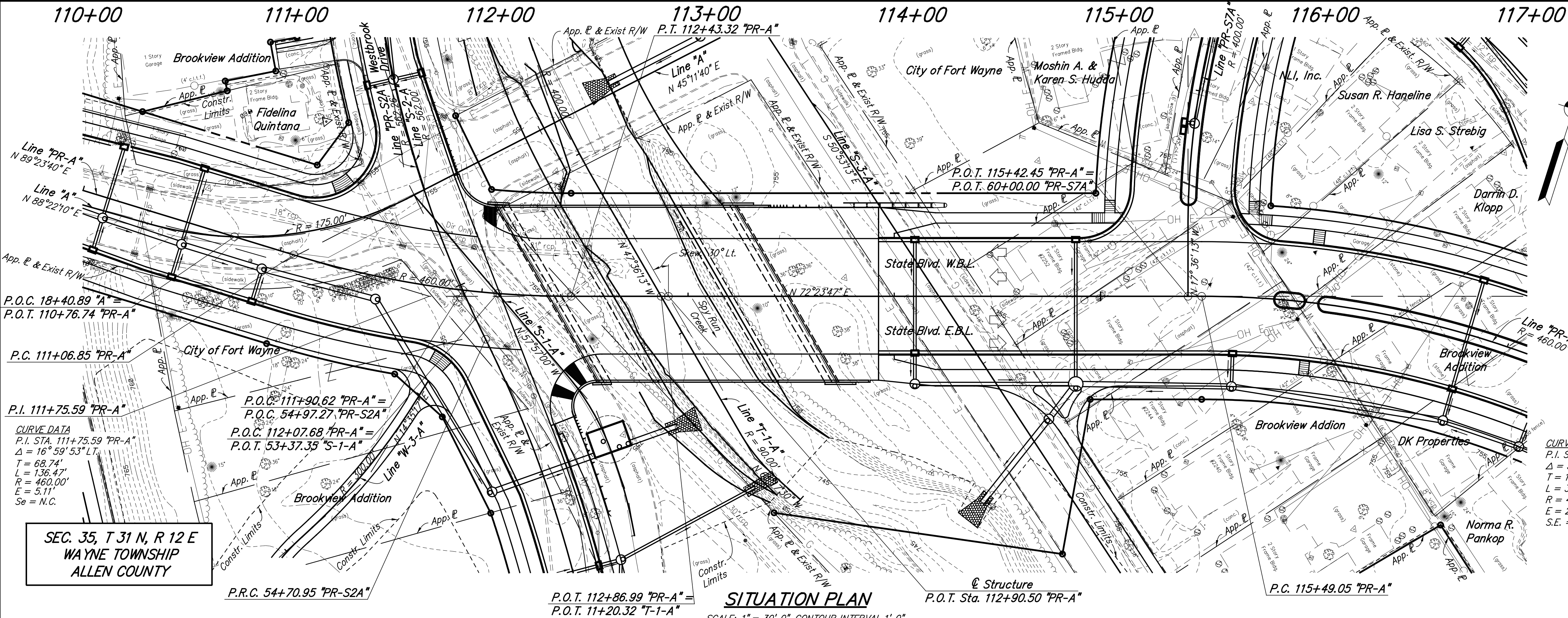
RECOMMENDED
FOR APPROVAL
DESIGN ENGINEER
DATE

DESIGNED: BNM
CHECKED: RMC
DRAWN: EWM
CHECKED: RMC

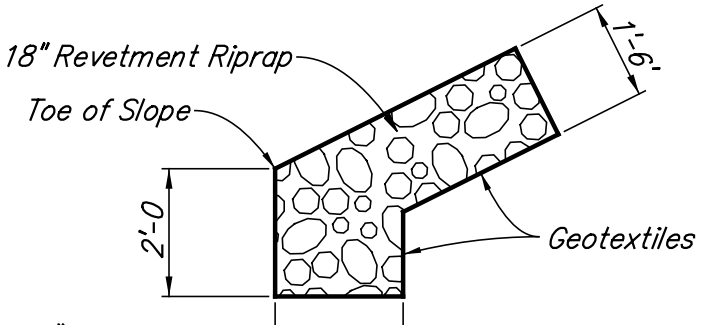
INDIANA
DEPARTMENT OF TRANSPORTATION

CHANNEL CHANGE LAYOUT

HORIZONTAL SCALE AS NOTED	BRIDGE FILE ALLEN CO. BR. #545
VERTICAL SCALE	DESIGNATION NO. 1005152
SURVEY BOOK	SHEETS 6 of 15
CONTRACT R-33130	PROJECT NO. 1005152

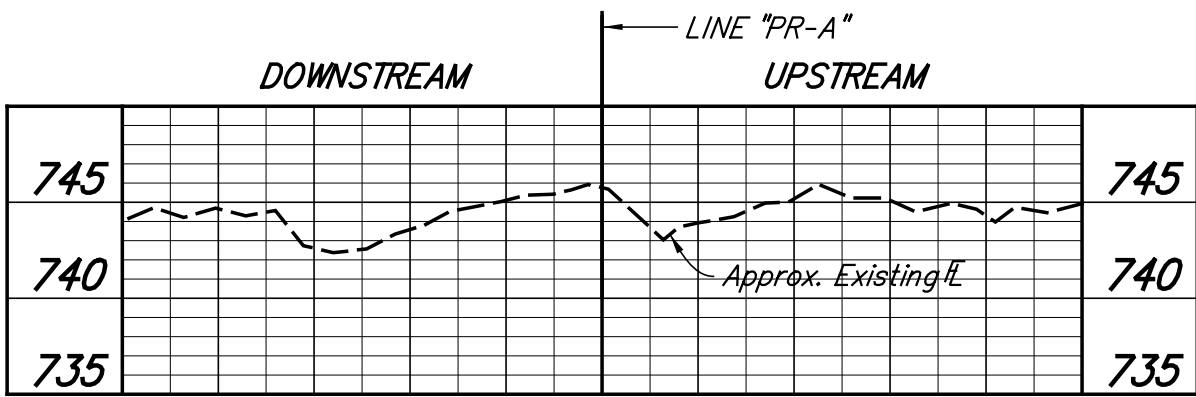


EXISTING STRUCTURE
(TO BE REMOVED)



TYPICAL RIPRAP
KEYWAY DETAIL
NO SCALE

CURVE DATA
P.I. Sta. 117+06.31 "PR-A"
 $\Delta = 37^\circ 44' 51''$ (RT)
T = 157.26'
L = 303.06'
R = 460.00'
E = 26.14'
S.E. = NC



EXISTING STREAM PROFILE
SCALES: 1" = 200'-0" HORIZONTAL, 1" = 10'-0" VERTICAL

HYDRAULIC DATA

Drainage Area	= 14.9 Sq. Mi.
Discharge (Q100)	= 2350 Cfs.
Q100 High Water Elev.	= 758.55 Ft.
Back Water @ Q100	= 0.11 Ft.
Velocity	= 2.56 Ft./Sec.
Waterway Opening Required (Below Elev. 758.55)	= 947.0 Sft.
Waterway Opening Provided (Below Elev. 758.55)	= 947.0 Sft.
Freeboard Provided (Above Elev. 758.55)	= 1.29 Ft.
Q500 Scour Elevation	= 741.05 Ft.

NOTES:

All R/W on This Sheet is Described From Line "A".

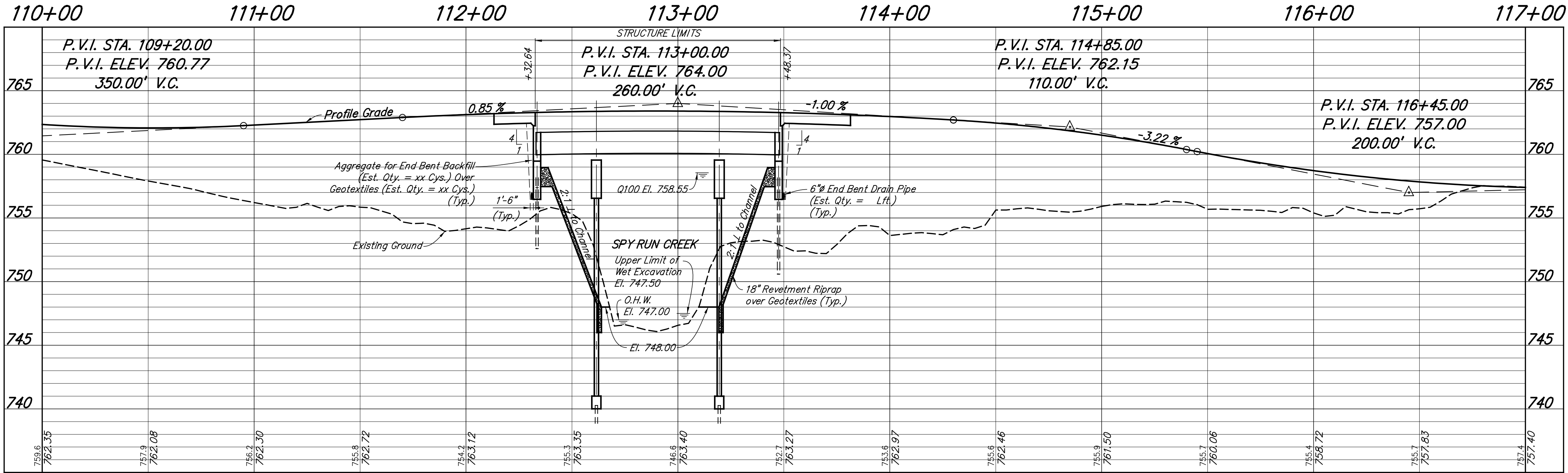
For Alignment References, Benchmarks and Topo References, See Road Plans.

Cross Hatched Areas Indicate Limits of 18" Revetment Riprap Over Geotextiles. (Est. Qty. = * Tons of 18" Revetment Riprap Over * Sys. of Geotextiles)

For Additional Road Alignment Data, see Roadway Geometrics Sheets in the Road Plans.

CONTINUOUS COMPOSITE PRESTRESSED
CONCRETE BOX BEAM BRIDGE

STATE BOULEVARD OVER SPY RUN CREEK
3 SPANS: 28'-0", 58'-0", 28'-0" SKEW: 30° LT. CLEAR ROADWAY: 56'-0"



PROFILE ON PROPOSED ROADWAY

SCALES: 1" = 30'-0" HORIZONTAL, 1" = 5'-0" VERTICAL

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: RMC

DRAWN: EWM

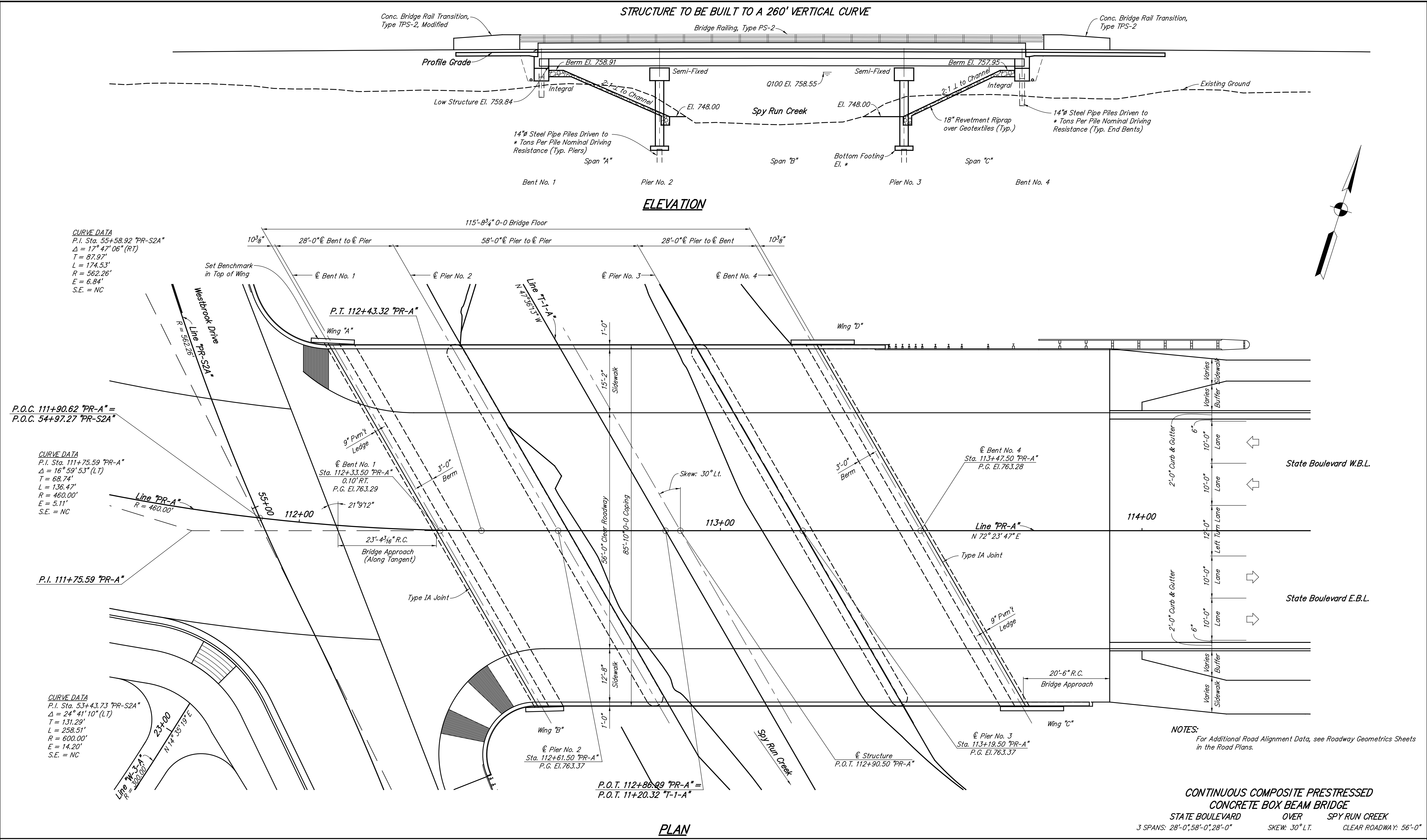
CHECKED: JPD

CHECKED: JPD

INDIANA
DEPARTMENT OF TRANSPORTATION

LAYOUT

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	ALLEN CO. BR. #545
VERTICAL SCALE	DESIGNATION NO.
AS NOTED	1005152
SURVEY BOOK	SHEETS
7	of 15
CONTRACT	PROJECT NO.
R-33130	1005152



GENERAL NOTES

Reinforcing steel covering to be 2½" in the top and 1" minimum in the bottom of floor slabs, 3" in the footings except the bottom steel which shall be 4", and 2" in all other parts, unless noted.

DESIGN DATA

- LIVE LOAD:

Superstructure and substructure designed for HL-93 loading, and sidewalk live load in accordance with the AASHTO LRFD Bridge Design Specifications, Fourth Edition, 2007, and Interims through 2008.
- DEAD LOAD:

Actual Weight plus 35 Lbs./Sft. for future wearing surface and 15 Lbs./Sft. for permanent metal deck forms.
- FLOOR SLAB:

Designed for 32,000 Lbs. axle load impact with a structural depth of 7½".
- UNIT STRESSES:

Reinforcing Steel, F_y = 60,000 p.s.i.
Concrete Class B, f_c = 3,000 psi
Concrete Class A, f_c = 3,500 psi
Concrete Class C, f_c = 4,000 psi

CONSTRUCTION LOADING

The exterior girder has been checked for strength, deflection, and overturning using the constructions loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior girder. The finishing machine was assumed to be supported 6" outside the vertical coping form. The top overhang brackets were assumed to be located 6" past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the girder bottom flange and web.

DESIGN DATA

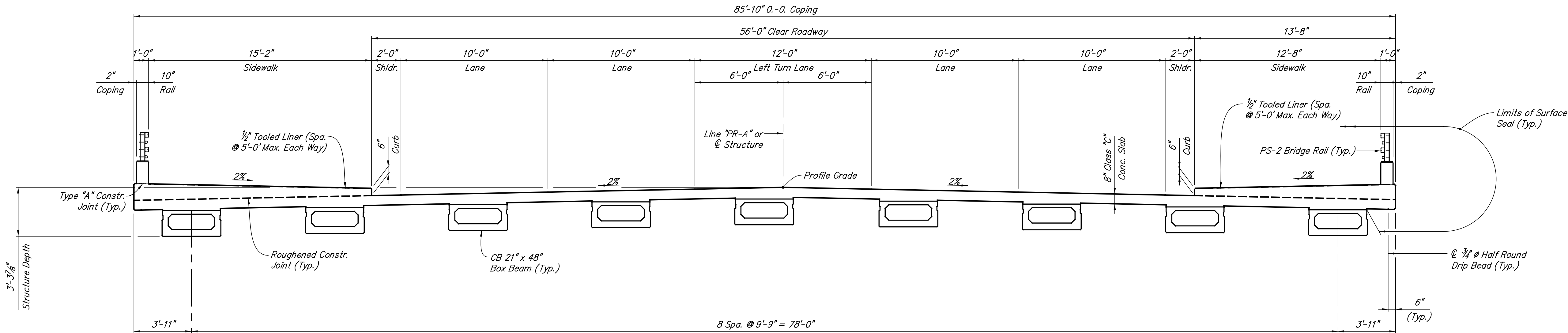
- DECK FALSEWORK LOADS:

Designed for 15 Lbs./Sft. for permanent metal stay-in-place deck forms, removable deck forms, and 2' exterior walkway.
- CONSTRUCTION LIVE LOAD:

Designed for 20 Lbs./Sft. extending 2' past the edge of coping and 75 Lbs./Ft. vertical force applied at a distance of 6" outside the face of coping over a 30' length of the deck centered with the finishing machine.
- FINISHING-MACHINE LOAD:

4500 Lbs. distributed over 10' along the coping.
- WIND LOAD:

Designed for 70 mph horizontal wind loading of 50 Lbs./Sft. in accordance with AASHTO Guide Design Specifications for Bridge Temporary Works (1995), Figure 2.1.



TYPICAL SECTION

CONTINUOUS COMPOSITE PRESTRESSED
CONCRETE BOX BEAM BRIDGE

STATE BOULEVARD OVER SPY RUN CREEK
3 SPANS: 28'-0", 58'-0", 28'-0" SKEW: 30° L.T. CLEAR ROADWAY: 56'-0"

		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE		
		DESIGN ENGINEER		DATE		1/4" = 1'-0", UNLESS NOTED		ALLEN CO. BR. #545		
		DESIGNED: RMC		DRAWN: EWM		VERTICAL SCALE		DESIGNATION NO.		
		CHECKED: JPD		CHECKED: JPD		1/4" = 1'-0", UNLESS NOTED		1005152		
		GENERAL PLAN				SURVEY BOOK		SHEETS		
								9 of 15		
						CONTRACT		PROJECT NO.		
						R-33130		1005152		

BRIDGE FILE	ITEM	STRUCTURE														QUANTITIES																					
		CONCRETE			CONCRETE RAILING CLASS C		CONC. BRIDGE RAILING TRANS., TBT	REINF. CONC. BRIDGE APPR., 10"	REINF. CONC. BRIDGE APPR., 12"	DENSE GRADED SUBBASE	REINF. STEEL TOTAL	STRUCT. STEEL ***	EPOXY COATED REINF. STEEL	ANCHOR PLATES MK -AP1	CONICAL PILE TIP, 14"	PILES						CAST IRON GRATES, BASINS & FITTINGS	EXPANSION JOINT TYPE BS9	EXPANSION JOINT TYPE M	EXPANSION JOINT CLASS SS	CONC. STR. BOX BEAMS TYPE	MEMBERS I BEAM TYPE	ANCHOR BOLT AB-1	BARRIER DELINEATOR	FIELD DRILLED HOLES IN CONCRETE	2" # GALV. STEEL PIPE CONDUIT	SURFACE SEAL *	THREADED TIE BAR ASSEMBLY	FIELD DRILLED HOLES	MASONRY COAT *	INTEG. CONC. CURB	
		CLASS C IN SUPERSTR.	CLASS A IN SUBSTR.	CLASS B IN FOOTING												EPOXY COATED SHELLS		TREATED TIMBER		STEEL PIPE, 14"																	STEEL H BEARING
		CU. YDS.	CU. YDS.	CU. YDS.	CU. YDS.	LIN. FT.	EACH	SYS.	SYS.	CYS.	LBS.	LBS.	LBS.	EACH	EACH	NO.	LIN. FT.	NO.	LIN. FT.	NO.	LIN. FT.	NO.	LIN. FT.	LBS.	LIN. FT.	LIN. FT.	LIN. FT.	SQ. FT.	LIN. FT.	EACH	EACH	EACH	LIN. FT.	SQ. FT.	EACH	EACH	SQ. FT.
																												</									

PROJECT	DESIGNATION
1005155	1005155
CONTRACT	BRIDGE FILE
R-33130	

STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
XXX	PREFABRICATED PEDESTRIAN BRIDGE	1 SPAN: 91'-6 1/2" SKEW: 23° RT.	STATE BOULEVARD	7+50.25 "W-1-A"

KIN DESIGNATION NUMBERS	
DESIGNATION	DESCRIPTION
1005151	STATE BOULEVARD RECONSTRUCTION PART 1
1005152	STATE BOULEVARD OVER SPY RUN CREEK
1005154	STATE BOULEVARD RECONSTRUCTION PART 2

INDIANA
DEPARTMENT OF
TRANSPORTATION

BRIDGE PLANS
FOR SPANS OVER 20 FEET
PUFFERBELLY TRAIL OVER STATE BOULEVARD

PROJECT NO. 1005155 P.E. R/W CONST.

The Proposed Pedestrian Bridge Along Pufferbelly Trail Over State Boulevard is Located Approximately 0.09 Miles East of the Intersection of Cass Street & State Boulevard in Section 35, T-31-N, R-12-E, Wayne Township, Allen County, Indiana.

BEGIN PROJECT
STA. 104+75.00 "PR-A"

END PROJECT
STA. 118+50.00 "PR-A"

ALLEN CO. BRIDGE NO. XXX

WAYNE TOWNSHIP
ALLEN COUNTY

TRAFFIC DATA	
A.A.D.T.	V.P.D.
A.A.D.T.	V.P.D.
D.H.V.	V.P.H.
DIRECTIONAL DISTRIBUTION	%
TRUCKS	% D.H.V.

DESIGN DATA	
DESIGN SPEED	20 M.P.H.
PROJECT DESIGN CRITERIA	
FUNCTIONAL CLASSIFICATION	
RURAL/URBAN	
TERRAIN	
ACCESS CONTROL	

PROJECT LOCATION SHOWN BY

LATITUDE: 41°05'46" N LONGITUDE: 85°08'32" W

BRIDGE LENGTH : 0.018 Mile
ROADWAY LENGTH : 0.148 Mile
TOTAL LENGTH : 0.166 Mile
MAX. GRADE : 5.00 %

HYDRAULIC UNIT CODE: 04100004060060

7260 SHADELAND STATION
INDIANAPOLIS, IN 46256-3957
TEL 317.547.5580 FAX 317.543.0270
www.structurepoint.com

PLANS PREPARED BY: American Structurepoint, Inc. (317) 547-5580 PHONE NUMBER

CERTIFIED BY: DATE

APPROVED FOR LETTING: CHIEF, DIVISION OF DESIGN DATE

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
	1005155
HYDRAULIC UNIT CODE	SHEETS
04100004060060	1 of 13
CONTRACT	PROJECT NO.
R-33130	1005155

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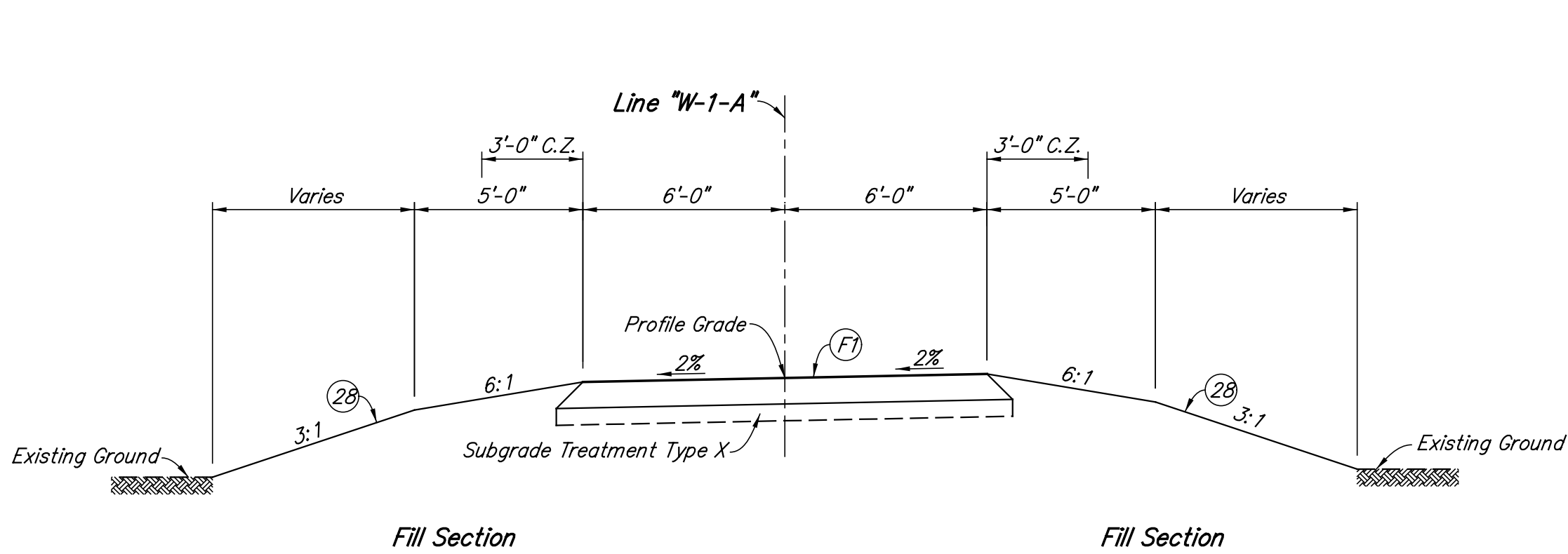
Appendix A
Page 21 of 129

UTILITIES	
<p><u>Electric</u> <i>American Electric Power Distribution PO Box 60 Ft. Wayne, IN 46801 Contact: Tom Kanning (260) 421-1766</i></p> <p><i>American Electric Power Transmission 700 Morrison Rd. Gahanna, OH 43230 Contact: Tina Hairston (614) 552-1801</i></p> <p><u>Television</u> <i>Comcast Cablevision 720 Taylor St. Ft. Wayne, IN 46802 Contact: John Gayday (260) 456-9000</i></p> <p><u>Gas</u> <i>NIPSCO 801 E. 86th. Ave. Merrillville, IN 46410 Contact: Jill Boganwright (419) 647-6502</i></p>	<p><u>Sewers</u> <i>City of Fort Wayne Utilities 1 East Main St., Room 480 Ft. Wayne, IN 46802 Contact: Craig Berndt (260) 427-2680</i></p> <p><u>Telephone</u> <i>Frontier Communications 8001 W. Jefferson Blvd, INIFAON Fort Wayne, IN 46804 Contact: Jean-Paul Kalonji (260) 461-3650</i></p> <p><u>Water</u> <i>City of Fort Wayne Utilities 1 East Main St., Room 480 Ft. Wayne, IN 46802 Contact: Craig Berndt (260) 427-2680</i></p>

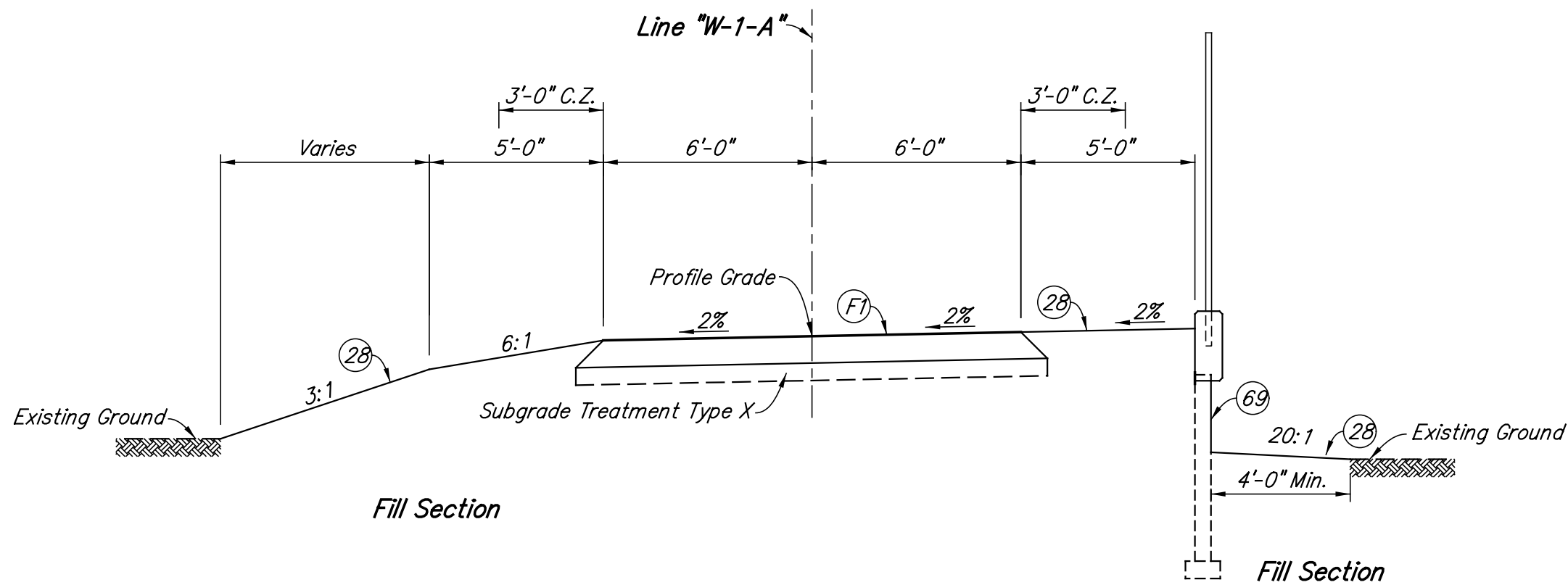
GENERAL NOTES	
**	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction. Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
**	All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.

[illegible][illegible]

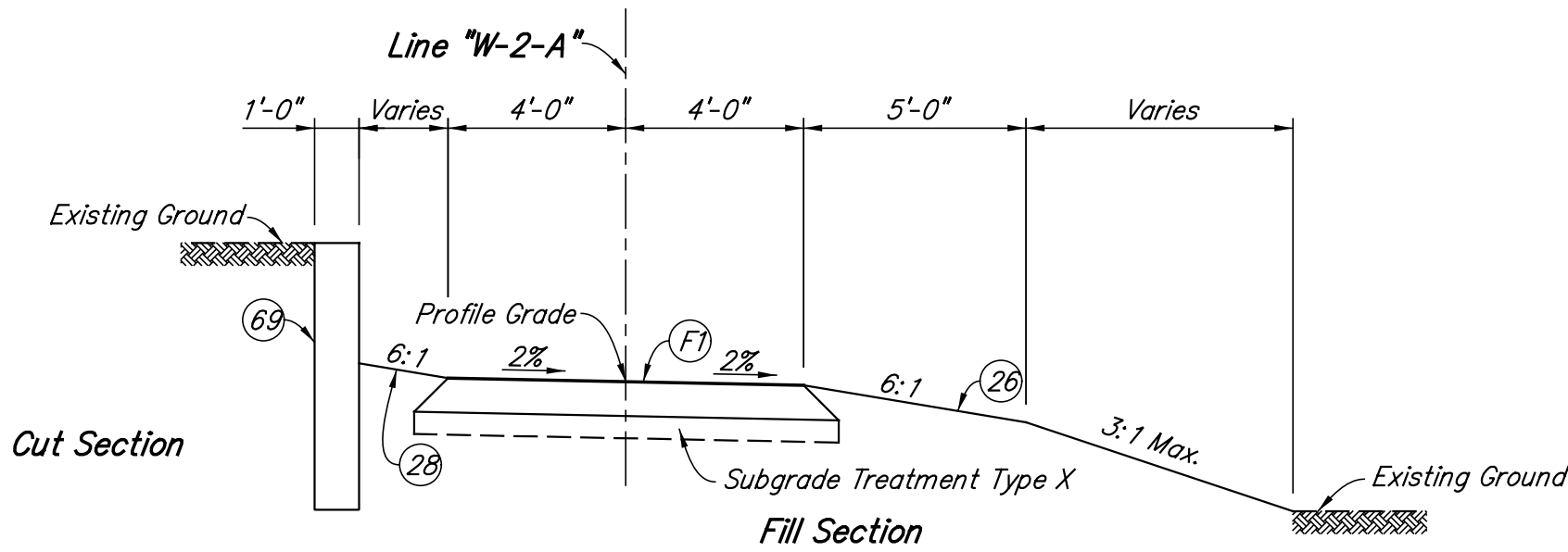
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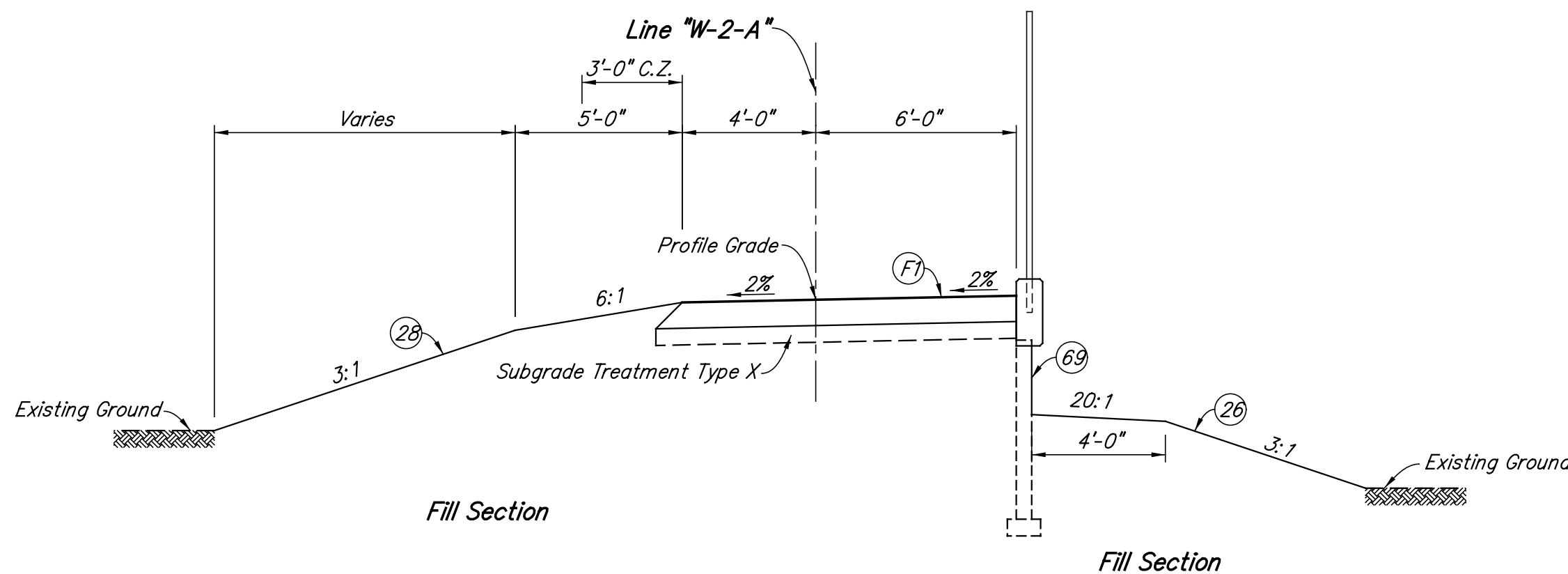
Typical Section Pufferbelly Trail
1+10.00 to 4+00.00 "W-1-A"
11+50.00 to 12+50.00 "W-1-A"



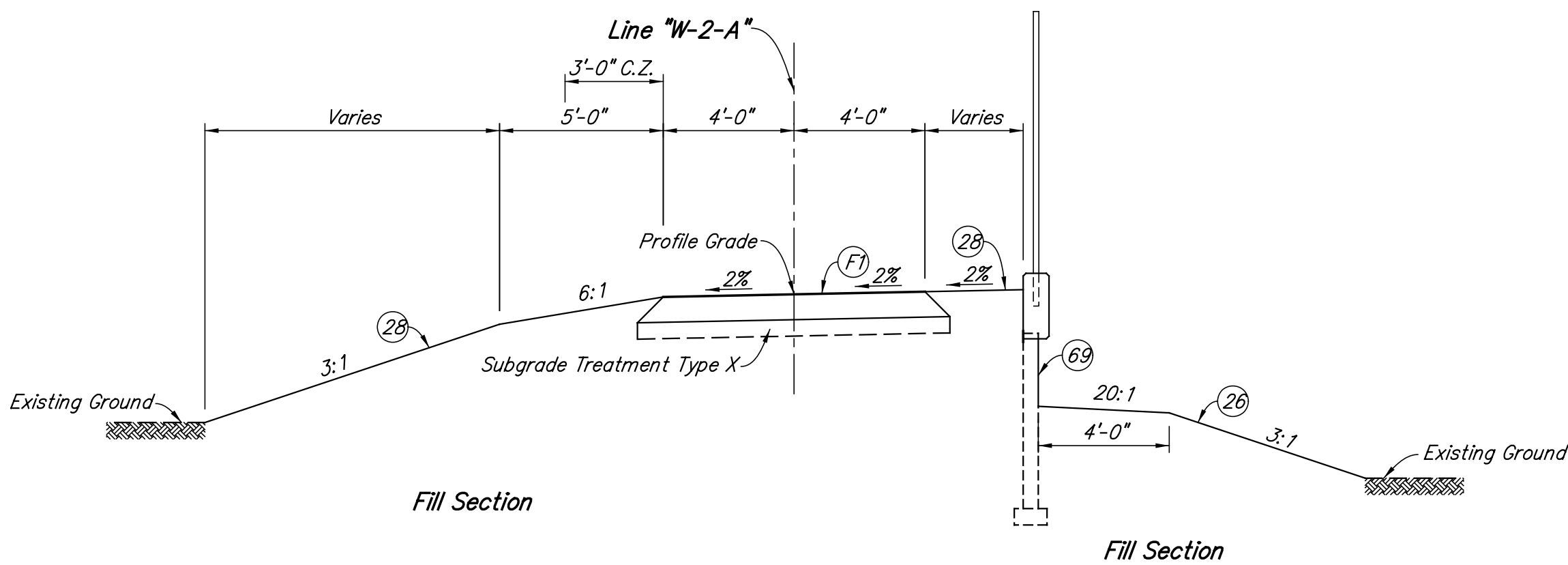
Typical Section Pufferbelly Trail
4+00.00 to 7+02.84 "W-1-A"
7+98.16 to 11+50.00 "W-1-A"



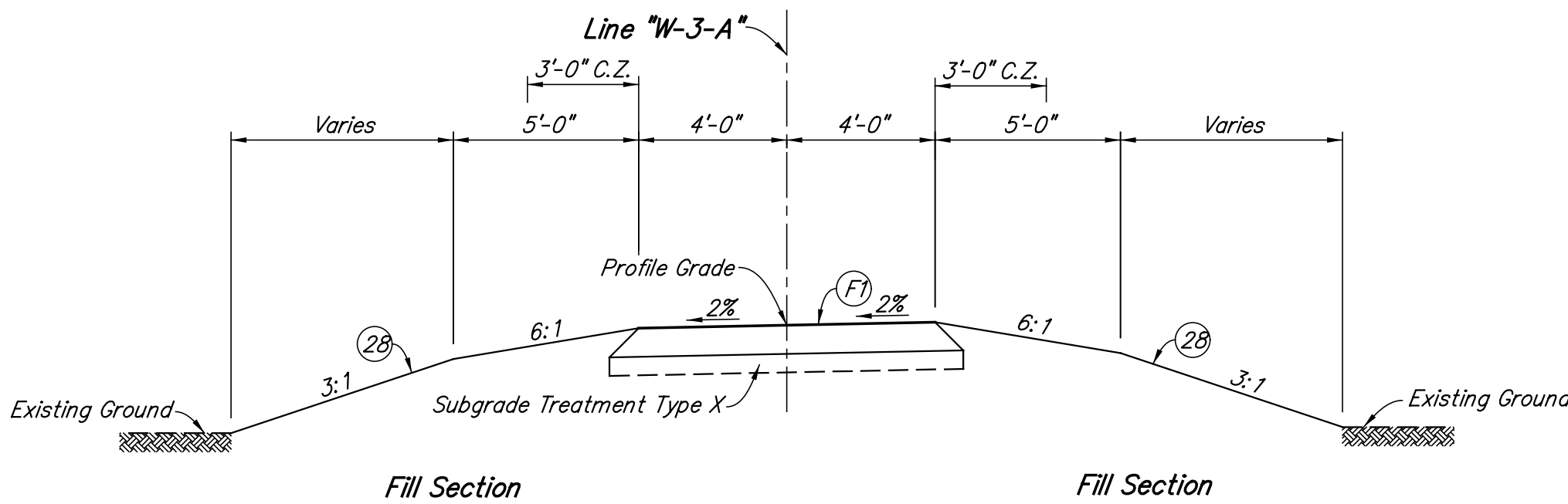
Typical Section Sidewalk Ramp
10+00.00 to 11+25.00 "W-2-A"



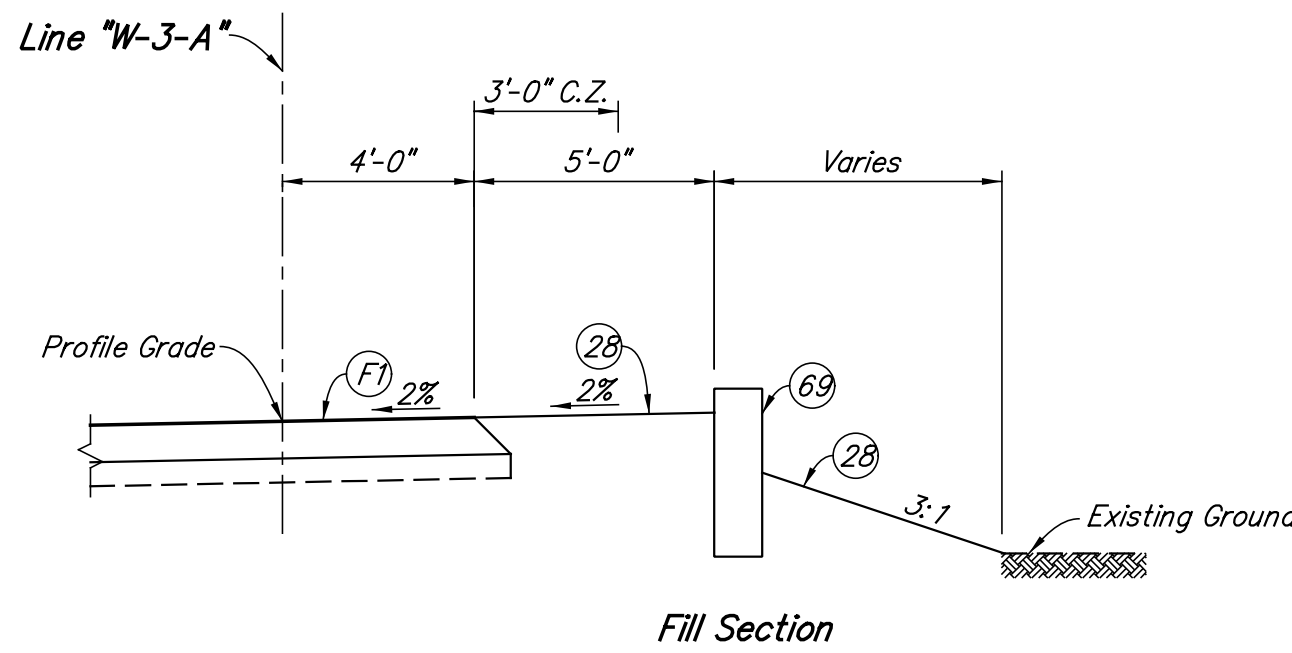
Typical Section Sidewalk Ramp
11+25.00 to 12+15.29 "W-2-A"



Typical Section Sidewalk Ramp
12+15.29 to 12+52.34 "W-2-A"



Typical Section Sidewalk Ramp
20+06.70 to 23+07.96 "W-3-A"



Retaining Wall Detail
20+50.00 to 21+25.00 Rt. "W-3-A"

FOR INFORMATION ONLY

(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx

(F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base

(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

(F) Concrete Sidewalk, 4"

(13) Concrete Curb
(14) Integral Concrete Curb
(15) Curb and Gutter, Concrete, Modified
(16) Curb and Gutter, C, Concrete

(22) Concrete Center Curb, Type "D"
(26) Sodding, Nursery
(28) Mulched Seeding, Type "U"
(69) Retaining Wall

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

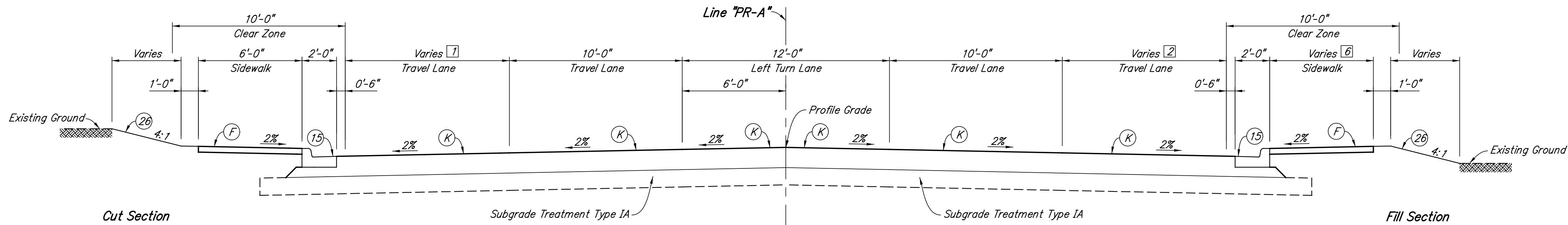
DESIGNED: ASU DRAWN: CAK

CHECKED: SMC CHECKED: SMC

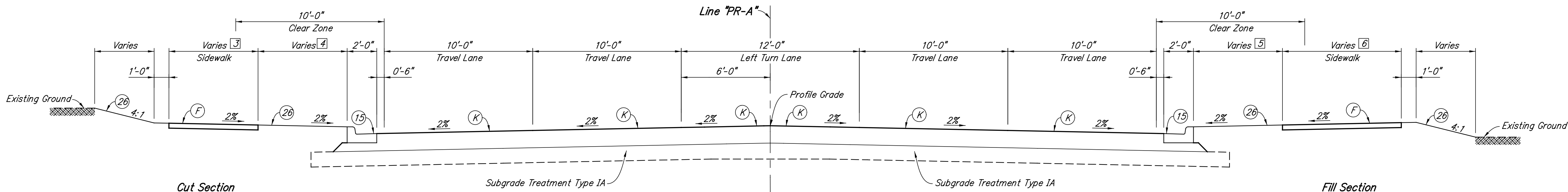
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
LINE "W-1-A" & "W-2-A" & "W-3-A"

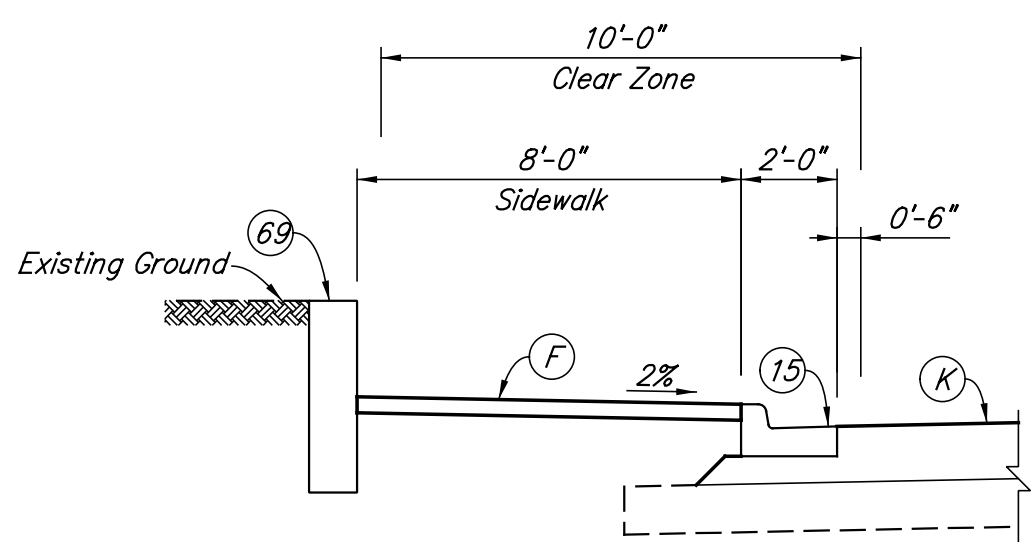
HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	DESIGNATION NO.
VERTICAL SCALE	1005155
SURVEY BOOK	SHEETS
CONTRACT	3 of 13
R-33130	PROJECT NO.
	1005155



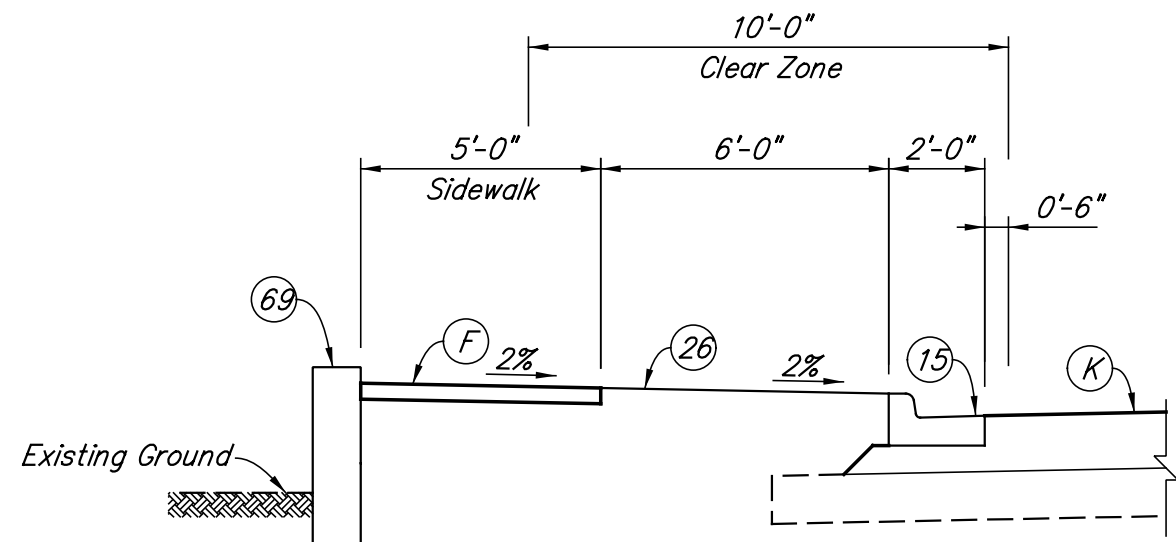
Typical Section - Incidental Construction
103+25.00 to 104+75.00 "PR-A"



Typical Section - State Boulevard
104+75.00 to 106+50.00 "PR-A"
110+40+00 to 112+07.58 "PR-A"

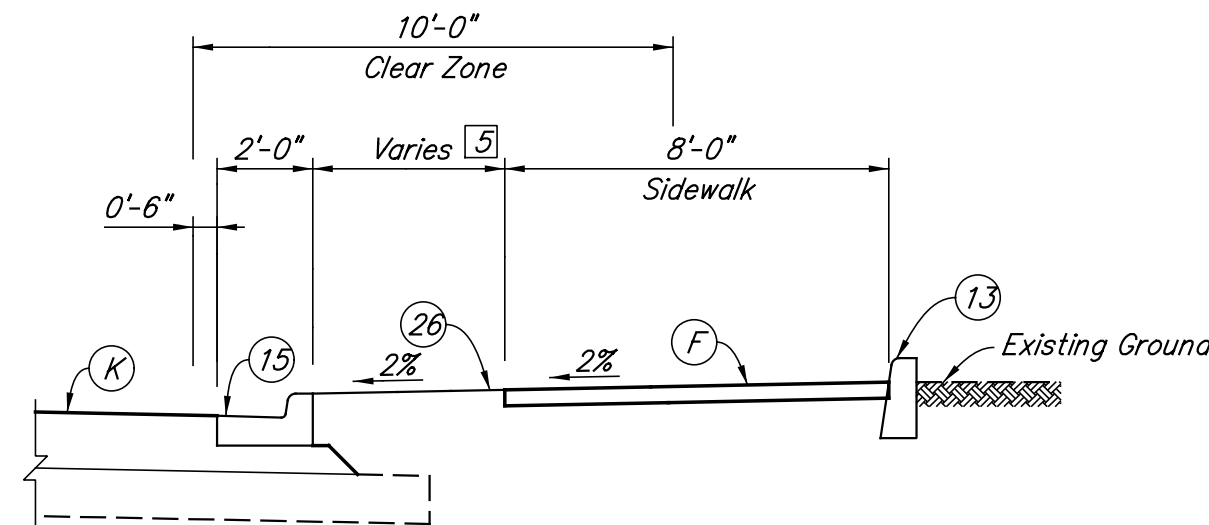


Cut Section
105+43.49 to 106+70.00 Lt. "PR-A"



Cut Section
109+85.00 to 110+39.68 Lt. "PR-A"

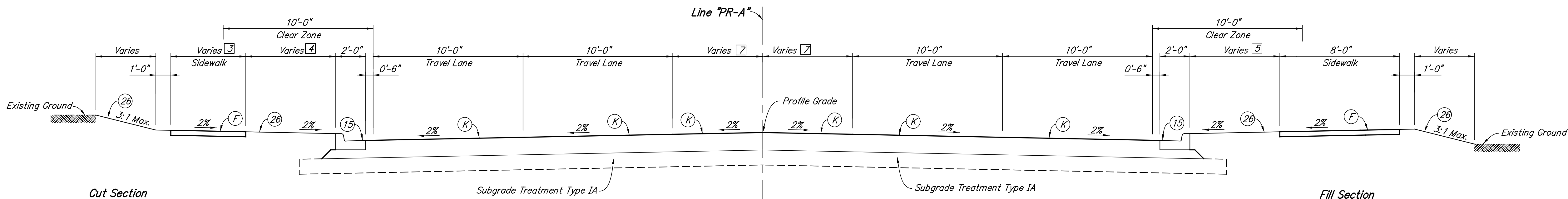
Paving Exception
112+07.58 to 113+92.34 "PR-A"



Cut Section
107+17.00 to 107+83.18 Rt. "PR-A"

- [1] Equals 9'-6" from 103+25.00 to 104+04.44 "PR-A"
Varies 9'-6" to 10'-0" from 104+04.44 to 104+73.72 "PR-A"
Equals 10'-0" from 104+73.72 to 104+75.00 "PR-A"
- [2] Equals 9'-6" from 103+25.00 to 104+50.00 "PR-A"
Varies 9'-6" to 10'-0" from 104+50.00 to 104+75.00 "PR-A"

- [3] Equals 6'-0" from 104+75.00 to 105+27.04 "PR-A"
Equals 8'-0" from 105+27.04 to 106+50.00 "PR-A"
Varies 8'-0" to 9'-0" from 106+50.00 to 106+70.00 "PR-A"
Varies 7'-0" to 5'-0" from 106+70.00 to 107+30.00 "PR-A"
Equals 5'-0" from 107+30.00 to 112+07.58 "PR-A"
- [4] Equals 0'-0" from 104+75.00 to 106+70.00 "PR-A"
Varies 2'-0" to 6'-0" from 106+70.00 to 107+50.00 "PR-A"
Equals 6'-0" from 107+50.00 to 112+07.58 "PR-A"
- [5] Equals 0'-0" from 104+75.00 to 106+90.00 "PR-A"
Varies 2'-0" to 6'-0" from 106+90.00 to 107+50.00 "PR-A"
Equals 6'-0" from 107+50.00 to 112+07.58 "PR-A"
- [6] Equals 6'-0" from 103+25.00 to 104+11.27 "PR-A"
Varies 6'-0" to 10'-5" from 104+11.27 to 104+85.03 "PR-A"
Equals 8'-0" from 104+85.03 to 112+07.58 "PR-A"
- [7] Varies 6'-0" to 1'-0" from 106+50.00 to 107+50.00 "PR-A"
Equals 1'-0" from 107+50.00 to 110+40.00 "PR-A"
Varies 1'-0" to 6'-0" from 109+40.00 to 110+40.00 "PR-A"



Typical Section - State Boulevard
106+50.00 to 110+40.00 "PR-A"

FOR INFORMATION ONLY

- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"

- (F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete

- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU

DRAWN: CAK

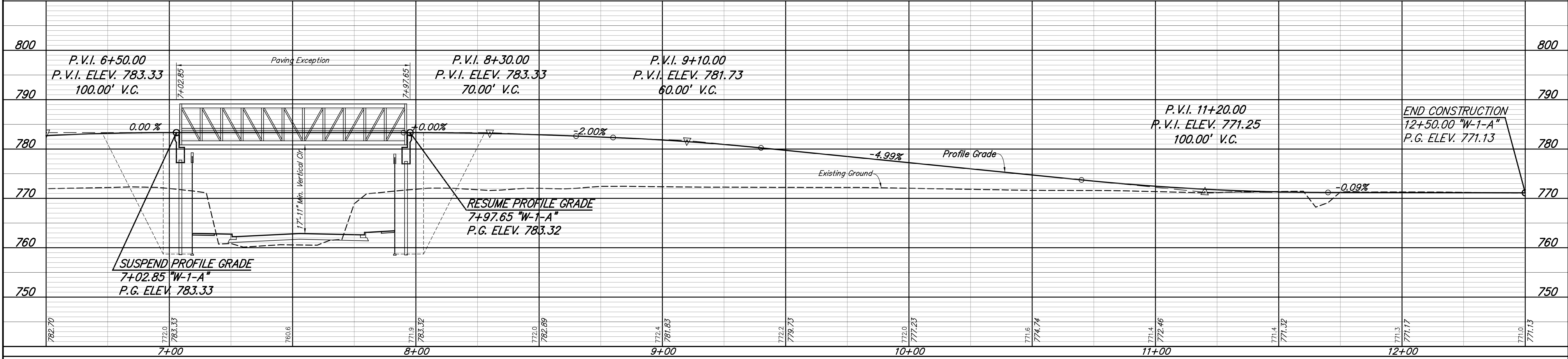
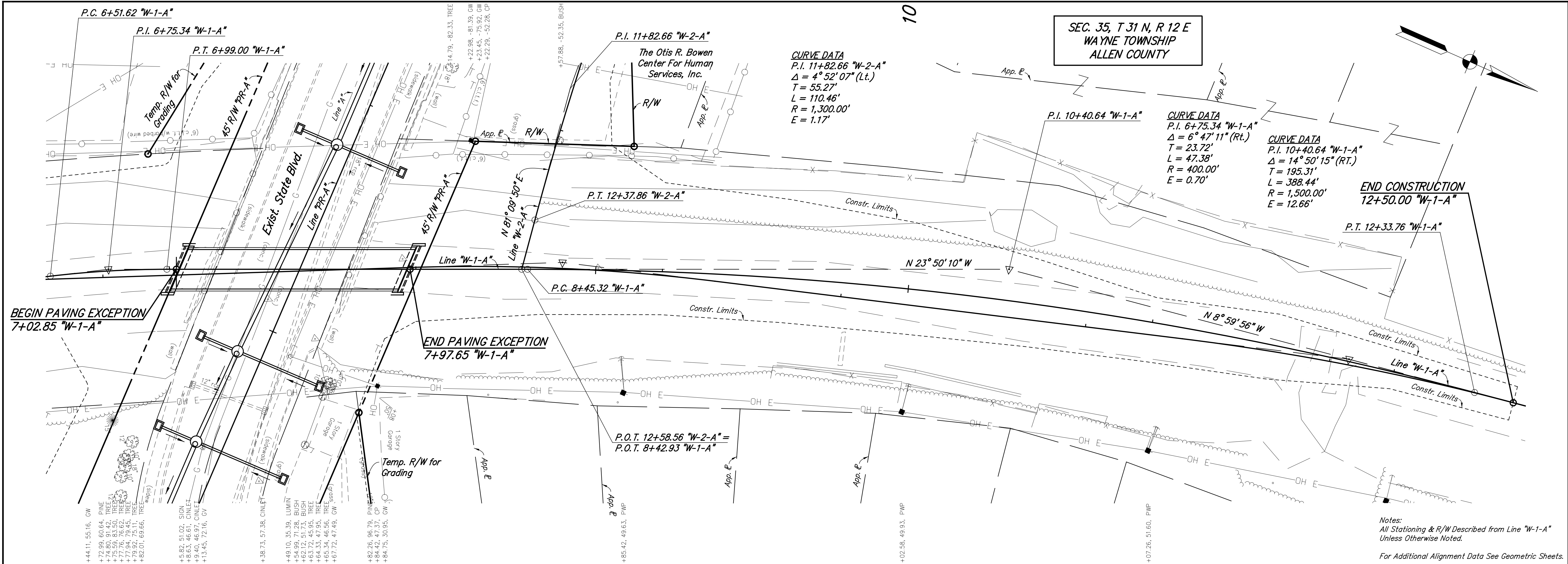
CHECKED: SMC

CHECKED: SMC

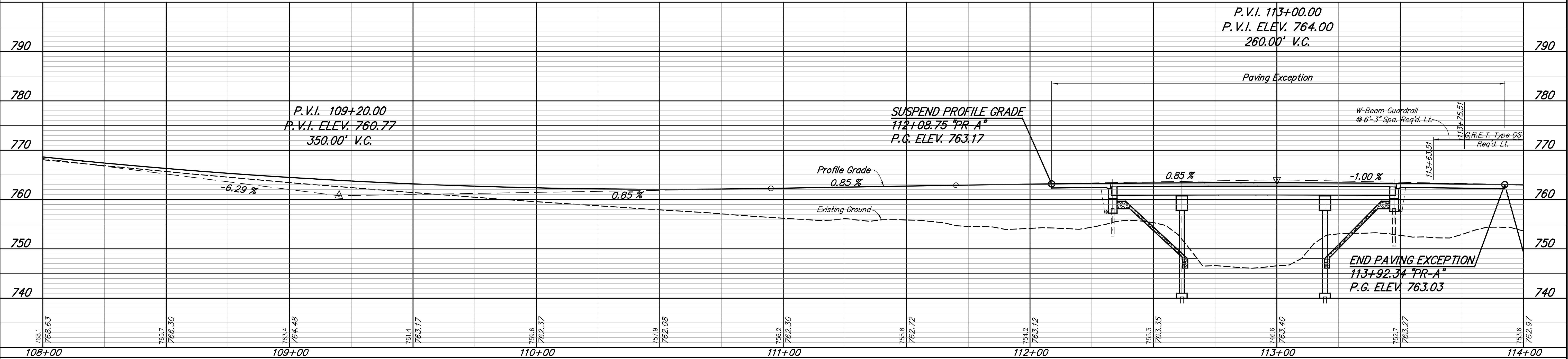
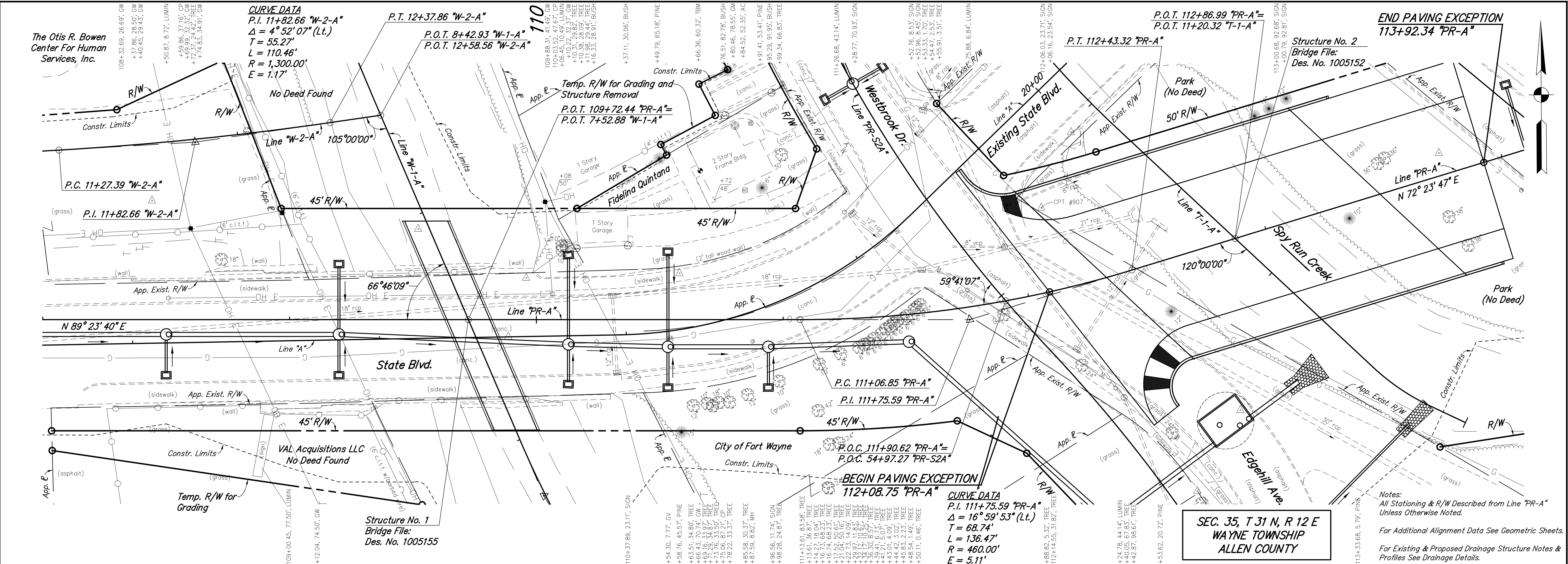
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
LINE "PR-A"

HORIZONTAL SCALE		BRIDGE FILE	
1/4" = 1'-0"		DESIGNATION NO.	
VERTICAL SCALE		1005155	
SURVEY BOOK		SHEETS	
CONTRACT		4	of 13
R-33130		PROJECT NO.	
		1005155	



FOR INFORMATION ONLY				INDIANA DEPARTMENT OF TRANSPORTATION		PLAN AND PROFILE LINE "W-1-A"		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
RECOMMENDED FOR APPROVAL				DESIGN ENGINEER		DATE		VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1005155	
DESIGNED: ASU				DRAWN: PJT				SURVEY BOOK		SHEETS	
CHECKED: SMC				CHECKED: SMC				CONTRACT R-33130		PROJECT NO. 1005155	
										5 of 13	



FOR INFORMATION ONLY				RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
												1" = 20'		1005155	
DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC		CHECKED: SMC		PLAN AND PROFILE LINE "PR-A"		SURVEY BOOK		SHEETS		PROJECT NO.	
										R-33130		6 of 13		1005155	

PROJECT	DESIGNATION
1005151	1005151
CONTRACT	BRIDGE FILE
R-30743	

INDIANA
DEPARTMENT OF
TRANSPORTATION

ROAD PLANS

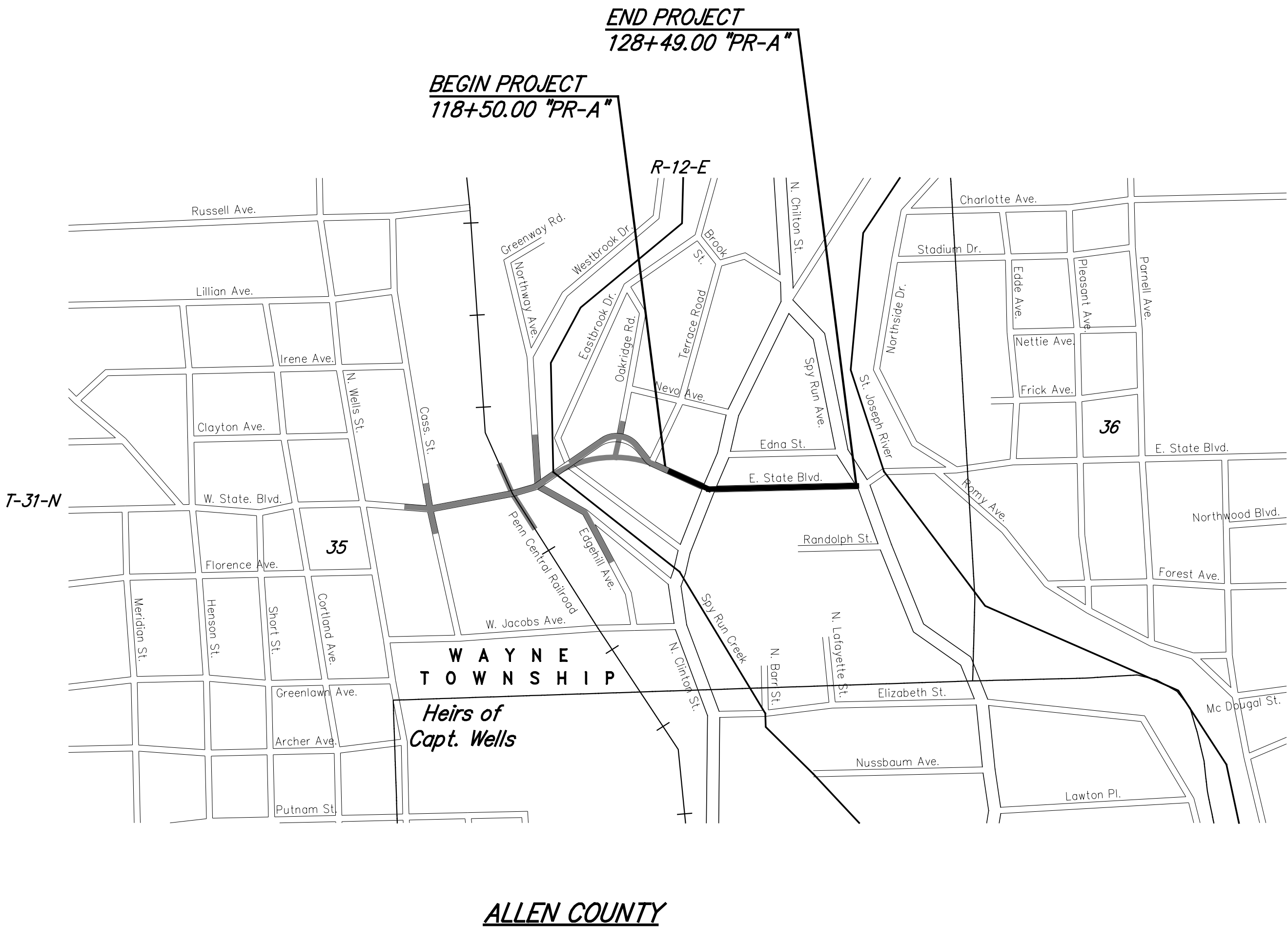
STATE BLVD. RECONSTRUCTION - PART 1

PROJECT NO. 1005151 P.E.

PROJECT NO. R/W

PROJECT NO. CONST.

Project Description: Road Reconstruction on State Boulevard Beginning 130.00 Feet East of the Intersection of State Boulevard and Terrace Road, Thence East 999.00 Feet Along Line "PR-A" to the Intersection of Spy Run Ave. and State Boulevard, All in Section 35, Township 31 North, Range 12 East, Wayne Township, Allen County, Indiana

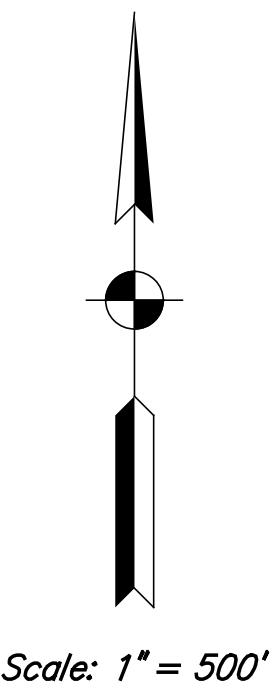


TRAFFIC DATA	STATE BLVD. - LINE "PR-A"
A.A.D.T. (2009)	20,650 V.P.D.
A.A.D.T. (2030)	26,200 V.P.D.
D.H.V.	2,620 V.P.H.
DIRECTIONAL DISTRIBUTION	60 %
TRUCKS	2 % D.H.V.
MAINLINE ESAL's	
DESIGN DATA	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Minor Arterial
RURAL/URBAN	Urban (Built-Up)
TERRAIN	Level
ACCESS CONTROL	None



BRIDGE LENGTH :	N/A	Mile
ROADWAY LENGTH :	0.189	Mile
TOTAL LENGTH :	0.189	Mile
MAX. GRADE :	1.00	%

HYDROLOGIC UNIT CODE: 04100004060060



INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2010
TO BE USED WITH THESE PLANS



AMERICAN
STRUCTUREPOINT
INC.

7260 SHADELAND STATION
INDIANAPOLIS, IN 46256-3967
TEL 317.547.5580 FAX 317.543.0270
www.structurepoint.com

PLANS PREPARED BY:	American Structurepoint, Inc.	(317) 547-5580 PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	
VERTICAL SCALE	DESIGNATION NO.
NONE	1005151
SURVEY BOOK	SHEETS
	1 of 50
CONTRACT	PROJECT NO.
R-30743	1005151

Electric

*American Electric Power
Distribution
PO Box 60
Ft. Wayne, IN 46801
Contact: Tom Kanning
(260) 421-1766*

*American Electric Power
Transmission
700 Morrison Rd.
Gahanna, OH 43230
Contact: Tina Hairston
(614) 552-1801*

Television

Comcast Cablevision
720 Taylor St.
Ft. Wayne, IN 46802
Contact: John Gayday
(260) 456-9000

Gas

NIPSCO
801 E. 86th. Ave.
Merrillville, IN 46410
Contact: Jill Boganwright
(419) 647-6502

Sewers

City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

Telephone

Frontier Communications
8001 W. Jefferson Blvd, INIFAON
Fort Wayne, IN 46804
Contact: Jean-Paul Kalonji
(260) 461-3650

Water

City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified

The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.

The paper relocation will be cross sectioned by the Engineer before construction.

Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.

The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.

All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.

**** REPRESENTS GENERAL NOTES REQUIRED**

SHEET NO.

DRAWING INDEX

1	Title Sheet
2	Index
3-5	Typical Cross Sections
6-7	Plat No. 1
8-11	Geometric Tie-Up Sheets
12-15	Maintenance of Traffic
16-19	Plan and Profile Sheets
20	Pavement Edge Transition Diagrams
21-23	Construction Details
24-26	Drainage Details
27	Retaining Wall Details
28-29	Pavement Marking and Signing Details
30-31	Signal Details

32-47	Cross Sections "Line PR-A"
48-50	Cross Sections "Line S-5-A"

SHEET NO.

DATE _____

REVISÉD

INDIANA

DEPARTMENT OF TRANSPORTATION

INDEX & GENERAL NOTES

HORIZONTAL SCALE

NONE

BRIDGE FILE

VERTICAL SCALE

NONE	1005151
------	---------

SURVEY BOOK

SHEETS

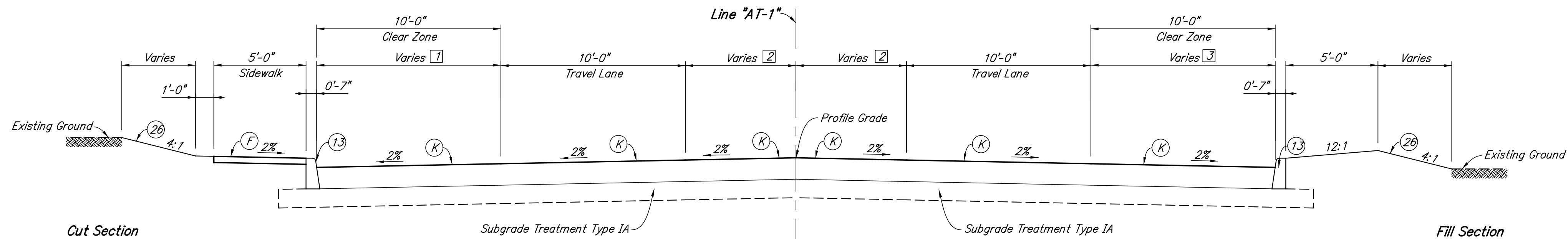
2	of	50
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CONTRACT

PROJECT NO.

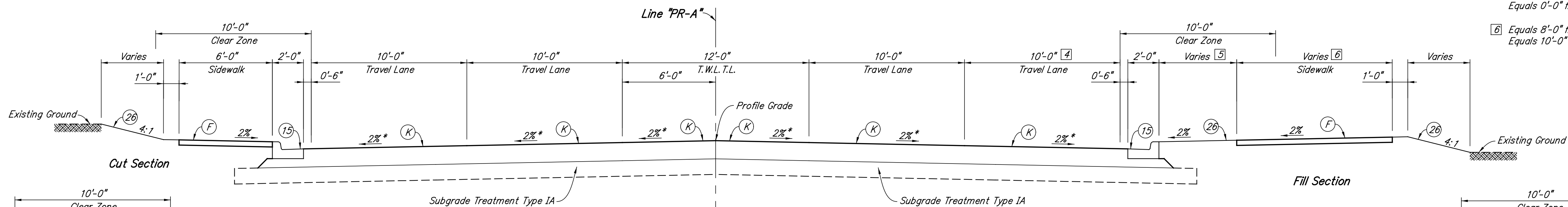
R-30743

1005151



Typical Section - Incidental Construction
117+38.96 to 118+49.91 "AT-1"

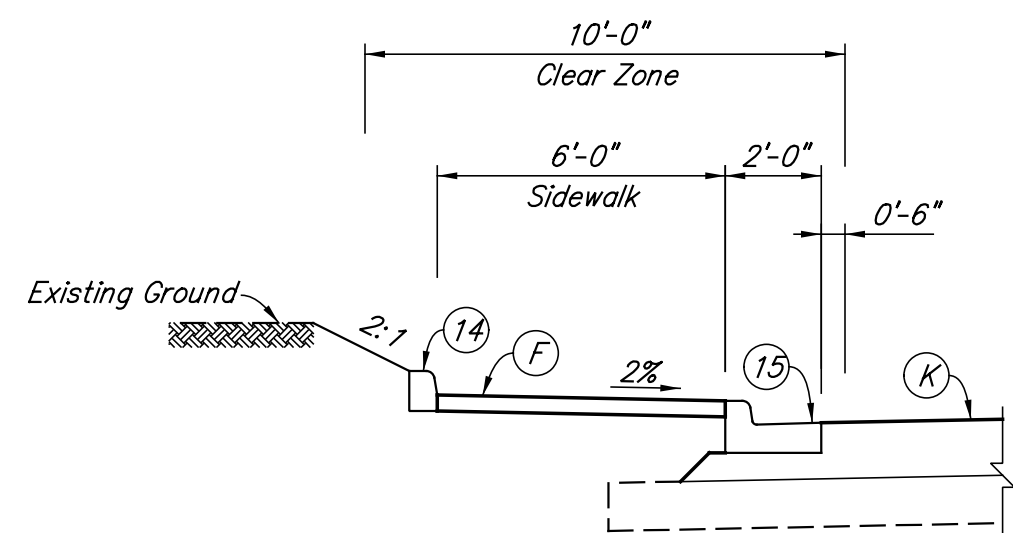
- 1 Equals 10'-0" from 118+50.00 to 119+70.00 "PR-A"
Varies 10'-0" to 12'-2" from 119+70.00 to 120+02.00 "PR-A"
Equals 12'-2" from 120+02.00 to 120+30.35 "PR-A"
- 2 Equals 0'-0" from 117+38.96 to 117+60.00 "AT-1"
Varies 0'-0" to 6'-0" from 117+60.00 to 118+49.91 "AT-1"
- 3 Equals 8'-0" from 118+50.00 to 118+99.05 "PR-A"
Equals 10'-0" from 118+99.05 to 120+30.35 "PR-A"



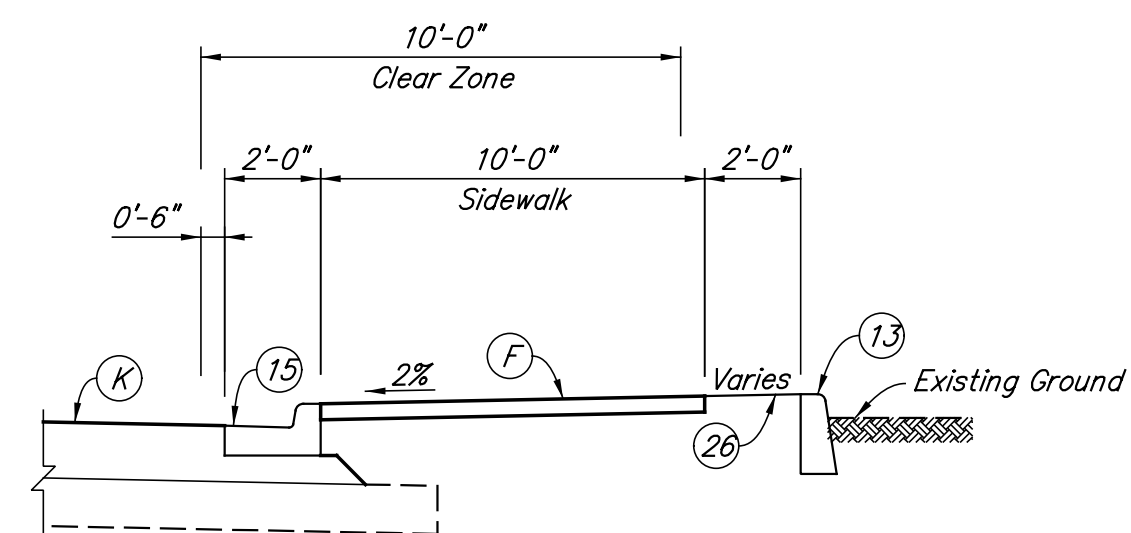
Typical Section - State Boulevard
118+50.00 to 120+30.35 "PR-A"

* Cross Slope Varies
See Pavement Edge Transition Sheets

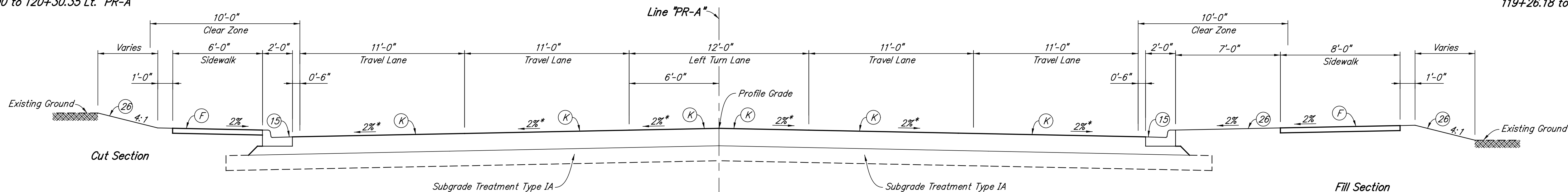
- 4 Equals 10'-0" from 118+50.00 to 119+70.00 "PR-A"
Varies 10'-0" to 12'-2" from 119+70.00 to 120+02.00 "PR-A"
Equals 12'-2" from 120+02.00 to 120+30.35 "PR-A"
- 5 Equals 7'-0" from 118+50.00 to 118+99.05 "PR-A"
Equals 0'-0" from 118+99.05 to 120+30.35 "PR-A"
- 6 Equals 8'-0" from 118+50.00 to 118+99.05 "PR-A"
Equals 10'-0" from 118+99.05 to 120+30.35 "PR-A"



Cut Section
118+85.00 to 120+30.35 Lt. "PR-A"

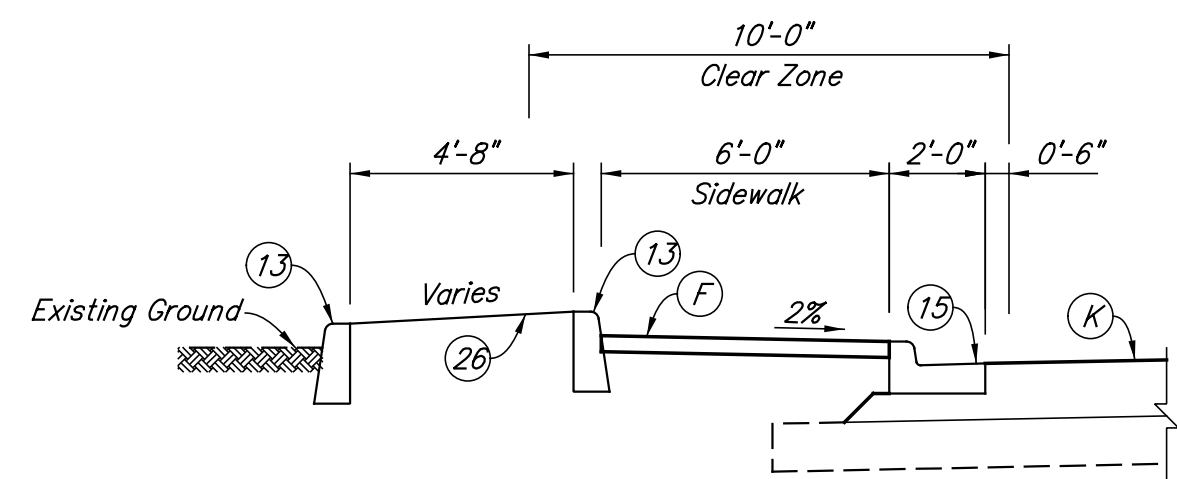


Cut Section
119+26.18 to 120+30.35 Rt. "PR-A"

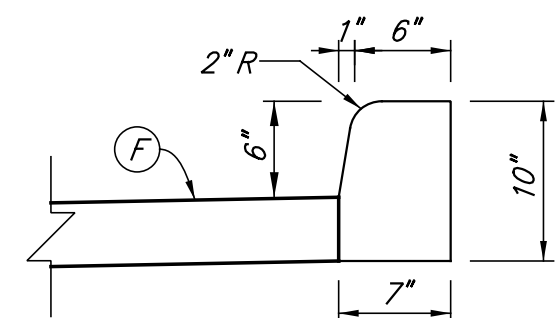


Typical Section - State Boulevard
120+63.35 to 122+80.00 "PR-A"

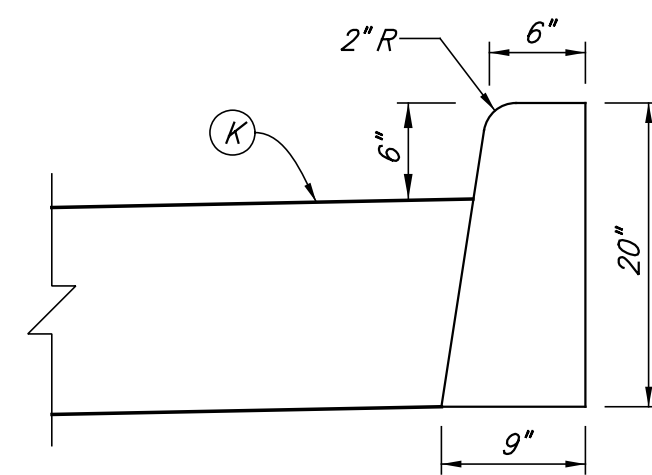
* Cross Slope Varies
See Pavement Edge Transition Sheets



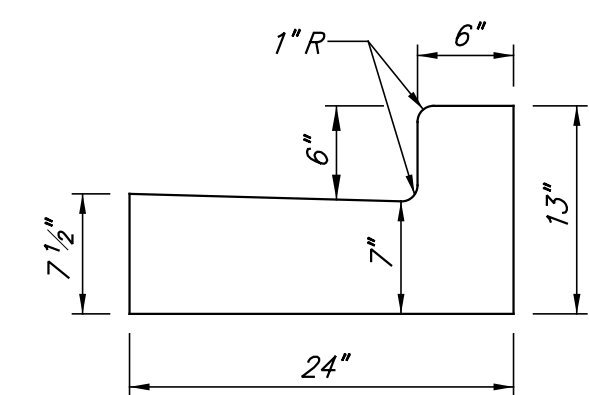
Cut Section
120+63.35 to 121+48.65 Lt. "PR-A"



Integral Concrete Curb Detail
Not to Scale



Concrete Curb Detail
Not to Scale



Curb & Gutter, Concrete, Modified Detail
Not to Scale

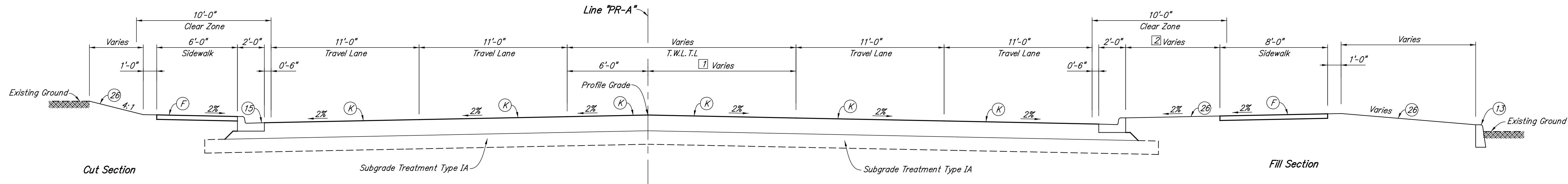
- LEGEND**
- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx
- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete
- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: CAK
CHECKED: SMC	CHECKED: SMC

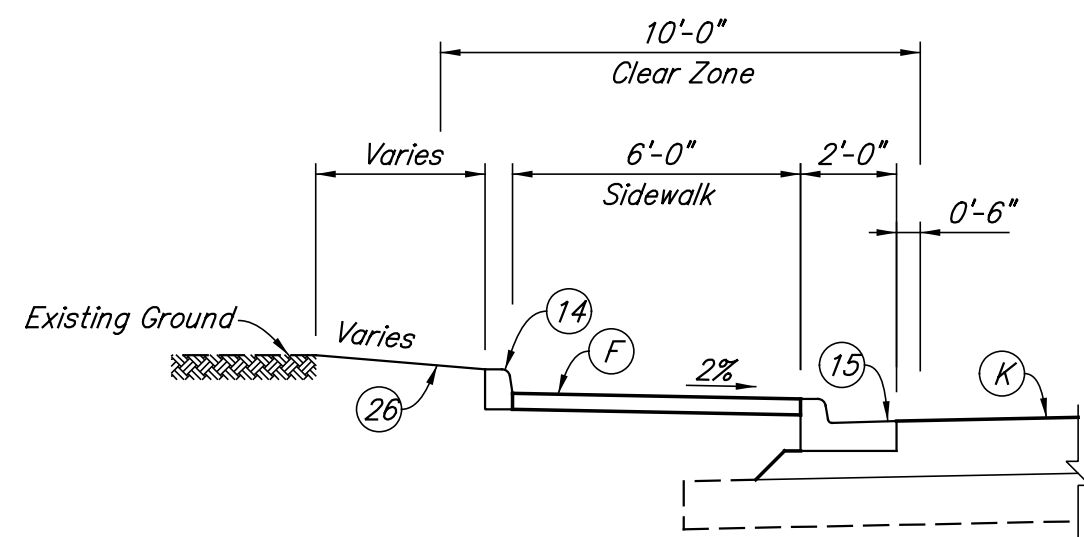
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "PR-A"	

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005151
SURVEY BOOK	SHEETS
CONTRACT R-30743	3 of 50
	PROJECT NO. 1005151

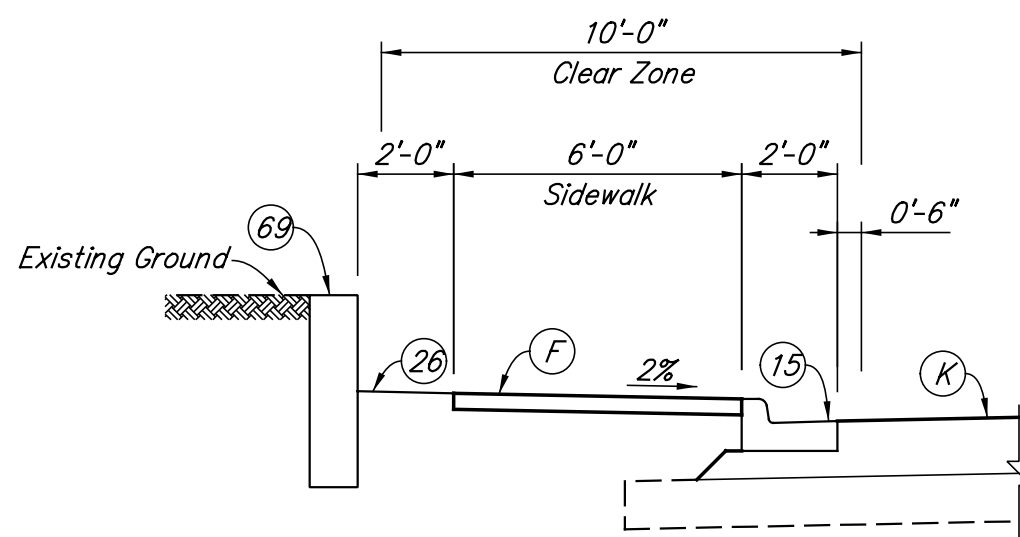
- ① Equals 6'-0" from 122+80.00 to 123+75.00 "PR-A"
Varies 6'-0" to 11'-0" from 123+75.00 to 125+10.00 "PR-A"
Equals 11'-0" from 125+10.00 to 126+30.00 "PR-A"
- ② Equals 7'-0" from 122+80.00 to 124+00.00 "PR-A"
Varies 7'-0" to 5'-8" from 124+00.00 to 124+53.14 "PR-A"
Varies 5'-8" to 7'-0" from 124+53.14 to 125+15.86 "PR-A"
Equals 7'-0" from 125+15.86 to 126+30.00 "PR-A"



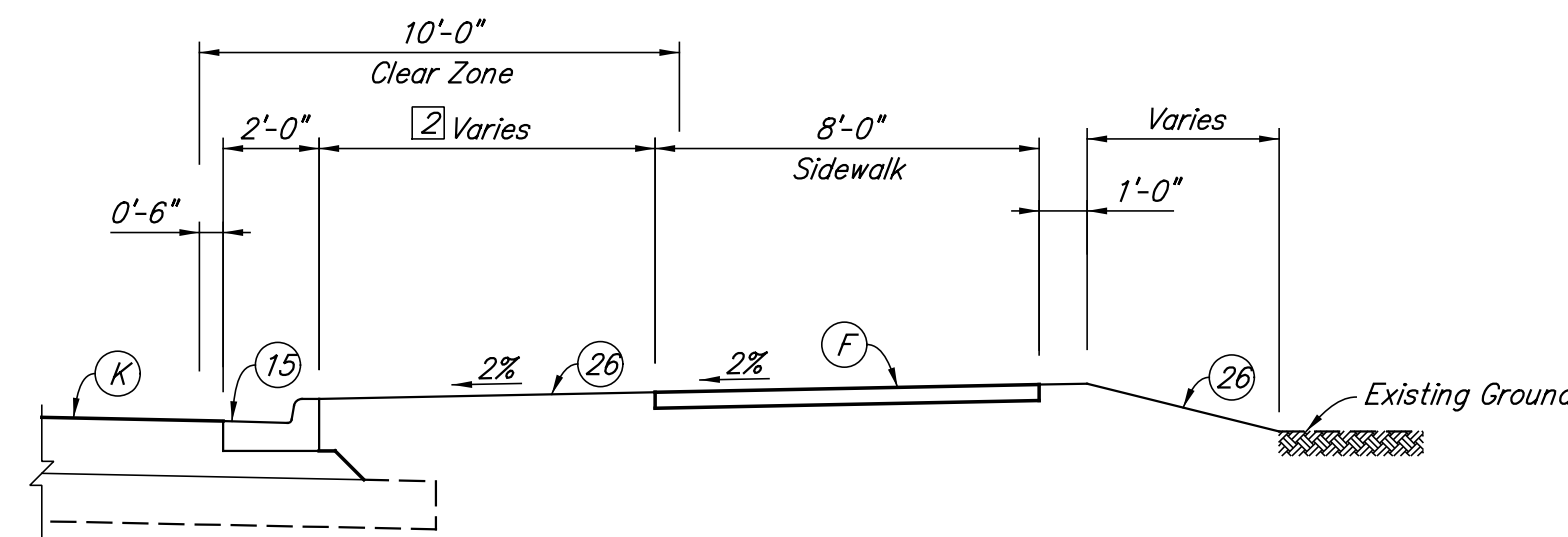
Typical Section - State Boulevard
122+80.00 to 126+30.00 "PR-A"



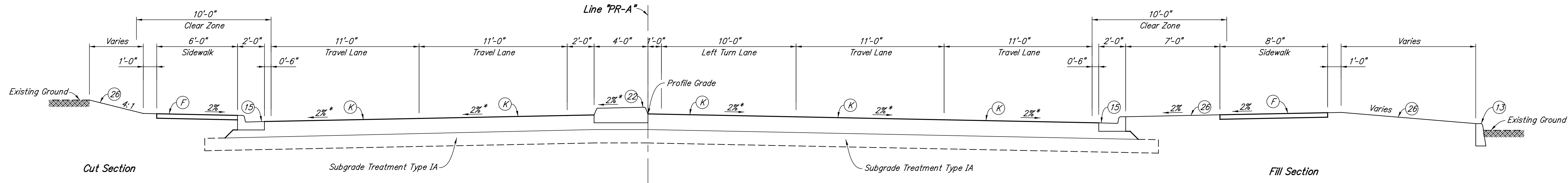
Cut Section
123+47.30 to 124+70.47 Lt. "PR-A"



Cut Section
124+79.00 to 126+39.00 Lt. "PR-A"



Fill Section
122+80.00 to 124+53.14 Rt. "PR-A"



Typical Section - State Boulevard
126+30.00 to 128+49.00 "PR-A"

* Cross Slope Varies
See Pavement Edge Transition Sheets

(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx

(F) Concrete Sidewalk, 4"

(F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base

(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

(13) Concrete Curb

(14) Integral Concrete Curb

(15) Curb and Gutter, Concrete, Modified

(16) Curb and Gutter, C, Concrete

(22) Concrete Center Curb, Type "D"

(26) Sodding, Nursery

(28) Mulched Seeding, Type "U"

(69) Retaining Wall

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU

DRAWN: CAK

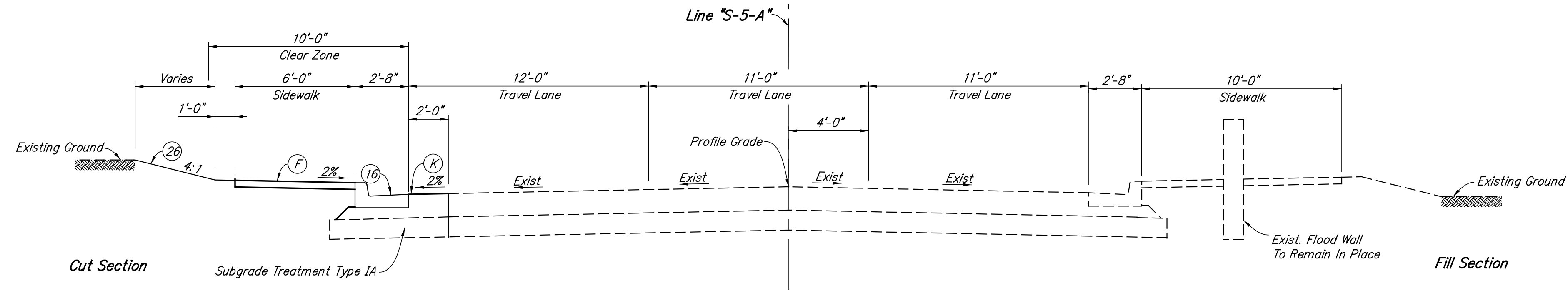
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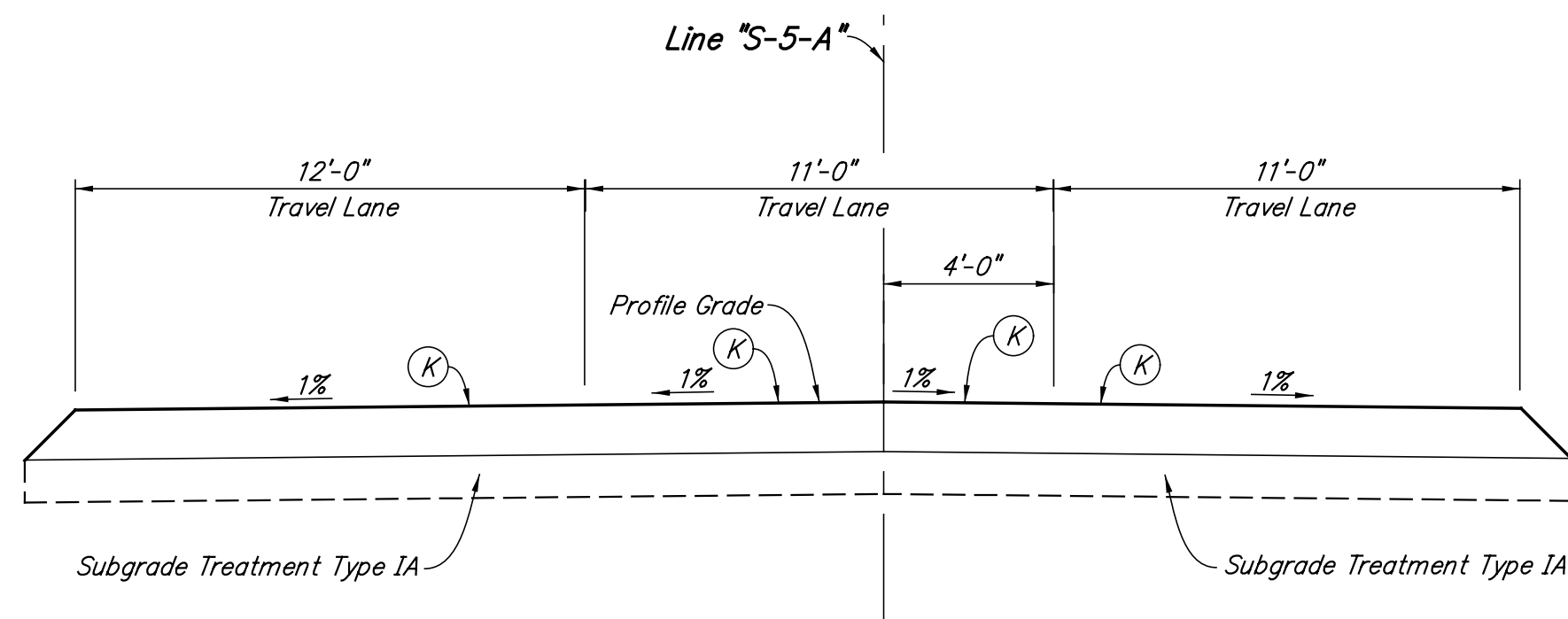
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
LINE "PR-A"

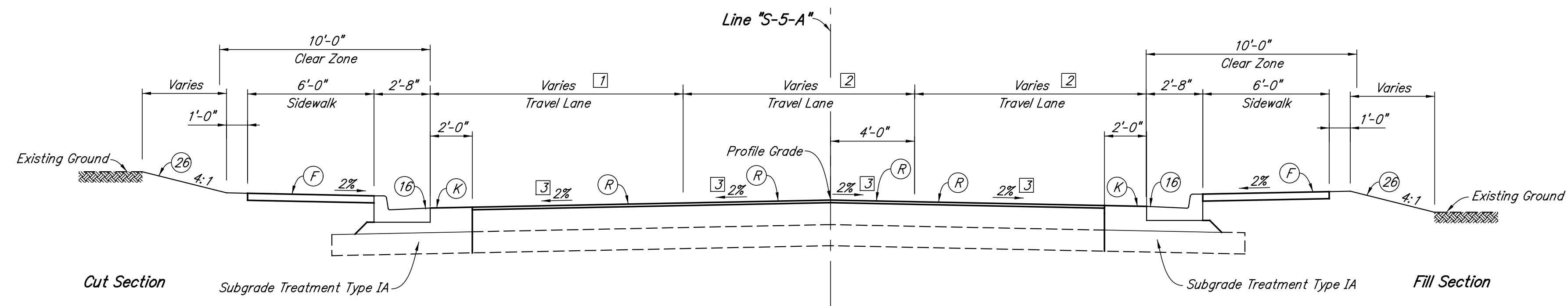
HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	DESIGNATION NO.
VERTICAL SCALE	1005151
NONE	
SURVEY BOOK	SHEETS
CONTRACT	4 of 50
R-30743	PROJECT NO.
	1005151



Incidental Construction
Typical Section - Clinton Street
61+44.33 to 61+72.00 Line "S-5-A"



Typical Section - Clinton Street
61+72.00 to 62+90.00 Line "S-5-A"



- 1 Varies from 12'-0" to 10'-3" 62+90.00 to 63+55.00 "S-5-A"
2 Varies from 11'-0" to 10'-5" 62+90.00 to 63+55.00 "S-5-A"
3 Varies 1% to 2% from 62+90.00 to 63+55.00 "S-5-A"

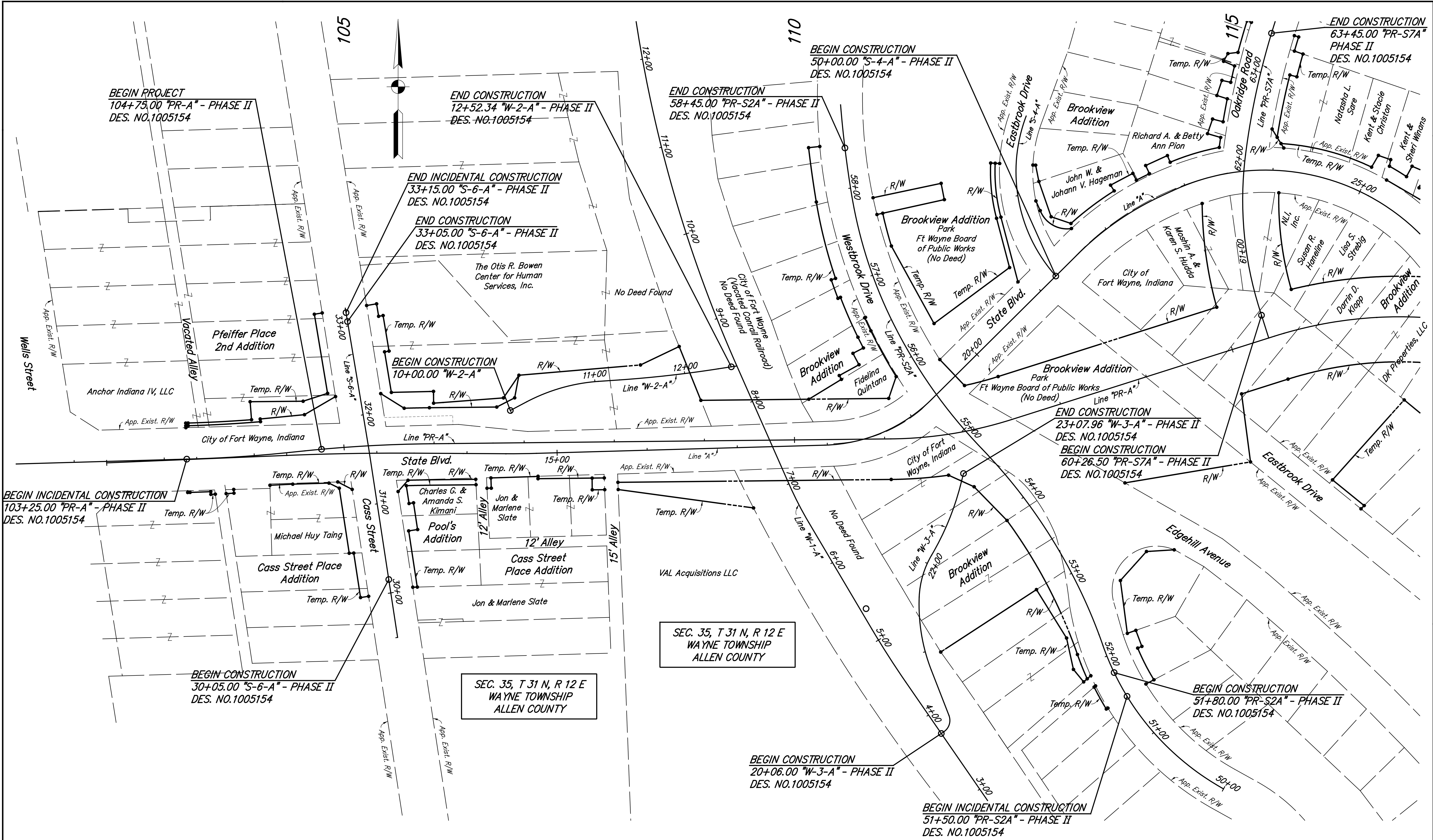
Incidental Construction
Typical Section - Clinton Street
62+90.00 to 63+55.00 Line "S-5-A"

LEGEND			
(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on xxx #/Syd. QC/QA-HMA, x, xx, Base, xx	(F) 140 #/Syd. HMA Surface, Type "A", on 220 #/Syd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 53, Base	(13) Concrete Curb	(22) Concrete Center Curb, Type "D"
(F) Concrete Sidewalk, 4"	(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx	(14) Integral Concrete Curb	(26) Sodding, Nursery
		(15) Curb and Gutter, Concrete, Modified	(28) Mulched Seeding, Type "U"
		(16) Curb and Gutter, C, Concrete	(69) Retaining Wall

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

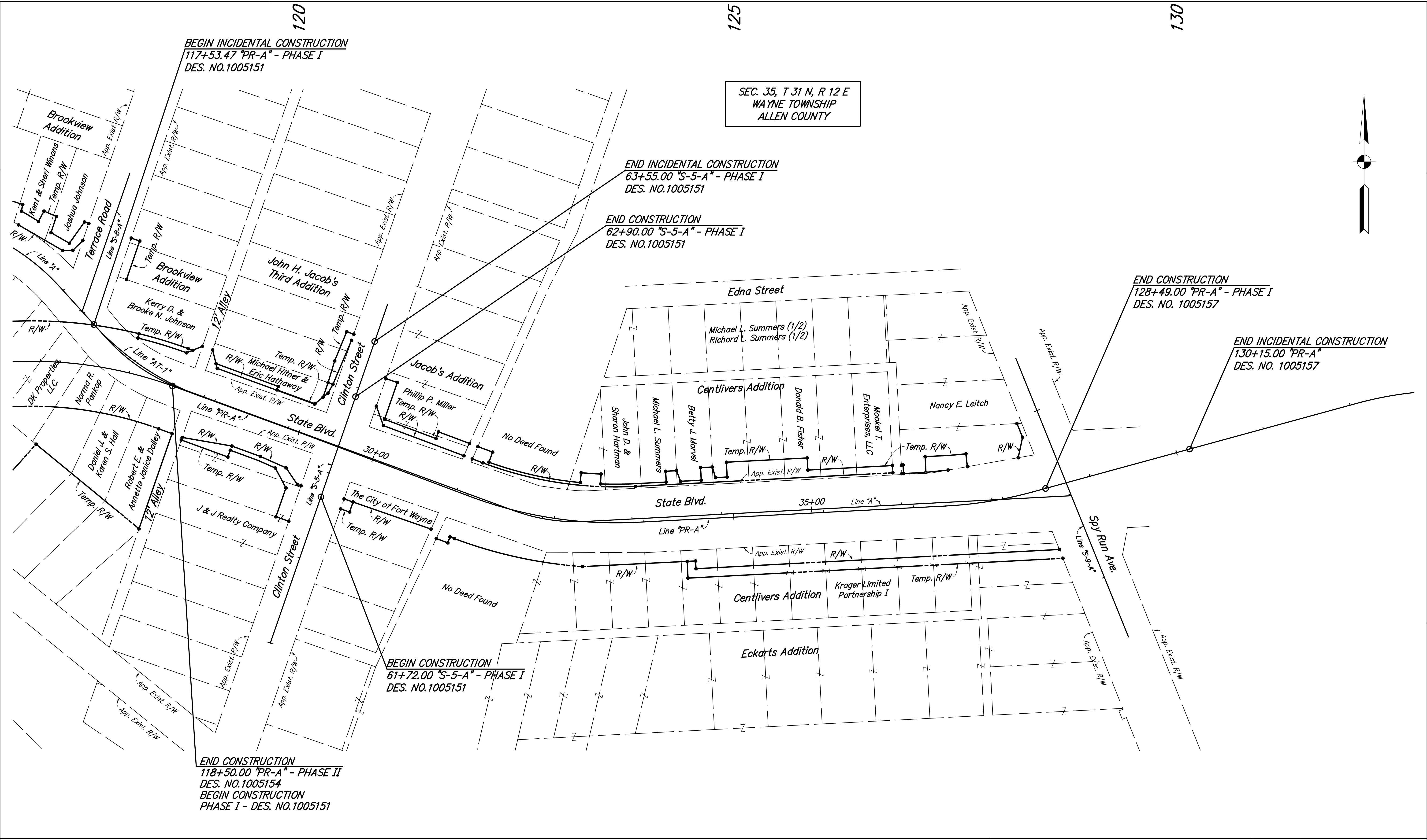
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS	
LINE "S-5-A"	

HORIZONTAL SCALE		BRIDGE FILE	
1/4" = 1'-0"		DESIGNATION NO.	
NONE		1005151	
SURVEY BOOK		SHEETS	
CONTRACT		5 of 50	PROJECT NO.
R-30743			1005151



		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
				1" = 50'			
				VERTICAL SCALE		DESIGNATION NO.	
				NONE		1005151	
PLAT NO. 1				SURVEY BOOK		SHEETS	
				6		of 50	
				CONTRACT		PROJECT NO.	
				R-30743		1005151	

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT			
CHECKED: SMC		CHECKED: SMC			



SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

BEGIN INCIDENTAL CONSTRUCTION
117+53.47 "PR-A" - PHASE I
DES. NO.1005151

END INCIDENTAL CONSTRUCTION
63+55.00 "S-5-A" - PHASE I
DES. NO.1005151

END CONSTRUCTION
62+90.00 "S-5-A" - PHASE I
DES. NO.1005151

END CONSTRUCTION
128+49.00 "PR-A" - PHASE I
DES. NO. 1005157

END INCIDENTAL CONSTRUCTION
130+15.00 "PR-A"
DES. NO. 1005157

BEGIN CONSTRUCTION
61+72.00 "S-5-A" - PHASE I
DES. NO.1005151

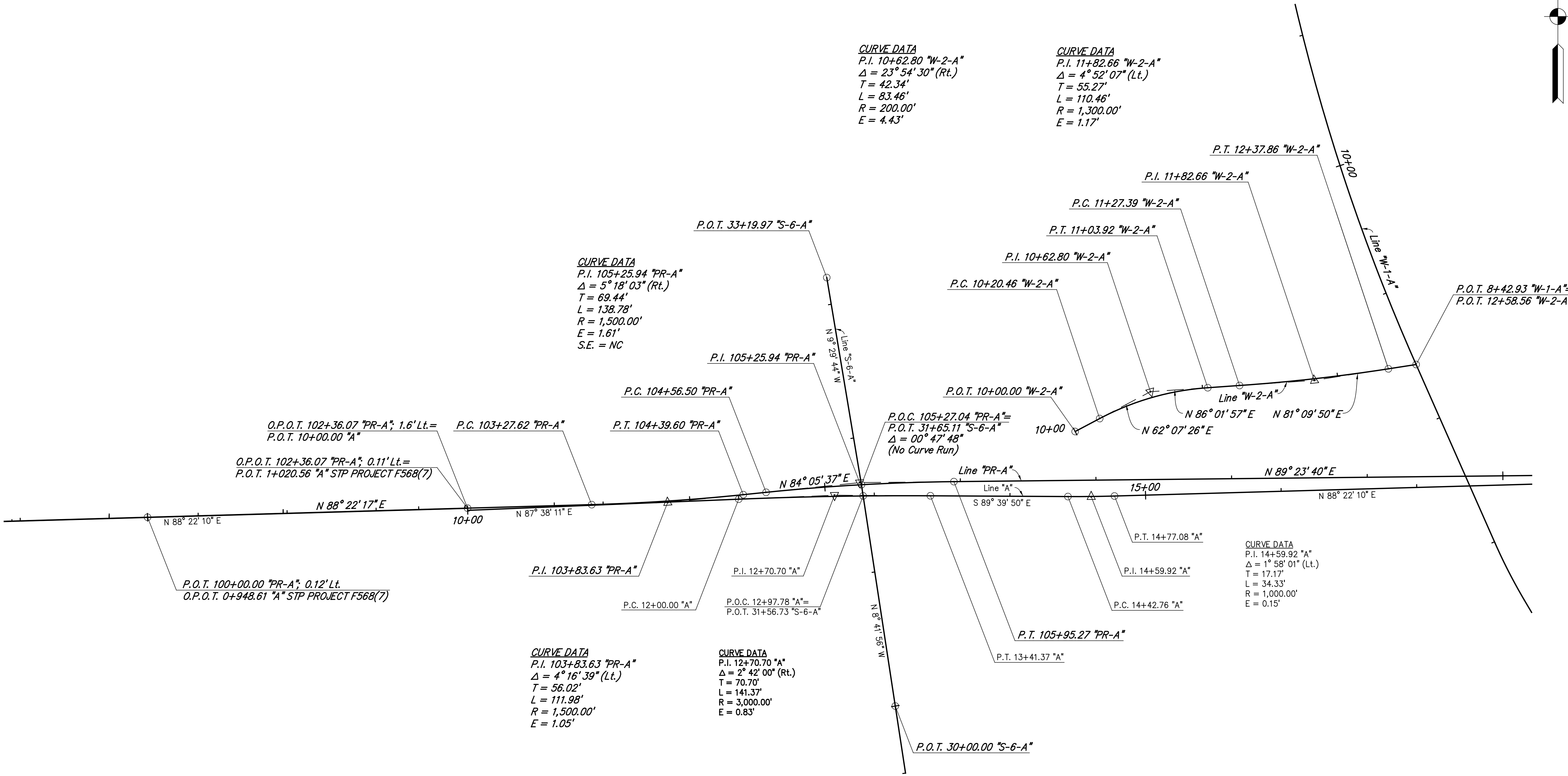
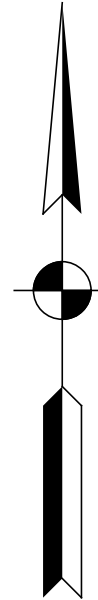
END CONSTRUCTION
118+50.00 "PR-A" - PHASE II
DES. NO.1005154
BEGIN CONSTRUCTION
PHASE I - DES. NO.1005151

RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
				1" = 50'	
DESIGNED: ASU		DRAWN: PJT		VERTICAL SCALE	DESIGNATION NO.
CHECKED: SMC		CHECKED: SMC		NONE	1005151
PLAT NO. 1		PLAT NO. 1		SURVEY BOOK	SHEETS
				7	of 50
				CONTRACT	PROJECT NO.
				R-30743	1005151

100

105

110



CURVE DATA
P.I. 10+62.80 "W-2-A"
 $\Delta = 23^\circ 54' 30''$ (Rt.)
T = 42.34'
L = 83.46'
R = 200.00'
E = 4.43'

CURVE DATA
P.I. 11+82.66 "W-2-A"
 $\Delta = 4^\circ 52' 07''$ (Lt.)
T = 55.27'
L = 110.46'
R = 1,300.00'
E = 1.17'

CURVE DATA
P.I. 105+25.94 "PR-A"
 $\Delta = 5^\circ 18' 03''$ (Rt.)
T = 69.44'
L = 138.78'
R = 1,500.00'
E = 1.61'
S.E. = NC

CURVE DATA
P.I. 14+59.92 "A"
 $\Delta = 1^\circ 58' 01''$ (Lt.)
T = 17.17'
L = 34.33'
R = 1,000.00'
E = 0.15'

CURVE DATA
P.I. 103+83.63 "PR-A"
 $\Delta = 4^\circ 16' 39''$ (Lt.)
T = 56.02'
L = 111.98'
R = 1,500.00'
E = 1.05'

CURVE DATA
P.I. 12+70.70 "A"
 $\Delta = 2^\circ 42' 00''$ (Rt.)
T = 70.70'
L = 141.37'
R = 3,000.00'
E = 0.83'

Notes:
See Sheet XX for Line "W-1-A" Geometrics.

		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
				1" = 40'			
				VERTICAL SCALE		DESIGNATION NO.	
				NONE		1005151	
		GEOMETRIC TIE-UP		SURVEY BOOK		SHEETS	
						8 of 50	
				CONTRACT		PROJECT NO.	
				R-30743		1005151	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE			
DESIGNED: ASU		DRAWN: PJT					
CHECKED: SMC		CHECKED: SMC					

CURVE DATA
P.I. 18+66.60 "A"
Δ = 43° 10' 30" (Lt.)
T = 69.24'
L = 131.87'
R = 175.00'
E = 13.20'

CURVE DATA
P.I. 22+14.84 "A"
Δ = 19° 44' 24" (Rt.)
T = 55.68'
L = 110.25'
R = 320.00'
E = 4.81'

CURVE DATA
P.I. 24+64.47 "A"
Δ = 72° 48' 36" (Rt.)
T = 179.19'
L = 308.80'
R = 243.00'
E = 58.92'

CURVE DATA
P.I. 27+23.73 "A"
Δ = 27° 36' 02" (Lt.)
T = 51.58'
L = 101.16'
R = 210.00'
E = 6.24'

CURVE DATA
P.I. 51+05.62 "S-4-A"
Δ = 62° 31' 02" (Rt.)
T = 105.62'
L = 189.86'
R = 174.00'
E = 29.55'

CURVE DATA
P.I. 51+05.37 "S-7-A"
Δ = 14° 43' 34" (Rt.)
T = 105.37'
L = 209.58'
R = 815.43'
E = 6.78'

CURVE DATA
P.I. 111+75.59 "PR-A"
Δ = 16° 59' 53" (Lt.)
T = 68.74'
L = 136.47'
R = 460.00'
E = 5.11'

CURVE DATA
P.I. 117+06.31 "PR-A"
Δ = 37° 44' 51" (Rt.)
T = 157.26'
L = 303.06'
R = 460.00'
E = 26.14'

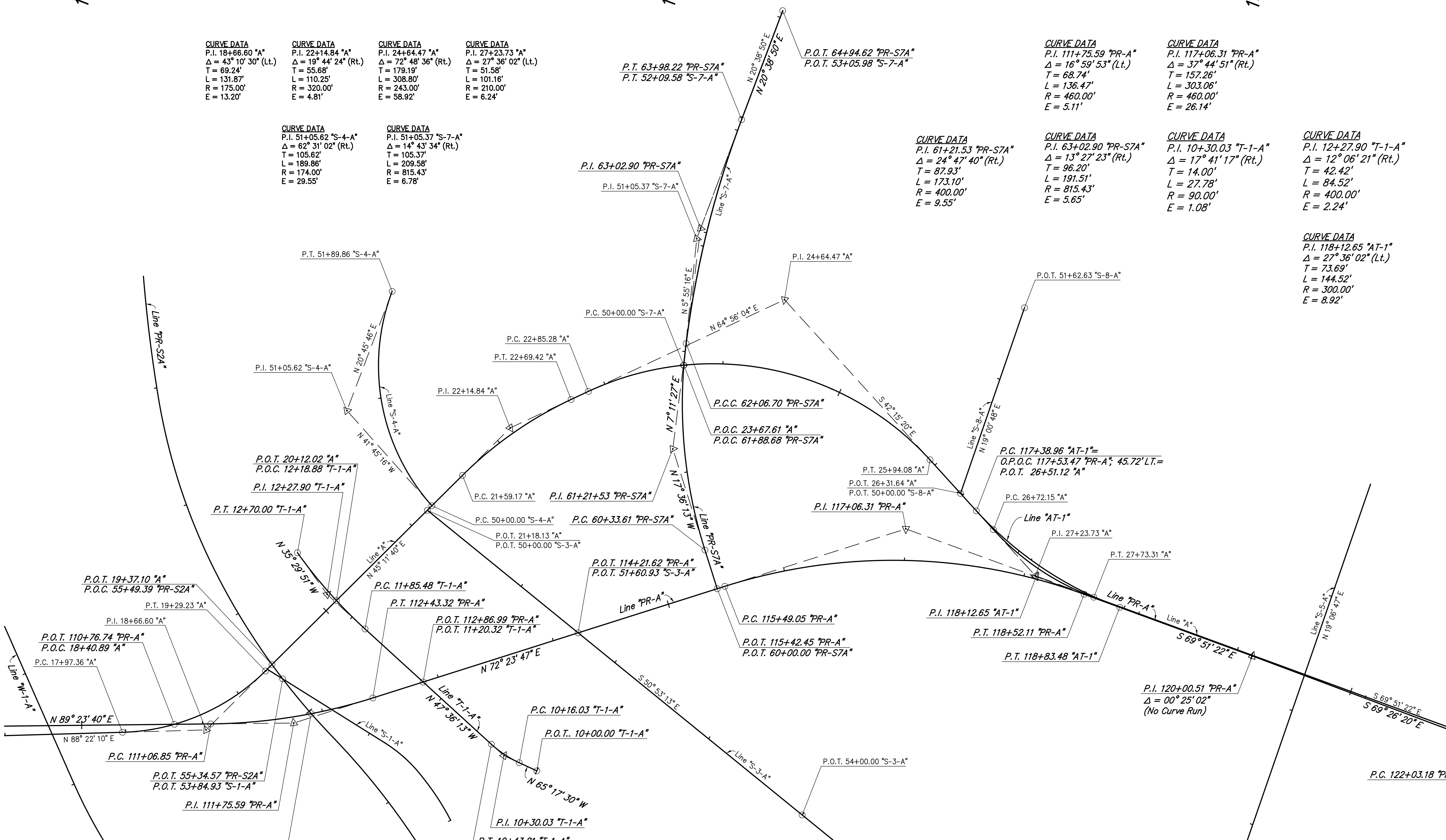
CURVE DATA
P.I. 61+21.53 "PR-S7A"
Δ = 24° 47' 40" (Rt.)
T = 87.93'
L = 173.10'
R = 400.00'
E = 9.55'

CURVE DATA
P.I. 63+02.90 "PR-S7A"
Δ = 13° 27' 23" (Rt.)
T = 96.20'
L = 191.51'
R = 815.43'
E = 5.65'

CURVE DATA
P.I. 10+30.03 "T-1-A"
Δ = 17° 41' 17" (Rt.)
T = 14.00'
L = 27.78'
R = 90.00'
E = 1.08'

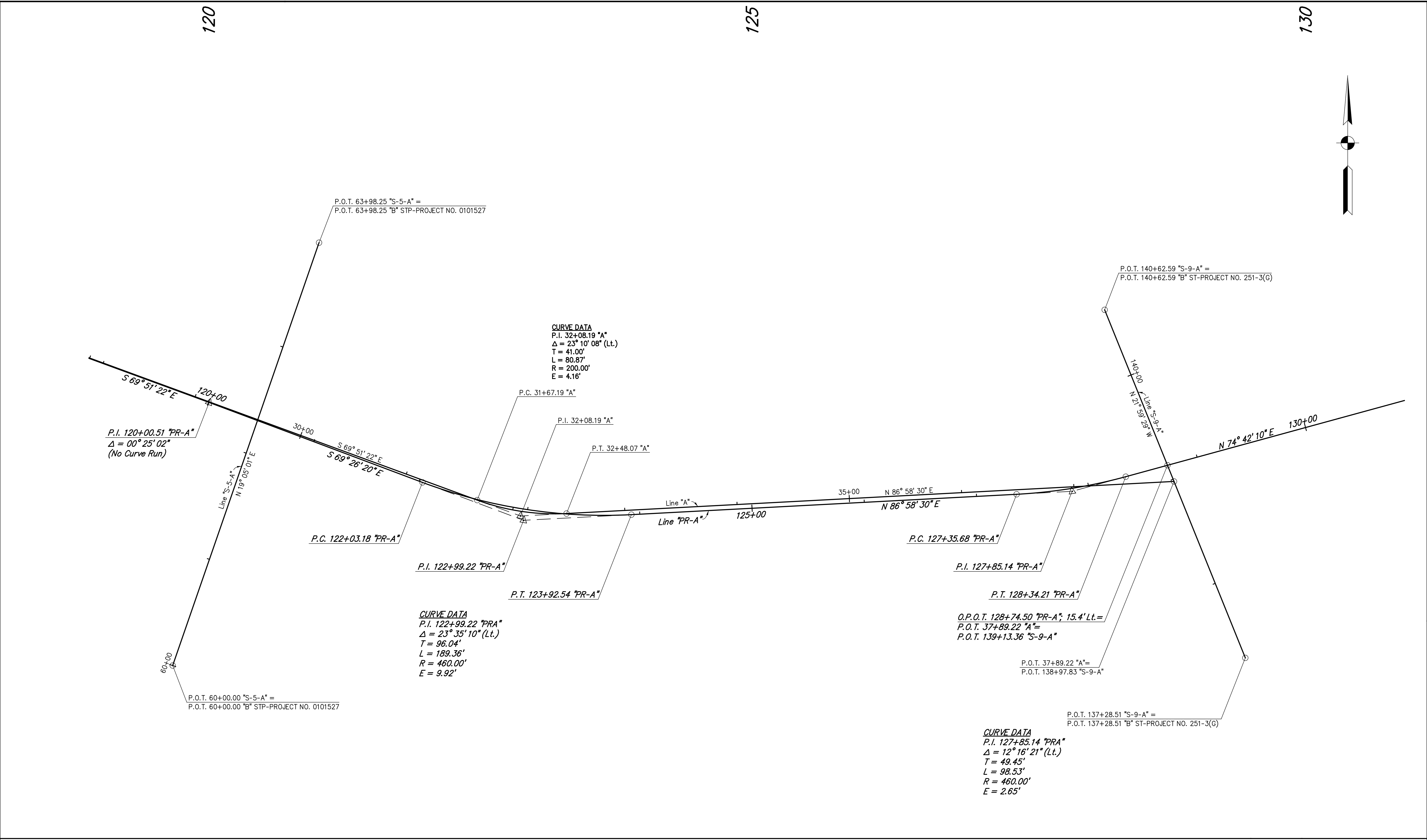
CURVE DATA
P.I. 12+27.90 "T-1-A"
Δ = 12° 06' 21" (Rt.)
T = 42.42'
L = 84.52'
R = 400.00'
E = 2.24'

CURVE DATA
P.I. 118+12.65 "AT-1"
Δ = 27° 36' 02" (Lt.)
T = 73.69'
L = 144.52'
R = 300.00'
E = 8.92'

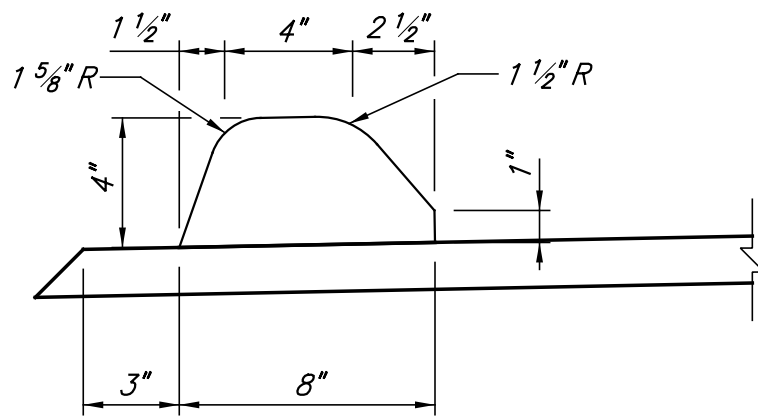


Notes:
See Sheets XX for Line "S-1-A", "S-2-A", "S-5-A", "W-1-A", "PR-S2A" Geometrics.

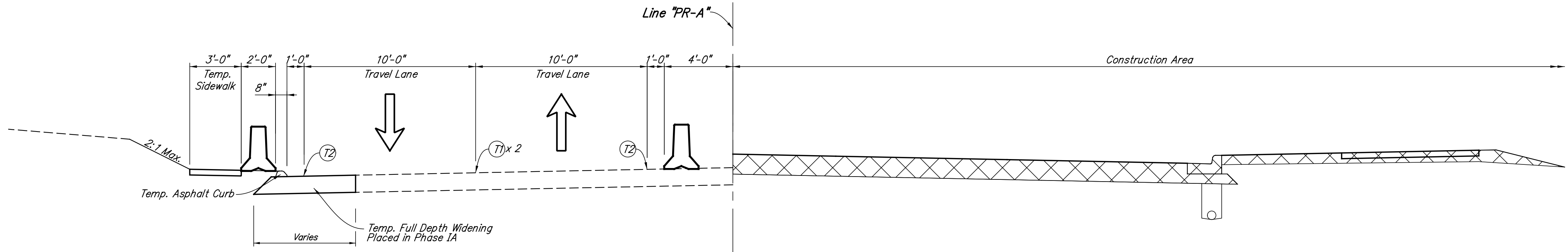
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						1" = 40'					
		DESIGN ENGINEER		DATE		VERTICAL SCALE		DESIGNATION NO.			
						NONE		1005151			
		DESIGNED: ASU		DRAWN: PJT		GEOMETRIC TIE-UP		SURVEY BOOK		SHEETS	
								9 of 50			
CHECKED: SMC		CHECKED: SMC				CONTRACT		PROJECT NO.			
						R-30743		1005151			



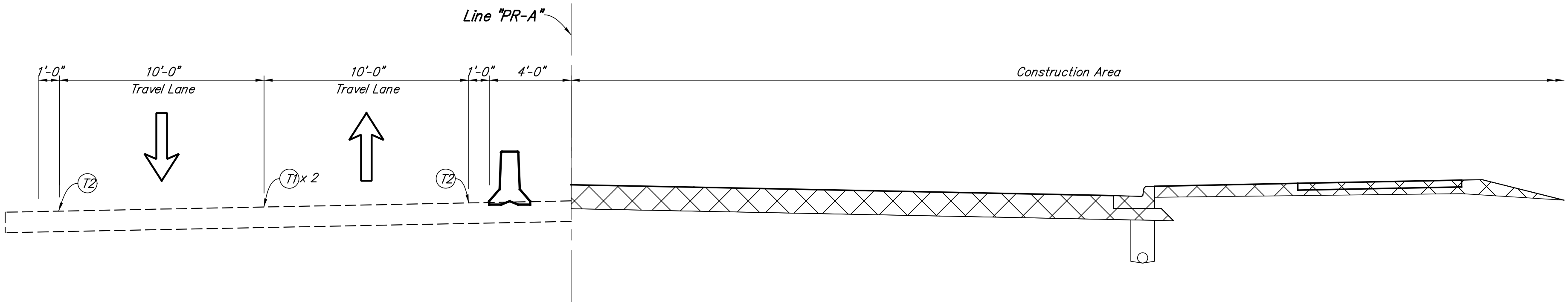
		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						1" = 40'			
		DESIGN ENGINEER		DATE		VERTICAL SCALE		DESIGNATION NO.	
		NONE				NONE		1005151	
		DESIGNED: ASU		DRAWN: PJT		SURVEY BOOK		SHEETS	
		CHECKED: SMC		CHECKED: SMC		10 of 50		PROJECT NO.	
						CONTRACT		1005151	
						R-30743			



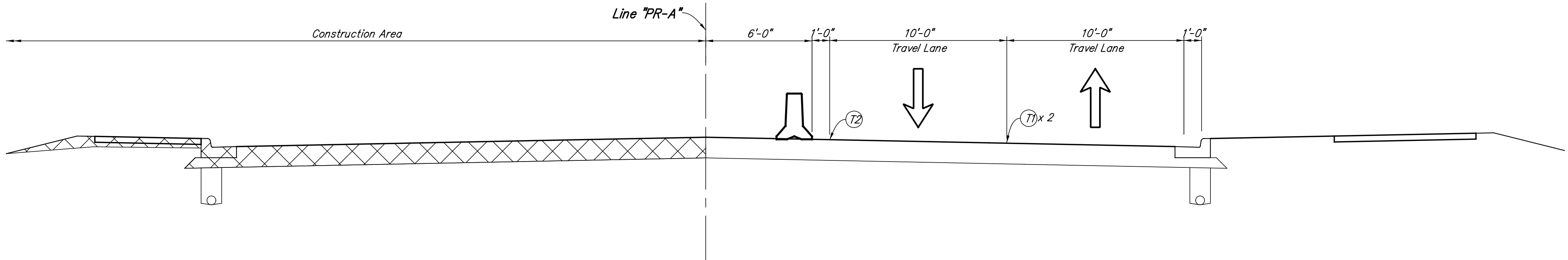
ASPHALT CURB (MOUNTABLE) DETAIL



PHASE I





PHASE I



PHASE II

Notes: HMA Surface to be Placed Following Phase II Under Live Traffic

 Temporary Concrete Barrier

 Standard Drum

LEGEND

(T1) Temporary Pavement Marking, Solid, Yellow, 4 in.

(T2) Temporary Pavement Marking, Solid, White, 4 in.

Construction Area

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED: ASU

DRAWN: CAK

CHECKED: SMC

CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS

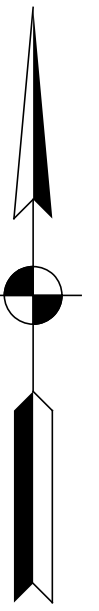
HORIZONTAL SCALE
1/4" = 1'-0"

VERTICAL SCALE
NONE

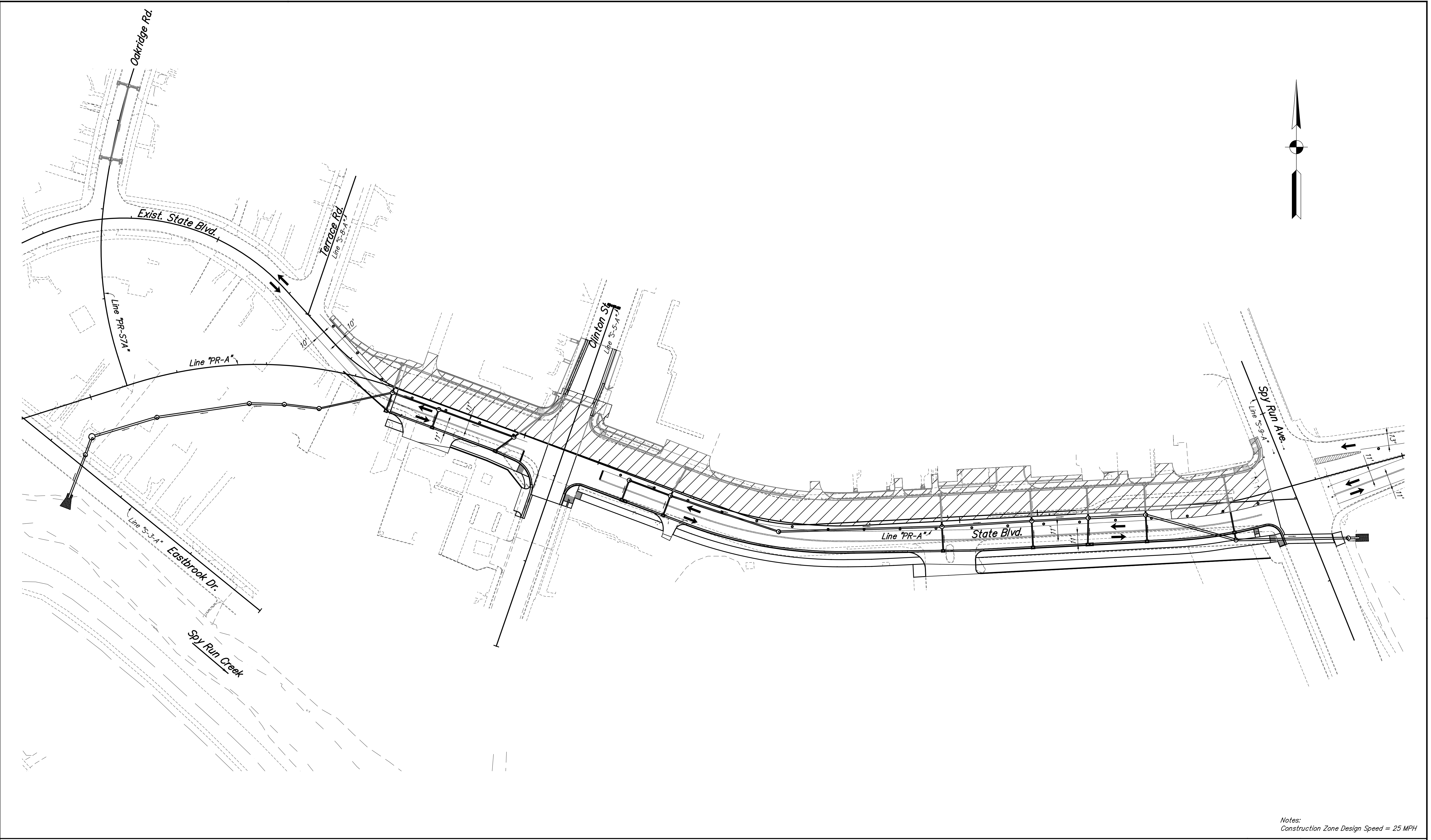
SURVEY BOOK
CONTRACT R-30743

BRIDGE FILE
DESIGNATION NO. 1005151

SHEETS
12 of 50
PROJECT NO. 1005151

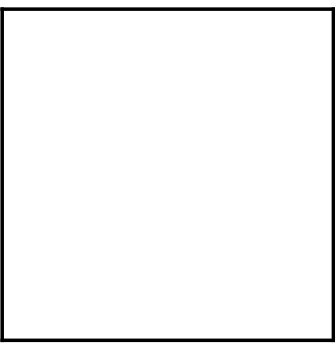


HORIZONTAL SCALE	BRIDGE FILE		
1" = 50'			
VERTICAL SCALE	DESIGNATION NO.		
NONE	1005151		
SURVEY BOOK	SHEETS		
	13	of	50
CONTRACT	PROJECT NO.		
R-30743	1005151		



Notes:
Construction Zone Design Speed = 25 MPH

LEGEND			
----- Temporary Barrier	Construction Area	Type "B" Construction Warning Light	T1 Temporary Pavement Marking, Solid, Yellow, 4 in.
--- Detour Route	Temp. Pavement Area	— Type III Barricade (See Callout for Type)	T2 Temporary Pavement Marking, Solid, White, 4 in.
Direction of Traffic	Standard Drum	Construction Sign w/ Light Warning	T3 Temporary Pavement Marking, Solid, White, 24 in.



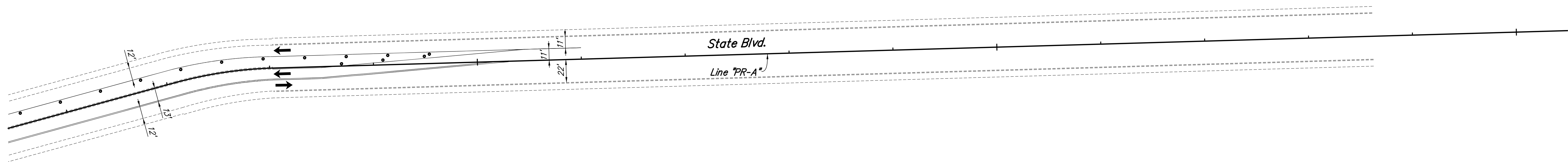
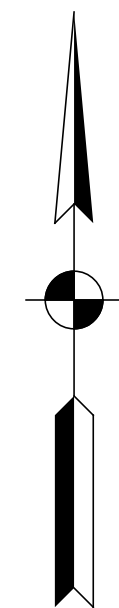
RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: PJT
CHECKED: SMC	CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC PHASE II	

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005151	
SURVEY BOOK	SHEETS	
	14 of 50	
CONTRACT R-30743	PROJECT NO. 1005151	










135

140



Notes:
Construction Zone Design Speed = 25 MPH

LEGEND

- | | | | |
|--|---|---|--|
| ----- Temporary Barrier |  Construction Area |  Type "B" Construction Warning Light |  Temporary Pavement Marking, Solid, Yellow, 4 in. |
| --- Detour Route |  Temp. Pavement Area | — Type III Barricade (See Callout for Type) |  Temporary Pavement Marking, Solid, White, 4 in. |
|  Direction of Traffic |  Standard Drum |  Construction Sign w/ Light Warning |  Temporary Pavement Marking, Solid, White, 24 in. |

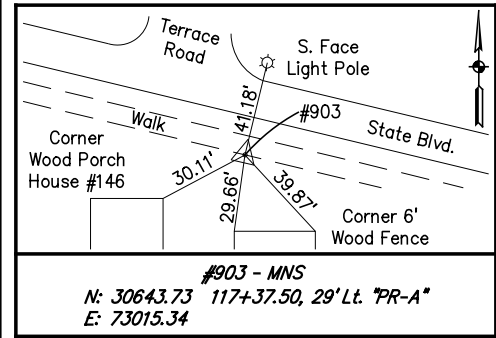
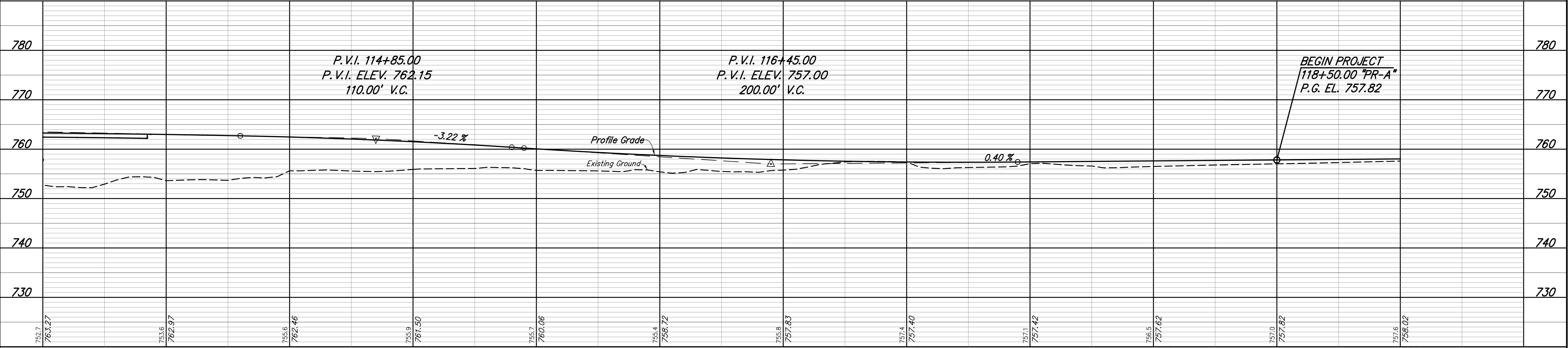
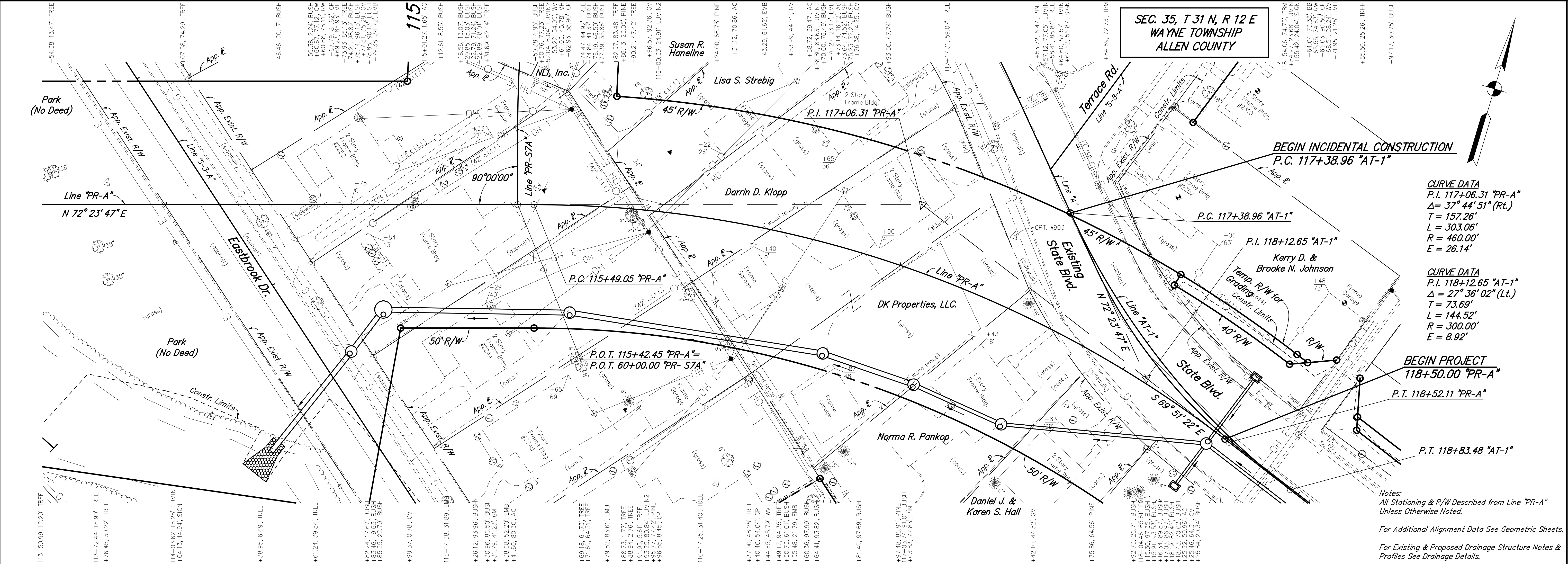
RECOMMENDED
FOR APPROVAL _____
DESIGN ENGINEER _____ DATE _____

DESIGNED: _____ ASU	DRAWN: _____ PJT
CHECKED: _____ SMC	CHECKED: _____ SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE II**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005151	
SURVEY BOOK	SHEETS	
	15	of 50
CONTRACT R-30743	PROJECT NO. 1005151	

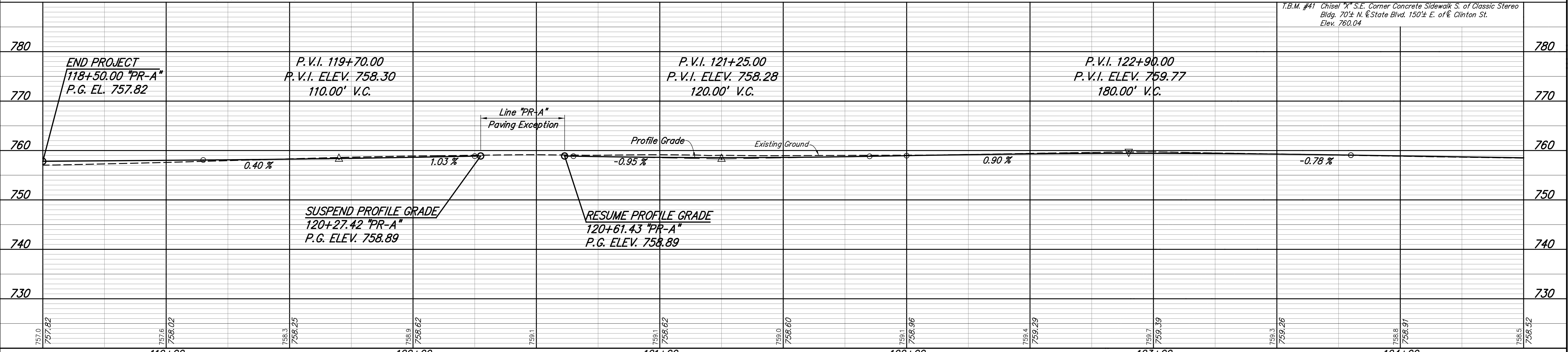
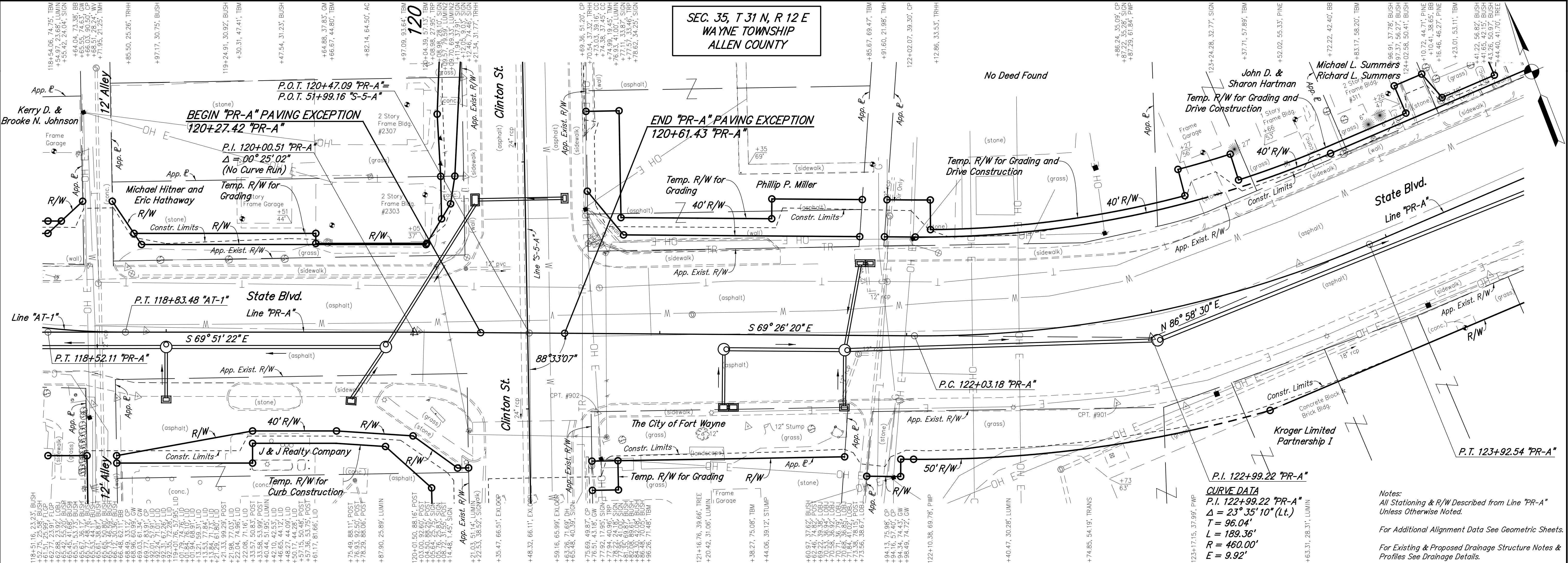


RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005151
SURVEY BOOK	SHEETS 16 of 50
CONTRACT R-30743	PROJECT NO. 1005151

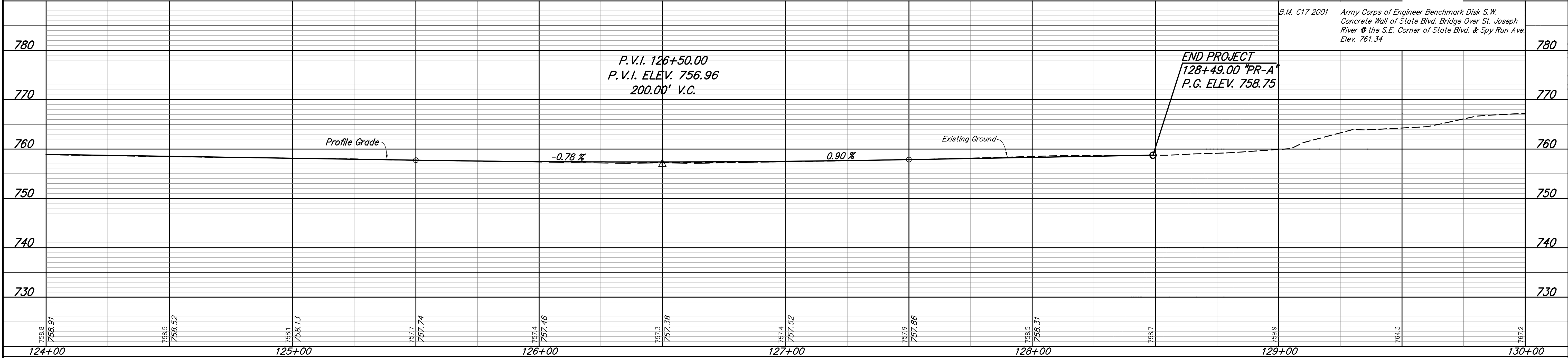
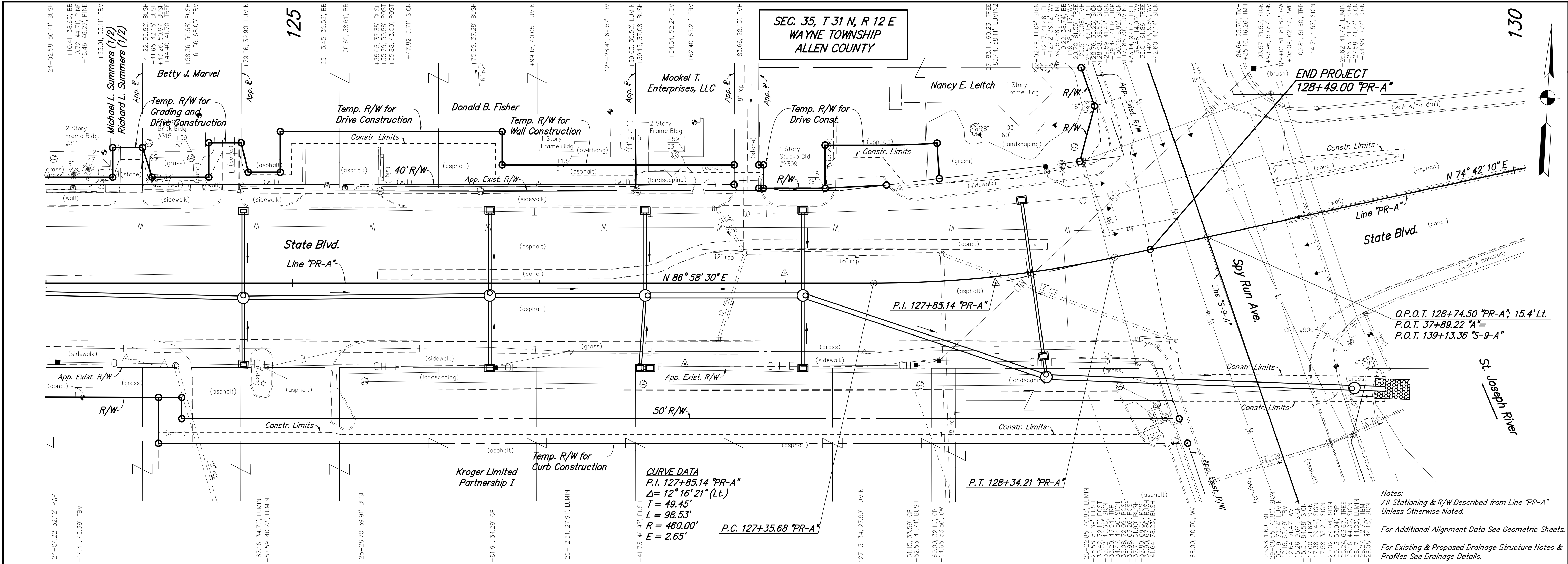


RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED:	ASU	DRAWN:	PJT		
CHECKED:	SMC	CHECKED:	SMC		

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-A"

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'			
VERTICAL SCALE		DESIGNATION NO.	
1" = 10'			1005151
SURVEY BOOK		SHEETS	
		17	of 50
CONTRACT		PROJECT NO.	
R-30743			1005151



N. 30445.71
E. 74166.14

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER DATE

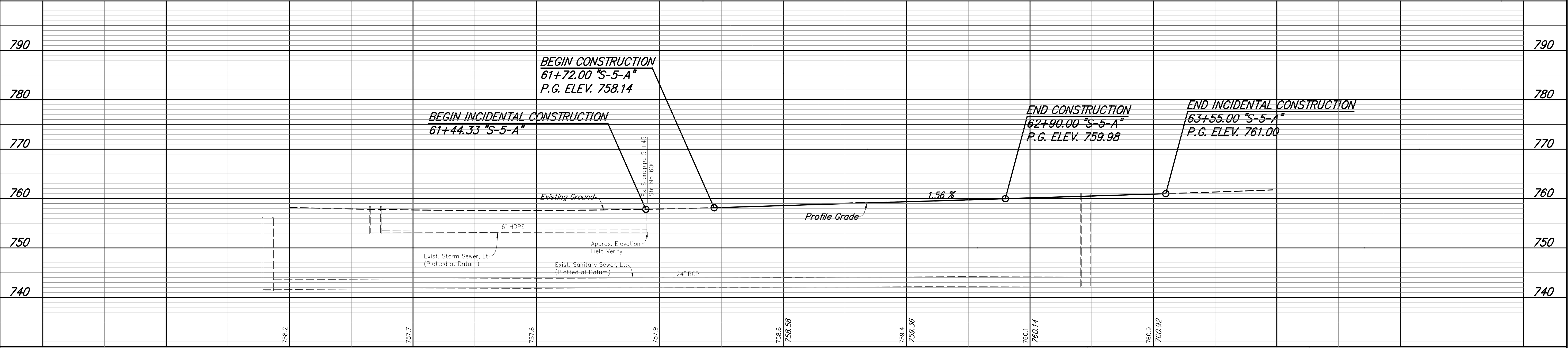
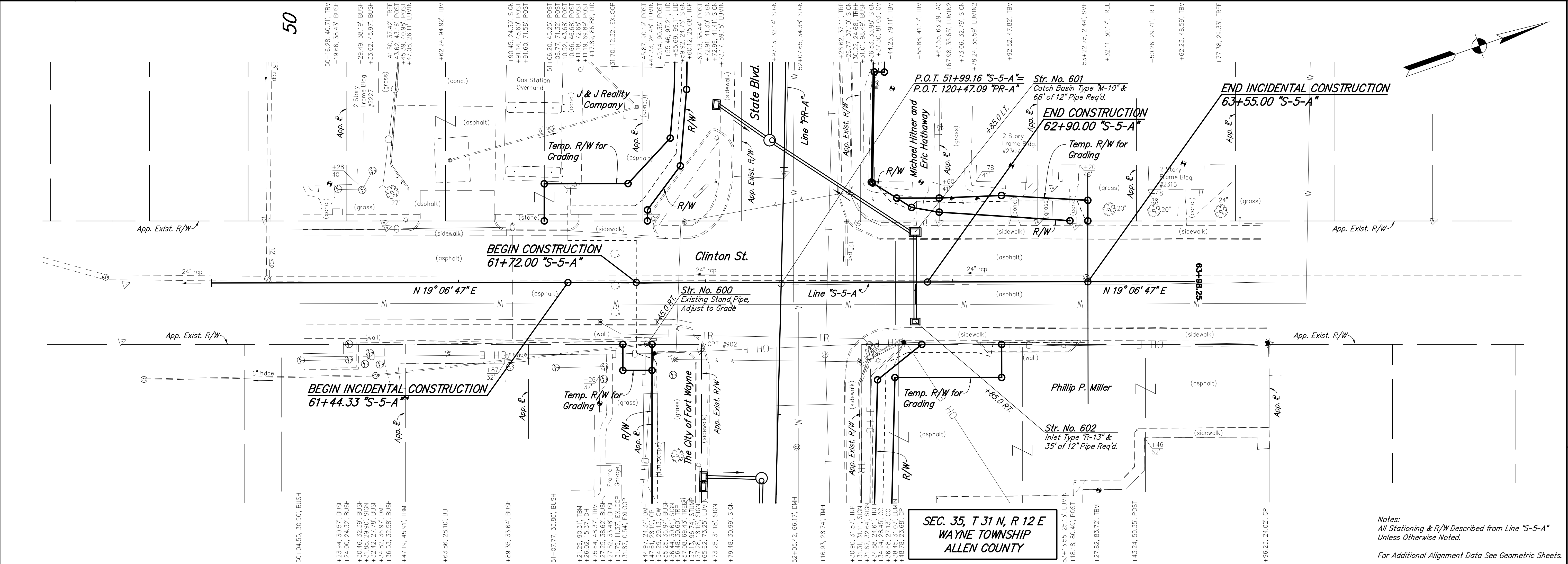
DESIGNED: ASU DRAWN: PJT

CHECKED: SMC CHECKED: SMC

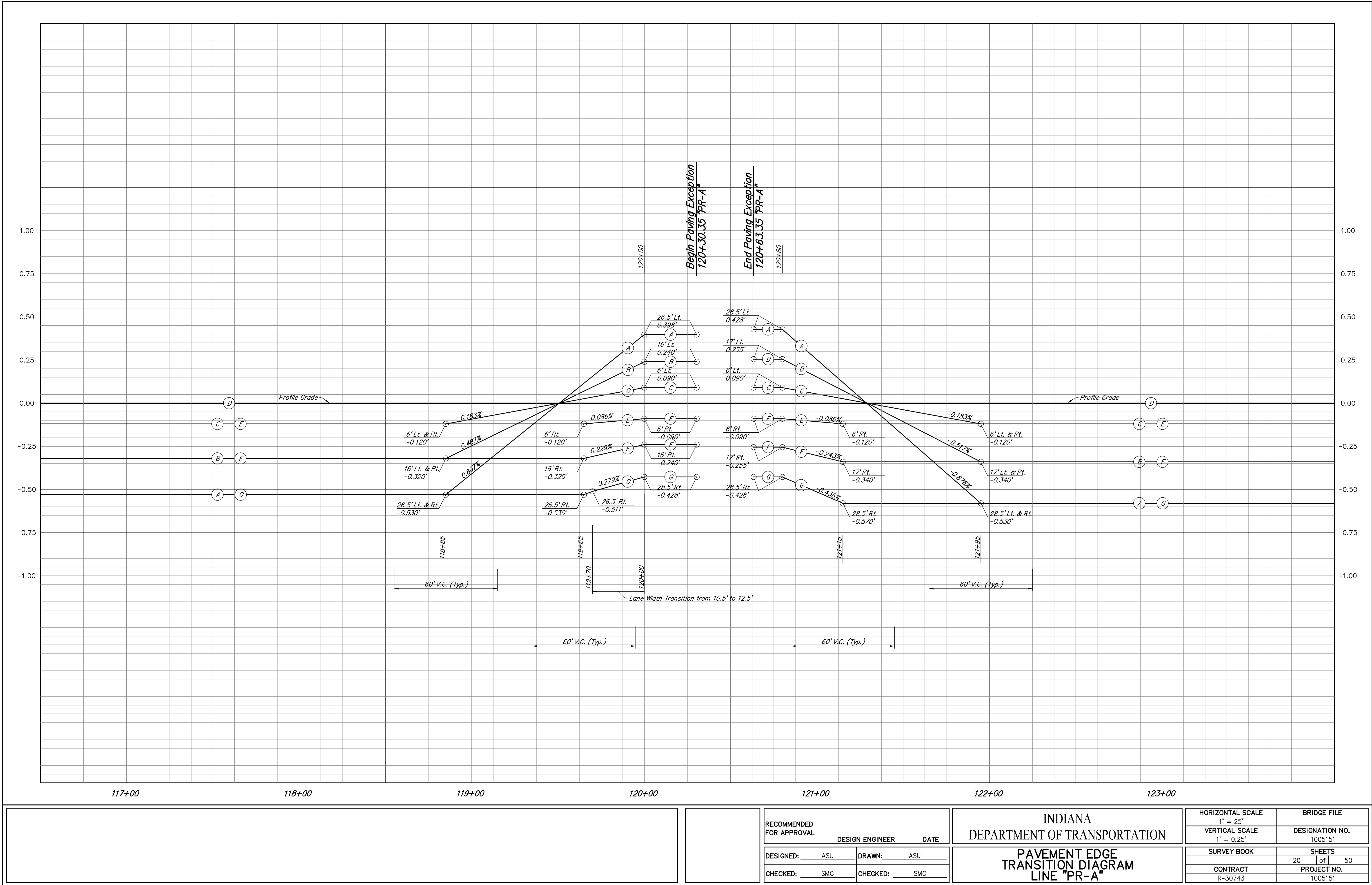
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "PR-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005151
SURVEY BOOK R-30743	SHEETS 18 of 50
CONTRACT R-30743	PROJECT NO. 1005151



		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						1" = 20'			
		DESIGN ENGINEER		DATE		VERTICAL SCALE		DESIGNATION NO.	
						1" = 10'		1005151	
		DESIGNED: ASU		DRAWN: PJT		SURVEY BOOK		SHEETS	
		CHECKED: SMC		CHECKED: SMC		CONTRACT R-30743		19 of 50	
				PLAN & PROFILE LINE "S-5-A"		PROJECT NO. 1005151			



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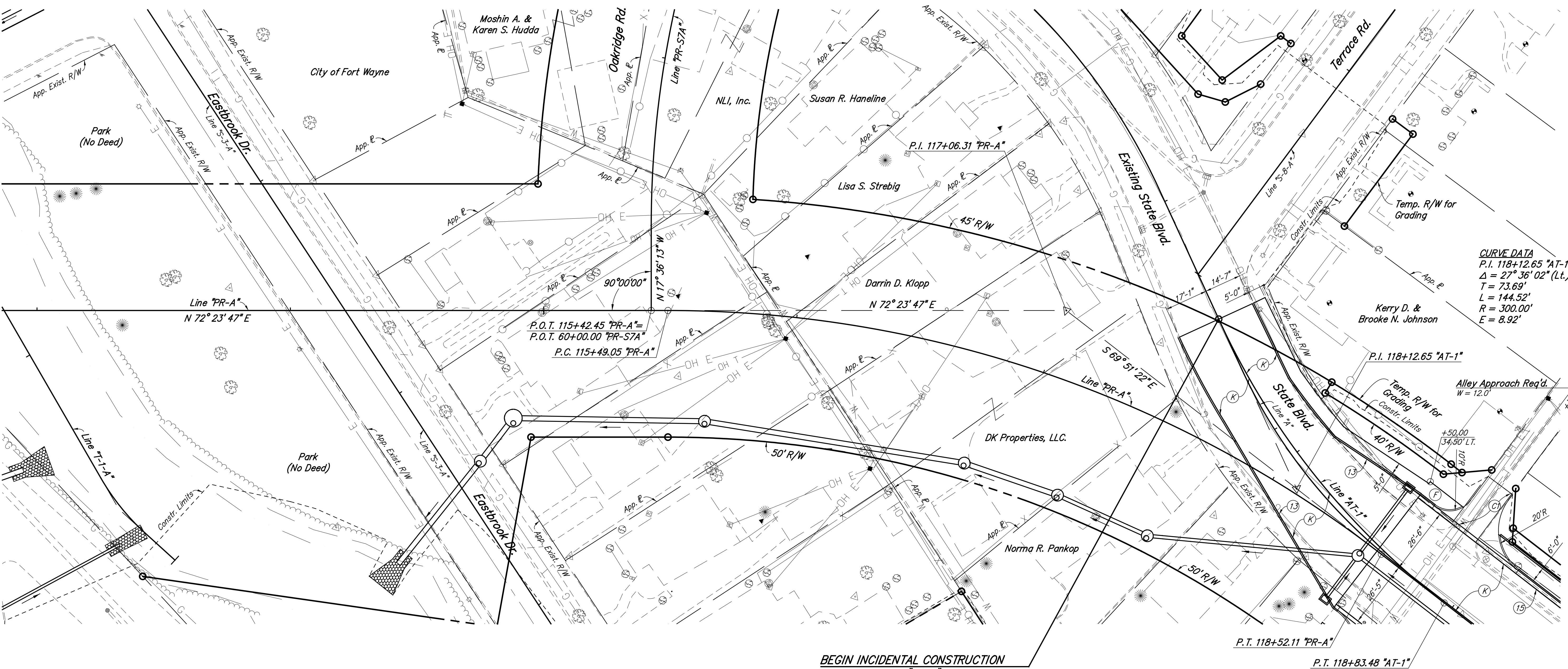
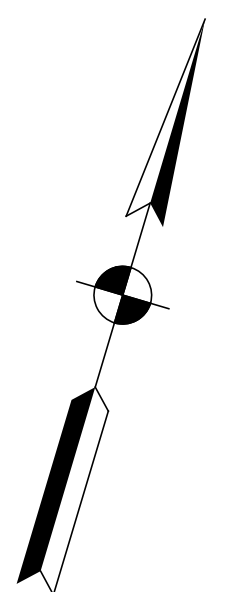
RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: ASU	DRAWN: ASU	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	PAVEMENT EDGE TRANSITION DIAGRAM LINE "PR-A"	

HORIZONTAL SCALE 1" = 25'	BRIDGE FILE	
VERTICAL SCALE 1" = 0.25'	DESIGNATION NO. 1005151	
SURVEY BOOK	SHEETS	
	20	of 50
CONTRACT R-30743	PROJECT NO. 1005151	

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

CURVE DATA
P.I. 117+06.31 "PR-A"
 $\Delta = 37^\circ 44' 51''$ (Rt.)
 $T = 157.26'$
 $L = 303.06'$
 $R = 460.00'$
 $E = 26.14'$



CURVE DATA
P.I. 118+12.65 "AT-1"
 $\Delta = 27^\circ 36' 02''$ (Lt.)
 $T = 73.69'$
 $L = 144.52'$
 $R = 300.00'$
 $E = 8.92'$

BEGIN INCIDENTAL CONSTRUCTION
P.C. 117+38.96 "AT-1"=
O.P.O.C. 117+53.47 "PR-A"; 45.72' Lt.=
P.O.T. 26+51.12 "A"

Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structure Notes See Drainage Detail Sheets.

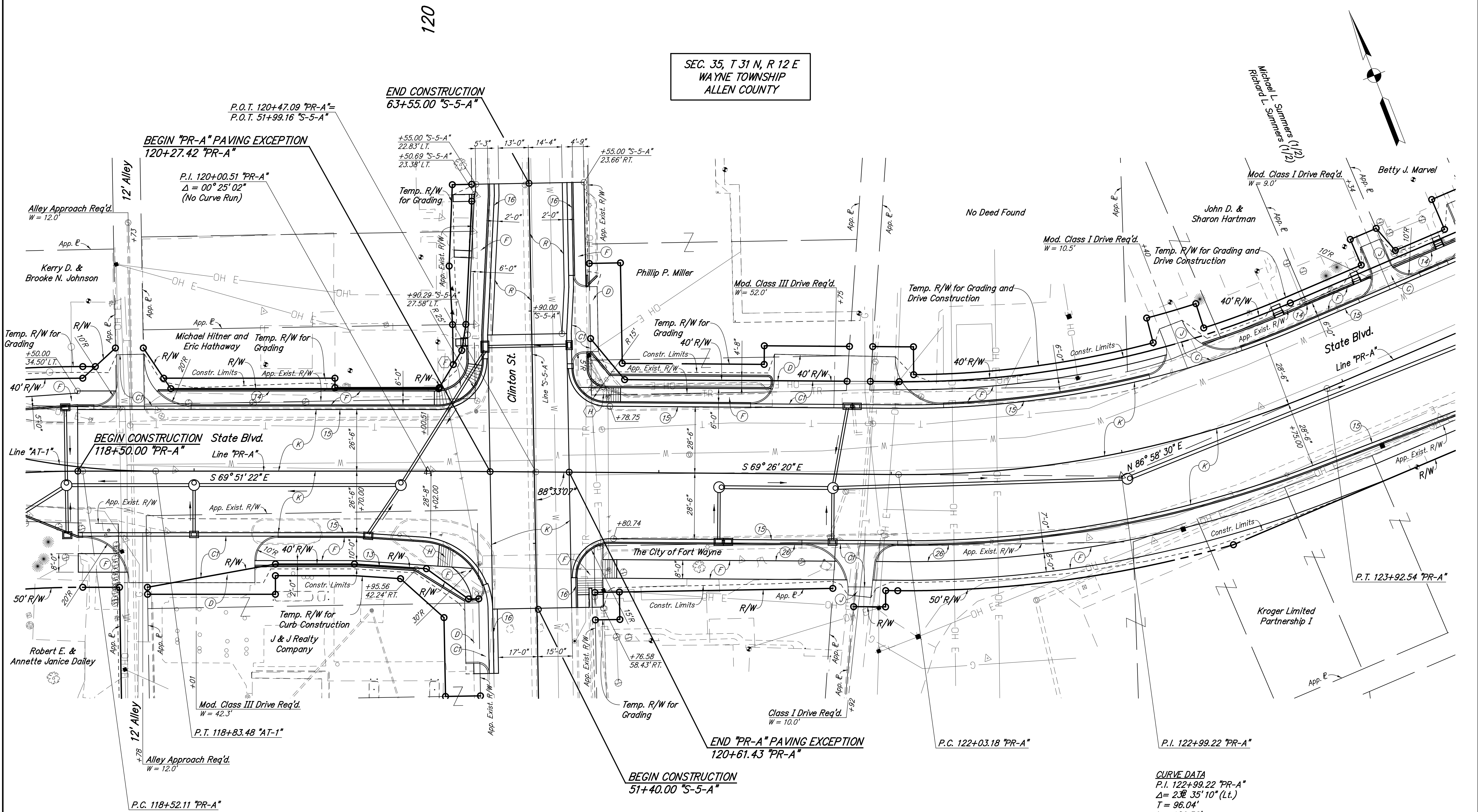
LEGEND			
(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall
	(J) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005151
SURVEY BOOK R-30743	SHEETS 21 of 50
CONTRACT R-30743	PROJECT NO. 1005151

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



CURVE DATA
P.I. 122+99.22 "PR-A"
Δ = 23° 35' 10" (Lt.)
T = 96.04'
L = 189.36'
R = 460.00'
E = 9.92'

Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structure Notes See Drainage Detail Sheets.

LEGEND	
(C) PCCP for Approaches, 6"	(K) Full Depth Pavement (See Typical Sections)
(C1) PCCP for Approaches, 9"	(13) Concrete Curb
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(14) Integral Concrete Curb
(F) Concrete Sidewalk, 4"	(15) Curb and Gutter, Concrete, Modified
(F1) Shared Use Path Pavement (See Typical Sections)	(22) Concrete Center Curb, Type "D"
(J) Compacted Aggregate, No. 53	(26) Sodding, Nursery
	(28) Mulched Seeding, Type "U"
	(69) Retaining Wall
	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
	Center Curb Removal Limits

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: PJT
CHECKED: SMC	CHECKED: SMC

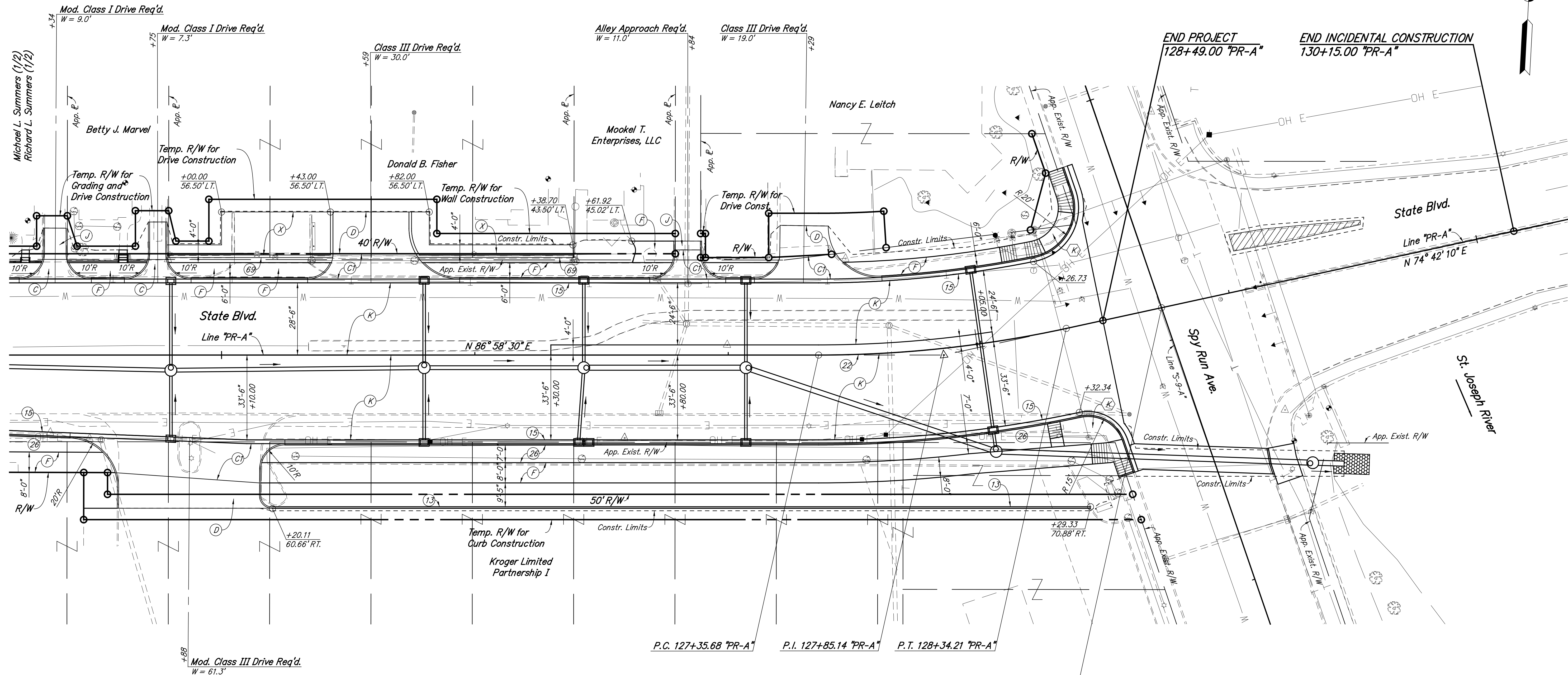
INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005151
SURVEY BOOK	SHEETS
CONTRACT R-30743	22 of 50 PROJECT NO. 1005151

125

130

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



CURVE DATA
P.I. 127+85.14 "PR-A"
 $\Delta = 12^\circ 16' 21"$ (Lt.)
 $T = 49.45'$
 $L = 98.53'$
 $R = 460.00'$
 $E = 2.65'$

O.P.O.T. 128+74.50 "PR-A", 15.4' Lt.
P.O.T. 37+89.22 "A"
P.O.T. 139+13.36 "S-9-A"

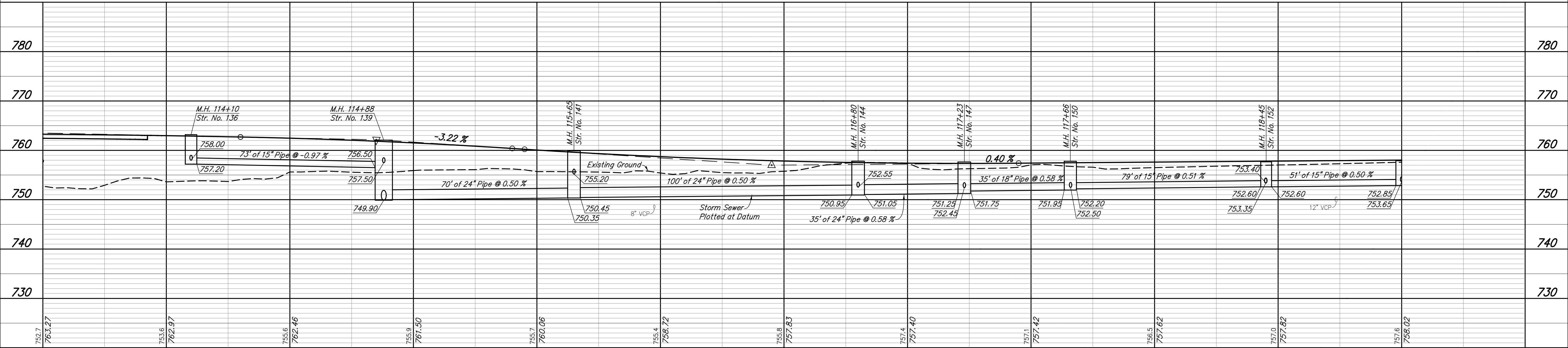
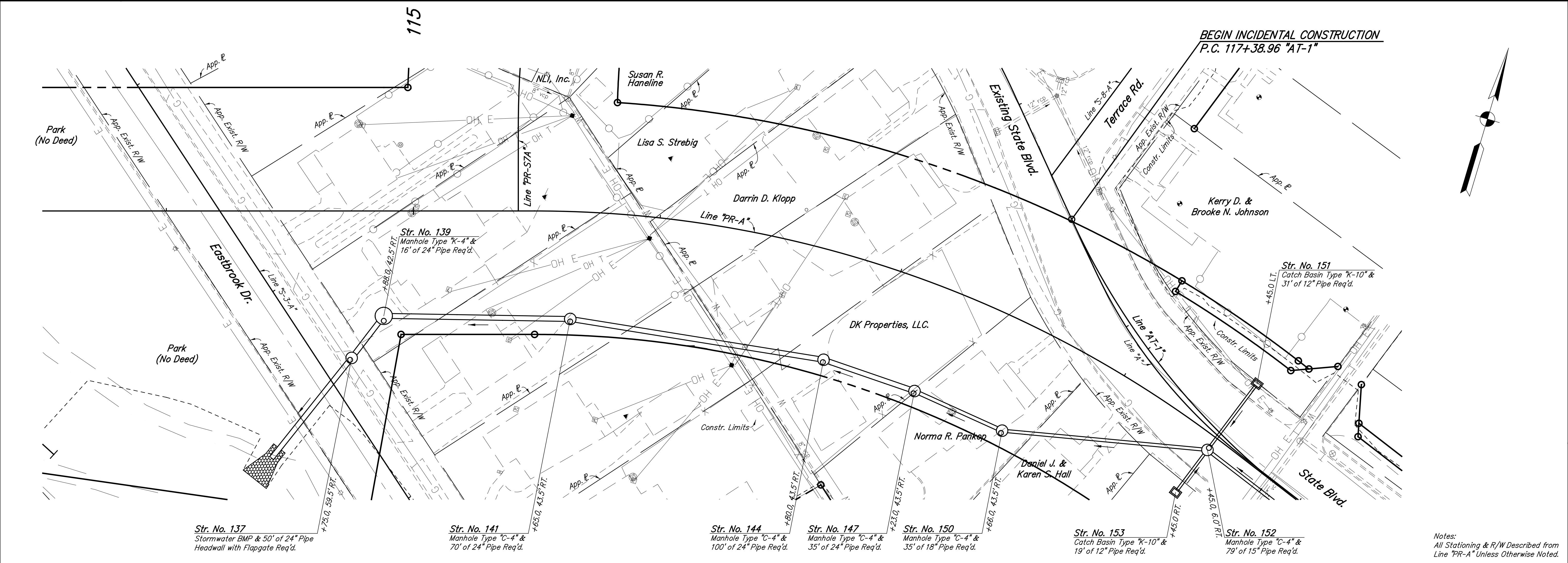
Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structure Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"		(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Shared Use Path Pavement (See Typical Sections)		(14) Integral Concrete Curb	(69) Retaining Wall
	(J) Compacted Aggregate, No. 53		(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
			(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

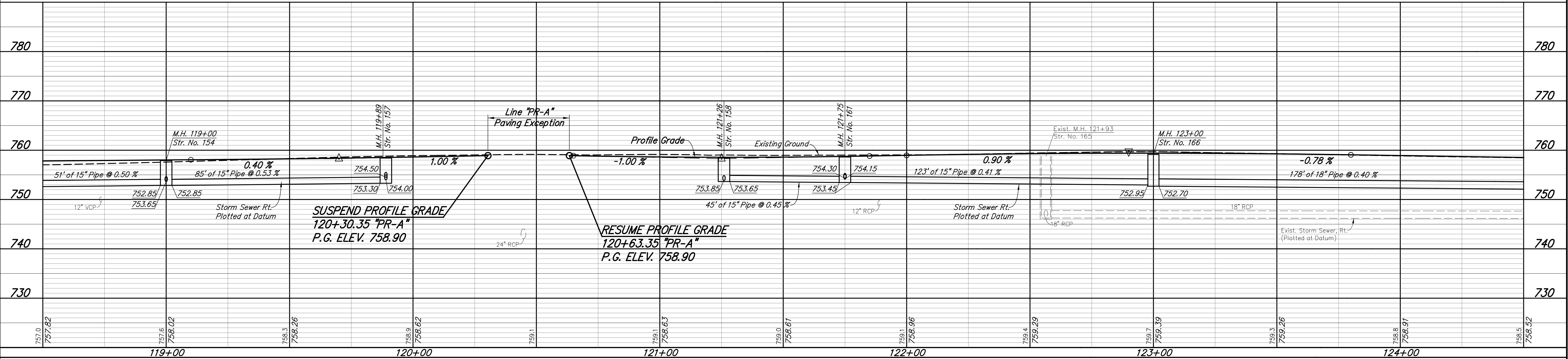
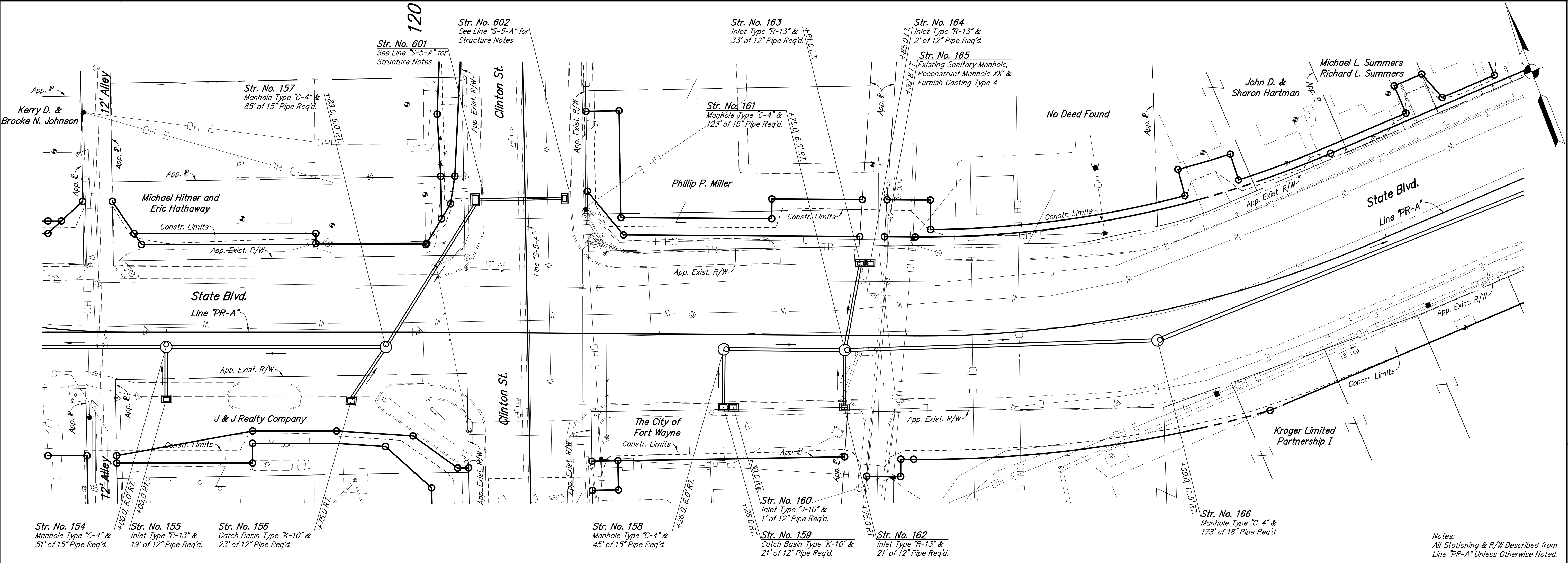
RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-A"	

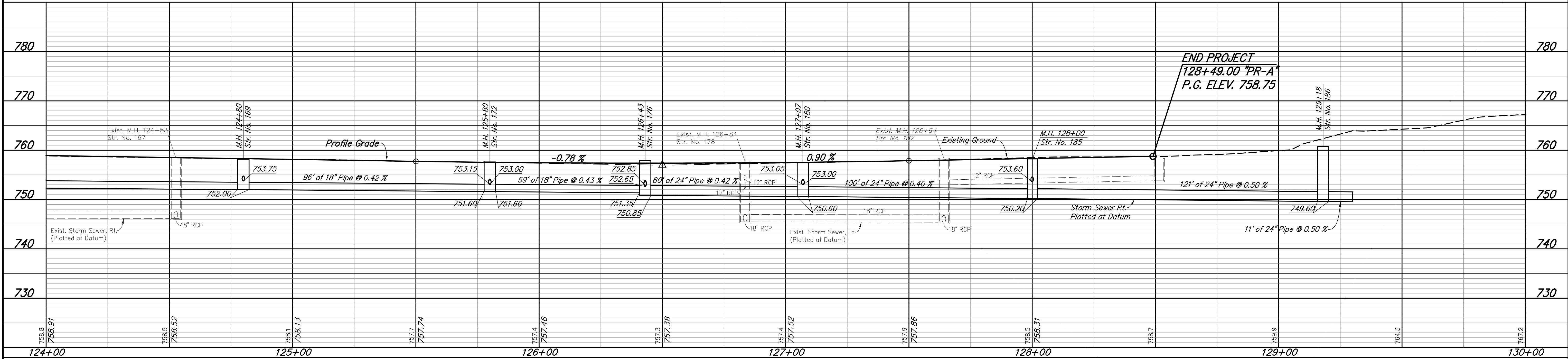
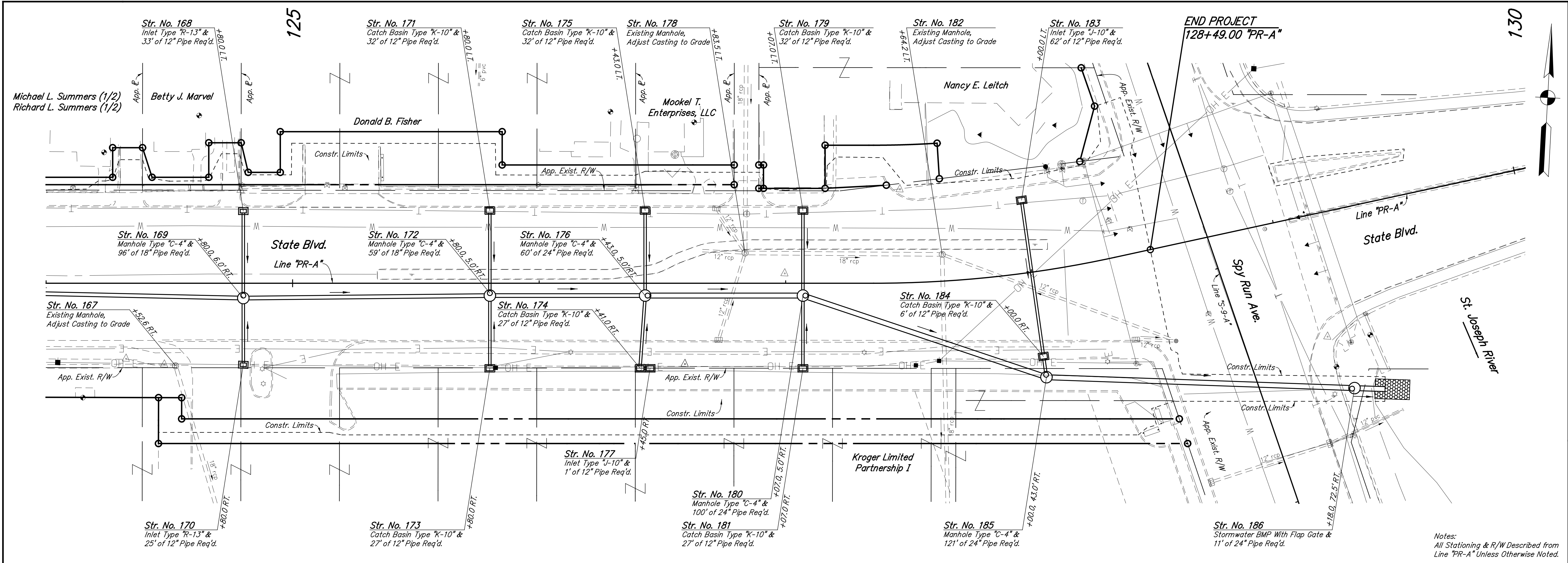
HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005151
SURVEY BOOK R-30743	SHEETS 23 of 50
CONTRACT R-30743	PROJECT NO. 1005151



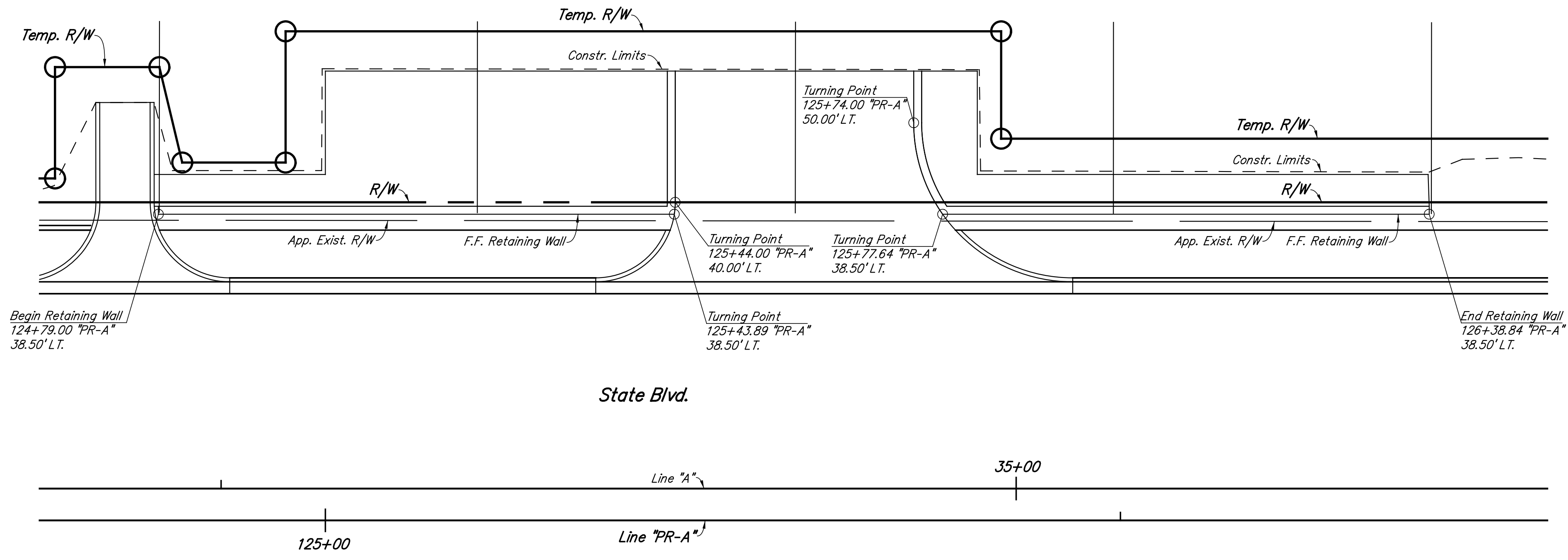
114+00				115+00				116+00				117+00				118+00				119+00								
																	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____				INDIANA DEPARTMENT OF TRANSPORTATION				HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
																	DESIGNED: _____ ASU DRAWN: _____ PJT				DRAINAGE DETAILS LINE "PR-A"				VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1005151	
																									SURVEY BOOK		SHEETS 24 of 50	
																	CHECKED: _____ SMC				CHECKED: _____ SMC				CONTRACT R-30743			



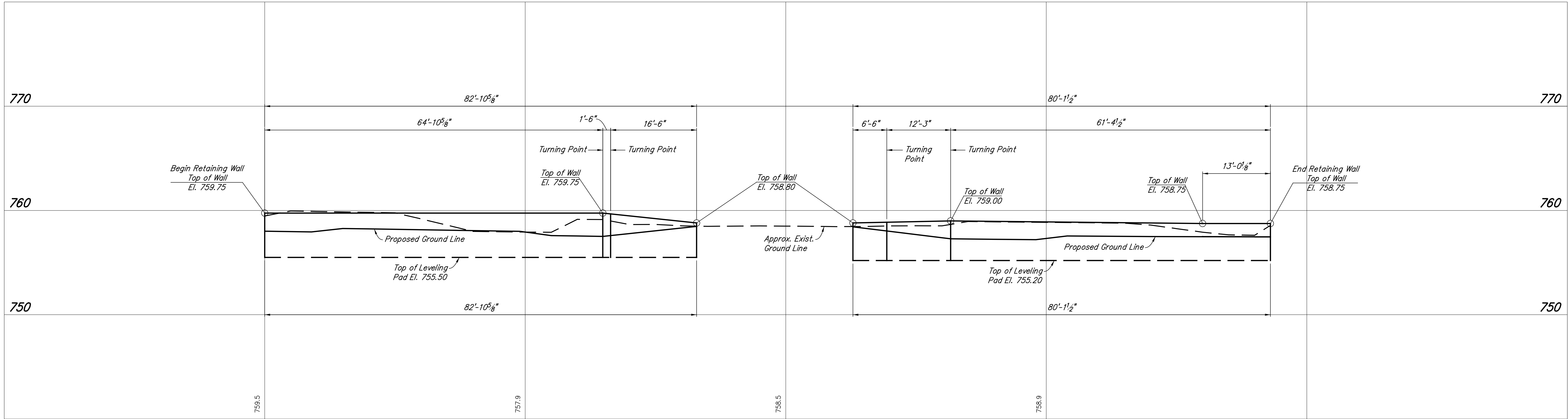
119+00		120+00		121+00		122+00		123+00		124+00	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		DRAINAGE DETAILS LINE "PR-A"		BRIDGE FILE	
DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC		HORIZONTAL SCALE 1" = 20'		DESIGNATION NO. 1005151		SHEETS 25 of 50	
CHECKED: SMC		CHECKED: SMC				SURVEY BOOK R-30743		PROJECT NO. 1005151			



		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION				HORIZONTAL SCALE		BRIDGE FILE	
												1" = 20'			
												VERTICAL SCALE		DESIGNATION NO.	
												1" = 10'		1005151	
		DESIGNED: ASU		DRAWN: PJT				DRAINAGE DETAILS				SURVEY BOOK		SHEETS	
														26 of 50	
		CHECKED: SMC		CHECKED: SMC				LINE "PR-A"				CONTRACT NO.		PROJECT NO.	
												R-30743		1005151	



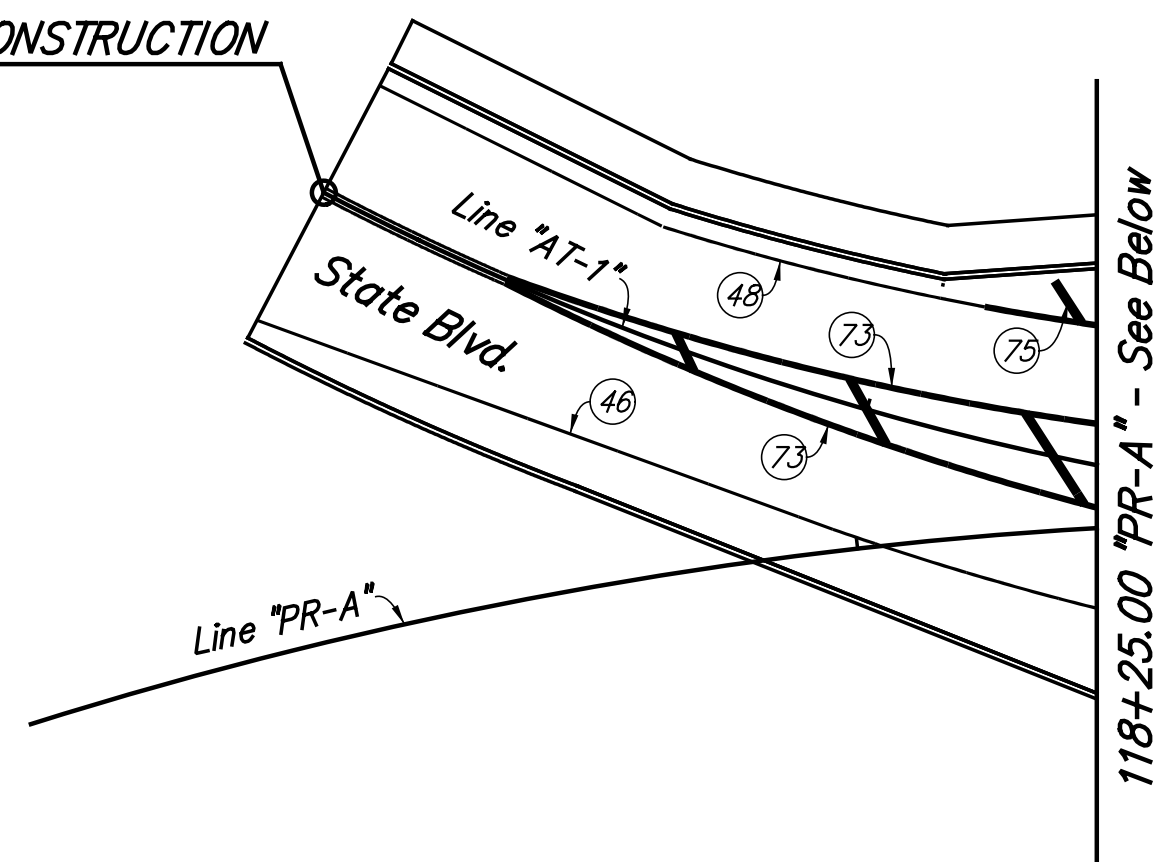
PLAN



ELEVATION

		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
									1" = 10'		DESIGNATION NO.	
									1" = 5'		1005151	
									SURVEY BOOK		SHEETS	
		DESIGNED: ASU		DRAWN: PJT			RETAINING WALL #9 DETAILS LINE "PR-A"		27		of 50	
									CONTRACT		PROJECT NO.	
									R-30743		1005151	
CHECKED: SMC		CHECKED: SMC										

BEGIN INCIDENTAL CONSTRUCTION
117+38.96 "AT-1"



118+55 "PR-A"
R2-1(30)

118+25.00 "PR-A" - See Above

BEGIN CONSTRUCTION
118+50.00 "PR-A"

LEGEND

- | | | |
|---|--|---|
| (38) Transverse Markings, Thermoplastic, Stop Line, White, 24 in. | (49) Line, Thermoplastic, Broken, Yellow, 4 in. | (75) Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in. |
| (46) Line, Thermoplastic, Solid, White, 4 in. | (71) Pavement Message Markings, Thermoplastic, Lane Indication Arrow | (77) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap) |
| (47) Line, Thermoplastic, Solid, Yellow, 4 in. | (72) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in. | (78) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap) |
| (48) Line, Thermoplastic, Broken, White, 4 in. | (73) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in. | |

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

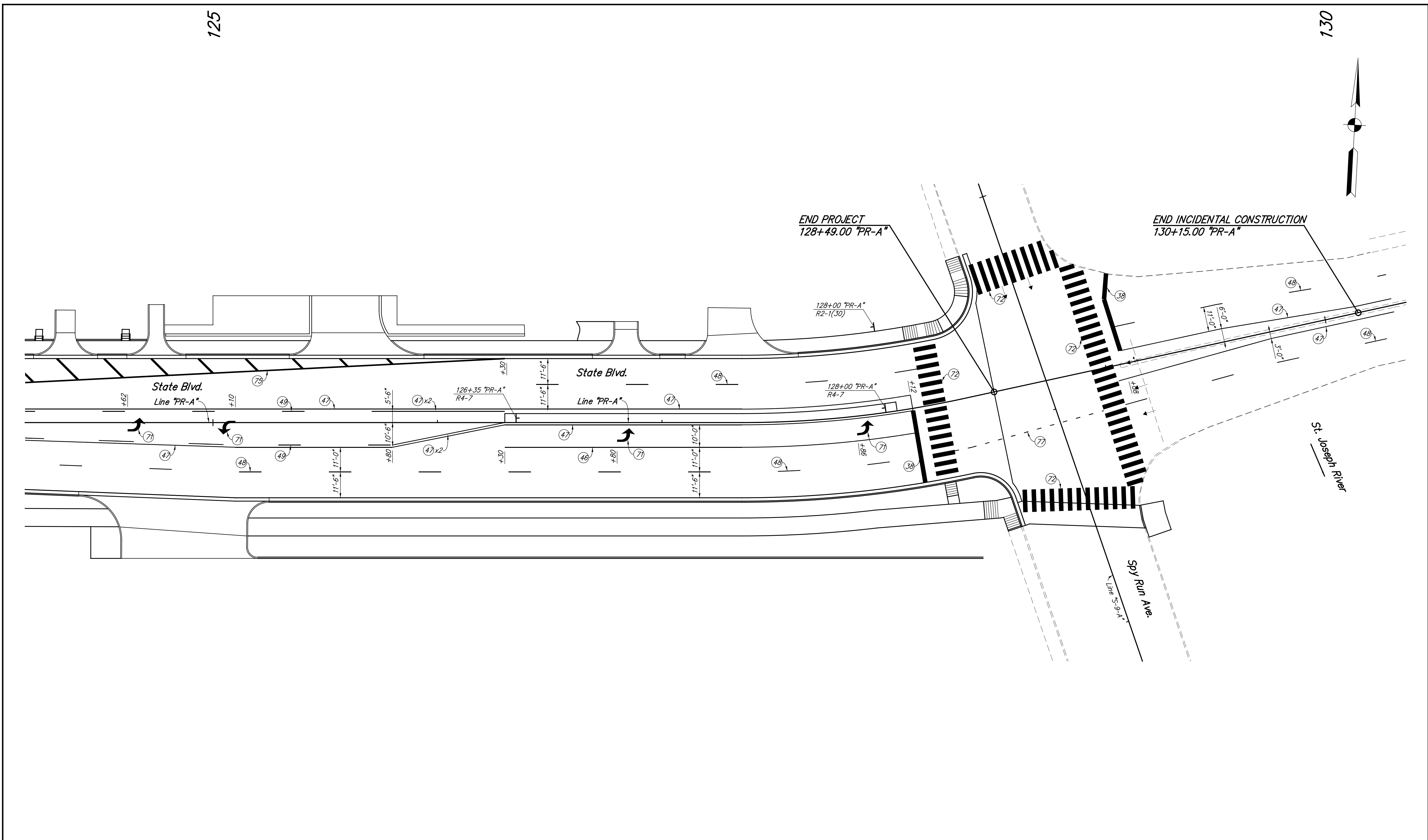
DESIGNED: ASU DRAWN: PJT

CHECKED: SMC CHECKED: SMC

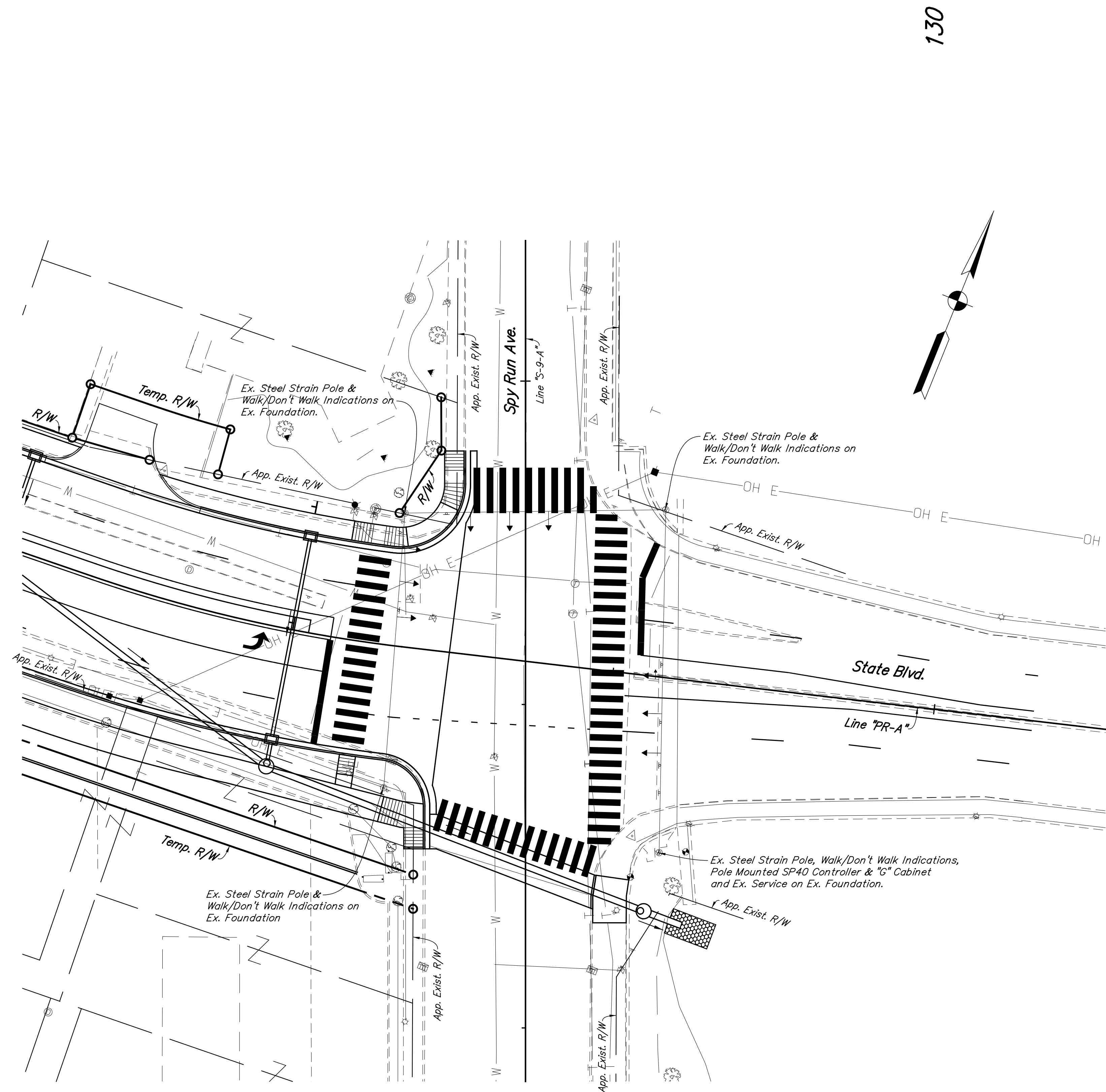
INDIANA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING &
SIGNING DETAILS - LINE "PR-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005151
SURVEY BOOK	SHEETS 28 of 50
CONTRACT R-30743	PROJECT NO. 1005151



LEGEND			INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
39 Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	49 Line, Thermoplastic, Broken, Yellow, 4 in.	79 Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in.			VERTICAL SCALE NONE		DESIGNATION NO. 1005151	
46 Line, Thermoplastic, Solid, White, 4 in.	71 Pavement Message Markings, Thermoplastic, Lane Indication Arrow	77 Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)	PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		SURVEY BOOK		SHEETS	
47 Line, Thermoplastic, Solid, Yellow, 4 in.	72 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	78 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)			CONTRACT R-30743		29 of 50 PROJECT NO. 1005151	
48 Line, Thermoplastic, Broken, White, 4 in.	73 Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.							
			RECOMMENDED FOR APPROVAL					
			DESIGN ENGINEER		DATE			
			DESIGNED: ASU		DRAWN: PJT			
			CHECKED: SMC		CHECKED: SMC			



- LEGEND**
- ⊕ Existing Signal Pole
 - ← Existing Signal Head

Notes:
Relocate the Existing Signal Heads and Signs
on the Span as Shown on the Plan.

**PRELIMINARY LAYOUT FOR FIELD CHECK
(NOT FOR CONSTRUCTION)**

INDIANA DEPARTMENT OF TRANSPORTATION			
DESIGN ENGINEER		DATE	
TRAFFIC SIGNAL DETAIL STATE BLVD. & SPY RUN AVE.			
CONTRACT NO. R-30743		DRAWN BY CAK	
COMM. NO.		DES. NO. 1005151	PROJECT NO. 1005151

SCALE: 1" = 20'		
YEAR 2012	SHEET 31	TOTAL 50

Electric

American Electric Power
Distribution
PO Box 60
Ft. Wayne, IN 46801
Contact: Tom Kanning
(260) 421-1766

American Electric Power
Transmission
700 Morrison Rd.
Gahanna, OH 43230
Contact: Tina Hairston
(614) 552-1801

Television

Comcast Cablevision
720 Taylor St.
Ft. Wayne, IN 46802
Contact: John Gayday
(260) 456-9000

Gas

NIPSCO
801 E. 86th. Ave.
Merrillville, IN 46410
Contact: Jill Boganwright
(419) 647-6502

Sewers

City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

Telephone

Frontier Communications
8001 W. Jefferson Blvd, INIFAON
Fort Wayne, IN 46804
Contact: Jean-Paul Kalonji
(260) 461-3650

Water

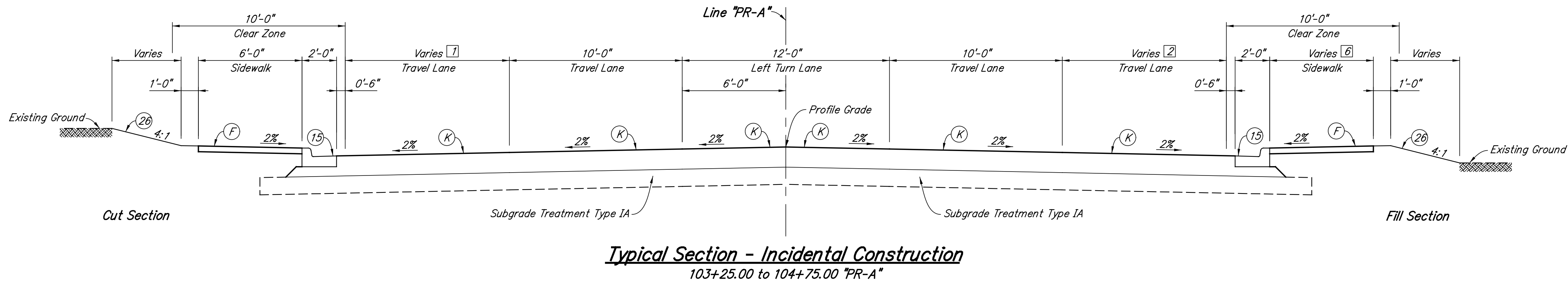
City of Fort Wayne Utilities
1 East Main St., Room 480
Ft. Wayne, IN 46802
Contact: Craig Berndt
(260) 427-2680

**	All earth shoulders, median areas, and cut and fill slopes shall be plain or mulch seeded except where sodding is specified
	The final cross sections of the grading contract will be the original cross sections of the paving contract. However, partial or complete cross sections shall be taken if necessary to determine the actual excavation quantities.
	The paper relocation will be cross sectioned by the Engineer before construction.
	Existing asphalt pavement located outside the construction limits, between Sta. _____ and Sta. _____, shall be removed as directed.
	The quantity of peat excavation shown on the plans has been estimated on the basis of theoretical cross sections by using treatment of existing fills, treatment by removal, or treatment by displacement, where each treatment applies.
	All limited access right-of-way (L.A. R/W) is to be fenced with chain link type fence (CLTF) or farm field type fence (FFTF) where specified in the plans.

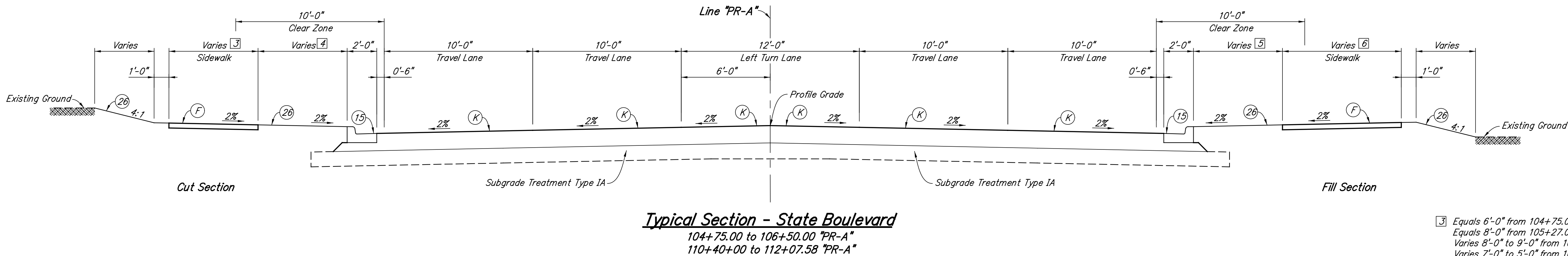
** REPRESENTS GENERAL NOTES REQUIRED

[illegible][illegible]

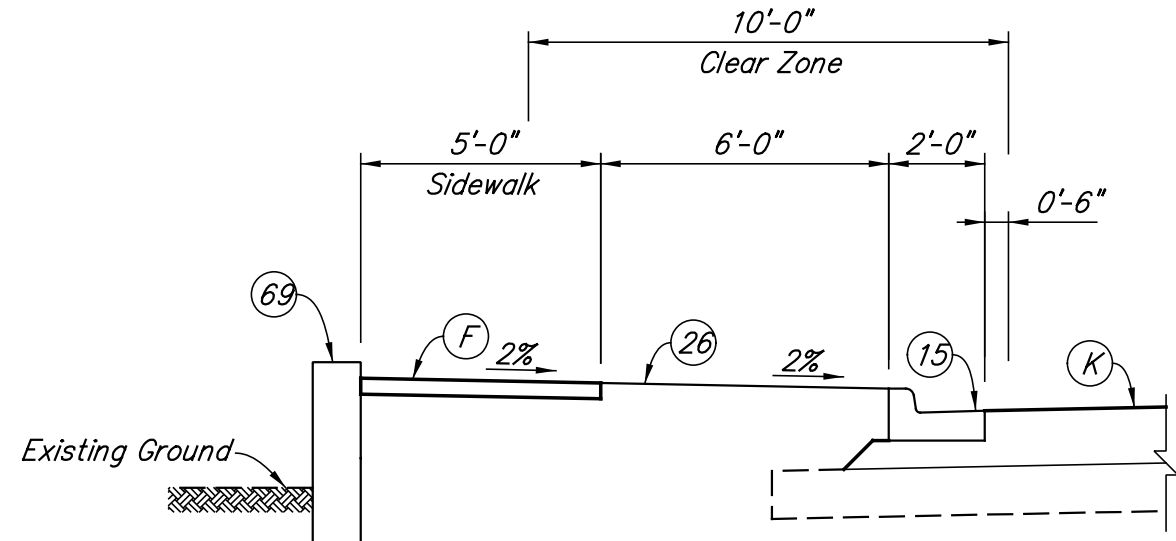
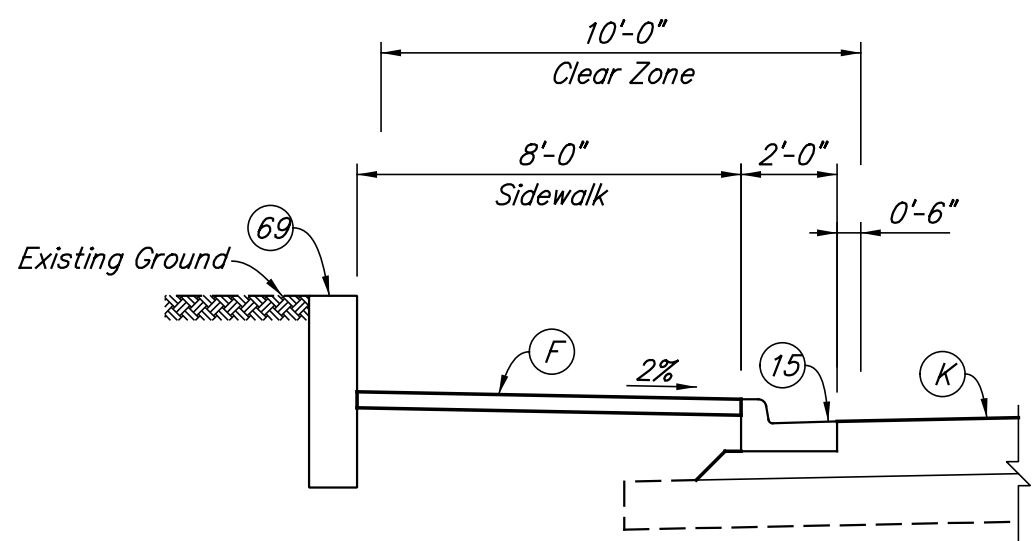
		RECOMMENDED FOR APPROVAL _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
		DESIGN ENGINEER _____ DATE _____				NONE			
		DESIGNED: ASU		DRAWN: CAK		VERTICAL SCALE		DESIGNATION NO.	
		CHECKED: SMC		CHECKED: SMC		NONE		1005154	
				INDEX & GENERAL NOTES		SURVEY BOOK		SHEETS	
								2 of 138	
						CONTRACT		PROJECT NO.	
						R-33130		1005154	



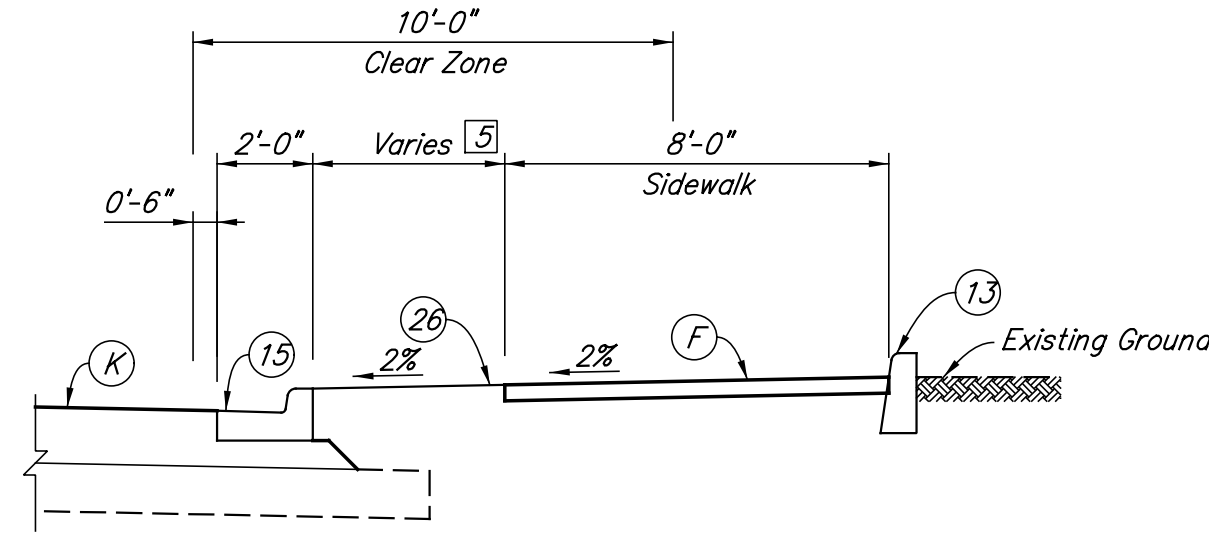
- 1 Equals 9'-6" from 103+25.00 to 104+04.44 "PR-A"
Varies 9'-6" to 10'-0" from 104+04.44 to 104+73.72 "PR-A"
Equals 10'-0" from 104+73.72 to 104+75.00 "PR-A"
- 2 Equals 9'-6" from 103+25.00 to 104+50.00 "PR-A"
Varies 9'-6" to 10'-0" from 104+50.00 to 104+75.00 "PR-A"



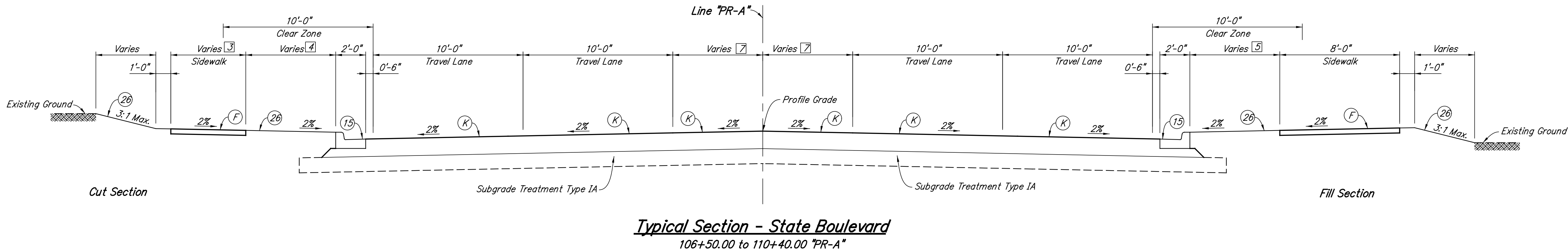
- 3 Equals 6'-0" from 104+75.00 to 105+27.04 "PR-A"
Equals 8'-0" from 105+27.04 to 106+50.00 "PR-A"
Varies 8'-0" to 9'-0" from 106+50.00 to 106+70.00 "PR-A"
Varies 7'-0" to 5'-0" from 106+70.00 to 107+30.00 "PR-A"
Equals 5'-0" from 107+30.00 to 112+07.58 "PR-A"



Paving Exception
112+07.58 to 113+92.34 "PR-A"



- 4 Equals 0'-0" from 104+75.00 to 106+70.00 "PR-A"
Varies 2'-0" to 6'-0" from 106+70.00 to 107+50.00 "PR-A"
Equals 6'-0" from 107+50.00 to 112+07.58 "PR-A"
- 5 Equals 0'-0" from 104+75.00 to 106+90.00 "PR-A"
Varies 2'-0" to 6'-0" from 106+90.00 to 107+50.00 "PR-A"
Equals 6'-0" from 107+50.00 to 112+07.58 "PR-A"
- 6 Equals 6'-0" from 103+25.00 to 104+11.27 "PR-A"
Varies 6'-0" to 10'-5" from 104+11.27 to 104+85.03 "PR-A"
Equals 8'-0" from 104+85.03 to 112+07.58 "PR-A"
- 7 Varies 6'-0" to 1'-0" from 106+50.00 to 107+50.00 "PR-A"
Equals 1'-0" from 107+50.00 to 110+40.00 "PR-A"
Varies 1'-0" to 6'-0" from 109+40.00 to 110+40.00 "PR-A"



- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"

- LEGEND
- (F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete

- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU

DRAWN: CAK

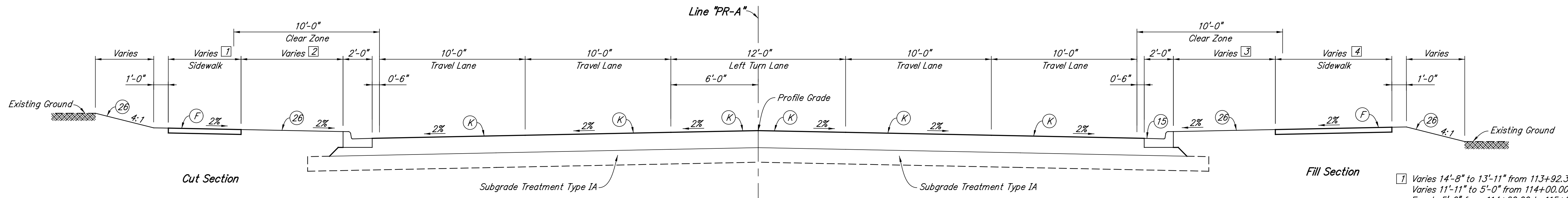
CHECKED: SMC

CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

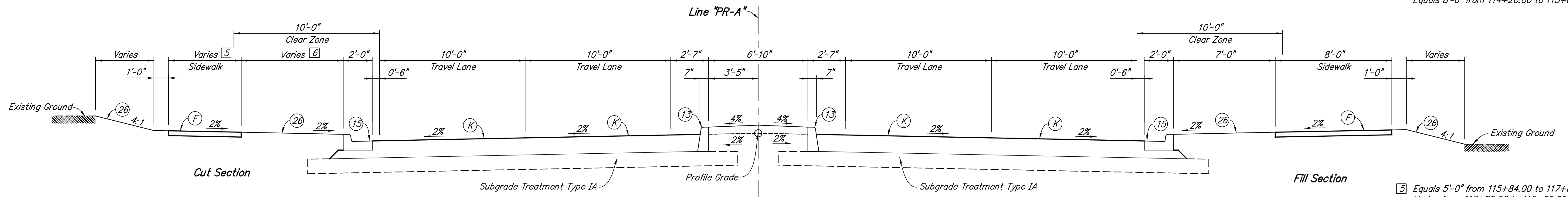
TYPICAL CROSS SECTIONS
LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	
VERTICAL SCALE	DESIGNATION NO.
NONE	1005154
SURVEY BOOK	SHEETS
	3 of 138
CONTRACT	PROJECT NO.
R-33130	1005154



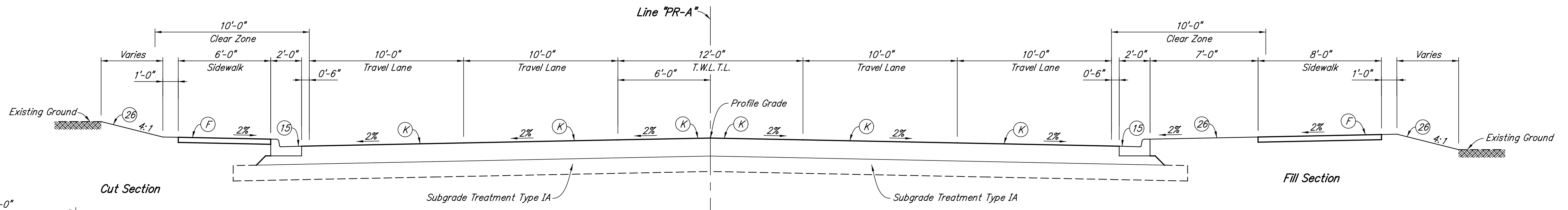
Typical Section - State Boulevard
113+92.34 to 115+84.00 "PR-A"

- 1 Varies 14'-8" to 13'-11" from 113+92.34 to 114+00.00 "PR-A"
Varies 11'-11" to 5'-0" from 114+00.00 to 114+20.00 "PR-A"
Equals 5'-0" from 114+20.00 to 115+84.00 "PR-A"
- 2 Equals 0'-0" from 113+92.34 to 114+00.00 "PR-A"
Varies 2'-0" to 7'-0" from 114+00.00 to 114+20.00 "PR-A"
Equals 7'-0" from 114+20.00 to 115+84.00 "PR-A"
- 3 Equals 0'-0" from 113+92.34 to 114+00.00 "PR-A"
Varies 2'-0" to 7'-0" from 114+00.00 to 114+20.00 "PR-A"
Equals 7'-0" from 114+20.00 to 115+84.00 "PR-A"
- 4 Varies 12'-2" to 13'-0" from 113+92.34 to 114+00.00 "PR-A"
Varies 11'-0" to 8'-0" from 114+00.00 to 114+20.00 "PR-A"
Equals 8'-0" from 114+20.00 to 115+84.00 "PR-A"

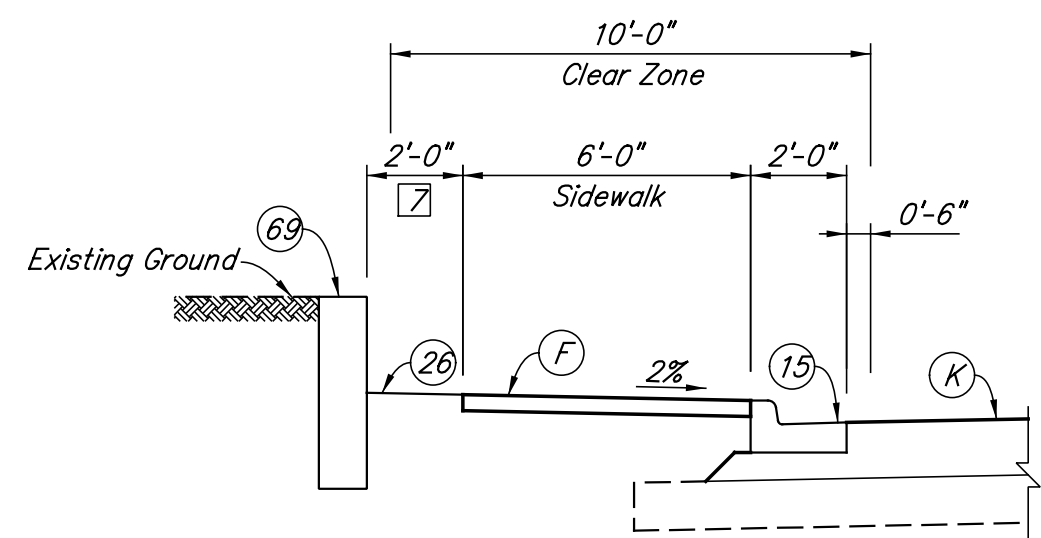


Typical Section - State Boulevard
115+84.00 to 118+06.00 "PR-A"

- 5 Equals 5'-0" from 115+84.00 to 117+89.00 "PR-A"
Varies from 117+89.00 to 118+00.00 "PR-A"
Equals 6'-0" from 118+00.00 to 118+06.00 "PR-A"
- 6 Equals 7'-0" from 116+00.00 to 117+89.00 "PR-A"
Varies 7'-0" to 2'-0" from 117+89.00 to 118+00.00 "PR-A"
Equals 0'-0" from 118+00.00 to 118+06.00 "PR-A"

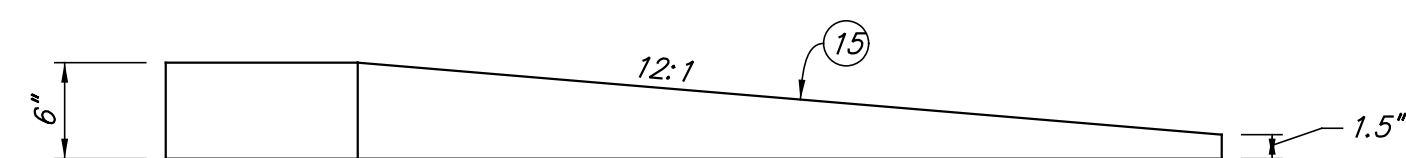


Typical Section - State Boulevard
118+06.00 to 118+50.00 "PR-A"

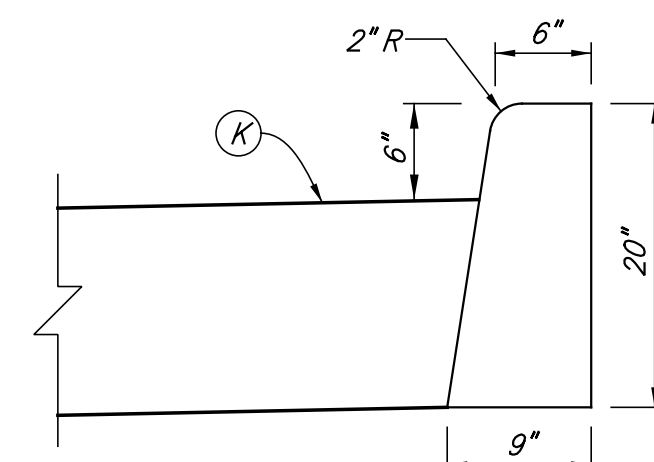


Cut Section
118+00.00 to 118+65.00 Lt. "PR-A"

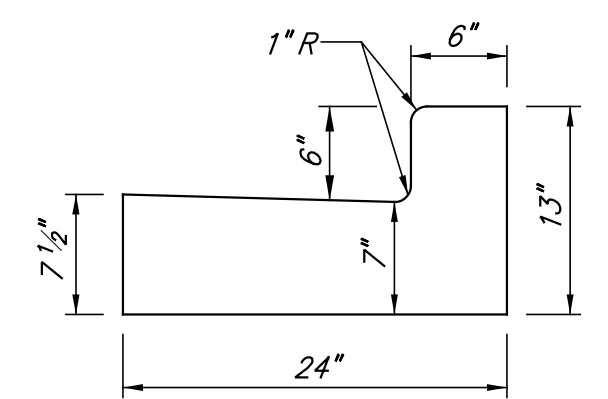
- 7 Equals 0'-0" from 118+00.00 to 118+02.11 "PR-A"
Varies 0'-0" to 2'-0" from 118+02.11 to 118+07.57 "PR-A"
Equals 2'-0" from 118+07.57 to 118+65.00 "PR-A"



Curb Transition Detail
Not to Scale



Concrete Curb Detail
Not to Scale



Curb & Gutter, Concrete, Modified Detail
Not to Scale

- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"

- (F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

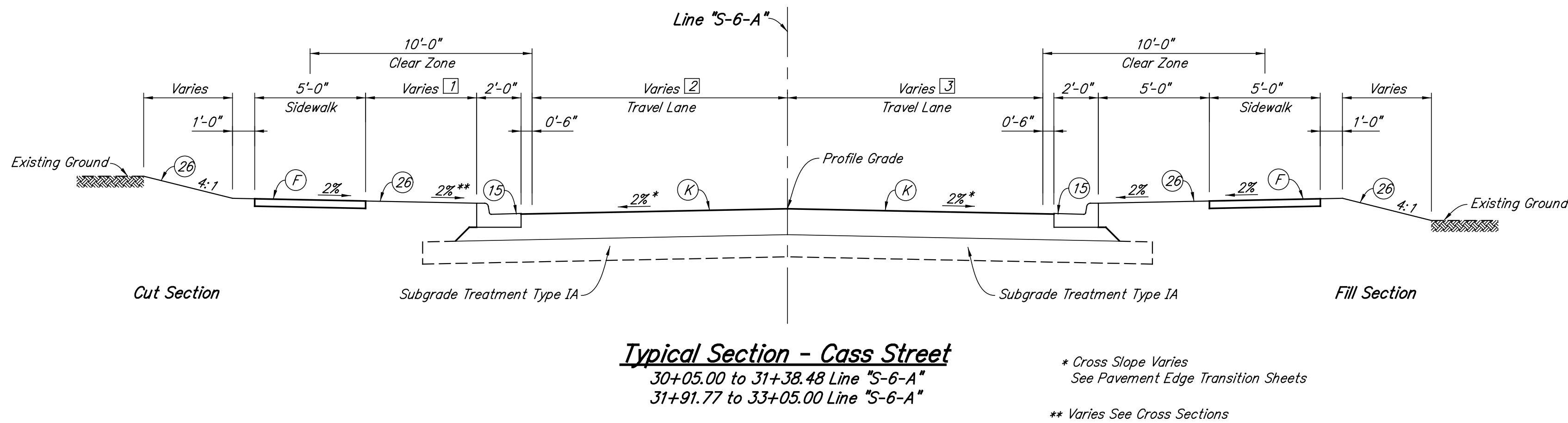
- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete

- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

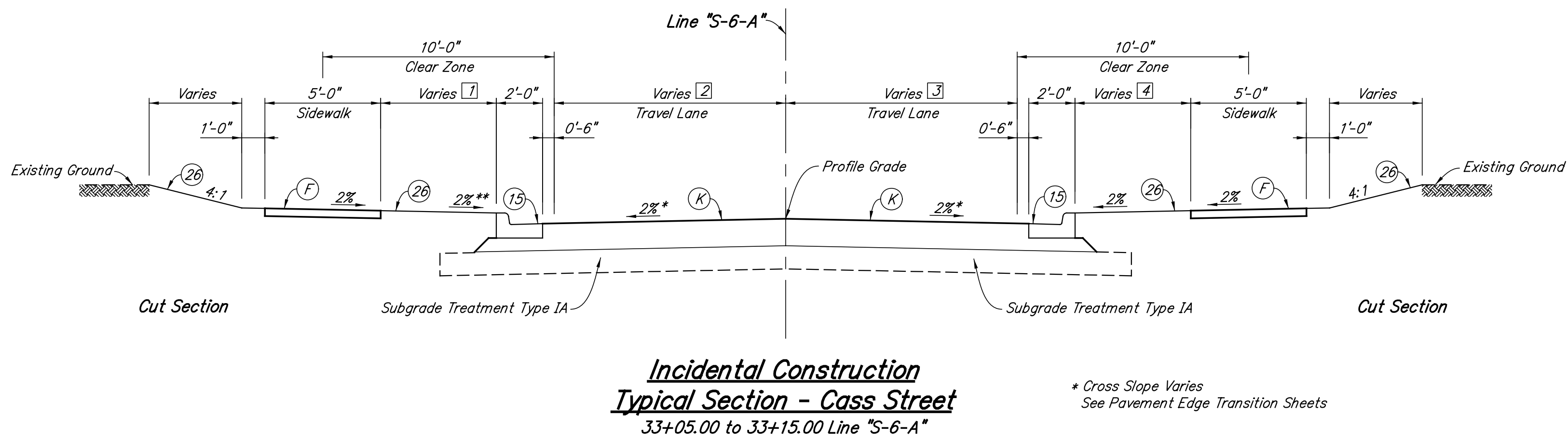
RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "PR-A"	

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK R-33130	SHEETS 4 of 138
CONTRACT R-33130	PROJECT NO. 1005154



- [1] Equals 4'-9" from 30+05.00 to 30+15.00 "S-6-A"
Varies 4'-9" to 5'-0" from 30+15.00 to 30+25.00 "S-6-A"
Equals 5'-0" from 30+30.00 to 31+38.48 "S-6-A"
Equals 8'-0" from 31+91.77 to 33+05.00 "S-6-A"
Varies 8'-0" to 8'-10" from 33+05.00 to 33+15.00 "S-6-A"
- [2] Equals 11'-0" from 30+05.00 to 31+38.48 "S-6-A"
Equals 10'-0" from 31+91.77 to 33+05.00 "S-6-A"
Varies 10'-0" to 9'-0" from 33+05.00 to 33+15.00 "S-6-A"
- [3] Equals 11'-6" from 30+05.00 to 30+15.00 "S-6-A"
Varies 11'-6" to 11'-0" from 30+15.00 to 30+30.00 "S-6-A"
Equals 11'-0" from 30+30.00 to 31+38.48 "S-6-A"
Equals 10'-0" from 31+91.77 to 33+05.00 "S-6-A"
Varies 10'-0" to 9'-4" from 33+05.00 to 33+15.00 "S-6-A"
- [4] Varies 6'-6" to 7'-2" from 33+05.00 to 33+15.00 "S-6-A"



(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx

(F) Concrete Sidewalk, 4"

(F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base

(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

(13) Concrete Curb

(14) Integral Concrete Curb

(15) Curb and Gutter, Concrete, Modified

(16) Curb and Gutter, C, Concrete

(22) Concrete Center Curb, Type "D"

(26) Sodding, Nursery

(28) Mulched Seeding, Type "U"

(69) Retaining Wall

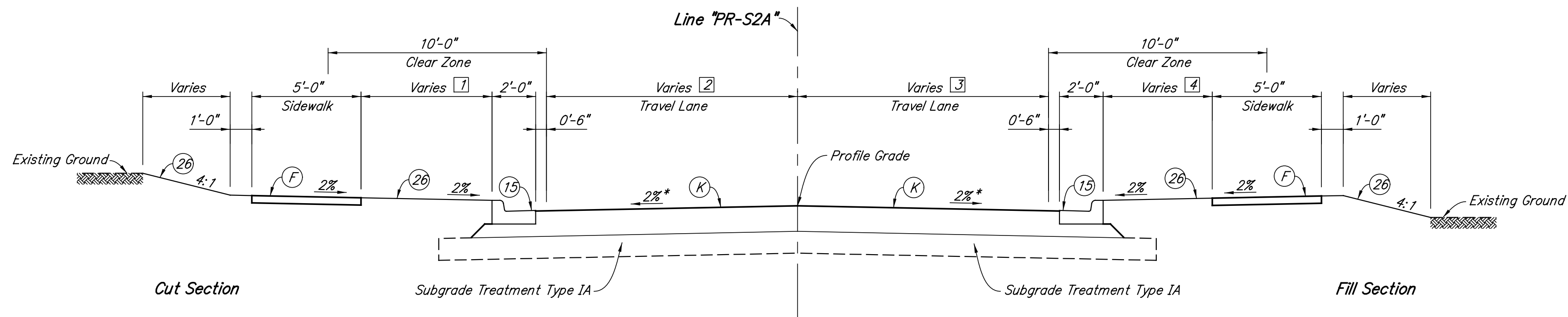
RECOMMENDED
FOR APPROVAL _____
DESIGN ENGINEER DATE

DESIGNED: ASU DRAWN: CAK
CHECKED: SMC CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

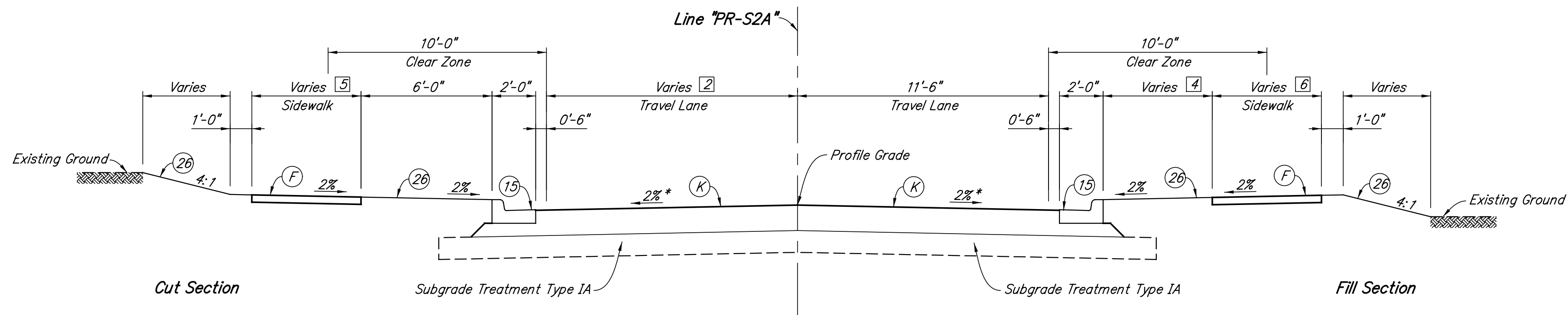
TYPICAL CROSS SECTIONS
LINE "S-6-A"

HORIZONTAL SCALE	BRIDGE FILE
1/4" = 1'-0"	
VERTICAL SCALE	DESIGNATION NO.
NONE	1005154
SURVEY BOOK	SHEETS
	5 of 138
CONTRACT	PROJECT NO.
R-33130	1005154



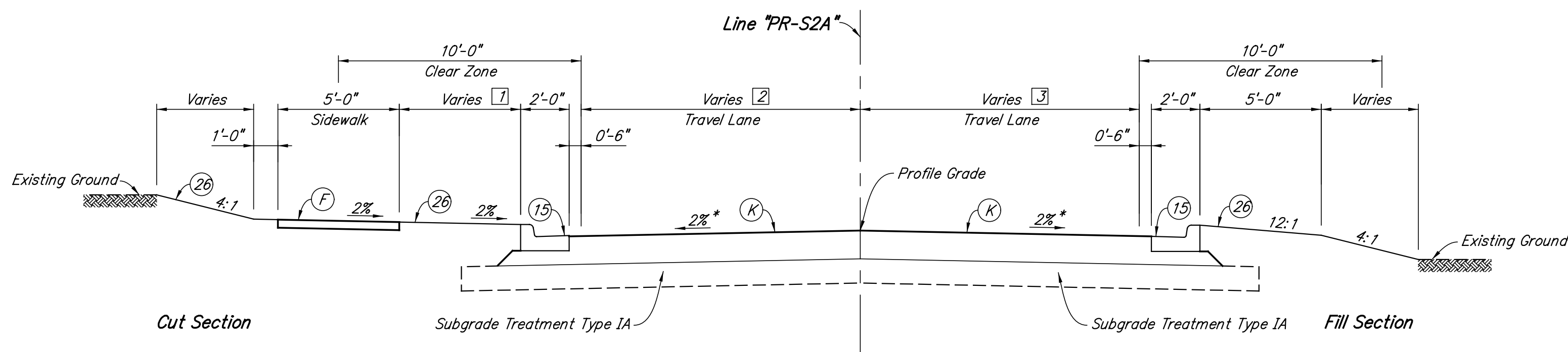
Incidental Construction
Typical Section - Edgehill Avenue
51+50.00 to 51+80.00 Line "PR-S2A"

* Cross Slope Varies
See Pavement Edge Transition Sheets



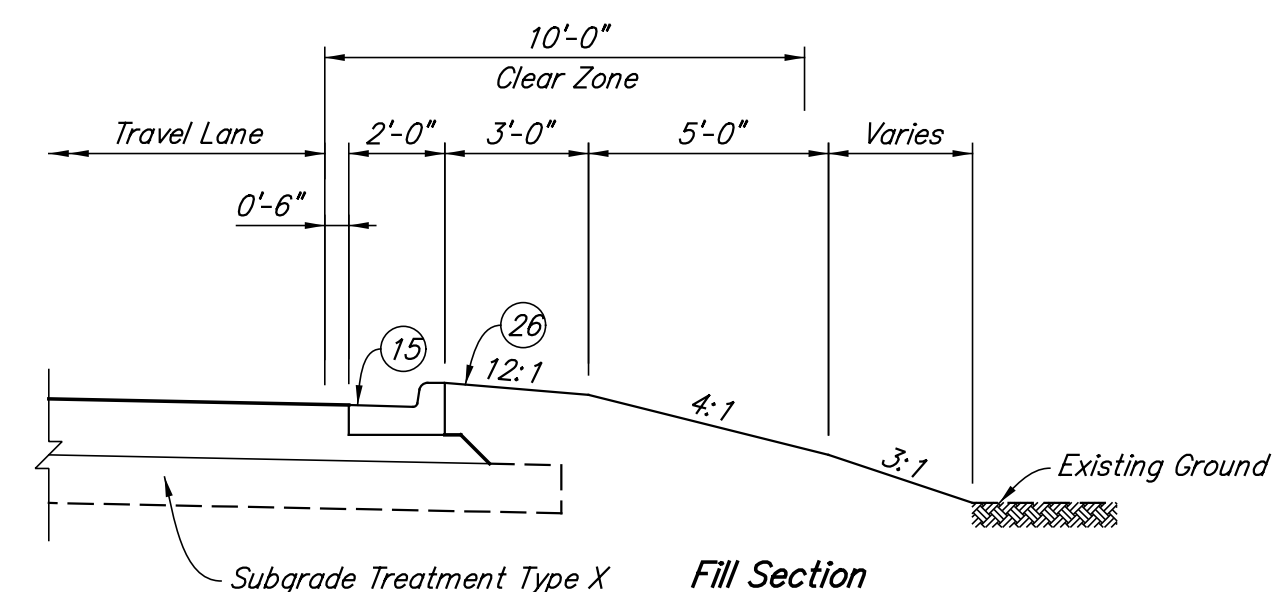
Typical Section - Edgehill Avenue
51+80.00 to 54+66.43 Line "PR-S2A"

* Cross Slope Varies
See Pavement Edge Transition Sheets



Typical Section - Westbrook Drive
55+27.76 to 58+45.00 Line "PR-S2A"

* Cross Slope Varies
See Pavement Edge Transition Sheets



Fill Section Detail
55+27.76 to 56+75.00 Rt. Line "PR-S2A"

(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx

(F) Concrete Sidewalk, 4"

(F1) 140 #/Syd. HMA Surface, Type "A", on
220 #/Syd. HMA Intermediate, Type "A", on
6" Compacted Aggregate, No. 53, Base

(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx

(13) Concrete Curb

(14) Integral Concrete Curb

(15) Curb and Gutter, Concrete, Modified

(16) Curb and Gutter, C, Concrete

(22) Concrete Center Curb, Type "D"

(26) Sodding, Nursery

(28) Mulched Seeding, Type "U"

(69) Retaining Wall

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

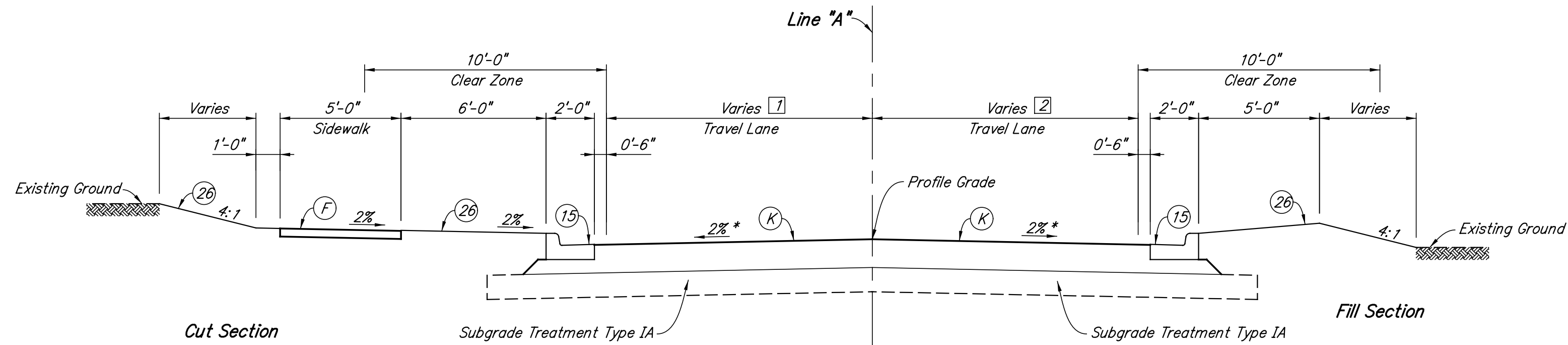
DESIGNED: ASU DRAWN: CAK

CHECKED: SMC CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
LINE "PR-S2A"

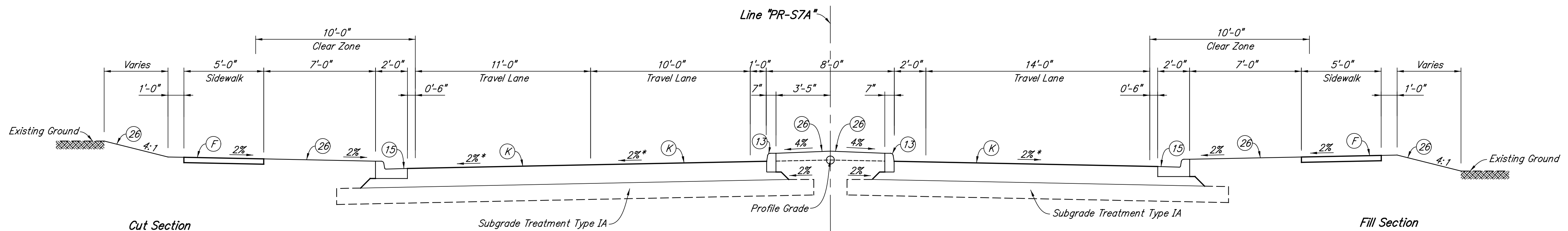
HORIZONTAL SCALE		BRIDGE FILE	
1/4" = 1'-0"		DESIGNATION NO.	
NONE		1005154	
SURVEY BOOK		SHEETS	
CONTRACT		6	of 138
R-33130		PROJECT NO.	
		1005154	



Typical Section - Existing State Blvd.
21+18.36 to 23+50.05 Line "A"
23+85.48 to 26.41.61 Line "A"

* Cross Slope Varies
See Pavement Edge Transition Sheets

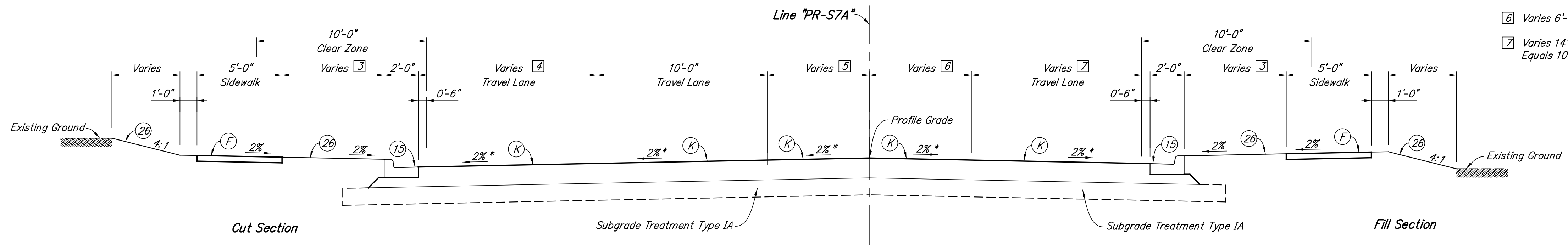
- 1 Equals 10'-6" from 21+88.00 to 23+50.05 "A"
Equals 10'-6" from 23+85.48 to 25+23.00 "A"
Varies 10'-6" to 15'-10" from 25+23.00 to 25+70.00 "A"
- 2 Varies 12'-7" to 10'-6" from 21+88.00 to 22+00.00 "A"
Equals 10'-6" from 22+00.00 to 23+50.05 "A"
Equals 10'-6" from 23+85.48 to 24+86.53 "A"
Varies 10'-6" to 24'-4" from 24+86.53 to 25+70.00 "A"



Typical Section - Oakridge Road
60+26.50 to 61+44.00 Line "PR-S7A"

* Cross Slope Varies
See Pavement Edge Transition Sheets

- 3 Equals 7'-0" from 61+44.00 to 61+88.68 "PR-S7A"
Equals 6'-0" from 61+88.68 to 63+45.00 "PR-S7A"
- 4 Equals 11'-0" from 61+44.00 to 61+45.55 "PR-S7A"
Varies 11'-0" to 0'-0" from 61+45.55 to 62+40.00 "PR-S7A"
- 5 Varies 5'-0" to 0'-0" from 61+44.00 to 62+34.00 "PR-S7A"
- 6 Varies 6'-0" to 0'-0" from 61+44.00 to 62+34.00 "PR-S7A"
- 7 Varies 14'-0" to 10'-0" from 61+44.00 to 62+34.00 "PR-S7A"
Equals 10'-0" from 62+34.00 to 63+45.00 "PR-S7A"



Typical Section - Oakridge Road
61+44.00 to 63+45.00 Line "PR-S7A"

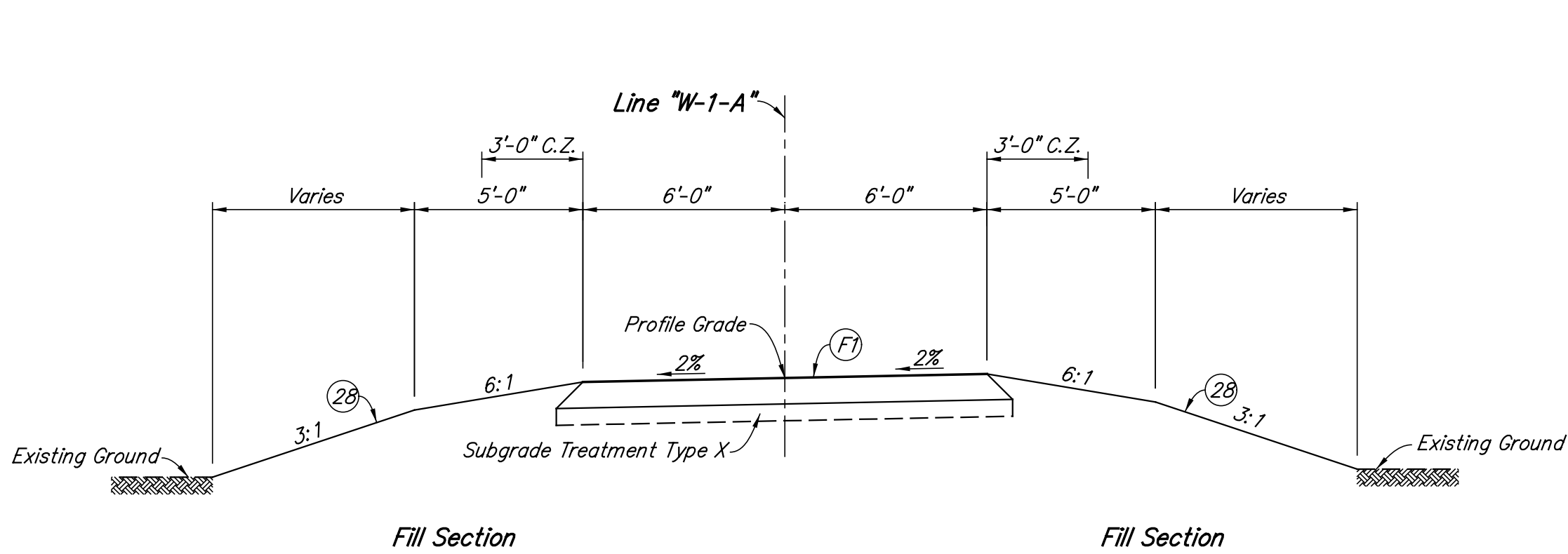
* Cross Slope Varies
See Pavement Edge Transition Sheets

- LEGEND**
- (K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on
xxx #/Syd. QC/QA-HMA, x, xx, Intermediate OG, 19mm on
xxx #/Syd. QC/QA-HMA, x, xx, Base, xx
- (F) Concrete Sidewalk, 4"
- (F1) 140 #/Syd. HMA Surface, Type "A" on
220 #/Syd. HMA Intermediate, Type "A" on
6" Compacted Aggregate, No. 53, Base
- (R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx
- (13) Concrete Curb
- (14) Integral Concrete Curb
- (15) Curb and Gutter, Concrete, Modified
- (16) Curb and Gutter, C, Concrete
- (22) Concrete Center Curb, Type "D"
- (26) Sodding, Nursery
- (28) Mulched Seeding, Type "U"
- (69) Retaining Wall

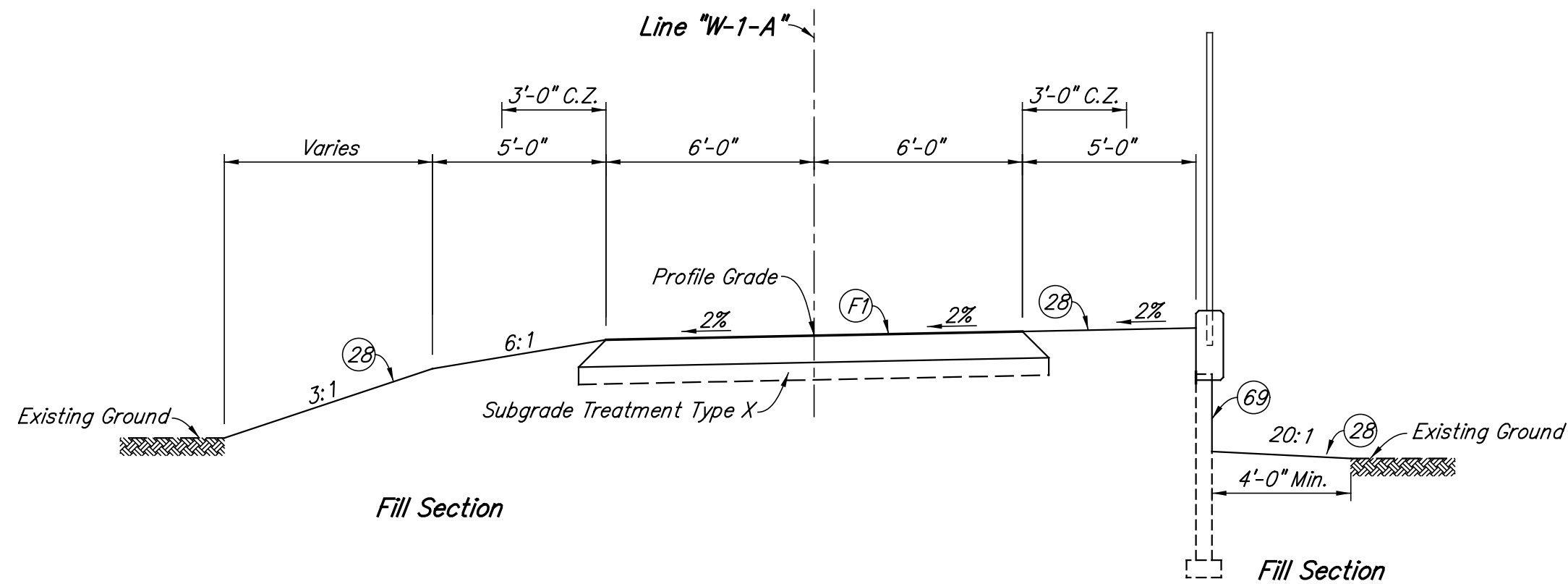
RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTIONS LINE "A" & "PR-S7A"	

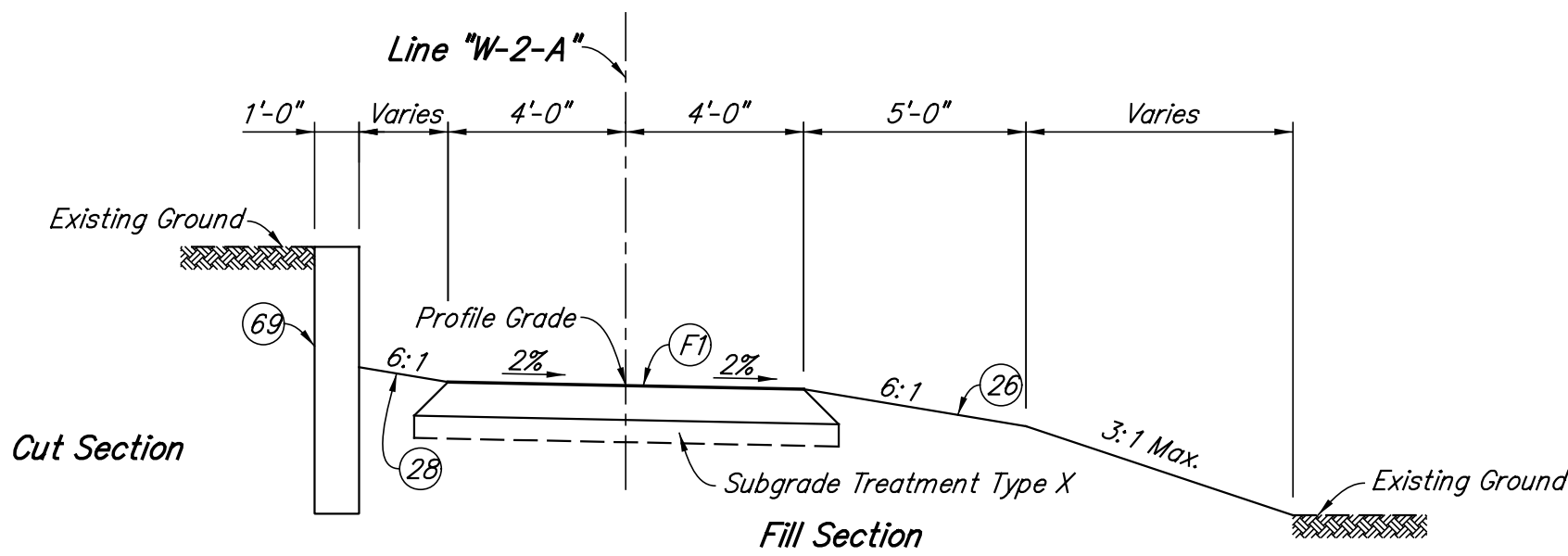
HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005154	
SURVEY BOOK	SHEETS	
CONTRACT R-33130	7	of 138
	PROJECT NO. 1005154	



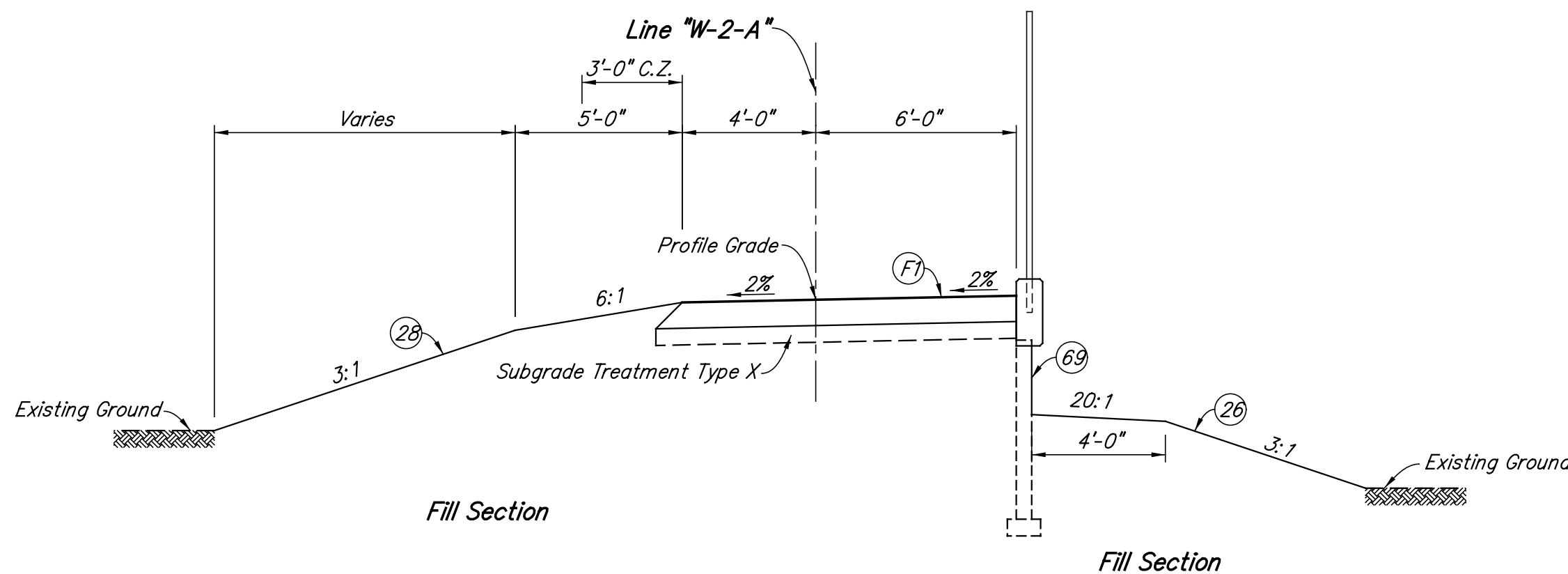
Typical Section Pufferbelly Trail
1+10.00 to 4+00.00 "W-1-A"
11+50.00 to 12+50.00 "W-1-A"



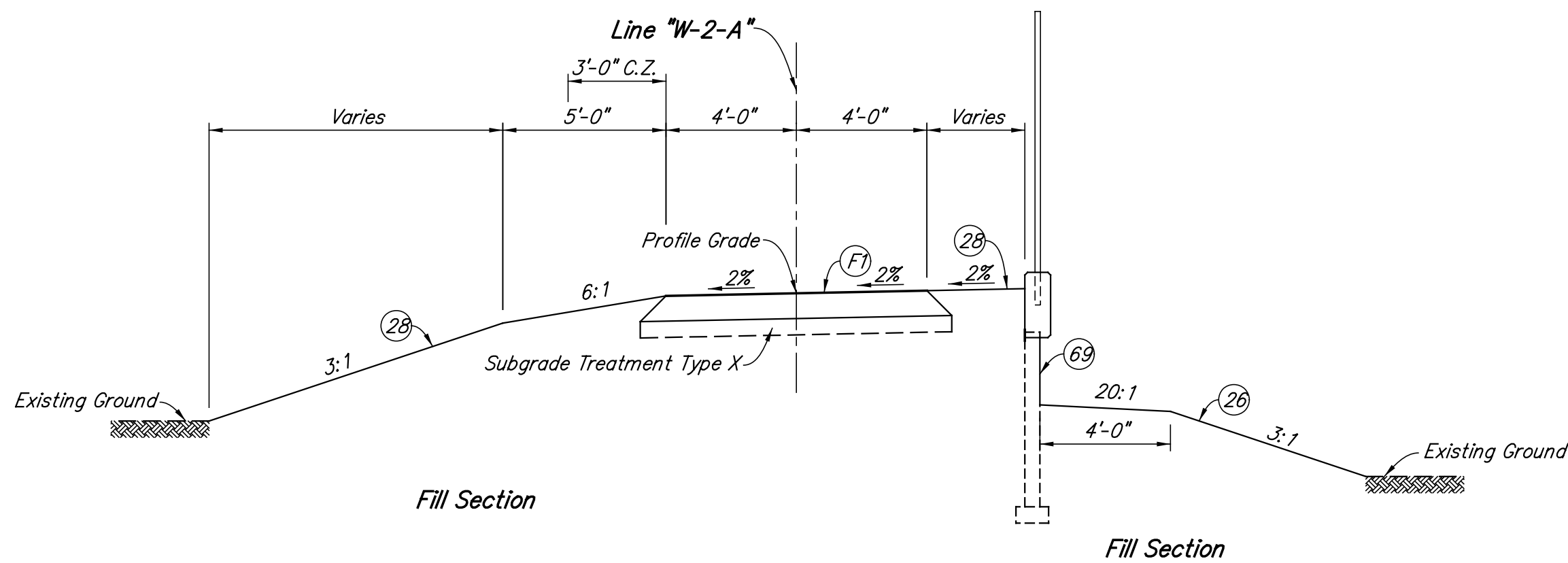
Typical Section Pufferbelly Trail
4+00.00 to 7+02.84 "W-1-A"
7+98.16 to 11+50.00 "W-1-A"



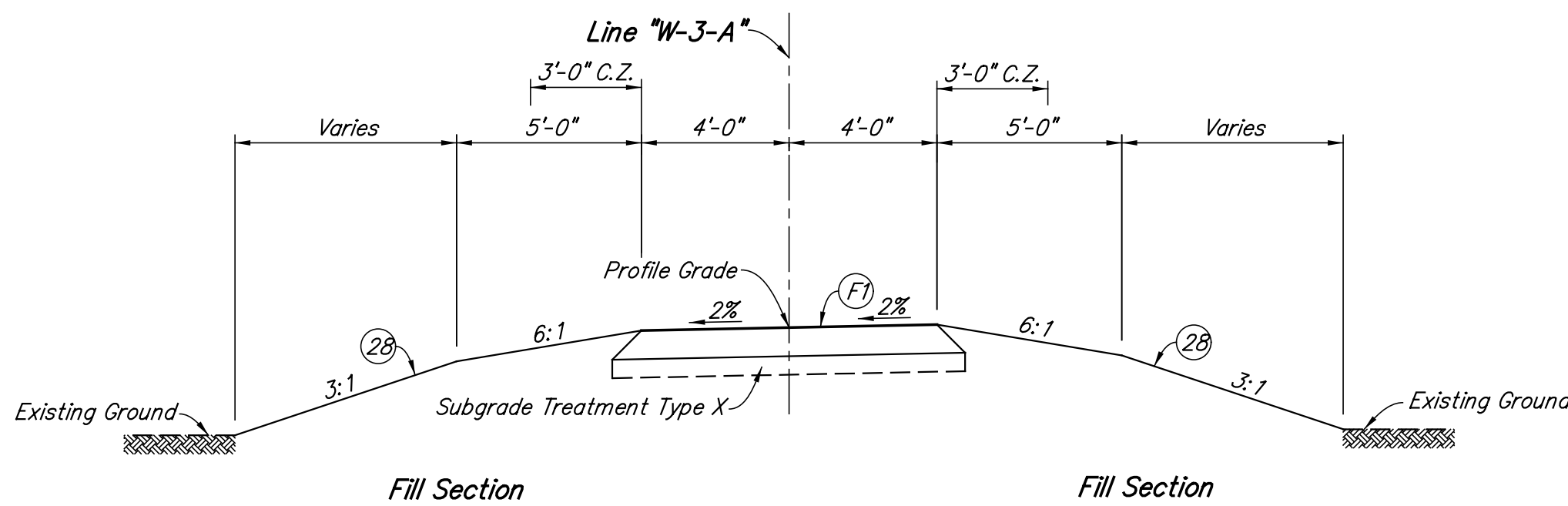
Typical Section Sidewalk Ramp
10+00.00 to 11+25.00 "W-2-A"



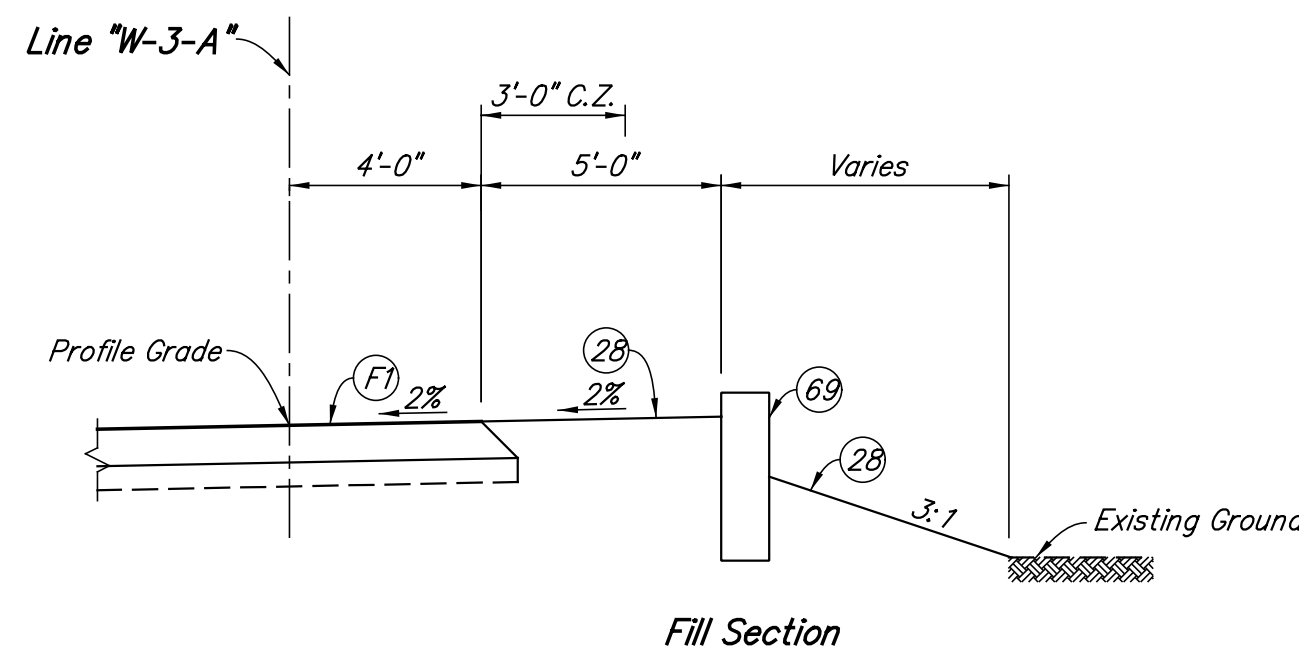
Typical Section Sidewalk Ramp
11+25.00 to 12+15.29 "W-2-A"



Typical Section Sidewalk Ramp
12+15.29 to 12+52.34 "W-2-A"



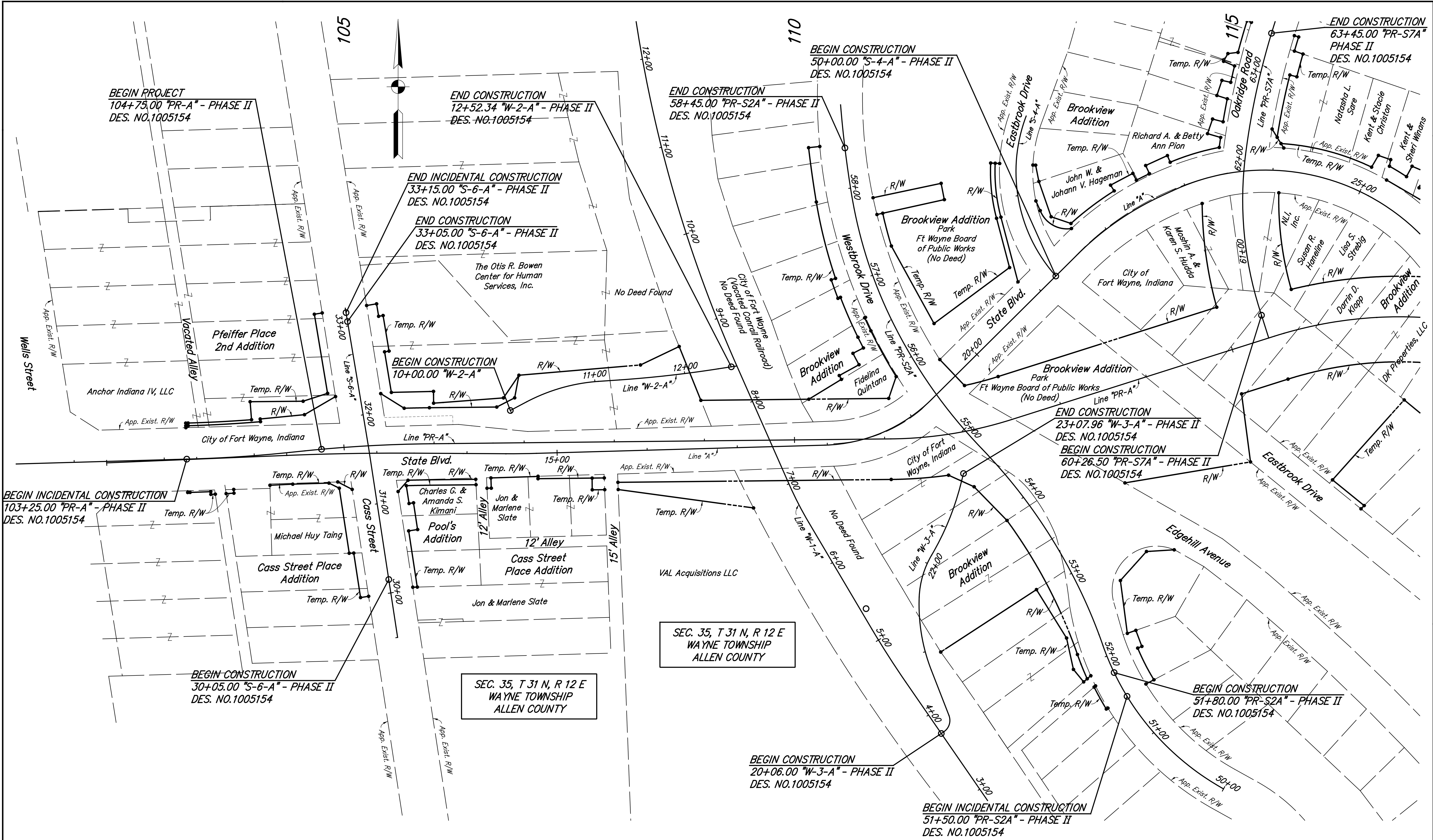
Typical Section Sidewalk Ramp
20+06.70 to 23+07.96 "W-3-A"



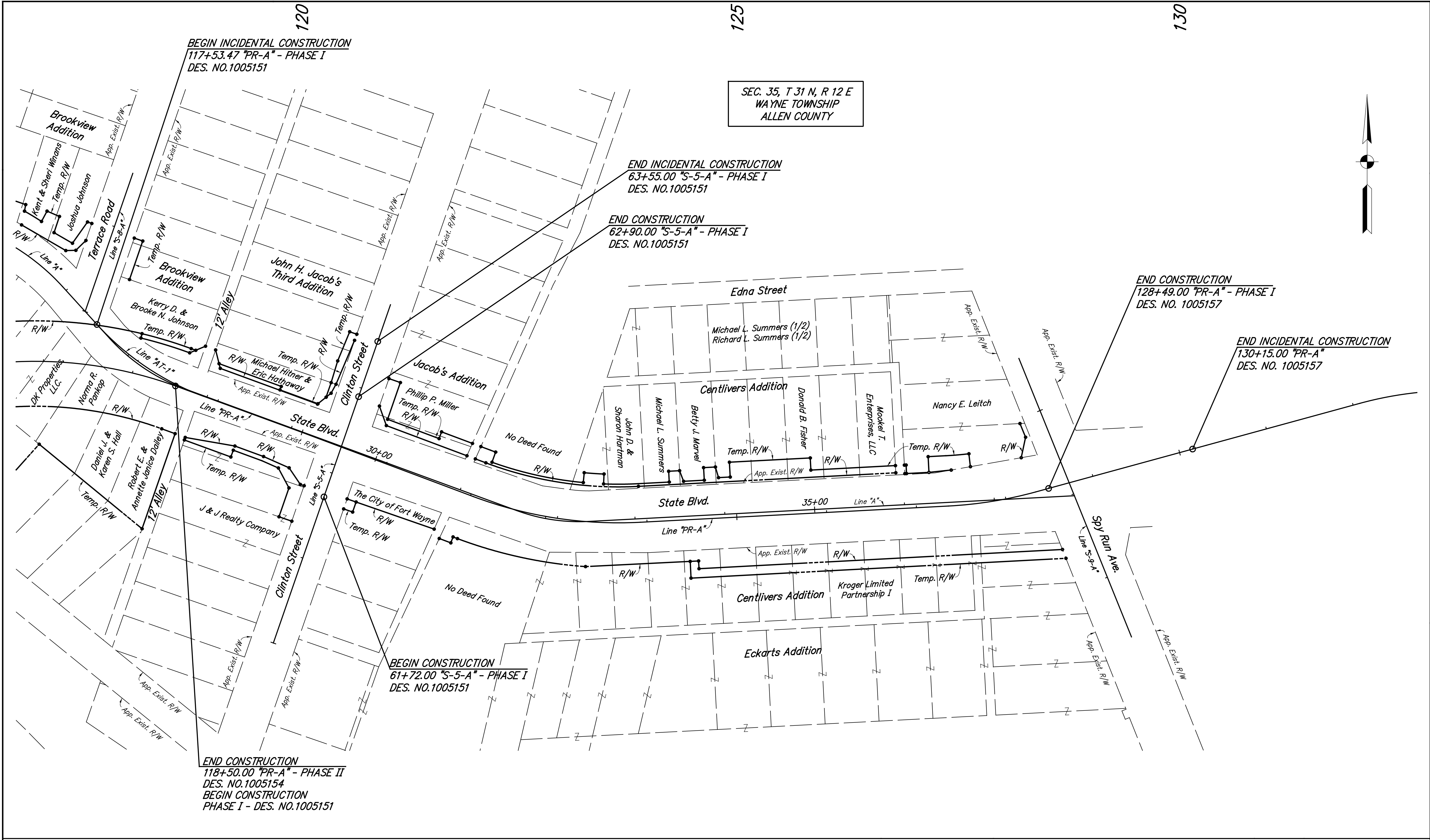
Retaining Wall Detail
20+50.00 to 21+25.00 Rt. "W-3-A"

LEGEND			
(K) xxx #/Syd. QC/QA-HMA, x, xx, Surface, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Intermediate, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Base, xx, on xxx #/Syd. QC/QA-HMA, x, xx, Intermediate DG, 19mm on xxx #/Syd. QC/QA-HMA, x, xx, Base, xx	(F1) 140 #/Syd. HMA Surface, Type "A", on 220 #/Syd. HMA Intermediate, Type "A", on 6" Compacted Aggregate, No. 53, Base	(13) Concrete Curb	(22) Concrete Center Curb, Type "D"
(F) Concrete Sidewalk, 4"	(R) 165 #/Syd. QC/QA-HMA, x, xx, Surface, xx	(14) Integral Concrete Curb	(26) Sodding, Nursery
		(15) Curb and Gutter, Concrete, Modified	(28) Mulched Seeding, Type "U"
		(16) Curb and Gutter, C, Concrete	(69) Retaining Wall

RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
DESIGNED: ASU	DRAWN: CAK	TYPICAL CROSS SECTIONS LINE "W-1-A" & "W-2-A" & "W-3-A"		1/4" = 1'-0"		DESIGNATION NO.	
CHECKED: SMC	CHECKED: SMC			NONE		1005154	
				SURVEY BOOK		SHEETS	
				CONTRACT R-33130		8 of 138	
						PROJECT NO. 1005154	



		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 50'		BRIDGE FILE	
		DESIGNED: ASU		VERTICAL SCALE NONE		DESIGNATION NO. 1005154	
		DRAWN: PJT		SURVEY BOOK		SHEETS	
		CHECKED: SMC		CONTRACT R-33130		9 of 138	
				PROJECT NO. 1005154			



				INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 50'		BRIDGE FILE	
						VERTICAL SCALE NONE		DESIGNATION NO. 1005154	
						SURVEY BOOK		SHEETS	
						CONTRACT R-33130		10 of 138	
								PROJECT NO. 1005154	

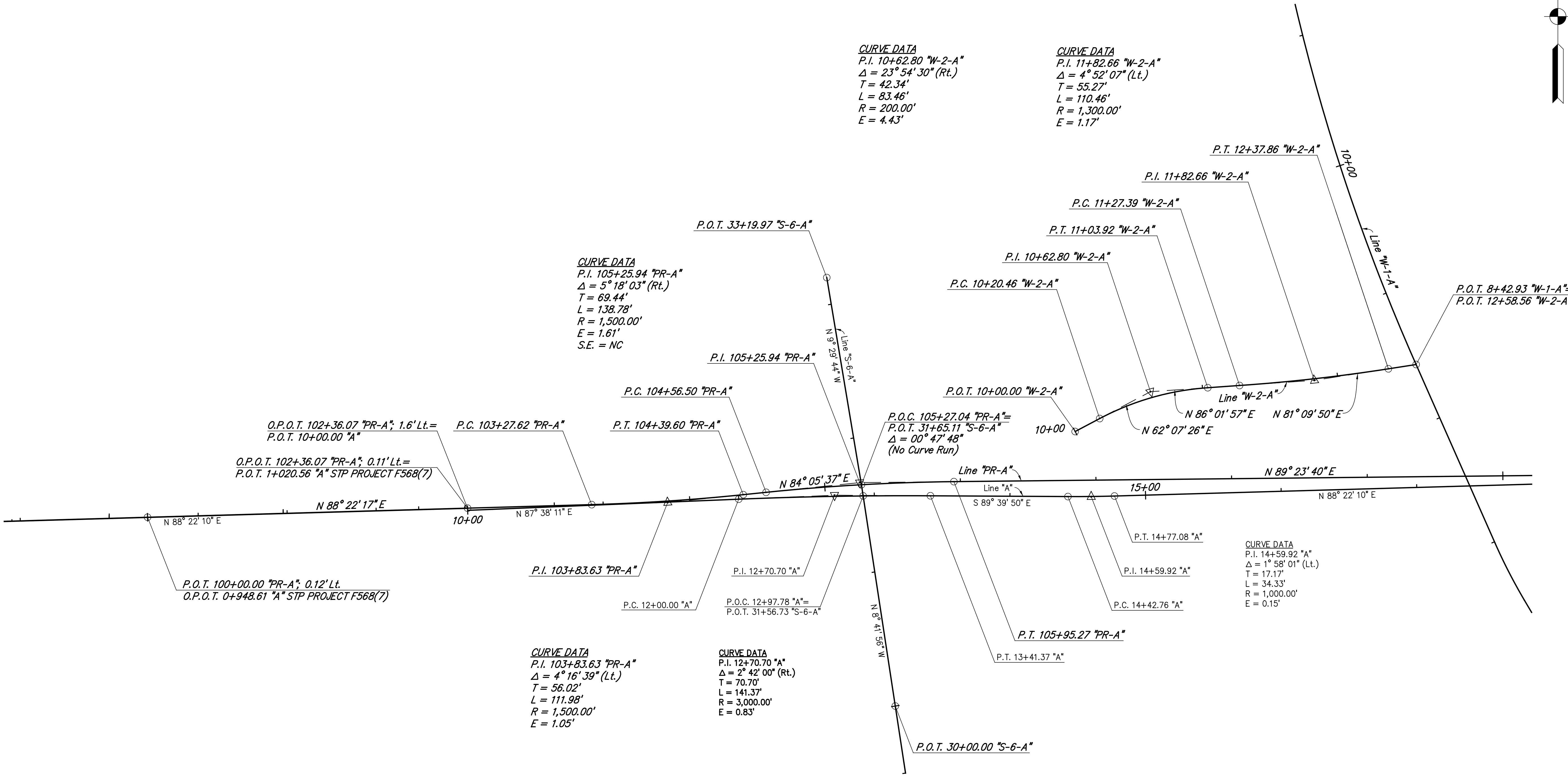
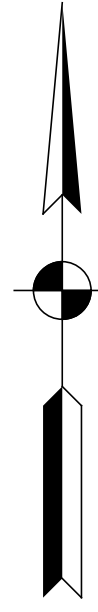
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT			
CHECKED: SMC		CHECKED: SMC			

PLAT NO. 1	
------------	--

100

105

110



Notes:
See Sheet XX for Line "W-1-A" Geometrics.

		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						1" = 40'			
		DESIGN ENGINEER		DATE		VERTICAL SCALE		DESIGNATION NO.	
						NONE		1005154	
		DESIGNED: ASU		DRAWN: CAK		GEOMETRIC TIE-UP			
		CHECKED: SMC		CHECKED: SMC					
						SURVEY BOOK		SHEETS	
						11 of 138			
						CONTRACT		PROJECT NO.	
						R-33130		1005154	

CURVE DATA
P.I. 18+66.60 "A"
Δ = 43° 10' 30" (Lt.)
T = 69.24'
L = 131.87'
R = 175.00'
E = 13.20'

CURVE DATA
P.I. 22+14.84 "A"
Δ = 19° 44' 24" (Rt.)
T = 55.68'
L = 110.25'
R = 320.00'
E = 4.81'

CURVE DATA
P.I. 24+64.47 "A"
Δ = 72° 48' 36" (Rt.)
T = 179.19'
L = 308.80'
R = 243.00'
E = 58.92'

CURVE DATA
P.I. 27+23.73 "A"
Δ = 27° 36' 02" (Lt.)
T = 51.58'
L = 101.16'
R = 210.00'
E = 6.24'

CURVE DATA
P.I. 51+05.62 "S-4-A"
Δ = 62° 31' 02" (Rt.)
T = 105.62'
L = 189.86'
R = 174.00'
E = 29.55'

CURVE DATA
P.I. 51+05.37 "S-7-A"
Δ = 14° 43' 34" (Rt.)
T = 105.37'
L = 209.58'
R = 815.43'
E = 6.78'

CURVE DATA
P.I. 111+75.59 "PR-A"
Δ = 16° 59' 53" (Lt.)
T = 68.74'
L = 136.47'
R = 460.00'
E = 5.11'

CURVE DATA
P.I. 117+06.31 "PR-A"
Δ = 37° 44' 51" (Rt.)
T = 157.26'
L = 303.06'
R = 460.00'
E = 26.14'

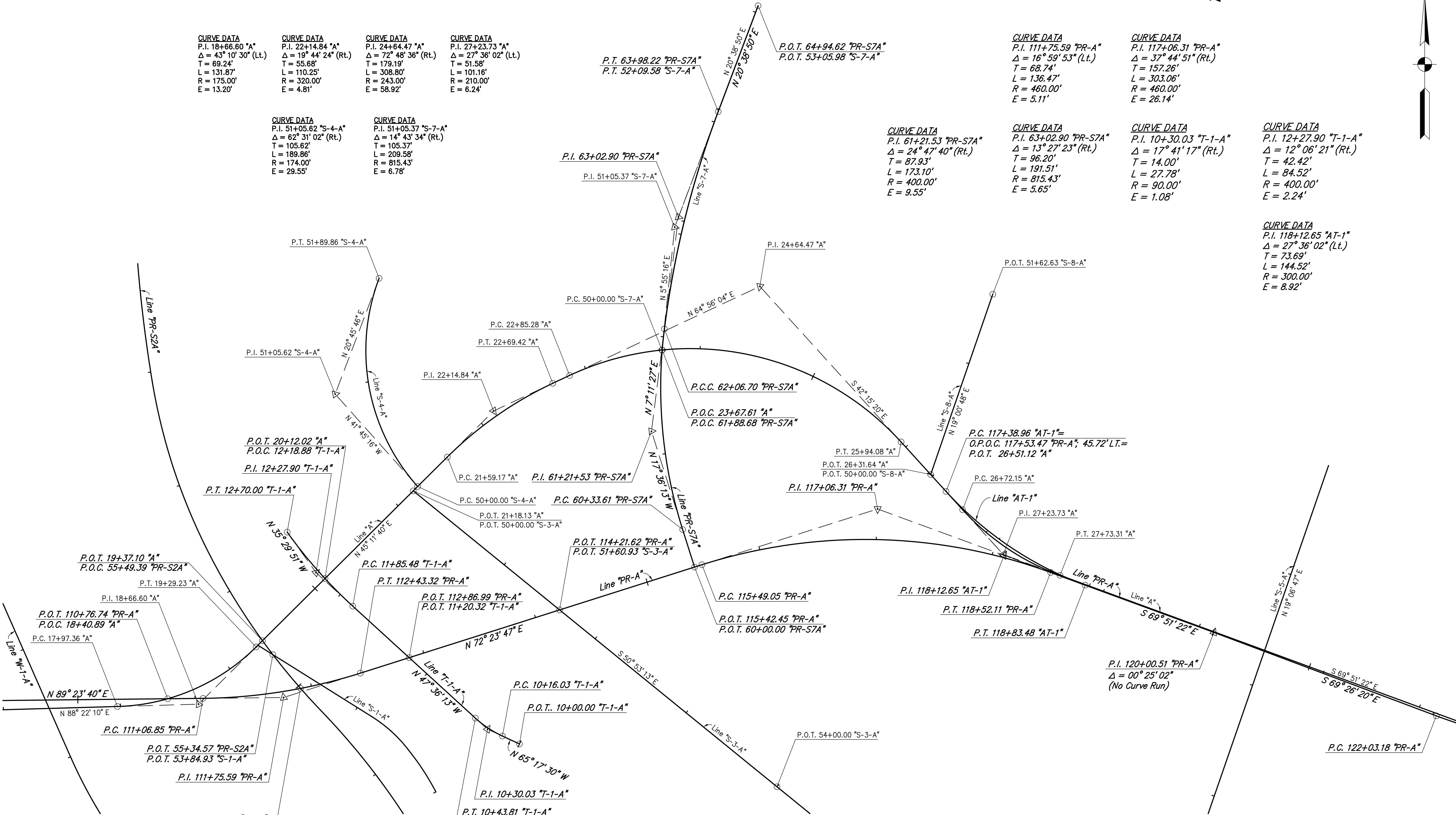
CURVE DATA
P.I. 61+21.53 "PR-S7A"
Δ = 24° 47' 40" (Rt.)
T = 87.93'
L = 173.10'
R = 400.00'
E = 9.55'

CURVE DATA
P.I. 63+02.90 "PR-S7A"
Δ = 13° 27' 23" (Rt.)
T = 96.20'
L = 191.51'
R = 815.43'
E = 5.65'

CURVE DATA
P.I. 10+30.03 "T-1-A"
Δ = 17° 41' 17" (Rt.)
T = 14.00'
L = 27.78'
R = 90.00'
E = 1.08'

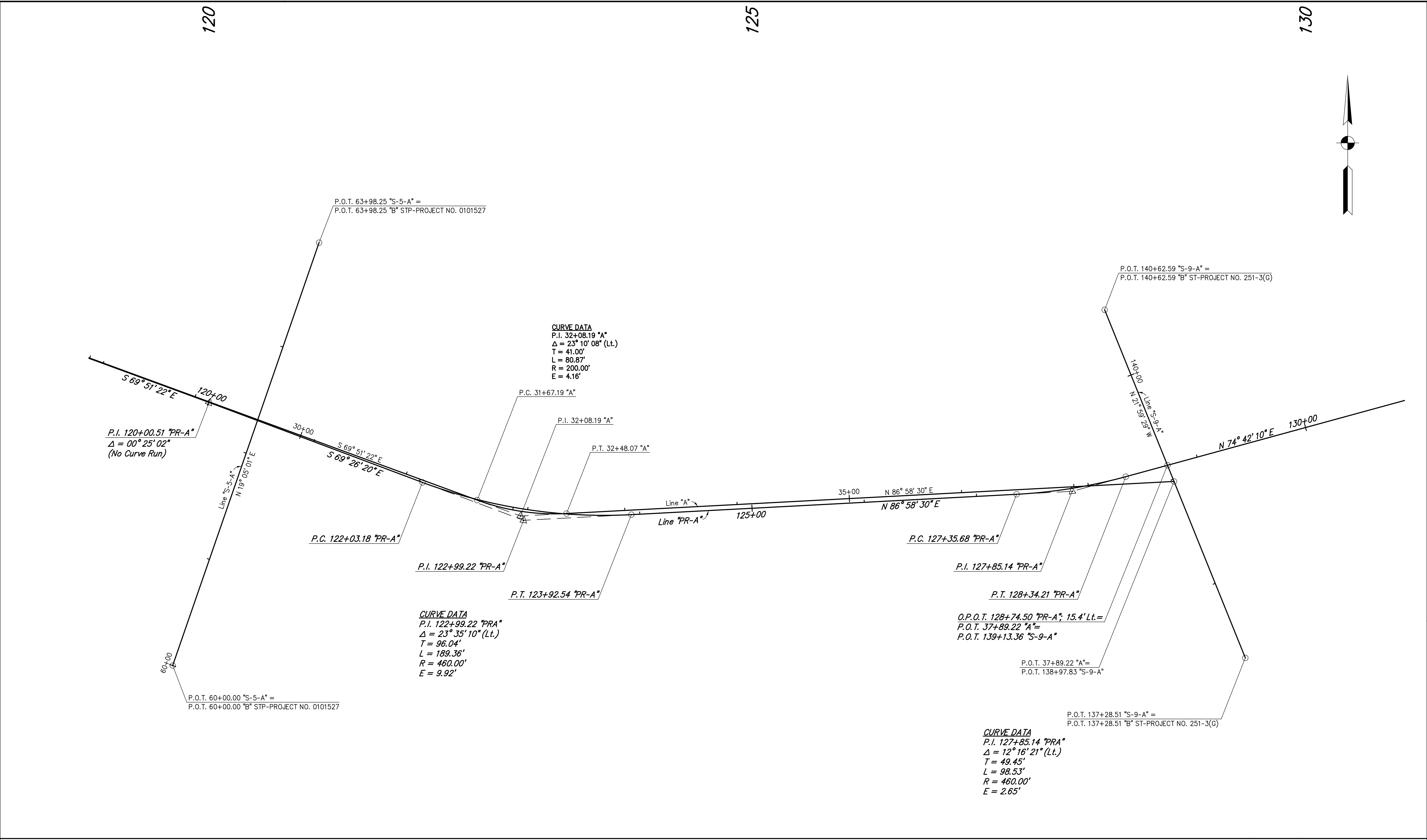
CURVE DATA
P.I. 12+27.90 "T-1-A"
Δ = 12° 06' 21" (Rt.)
T = 42.42'
L = 84.52'
R = 400.00'
E = 2.24'

CURVE DATA
P.I. 118+12.65 "AT-1"
Δ = 27° 36' 02" (Lt.)
T = 73.69'
L = 144.52'
R = 300.00'
E = 8.92'

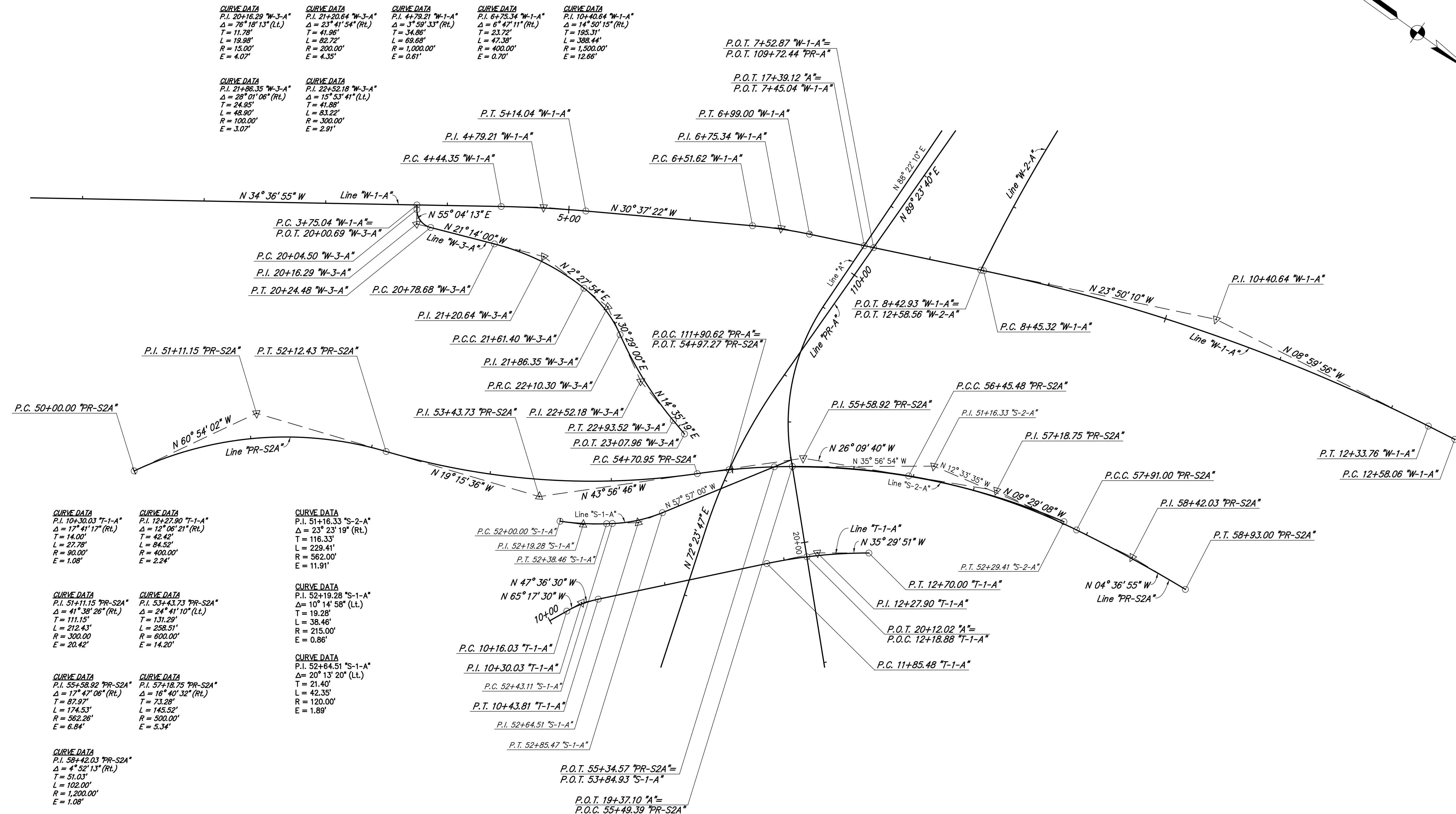
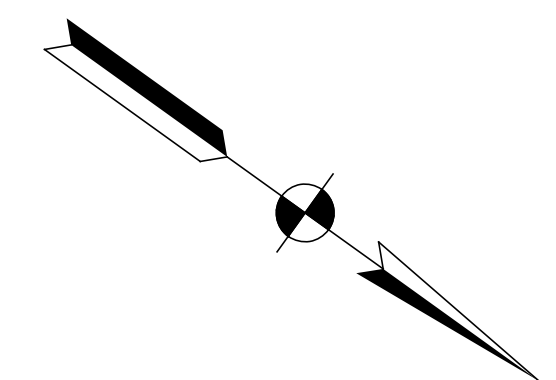


Notes:
See Sheets XX for Line "S-1-A", "S-2-A", "S-5-A", "W-1-A", "PR-S2A" Geometrics.

		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE		BRIDGE FILE	
								1" = 40'			
								VERTICAL SCALE		DESIGNATION NO.	
								NONE		1005154	
		DESIGNED: ASU		DRAWN: CAK			GEOMETRIC TIE-UP	SURVEY BOOK		SHEETS	
								12 of 138			
		CHECKED: SMC		CHECKED: SMC				CONTRACT		PROJECT NO.	
							R-33130		1005154		

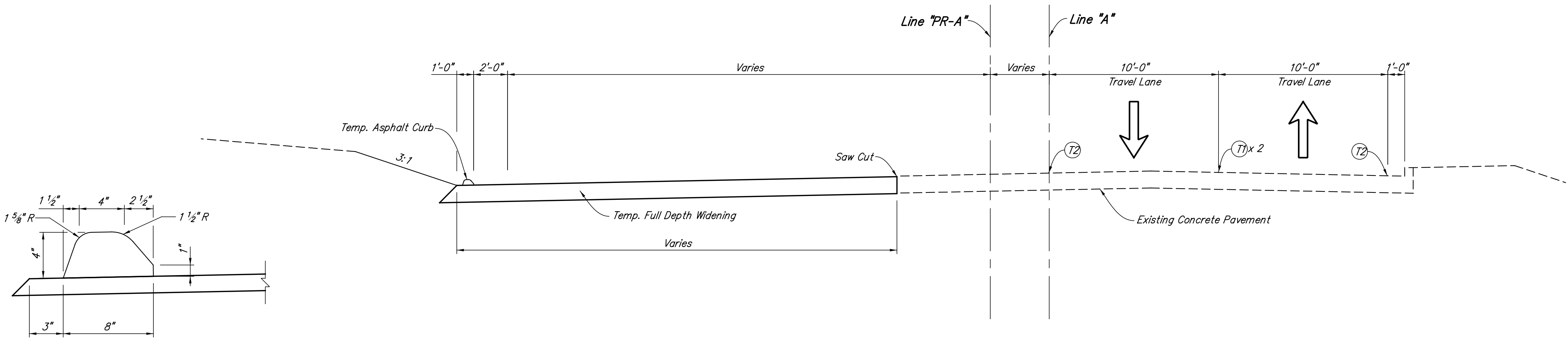


		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						1" = 40'			
						VERTICAL SCALE		DESIGNATION NO.	
						NONE		1005154	
		DESIGNED: ASU DRAWN: CAK CHECKED: SMC		GEOMETRIC TIE-UP		SURVEY BOOK		SHEETS	
								13 of 138	
						CONTRACT		PROJECT NO.	
						R-33130		1005154	



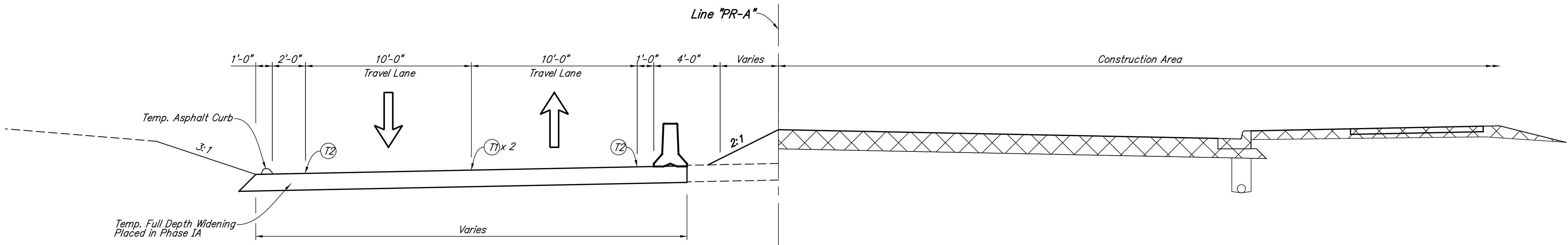
Notes:
See Sheets XX for Line "A", "PR-A", "W-2-A", "T-1-A", "PR-S2A" Geometrics.

		INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
				HORIZONTAL SCALE	1" = 40'
				VERTICAL SCALE	NONE
				DESIGNATION NO.	1005154
		GEOMETRIC TIE-UP		SHEETS	
				14	of 138
				PROJECT NO.	
				1005154	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK			
CHECKED: SMC		CHECKED: SMC			

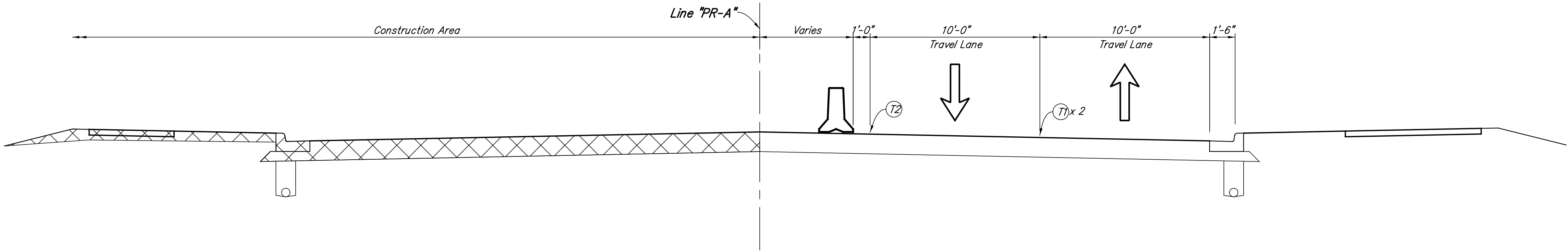


ASPHALT CURB (MOUNTABLE) DETAIL

PHASE I-A



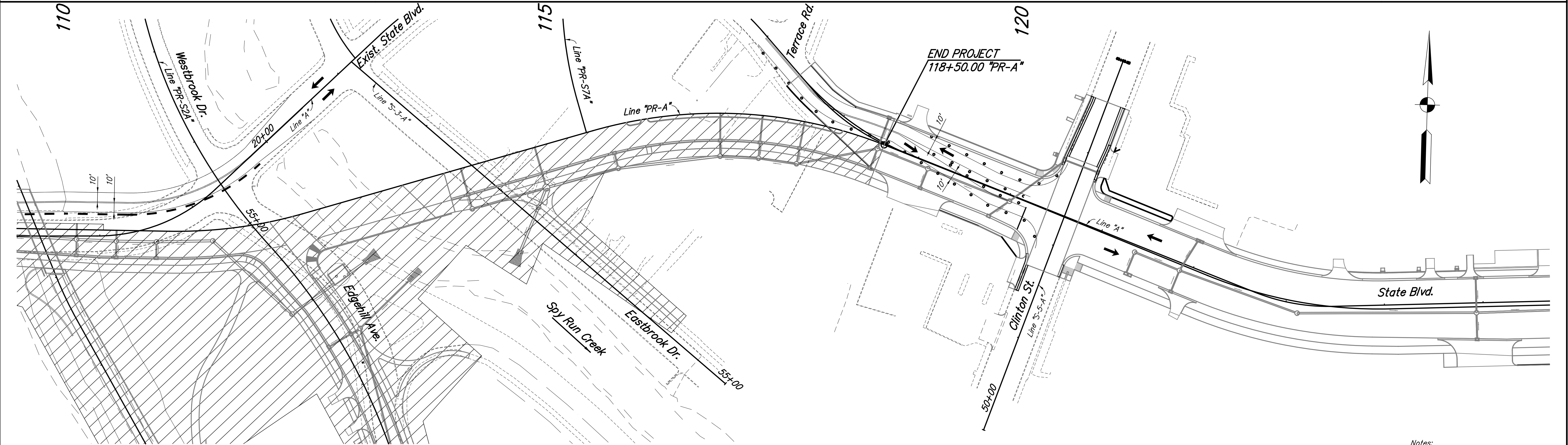
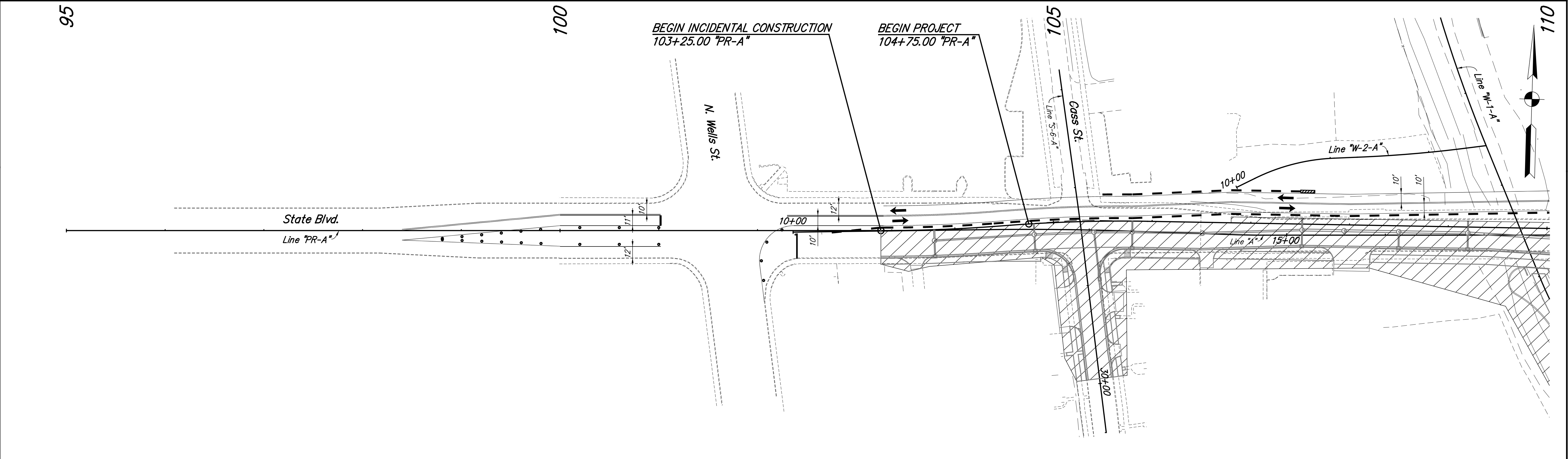
PHASE I



PHASE II

Notes: HMA Surface to be Placed Following Phase II' Under Live Traffic

Temporary Concrete Barrier Standard Drum <p>LEGEND</p> <p>(T1) Temporary Pavement Marking, Solid, Yellow, 4 in.</p> <p>(T2) Temporary Pavement Marking, Solid, White, 4 in.</p> <p>Construction Area</p>		<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER DATE</p>	<p>INDIANA DEPARTMENT OF TRANSPORTATION</p>		<p>HORIZONTAL SCALE</p> <p>1/4" = 1'-0"</p>	<p>BRIDGE FILE</p>	
		<p>DESIGNED: ASU</p> <p>CHECKED: SMC</p>	<p>DRAWN: CAK</p> <p>CHECKED: SMC</p>	<p>MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS</p>		<p>VERTICAL SCALE</p> <p>NONE</p>	<p>DESIGNATION NO.</p> <p>1005154</p>
				<p>SURVEY BOOK</p> <p>R-33130</p>	<p>SHEETS</p> <p>15 of 138</p>		
				<p>PROJECT NO.</p> <p>1005154</p>			



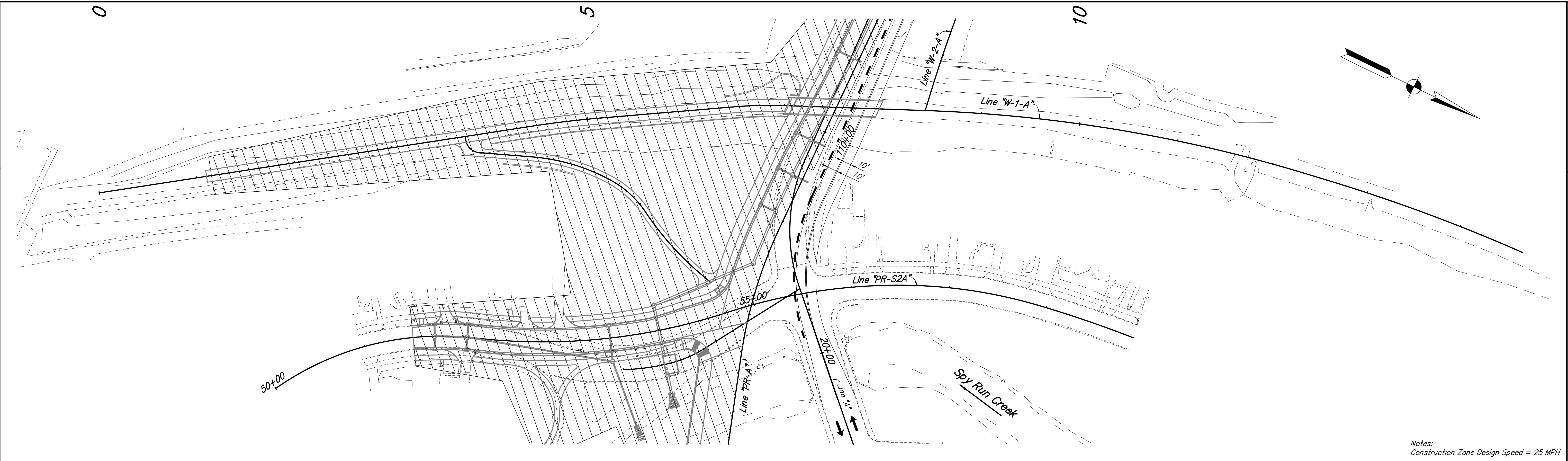
Notes:
Construction Zone Design Speed = 25 MPH

LEGEND			
----- Temporary Barrier	Construction Area	--- Type "B" Construction Warning Light	(T1) Temporary Pavement Marking, Solid, Yellow, 4 in.
--- Detour Route	Temp. Pavement Area	--- Type III Barricade (See Callout for Type)	(T2) Temporary Pavement Marking, Solid, White, 4 in.
Direction of Traffic	Standard Drum	Construction Sign w/ Light Warning	(T3) Temporary Pavement Marking, Solid, White, 24 in.

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC PHASE I	

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005154	
SURVEY BOOK	SHEETS	
CONTRACT R-33130	16 of 138	PROJECT NO. 1005154



Notes:
Construction Zone Design Speed = 25 MPH

----- Temporary Barrier

--- Detour Route

↓ Direction of Traffic

Construction Area

Temp. Pavement Area

Standard Drum

Type "B" Construction Warning Light

Type III Barricade (See Callout for Type)

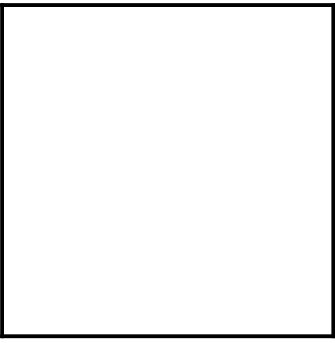
Construction Sign w/ Light Warning

Temporary Pavement Marking, Solid, Yellow, 4 in.

Temporary Pavement Marking, Solid, White, 4 in.

Temporary Pavement Marking, Solid, White, 24 in.

LEGEND

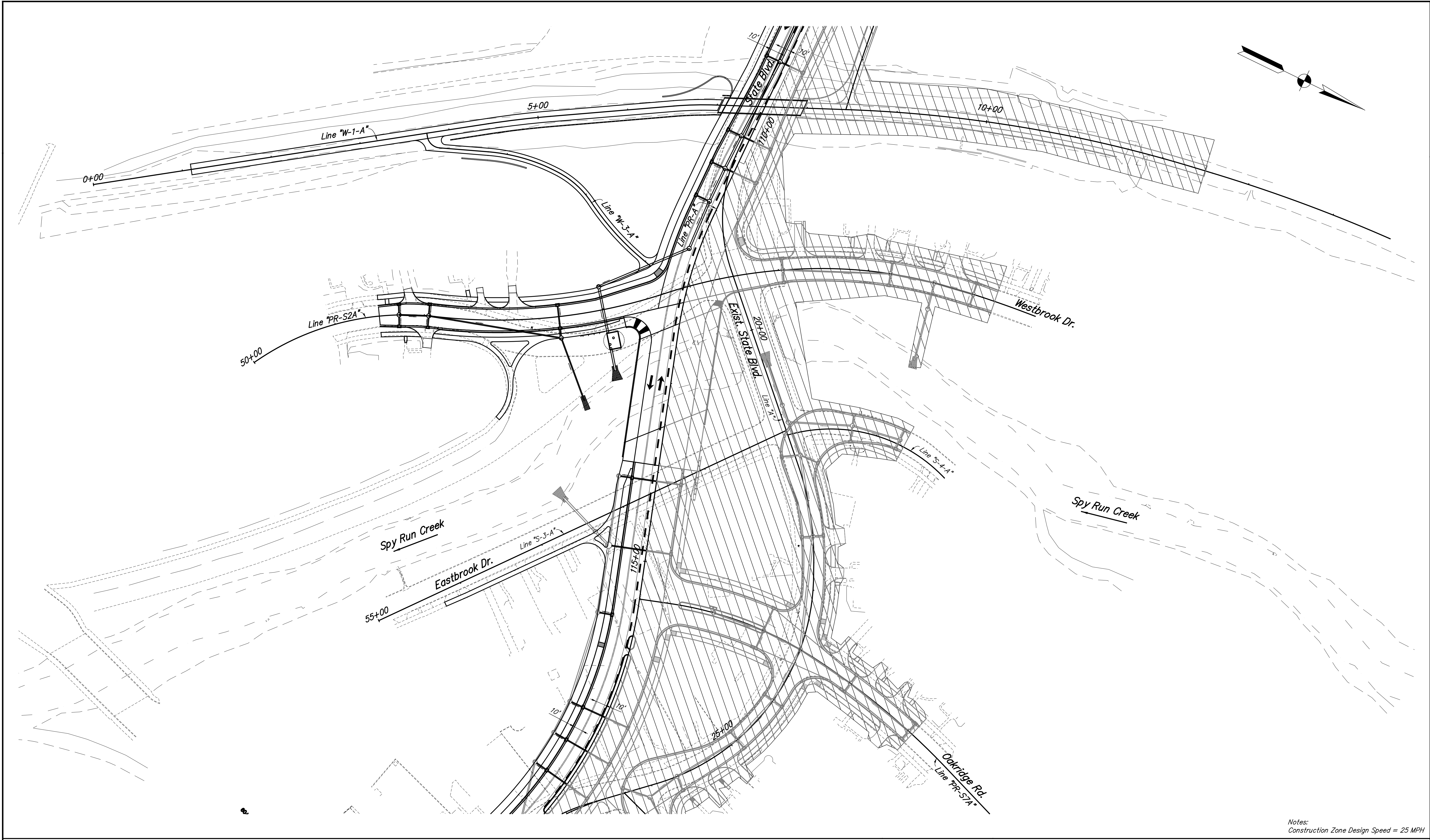


RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED:	ASU	DRAWN: PJT
CHECKED:	SMC	CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE I

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005154	
SURVEY BOOK	SHEETS	
	17	of 138
CONTRACT R-33130	PROJECT NO. 1005154	



Notes:
Construction Zone Design Speed = 25 MPH

LEGEND

----- Temporary Barrier

--- Detour Route

↓ Direction of Traffic

Construction Area

Temp. Pavement Area

Standard Drum

Type "B" Construction Warning Light

Type III Barricade (See Callout for Type)

Construction Sign w/ Light Warning

Temporary Pavement Marking, Solid, Yellow, 4 in.

Temporary Pavement Marking, Solid, White, 4 in.

Temporary Pavement Marking, Solid, White, 24 in.

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU

CHECKED: SMC

DRAWN: PJT

CHECKED: SMC

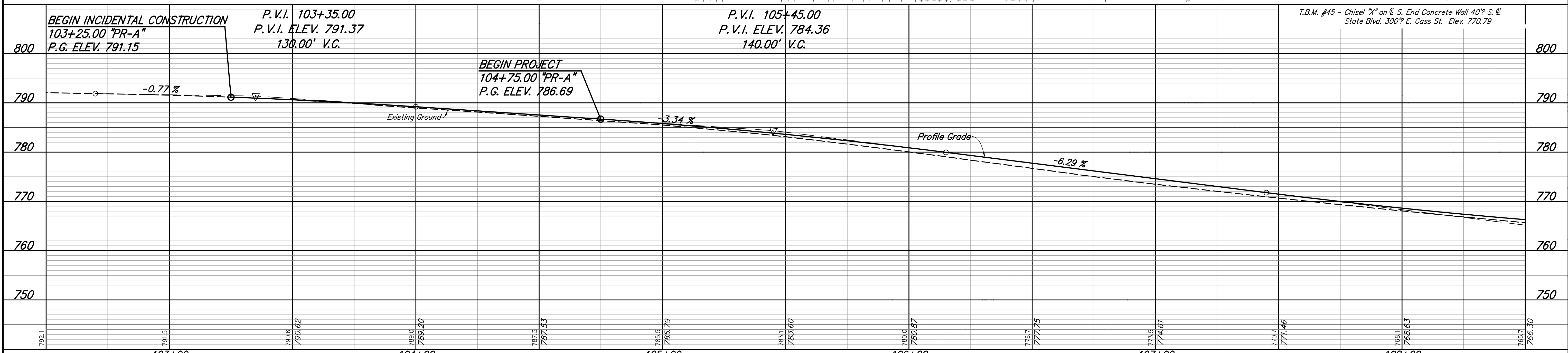
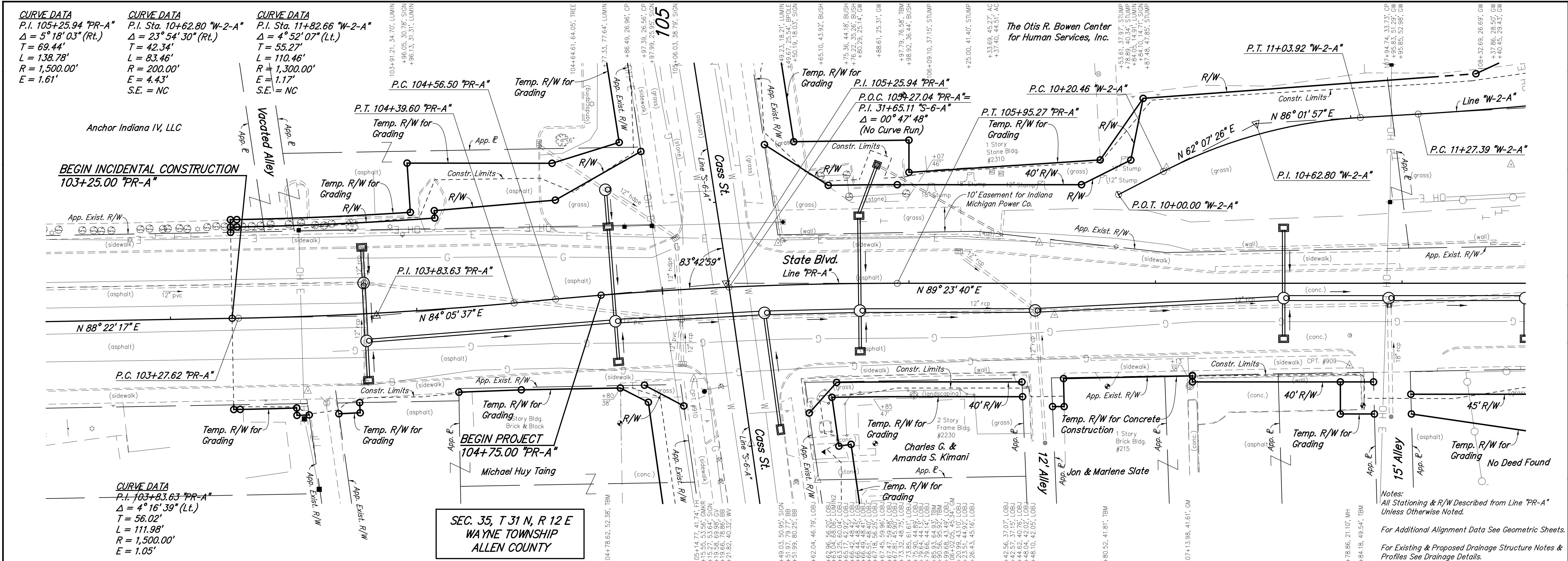
INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE II

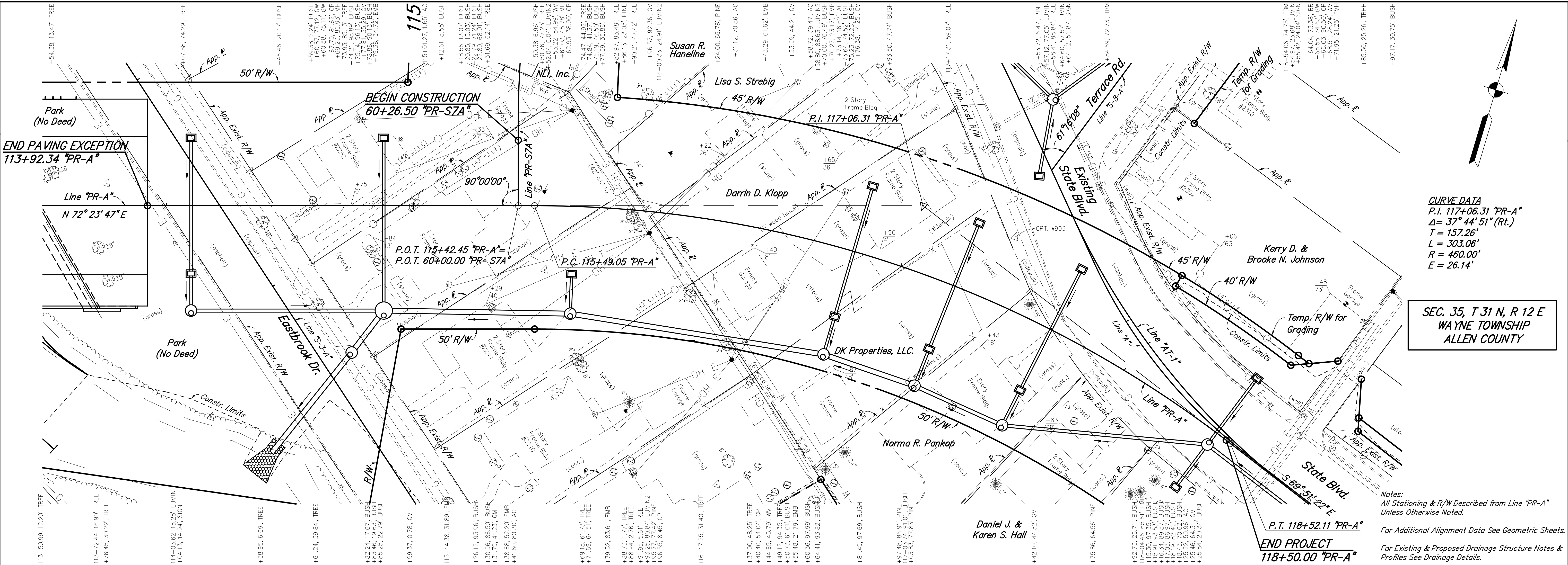
HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	
VERTICAL SCALE	DESIGNATION NO.
NONE	1005154
SURVEY BOOK	SHEETS
	20 of 138
CONTRACT	PROJECT NO.
R-33130	1005154

ckramer 1/23/2012 3:53:01 PM P:*IN2007\1404\RD.D Drawings\20071404.RD.P2.TP.05.PH2.dgn

Appendix A
Page 80 of 129



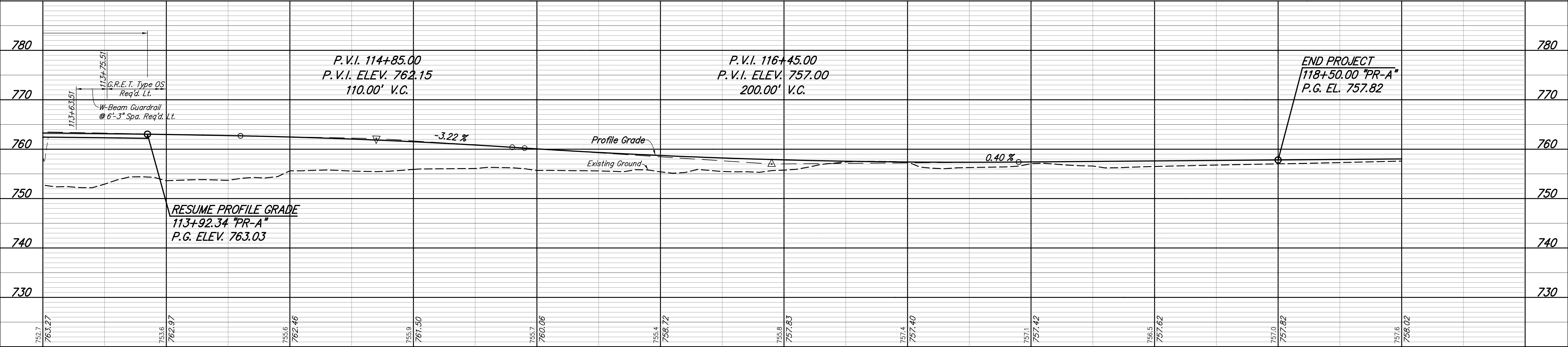
103+00		104+00		105+00		106+00		107+00		108+00	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		PLAN & PROFILE LINE "PR-A"		BRIDGE FILE	
DESIGNED: ASU		DRAWN: PJT		CHECKED: SMC		HORIZONTAL SCALE 1" = 20'		VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1005154	
CHECKED: SMC		CHECKED: SMC				SURVEY BOOK		SHEETS 21 of 138		PROJECT NO. 1005154	
						CONTRACT R-33130					



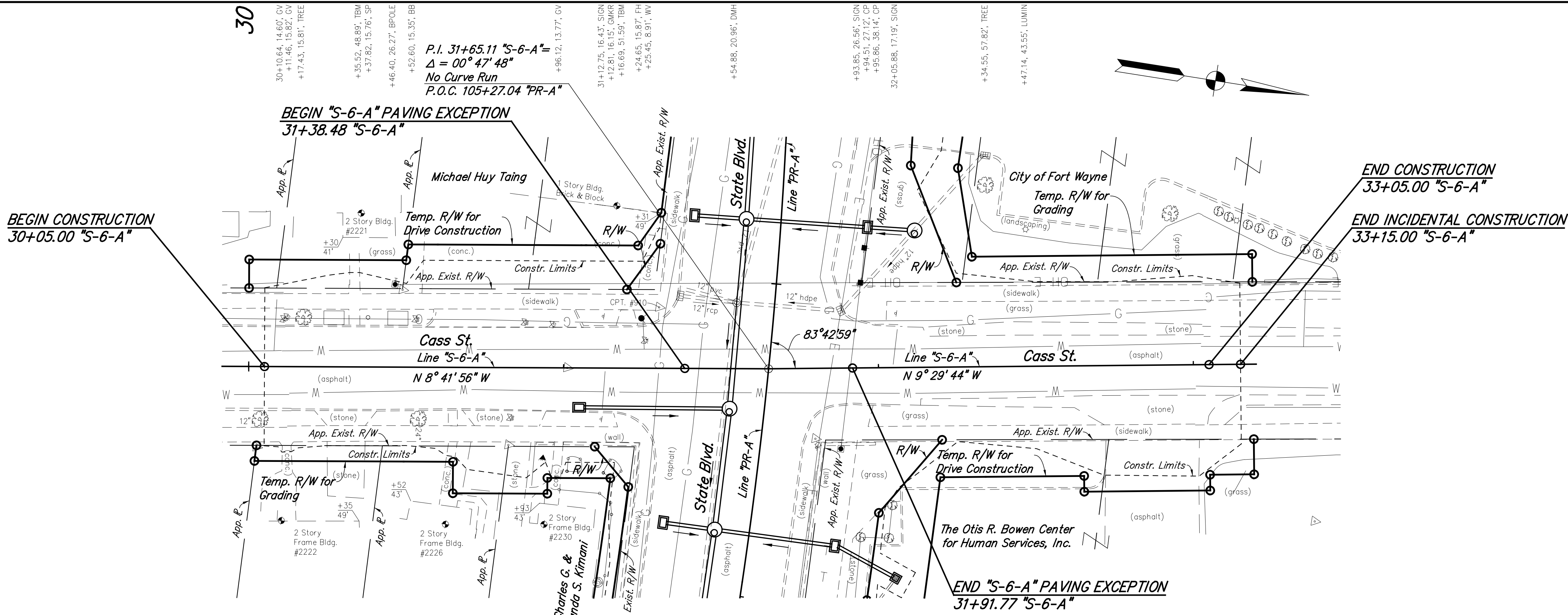
CURVE DATA
P.I. 117+06.31 "PR-A"
 $\Delta = 37^\circ 44' 51''$ (Rt.)
 $T = 157.26'$
 $L = 303.06'$
 $R = 460.00'$
 $E = 26.14'$

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

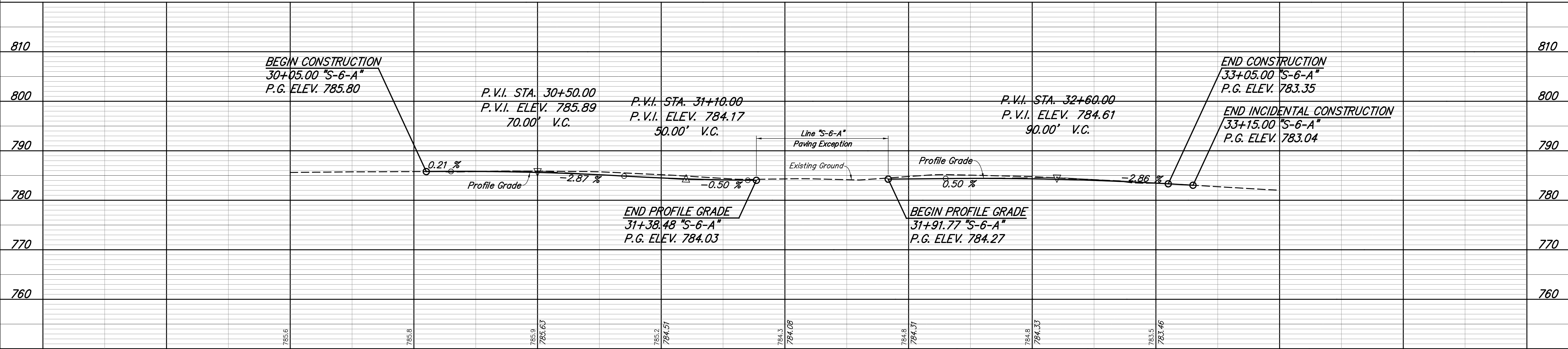
Notes:
All Stationing & R/W Described from Line "PR-A"
Unless Otherwise Noted.
For Additional Alignment Data See Geometric Sheets.
For Existing & Proposed Drainage Structure Notes & Profiles See Drainage Details.



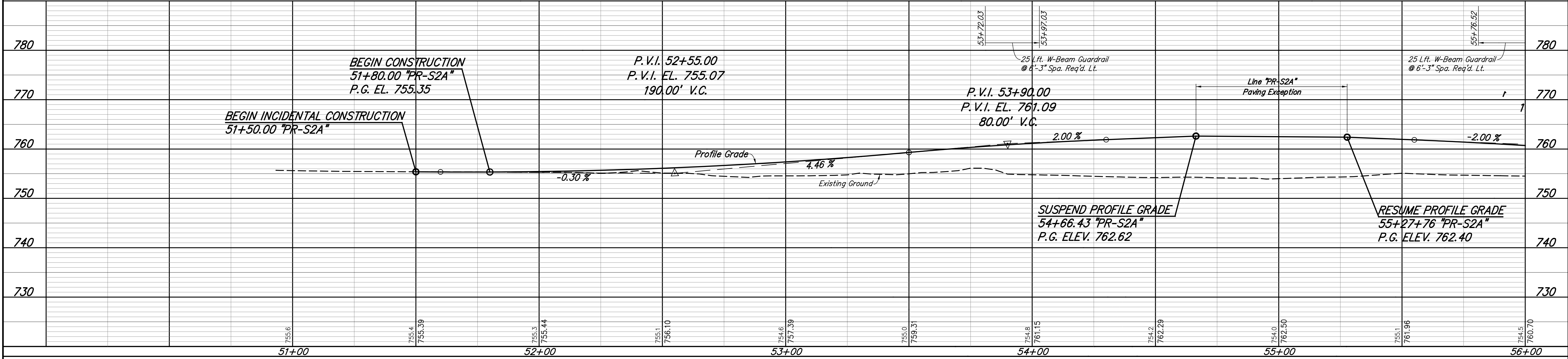
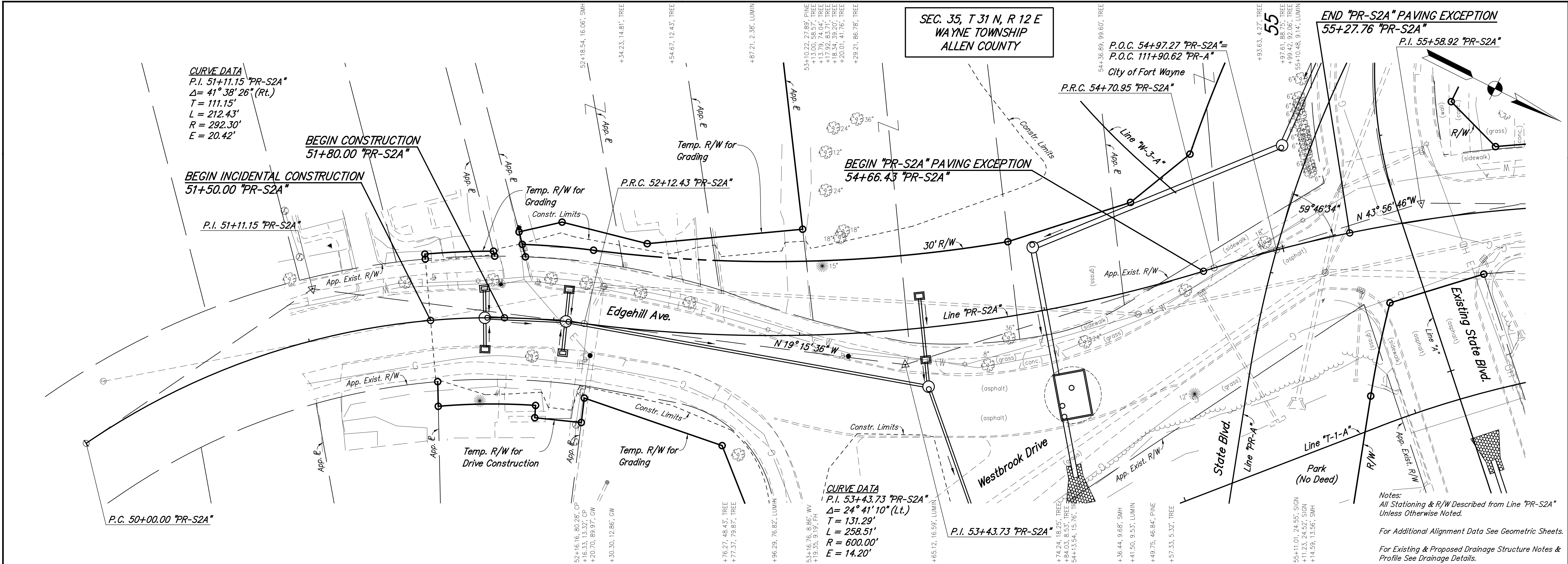
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		DESIGNED: ASU	DRAWN: PJT	CHECKED: SMC	CHECKED: SMC		
INDIANA DEPARTMENT OF TRANSPORTATION PLAN & PROFILE LINE "PR-A"						HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
						VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005154
						SURVEY BOOK	SHEETS
						CONTRACT R-33130	23 of 138 PROJECT NO. 1005154



Notes:
All Stationing & R/W Described from Line "S-6-A"
Unless Otherwise Noted.
For Additional Alignment Data See Geometric Sheets.



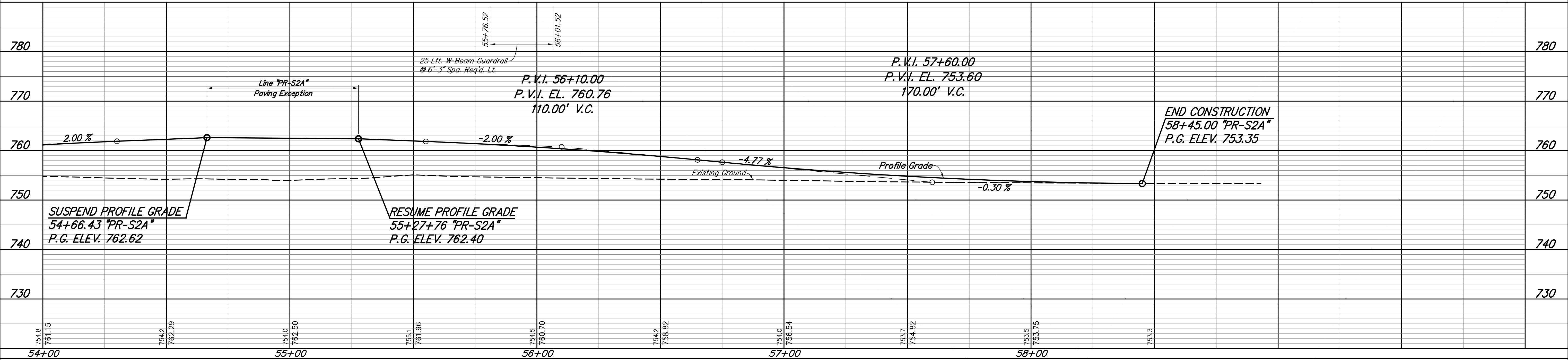
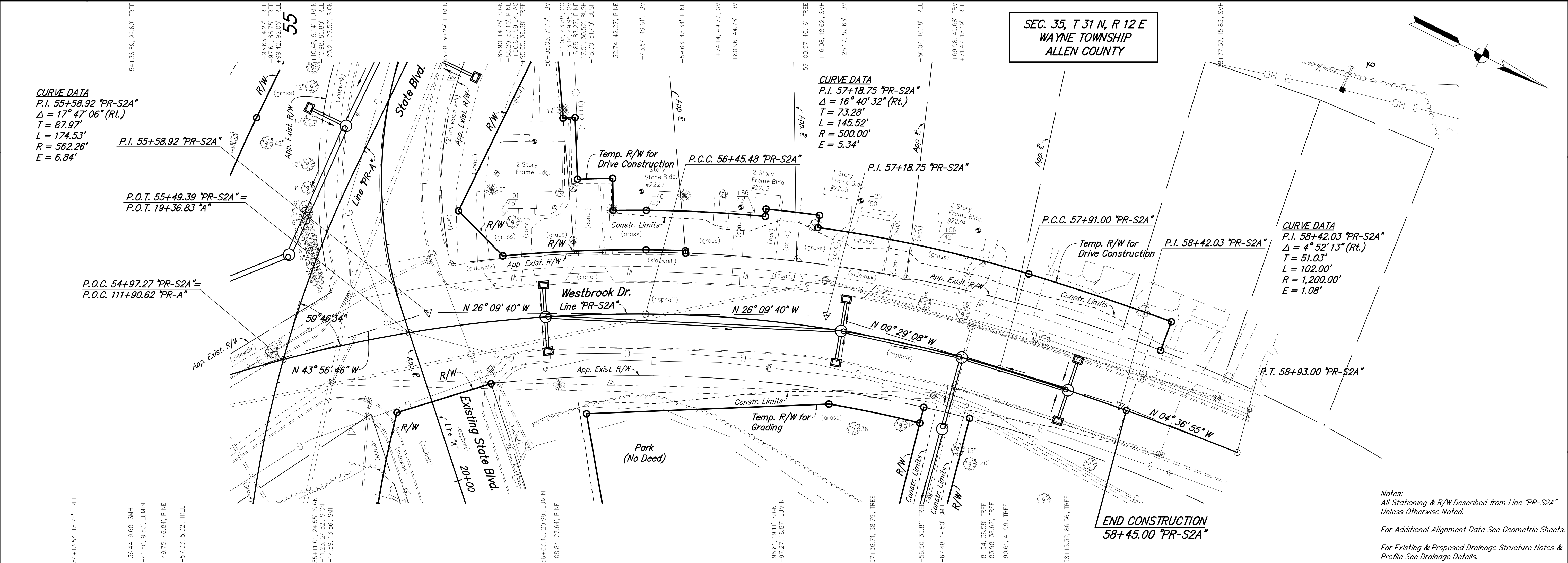
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		DESIGN ENGINEER _____		DATE _____		VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1005154	
		DESIGNED: ASU		DRAWN: PJT		SURVEY BOOK		SHEETS 24 of 138	
		CHECKED: SMC		CHECKED: SMC		CONTRACT R-33130		PROJECT NO. 1005154	
						PLAN & PROFILE LINE "S-6-A"			



RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE
	DESIGNED: ASU	DRAWN: PJT	
	CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN & PROFILE LINE "PR-S2A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS 25 of 138
CONTRACT R-33130	PROJECT NO. 1005154



RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
DESIGNED: ASU	DRAWN: PJT	CHECKED: SMC	PLAN & PROFILE LINE "PR-S2A"	DESIGNATION NO.	1005154
CHECKED: SMC	DRAWN: PJT	CHECKED: SMC		SHEETS	26 of 138
				PROJECT NO.	1005154

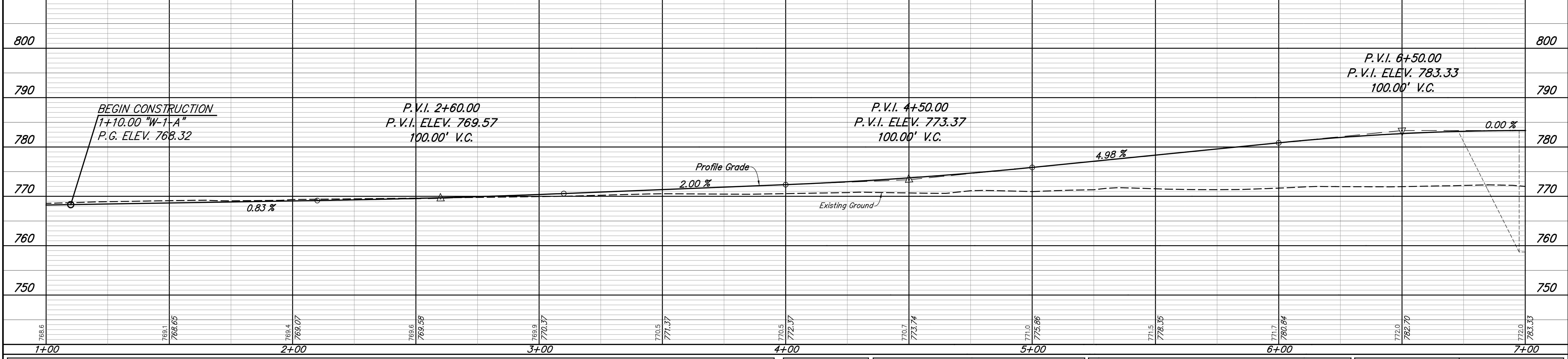
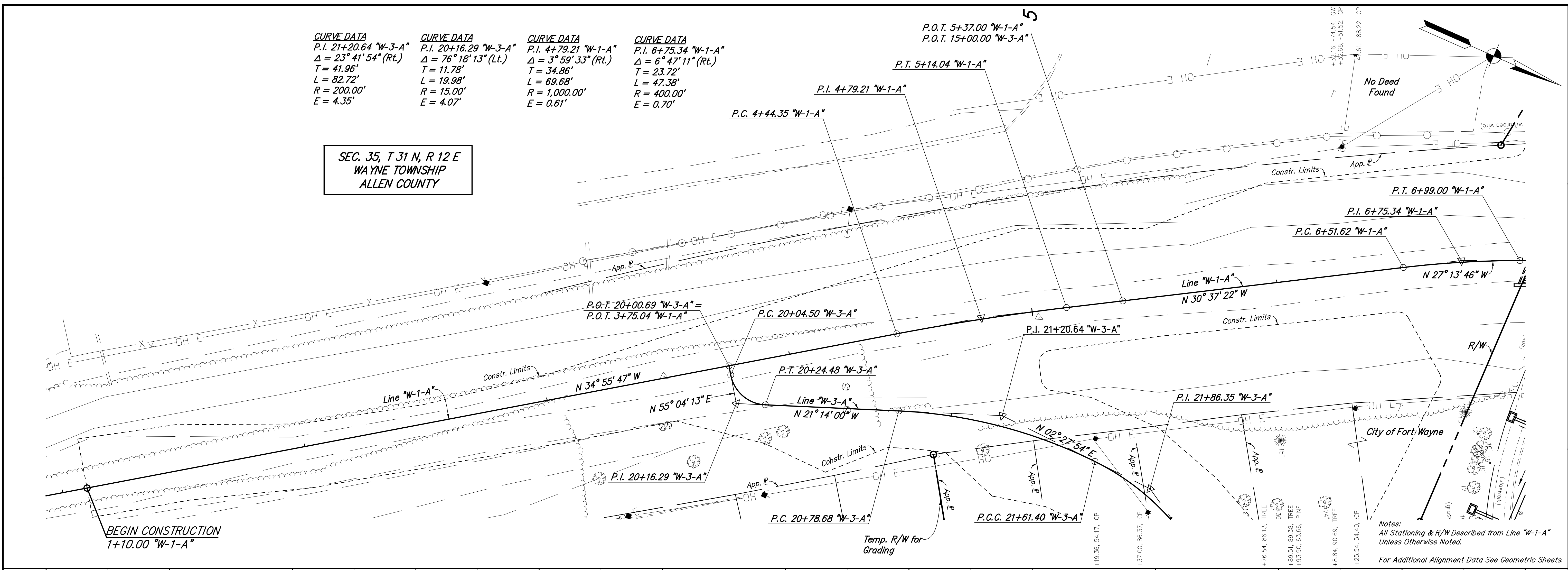
CURVE DATA
P.I. 21+20.64 "W-3-A"
Δ = 23° 41' 54" (Rt.)
T = 41.96'
L = 82.72'
R = 200.00'
E = 4.35'

CURVE DATA
P.I. 20+16.29 "W-3-A"
Δ = 76° 18' 13" (Lt.)
T = 11.78'
L = 19.98'
R = 15.00'
E = 4.07'

CURVE DATA
P.I. 4+79.21 "W-1-A"
Δ = 3° 59' 33" (Rt.)
T = 34.86'
L = 69.68'
R = 1,000.00'
E = 0.61'

CURVE DATA
P.I. 6+75.34 "W-1-A"
Δ = 6° 47' 11" (Rt.)
T = 23.72'
L = 47.38'
R = 400.00'
E = 0.70'

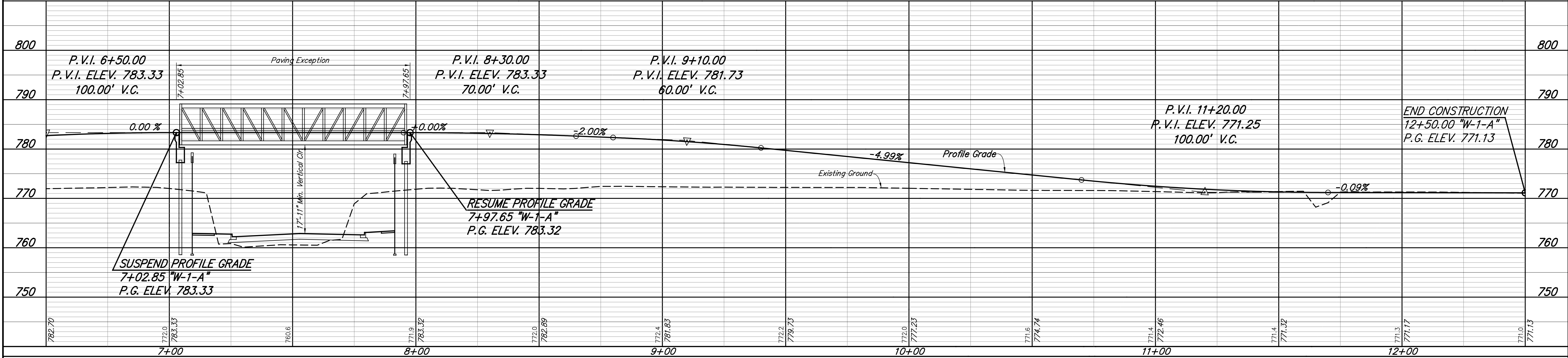
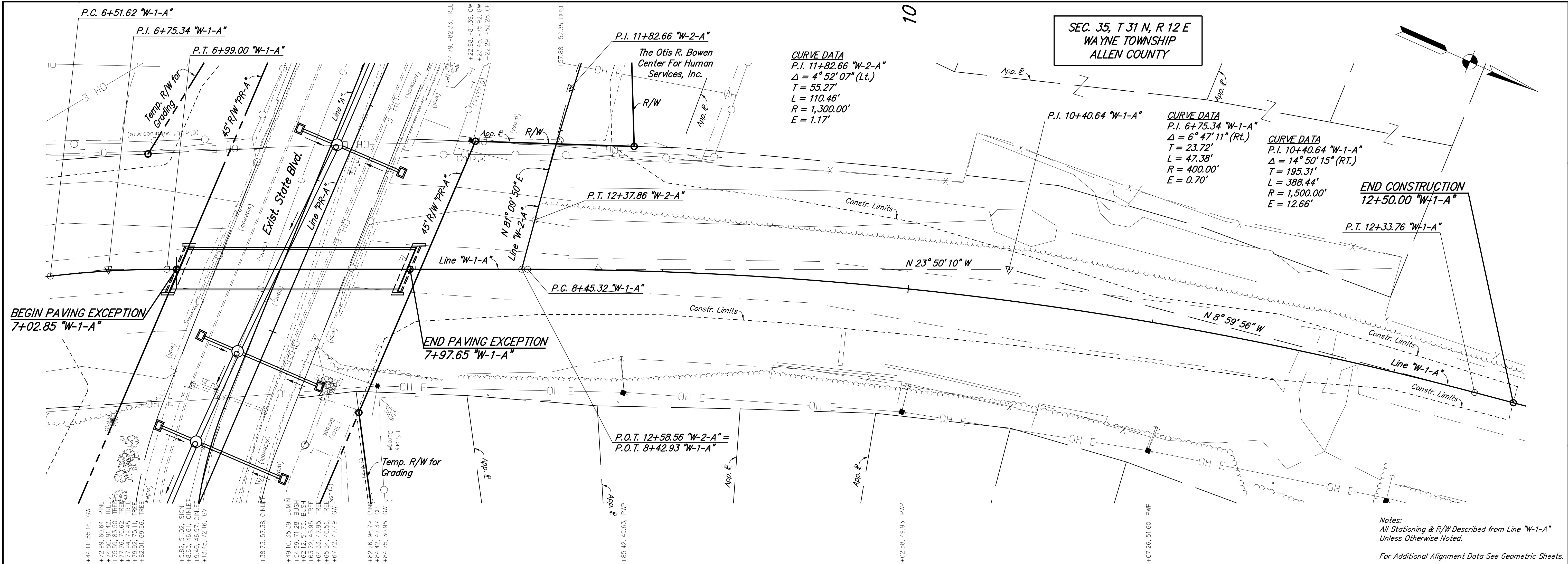
SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



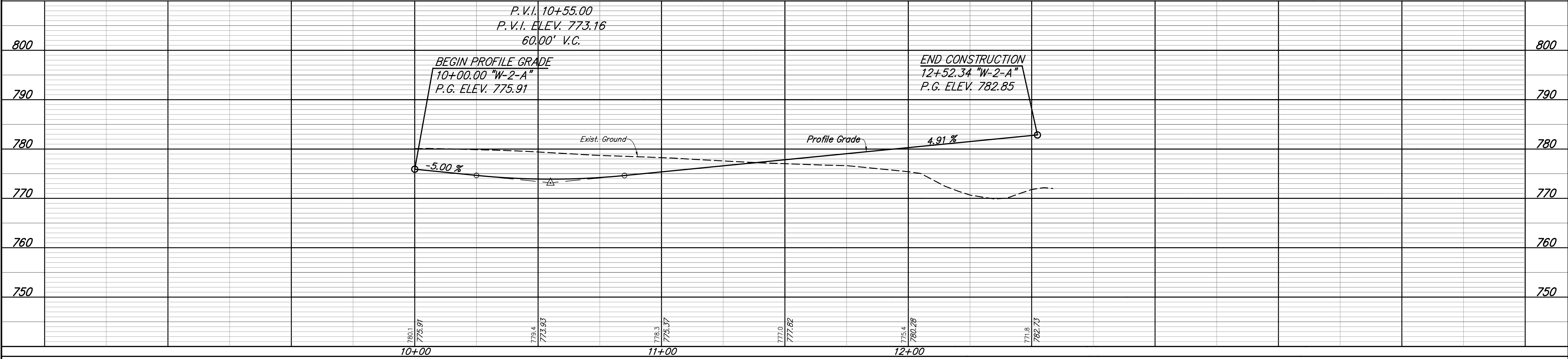
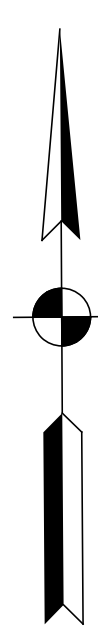
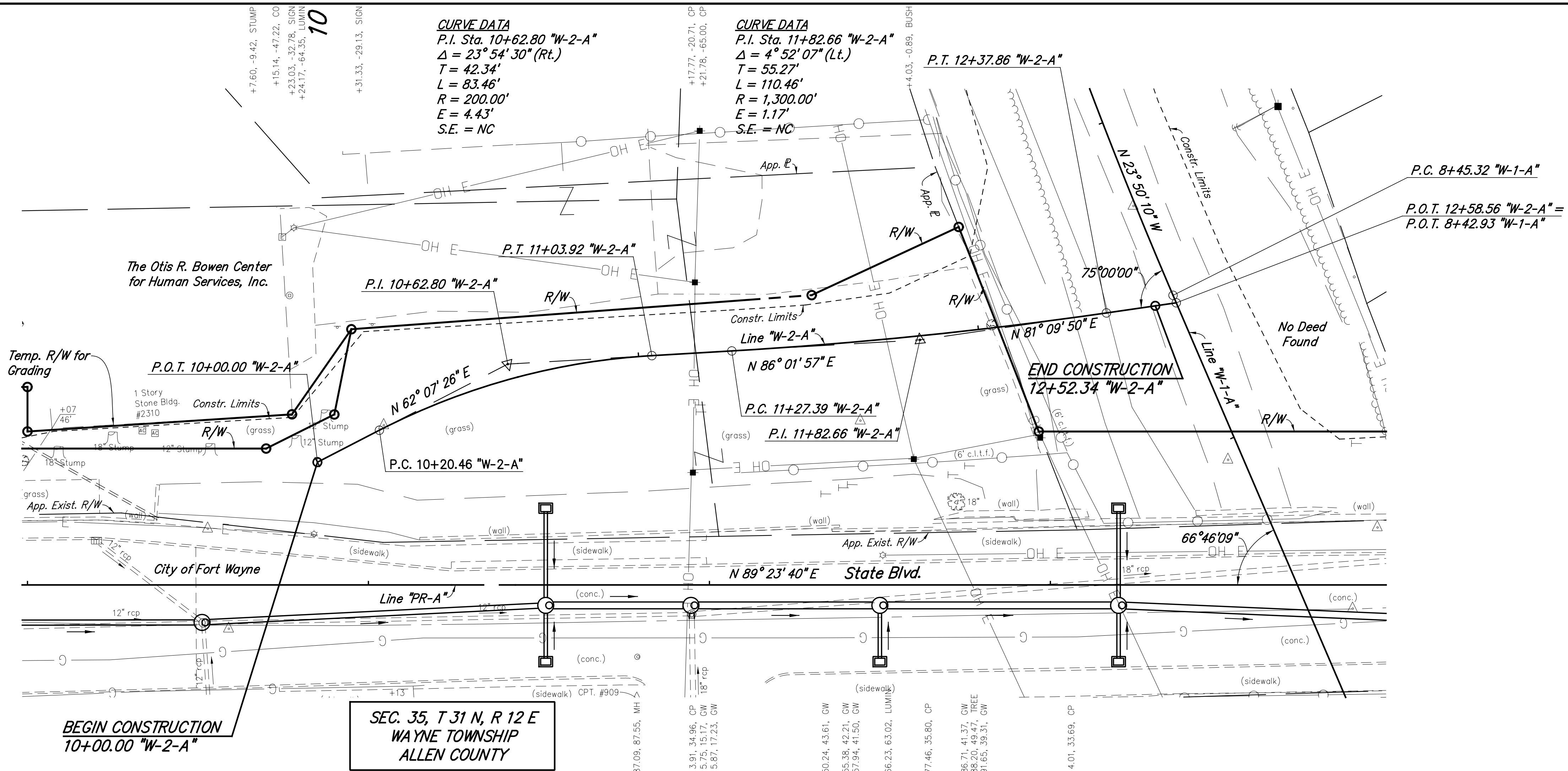
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DESIGNED: ASU		DRAWN: PJT			
CHECKED: SMC		CHECKED: SMC			

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN & PROFILE LINE "W-1-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS 29 of 138
CONTRACT R-33130	PROJECT NO. 1005154



		INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT		PROJECT NO.	
CHECKED: SMC		CHECKED: SMC		SHEETS	
				30 of 138	
				CONTRACT	
				R-33130	
				PROJECT NO.	
				1005154	



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE
		ASU	PJT	
CHECKED: SMC		CHECKED: SMC		

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN & PROFILE LINE "W-2-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS 31 of 138
CONTRACT R-33130	PROJECT NO. 1005154

P.I. Sta. 20+16.29 "W-3-A"
 $\Delta = 76^{\circ}18'13''$ (Lt.)
 $T = 11.78'$
 $L = 19.98'$
 $R = 15.00'$
 $E = 4.07'$
 $S.E. = NC$

P.l. Sta. 21+20.64 "W-3-A"
 $\Delta = 23^{\circ} 41' 54''$ (Rt.)
 $T = 41.96'$
 $L = 82.72'$
 $R = 200.00'$
 $E = 4.35'$
 $S.E. = NC$

P.I. Sta. 21+86.35 "W-3-A"
 $\Delta = 28^{\circ} 01' 06''$ (Rt.)
 $T = 24.95'$
 $L = 48.90'$
 $R = 100.00'$
 $E = 3.07'$
 $S.E. = NC$

P.I. Sta. 22+52.18 "W-3-A"
 $\Delta = 15^{\circ} 53' 41''$ (Lt.)
 $T = 41.88'$
 $L = 83.22'$
 $R = 300.00'$
 $E = 2.91'$
 $S.E. = NC$

$$\frac{P.O.T. 20+00.00 \text{ "W-3-A"} =}{P.O.T. 3+75.00 \text{ "W-1-A"}}$$

P.C. 20+04.50 "W-3-A"

BEGIN CONSTRUCTION
20+06.00 "W-3-A"

P.I. 20+16.29 "W-3-A"

P.T. 20+24.48 "W-3-A"

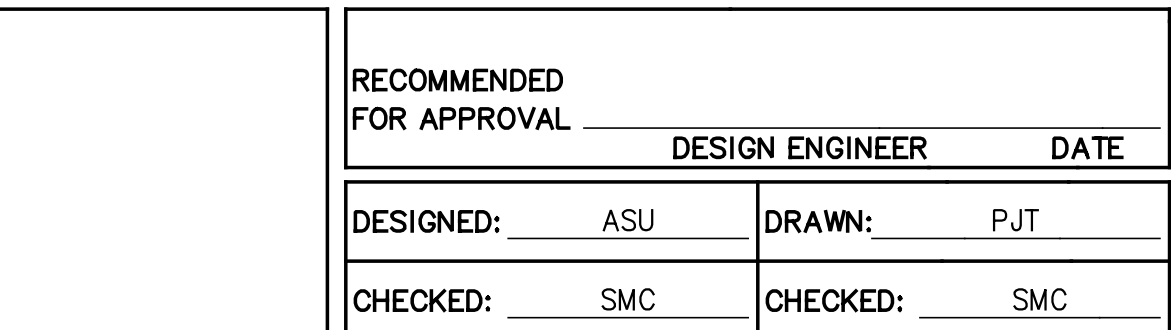
+10.73, 76.72, TREE
+98.48, 86.56, TREE
+91.67, 4.66, CP

+98.48, 86.56, TREE

+98.48, 86.56, TREE

P.I. 22+52.18 "W-3-A"

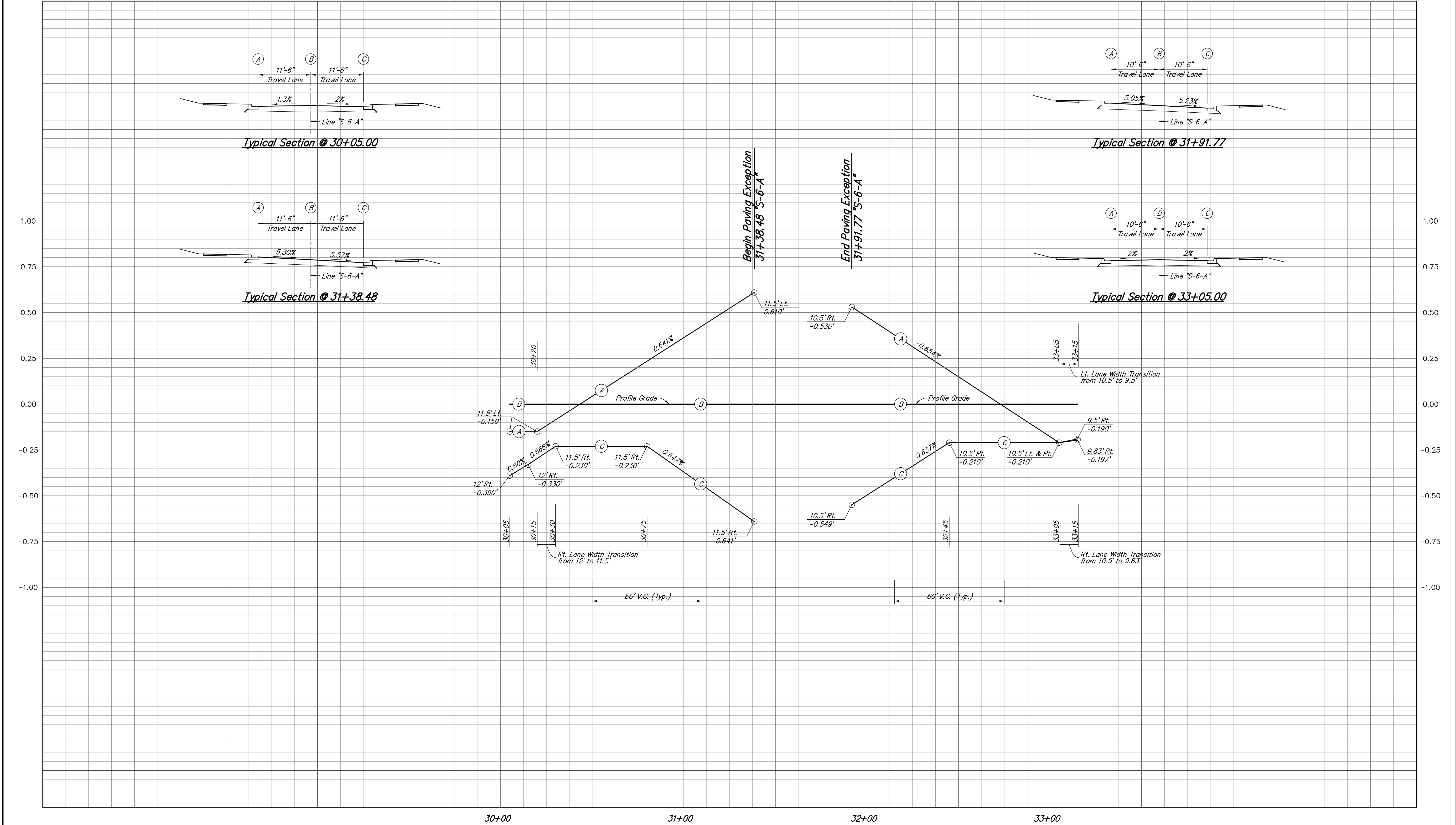
END CONSTRUCTION
23+07.96 "W-3-A"

$$\frac{P.O.T. 23+07.96 \text{ "W-3-A"} =}{P.O.T. 23+07.96 \text{ "S-1-A"}}$$


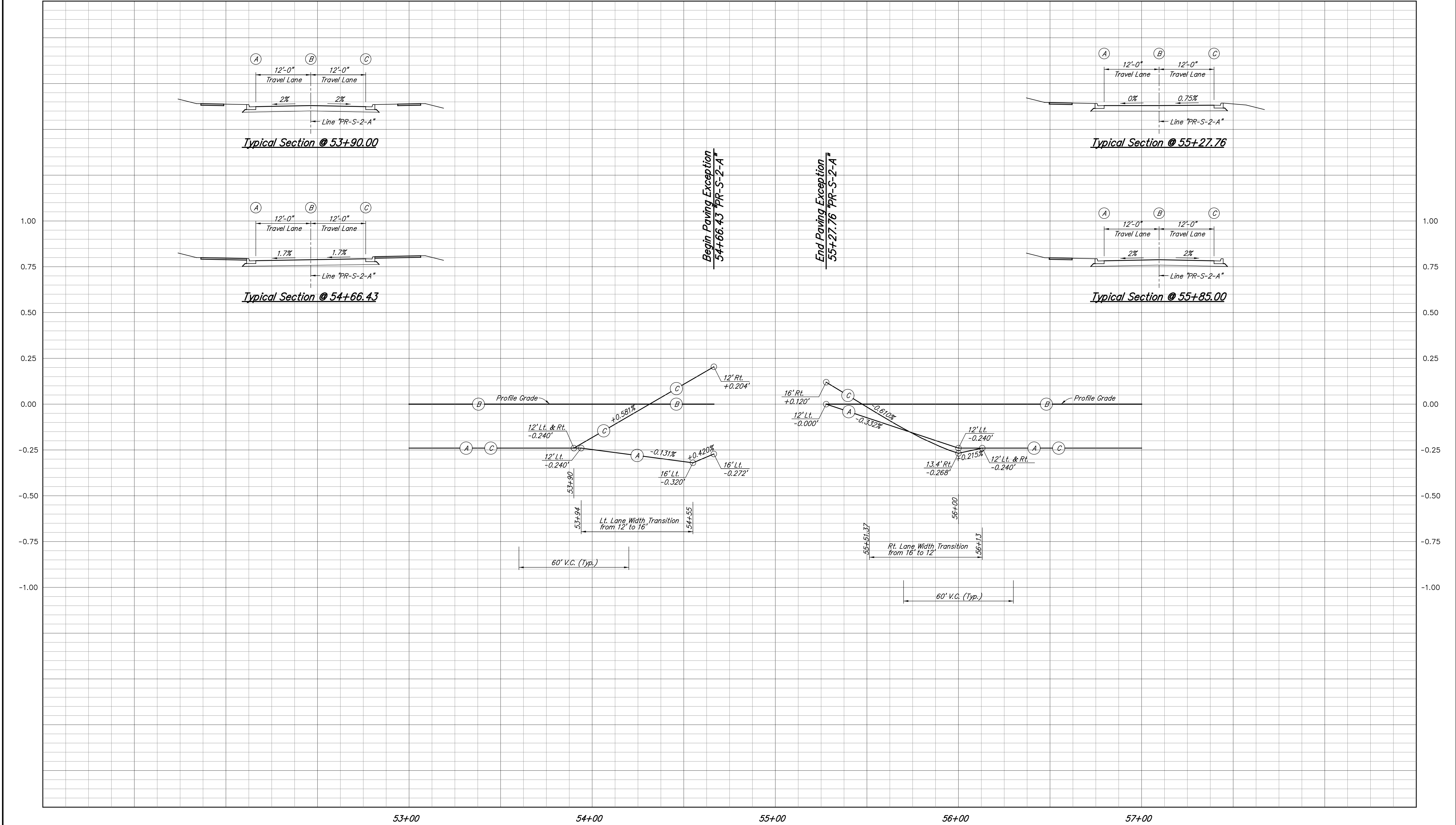
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN & PROFILE
LINE "W-3-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE		
VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1005154		
SURVEY BOOK	SHEETS		
	32	of	138
CONTRACT R-33130	PROJECT NO. 1005154		

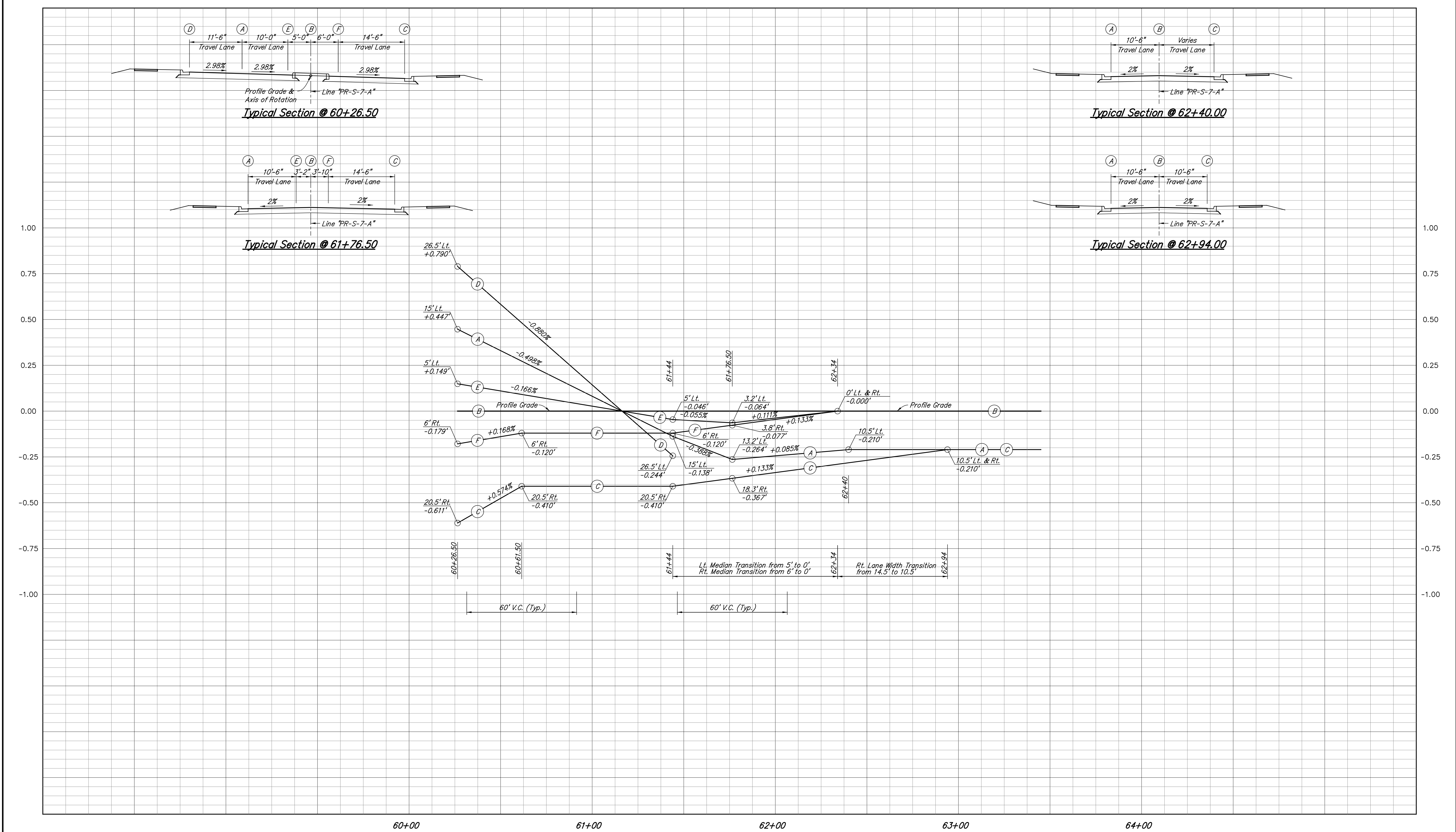


		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE		BRIDGE FILE	
								1" = 25'		1005154	
								VERTICAL SCALE		DESIGNATION NO.	
								1" = 0.25'		1005154	
		DESIGNED: ASU		DRAWN: ASU			PAVEMENT EDGE TRANSITION DIAGRAM LINE "S-6-A"	SURVEY BOOK		SHEETS	
								33 of 138			
		CHECKED: SMC		CHECKED: SMC				CONTRACT		PROJECT NO.	
								R-33130		1005154	

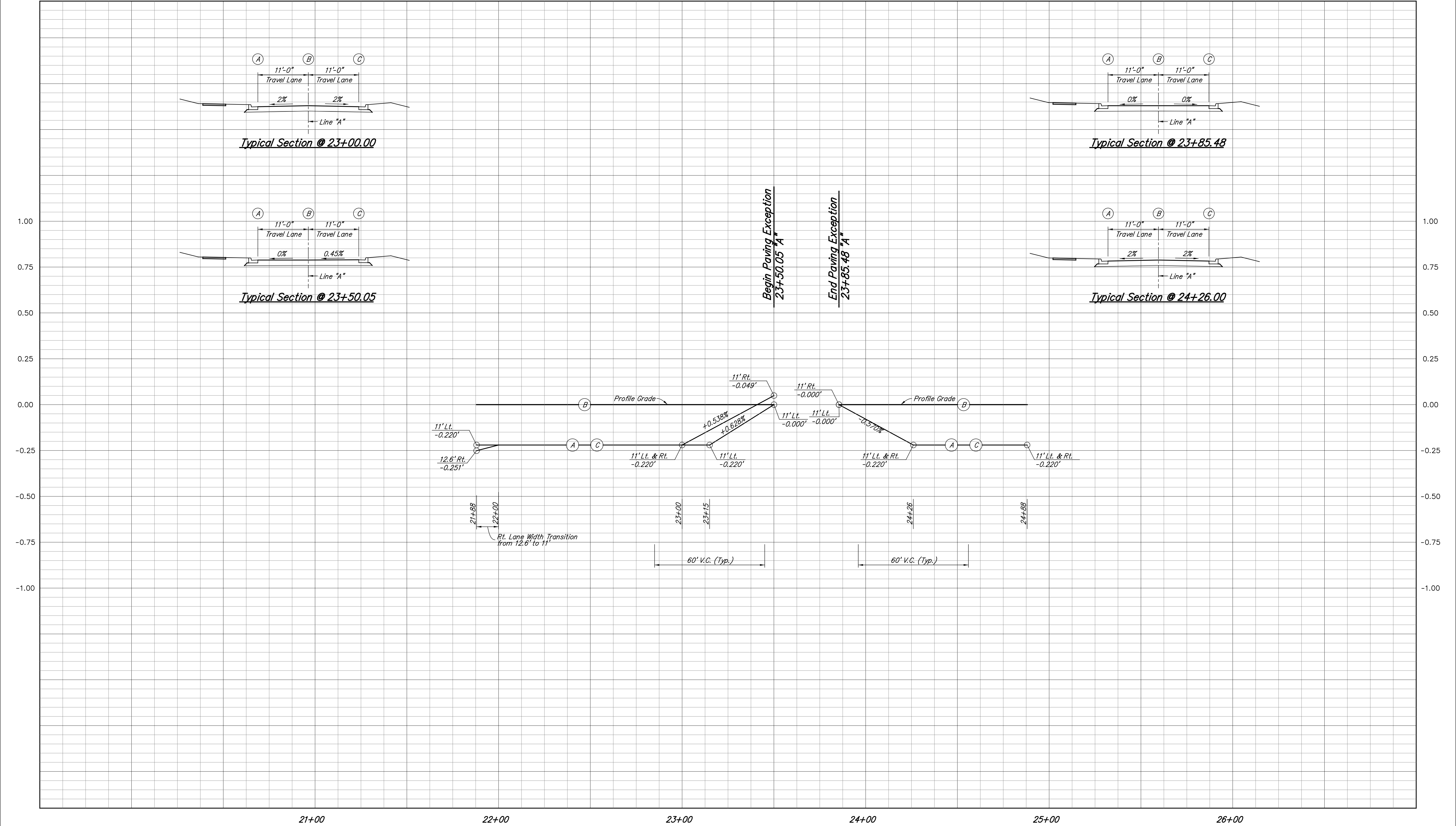


		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 25'	BRIDGE FILE
		PAVEMENT EDGE TRANSITION DIAGRAM LINE "PR-S-2-A"		VERTICAL SCALE 1" = 0.25'	DESIGNATION NO. 1005154
				SURVEY BOOK R-33130	SHEETS 34 of 138
				CONTRACT R-33130	PROJECT NO. 1005154

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: ASU	
CHECKED: SMC	CHECKED: SMC	



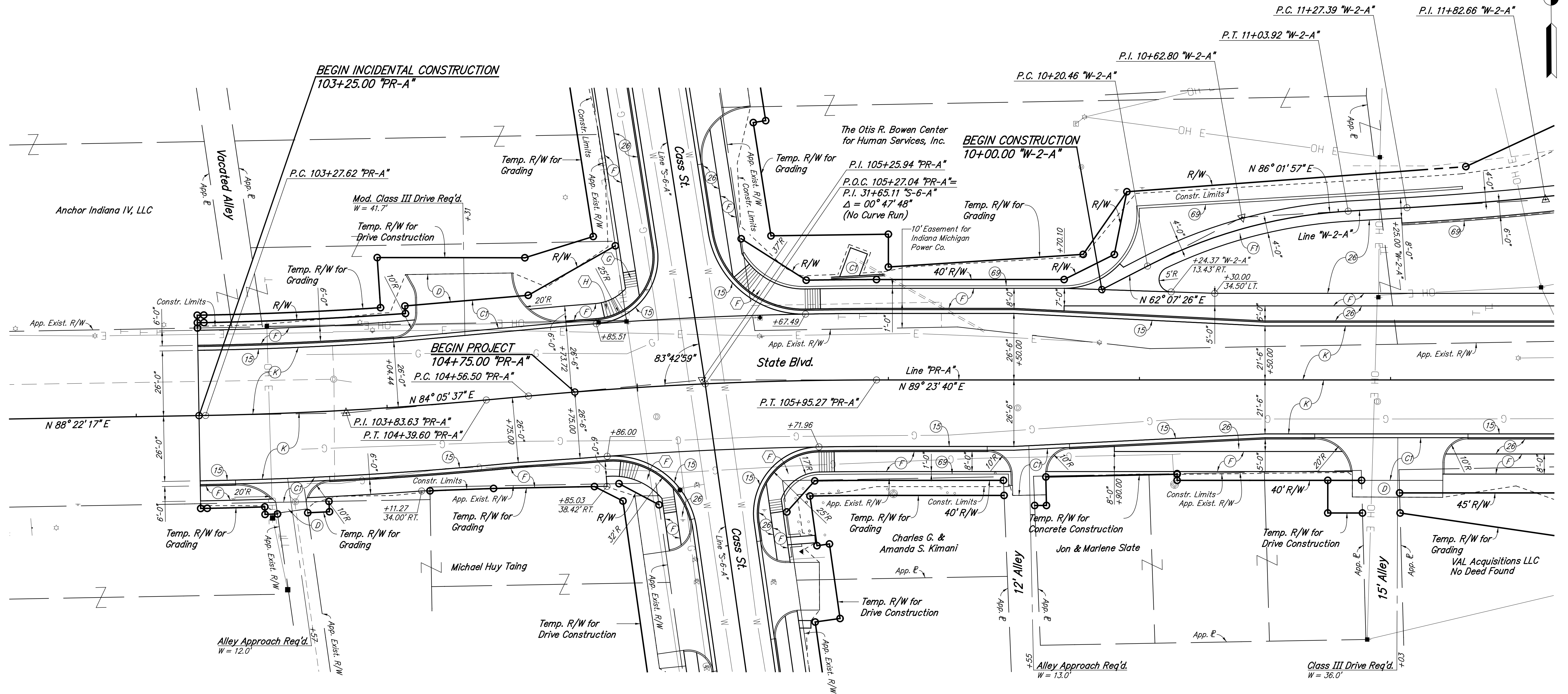
		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 25'		BRIDGE FILE	
				VERTICAL SCALE 1" = 0.25'		DESIGNATION NO. 1005154	
				SURVEY BOOK		SHEETS	
				CONTRACT R-33130		35 of 138 PROJECT NO. 1005154	
		PAVEMENT EDGE TRANSITION DIAGRAM LINE "PR-S-7-A"		RECOMMENDED FOR APPROVAL			
				DESIGN ENGINEER		DATE	
				DESIGNED: ASU		DRAWN: ASU	
				CHECKED: SMC		CHECKED: SMC	



		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
						1" = 25'			
						VERTICAL SCALE		DESIGNATION NO.	
						1" = 0.25'		1005154	
		DESIGNED: ASU CHECKED: SMC		DRAWN: ASU CHECKED: SMC		PAVEMENT EDGE TRANSITION DIAGRAM LINE "A"		SURVEY BOOK	
								SHEETS	
								36 of 138	
								PROJECT NO.	
						CONTRACT R-33130		1005154	

CURVE DATA
P.I. 10+62.80 "W-2-A"
 $\Delta = 23^\circ 54' 30''$ (Rt.)
T = 42.34'
L = 83.46'
R = 200.00'
E = 4.43'

CURVE DATA
P.I. 11+82.66 "W-2-A"
 $\Delta = 4^\circ 52' 07''$ (Lt.)
T = 55.27'
L = 110.46'
R = 1,300.00'
E = 1.17'



CURVE DATA
P.I. 103+83.63 "PR-A"
 $\Delta = 4^\circ 16' 39''$ (Lt.)
T = 56.02'
L = 111.98'
R = 1,500.00'
E = 1.05'

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

CURVE DATA
P.I. 105+25.94 "PR-A"
 $\Delta = 5^\circ 18' 03''$ (Rt.)
T = 69.44'
L = 138.78'
R = 1,500.00'
E = 1.61'

Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<div>C</div> PCCP for Approaches, 6"	<div>D</div> HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	<div>K</div> Full Depth Pavement (See Typical Sections)	<div>26</div> Sodding, Nursery
<div>C</div> PCCP for Approaches, 9"	<div>F</div> Concrete Sidewalk, 4"	<div>13</div> Concrete Curb	<div>28</div> Mulched Seeding, Type "U"
<div>D</div> HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	<div>F</div> Shared Use Path Pavement (See Typical Sections)	<div>14</div> Integral Concrete Curb	<div>69</div> Retaining Wall
<div>J</div> Compacted Aggregate, No. 53	<div>J</div> Compacted Aggregate, No. 53	<div>15</div> Curb and Gutter, Concrete, Modified	<div>X</div> Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		<div>22</div> Concrete Center Curb, Type "D"	<div>X</div> Center Curb Removal Limits

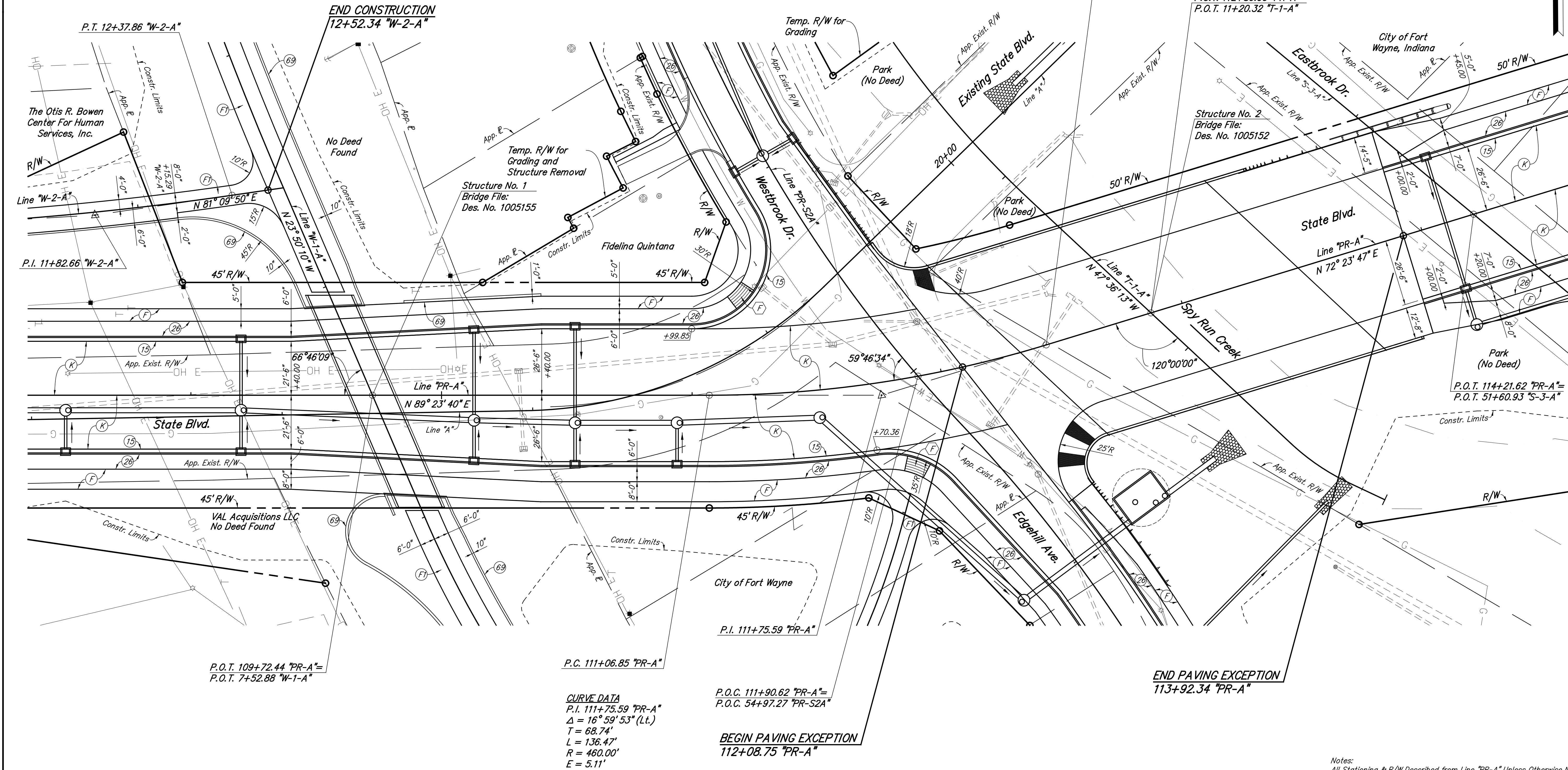
RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK R-33130	SHEETS 37 of 138
CONTRACT R-33130	PROJECT NO. 1005154

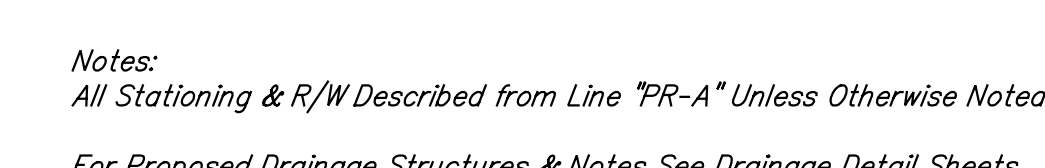
CURVE DATA
P.I. 11+82.66 "W2A"
 $\Delta = 4^{\circ} 52' 07''$ (Lt.)
T = 55.27'
L = 110.46'
R = 1,300.00'
E = 1.17'


SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

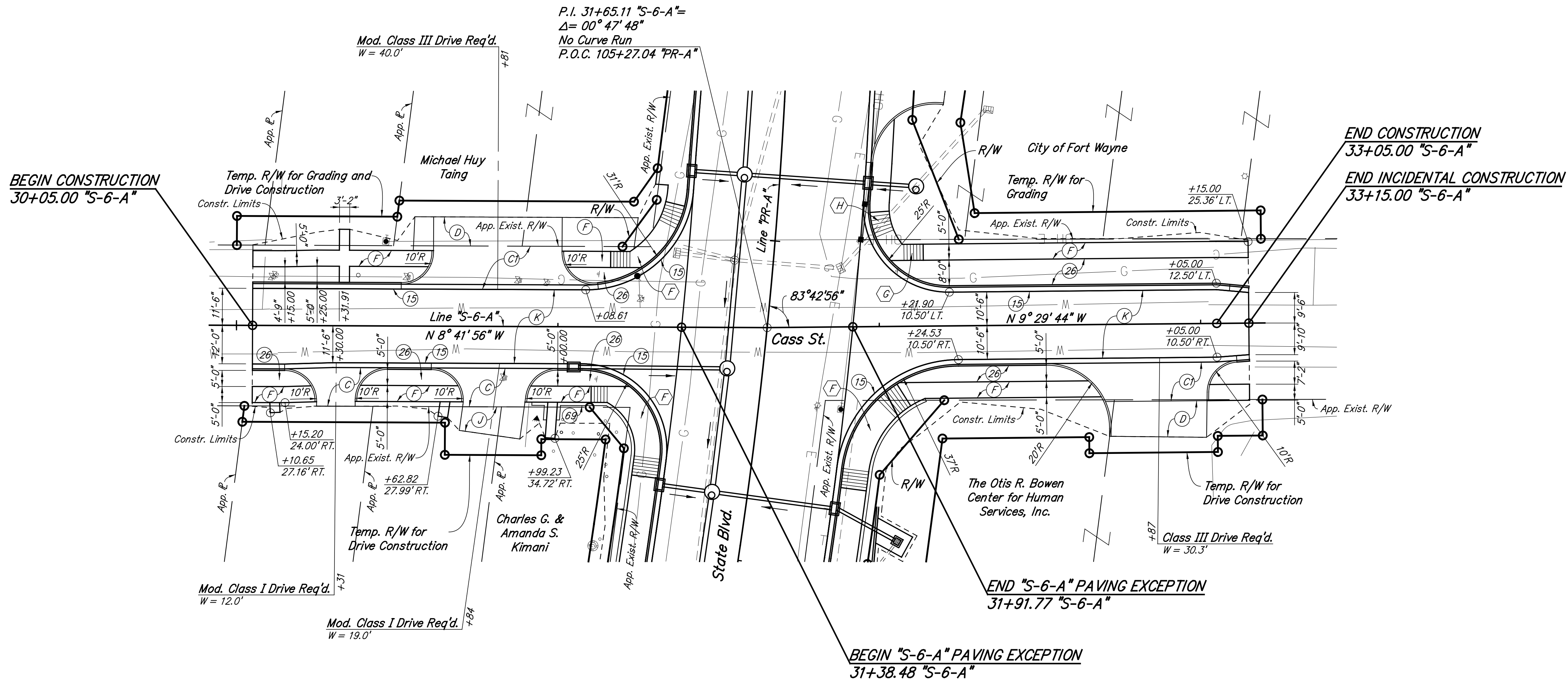
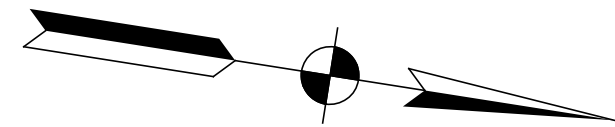


Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.


<p>(C) PCCP for Approaches, 6"</p> <p>(C) PCCP for Approaches, 9"</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"</p>		<p>LEGEND</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</p> <p>(F) Concrete Sidewalk, 4"</p> <p>(F) Shared Use Path Pavement (See Typical Sections)</p> <p>(J) Compacted Aggregate, No. 53</p>		<p>(K) Full Depth Pavement (See Typical Sections)</p> <p>(13) Concrete Curb</p> <p>(14) Integral Concrete Curb</p> <p>(15) Curb and Gutter, Concrete, Modified</p> <p>(22) Concrete Center Curb, Type "D"</p>		<p>(26) Sodding, Nursery</p> <p>(28) Mulched Seeding, Type "U"</p> <p>(69) Retaining Wall</p> <p>(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)</p> <p>Center Curb Removal Limits</p>		<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER DATE</p> <p>DESIGNED: ASU DRAWN: PJT</p> <p>CHECKED: SMC CHECKED: SMC</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS LINE "PR-A"</p>		<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE NONE</p> <p>SURVEY BOOK R-33130</p>		<p>BRIDGE FILE DESIGNATION NO. 1005154</p> <p>SHEETS 38 of 138 PROJECT NO. 1005154</p>	
--	--	---	--	---	--	--	--	--	--	---	--	--	--	--	--



<div><div><div>C</div><div>PCCP for Approaches, 6"</div></div><div><div>C1</div><div>PCCP for Approaches, 9"</div></div><div><div>D</div><div>HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"</div></div></div> <div><div><div>D1</div><div>HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</div></div><div><div>F</div><div>Concrete Sidewalk, 4"</div></div><div><div>F1</div><div>Shared Use Path Pavement (See Typical Sections)</div></div><div><div>J</div><div>Compacted Aggregate, No. 53</div></div></div> <div><div><div>K</div><div>Full Depth Pavement (See Typical Sections)</div></div><div><div>L3</div><div>Concrete Curb</div></div><div><div>L4</div><div>Integral Concrete Curb</div></div><div><div>L5</div><div>Curb and Gutter, Concrete, Modified</div></div><div><div>L22</div><div>Concrete Center Curb, Type "D"</div></div></div> <div><div><div>26</div><div>Sodding, Nursery</div></div><div><div>28</div><div>Mulched Seeding, Type "U"</div></div><div><div>69</div><div>Retaining Wall</div></div><div><div>X</div><div>Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)</div></div><div><div></div><div>Center Curb Removal Limits</div></div></div>		<div>RECOMMENDED FOR APPROVAL <div>DESIGN ENGINEER</div><div>DATE</div></div> <div><div>DESIGNED: ASU</div><div>DRAWN: PJT</div></div> <div><div>CHECKED: SMC</div><div>CHECKED: SMC</div></div>		<div>INDIANA DEPARTMENT OF TRANSPORTATION</div> <div>CONSTRUCTION DETAILS LINE "PR-A"</div>		<table><tr><td>HORIZONTAL SCALE 1" = 20'</td><td>BRIDGE FILE</td></tr><tr><td>VERTICAL SCALE NONE</td><td>DESIGNATION NO. 1005154</td></tr><tr><td>SURVEY BOOK</td><td>SHEETS 39 of 138</td></tr><tr><td>CONTRACT R-33130</td><td>PROJECT NO. 1005154</td></tr></table>	HORIZONTAL SCALE 1" = 20'	BRIDGE FILE	VERTICAL SCALE NONE	DESIGNATION NO. 1005154	SURVEY BOOK	SHEETS 39 of 138	CONTRACT R-33130	PROJECT NO. 1005154
HORIZONTAL SCALE 1" = 20'	BRIDGE FILE													
VERTICAL SCALE NONE	DESIGNATION NO. 1005154													
SURVEY BOOK	SHEETS 39 of 138													
CONTRACT R-33130	PROJECT NO. 1005154													



Notes:
All Stationing & R/W Described from Line "S-6-A" Unless Otherwise Noted.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"		(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Shared Use Path Pavement (See Typical Sections)		(14) Integral Concrete Curb	(69) Retaining Wall
	(J) Compacted Aggregate, No. 53		(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
			(22) Concrete Center Curb, Type "D"	 Center Curb Removal Limits

RECOMMENDED FOR APPROVAL		
DESIGNED: ASU	DRAWN: PJT	DATE
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "S-6-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK R-33130	SHEETS 40 of 138
CONTRACT R-33130	PROJECT NO. 1005154

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

END "PR-S2A" PAVING EXCEPTION
55+27.76 "PR-S2A"

P.O.C. 54+97.27 "PR-S2A"=
P.O.C. 111+90.62 "PR-A"

BEGIN "PR-S2A" PAVING EXCEPTION
54+66.43 "PR-S2A"

CURVE DATA
P.I. 51+11.15 "PR-S2A"
 $\Delta = 41^\circ 38' 26''$ (Rt.)
T = 111.15'
L = 212.43'
R = 292.30'
E = 20.42'

BEGIN INCIDENTAL CONSTRUCTION
51+50.00 "PR-S2A"

P.I. 51+11.15 "PR-S2A"

BEGIN CONSTRUCTION
51+80.00 "PR-S2A"

P.C. 50+00.00 "PR-S2A"

P.R.C. 52+12.43 "PR-S2A"

P.I. 53+43.73 "PR-S2A"

CURVE DATA
P.I. 53+43.73 "PR-S2A"
 $\Delta = 24^\circ 41' 10''$ (Lt.)
T = 131.29'
L = 258.51'
R = 600.00'
E = 14.20'

P.R.C. 54+70.95 "PR-S2A"

P.I. 55+58.92 "PR-S2A"

CURVE DATA
P.I. 55+58.92 "PR-S2A"
 $\Delta = 17^\circ 47' 06''$ (Rt.)
T = 87.97'
L = 174.53'
R = 562.26'
E = 6.84'

Notes:
All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

- (C) PCCP for Approaches, 6"
(C) PCCP for Approaches, 9"
(D) HMA for Approaches
165 #/Syd. HMA Surface Type "B" on
275 #/Syd. HMA Intermediate Type "B"
880 #/Syd. HMA Base Type "B"

- (D) HMA for Approaches
165 #/Syd. HMA Surface Type "B" on
275 #/Syd. HMA Intermediate Type "B"
(F) Concrete Sidewalk, 4"
(F) Shared Use Path Pavement (See Typical Sections)
(J) Compacted Aggregate, No. 53

LEGEND

- (K) Full Depth Pavement (See Typical Sections)
(13) Concrete Curb
(14) Integral Concrete Curb
(15) Curb and Gutter, Concrete, Modified
(22) Concrete Center Curb, Type "D"

- (26) Sodding, Nursery
(28) Mulched Seeding, Type "U"
(69) Retaining Wall
(X) Concrete Curb Ramp
(Type is Indicated by Letter Inside Hexagon)
Center Curb Removal Limits

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: ASU

DRAWN: CAK

CHECKED: SMC

CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-S2A"

HORIZONTAL SCALE

1" = 20'

VERTICAL SCALE

NONE

SURVEY BOOK

R-33130

BRIDGE FILE

DESIGNATION NO.

1005154

SHEETS

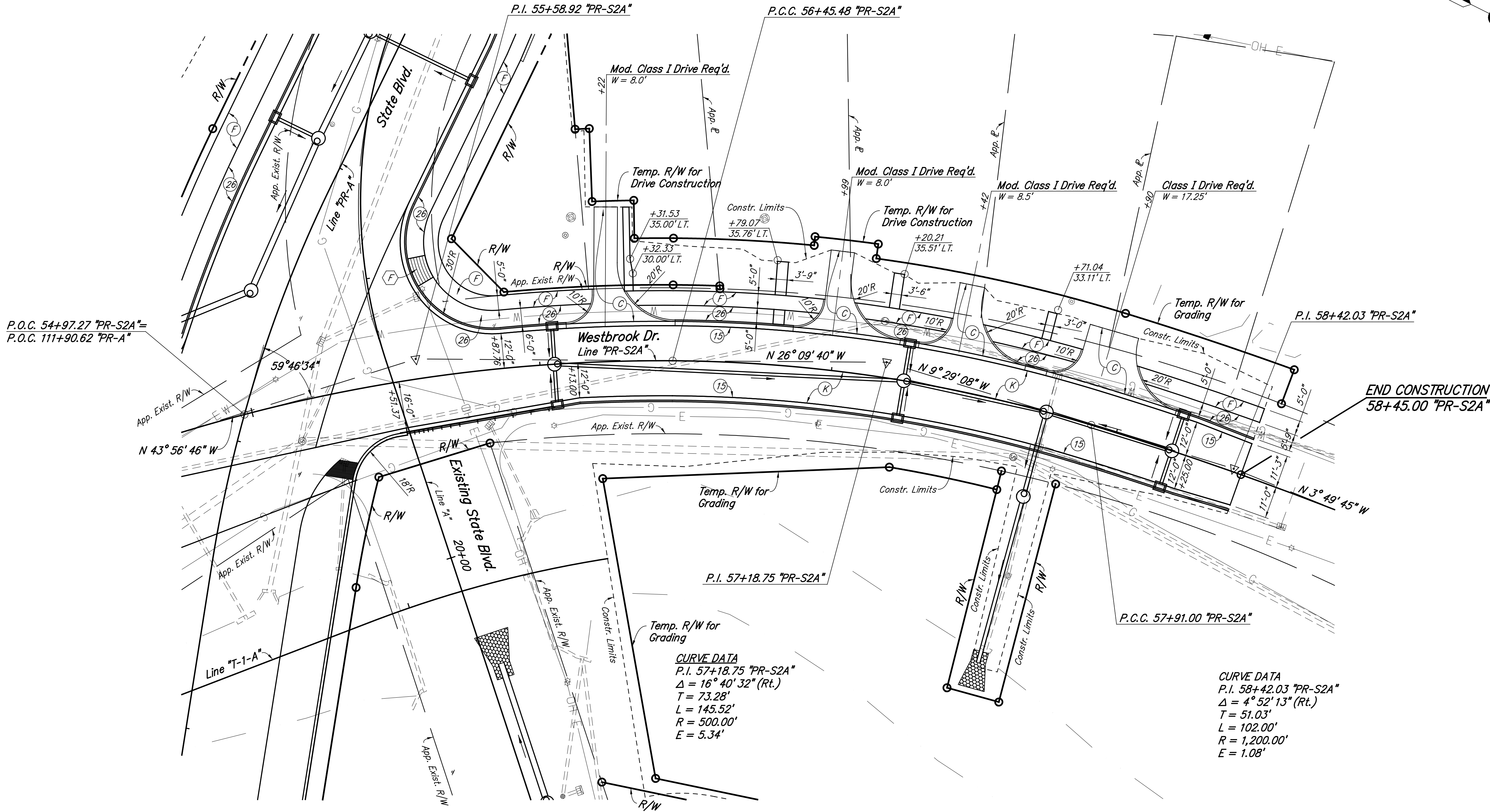
41 of 138

PROJECT NO.

1005154

CURVE DATA
P.I. 55+58.92 "PR-S2A"
 $\Delta = 17^{\circ} 47' 06''$ (Rt.)
T = 87.97'
L = 174.53'
R = 562.26'
E = 6.84'

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"			(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Concrete Sidewalk, 4"		(14) Integral Concrete Curb	(69) Retaining Wall
	(F) Shared Use Path Pavement (See Typical Sections)		(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
	(J) Compacted Aggregate, No. 53		(22) Concrete Center Curb, Type "D"	
				Center Curb Removal Limits

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "PR-S2A"

HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
VERTICAL SCALE NONE		DESIGNATION NO. 1005154	
SURVEY BOOK		SHEETS	
CONTRACT R-33130		42 of 138	PROJECT NO. 1005154

P.C.C. 62+06.70 "PR-S7A"

Mod. Class I Drive Req'd.
W = 8.2'

Mod. Class I Drive Req'd.
W = 9.3'

Mod. Class I Drive Req'd.
W = 6.8'

P.T. 63+98.22 "PR-S7A"


END CONSTRUCTION
63+45.00 "PR-S7A"

Mod. Class I Drive Req'd.
 $W = 15.8'$

BEGIN CONSTRUCTION/
60+26.50 "PR-S7A"

P.C. 60+33.61 "PR-S7A"

(Type is Indicated by Letter)

 Center Curb Removal Limits

CHECKED:	SMC
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CONSTRUCTION DETAILS
LINE "PR-S7A"

CONTRACT
D. 73170

PROJECT NO.	1005154
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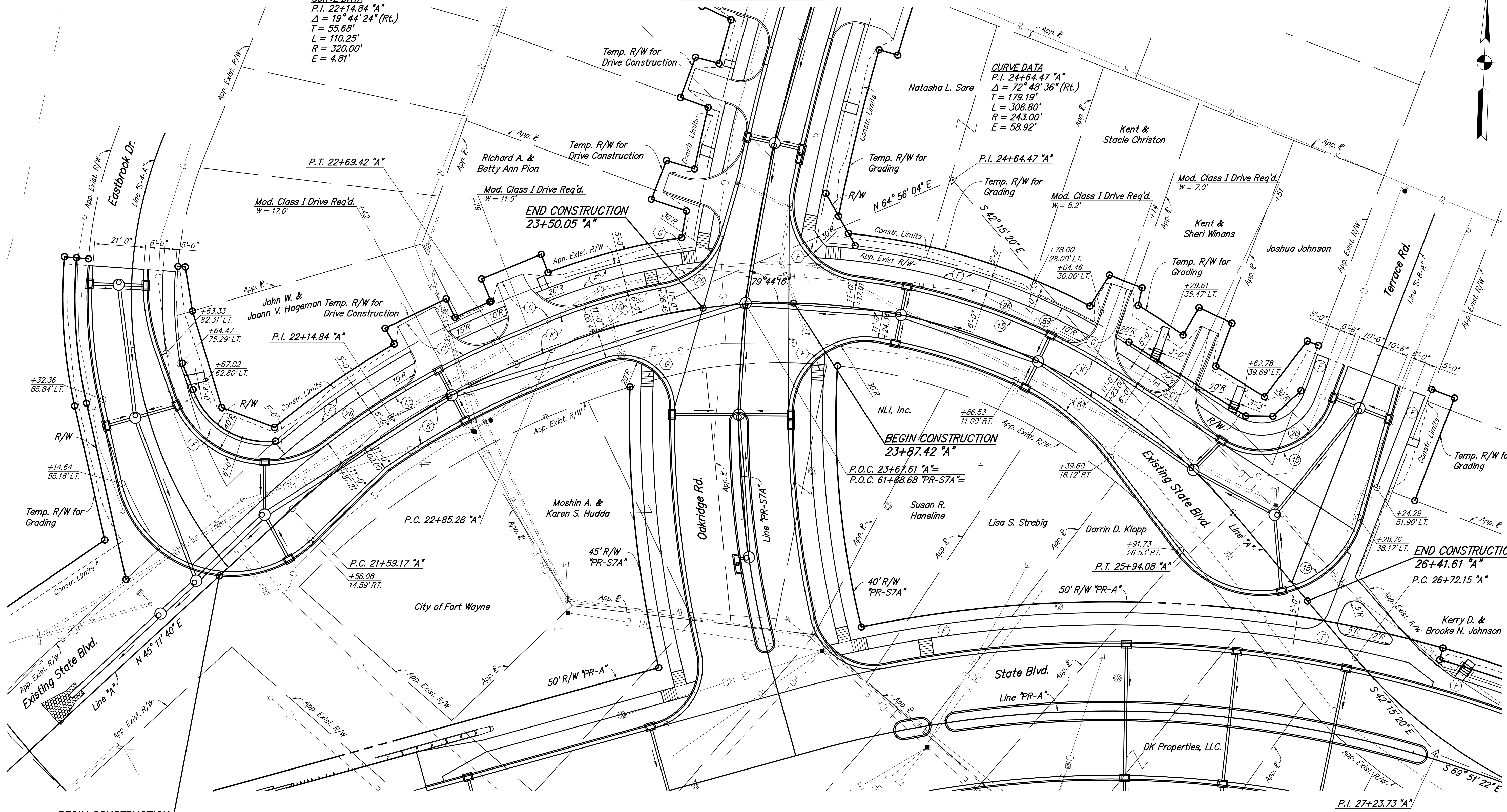
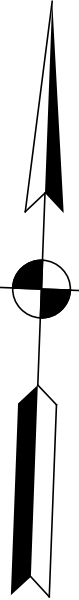
Notes:
All Stationing & R/W Described from Line "PR-S7A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

25

CURVE DATA
P.I. 22+14.84 "A"
 $\Delta = 19^\circ 44' 24''$ (Rt.)
T = 55.68'
L = 110.25'
R = 320.00'
E = 4.81'


CURVE DATA
P.I. 24+64.47 "A"
 $\Delta = 72^\circ 48' 36''$ (Rt.)
T = 179.19'
L = 308.80'
R = 243.00'
E = 58.92'



BEGIN CONSTRUCTION
21+18.36 "A"

END CONSTRUCTION
26+41.61 "A"
P.C. 26+72.15 "A"

Notes:
All Stationing & R/W Described from Line "A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"	
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall	
	(J) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)	
		(22) Concrete Center Curb, Type "D"	 Center Curb Removal Limits	

RECOMMENDED FOR APPROVAL		
DESIGN ENGINEER		DATE
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS
CONTRACT R-33130	44 of 138 PROJECT NO. 1005154

CURVE DATA

P.I. 4+79.21 "W-1-A"
 $\Delta = 3^{\circ} 59' 33"$ (Rt.)
 $T = 34.86'$
 $L = 69.68'$
 $R = 1,000.00'$
 $E = 0.61'$

CURVE DATA

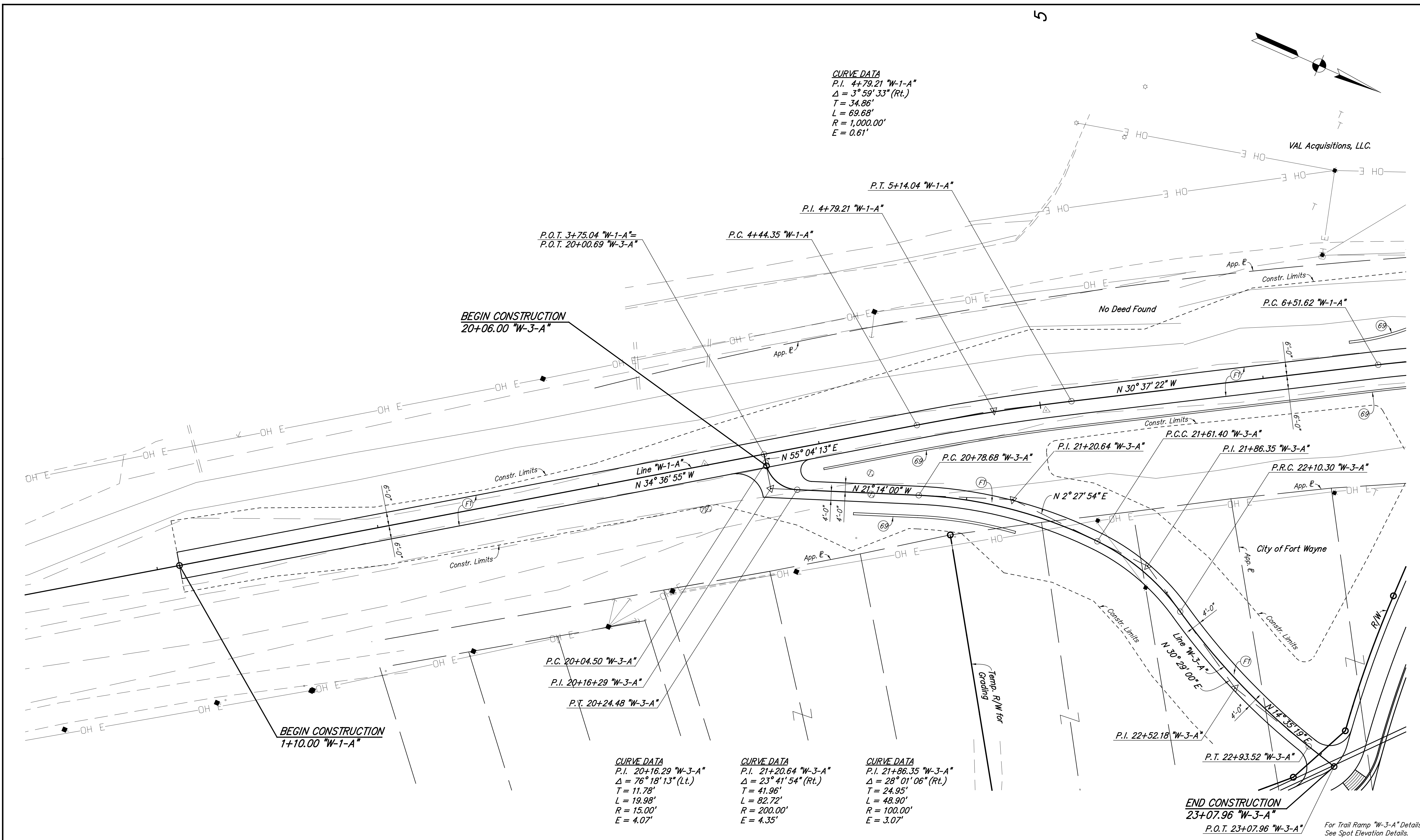
P.I. 20+16.29 "W-3-A"
 $\Delta = 76^{\circ} 18' 13"$ (Lt.)
 $T = 11.78'$
 $L = 19.98'$
 $R = 15.00'$
 $E = 4.07'$

CURVE DATA

P.I. 21+20.64 "W-3-A"
 $\Delta = 23^{\circ} 41' 54"$ (Rt.)
 $T = 41.96'$
 $L = 82.72'$
 $R = 200.00'$
 $E = 4.35'$

CURVE DATA

P.I. 21+86.35 "W-3-A"
 $\Delta = 28^{\circ} 01' 06"$ (Rt.)
 $T = 24.95'$
 $L = 48.90'$
 $R = 100.00'$
 $E = 3.07'$



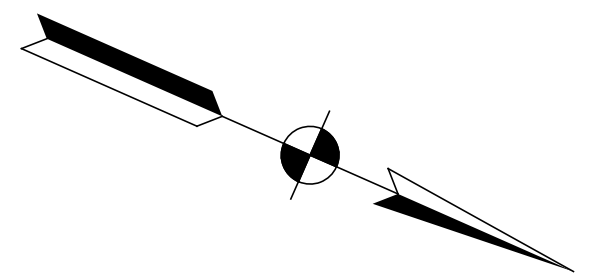
LEGEND

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C1) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F1) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall
	(J) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

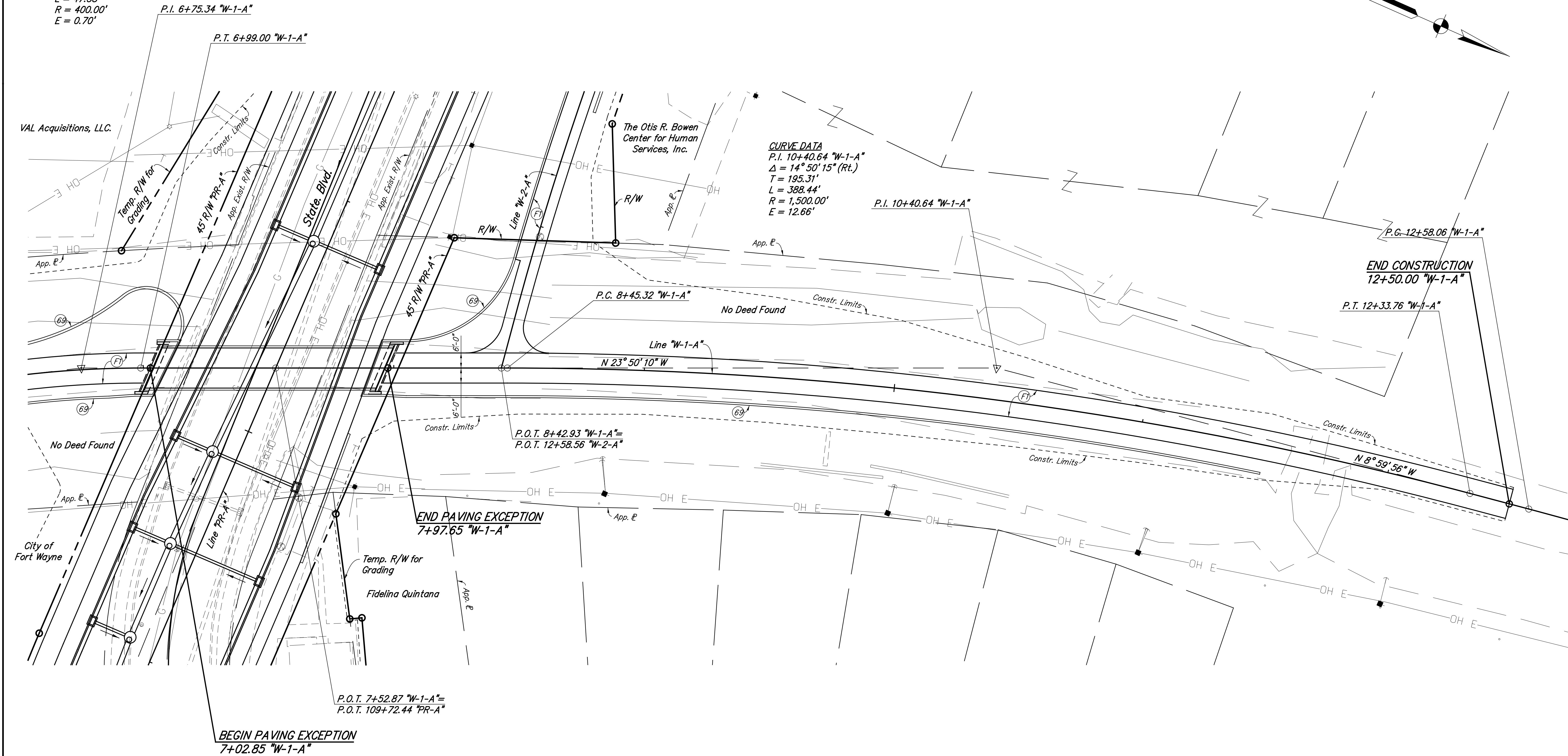
INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "W-1-A" & "W-3-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK R-33130	SHEETS 45 of 138
	PROJECT NO. 1005154



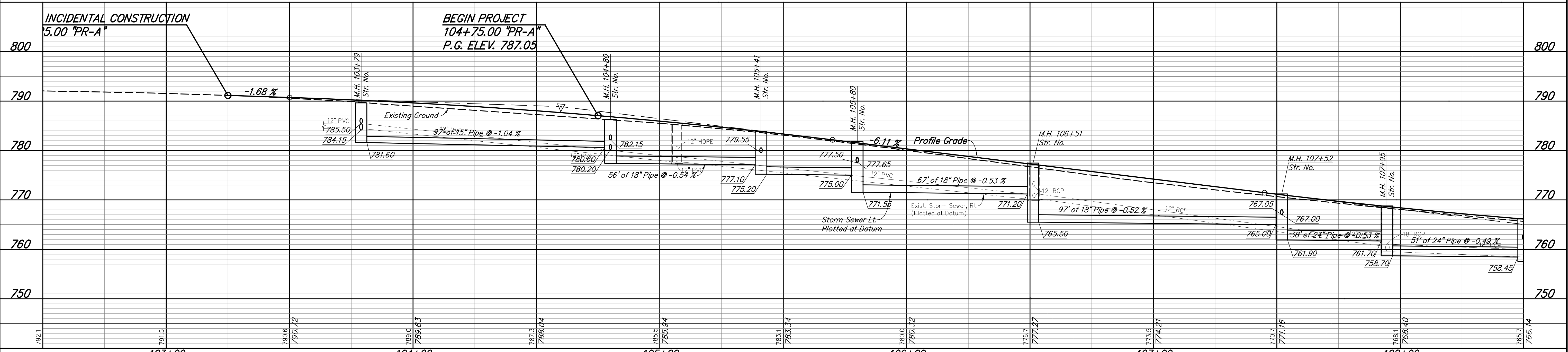
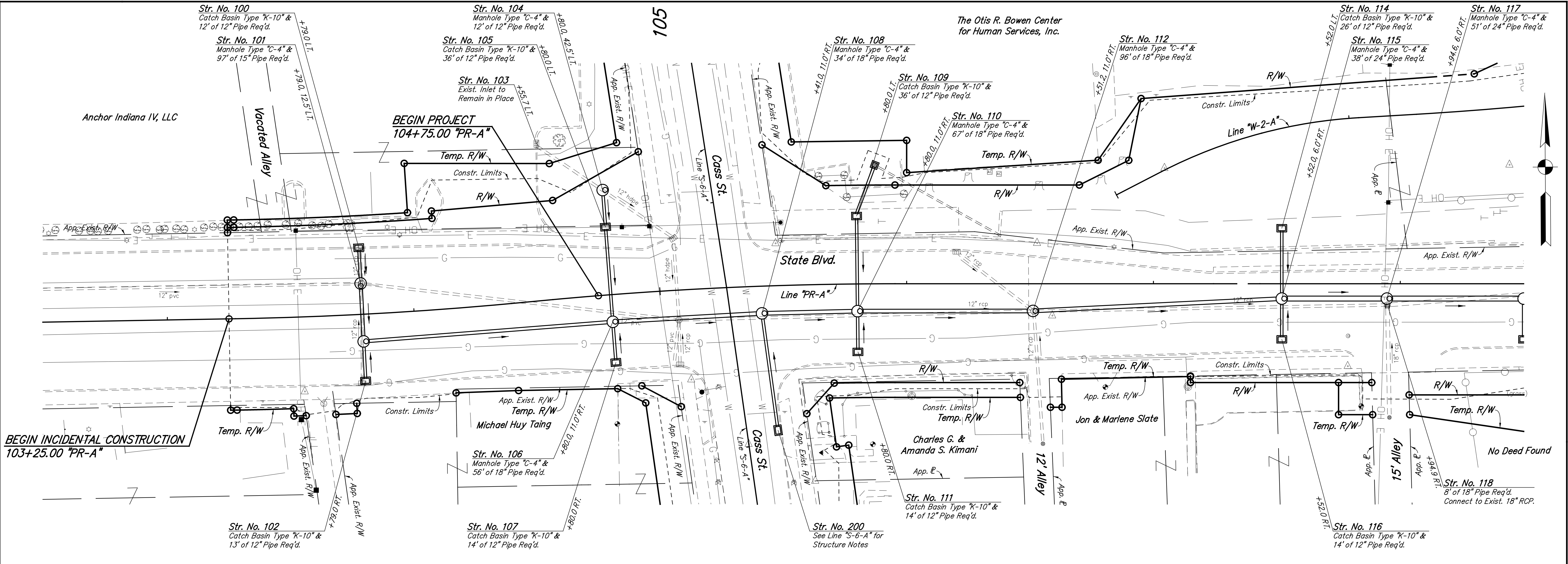
CURVE DATA
P.I. 6+75.34 "W-1-A"
 $\Delta = 6^{\circ} 47' 11''$ (Rt.)
T = 23.72'
L = 47.38'
R = 400.00'
E = 0.70'

CURVE DATA
P.I. 10+40.64 "W-1-A"
 $\Delta = 14^{\circ} 50' 15''$ (Rt.)
T = 195.31'
L = 388.44'
R = 1,500.00'
E = 12.66'

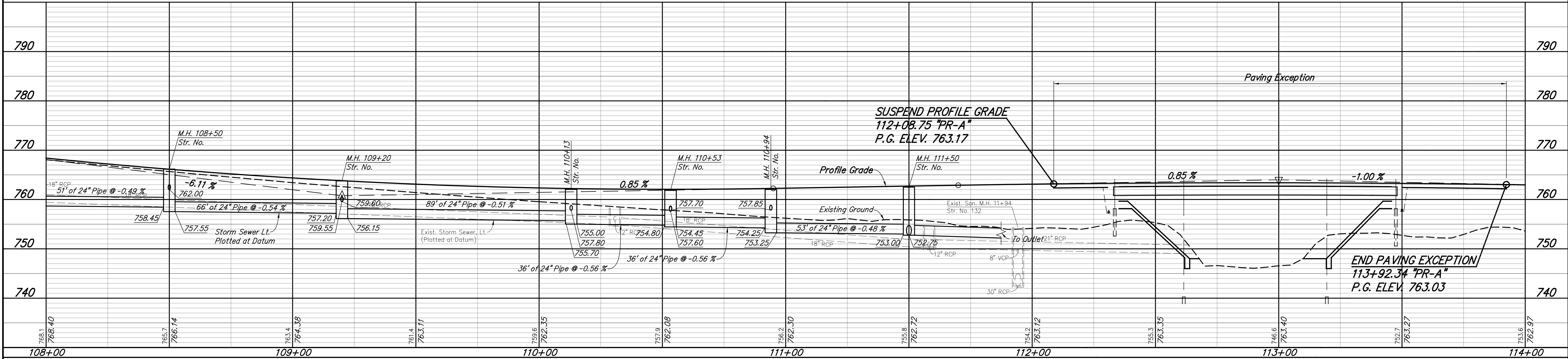
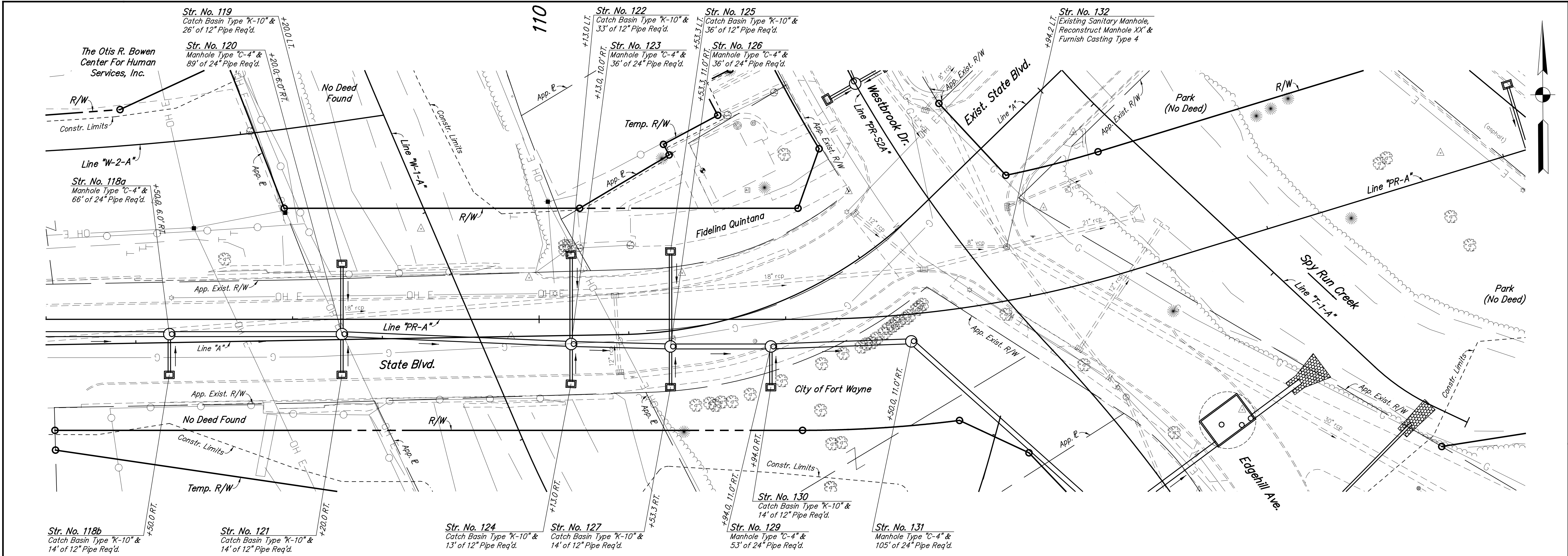


(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	LEGEND	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(69) Retaining Wall	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"			(14) Integral Concrete Curb	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
	(F) Shared Use Path Pavement (See Typical Sections)		(15) Curb and Gutter, Concrete, Modified	Center Curb Removal Limits
	(J) Compacted Aggregate, No. 53		(22) Concrete Center Curb, Type "D"	

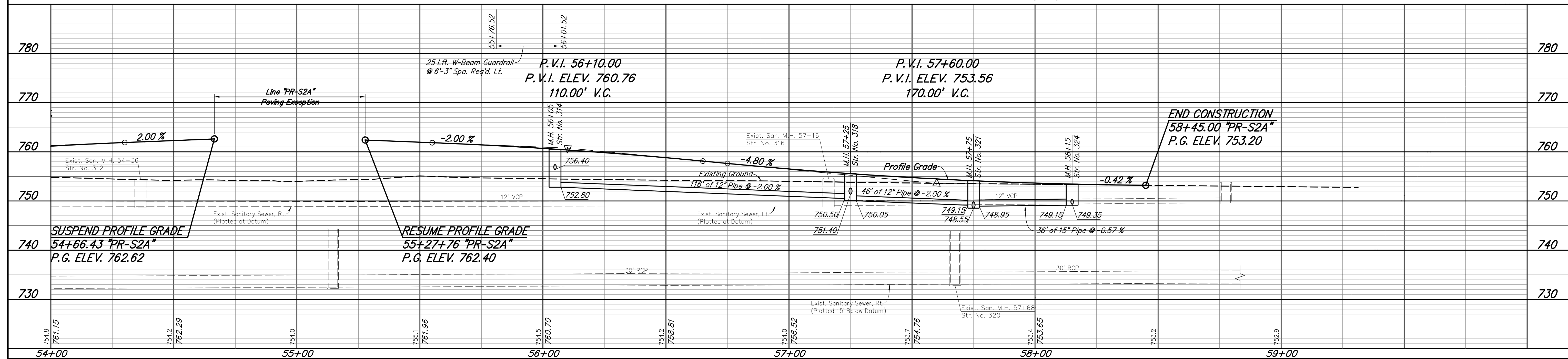
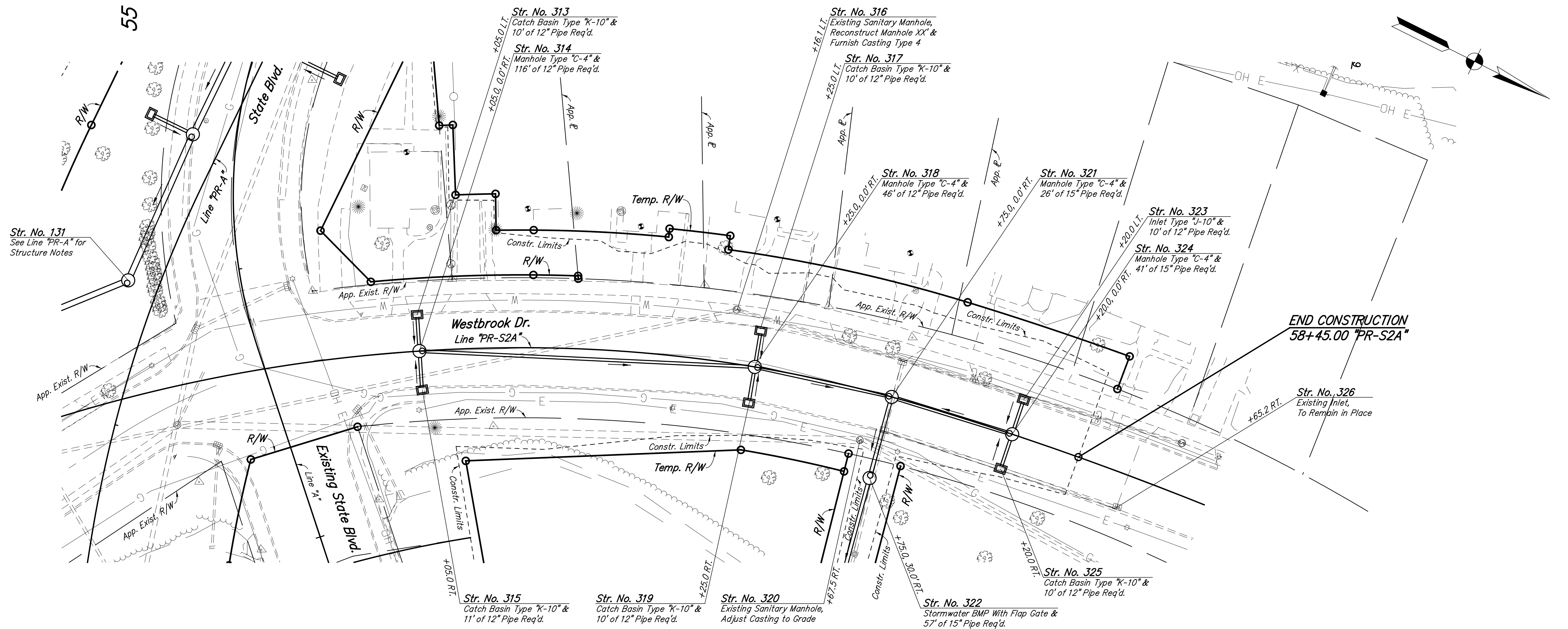
RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
DESIGN ENGINEER		DATE		VERTICAL SCALE NONE		DESIGNATION NO. 1005154	
DESIGNED: ASU	DRAWN: PJT	CONSTRUCTION DETAILS		SURVEY BOOK		SHEETS	
CHECKED: SMC	CHECKED: SMC	LINE "W-1-A"		CONTRACT R-33130		46 of 138	
						PROJECT NO. 1005154	



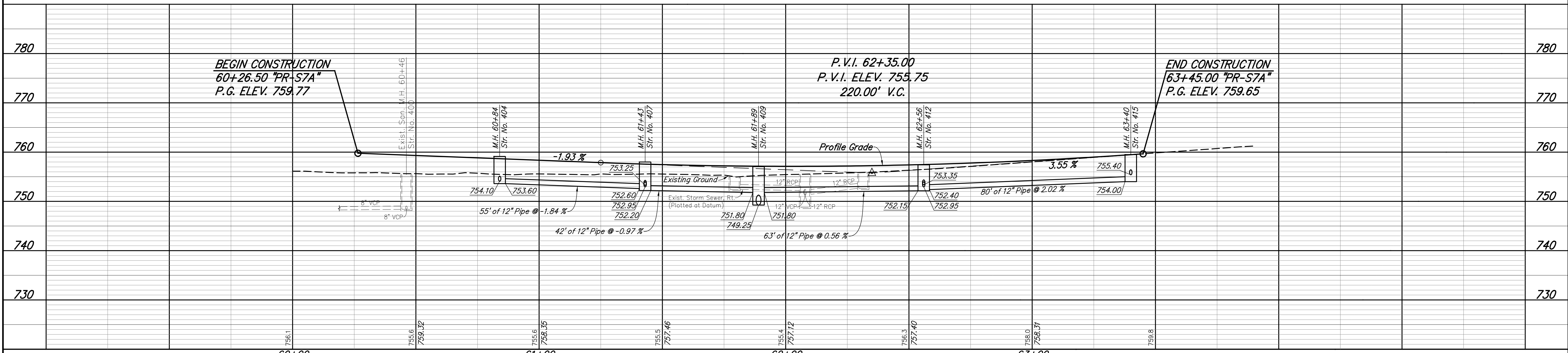
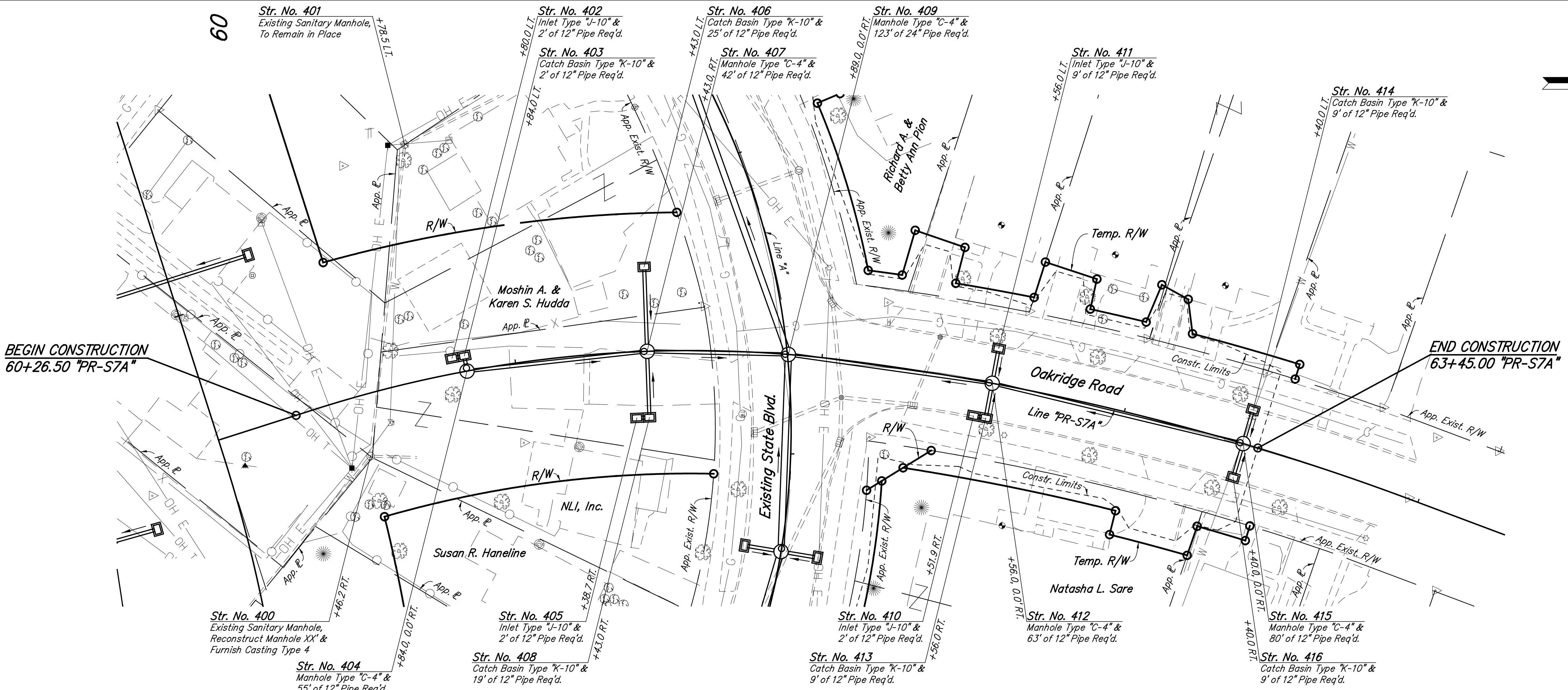
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		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		DESIGNATION NO. 1005154	
DESIGNED: ASU		DRAWN: CAK				SHEETS 48 of 138	
CHECKED: SMC		CHECKED: SMC				PROJECT NO. 1005154	
		DRAINAGE DETAILS LINE "PR-A"					
						CONTRACT R-33130	



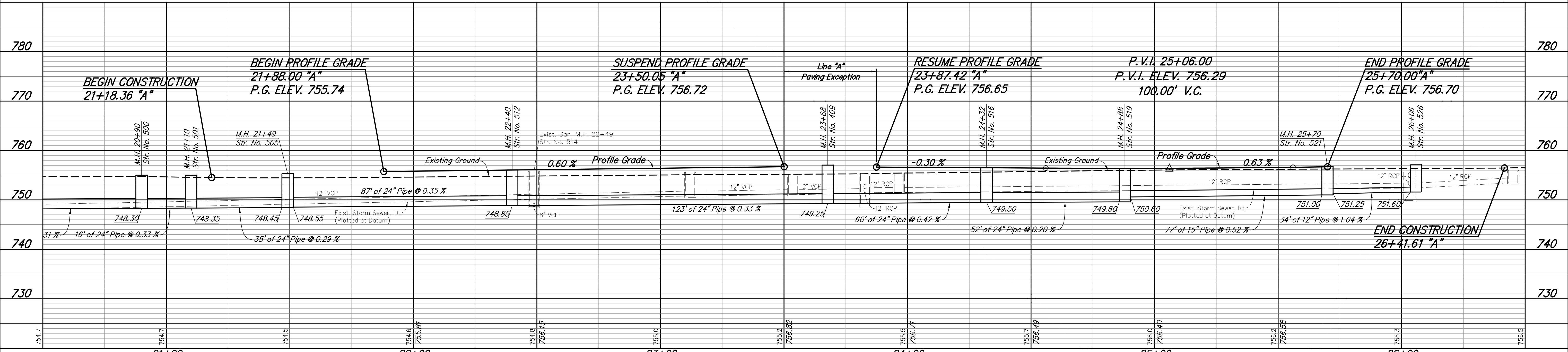
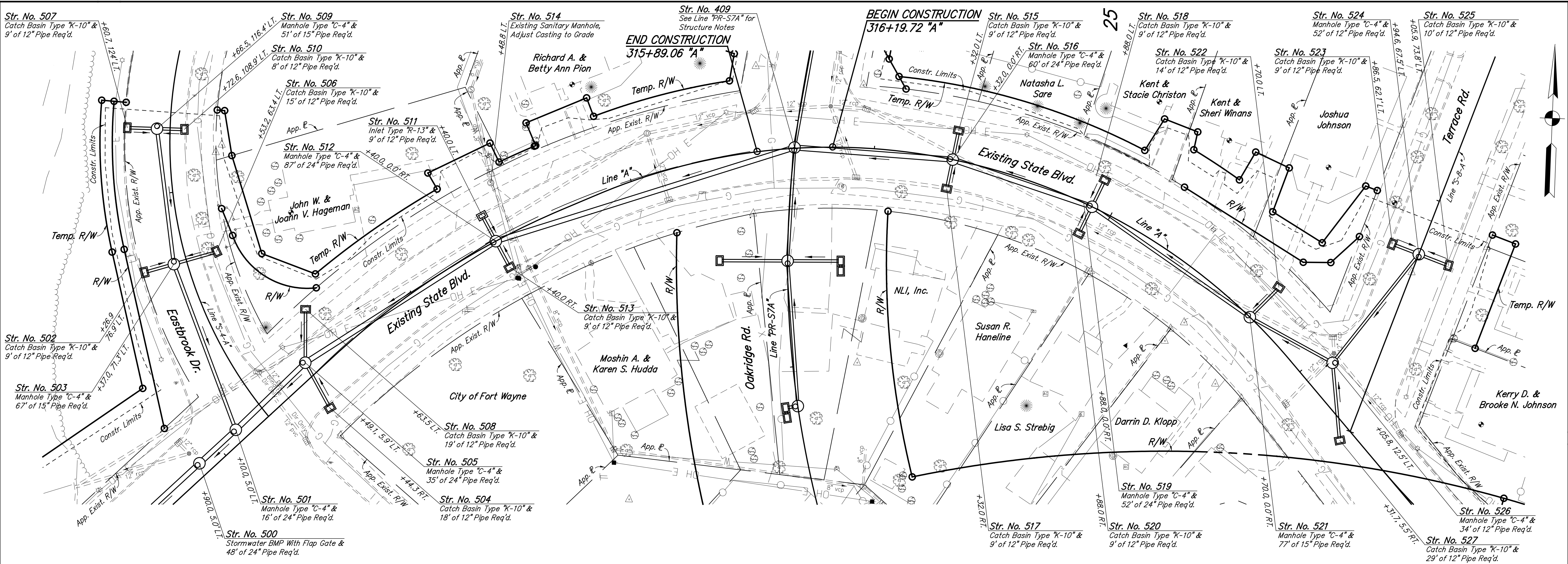
RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
				VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1005154	
DESIGNED: _____ ASU		DRAWN: _____ CAK		SURVEY BOOK		SHEETS 51 of 138	
CHECKED: _____ SMC		CHECKED: _____ SMC		CONTRACT R-33130		PROJECT NO. 1005154	
		DRAINAGE DETAILS LINE "PR-S2A"					



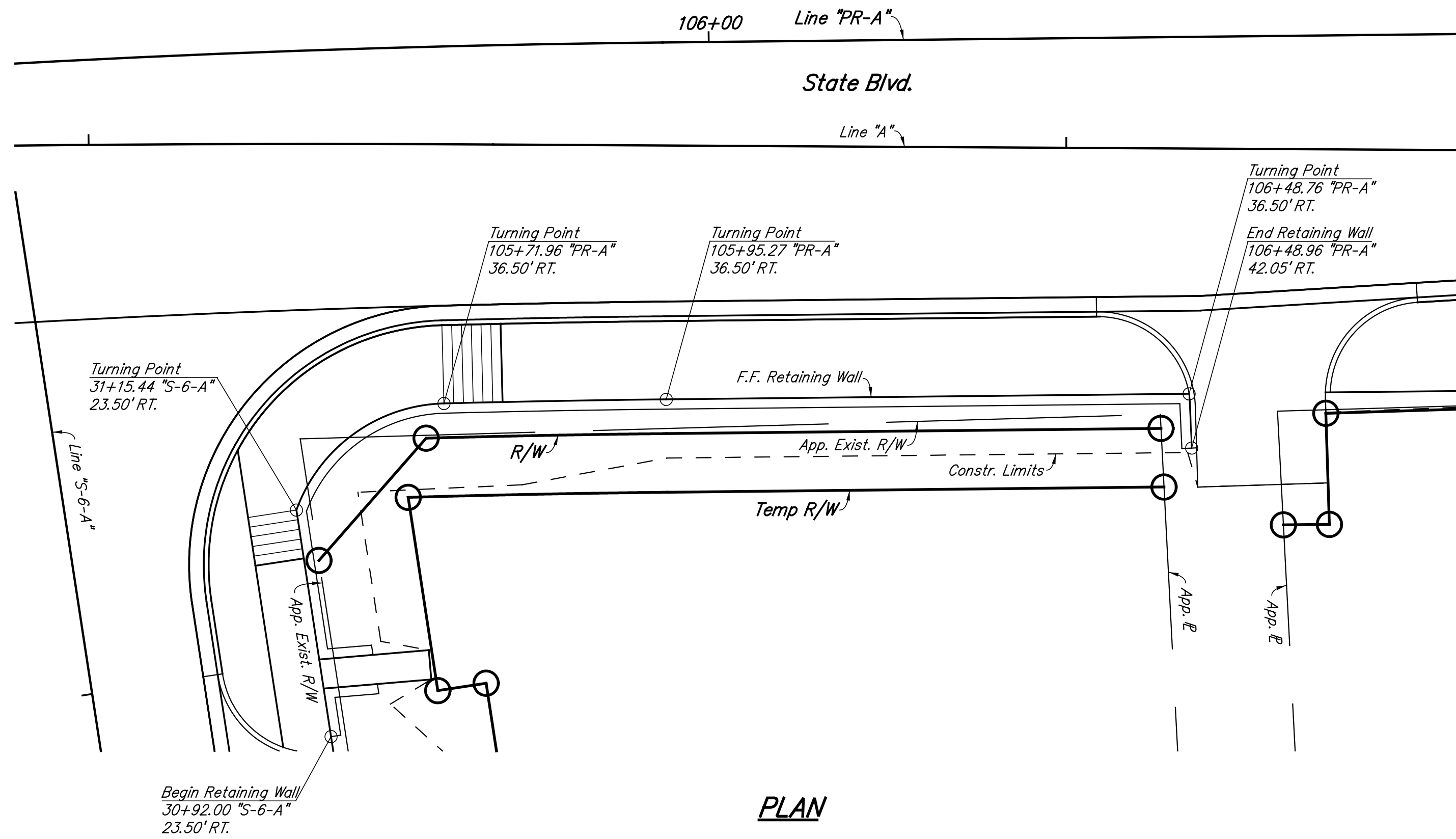
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK			
CHECKED: SMC		CHECKED: SMC			

INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
		1" = 20'			
		VERTICAL SCALE		DESIGNATION NO.	
		1" = 10'		1005154	
		SURVEY BOOK		SHEETS	
				52 of 138	
		CONTRACT		PROJECT NO.	
		R-33130		1005154	

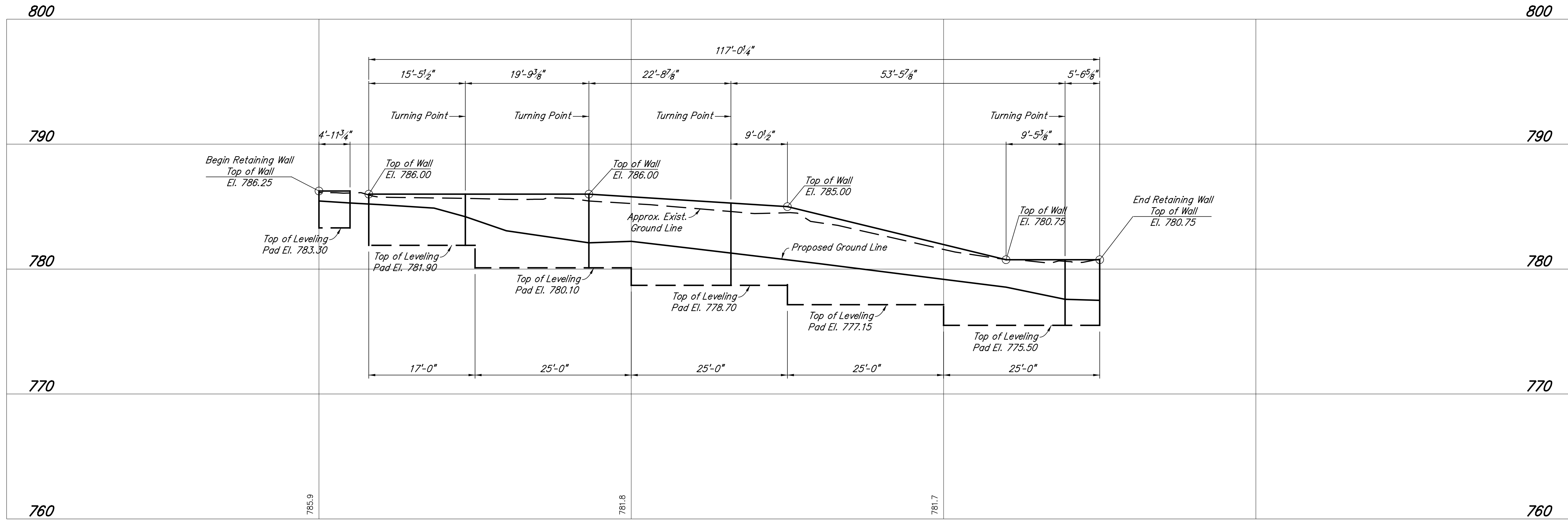
DRAINAGE DETAILS	
LINE "PR-S7A"	



21+00										22+00										23+00										24+00										25+00										26+00																													
																														RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____										INDIANA DEPARTMENT OF TRANSPORTATION										HORIZONTAL SCALE										BRIDGE FILE																			
																																																		1" = 20'										DESIGNATION NO.																			
																																																		VERTICAL SCALE										1005154																			
																																																		1" = 10'																													
																														DESIGNED: _____ ASU										DRAWN: _____ CAK										DRAINAGE DETAILS LINE "A"										SURVEY BOOK										SHEETS									
																																																												53 of 138																			
																														CHECKED: _____ SMC										CHECKED: _____ SMC																				CONTRACT										PROJECT NO.									
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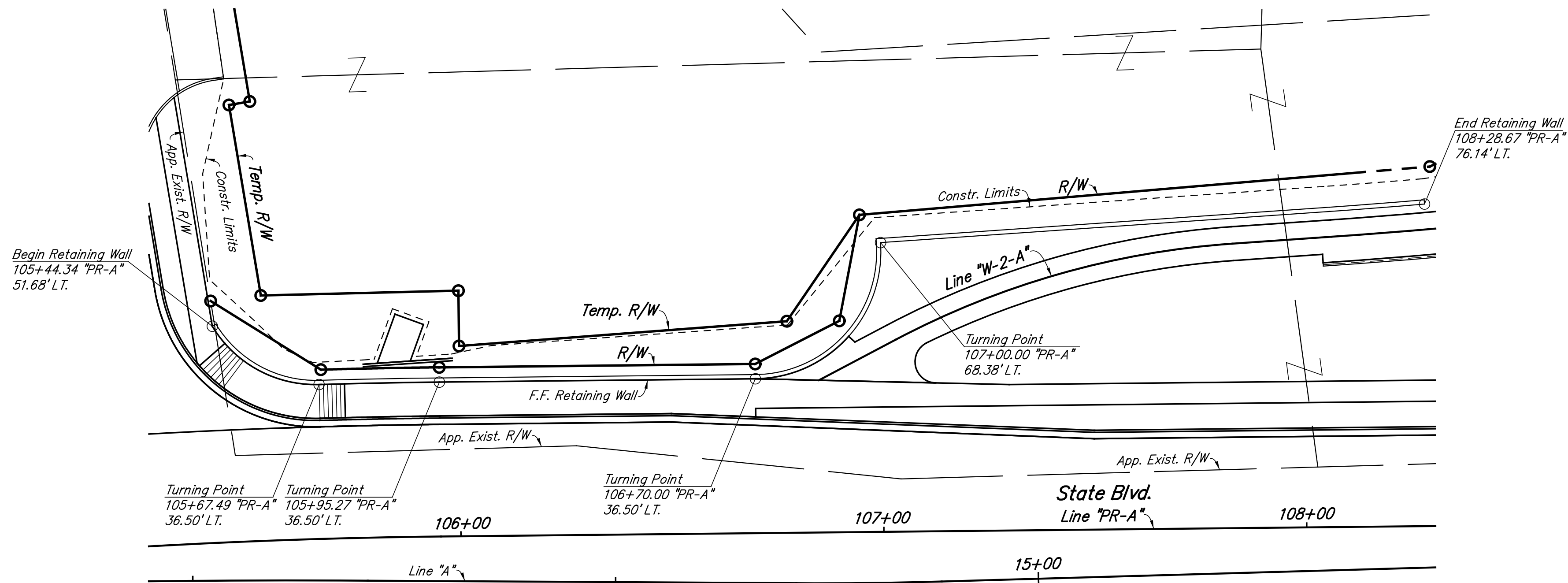


PLAN

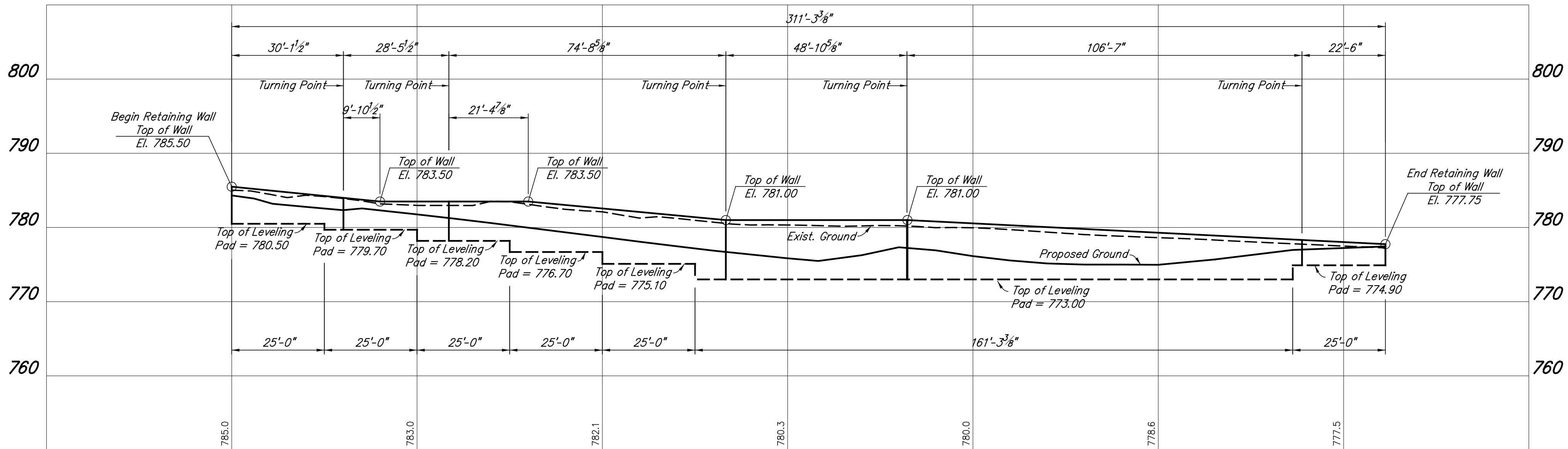


ELEVATION

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						1" = 5'		1005154	
						SURVEY BOOK		SHEETS	
		DESIGNED: ASU DRAWN: CAK CHECKED: SMC CHECKED: SMC		RETAINING WALL #1 DETAILS LINE "W-1-A"		CONTRACT		54 of 138	
						R-33130		PROJECT NO.	
								1005154	

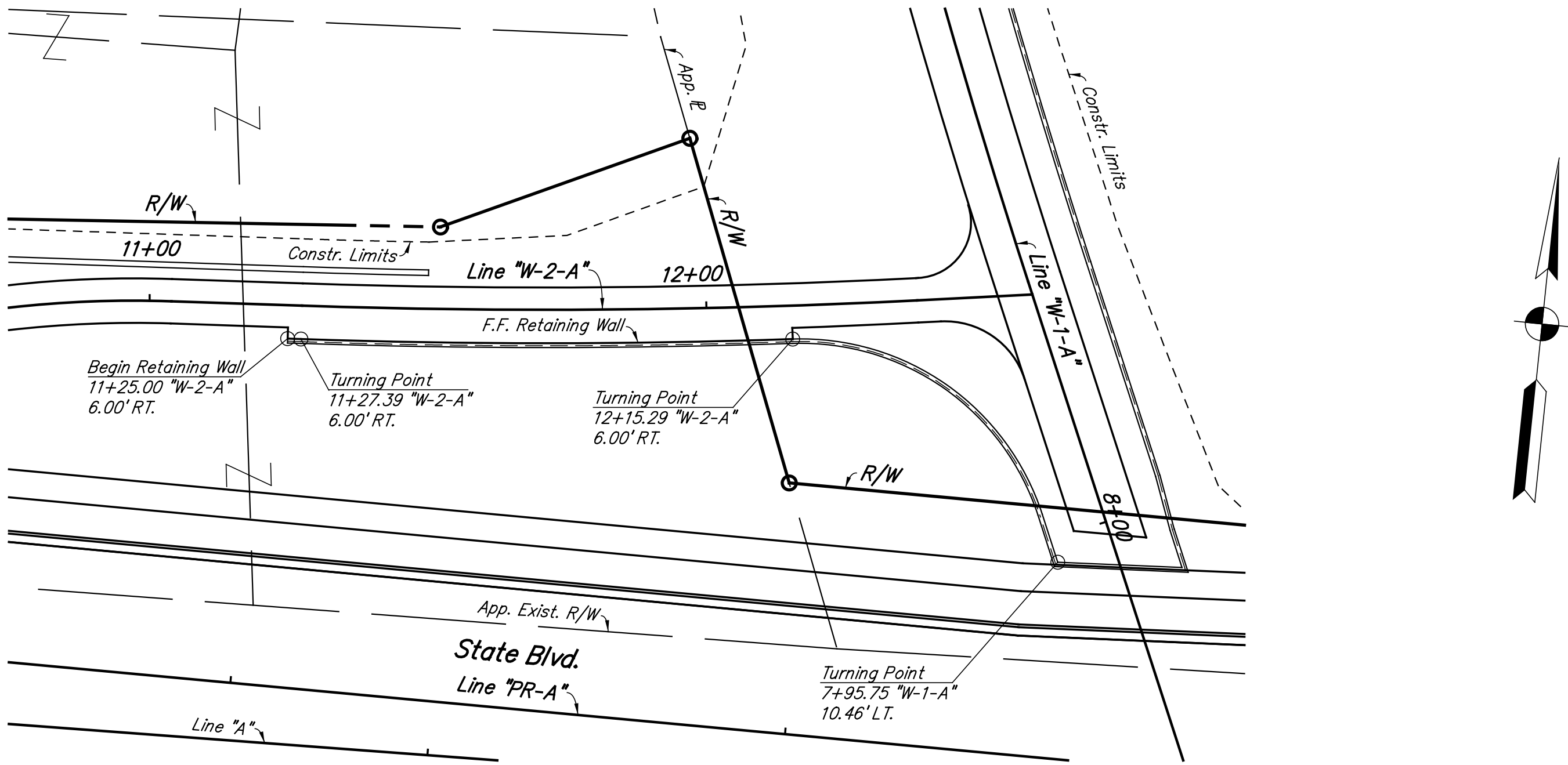


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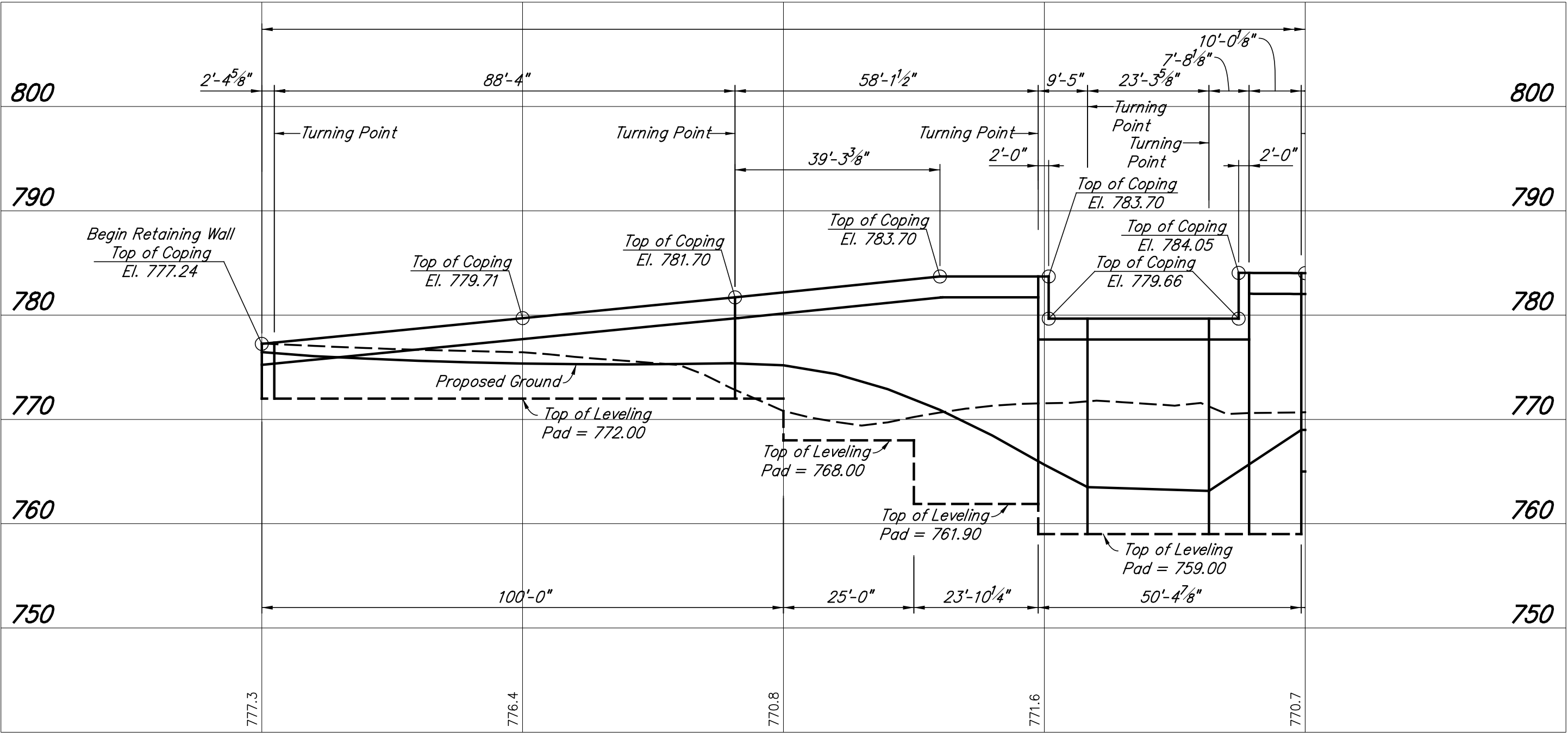


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		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
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		DESIGNED: ASU		DRAWN: CAK			RETAINING WALL #2 DETAILS LINE "W-2-A"		SURVEY BOOK		SHEETS	
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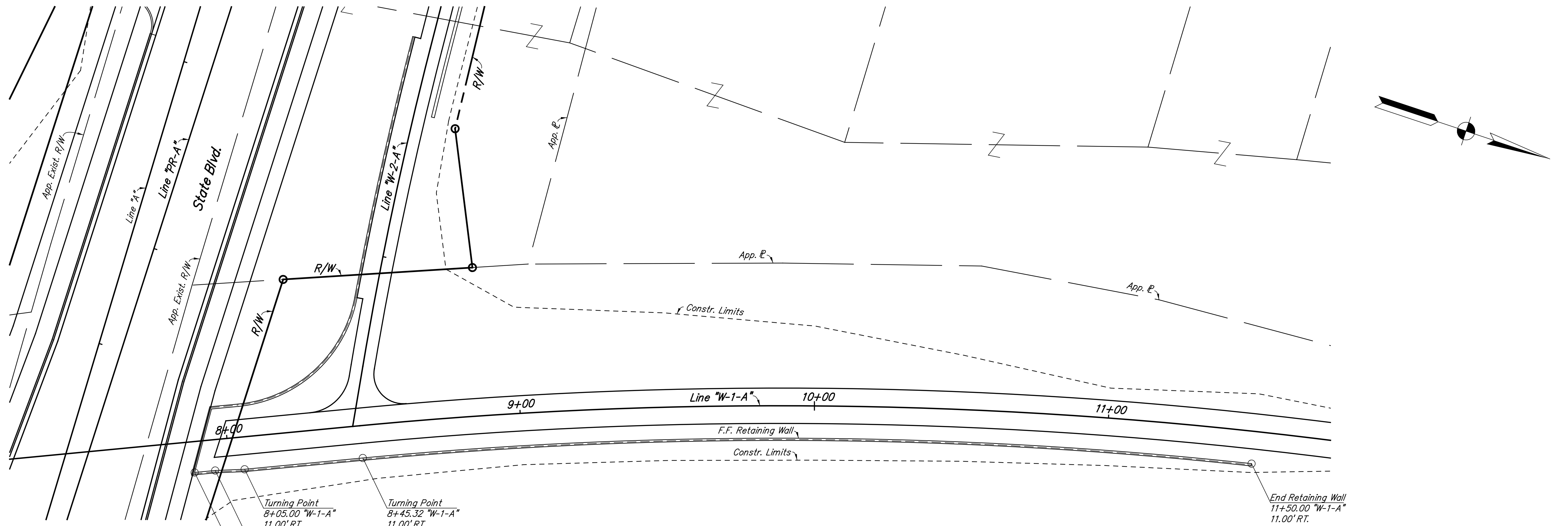


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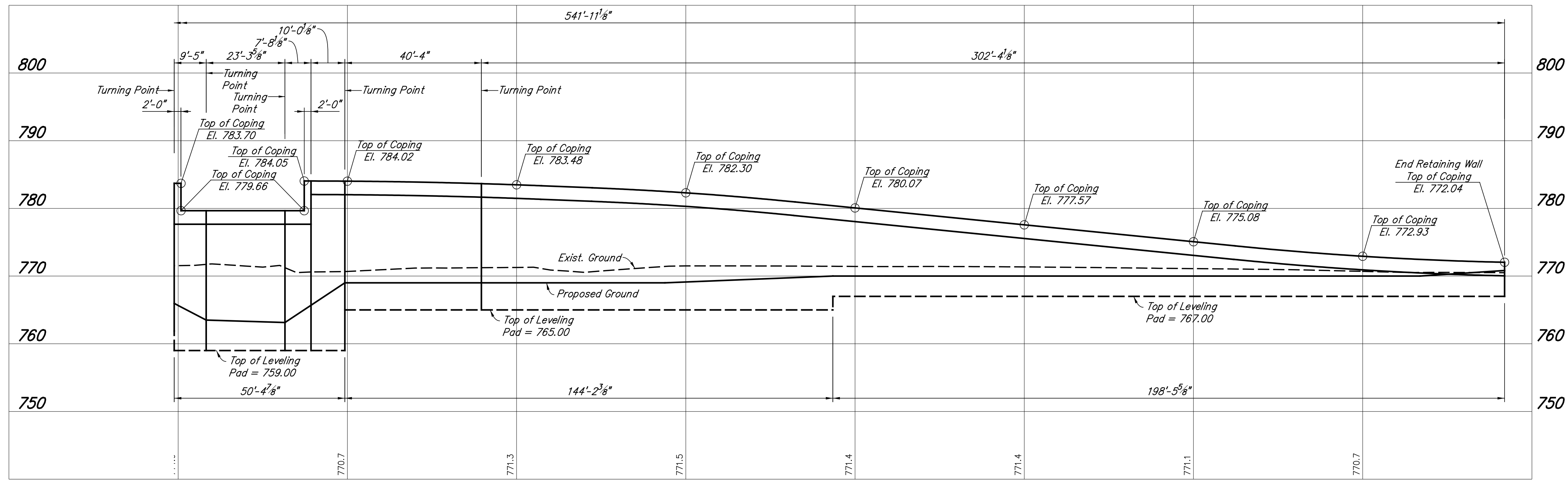


ELEVATION

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			CHECKED: SMC	CHECKED: SMC	CONTRACT R-33130	PROJECT NO. 1005154

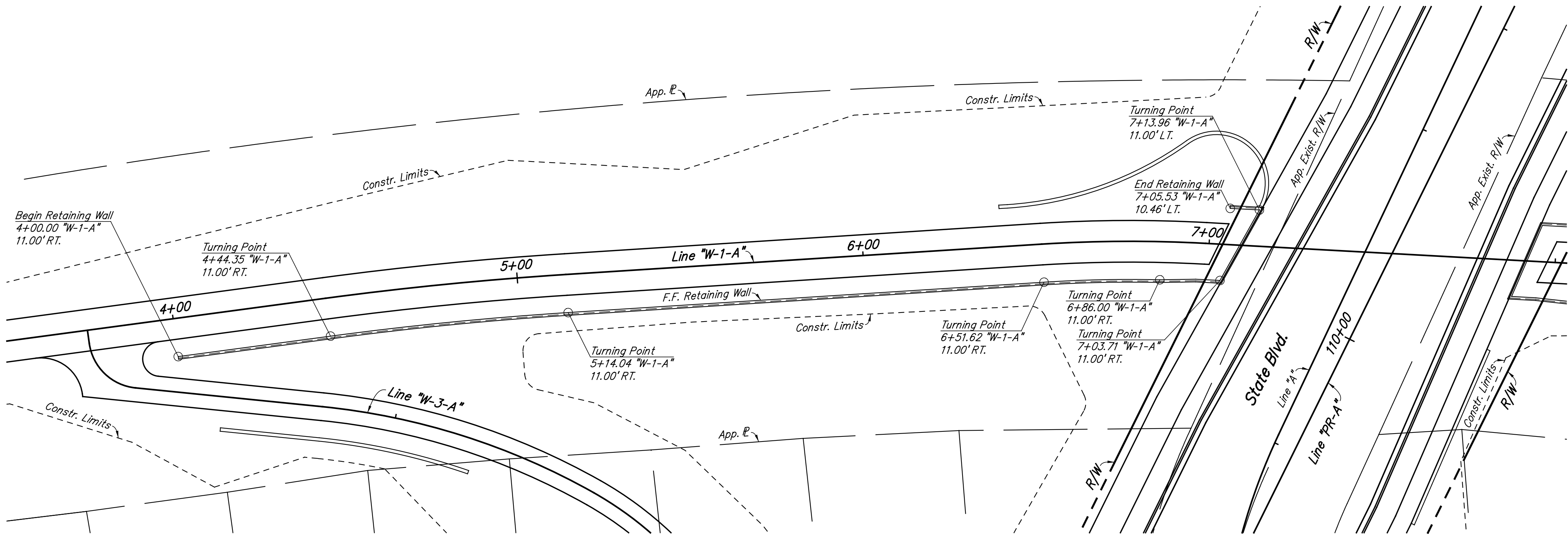


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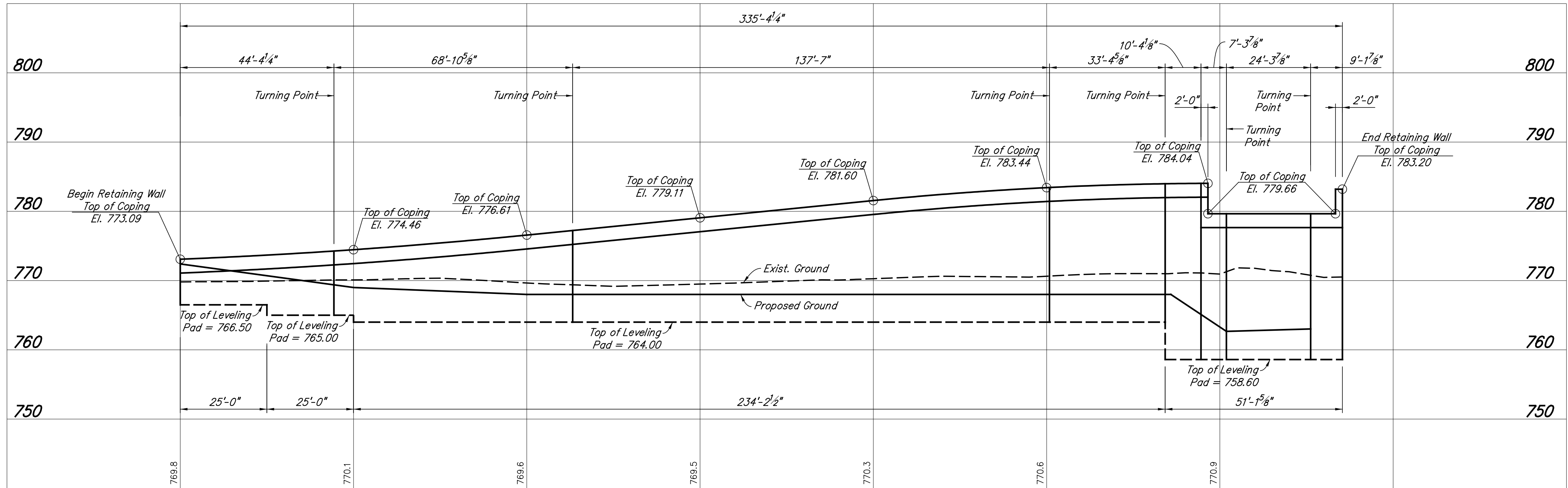


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										1" = 10'		1005154	
										SURVEY BOOK		SHEETS	
		DESIGNED: ASU		DRAWN: CAK				RETAINING WALL #3 DETAILS LINE "W-1-A"		CONTRACT		57 of 138	
										R-33130		PROJECT NO.	
												1005154	
CHECKED: SMC		CHECKED: SMC											

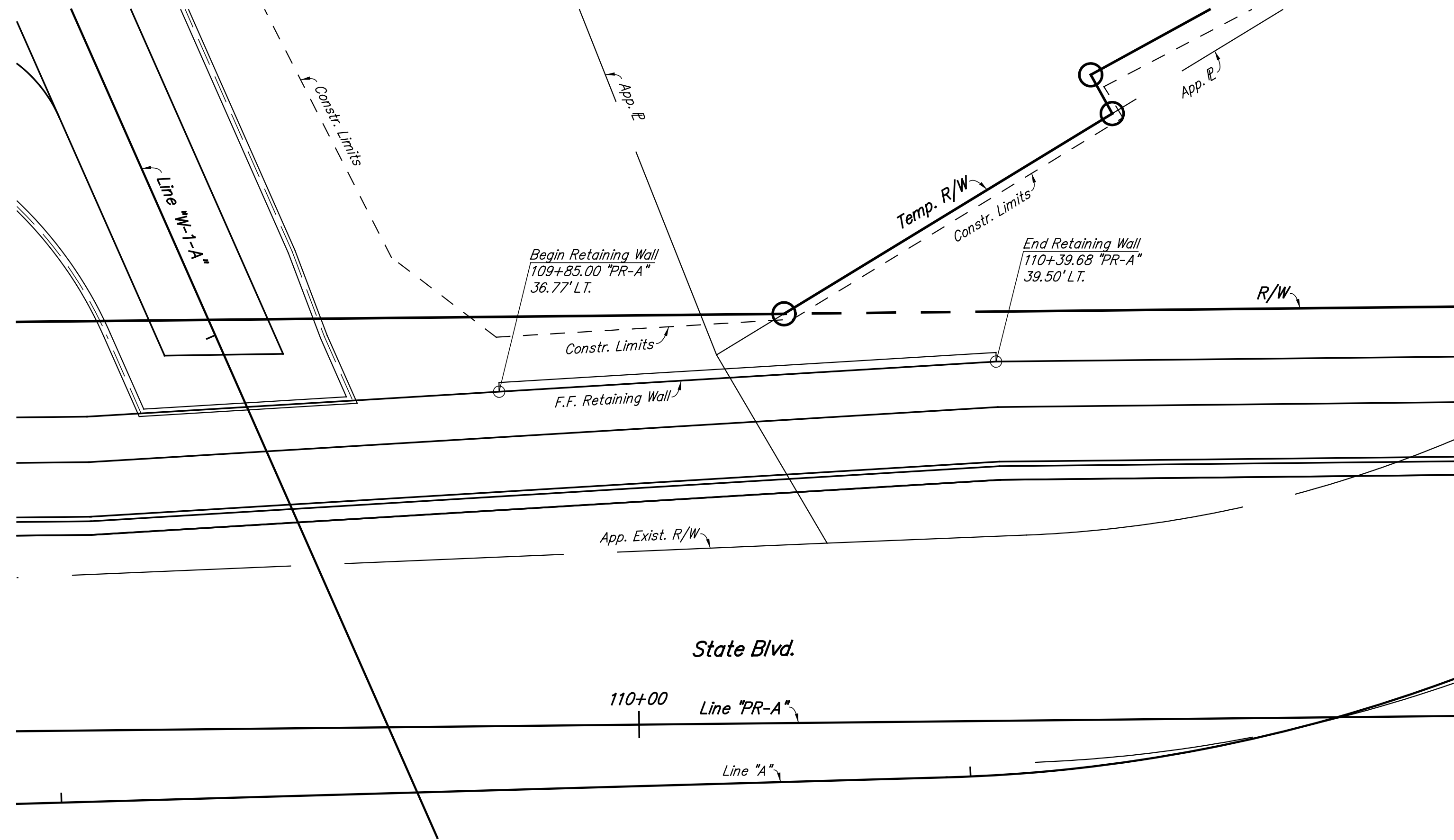


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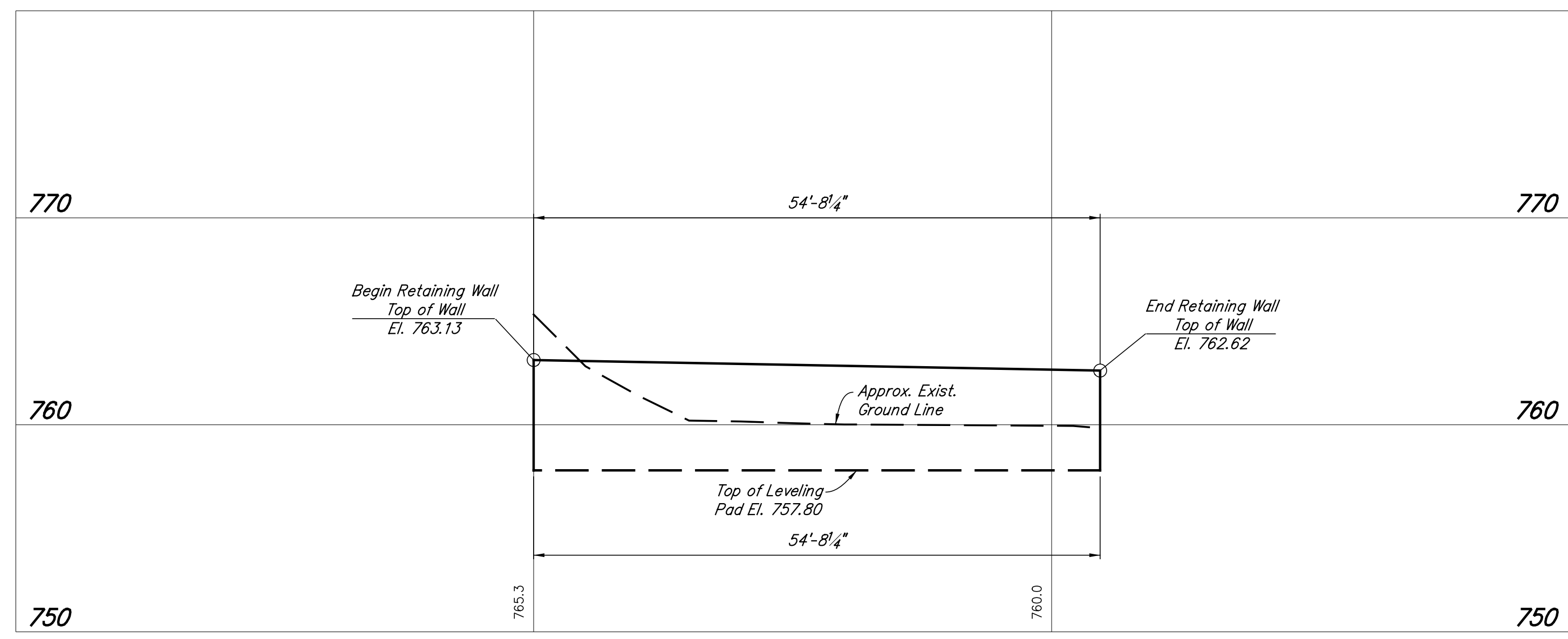


ELEVATION

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						VERTICAL SCALE		DESIGNATION NO.	
						1" = 10'		1005154	
		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		RETAINING WALL #4 DETAILS LINE "W-1-A"	
								SURVEY BOOK	
								SHEETS	
		DESIGNED: ASU		DRAWN: CAK				58 of 138	
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								R-33130	
								PROJECT NO.	
								1005154	

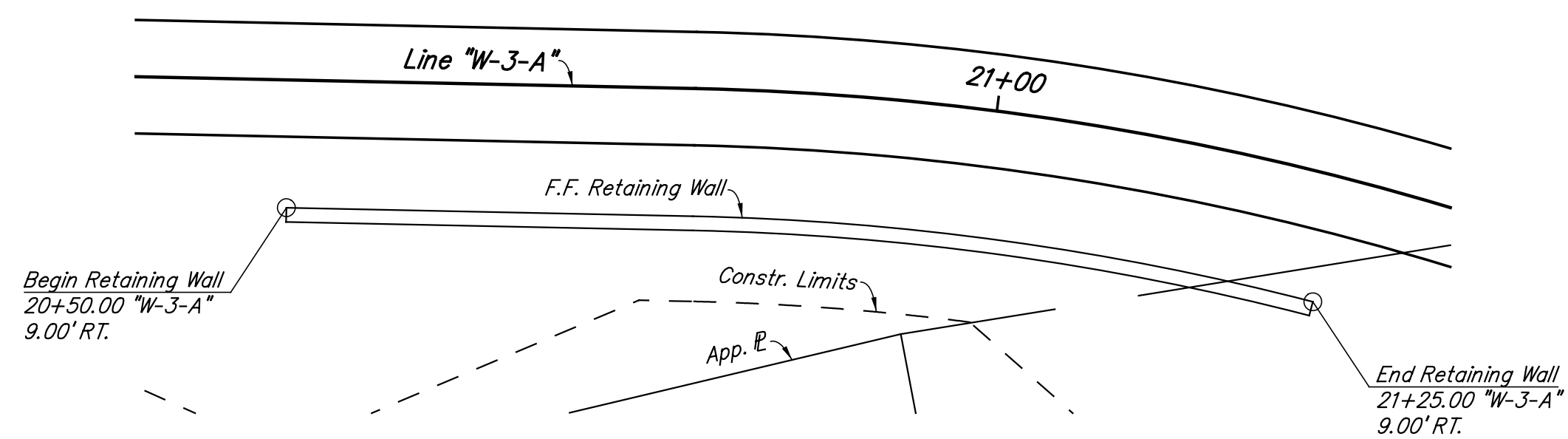


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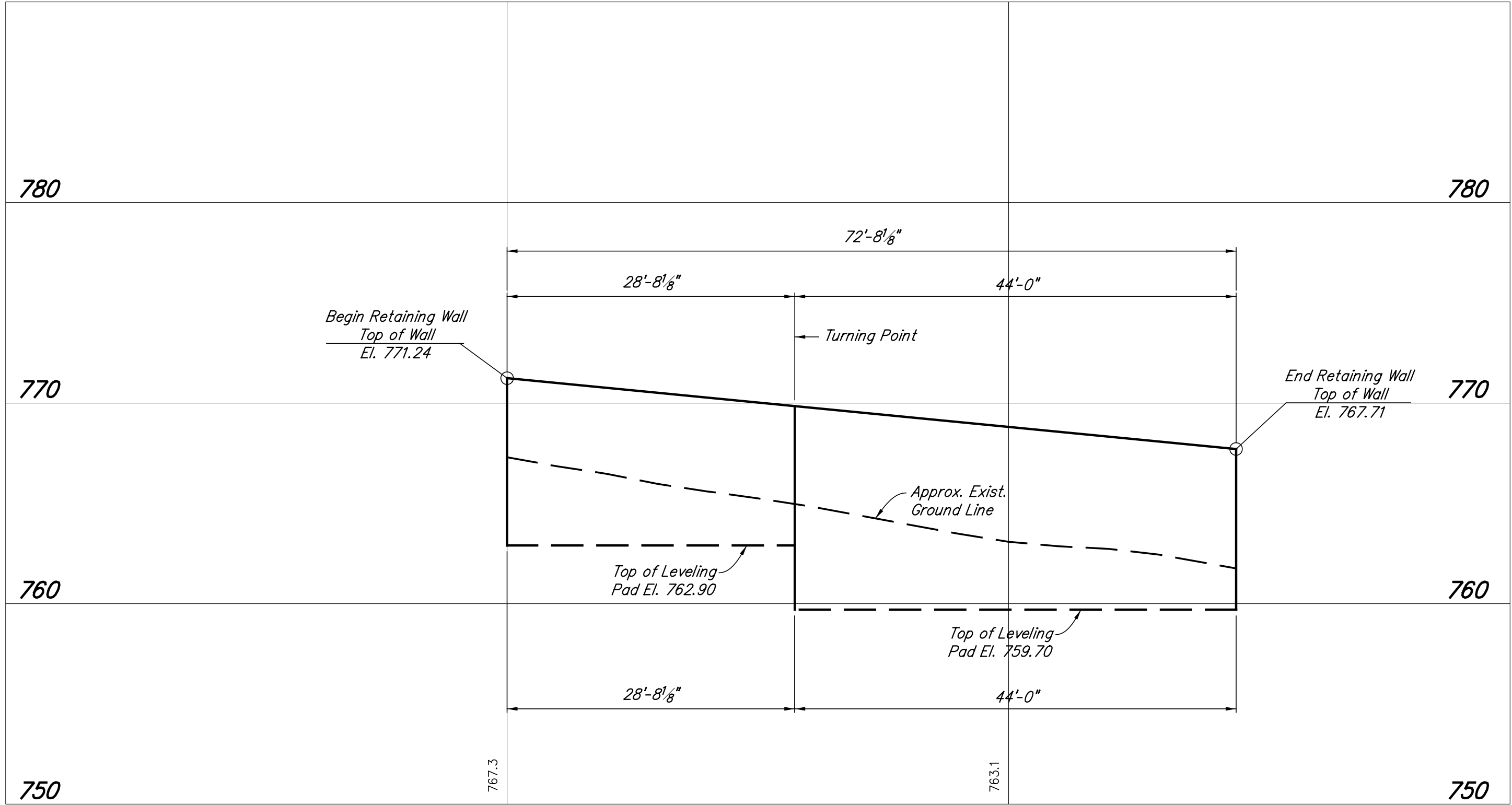


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						VERTICAL SCALE		DESIGNATION NO.	
						1" = 5'		1005154	
				RETAINING WALL #5 DETAILS LINE "PR-A"		SURVEY BOOK		SHEETS	
								59 of 138	
						CONTRACT		PROJECT NO.	
						R-33130		1005154	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		DESIGNED: ASU		DRAWN: CAK	
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PLAN



ELEVATION

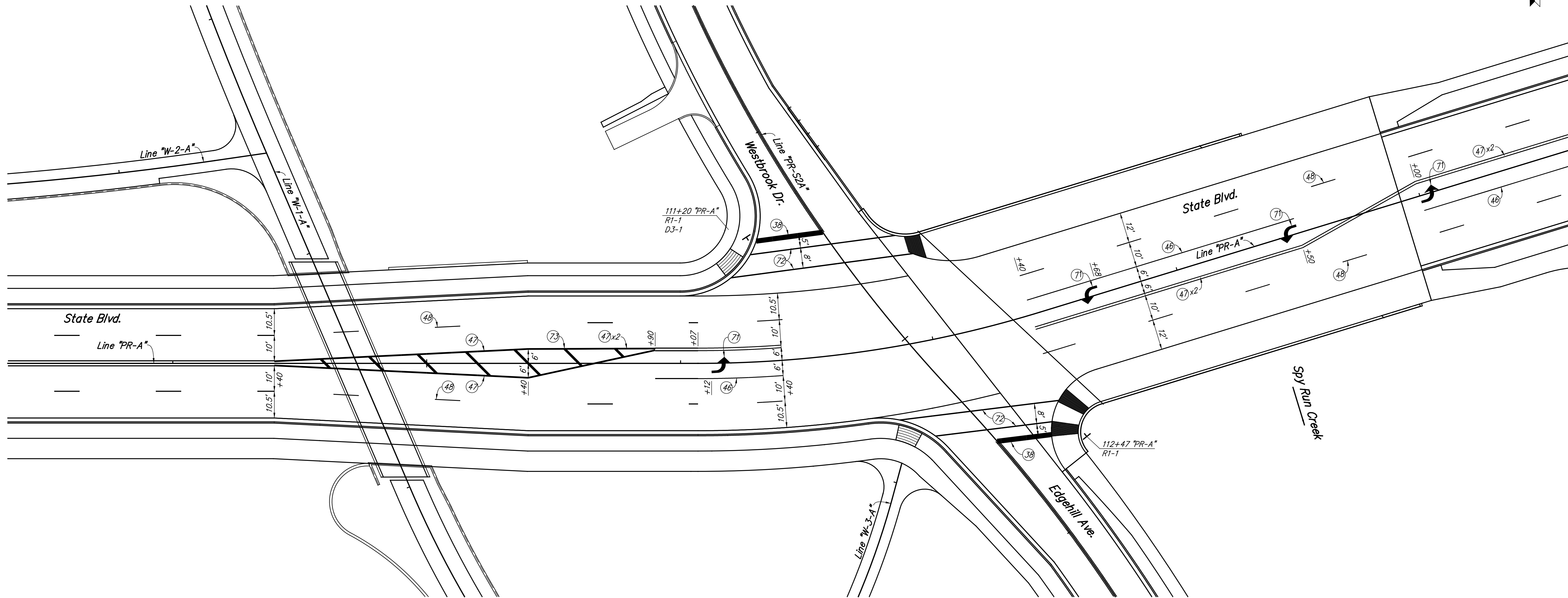
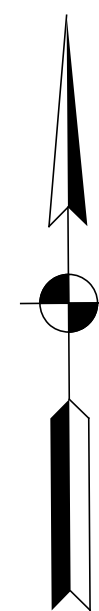
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RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: CAK
CHECKED: SMC	CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION
RETAINING WALL #6 DETAILS LINE "W-3-A"

HORIZONTAL SCALE 1" = 10'	BRIDGE FILE
VERTICAL SCALE 1" = 5'	DESIGNATION NO. 1005154
SURVEY BOOK R-33130	SHEETS 60 of 138 PROJECT NO. 1005154



LEGEND

- | | | |
|---|--|---|
| ③⑧ Transverse Markings, Thermoplastic, Stop Line, White, 24 in. | ④⑨ Line, Thermoplastic, Broken, Yellow, 4 in. | ⑦⑤ Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in. |
| ④⑥ Line, Thermoplastic, Solid, White, 4 in. | ⑦① Pavement Message Markings, Thermoplastic, Lane Indication Arrow | ⑦⑦ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap) |
| ④⑦ Line, Thermoplastic, Solid, Yellow, 4 in. | ⑦② Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in. | ⑦⑧ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap) |
| ④⑧ Line, Thermoplastic, Broken, White, 4 in. | ⑦③ Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in. | |

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

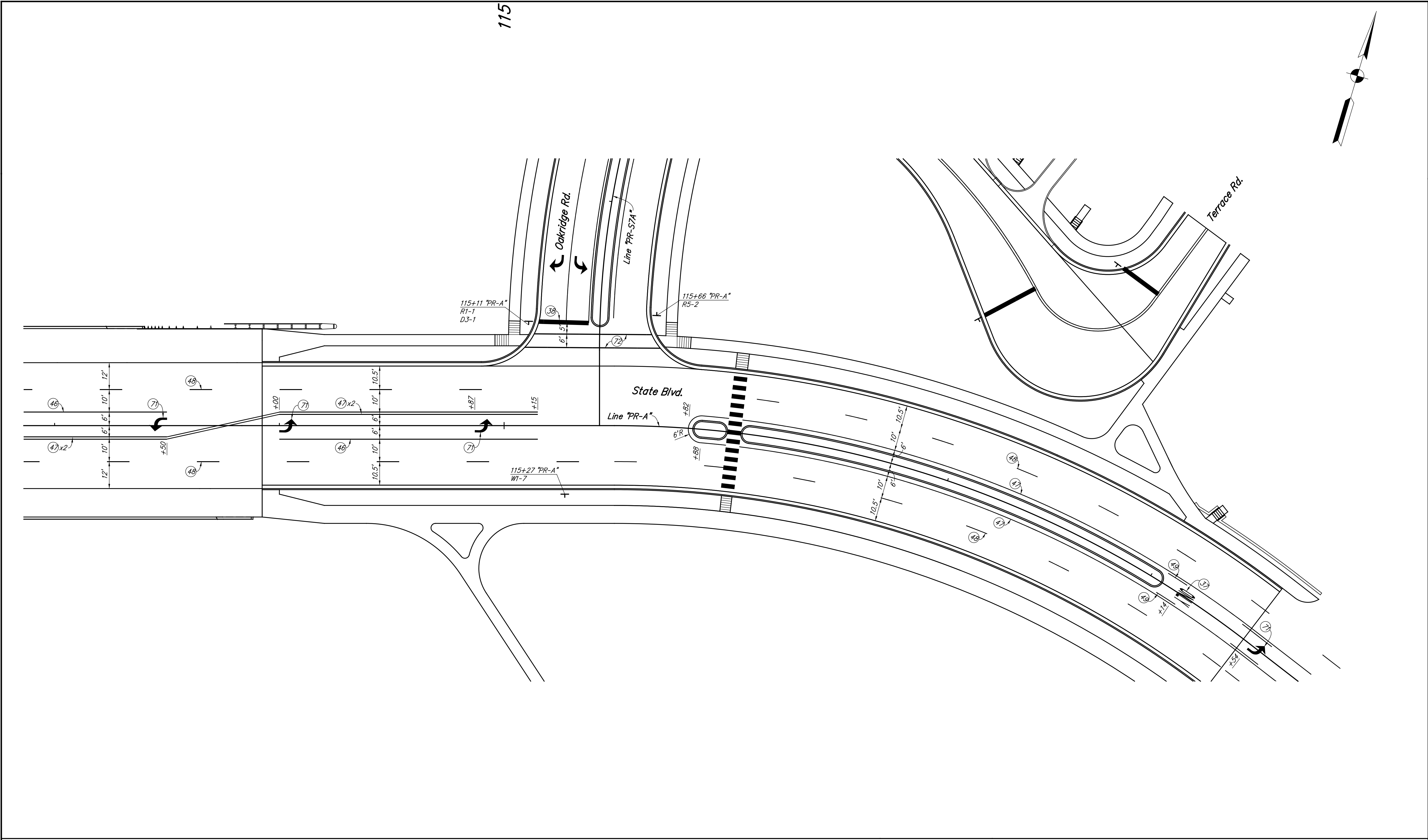
DESIGNED: ASU DRAWN: PJT

CHECKED: SMC CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING &
SIGNING DETAILS - LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE	
1" = 20'		
VERTICAL SCALE	DESIGNATION NO.	
NONE	1005154	
SURVEY BOOK	SHEETS	
	64	of 138
CONTRACT	PROJECT NO.	
R-33130	1005154	

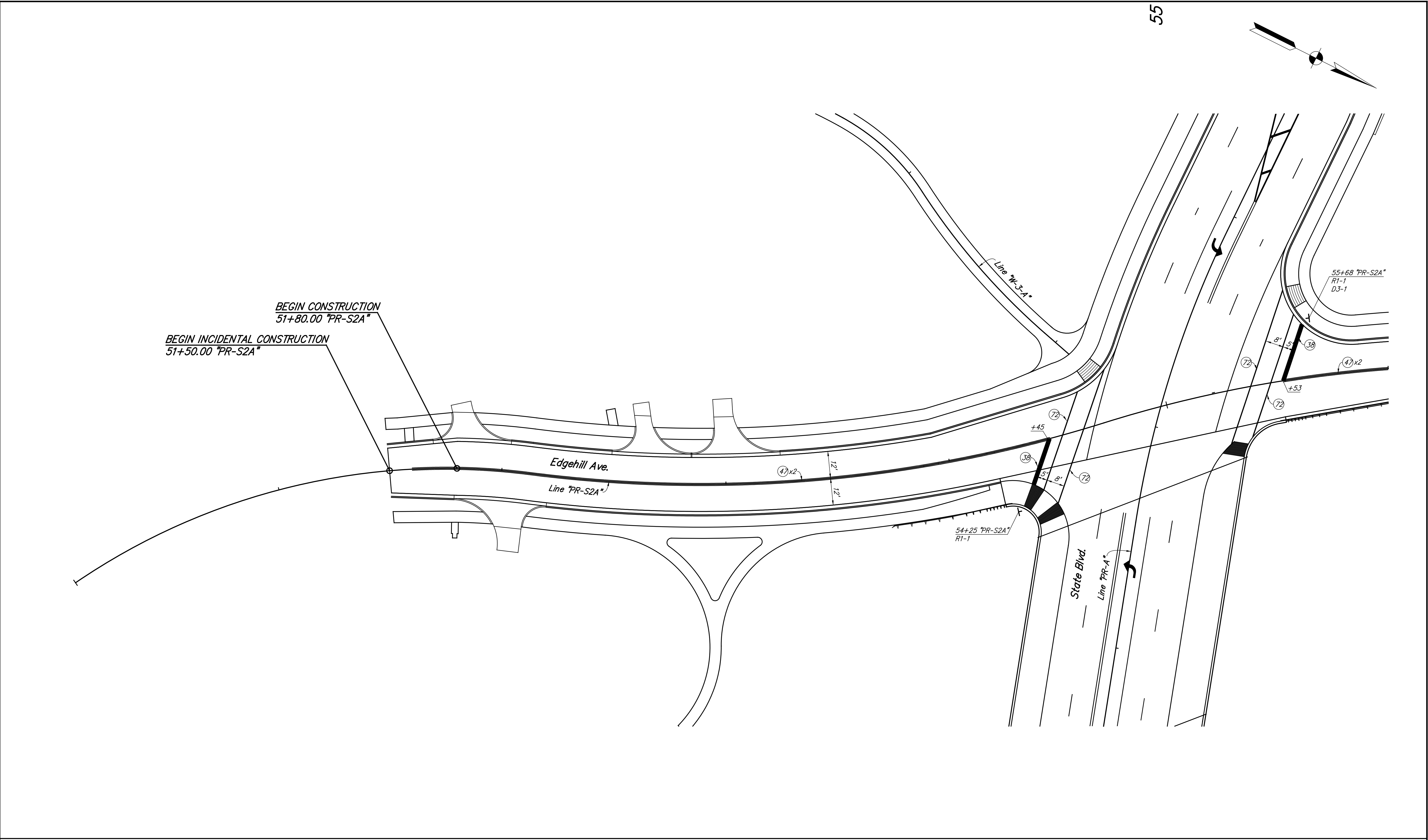


LEGEND					
39	Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	49	Line, Thermoplastic, Broken, Yellow, 4 in.	79	Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in.
46	Line, Thermoplastic, Solid, White, 4 in.	71	Pavement Message Markings, Thermoplastic, Lane Indication Arrow	77	Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)
47	Line, Thermoplastic, Solid, Yellow, 4 in.	72	Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	78	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)
48	Line, Thermoplastic, Broken, White, 4 in.	73	Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.		

	RECOMMENDED FOR APPROVAL			
			DESIGN ENGINEER	DATE
	DESIGNED: ASU	DRAWN: PJT		
	CHECKED: SMC	CHECKED: SMC		

INDIANA DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"	

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE	
VERTICAL SCALE NONE	DESIGNATION NO. 1005154	
SURVEY BOOK	SHEETS	
CONTRACT R-33130	65 of 138	PROJECT NO. 1005154



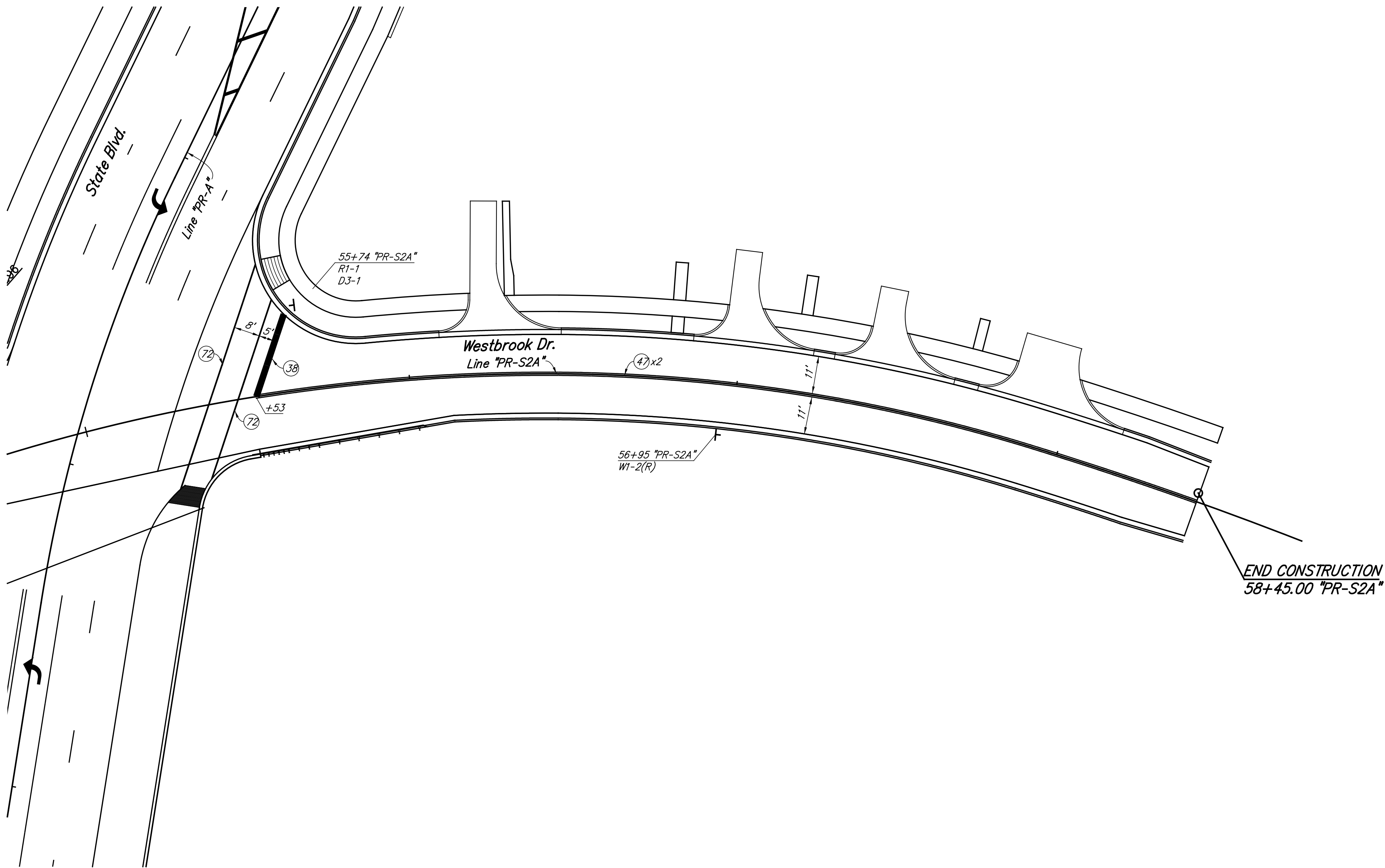
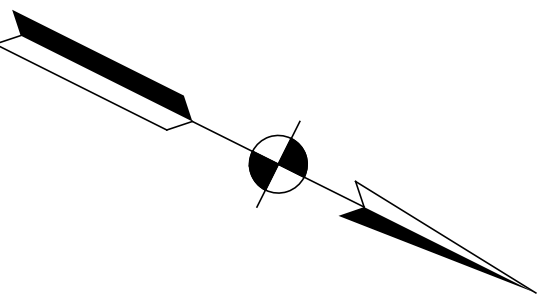
LEGEND

③⑨ Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	④⑨ Line, Thermoplastic, Broken, Yellow, 4 in.	⑦⑨ Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in.
④⑥ Line, Thermoplastic, Solid, White, 4 in.	⑦① Pavement Message Markings, Thermoplastic, Lane Indication Arrow	⑦⑦ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)
④⑦ Line, Thermoplastic, Solid, Yellow, 4 in.	⑦② Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	⑦⑧ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)
④⑧ Line, Thermoplastic, Broken, White, 4 in.	⑦③ Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.	

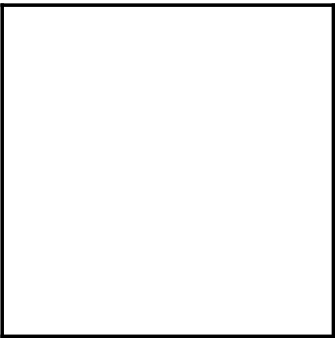
RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: CAK
CHECKED: SMC	CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-S2A"	

HORIZONTAL SCALE	BRIDGE FILE
1" = 20'	
VERTICAL SCALE	DESIGNATION NO.
NONE	1005154
SURVEY BOOK	SHEETS
	66 of 138
CONTRACT	PROJECT NO.
R-33130	1005154



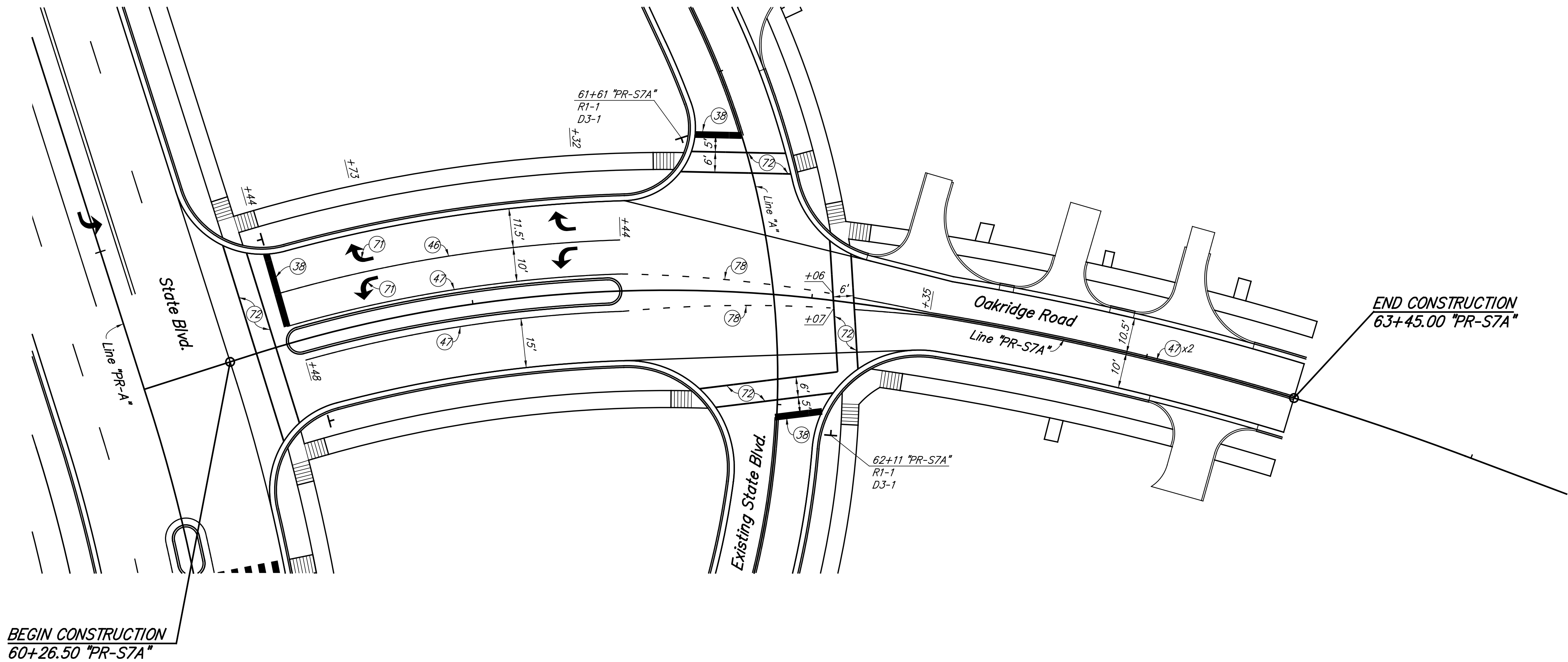
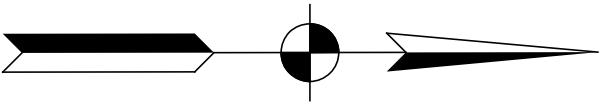
LEGEND					
39	Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	49	Line, Thermoplastic, Broken, Yellow, 4 in.	79	Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in.
46	Line, Thermoplastic, Solid, White, 4 in.	71	Pavement Message Markings, Thermoplastic, Lane Indication Arrow	77	Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)
47	Line, Thermoplastic, Solid, Yellow, 4 in.	72	Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	78	Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)
48	Line, Thermoplastic, Broken, White, 4 in.	73	Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.		



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT			
CHECKED: SMC		CHECKED: SMC			

INDIANA	
DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-S2A"	

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'			
VERTICAL SCALE		DESIGNATION NO.	
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SURVEY BOOK		SHEETS	
		67 of 138	
CONTRACT		PROJECT NO.	
R-33130		1005154	

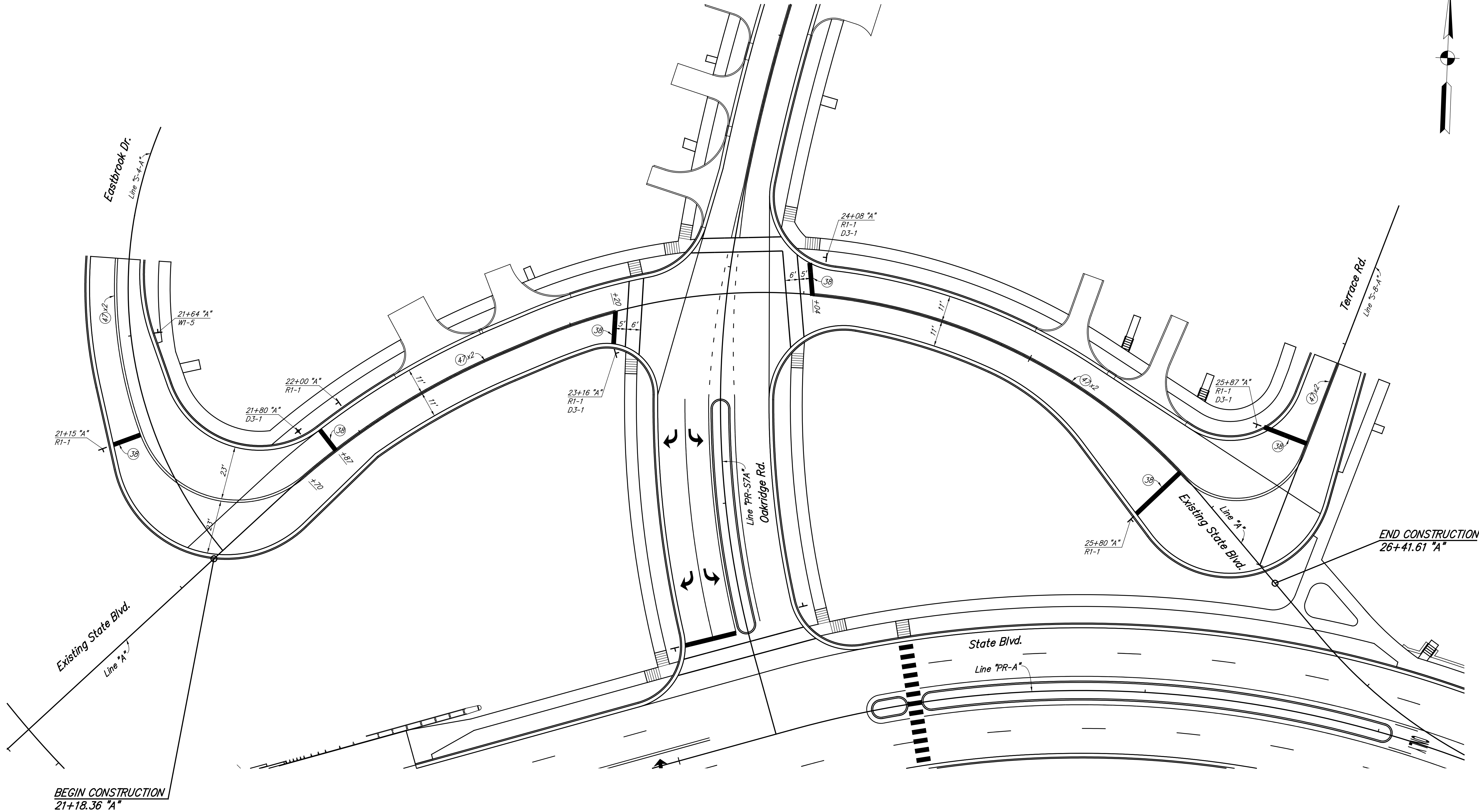
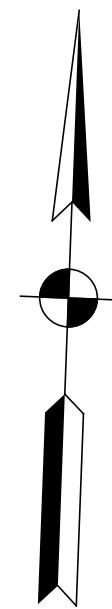


LEGEND					
38 Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	49 Line, Thermoplastic, Broken, Yellow, 4 in.	75 Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in.			
46 Line, Thermoplastic, Solid, White, 4 in.	71 Pavement Message Markings, Thermoplastic, Lane Indication Arrow	77 Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)			
47 Line, Thermoplastic, Solid, Yellow, 4 in.	72 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	78 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)			
49 Line, Thermoplastic, Broken, White, 4 in.	73 Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.				

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: PJT			
CHECKED: SMC		CHECKED: SMC			

INDIANA DEPARTMENT OF TRANSPORTATION	
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-S7A"	

HORIZONTAL SCALE		BRIDGE FILE	
1" = 20'		DESIGNATION NO.	
VERTICAL SCALE		1005154	
NONE			
SURVEY BOOK		SHEETS	
CONTRACT		68 of 138	
R-33130		PROJECT NO.	
		1005154	



BEGIN CONSTRUCTION
21+18.36 "A"

END CONSTRUCTION
26+41.61 "A"

LEGEND

- | | | |
|---|--|---|
| (38) Transverse Markings, Thermoplastic, Stop Line, White, 24 in. | (49) Line, Thermoplastic, Broken, Yellow, 4 in. | (75) Transverse Markings, Thermoplastic, Crosshatch Line, (White), 12 in. |
| (46) Line, Thermoplastic, Solid, White, 4 in. | (71) Pavement Message Markings, Thermoplastic, Lane Indication Arrow | (77) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap) |
| (47) Line, Thermoplastic, Solid, Yellow, 4 in. | (72) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in. | (78) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap) |
| (48) Line, Thermoplastic, Broken, White, 4 in. | (73) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in. | |

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER

DATE

DESIGNED: ASU

DRAWN: PJT

CHECKED: SMC

CHECKED: SMC

INDIANA
DEPARTMENT OF TRANSPORTATION

**PAVEMENT MARKING &
SIGNING DETAILS - LINE "A"**

HORIZONTAL SCALE

1" = 20'

VERTICAL SCALE

NONE

SURVEY BOOK

CONTRACT

R-33130

BRIDGE FILE

DESIGNATION NO.

1005154

SHEETS

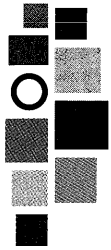
69 of 138

PROJECT NO.

1005154

Appendix B: Early Coordination

- Early Coordination Letter – March 16, 2009
- Early Coordination Mailing List
- Natural Resources Conservation Service – March 20, 2009
- U.S. Department of Homeland Security United States Coast Guard – March 30, 2009
- Indiana Department of Transportation, Fort Wayne District – March 30, 2009
- City of Fort Wayne – April 03, 2009
- Indiana Geological Survey – April 06, 2009
- Indiana Department of Natural Resources, Division of Outdoor Recreation – April 07, 2009
- US Fish and Wildlife Service – April 20, 2009
- US Army Corps of Engineers – May 11, 2009
- Indiana Department of Natural Resources, Division of Water – March 18, 2009
- Indiana Department of Environmental Management Environmental Review Letter – April 24, 2013
- US Fish and Wildlife Service – March 19, 2014
- Land and Water Conservation Fund listing for Allen County



AMERICAN
STRUCTUREPOINT
INC.

March 16, 2009

Ms. Susan Ostby
Division of Outdoor Recreation
Indiana Department of Natural Resources
402 West Washington Street
271 Government Center West
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Project No. IN20071404

Dear Ms. Ostby:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle Map, in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. Please see the attached maps and aerial photographs for specific project location.

The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes, while correcting the sub-standard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center, 2-way, left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

You are asked to review this information and provide any comments you may have relative to anticipated impacts of the project on areas in which you have jurisdiction or special expertise.

To facilitate the development of this project, you are asked to reply by April 15, 2009. If no response is received by that date, it will be assumed you have no comments. Your timely cooperation in the development of this project is appreciated. If there are any questions or if additional information is needed, please contact me at (317) 547-5580 or by e-mail at hsteele@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Hayley M. Steele
Environmental Scientist

HMS:ecb

Enclosure

IN20071404





AMERICAN STRUCTUREPOINT, INC.

Ms. Susan Ostby

March 16, 2009

Page 2

cc: US Fish and Wildlife Service
Natural Resource Conservation Service
Indiana Geological Survey
Indiana Department of Transportation
Indiana Department of Environmental Management
Indiana Department of Natural Resources
US Army Corps of Engineers
Allen County Highway Department
Allen County Sherriff's Department
Allen County Executive Board of Health
Allen County Department of Planning Services
Allen County Parks and Recreation
Allen County Board of Commissioners
Allen County Engineer
Allen County Surveyor
City of Fort Wayne Mayor's Office
Fort Wayne City Engineer



Allen County, Indiana



Project Location

Not to Scale



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INC.
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Project Location Map

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

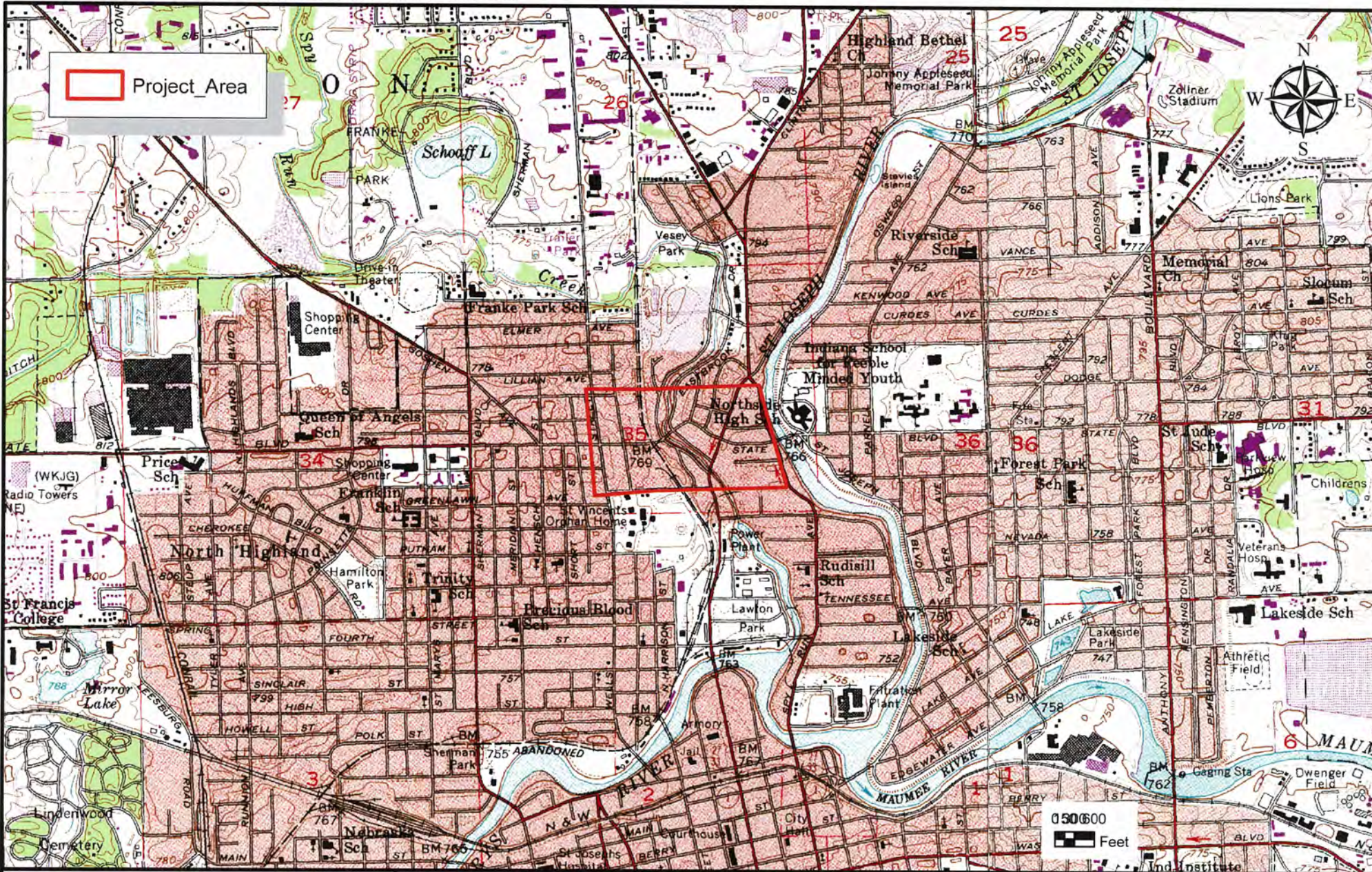
State Boulevard Reconstruction

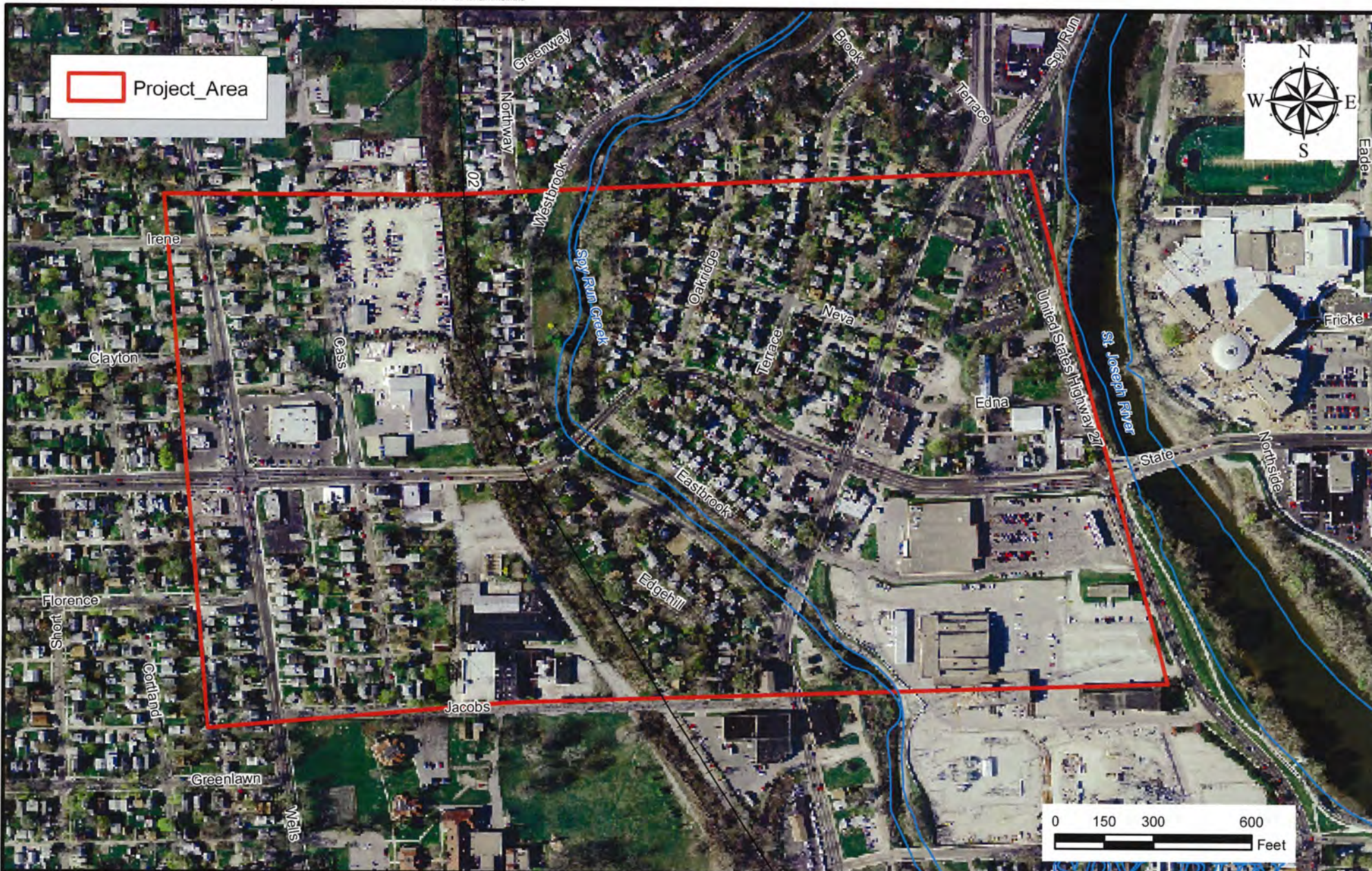
Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Appendix B
Page 4 of 28
Sheet 1 of 3

Date: 03/12/2009

P:\IN\2007\1404\ID Drawings\Environmental\Arcview\Exhibits\Early Coordination\IN20071404 EV.MAP\2009-3-12 StateLocation.hms.mxd





Project Location - 2005 Aerial Photography

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Appendix B
Page 6 of 28

Date: 03/12/2009

Sheet 3 of 3

Early Coordination Mailing List
Project Name: State Blvd. Reconstruction
Route/Street: State Blvd.
Des No.: 0400587
Project No.: IN20071404

Salu	Name	Title	Attn:	Agency/Company	Address 1	Address 2	City	State	Zip
Ms. McCloskey	Ms. Elizabeth McCloskey	, Supervisor		US Fish and Wildlife Service	Northern Indiana Ecological Services Suboffice	PO Box 2616	Chesterton	Indiana	46304-2616
Ms. Hardisty	Ms. Jane Hardisty		State Conservationist	US Natural Resources Conservation Service	6103 Lakeside Boulevard		Indianapolis	Indiana	46278
Ms. Hasenmueller	Ms. Nancy Hasenmueller	, Head	Indiana Geological Survey	Environmental Geology Section	Indiana University	611 North Walnut Grove	Bloomington	Indiana	47405
Mr. Keefer	Mr. Jim Keefer	, Manager		INDOT – Aeronautics Section	901 Government Center North	100 North Senate Avenue	Indianapolis	Indiana	46204
Mr. Easterly	Mr. Thomas Easterly	, Commissioner		Indiana Department of Environmental Management	Submit electronically - DO NOT MAIL		Indianapolis	Indiana	
Mr. Carter	Mr. Robert Carter	, Director	Attn: Christie Stanifer, Environmental Review Coordinator	Indiana Department of Natural Resources	264 Government Center West	402 West Washington Street	Indianapolis	Indiana	46204
Mr. Shelton	Mr. Doug Shelton	, Chief, Regulatory Office	CELRL-OP-FN	US Army Corps of Engineers, Louisville District	Regulatory Branch North	PO Box 59	Louisville	Kentucky	40201-0059
Mr. Bloom	Mr. Robert Bloom	, Commander		Ninth Coast Guard District	1240 East 9th Street		Cleveland	Ohio	44199
Ms. Ostby	Ms. Susan Ostby		Division of Outdoor Recreation	Indiana Department of Natural Resources	271 Government Center West	402 West Washington Street	Indianapolis	Indiana	46204
Mr. Alderman	Mr. Robert Alderman	, District Deputy Commissioner	Fort Wayne District	INDOT - Fort Wayne District	5333 Hatfield Road		Fort Wayne	Indiana	46808
Mr. Hartman	Mr. Bill Hartman	, Director		Allen County Highway Department	1 West Superior Street, Room 211		Fort Wayne	Indiana	46802
Sheriff Fries	Mr. Kenneth Fries	, Sheriff		Allen County Sheriffs Department	715 South Calhoun Street	Room 101 Courthouse	Fort Wayne	Indiana	46802-1898
Dr. Schmitt	Dr. Gregory S. Schmitt, MD	, President		Allen County Executive Board of Health	1 East Main Street, 5th Floor		Fort Wayne	Indiana	46802
Ms. Bowman	Ms. Kimberly Bowman	, Executive Director		Department of Planning Services	630 City-Council Building	1 East Main Street	Fort Wayne	Indiana	46802
Mr. Baxter	Mr. Jeff Baxter	, Superintendent		Allen County Parks and Recreation	7324 Yohne Road		Fort Wayne	Indiana	46809
				Allen County Board of Commissioners	1 East Main Street, Room 200		Fort Wayne	Indiana	46802
Mr. Thomson	Mr. Mike Thomson	, County Engineer		Allen County Engineer	1 West Superior Street, Room 211		Fort Wayne	Indiana	46802
Mr. Frisinger	Mr. Allen D. Frisinger	, County Surveyor		Allen County Surveyor	1 East Main Street, Room 610		Fort Wayne	Indiana	46802-1804
Dr. Robinson	Dr. Wendy Robinson	, Superintendent		Fort Wayne Community Schools	1200 South Clinton Street		Fort Wayne	Indiana	46802
Mr. DeFord	Mr. Charles DeFord			North Side High School	475 East State Boulevard		Fort Wayne	Indiana	46805
				Imagine Master Academy	2000 North Wells Street		Fort Wayne	Indiana	46808
				Forest Park Elementary School	2004 Alabama Avenue		Fort Wayne	Indiana	46805
Mayor Henry	Mr. Tom Henry	, Mayor		City of Fort Wayne	One Main Street		Fort Wayne	Indiana	46802
Mr. Ross	Mr. Dave Ross	, City Engineer		City of Fort Wayne	One Main Street		Fort Wayne	Indiana	46802

United States Department of Agriculture



Natural Resources Conservation Service
6013 Lakeside Blvd.
Indianapolis, IN 46268

March 20, 2009

Hayley M. Steele
Environmental Specialist
American Consulting, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Dear Ms. Steele:

The proposed project to make improvements to State Boulevard in the City of Ft. Wayne, Allen County, Indiana, as stated in your letter dated March 16, 2009, will not cause a conversion of prime farmland.

If you need further information, please contact Lisa Bolton at 317-290-3200, extension 342.

Sincerely,

A handwritten signature in black ink that reads "Jane E. Hardisty". The signature is written in a cursive style.

JANE E. HARDISTY
State Conservationist

U.S. Department of
Homeland Security

United States
Coast Guard



Commander (dpb)
Ninth Coast Guard District
1240 E. Ninth Street, Room 2025
Cleveland, OH 44199-2060

Phone: (216) 902-6087
FAX: (216) 902-6088

16590
B-029/wbs
March 30, 2009

Ms. Hayley Steele – Environmental Scientist
American Structurepoint Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

Dear Ms. Steele,

I am responding to your letter dated March 16, 2009 regarding the State Street Bridge over Spy Run Creek in Allen County, Indiana.

The proposed project is located on a waterway that is in Advance Approval status for Coast Guard Bridge Permitting purposes. Accordingly, a Coast Guard Bridge Permit is not required.

Though a Coast Guard Bridge Permit is not required, you must still comply with the requirements of other federal, state, or local agencies. Please ensure these requirements are satisfied.

Please contact me at (216) 902-6087 if you have further questions or concerns regarding this matter.

Sincerely,

A handwritten signature in black ink, appearing to read "Scot M. Striffler".

SCOT M. STRIFFLER
Chief, Bridge Branch
By direction of Commander,
Ninth Coast Guard District

March 30, 2009

Ms. Hayley M. Steele
Environmental Scientist
American Structure Point Inc.
7260 Shadeland Station
Indianapolis, IN 46256

Re: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des No. 0400587
Project No. IN20071404

Dear Ms. Steele,

The Indiana Department of Transportation (INDOT), Fort Wayne District, has received your Early Coordination Letter Dated March 16, 2009. Upon review of the project and surrounding area we have the following concern:

This project will be taking place within the National Register Eligible Brookview/Irvington Park Historic District. This neighborhood is eligible due to the layout of the streets following Spy Run Creek. Taking of right-of-way in the area will most likely constitute a Section 4(f) impact. Due to these situations, minimization of impacts in this area should be considered by multiple alternatives to show the proposed plan is the most feasible and prudent. Context sensitive design to fit the historic setting of the neighborhood should also be investigated.

INDOT offers this concern to advise the Consultant and the City of Fort Wayne of the necessary timeline for completion of all Section 106 and Section 4(f) impacts.

Please keep us updated of changes in scope and concerns that arise during the design phase. Feel free to contact me with any questions directly at (260) 969-8302 or by email at ddidion@indot.in.gov.

Thanks,

David J. Didion
Environmental Scientist
INDOT - Fort Wayne District

EmailCc: David Ross, Fort Wayne City Engineer
Mike Fitch, INDOT - Fort Wayne District LPA Coordinator



CITY OF FORT WAYNE

THOMAS C. HENRY, MAYOR

April 3, 2009

Ms. Hayley M. Steele
Environmental Scientist
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
DES. No: 0400587
Project No. IN20071404

Dear Ms. Steele:

The City of Fort Wayne has received your early coordination letter dated March 16, 2009, for the above project and have the following comments:

The area of the Spy Run Creek that falls within the limits of this project is in a flood plain and has been subjected to chronic flooding every year for the past several years. The City has implemented a voluntary flood buyout program, which is funded entirely with local utility funds, to remove residents that live on the banks of the Spy Run Creek between State Blvd and Clinton Street. All properties on the west bank of the Spy Run Creek within these limits have been purchased and the homes demolished. Last year, the City's Utility Department constructed a rain garden and an earthen berm on the site of these demolished homes to protect the homes further west of the creek.

The properties on the east bank of the Spy Run Creek were also identified to be removed as part of the voluntary buy out program, and several of these properties were acquired and demolished. However, the City was informed by the Federal Highway Administration last year that the remaining properties could not be removed under the voluntary buyout program as they fall within the footprint of the proposed State Blvd Realignment project, which has been funded by the Federal Surface Transportation Program. The acquisition of these properties would therefore need to follow Uniform Relocation Act requirements including Section 106 review and environmental documentation. These properties are still occupied and were exposed to the most recent flooding event that occurred last month. It is the City's intent to ultimately remove all these homes on the east bank and construct a rain garden with an earthen berm to protect the properties further to the east.

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One Main St. • Fort Wayne, Indiana • 46802-1804 • www.cityoffortwayne.org

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Appendix B
Page 11 of 28



CITY OF FORT WAYNE

THOMAS C. HENRY, MAYOR

Contributing to the above flooding of the area is the inadequate opening of the bridges on State Blvd and Clinton Street over Spy Run Creek. INDOT is currently in the process of developing a project which will replace the Clinton Street bridge. With the completion of both these projects, we expect property damage along the banks on the Spy Run Creek to be substantially reduced in this area.

Every year the City spends thousands of dollars to protect homes in this area from chronic flooding. We have identified several initiatives, including the voluntary buyout program and the construction of rain gardens. This project will be another critical initiative in addressing the flood related issues in this area in addition to being a project identified in the City's comprehensive transportation plan.

It is our desire to get this project developed and relocate affected property owners away from the flood plain as quickly as possible.

Sincerely,

A handwritten signature in black ink, appearing to read "Thomas C. Henry", written over a horizontal line.

Thomas C. Henry
Mayor

SAFE CITY • QUALITY JOBS • B.E.S.T.

One Main St. • Fort Wayne, Indiana • 46802-1804 • www.cityoffortwayne.org

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Appendix B
Page 12 of 28



812-855-7428

INDIANA
GEOLOGICAL SURVEY

April 6, 2009

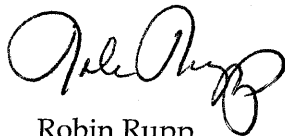
Hayley M. Steele
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

Dear Ms. Steele,

This is in response to your letter dated March 16, 2009, Re: State
Boulevard Reconstruction, Fort Wayne, Allen County, Indiana, Des. No.
0400587, Project No. IN20071404.

The State Boulevard Reconstruction project should not affect nor be
affected by the geology of the project site.

Sincerely,



Robin Rupp
Geologist

April 7, 2009

Ms. Hayley M. Steele, Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

Re: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana

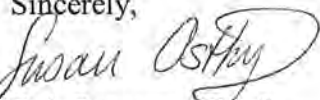
DES# 0400587

Dear Ms. Steele:

This letter is in response to your request for a 6(f)3 determination regarding the proposed State Boulevard Reconstruction located in Fort Wayne, Allen County, Indiana. Through your description of the project our department determined that there will be no negative effect on any site currently encumbered under 6(f)3 through the federally sponsored Land and Water Conservation Fund (LWCF). There are no LWCF properties within the project area; therefore there will be no taking of LWCF property out of outdoor recreational use. We recommend consulting the Fort Wayne Department of Parks and Recreation during your 4f investigation as there appears to be a park and a river greenway trail within/near the proposed project.

Thank you for consulting with our department. If you have other question or concerns please do not hesitate to contact Susan Ostby at 317-232-4074.

Sincerely,

(for) 

Bob Bronson, Chief
State and Community Outdoor Recreation Planning Section
Division of Outdoor Recreation, IDNR

RJB:sdo



United States Department of the Interior

Fish and Wildlife Service



Bloomington Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

April 20, 2009

Ms. Hayley M. Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Project No.: Des. 0400587; IN20071404
Project: State Boulevard Reconstruction
Location: Fort Wayne, Allen County

Dear Ms. Steele:

This responds to your letter dated March 16, 2009, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project consists of widening a 2-lane section of State Boulevard between Clinton Street and Cass Street to 5 lanes, while correcting a sub-standard curve. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated multi-use trail bridge over State Boulevard at an abandoned railroad right-of-way which is being converted into a trail.

The land use in the area is a mix of residential and commercial, but most of the work would be within the residential area where the substandard curve is found. It appears that some yards, and maybe even some houses, may need to be taken to widen and reconstruct the curve area.

Shade trees and other landscaping that provide habitat for songbirds and small mammals are likely to be lost. Therefore we request that trees lost to the project be replaced as close to the project impact area as possible, such as along Spy Run Creek, the St. Joseph River, and the new trail.

ENDANGERED SPECIES

The proposed project is within the range of the Federally endangered Indiana bat (Myotis sodalis) and the candidate eastern massasauga rattlesnake (Sistrurus catenatus catenatus). However, there is no habitat for either species within the project impact area, so we agree that the proposed project is not likely to adversely affect these endangered and candidate species.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change, please recoordinate with our office as soon as possible. If you have any questions, please contact Elizabeth McCloskey at (219) 983-9753 or elizabeth_mccloskey@fws.gov.

Sincerely yours,

Elizabeth S. McCloskey
for Scott E. Pruitt
Supervisor

cc: Christie Stanifer, Indiana, Division of Water, Indianapolis, IN
Federal Highway Administration, Indianapolis, IN



REPLY TO
ATTENTION OF:

DEPARTMENT OF THE ARMY
DETROIT DISTRICT, CORPS OF ENGINEERS
REGULATORY OFFICE
SOUTH BEND FIELD OFFICE
2422 VIRIDIAN DRIVE SUITE # 101
SOUTH BEND, INDIANA 46628

May 11, 2009

Engineering & Technical Services
Regulatory Office
File No. LRE-2009-00206-102

Ms. Hayley M. Steele
American Structurepoint, INC.
7260 Shadeland Station
Indianapolis, IN 46256

Dear Ms. Steele:

This is in response to your March 31, 2009 letter concerning the State Boulevard Reconstruction located in Fort Wayne, Allen County, Indiana. In all waters of the United States, including adjacent wetlands, any discharge of dredged and/or fill material must be authorized by the Department of the Army. The authority of the Corps of Engineers to regulate the discharge of dredged and/or fill material is contained in Section 404 of the Clean Water Act and regulations promulgated pursuant to that Act. Please be advised that filling and grading work, mechanized landclearing, the sidecasting of excavated material, and some forms of piling installation constitute or otherwise involve discharges of dredged and/or fill material under the Corps' regulatory authority.

Based upon a review of the applicable National Wetland Inventory (NWI) maps and the information provided, it would appear that the property may contain wetlands and/or other waters that are within the jurisdiction of the Corps. Development of this property, therefore, will likely require a Federal Permit prior to the initiation of any work.

We recommend that you contact a private consulting firm to identify and accurately map any wetlands on the property via the Federal Wetlands Delineation Manual. **Please Note:** For any wetlands delineations conducted in areas defined by the Natural Resource Conservation Service (NRCS) as being in the Midwest Region (MLRA Region M), the Midwest Regional Wetland Delineation Supplement must be used. My representative can be made available to meet on-site with your consultant to discuss Corps wetland criteria, answer any questions and field check the wetland delineation. For your convenience, please find enclosed the necessary permit application and informational brochures. Plan view and cross-sectional view drawings in 8 1/2" by 11" format, should accompany the application package. Drawings and the application should include a description of all quantities, dimensions, nature of material placement and soil movement within wetlands. Upon completion, please have your consultant forward the completed wetland inventory and permit application to the attention of Aaron Damrill.

Any maps produced by the consultant or from other sources, including the NWI maps, are general guides only. The Corps will determine jurisdiction on a case-by-case basis as each project is reviewed. NWI maps can be obtained from the US Fish and Wildlife Service, 718 North Walnut Street, Bloomington, Indiana, 47401 (812) 334-4264.

This delineation/determination has been conducted to identify the limits of the Corps Clean Water Act jurisdiction for the particular site identified in this request. This delineation/determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985, as amended. If you or your tenant are United States Department of Agriculture (USDA) program participants or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resources Conservation Service prior to starting work in the site in question.

Thank you for contacting the Corps prior to the initiation of any work. Should you have any questions, please contact me at the above address or telephone 574-232-1952 or E-mail at aaron.w.damrill@usace.army.mil. Please refer to File No. LRE-2009-00206-102 in all future communications with this office.

We are interested in your thoughts and opinions concerning your experience with the Detroit District, Corps of Engineers Regulatory Program. If you are interested in letting us know how we are doing, you can complete an electronic Customer Service Survey from our web site at: <http://per2.nwp.usace.army.mil/survey.html>. Alternatively, you may contact us and request a paper copy of the survey that you may complete and return to us by mail or fax. Thank you for taking the time to complete the survey, we appreciate your feedback.

Sincerely,



Ryan Cassidy
Project Manager
Regulatory Office, South Bend Field Office

Enclosures

Copy Furnished

IDEM - Office of Water, w/encl.
IDNR - Division of Water, w/encl.
Hayley M. Steele, w/encl✓

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Water

Early Coordination/Environmental Assessment

DNR #: ER-13872 **Request Received:** March 18, 2009

Requestor: American Structurepoint, Inc
Hayley M. Steele
7260 Shadeland Station
Indianapolis, IN 46256-3957

Project: State Boulevard reconstruction from Spy Run to Cass Street, Ft. Wayne; Des #0400587, Project #IN20071404

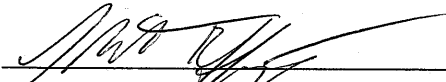
County/Site info: Allen

Regulatory Assessment: The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969. This proposal will require the formal approval for construction in a floodway under the Flood Control Act, IC 14-28-1. Please submit a copy of this letter with the permit application.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Fish, wildlife, and botanical resource losses as a result of this project can be minimized through implementation of the following measures.
Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
Do not cut any trees suitable for Indiana bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark) from April 1 through September 30.
Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please do not hesitate to contact the above staff member at (317) 232-4160 or 1-877-928-3755 (toll free) if we can be of further assistance.


J. Matthew Buffington
Environmental Supervisor
Division of Fish and Wildlife

Date: November 18, 2009



Indiana Department of Environmental Management

We make Indiana a cleaner, healthier place to live.

Mitchell E. Daniels, Jr.
Governor

100 North Senate Avenue
Indianapolis, Indiana 46206

Thomas W. Easterly
Commissioner

(317) 232-8603
800) 451-6027
www.IN.gov/idem

City of Fort Wayne
Shan Gunawardena
Citizens Square, 200 East Berry Street
Suite 210
Fort Wayne, IN 46802

American Structurepoint
Briana Hope
7260 Shadeland Station
Indianapolis, IN 4646256

Wednesday, April 24, 2013

Dear Grant Administrator or Other Finance Approval Authority:

RE: The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle Map, in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes, while correcting the sub-standard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center, 2-way, left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

The Indiana Department of Environmental Management (IDEM) is aware that many local government or not-for-profit entities are seeking grant monies, a bond issuance, or another public funding mechanism to cover some portion of the cost of a public works, infrastructure, or community development project. IDEM also is aware that in order to be eligible for such funding assistance, applicants are required to first evaluate the potential impacts that their particular project may have on the environment. In order to assist applicants seeking such financial assistance and to ensure that such projects do not have an adverse impact on the environment, IDEM has prepared the following list of environmental issues that each applicant must consider in order to minimize environmental impacts in compliance with all relevant state laws.

IDEM recommends that each applicant consider the following issues when moving forward with their project. IDEM also requests that, in addition to submitting the information requested above, each applicant also sign the attached certification, attesting to the fact that they have read the letter in its entirety, agree to abide by the recommendations of the letter, and to apply for any permits required from IDEM for the completion of their project.

IDEM recommends that any person(s) intending to complete a public works, infrastructure, or community development project using any public funding consider each of the following applicable recommendations and requirements:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE [Permits and Public Notices](http://www.lrl.usace.army.mil/orf/default.asp) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm>. IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality. To learn more about the water quality certification program, visit: <http://www.in.gov/idem/4384.htm>.
3. If the USACE determines that a wetland or other body of water is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A state isolated wetland permit from IDEM's Office of Water Quality is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the Office of Water Quality at 317-233-8488.
4. If your project will impact more than 0.5 acres of wetland, stream relocation, or other large-scale alterations to bodies of water such as the creation of a dam or a water diversion, you should seek additional input from the Office of Water Quality, Wetlands staff at 317-233-8488.
5. Work within the one-hundred year floodway of a given body of water is regulated by the Department of Natural Resources, Division of Water. Contact this agency at 317-232-4160 for further information.
6. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.
7. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - o <http://www.in.gov/idem/4902.htm>

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreg>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF], pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm>.

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

8. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317-232-4080) for additional project input.
9. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
10. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
11. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project (see page 1) should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed under specific conditions (<http://www.in.gov/idem/4148.htm>). You also can seek an open burning variance from IDEM.

IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on-site. You must register with IDEM if more than 2,000 pounds is to be composted; contact 317-232-0066. The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) on-site, although burying large quantities of such material can lead to subsidence problems.

2. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

If construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for three to five years, precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for three to five years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at 317-233-7272.

3. The U.S. EPA and the U.S. Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. For a county-by-county map of predicted radon levels in Indiana, visit <http://www.in.gov/idem/4267.htm>.

The U.S. EPA further recommends that all homes and apartments (within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L or higher, then U.S. EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L or higher, then U.S. EPA recommends the installation of radon-reduction measures. For a list of qualified radon testers and radon mitigation (or reduction) specialists, visit http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf. Also, it is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure, visit <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>, <http://www.in.gov/idem/4145.htm>, or <http://www.epa.gov/radon/index.html>.

4. With respect to asbestos removal, all facilities slated for renovation or demolition (except residential buildings that have four or fewer units) must have an asbestos management plan. For more information, visit <http://www.in.gov/idem/4145.htm>.

fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

In all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at www.in.gov/icpr/webfile/formsdiv/44593.pdf.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. Billings will occur on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm>.

5. With respect to lead-based paint removal, IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal, visit <http://www.in.gov/idem/permits/guide/waste/leadabatement.html>.
6. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months of April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
7. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (www.ai.org/legislative/iac/t03260/a00020.pdf). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
8. For more information on air permits, visit <http://www.in.gov/idem/4223.htm>, or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or oamprod at idem.in.gov.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm>.
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If Polychlorinated Biphenyls (PCBs) are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes. (Asbestos removal is addressed above, under Air Quality.)
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317-308-3039 (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should the applicant need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that they notify all adjoining property owners and/or occupants within ten days of your submittal of each permit application. Applicants seeking multiple permits, may still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Please note that this letter does not constitute a permit, license, endorsement, or any other form of approval on the part of either the Indiana Department of Environmental Management or any other Indiana state agency.

Should you have any questions relating to the content or recommendations of this letter, or if you have additional questions about whether a more complete environmental review of your project should be conducted, please feel free to contact Brad Baughn at (317) 234-3386, BBaughn@idem.in.gov.

Sincerely,



Thomas W. Easterly
Commissioner

Signature(s) of the Applicant

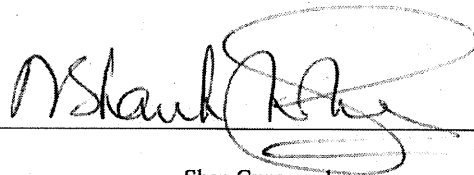
I acknowledge that I am seeking grant monies, a bond issuance, or other public funding mechanism to cover some portion of the cost of the public works, infrastructure, or community development project as described herein, which I am working (possibly with others) to complete.

Project Description

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle Map, in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes, while correcting the sub-standard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center, 2-way, left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environmental Management that appears directly above. In addition, I understand that in order to complete the project in which I am interested, with a minimum impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

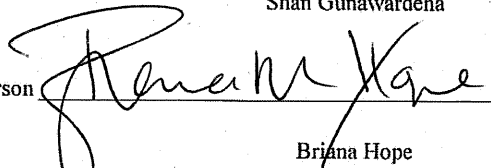
Dated Signature of the Public Owner
Contact/Responsible Elected Official



04/24/13

Shan Gunawardena

Dated Signature of the Project
Planner/Consultant Contact Person



04/24/2013

Briana Hope

From: [McCloskey, Elizabeth](#)
To: [Marlatt, Amy](#)
Cc: [Hope, Briana](#)
Subject: Re: Des. 0400587 State Boulevard Re-Coordination
Date: Wednesday, March 19, 2014 10:04:01 AM

Dear Ms. Marlatt,

Since the time of our initial review of this proposed project in 2009, the list of endangered species in Allen County, Indiana has been revised. Allen County is now within the range of the Federally endangered Indiana bat (*Myotis sodalis*) and rayed bean mussel (*Villosa fabalis*), the proposed endangered northern long-eared bat (*Myotis septentrionalis*), and the candidate eastern massasauga rattlesnake (*Sistrurus catenatus*). However, there is no habitat for any of these species within the proposed project area, so we agree that the proposed project is not likely to adversely affect these endangered, proposed endangered, and candidate species.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation.

Thank you for contacting us again about this proposed project.

Elizabeth McCloskey

U.S. Fish and Wildlife Service

Northern Indiana Suboffice

Ecological Services

P.O. Box 2616

Chesterton, Indiana 46304

elizabeth_mccloskey@fws.gov

On Wed, Mar 19, 2014 at 8:13 AM, Marlatt, Amy <Amarlatt@structurepoint.com> wrote:

| Elizabeth,

Early coordination was initiated for the State Boulevard Reconstruction project (Des. 0400587) in 2009. Since then the northern long-eared bat has been proposed as endangered, so would be reinitiating consultation as required per the USFWS early coordination response letter. I've attached the early coordination letter and the USFWS response letter from 2009. Please let me know if you need additional information. Thank you for your time!

Amy Marlatt

Staff Scientist, Environmental Science Group

7260 Shadeland Station, Indianapolis, IN 46256

T 317.547.5580 E amarlatt@structurepoint.com

F 317.543.0270 W www.structurepoint.com



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<http://www.emaildisclaimers.com/>

**United States Department of the Interior
National Park Service
Land & Water Conservation Fund**

Detailed Listing of Grants Grouped by County

Today's Date: 3/18/2013

INDIANA - 18

Page: 2

Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
ALLEN								
30 - XXX	A	FRANKE PARK	FORT WAYNE PARK BOARD	\$3,750.00	C	12/30/1967	7/31/1969	4
32 - XXX	A	KREAGER PARK	FORT WAYNE PARK BOARD	\$54,110.00	C	12/30/1967	6/24/1969	4
67 - XXX	A	FOX ISLAND NATURAL PARK	ALLEN COUNTY PARK BOARD	\$97,213.65	C	5/14/1970	12/31/1972	4
97 - XXX	D	JURY PARK DEVELOPMENT	NEW HAVEN-ADAMS TWP. PARK BOARD	\$24,640.91	C	8/30/1971	6/30/1974	4
105 - XXX	A	FRANKE PARK-AFRICAN VELDT	FORT WAYNE PARK BOARD	\$49,297.50	C	2/15/1972	12/31/1974	4
153 - XXX	D	MOSER PARK LIGHTING PROJECT	NEW HAVEN-ADAMS TWP. PARK BOARD	\$11,535.12	C	5/24/1973	12/31/1975	4
188 - XXX	A	LAND ACQ. FOR FRANKE PARK	FORT WAYNE PARK BOARD	\$13,150.00	C	2/4/1975	12/31/1977	4
201 - XXX	D	FOSTER PARK LIGHTED TENNIS COURTS	FORT WAYNE PARK BOARD	\$39,603.98	C	3/3/1975	12/31/1977	4
315 - XXX	A	D/FOX ISLAND PARK ACQ.	ALLEN COUNTY PARK BOARD	\$62,500.00	C	5/1/1978	6/30/1980	4
369 - A	C	D/FOX ISLAND PARK - PHASE III	ALLEN COUNTY PARK BOARD	\$137,184.93	C	2/26/1980	12/31/1984	4
369 - K	R	MOSER PARK POND	NEW HAVEN-ADAMS TWP. PARK BOARD	\$12,500.00	C	2/26/1980	12/31/1984	4
369 - N	A	FRANKE PARK - FOX ACQUISITION	FORT WAYNE PARK BOARD	\$40,000.00	C	2/26/1980	12/31/1984	4
371 - XXX	C	JEHL PARK	FORT WAYNE PARK BOARD	\$40,074.50	C	1/9/1980	12/31/1984	4
392 - XXX	D	HAVENHURST PARK DEVELOPMENTS	NEW HAVEN-ADAMS TWP. PARK BOARD	\$50,000.00	C	2/9/1981	12/31/1985	4
396 - XXX	D	SHERMAN ST. RIVERGREENWAY	FORT WAYNE PARK BOARD	\$280,000.00	C	7/27/1981	12/31/1986	4
408 - XXX	D	ALLEN COUNTY ROADSIDE PARKS	ALLEN COUNTY PARK BOARD	\$5,782.14	C	9/23/1983	6/30/1988	4
419 - XXX	D	FT. WAYNE RIVERGREENWAY-PHASE II	FORT WAYNE PARK BOARD	\$75,000.00	C	3/20/1984	6/30/1989	4
465 - XXX	D	ST. MARY'S RIVERGREENWAY	FORT WAYNE PARK BOARD	\$48,877.00	C	6/27/1988	12/31/1992	4
469 - XXX	D	ST. MARY'S RIVERGREENWAY-PHASE II	FORT WAYNE PARK BOARD	\$100,000.00	C	7/18/1989	6/30/1994	4
500 - XXX	C	GRABILL COMMUNITY PARK EXPANSION	GRABILL PARK BOARD	\$34,200.00	C	5/20/1994	6/30/1999	4
526 - XXX	C	BUCKNER FARM PARK	FORT WAYNE PARK BOARD	\$178,300.00	C	4/1/2002	12/31/2006	3

**United States Department of the Interior
National Park Service
Land & Water Conservation Fund**

Detailed Listing of Grants Grouped by County

Today's Date: 3/18/2013

Page: 3

INDIANA - 18

Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
ALLEN								
527 - XXX	D	METEA PARK NATURE CENTER	ALLEN COUNTY PARK BOARD	\$200,000.00	C	4/4/2002	12/31/2006	99
570 - XXX	D	KREAGER PARK BOUNDLESS PLAYGROUND	FORT WAYNE PARK BOARD	\$200,000.00	C	5/5/2010	12/31/2014	3
577 - XXX	C	RIVERSIDE GARDEN PARK	LEO-CEDARVILLE PARK BOARD	\$199,550.00	A	4/18/2012	12/31/2016	3
ALLEN County Total:				\$1,957,269.73		County Count:	24	
BARTHOLOMEW								
269 - XXX	D	CLIFTY PARK DEV	COLUMBUS PARK BOARD	\$88,376.89	C	2/4/1977	12/31/1980	2
398 - XXX	C	D/HARRISON RIDGE PARK	COLUMBUS PARK BOARD	\$87,490.47	C	2/13/1981	12/31/1985	2
399 - XXX	C	D/ANDERSON FALLS NATURE PRESERVE	BARTHOLOMEW COUNTY PARK BOARD	\$55,000.00	C	2/17/1981	12/31/1985	2
412 - XXX	D	HARRISON RIDGE PARK - PHASE II	COLUMBUS PARK BOARD	\$9,174.47	C	6/21/1983	9/15/1984	2
518 - XXX	C	D/MCCULLOUGHES RUN PARK	COLUMBUS PARK BOARD	\$143,166.85	C	9/6/2000	12/31/2006	9
BARTHOLOMEW County Total:				\$383,208.68		County Count:	5	
BENTON								
27 - XXX	D	FOWLER COMMUNITY SWIMMING POOL	FOWLER PARK BOARD	\$15,879.30	C	12/28/1967	9/1/1969	3
66 - XXX	D	FOWLER PARK	VIGO COUNTY PARK BOARD	\$7,950.74	C	3/13/1970	9/1/1971	5
535 - XXX	D	FOWLER POOL AND PARK RENOVATIONS	FOWLER PARK BOARD	\$117,970.00	C	3/19/2003	12/31/2008	5
569 - XXX	R	FOWLER PARK POOL REPLACEMENT	FOWLER PARK BOARD	\$133,737.09	C	3/30/2009	12/31/2013	1
BENTON County Total:				\$275,537.13		County Count:	4	
BLACKFORD								
347 - XXX	C	D/MONTPELIER COMMUNITY PARK	MONTPELIER PARK BOARD	\$55,186.00	C	2/23/1979	6/30/1984	5
BLACKFORD County Total:				\$55,186.00		County Count:	5	

Appendix B
Page 28 of 28

Appendix C: Section 106 of the NHPA

- Section 106 Documentation and Findings – February 27, 2013
- SHPO Concurrence – April 1, 2013
- Draft Memorandum of Agreement
- Public Notice and Proof of Publication – State Boulevard Bridge over Spy Run

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECTS FINDING
STATE BOULEVARD RECONSTRUCTION
FROM SPY RUN TO CASS STREET
FORT WAYNE, ALLEN COUNTY, INDIANA
DES. NO. 0400587
FEDERAL PROJECT NUMBER: IN20071404**

AREA OF POTENTIAL EFFECTS

(Pursuant to 36 CFR Section 800.4(a)(1))

The Area of Potential Effects (APE) is centered on State Boulevard in Fort Wayne, Wayne Township, Allen County, Indiana. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway.

The archaeological APE is defined as the project footprint.

ELIGIBILITY DETERMINATIONS

(Pursuant to 36 CFR 800.4(c)(2))

Two historic properties are listed in the National Register of Historic Places (NR): Fort Wayne Park and Boulevard System Historic District and Brookview-Irvington Park Historic District. One historic property has previously been determined eligible for the NR: Bridge over Spy Run Creek.

Fort Wayne Park and Boulevard System Historic District (NR, 2010). The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. It encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler. The district includes nearly 2,000 acres of parks, boulevards, and sites. Eight resources (seven of which are contributing) identified as part of the Fort Wayne Park and Boulevard System Historic District are located within the APE for this project. The FWPB is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909 to 1955.

Brookview-Irvington Park Historic District (NR, 2011). The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906 to 1965.

Bridge over Spy Run (NBI No. 0200273). The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927. The Bridge over Spy Run is also identified as a Contributing resource in the Fort Wayne Park and Boulevard System Historic District and the Brookview-Irvington Park Historic District.

EFFECT FINDING

Fort Wayne Park and Boulevard System Historic District (NR, 2010)—Adverse Effect
Brookview-Irvington Park Historic District (NR, 2011)—Adverse Effect
Bridge over Spy Run (NBI No. 0200273)—Adverse Effect

Federal Highway Administration (FHWA) has determined a finding of Adverse Effect is appropriate for this undertaking.

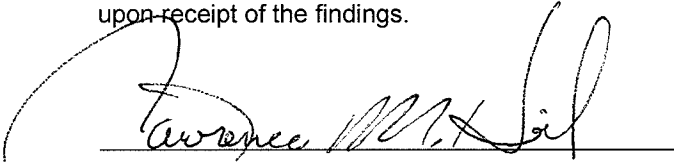
SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Fort Wayne Park and Boulevard System Historic District – This undertaking will convert property from the Fort Wayne Park and Boulevard System Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Fort Wayne Park and Boulevard System Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Brookview-Irvington Park Historic District – This undertaking will convert property from the Brookview-Irvington Park Historic District, a Section 4(f) historic property, to a transportation use; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Brookview-Irvington Park Historic District. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Bridge over Spy Run (NBI No. 0200273) – This resource is used for transportation purposes. This undertaking will have an "Adverse Effect" on the Bridge over Spy Run, a Section 4(f) property; the FHWA has determined the appropriate Section 106 finding is "Adverse Effect"; and therefore a Section 4(f) evaluation must be completed for the Bridge over Spy Run. FHWA respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "Adverse Effect."

Consulting parties will be provided a copy of the findings and determinations of FHWA, in accordance with FHWA's Section 106 procedures. Comments will be accepted for thirty (30) days upon receipt of the findings.

A handwritten signature in black ink, appearing to read "Lawrence M. Hil", is written over a horizontal line. To the left of the signature, there is a large, loopy handwritten mark that partially encircles the signature.

for Karen Bobo
Acting Division Administrator
Federal Highway Administration

2-27-2013

Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR SECTION 800.6(a)(3)
STATE BOULEVARD RECONSTRUCTION
FROM SPY RUN TO CASS STREET
FORT WAYNE, ALLEN COUNTY, INDIANA
DES. NO.: 0400587
FEDERAL PROJECT NUMBER: IN20071404**

1. DESCRIPTION OF THE UNDERTAKING

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The Preferred Alternative for this project is Alternative 3A, Access Alternate 2. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four ten-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an eight-foot wide raised median and a two-way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment will be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four eleven-foot travel lanes, two in each direction, separated by a twelve-foot two-way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway. Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. The project also includes a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive, which will connect the Pufferbelly Trail.

36 CFR § 800.16(d) defines the Area of Potential Effects (APE) as the “geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of potential effects is influenced by the scale and nature of an undertaking and may be different for different kinds of effects caused by the undertaking.”

The APE is centered on State Boulevard. From the alley west of Cass Street to the abandoned New York Central Railroad, the APE will extend 250 feet from the centerline of the existing roadway. It encompasses the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway. (See Appendix B, Maps and Site Plans for a map of the APE.) The archaeological APE is defined as the project footprint.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

Pursuant to 36 CFR Section 800.4(b), project consultants conducted efforts to identify historic properties—including consultation—as part of this Section 106 undertaking.

The Westerly Group, Inc. (WGI) initiated efforts to identify historic properties. According to WGI, historians investigated the National Register of Historic Places (NR), Indiana Register of Historic Sites and Structures (SR), *Indiana Historic Bridge Inventory*, *Fort Wayne Interim Report*, Sanborn Fire Insurance maps, as well as other primary and secondary sources. Online resources were also accessed to complete the research. Research included a review of the property listing: “The Civilizing of a Midwestern City: The Park and Boulevard System of Fort Wayne, Indiana--A Plan for the Ideal Development of Transportation, Parks and Residential Subdivision,” the Historic Property Report (HPR) for the US 27 Southbound Bridge Replacement Over Spy Run Creek (Ross Nelson), and a preliminary NR nomination for Wildwood Park Historic District.¹

On March 18, 2009, archaeologists for Archaeological Consultants of Ossian conducted a field reconnaissance for a project area of approximately 6.43 acres. The reconnaissance included shovel testing, pedestrian walkover, and auger testing.

On March 23, 2009, American Structurepoint, Inc. (Structurepoint) sent a Section 106 Early Coordination Letter, describing the proposed project and inviting the following parties to join Section 106 consultation: Indiana State Historic Preservation Officer (SHPO); Indiana Department of Transportation (INDOT); INDOT—Fort Wayne District, City of Fort Wayne Engineer; Historic Landmarks Foundation of Indiana (now Indiana Landmarks); Allen County Historian; Allen County—Fort Wayne Historical Society; ARCH, Inc.; Fort Wayne Historic Preservation Review Board; and John Shoaff, Fort Wayne city council member. The Allen County historian declined to participate. The City of Fort Wayne, ARCH, Inc., Fort Wayne Historic Preservation Commission, and Indiana Landmarks Northern Regional Office, accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

Archaeological Consultants of Ossian completed an Archeological Field Reconnaissance Report on April 2, 2009. In the report, archaeologists stated, “It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places.” The archaeologist recommended no further work and project clearance. (See Appendix E: Report Summaries.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Both organizations accepted the invitation to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

In a letter dated April 23, 2009, SHPO concurred with ARCH, Inc.’s request that Friends of the Parks and Brookview Neighborhood Association be invited to join consultation. Also, due to potential eligibility of the Bridge over Spy Run Creek, SHPO requested that bridge historian Dr. James Cooper and Historic Spans Taskforce representative Paul Brandenburg be invited to join consultation. Historic Spans Taskforce accepted the invitation to join consultation. The SHPO also requested that the APE be “carefully delineate[d]” to take into account impacts. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On July 2, 2009, Structurepoint transmitted the Archaeological Field Reconnaissance report to the SHPO. (See Appendix F: Correspondence and Meeting Minutes.)

On July 9, 2009, the Irvington Park Neighborhood Association joined consultation. (See Appendix B: Consulting Parties.)

¹ These are the sources that the Westerly Group lists in the Historic Property Report (2009), page 8.

On November 9, 2009, Structurepoint sent consulting parties and SHPO a copy of the HPR for review and comment. The HPR recommended the Brookview-Irvington Park National Register Historic District, Bridge over Spy Run (NBI No. 0200273), and the House at 315 East State Boulevard eligible for listing in the NR. The HPR also recommended the portion of State Boulevard within the Brookview-Irvington Park district as individually NR eligible and contributing to the district. (See Appendix E: Report Summaries and Appendix F: Correspondence and Meeting Minutes.)

On December 1, 2009, Structurepoint sent an invitation to consulting parties to attend a meeting on December 15, 2009, to discuss the identification of historic properties. On December 8, 2009, Structurepoint provided a revised agenda. On December 15, 2009, a consulting party meeting was held at the City-County Building in Fort Wayne to discuss the identification and evaluation of historic resources per the HPR and future steps in the Section 106 process. Consulting parties requested that the APE be enlarged but offered no additional information regarding the identification and evaluation of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On July 6, 2011, Structurepoint sent an email to consulting parties conveying digital copies of its letters to SHPO. Structurepoint stated, "Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15 day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters." The letter also stated, "if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a 'cc' during this Section 106 process." (See Appendix F: Correspondence and Meeting Minutes.)

On July 13, 2011, an agency meeting was held with FHWA, INDOT, and SHPO to discuss SHPO's comments on the recent Purpose and Need submission; how to address the comments and concerns of consulting parties; Section 4(f); and ACHP involvement. (See Appendix F: Correspondence and Meeting Minutes.)

On August 15, 2011, Structurepoint sent a letter by post to consulting parties informing them of a consulting party meeting scheduled for September 1, 2011. Enclosures included a meeting agenda, agency coordination meeting minutes, a copy of a letter to SHPO, a copy of a letter to FHWA, Historic Bridge Programmatic Agreement Alternatives Analysis, Individual Section 4(f) Alternatives Analysis, Corridor Alternatives Map, and Consulting Party Questions/Comments and Responses. Structurepoint conveyed the same data electronically in an email dated August 16, 2011. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated August 29, 2011, FHWA wrote to the Advisory Council on Historic Preservation (ACHP) asking if they would be involved in consultation for this project. According to FHWA's letter, "FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A - Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), " *... adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*" and (c)(3) for " *... cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council's involvement could help resolve ...* " (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 1, 2011, at Citizens Square in the City of Fort Wayne to discuss project updates; purpose and need update; consulting party comments and responses document; alternatives review; and future steps. Following the meeting, Structurepoint agreed to prepare a three-lane design alternative for review. It was also decided that a another

consulting party meeting would take place after the issuance of the 800.11 documentation and the finding of “Adverse Effect” and at that time the group would discuss mitigation and forming an advisory committee to consult on mitigation measures. (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, an agency meeting with FHWA, INDOT, and Structurepoint was held to follow-up on the consulting party meeting. (See Appendix F: Correspondence and Meeting Minutes.)

On September 29, 2011, Structurepoint transmitted the following material to consulting parties: meeting minutes from the September 1, 2011 consulting party meeting; agency coordination meeting summary from September 2, 2011; revised individual Section 4(f) analysis including an additional alternative; traffic data from NIRCC; and ACHP correspondence. (See Appendix F: Correspondence and Meeting Minutes.)

In February 2012, Structurepoint contracted with Weintraut & Associates, Inc. (W&A) to update the HPR through an Additional Information Report (AI). The purpose of the AI Report was to supplement the HPR following the inclusion of two new NR-listed resources within the APE.

On March 6, 2012, historians from W&A walked and drove the APE, viewed all the resources within the APE, and photographed and recorded survey notes about resources greater than fifty years of age considered or rated Contributing or higher. Historians also field verified the APE at that time.

Historians for W&A published an AI report in April 2012. The report identified two districts that were listed in the NR after publication of the HPR: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). Portions of both districts are within the APE. The Bridge over Spy Run (NBI No. 0200273) was previously determined eligible for listing in the NR. Historians expressed the opinion that proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts and recommended a finding of “Historic Properties Affected—Adverse Effect” for the project. (See Appendix E: Report Summaries.)

On May 22, 2012, Structurepoint conveyed the AI Report to consulting parties, superseding their letter of May 17, 2012. (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an agency meeting was held with FHWA, INDOT, and SHPO to discuss the State Boulevard Project and the eligibility recommendations and findings.

On July 3, 2012, Archaeological Consultants of Ossian conducted an additional records check at the DHPA and on July 5 and 6, 2012 conducted a field reconnaissance for additional project areas. An Archaeological Short Report, completed July 11, 2012, concluded the Phase Ia reconnaissance located no archaeological resources and recommended project clearance. (See Appendix E: Report Summaries.)

On July 16, 2012, Structurepoint conveyed an archaeological short report for the additional area required for the State Boulevard Improvements project. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2012, Structurepoint sent a letter to consulting parties conveying FHWA's Findings and Determinations for the project. Structurepoint invited consulting parties to comment on the letter and to attend a consulting party meeting on September 19, 2012, to discuss the resolution of adverse effects. No comments were received regarding the identification of historic properties. (See Appendix F: Correspondence and Meeting Minutes.)

On September 18, 2012, Structurepoint sent an email and attachment to consulting parties regarding project impacts to houses at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard—within the Brookview-Irvington Historic District. The letter stated: “It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions.” (See Appendix F: Correspondence and Meeting Minutes.)

A consulting party meeting was held September 19, 2012, to discuss the resolution and mitigation of adverse effects. (See Appendix F: Correspondence and Meeting Minutes.)

No further efforts to identify historic properties, including consultation, took place as part of this Section 106 undertaking.

Timeline of Formal Consultation

(See Appendix F: Correspondence and Meeting Minutes for referenced correspondence.)

March 23, 2009:	Structurepoint initiates consultation by sending a Section 106 Early Coordination Letter
April 23, 2009:	SHPO comments on the Early Coordination Letter
July 2, 2009:	Structurepoint sends archaeological report to SHPO
November 9, 2009:	Structurepoint transmits a copy of the HPR to SHPO and consulting parties
December 1, 2009:	Structurepoint sends invitation to a consulting party meeting
December 14, 2009:	SHPO comments on the HPR and Archaeology Report
December 15, 2009:	Consulting party meeting held in Fort Wayne
December 28, 2009:	Structurepoint sends minutes from consulting party meeting
January 27, 2010:	SHPO comments on minutes of December 15, 2009, consulting party meeting and asks questions regarding Purpose and Need
February 4, 2010:	Structurepoint sends a CD to SHPO containing the City of Fort Wayne 2005 Flood Control Study, traffic data, and revised meeting minutes
March 10, 2010:	SHPO comments on revised minutes from the consulting party meeting and the other informational items sent on February 4, 2010
May 19, 2011:	Structurepoint responds to questions raised in SHPO correspondence comments of January 27, 2010 and March 10, 2010
June 17, 2011:	Structurepoint sends documents missing from May 19, 2011 transmittal and extends comment period for another fifteen days
July 5, 2011:	SHPO responds to Structurepoint’s letters of January 27, 2010 and March 10, 2010

July 6, 2011:	Structurepoint emails consulting parties digital copies of letters to SHPO
July 13, 2011:	Agency meeting with FHWA, INDOT, and SHPO discussing SHPO's comments on recent Purpose and Need submission; how to address consulting parties comments; Section 4(f); and Inviting ACHP involvement
August 15, 2011:	Structurepoint sends an invitation to consulting party meeting scheduled for September 1, 2011
August 15, 2011:	Structurepoint sends a letter to FHWA requesting the agency invite the ACHP to participate
August 16, 2011:	Structurepoint conveys August 15, 2011, letter to FHWA electronically to consulting parties
August 29, 2011:	FHWA asks the ACHP to participate in consultation
September 1, 2011:	Consulting party meeting held in Fort Wayne
September 2, 2011:	Agency meeting with FHWA, INDOT, and Structurepoint to follow-up on the consulting party meeting held the previous day
September 22, 2011:	ACHP requests additional information regarding the project in order to determine if its participation is warranted
September 29, 2011:	Structurepoint transmits meeting minutes and additional information that consulting parties had requested to consulting parties
November 7, 2011:	SHPO responds to the Structurepoint's letters of August 15, 2011, and September 29, 2011
May 22, 2012:	Structurepoint sends a letter conveying the AI Report to consulting parties and replacing a letter sent May 17, 2012
June 20, 2012:	Agency meeting discusses the State Boulevard Project and the eligibility recommendations and findings
June 22, 2012:	SHPO responds to AI Report
July 2, 2012:	Structurepoint sends SHPO a letter answering questions raised in its letter dated November 7, 2011
July 11, 2012:	Archaeological Consultants of Ossian prepares an Archaeological Short Report for additional areas of the project. In the report, archaeologists conclude the Phase Ia reconnaissance located no archaeologists and recommended project clearance
July 16, 2012:	Structurepoint sends an archaeological short report for the additional area required for the State Boulevard Improvements project to SHPO
July 31, 2012:	ACHP declines to participate in consultation

August 1, 2012:	SHPO responds to the June 20, 2012, agency meeting
August 13, 2012:	SHPO concurs with the archaeological short report
August 29, 2012:	Structurepoint sends letter conveying FHWA's Findings and Determination of Adverse Effect and the draft 800.11(e) documentation. The letter includes an invitation to a consulting party meeting
September 18, 2012:	Structurepoint transmits letter regarding project impacts to three parcels on East State Boulevard
September 19, 2012:	Consulting parties meeting
October 5, 2012:	INDOT informs consulting parties of they will have an opportunity to comment on Section 106 materials during the Environmental Assessment review period
November 15, 2012:	SHPO responds to draft MOA
December 18, 2012:	Agency meeting to discuss mitigation

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Three historic properties will be affected by the undertaking: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek.

Fort Wayne Park and Boulevard System Historic District (NR, 2010). The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of eleven parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and Bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NR in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized."

Brookview-Irvington Park Historic District (NR, 2011). The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street and Jacobs Avenue. The district contains a total of 424 Contributing resources including houses, garages, and the combined plats of the district, as well as the previously-determined eligible Bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965 and represents the construction dates of most buildings within the

historic district and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination.

Bridge over Spy Run (NBI No. 0200273). The Bridge over Spy Run (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NR per the *Indiana Statewide Historic Bridge Inventory* (2010). The Bridge over Spy Run is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

The proposed project will include the widening, realignment, and elevation of State Boulevard approximately 190 feet south and the construction of a new access road between the existing and proposed State Boulevard alignment (and the conversion of some existing intersections to cul-de-sacs). The project also proposes modified alignments at Westbrook Drive and Clinton Street. Additionally, the proposed project requires approximately fifteen residential relocations, and the removal of trees and older street amenities. Construction of concrete curb and gutter, raised median, left turn lanes, decorative lighting, modified traffic lights, landscaped utility strip, curb inlets, and storm sewers. A new bridge structure will replace the existing bridge over Spy Run Creek. The project also includes a prefabricated pedestrian trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive. Sidewalk ramps will be extended from the proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. Additionally, removal and relocation has been selected as the most prudent and feasible alternative for the Bridge over Spy Run. The elevation of State Boulevard and the removal of the properties that contribute to the district will result in the creation of new public spaces in a residential setting and the bifurcation of the Brookview-Irvington Historic District. These elements will result in an adverse effect on the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT--INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." Because construction of the Pufferbelly Trail is reasonably foreseeable and because environmental assessment of the Pufferbelly Trail undertaking is currently taking place, effects from that project have been included in this application of the criteria of adverse effects.

Fort Wayne Park and Boulevard System Historic District. The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Fort Wayne Park and Boulevard System Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause "[p]hysical destruction of or damage to all or part of the property," particularly through the realignment and elevation of State Boulevard. Further, land from identified Contributing features within the district, including the Bridge over Spy Run and "Sloping Hills and Natural Features" (SR-10), will be taken.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause "[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines."

Per 36 CFR 800.5(a)(2)(iii), the undertaking will cause “[r]emoval of the property from its historic location,” at the location of the State Boulevard realignment.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” The Contributing Bridge over Spy Run Creek will be replaced, a prefabricated trail bridge, retaining walls, and ramps (associated with the Pufferbelly Trail) will be constructed over State Boulevard at the abandoned New York Central railroad right-of-way, the Contributing State Boulevard will be realigned, widened, and elevated. The Contributing Eastbrook Drive will be converted to a cul-de-sac.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, widening and elevation of the Contributing State Boulevard, the construction of a cul-de-sac at the Contributing Eastbrook drive, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps and retaining walls (associated with the Pufferbelly Trail) over the Contributing State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the Contributing Bridge over Spy Run Creek.

Per 36 CFR 800.5(a)(2)(vi), the project will not result in “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), there will be no “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

Brookview-Irvington Park Historic District. The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the NR-listed Brookview-Irvington Park Historic District.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” The proposed undertaking will result in the relocation of Contributing residential resources, the removal of private space, and change the orientation of the Brookview neighborhood plat, a Contributing resource. The elevation of the re-aligned State Boulevard will result in the bifurcation of the district.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.”

Per 36 CFR 800.5(a)(2)(iii), the undertaking will result in the “[r]emoval of the property from its historic location.” State Boulevard will be widened, elevated, and realigned south from its historic location; residential relocations will take place within the Brookview-Irvington Park Historic District. The Bridge over Spy Run, a Contributing resource, will be removed from its historic location.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance” through the realignment of some roadways, conversion of some intersections to cul-de-sacs, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. The landscape of the area will be modified by the realigned and elevated State Boulevard and by the realignment of the original Brookview-Irvington Park plat, both of which

were identified as Contributing to the district. The removal of the Contributing homes will change the character of the plat since the open, public spaces will be much larger than they are presently.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The district’s setting will change with the realignment, elevation, and widening of State Boulevard, the realignment of Westbrook Drive and Clinton Street, the construction of cul-de-sacs at some locations, the removal of some streetscape elements and the introduction of other streetscape elements, construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way, replacement of the Bridge over Spy Run Creek, and the approximately fifteen residential relocations. All of these elements will change the character of the district along State Boulevard.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .”

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

Bridge over Spy Run (NBI No. 0200273). The criteria of adverse effect, as defined in 36 CFR 800.5(a)(1) and described in the examples in 36 CFR 800.5(a)(2), apply to the previously determined NR-eligible Bridge over Spy Run.

Per 36 CFR 800.5(a)(2)(i), the undertaking will cause “[p]hysical destruction of or damage to all or part of the property.” Bridge over Spy Run will either be removed from its present location or demolished as part of this undertaking. The removal or demolition will be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The pending removal or demolition of the bridge is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will cause the “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines.” Bridge over Spy Run will be removed or demolished as part of the undertaking, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iii), the property will be removed from its historic location either by demolition or removal, causing an adverse effect.

Per 36 CFR 800.5(a)(2)(iv), the undertaking will cause a “[c]hange of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance.” Bridge over Spy Run will either be removed from its present location or demolished. The removal or demolition is considered an adverse effect.

Per 36 CFR 800.5(a)(2)(v), the undertaking will cause an “[i]ntroduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” The bridge’s setting will change as a result of: the realignment and widening of State Boulevard, the construction of cul-de-sacs near the bridge, and the introduction of various streetscape elements, construction of a prefabricated trail bridge and ramps (associated with the Pufferbelly Trail) over State Boulevard at the abandoned New York Central railroad right-of-way, and replacement of the bridge over Spy Run Creek, and the approximately fifteen residential relocations.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not cause “[n]eglect of a property which causes its deterioration . . .” though the bridge will be removed and demolished.

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause “[t]ransfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.”

FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

Consultation has occurred regarding Purpose and Need, Alternatives, and Traffic Data in order to reduce impacts on historic properties. Consulting parties offered comments regarding the project purpose and need, selection of project alternatives, and traffic data during the Section 106 process for this undertaking and requested changes to the alternatives analysis as a result of that information. At the request of consulting parties, a third alternative—Alternative 3D—was considered. (Consulting party comments on purpose and need, alternatives selection, and traffic data—which will be included as part of the Environmental documentation—are included in Appendix F: Correspondence and Meeting Minutes.)

Consultants have met with consulting parties and with the Indiana SHPO to discuss options to avoid, minimize and mitigate adverse effects on December 15, 2009 (consulting parties and SHPO), July 13, 2011 (SHPO), September 1, 2011 (consulting parties and SHPO), September 2, 2011 (SHPO), June 20, 2012 (SHPO), September 19, 2012 (consulting parties and SHPO), and December 18, 2012 (SHPO). The Indiana SHPO agreed to enter into a Memorandum of Agreement (MOA) at a meeting held on June 20, 2012. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of the Bridge over Spy Run; Restore character of State Boulevard within the district; and Educational mitigation.

A consulting party meeting was held on September 19, 2012, to discuss stipulations for the MOA and to solicit other mitigation ideas. At that meeting, Structurepoint shared the following efforts to minimize impacts: 1. A three-lane road with center turn lane was discarded because it did not provide the required level of service. 2. Reduction of right-of-way impacts to allow three properties to remain in place. (After agency consultation, this was discarded.) 3. Maintaining existing curvature between Eastbrook and Terrace Road. 4. Maintaining existing curb lines of Eastbrook Drive where possible.

The following alternatives have been evaluated for the Bridge over Spy Run:

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structures Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge structure is five years from the date of the inspection of the report (2011). The existing bridge is currently below the flood elevation of the St. Mary’s River which causes the bridge to be overtopped with backwater from the St. Mary’s River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) “This flooding is caused primarily by backwater from the [St.] Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

According to the recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010. Six out of the seventeen years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed

due to the flooding events. Road closure due flooding events appears to be happening more consistently in recent years, restricting emergency traffic more often.

1. Rehabilitation for Continued Vehicular Use (two-way option)

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

2. Rehabilitation for Continued Vehicular Use (one-way option)

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven

feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

3. Bypass (Non-vehicular use)

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the *Fort Wayne Journal-Gazette* and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed seven feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Structurepoint has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546.

The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is five years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

4. Replacement

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

5. Relocate and Replacement

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately seven feet to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from two lanes to four lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new four-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure was marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge was listed as available for re-use in the *Fort Wayne Journal-Gazette* on February 2, 2010. The bridge was also listed as available on the INDOT Website and signs were posted on the bridge offering it for re-use. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use.

Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for

vehicular use. While this alternative does meet the project's purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

The following alternatives have been evaluated for the State Boulevard Reconstruction project:

Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) terminus of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the

corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately seventy-five residential relocations and fifteen commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility. This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

Access Alternate 1

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Access Alternate 2 (Preferred Access Alternative)

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State

Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.

Access Alternate 3

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to four-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately twenty residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety

concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to four-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately four to five-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project’s purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

Alternative 3D: Preferred Alignment with 3-Lane Typical Section

This alternative is similar to Alternative 3A (Preferred Alternative) but features a three-lane typical section rather than a four-lane typical section. This alternative involves widening the existing two-lane section of State Boulevard between Clinton Street and Cass Street to three-lanes while correcting the substandard horizontal curve.

By reducing the typical section from four-lanes (Alternative 3A/Preferred Alternative) to three-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four-lanes to three-lanes is only ten-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton

Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.

The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

In response to requests for additional review time for the 800.11(e) and findings and determinations distributed on August 29, 2012 INDOT wrote an email on October 5, 2012, to consulting parties regarding comments on the project. INDOT informed consulting parties that the 800.11 documentation would be updated and the finding, draft Memorandum of Agreement [MOA], draft 4(f) evaluation, “will be released with the Draft Environmental Assessment for an additional comment for both consulting parties and the public.” Therefore, the comment period for the 800.11 documentation, draft MOA, and draft 4(f) evaluation would not be extended. (See Appendix F: Correspondence.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet with it and its consultants to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain, landscape architect, made the presentation. Cain’s plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A: Plans.)

After discussion, the City and its consultants agreed that in addition to the mitigation landscape plan, they would look for other ways to mitigate the adverse effect, such as grants to rehabilitate the facades of existing houses (if practical and legally viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

During the course of consultation, the following organizations have responded affirmatively to the invitation to join consultation: City of Fort Wayne; Friends of the Parks of Allen County; Allen County Historian; Indiana Landmarks—Northern Regional Office; Fort Wayne Historic Preservation Commission; ARCH, Inc.; Brookview Neighborhood Association; Indiana Historic Spans Taskforce; Irvington Park Neighborhood Association. Additionally, the following individuals or organizations participated in or requested to join consultation: Charley Shirmeyer, Northside Galleries; Albert Cohan, Westbrook 5, LLC; Thomas Niezer, Barret & McNagny, LLP; Ronald Ross, Martin Riley Architects and Engineers; Dan Ernst, Earth Source, Inc.; Jan Dailey, State Boulevard Resident. (See Appendix B: Consulting Parties.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks of Allen County and Brookview Neighborhood Association be invited to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On April 23, 2009, SHPO wrote in response to the notification concerning the reconstruction of State Boulevard and requested a literature review, historic context, research methodology, property descriptions, and NR eligibility evaluations and recommendations to aid analysis of the project. SHPO recommended the Friends of the Parks and Boulevard Neighborhood Association, Indiana Historic Spans Task Force, and bridge historian Dr. James L. Cooper be invited to participate as consulting parties. (See Appendix F: Correspondence and Meeting Minutes.)

On December 7, 2009, Jan Dailey wrote in response to the HPR: “I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effects.” In regard to the project, she stated, “While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling

that residents of this neighborhood share.” She also requested that “a separate study be conducted in possible land use of the former Kroger Fuel Center.” (See Appendix F: Correspondence and Meeting Minutes.)

On December 8, 2009, Indiana Landmarks—Northern Regional Office wrote in response to the HPR. Landmarks agreed that Brookview-Irvington Park Historic District is eligible for the NR and suggested modifications to the HPR recommendations in light of NR nominations being composed by ARCH, Inc. Indiana Landmarks also requested more information on the proposed design in order to comment on a preliminary effect finding. Indiana Landmarks disagreed with the APE, asked some preliminary questions regarding the purpose and need in relation to historic properties, questioned the appropriateness of including a “trail bridge” in this Section 106 investigation, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, and expressed the need for a “broad range of alternatives” to be included as part of the project options, and expressed concerns about the impacts of a different project on this Section 106 undertaking. (See Appendix F: Correspondence and Meeting Minutes.)

On December 9, 2009, ARCH, Inc. wrote in response to the HPR. Arch, Inc. agreed with the recommendation of eligibility for the Brookview-Irvington Park Historic District, noting that an NR nomination was being prepared. ARCH, Inc. requested the inclusion of proposed design maps, requested more detailed data regarding the project purpose and need, questioned the inclusion of the “trail bridge” in this Section 106 study, expressed the opinion that the “substandard horizontal curve” was a “character defining” element of the Brookview-Irvington Park historic district, disagreed with the APE, stated the importance of consulting “early in the undertaking’s planning,” expressed concerns about the impacts of a different project on this Section 106 undertaking and specifically stated “we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate.” Finally, ARCH, Inc. agreed with statements regarding flooding in the area, but stated they “contend that this is an issue which is recent.”

In a letter dated December 10, 2009, Julie Donnell, president of the Friends of the Parks of Allen County, Inc. wrote in response to the meeting agenda and HPR. Donnell expressed concern over the project’s Section 106 process, including the concern “that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and that after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.” The letter also commented on the Brookview Neighborhood, concurring with other consulting party comments on the resource and posing questions regarding the project’s effects on the landscape, and expressed the integral importance of the landscape in the Brookview neighborhood’s integrity. The letter requested considering the inclusion of the Cultural Landscape Foundation in the Section 106 process. (See Appendix F: Correspondence and Meeting Minutes.)

On December 14, 2009, SHPO wrote in response to the Draft HPR. Regarding the APE, SHPO wrote that “we are not yet prepared to comment on the adequacy of the APE.” SHPO commented on the HPR in the same letter, stating, “[o]ur initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.” SHPO also wrote in response to the archaeological report that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [NR] within the area which was surveyed for this project by Archaeological Consultants of Ossian,” but noted that the final alignment was not yet determined and that further archaeological investigations may be necessary. SHPO asked for more

information on the project alignment and the purpose and need. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held December 15, 2009 in Fort Wayne, consulting parties expressed concern with the APE used in the HPR, noted the importance of the “park-like setting” to the Brookview neighborhood, and questioned the selection of alternatives. (See Appendix F: Correspondence and Meeting Minutes.)

On January 27, 2010, SHPO responded to minutes of the consulting party meeting held December 15, 2009. SHPO requested more information regarding the purpose and need but stated that perhaps their questions would be answered in the forthcoming information packet for consulting parties. SHPO expressed concern about the Purpose and Need of the project. SHPO also asked for “clarification” on “the substandard nature of the roadway curvature on State Boulevard,” especially in light of statements from consulting parties “that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard.” SHPO also stated “[w]e believe it is important for FHWA to evaluate this project’s purpose and need carefully before the Section 106 consultation proceeds much further. . . . Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.” Regarding the APE, SHPO asked some questions given the list of the alternatives provided at the December 15, 2009, consulting party meeting as well as in light of statements from consulting parties. “If . . . diversion of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.” SHPO also stated that “we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.” (See Appendix F: Correspondence and Meeting Minutes.)

SHPO wrote on March 10, 2010, in response to the revised meeting minutes from the December 15, 2009, meeting. In the letter, SHPO stated that the Spy Run Bridge had been finalized as a Non-Select, NR-eligible bridge per the Indiana Statewide Historic Bridge Inventory. SHPO restated the understanding that Arthur Shurcliff intended “that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses. . . that contribute to the Brookview-Irvington Park Historic District are demolished.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 15, 2011, Jill D. Downs, chairperson of the Preservation Committee of ARCH, Inc., wrote to the Deputy SHPO regarding Structurepoint’s May 19, 2011, letter. Downs questioned whether the revised Purpose and Need would “trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.” (See Appendix F: Correspondence.)

On June 16, 2011, John H. Shoaff wrote that as a member of the city council, they “face an unpleasant two-fold task of fighting for a properly democratic, participatory process...” (See Appendix F: Correspondence and Meeting Minutes.)

On June 16, 2012, Todd Zeiger, Indiana Landmarks sent an email asking for clarification of whether consulting parties were to comment on the May 19, 2012, letter and requesting a thirty day extension to the review period. (See Appendix F: Correspondence and Meeting Minutes.)

On June 17, 2011, Julie Donnell of the Friends of the Parks of Allen County sent an email to American Structurepoint conveying her letter dated June 14, 2011, in which she requested an additional thirty days of review. She expressed surprise that changes were made to Purpose and Need without “communicating this.” In the text of the email, Donnell wrote: “In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.” The email also said, “We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 1, 2011, John H. Shoaff wrote to point out discrepancies in traffic numbers presented. (See Appendix F: Correspondence and Meeting Minutes.)

On July 5, 2011, SHPO responded to Structurepoint’s letter of May 19, 2011. In their letter, SHPO wrote that it appeared appropriate to expand the APE “if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project” and stated foreseeable “areas where the character of use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well.” SHPO also requested Structurepoint review previous correspondence and meeting minutes and “make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 10 letter.” SHPO also suggested that Structurepoint share comments “that have been or shortly will be received in response to your May 19 and June 17 letters.” The letter re-stated comments from December 14, 2009, regarding the archaeology report. (See Appendix F: Correspondence and Meeting Minutes.)

Suzanne Slick, of the Irvington Park Neighborhood Association, sent an email on July 6, 2011, expressing disappointment with the project’s evaluation of impacts to neighborhood residents. The letter also stated, “There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 7, 2011, Michelle Briggs Wedaman of the Brookview Neighborhood Association emailed Structurepoint and asked that her email address be updated in the project record and that she would provide comments on behalf of the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

At an Agency Coordination meeting held July 13, 2011, SHPO suggested that Structurepoint coordinate to evaluate if the project would result in a need to change the NR district boundaries. SHPO also suggested that American Structurepoint more specifically address the consulting party issues and comments in coordination. It was also agreed upon that the ACHP should be invited to participate in the State Boulevard project at this stage in the Section 106 process, rather than later. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2011, Suzanne Slick wrote regarding the consulting party comment and response form. Slick wrote regarding the consultation process, “People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive.” Slick expressed concern with the proposed project and provided links to websites associated with

various aspects encountered in this project. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held September 1, 2011, consulting parties questioned the response process and whether all comments had been shared. Consulting parties were encouraged to respond to any Section 106 correspondence, even if the thirty day time period had passed. An effort would be made to post all Section 106 documentation on the City of Fort Wayne's website. Consulting parties suggested that the project include consultation with a professional landscape architect. It was also noted that the State Boulevard curve is included in the Fort Wayne Park and Boulevard System Historic District which is different from the Brookview-Irvington Historic District. SHPO requested the consultant "look at the implications of reduction the width of a new alignment. . .[and]. . . evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements." (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, at the Agency Meeting with FHWA and INDOT, FHWA stated it would follow-up on its invitation to the ACHP, noting that the ACHP's involvement in the process would be beneficial. During the meeting it was agreed that Structurepoint would provide consulting parties with a more elaborate alternatives analysis, would look into developing a Section 106 page for this project on the City of Fort Wayne's website, and that an addendum to the HPR would be prepared. (See Appendix F: Correspondence and Meeting Minutes.)

The ACHP responded to FHWA's invitation to join consultation on September 22, 2011. ACHP requested additional documentation in order to "determine whether our participation in the consultation to resolve adverse effects is warranted." (See Appendix F: Correspondence and Meeting Minutes.)

On November 7, 2011, SHPO responded to the material conveyed August 15, 2011, and September 29, 2011. Regarding the Brookview-Irvington Park Historic District, SHPO stated, "Having considered the marked aerial photograph shown at the last consulting party meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative." However, SHPO added, the proposed realignment of State Boulevard within the district "is not an ideal situation from a [NR] boundary delineation standpoint." Further, SHPO stated, "We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished." SHPO also offered additional comments from the September consulting party meeting that had not been recorded in the meeting minutes regarding the alternatives analysis. SHPO also questioned the feasibility of converting the existing Spy Run Bridge into a pedestrian bridge. SHPO stated they would also recommend, "where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden." SHPO also suggested shifting the proposed alignment somewhat to the east to better reflect Kessler's original plan for connecting State Boulevard. SHPO noted that this change may "result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along State Boulevard. Even if the project costs were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan." (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an Agency meeting was held to discuss the State Boulevard Project. At the meeting, Structurepoint reviewed the responses to the SHPO letter of November 7, 2011, and

agreed to send them in writing. It was decided to hold a meeting with consulting parties in early September to discuss the Additional Information HPR, to present the preferred alternative and to discuss the MOA. Mitigation ideas from that meeting included: Advisory team similar to SR 27; Photographic documentation of bridge over Spy Run; Restore character of State Blvd within the district; and Educational mitigation.

On June 22, 2012, SHPO provided comment on the AI Report. In the letter, SHPO stated, “we agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the [APE], for inclusion in the [NR].” SHPO agreed that the house at 315 East State Boulevard “does not appear to possess sufficient historical or architectural significance or integrity to be eligible of inclusion in the [NR].” SHPO also commented on the explanatory note contained in the Fort Wayne Park and Boulevard NR nomination form which stated the portion of State Boulevard within the Brookview-Irvington Historic District was individually eligible for the NR. SHPO stated, “we do not consider that comment . . . to confer individual eligibility on State Boulevard or any part of it.” SHPO further stated, “we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the [APE] is individually eligible” for the NR, but added “[w]e do not disagree, however, with the Fort Wayne Park and Boulevard system nomination identification of the portion of State Boulevard in question as a contributing resource to that historic district.” (See Appendix F: Correspondence and Meeting Minutes.)

Regarding archaeology, SHPO stated, “Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence of absence of archaeological resources.” SHPO noted that one example of areas that may need archaeological survey included “a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report.” If the entire lot would need to be acquired as part of the project, “then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated July 31, 2012, the ACHP wrote that “[b]ased upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time.” However, the Council did request to be notified in the event of an Adverse Effect finding and at that time the Council would “re-evaluate the undertaking . . . and advise you whether or not we have changed our decision regarding participation in consultation.” (See Appendix F: Correspondence and Meeting Minutes.)

On August 13, 2012, the Indiana SHPO concurred with the archaeology short report (Stilwell, 7/11/12) that “no further investigations appear necessary at these additional portions of the project area” and that the office had not identified any archaeological resources listed or eligible for listing in the NR. (See Appendix F: Correspondence and Meeting Minutes.)

At the consulting party meeting held on September 19, 2012, consulting parties were asked to provide input into mitigation for the proposed undertaking. Most comments focused on purpose and need for the project; some spoke about traffic issues. Michelle Briggs Wedaman (Brookview Neighborhood Association) asked for context sensitive solutions at the beginning of the project rather than the end. Susan Haneline (property owner) asked why the owners of the three residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes. Todd Zeiger (Indiana Landmarks) encouraged the involvement of the ACHP because he feels that there was anticipatory demolition as part of a flood control project. He asked that it be noted in this documentation that there is a bifurcation of the district. Tom Cain (City of Fort Wayne) pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in

those landscape elements needs discussion in the documentation. The visual and special components of the larger landscape need to be understood so they can be addressed in a mitigation discussion. Michael Galbraith (ARCH, Inc.) encouraged ACHP involvement, objected to the change in historic consultant, asserted that the APE is inappropriate, and raised the question of cumulative impacts. Edward Welling (Friends of the Parks of Allen County) said that mitigation is premature since the APE is not appropriate; the MOA should be postponed until Environmental Assessment is complete. Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. Dr. James Glass (Deputy SHPO) expressed reservations that consensus can be developed for this project; he stated that this meeting was the time for consulting parties to put forth mitigation ideas. John Carr (SHPO staff) requested any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion. Mr. Galbraith objected to the timing of the consulting party meeting; Patrick Carpenter, manager of the INDOT-CRO, said that the timing was established so that consulting parties could discuss mitigation and formulate new ideas. Ms. Wedamen said that she did not believe that the public process has been followed. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 14, 2012, Karl Dietsch wrote regarding a safety issue in the proposed project area. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 17, 2012, eleven residents of the Brookview Neighborhood jointly submitted a letter regarding the State Boulevard project. The letter expressed support of the project. The residents stated, "We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated to the south of its current location" and went on to conclude, "We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issue unresolved." (See Appendix F: Correspondence and Meeting Minutes.)

Sara Kruger Geyman, a member of the public, wrote in response to the meeting held September 19, 2012. (Note that the letter conveying responses to the consulting party meetings was dated August 21, 2012, and is likely a typo.) Geyman expressed concern "that residents are not and have not been consulted in this matter" and expressed dissatisfaction with meeting's facilitation. Geyman offered comments to the project in general, objecting to its necessity and, regarding Section 106, stating: "Mitigation is premature in a plan and a process that has been faulty from the beginning. It is a proverbial lollipop stuck in the hands of resident to quiet them down and distract them from the truth." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 1, 2012, Susan R. Haneline, a Brookview neighborhood home owner, expressed support for the project, noting that the current problems with flooding and bridge deterioration "do nothing to showcase what IS historical about the neighborhood." Haneline added, "We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood." Haneline's letter also included photographs showing recent flooding in the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

Susan Haneline submitted an additional letter dated October 2, 2012. Haneline stated the current proposed design, "seems . . . to actually enhance historic vision, not cause it to be destroyed." Haneline offered suggestions to "respect the historic vision," including: 1.) "Installing historically correct lighting in the area"; 2.) "Plantings and green space that gives the area a park like feel, such as period style benches, grouping of trees and flowers, perhaps even brick style side walks"; 3.) "stone or brick entrance pillars for the neighborhood"; 4.) adding trees and flower beds to the bifurcated State Boulevard; 5.) "small monuments" conveying the history of the neighborhood and

Arthur Shurcliff; 6.) “find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 3, 2012, John Shoaff wrote regarding the project, consulting party meeting, and 800.11 material. Shoaff wrote, “I cannot support the current State Boulevard widening plan in anything like its present form. . .” In particular, Shoaff objected to plans to elevate the road as a “perversion of the proper use of the ‘By-pass and Arterial concept’ . . .” Shoaff identified “two legitimate needs” in the Brookview neighborhood: the repair or replacement of the Bridge over Spy Run Creek and the elimination of a “blind spot at the foot of State Boulevard, near the intersection with Westbrook.” Shoaff stated that project plans should address these needs but be “minimally harmful to the historic district.” Shoaff added that discussion of project planning and mitigation discussion “should await the outcome of the Environmental Assessment.” (See Appendix F: Correspondence and Meeting Minutes.)

Shoaff also included comments on the September 19, 2012, consulting party meeting. Shoaff responded to comments received by Michelle Briggs Wedaman from FHWA’s representative. Shoaff objected to the facilitation of the meeting stating “the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.”

Shoaff enclosed letter “signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen.” The letter objected to the State Boulevard project. (See Appendix F: Correspondence and Meeting Minutes.)

Also on October 3, 2012, Suzanne Slick wrote regarding the project and the consulting party meeting of September 19, 2012. Slick stated that not building the project is preferable to mitigation and objected to the facilitation of the consulting party meeting. The letter re-stated some comments offered previously by consulting parties regarding the Purpose and Need and design. Slick objected to the traffic data previously supplied by Structurepoint and offered two examples in which she found low-volume traffic while utilizing the State Boulevard. Slick stated the APE was inappropriate. (See Appendix F: Correspondence and Meeting Minutes.)

Julie Downs, Friends of the Parks of Allen County, submitted comments via a letter dated October 3, 2012. Downs stated the Friends of the Parks of Allen County agreed with the finding of adverse effect for the project but added “any discussion of mitigation is, at best, premature; at worst, the proposed [MOA] is a bad faith attempt to confuse an already complicated and unfair process.” Downs also stated the “APE is not comprehensive enough and should include historic districts along State Boulevard” and “it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.” Finally, on behalf of members of the Friends of the Parks of Allen County who attended the September 19, 2012, consulting party meeting, Downs objected to the facilitation of the meeting and concluded, “Under these circumstances, the public is not being served properly at all.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Jill Downs wrote regarding the 800.11(e) and draft MOA. Downs agreed with the project’s adverse effect finding but noted “the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration” and pointed to the September 19, 2012, consulting party meeting as proof of this breakdown. She stated it was premature to discuss mitigation because the Environmental Assessment had not been completed; the bifurcation of the district, elevation of State Boulevard, and the Pufferbelly Trail project should be added to the list of adverse effects; the Pufferbelly Trail project should be incorporated into the effects discussion; and the project has not fully accounted for the previous removal of several homes by the City of Fort Wayne which creates the impression of less impact

as a result of the project. Downs concluded by stating she did not see the need to reconstruct State Boulevard. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michael Galbraith of ARCH, Inc., wrote formally requesting an extension of the thirty-day comment period for the proposed MOA and mitigation measures. Galbraith stated, "We do not in any form, fashion, or manner concur with the proposed mitigation as present either in the draft supplied with the FHWA 4(f) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintraut." Galbraith also stated that "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact." (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michelle Briggs Wedaman of the Brookview Neighborhood Association, wrote requesting a thirty-day extension of the consulting party comment period to incorporate the material provided on September 18, 2012, into their comments. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Wedaman stated that previous questions from the December 2009 and September 2011 consulting party meetings "have remained unanswered," particularly those dealing "Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the area of impact of this project." Wedaman questioned how an appropriate discussion of mitigation could take place prior to the completion of the environmental assessment. (See Appendix F: Correspondence and Meeting Minutes.)

The SHPO wrote in response to the project in a letter dated October 4, 2012. SHPO concurred with the opinion of the archaeological short report, the Section 106 finding of effect and that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and Bridge on State Boulevard over Spy Run would all be adversely affected as part of this undertaking. SHPO expressed concern "about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District and suggested some minimization measures. In particular, SHPO wondered if "it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive." SHPO expressed sympathy for the preference of some property owners along the south side of State Boulevard who preferred to have their entire property, rather than a smaller portion, purchased, "However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect."

SHPO also offered suggestions for design for minimizing impacts and suggestions for mitigation, including an advisory team, use of context-sensitive designs, photographic documentation of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Todd Zeiger of Indiana Landmarks—Northern Regional Office wrote formally requesting a thirty-day extension on the comment period in light of the material conveyed September 18, 2012. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Zeiger stated "We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(f) compliance document." Zeiger added "we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that

information and assess it within the context of the other informant provided in the 4(F) document.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Tom Cain, Fort Wayne urban designer and Creager Smith, Fort Wayne historic preservation planner, wrote regarding the project. Both agreed with the project’s adverse effect finding. The letter listed twenty-one specific adverse effects of the project on the landscape to serve as the “potential basis of mitigation measures.” Cain and Smith also stated “we are available to assist in the development of mitigation design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on a team.” (See Appendix F: Correspondence and Meeting Minutes.)

On October 15, 2012, Tom Cain, City of Fort Wayne, called W&A to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but wanted to make suggestions within the context of SHPO’s assessment of project impacts so that the City may address all adverse effects. Cain also stated that impacts to the Brookview neighborhood should be enumerated. (See Appendix F: Correspondence and Meeting Minutes.)

On October 16, 2012, W&A contacted Tom Cain in response to his phone call the previous day. W&A explained that Structurepoint was very glad to have his input on this project and, at a minimum, would consult with him prior to the agency meeting. Cain spoke about the landscape changes that would take place as a result of the undertaking, particularly the changes from private to public space around the undertaking. He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s. Cain stated he would like for mitigation to deal with changes in scale that will occur; tree planting should occur within three feet of the roadway (and not the standard ten feet required on highways.) Cain stated this would change the scale of the undertaking for the residents. Cain also stated he would convey additional mitigation suggestions via email and stated the importance of achieving the “right feel” for the space. (See Appendix F: Correspondence and Meeting Minutes.)

On November 15, 2012, SHPO wrote in response to Structurepoint’s offer to draft specific language for the MOA. (See Appendix F: Correspondence and Meeting Minutes.)

On December 18, 2012, Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain (landscape architect/City of Fort Wayne) made the presentation. Cain’s plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A, Plans.) Dr. James Glass (SHPO) expressed appreciation for the effort Mr. Cain had put forth for a thoughtful landscape plan. Dr. Glass said that his office needed time to digest but that he understood Mr. Cain’s point that in a Section 106 sense, there was a need to mitigate for the houses and for the loss of historic character. He also understood that there are larger issues of flood control and engineering that make this project difficult. There was discussion of other resources that may be preserved as far as compensation for the lost historic resources (houses and landscaping). It was agreed that SHPO would be given time to digest the landscape design presented at the meeting and that the City and its consultants would look for additional ways to mitigate, such as grants to rehabilitate the facades of existing houses (if practical and legally

viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. Mary Ann Naber (FHWA preservation officer) suggested that the attendees look at the mitigation provided in Tampa. (See Appendix F: Correspondence and Meeting Minutes.)

No other comments were received.

A public notice of Adverse Effect will be posted in a local newspaper and the public afforded thirty (30) days to comment. If appropriate, this document will be revised to reflect those comments.

APPENDIX A. Plans

PROJECT	DESIGNATION
1005151	1005151
CONTRACT	BRIDGE FILE

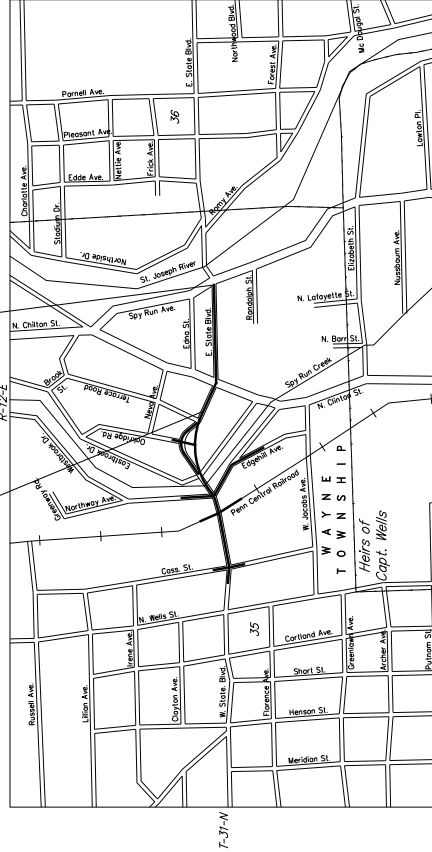
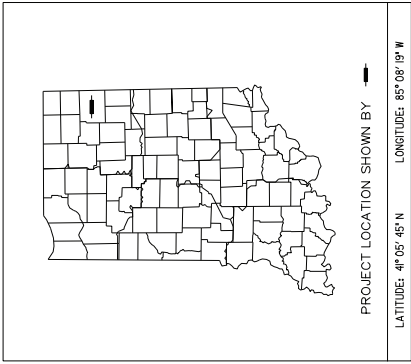
TRAFFIC DATA	STATE BLVD. - LINE "PR-A"
A.A.D.T. (2009)	20,650 V.P.D.
A.A.D.T. (2030)	25,000 V.P.D.
DIRECTIONAL DISTRIBUTION	50% V.P.D.
TRUCKS	2% D.H.V.
MAINLINE EB/AB	2
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Minor Arterial
URBAN / RURAL	Urban (Built-Up)
ACCESS CONTROL	None

INDIANA DEPARTMENT OF TRANSPORTATION

ROAD PLANS

STATE BLVD. RECONSTRUCTION - PART 1
PROJECT NO. 1005151 P.E.
PROJECT NO. R/W
PROJECT NO. CONST.

Project Description: Road Reconstruction on State Boulevard Beginning 130.00 Feet East of the Intersection of State Boulevard and Terrace Road, Thence East 999.00 Feet Along Line "PR-A" to the Intersection of Spy Run Ave. and State Boulevard, All in Section 35, Township 31 North, Range 12 East, Wayne Township, Allen County, Indiana



Scale: 1" = 500'

BRIDGE LENGTH :	N/A Miles
ROADWAY LENGTH :	0.89 Miles
TOTAL LENGTH :	0.89 Miles
MAX. GRADE :	1.50 %

HYDROLOGIC UNIT CODE: 0410000406060

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2010
TO BE USED WITH THESE PLANS

ALLEN COUNTY

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005151
CONTRACT	SHEETS
	1 of 89
	PROJECT NO.
	1005151

PLANS PREPARED BY:	American Structurepoint, Inc.	(317) 547-5580
CERTIFIED BY:		PHONE NUMBER
APPROVED		DATE
FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE



INDEX TO SHEETS

SHEET NO.	DRAWING INDEX
1	Title Sheet
2	Unit 1 Core Sections
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99	Ref No.
100	Ref No.

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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE
	ASU	DRAWN: CAK	
DESIGNED:	SMC	CHECKED:	SMC

INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
		VERTICAL SCALE	DESIGNATION NO. 1005151

INDEX & GENERAL NOTES		SURVEY BOOK	SHEETS
		CONTRACT	2 OF 41 89
			PROJECT NO. 1005151

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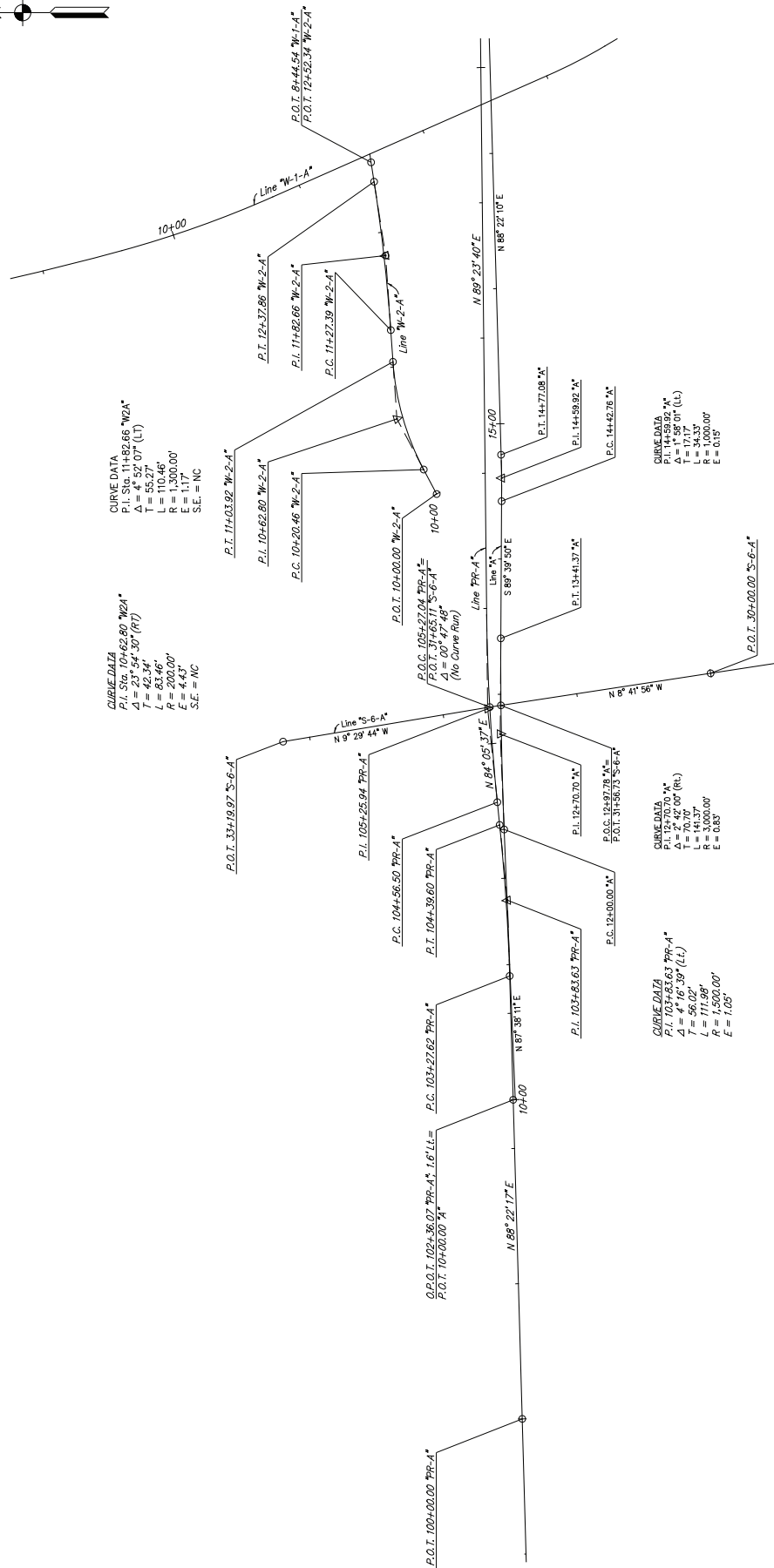
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SEC. 35, T. 31 N., R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

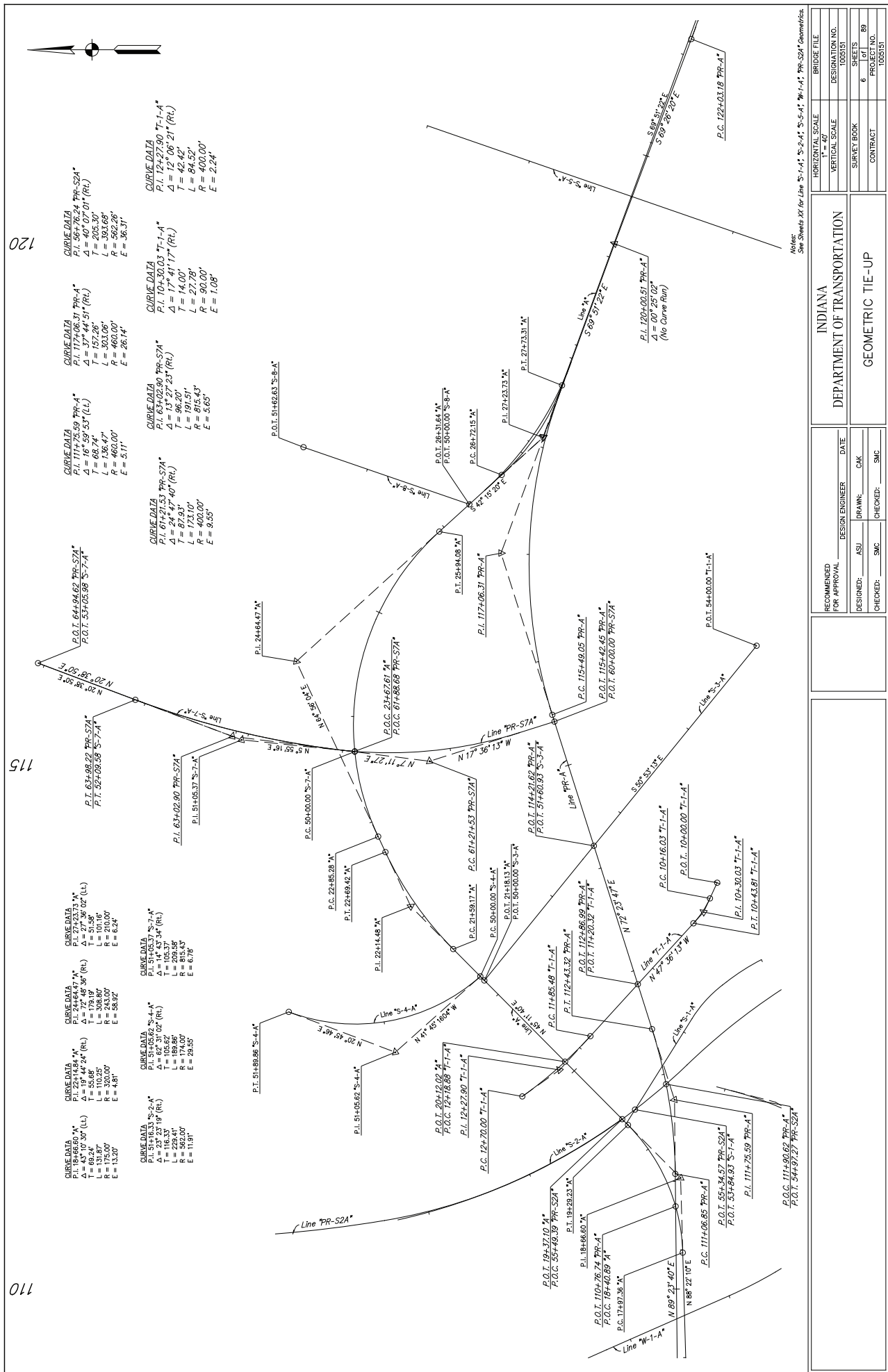


				RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE	
												VERTICAL SCALE		DESIGNATION NO. 1005151	
				DESIGNED: ASU		DRAWN: PJT						SURVEY BOOK		SHEETS 4 of 89	
				CHECKED: SMC		CHECKED: SMC						CONTRACT		PROJECT NO. 1005151	



Notes:

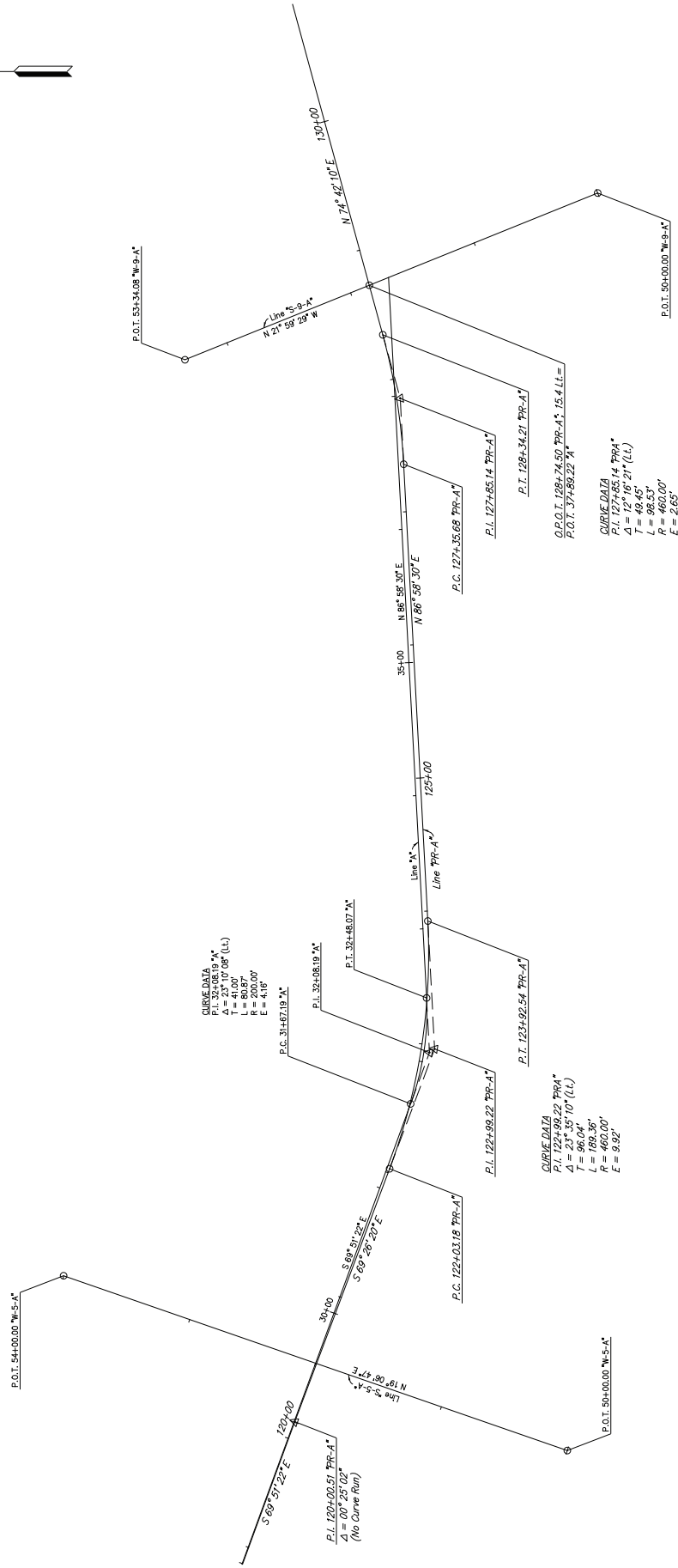
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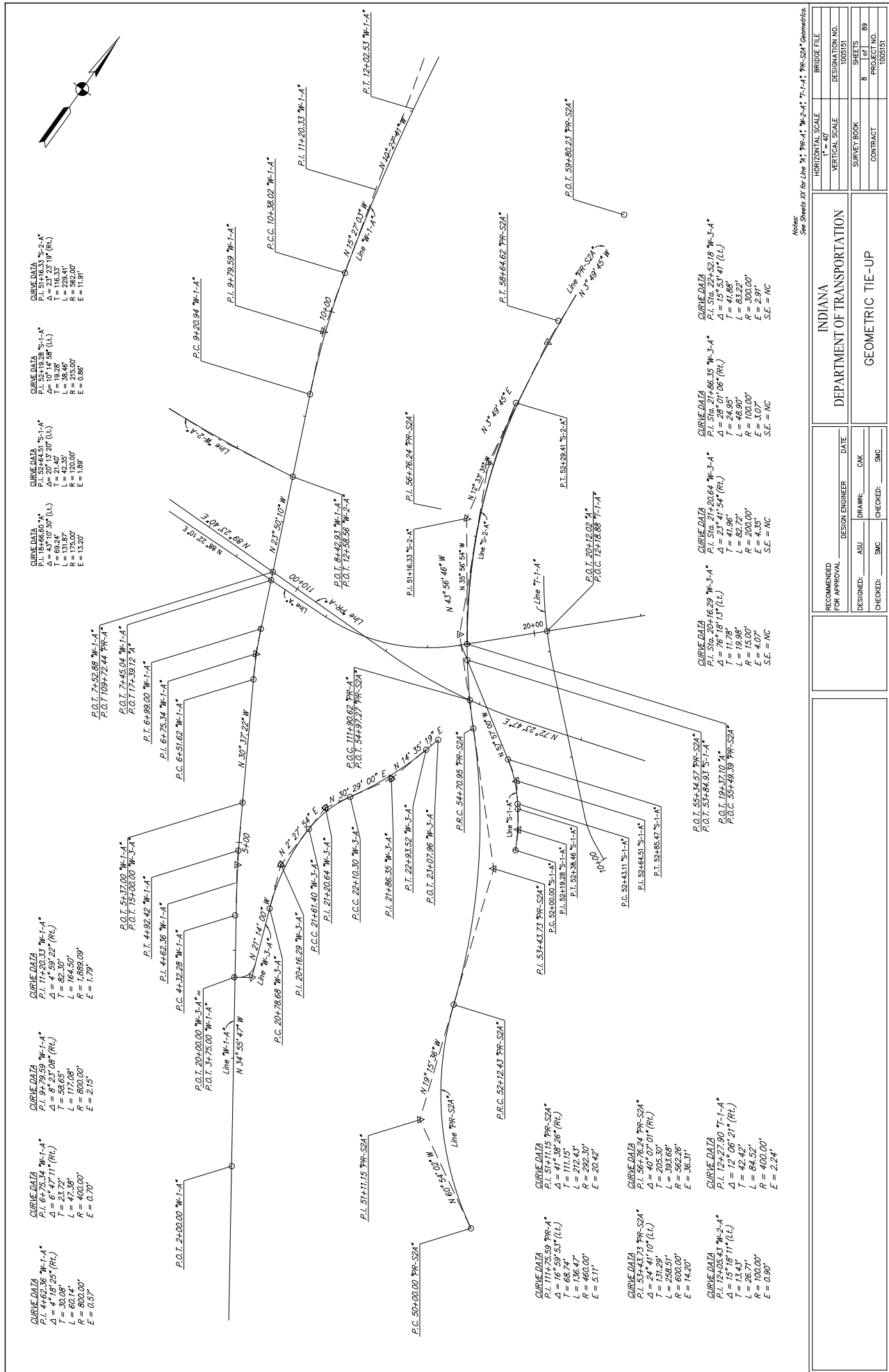
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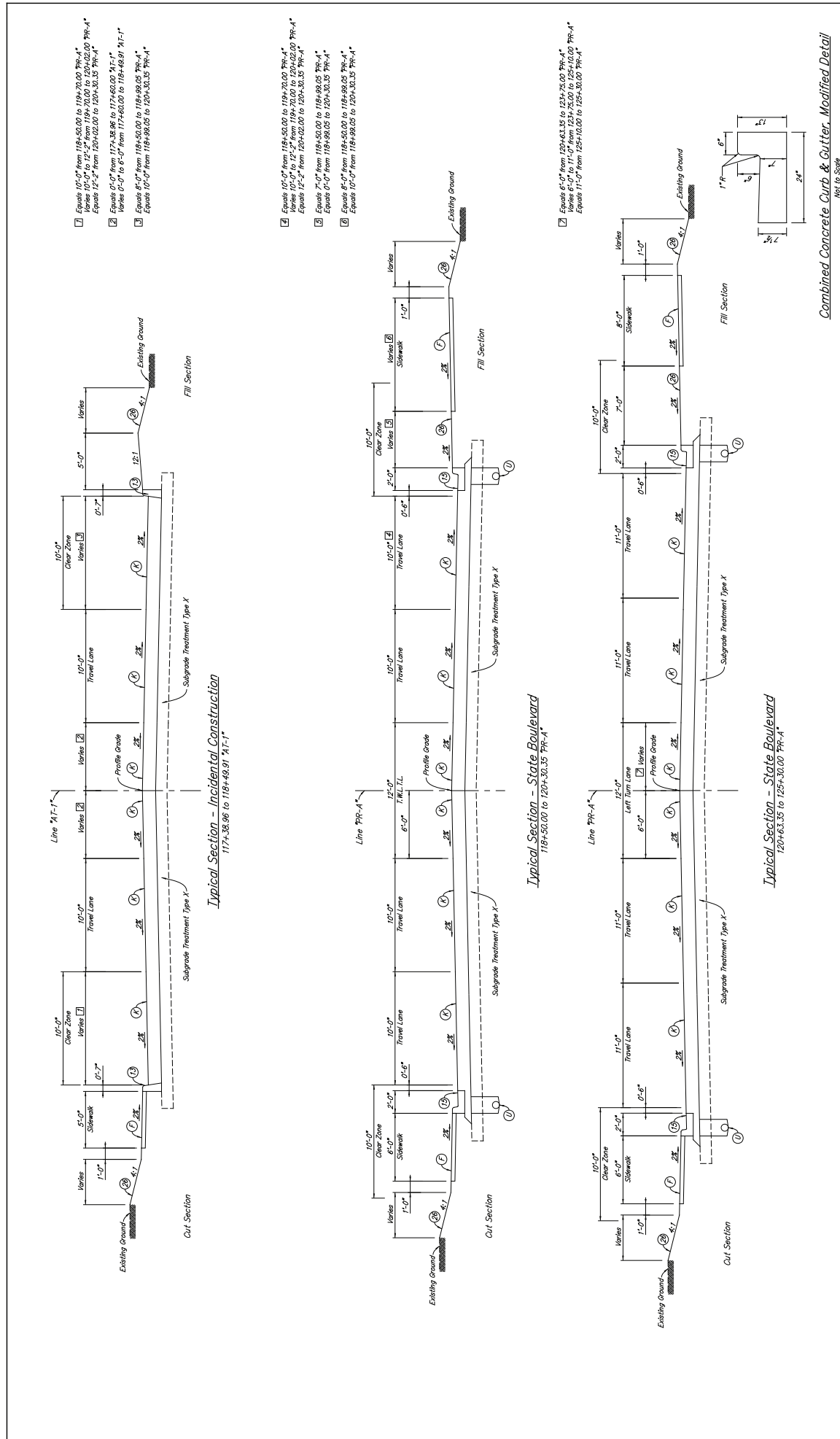
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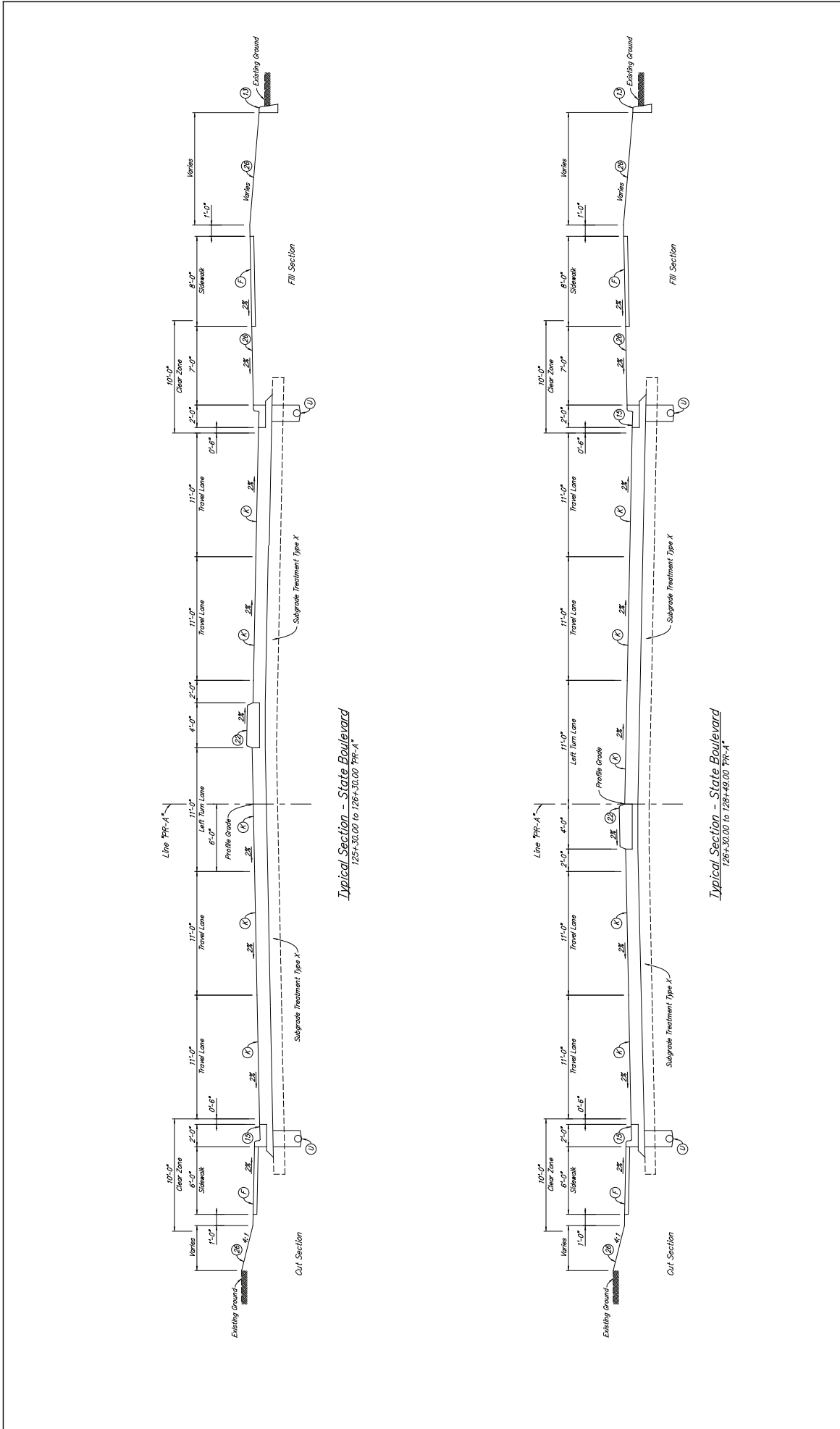


RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: ASU		DRAWN: CAK			
CHECKED: SMC		CHECKED: SMC			
INDIANA					
DEPARTMENT OF TRANSPORTATION					
GEOMETRIC TIE-UP					
HORIZONTAL SCALE		BRIDGE FILE			
VERTICAL SCALE		DESIGNATION NO. 10051			
SURVEY BOOK		SHEETS 7 of 89			
CONTRACT		PROJECT NO. 10051			



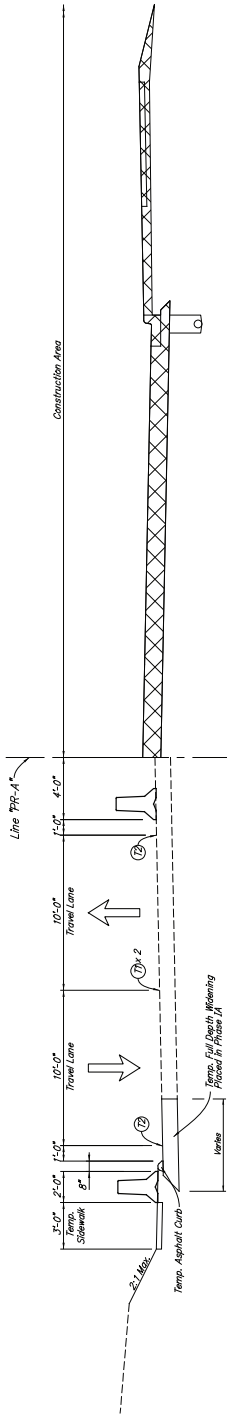


<div>LEGEND</div> <div><div><div>1</div><div>see #5/Std. C2/04-HMA, x, x/s, Surface, x/s, on</div></div><div><div>2</div><div>see #5/Std. C2/04-HMA, x, x/s, Surface, x/s, on</div></div><div><div>3</div><div>see #5/Std. C2/04-HMA, x, x/s, Base, x/s, on</div></div><div><div>4</div><div>see #5/Std. C2/04-HMA, x, x/s, Base, x/s, on</div></div><div><div>5</div><div>see #5/Std. C2/04-HMA, x, x/s, Intermediate OG, 1/2mm on</div></div><div><div>6</div><div>see #5/Std. C2/04-HMA, x, x/s, Base, 1/2</div></div><div><div>7</div><div>Concrete Slabwork, 4"</div></div><div><div>8</div><div>see #5/Std. C2/04-HMA, Type 1, 4", on</div></div><div><div>9</div><div>see #5/Std. C2/04-HMA, Type 2, 4", on</div></div><div><div>10</div><div>see #5/Std. C2/04-HMA, Type 3, 4", on</div></div><div><div>11</div><div>see #5/Std. C2/04-HMA, Type 4, 4", on</div></div><div><div>12</div><div>see #5/Std. C2/04-HMA, Type 5, 4", on</div></div><div><div>13</div><div>see #5/Std. C2/04-HMA, Type 6, 4", on</div></div><div><div>14</div><div>see #5/Std. C2/04-HMA, Type 7, 4", on</div></div><div><div>15</div><div>see #5/Std. C2/04-HMA, Type 8, 4", on</div></div><div><div>16</div><div>see #5/Std. C2/04-HMA, Type 9, 4", on</div></div><div><div>17</div><div>see #5/Std. C2/04-HMA, Type 10, 4", on</div></div><div><div>18</div><div>see #5/Std. C2/04-HMA, Type 11, 4", on</div></div><div><div>19</div><div>see #5/Std. C2/04-HMA, Type 12, 4", on</div></div><div><div>20</div><div>see #5/Std. C2/04-HMA, Type 13, 4", on</div></div><div><div>21</div><div>see #5/Std. C2/04-HMA, Type 14, 4", on</div></div><div><div>22</div><div>see #5/Std. C2/04-HMA, Type 15, 4", on</div></div><div><div>23</div><div>see #5/Std. C2/04-HMA, Type 16, 4", on</div></div><div><div>24</div><div>see #5/Std. C2/04-HMA, Type 17, 4", on</div></div><div><div>25</div><div>see #5/Std. C2/04-HMA, Type 18, 4", on</div></div><div><div>26</div><div>see #5/Std. C2/04-HMA, Type 19, 4", on</div></div><div><div>27</div><div>see #5/Std. C2/04-HMA, Type 20, 4", on</div></div><div><div>28</div><div>see #5/Std. C2/04-HMA, Type 21, 4", on</div></div><div><div>29</div><div>see #5/Std. C2/04-HMA, Type 22, 4", on</div></div><div><div>30</div><div>see #5/Std. C2/04-HMA, Type 23, 4", on</div></div><div><div>31</div><div>see #5/Std. C2/04-HMA, Type 24, 4", on</div></div><div><div>32</div><div>see #5/Std. C2/04-HMA, Type 25, 4", on</div></div><div><div>33</div><div>see #5/Std. C2/04-HMA, Type 26, 4", on</div></div><div><div>34</div><div>see #5/Std. C2/04-HMA, Type 27, 4", on</div></div><div><div>35</div><div>see #5/Std. C2/04-HMA, Type 28, 4", on</div></div><div><div>36</div><div>see #5/Std. C2/04-HMA, Type 29, 4", on</div></div><div><div>37</div><div>see #5/Std. C2/04-HMA, Type 30, 4", on</div></div><div><div>38</div><div>see #5/Std. C2/04-HMA, Type 31, 4", on</div></div><div><div>39</div><div>see #5/Std. C2/04-HMA, Type 32, 4", on</div></div><div><div>40</div><div>see #5/Std. C2/04-HMA, Type 33, 4", on</div></div><div><div>41</div><div>see #5/Std. C2/04-HMA, Type 34, 4", on</div></div><div><div>42</div><div>see #5/Std. C2/04-HMA, Type 35, 4", on</div></div><div><div>43</div><div>see #5/Std. C2/04-HMA, Type 36, 4", on</div></div><div><div>44</div><div>see #5/Std. C2/04-HMA, Type 37, 4", on</div></div><div><div>45</div><div>see #5/Std. C2/04-HMA, Type 38, 4", on</div></div><div><div>46</div><div>see #5/Std. C2/04-HMA, Type 39, 4", on</div></div><div><div>47</div><div>see #5/Std. C2/04-HMA, Type 40, 4", on</div></div><div><div>48</div><div>see #5/Std. C2/04-HMA, Type 41, 4", on</div></div><div><div>49</div><div>see #5/Std. C2/04-HMA, Type 42, 4", on</div></div><div><div>50</div><div>see #5/Std. C2/04-HMA, Type 43, 4", on</div></div><div><div>51</div><div>see #5/Std. C2/04-HMA, Type 44, 4", on</div></div><div><div>52</div><div>see #5/Std. C2/04-HMA, Type 45, 4", on</div></div><div><div>53</div><div>see #5/Std. C2/04-HMA, Type 46, 4", on</div></div><div><div>54</div><div>see #5/Std. C2/04-HMA, Type 47, 4", on</div></div><div><div>55</div><div>see #5/Std. C2/04-HMA, Type 48, 4", on</div></div><div><div>56</div><div>see #5/Std. C2/04-HMA, Type 49, 4", on</div></div><div><div>57</div><div>see #5/Std. C2/04-HMA, Type 50, 4", on</div></div><div><div>58</div><div>see #5/Std. C2/04-HMA, Type 51, 4", on</div></div><div><div>59</div><div>see #5/Std. C2/04-HMA, Type 52, 4", on</div></div><div><div>60</div><div>see #5/Std. C2/04-HMA, Type 53, 4", on</div></div><div><div>61</div><div>see #5/Std. C2/04-HMA, Type 54, 4", on</div></div><div><div>62</div><div>see #5/Std. C2/04-HMA, Type 55, 4", on</div></div><div><div>63</div><div>see #5/Std. C2/04-HMA, Type 56, 4", on</div></div><div><div>64</div><div>see #5/Std. C2/04-HMA, Type 57, 4", on</div></div><div><div>65</div><div>see #5/Std. C2/04-HMA, Type 58, 4", on</div></div><div><div>66</div><div>see #5/Std. C2/04-HMA, Type 59, 4", on</div></div><div><div>67</div><div>see #5/Std. C2/04-HMA, Type 60, 4", on</div></div><div><div>68</div><div>see #5/Std. C2/04-HMA, Type 61, 4", on</div></div><div><div>69</div><div>see #5/Std. C2/04-HMA, Type 62, 4", on</div></div><div><div>70</div><div>see #5/Std. C2/04-HMA, Type 63, 4", on</div></div><div><div>71</div><div>see #5/Std. C2/04-HMA, Type 64, 4", on</div></div><div><div>72</div><div>see #5/Std. C2/04-HMA, Type 65, 4", on</div></div><div><div>73</div><div>see #5/Std. C2/04-HMA, Type 66, 4", on</div></div><div><div>74</div><div>see #5/Std. C2/04-HMA, Type 67, 4", on</div></div><div><div>75</div><div>see #5/Std. C2/04-HMA, Type 68, 4", on</div></div><div><div>76</div><div>see #5/Std. C2/04-HMA, Type 69, 4", on</div></div><div><div>77</div><div>see #5/Std. C2/04-HMA, Type 70, 4", on</div></div><div><div>78</div><div>see #5/Std. C2/04-HMA, Type 71, 4", on</div></div><div><div>79</div><div>see #5/Std. C2/04-HMA, Type 72, 4", on</div></div><div><div>80</div><div>see #5/Std. C2/04-HMA, Type 73, 4", on</div></div><div><div>81</div><div>see #5/Std. C2/04-HMA, Type 74, 4", on</div></div><div><div>82</div><div>see #5/Std. C2/04-HMA, Type 75, 4", on</div></div><div><div>83</div><div>see #5/Std. C2/04-HMA, Type 76, 4", on</div></div><div><div>84</div><div>see #5/Std. C2/04-HMA, Type 77, 4", on</div></div><div><div>85</div><div>see #5/Std. C2/04-HMA, Type 78, 4", on</div></div><div><div>86</div><div>see #5/Std. C2/04-HMA, Type 79, 4", on</div></div><div><div>87</div><div>see #5/Std. C2/04-HMA, Type 80, 4", on</div></div><div><div>88</div><div>see #5/Std. C2/04-HMA, Type 81, 4", on</div></div><div><div>89</div><div>see #5/Std. C2/04-HMA, Type 82, 4", on</div></div><div><div>90</div><div>see #5/Std. C2/04-HMA, Type 83, 4", on</div></div><div><div>91</div><div>see #5/Std. C2/04-HMA, Type 84, 4", on</div></div><div><div>92</div><div>see #5/Std. C2/04-HMA, Type 85, 4", on</div></div><div><div>93</div><div>see #5/Std. C2/04-HMA, Type 86, 4", on</div></div><div><div>94</div><div>see #5/Std. C2/04-HMA, Type 87, 4", on</div></div><div><div>95</div><div>see #5/Std. C2/04-HMA, Type 88, 4", on</div></div><div><div>96</div><div>see #5/Std. C2/04-HMA, Type 89, 4", on</div></div><div><div>97</div><div>see #5/Std. C2/04-HMA, Type 90, 4", on</div></div><div><div>98</div><div>see #5/Std. C2/04-HMA, Type 91, 4", on</div></div><div><div>99</div><div>see #5/Std. C2/04-HMA, Type 92, 4", on</div></div><div><div>100</div><div>see #5/Std. C2/04-HMA, Type 93, 4", on</div></div><div><div>101</div><div>see #5/Std. C2/04-HMA, Type 94, 4", on</div></div><div><div>102</div><div>see #5/Std. C2/04-HMA, Type 95, 4", on</div></div><div><div>103</div><div>see #5/Std. C2/04-HMA, Type 96, 4", on</div></div><div><div>104</div><div>see #5/Std. C2/04-HMA, Type 97, 4", on</div></div><div><div>105</div><div>see #5/Std. C2/04-HMA, Type 98, 4", on</div></div><div><div>106</div><div>see #5/Std. C2/04-HMA, Type 99, 4", on</div></div><div><div>107</div><div>see #5/Std. C2/04-HMA, Type 100, 4", on</div></div><div><div>108</div><div>see #5/Std. C2/04-HMA, Type 101, 4", on</div></div><div><div>109</div><div>see #5/Std. C2/04-HMA, Type 102, 4", on</div></div><div><div>110</div><div>see #5/Std. C2/04-HMA, Type 103, 4", on</div></div><div><div>111</div><div>see #5/Std. C2/04-HMA, Type 104, 4", on</div></div><div><div>112</div><div>see #5/Std. C2/04-HMA, Type 105, 4", on</div></div><div><div>113</div><div>see #5/Std. C2/04-HMA, Type 106, 4", on</div></div><div><div>114</div><div>see #5/Std. C2/04-HMA, Type 107, 4", on</div></div><div><div>115</div><div>see #5/Std. C2/04-HMA, Type 108, 4", on</div></div><div><div>116</div><div>see #5/Std. C2/04-HMA, Type 109, 4", on</div></div><div><div>117</div><div>see #5/Std. C2/04-HMA, Type 110, 4", on</div></div><div><div>118</div><div>see #5/Std. C2/04-HMA, Type 111, 4", on</div></div><div><div>119</div><div>see #5/Std. C2/04-HMA, Type 112, 4", on</div></div><div><div>120</div><div>see #5/Std. C2/04-HMA, Type 113, 4", on</div></div><div><div>121</div><div>see #5/Std. C2/04-HMA, Type 114, 4", on</div></div><div><div>122</div><div>see #5/Std. C2/04-HMA, Type 115, 4", on</div></div><div><div>123</div><div>see #5/Std. C2/04-HMA, Type 116, 4", on</div></div><div><div>124</div><div>see #5/Std. C2/04-HMA, Type 117, 4", on</div></div><div><div>125</div><div>see #5/Std. C2/04-HMA, Type 118, 4", on</div></div><div><div>126</div><div>see #5/Std. C2/04-HMA, Type 119, 4", on</div></div><div><div>127</div><div>see #5/Std. C2/04-HMA, Type 120, 4", on</div></div><div><div>128</div><div>see #5/Std. C2/04-HMA, Type 121, 4", on</div></div><div><div>129</div><div>see #5/Std. C2/04-HMA, Type 122, 4", on</div></div><div><div>130</div><div>see #5/Std. C2/04-HMA, Type 123, 4", on</div></div><div><div>131</div><div>see #5/Std. C2/04-HMA, Type 124, 4", on</div></div><div><div>132</div><div>see #5/Std. C2/04-HMA, Type 125, 4", on</div></div><div><div>133</div><div>see #5/Std. C2/04-HMA, Type 126, 4", on</div></div><div><div>134</div><div>see #5/Std. C2/04-HMA, Type 127, 4", on</div></div><div><div>135</div><div>see #5/Std. C2/04-HMA, Type 128, 4", on</div></div><div><div>136</div><div>see #5/Std. C2/04-HMA, Type 129, 4", on</div></div><div><div>137</div><div>see #5/Std. C2/04-HMA, Type 130, 4", on</div></div><div><div>138</div><div>see #5/Std. C2/04-HMA, Type 131, 4", on</div></div><div><div>139</div><div>see #5/Std. C2/04-HMA, Type 132, 4", on</div></div><div><div>140</div><div>see #5/Std. C2/04-HMA, Type 133, 4", on</div></div><div><div>141</div><div>see #5/Std. C2/04-HMA, Type 134, 4", on</div></div><div><div>142</div><div>see #5/Std. C2/04-HMA, Type 135, 4", on</div></div><div><div>143</div><div>see #5/Std. C2/04-HMA, Type 136, 4", on</div></div><div><div>144</div><div>see #5/Std. C2/04-HMA, Type 137, 4", on</div></div><div><div>145</div><div>see #5/Std. C2/04-HMA, Type 138, 4", on</div></div><div><div>146</div><div>see #5/Std. C2/04-HMA, Type 139, 4", on</div></div><div><div>147</div><div>see #5/Std. C2/04-HMA, Type 140, 4", on</div></div><div><div>148</div><div>see #5/Std. C2/04-HMA, Type 141, 4", on</div></div><div><div>149</div><div>see #5/Std. C2/04-HMA, Type 142, 4", on</div></div><div><div>150</div><div>see #5/Std. C2/04-HMA, Type 143, 4", on</div></div><div><div>151</div><div>see #5/Std. C2/04-HMA, Type 144, 4", on</div></div><div><div>152</div><div>see #5/Std. C2/04-HMA, Type 145, 4", on</div></div><div><div>153</div><div>see #5/Std. C2/04-HMA, Type 146, 4", on</div></div><div><div>154</div><div>see #5/Std. C2/04-HMA, Type 147, 4", on</div></div><div><div>155</div><div>see #5/Std. C2/04-HMA, Type 148, 4", on</div></div><div><div>156</div><div>see #5/Std. C2/04-HMA, Type 149, 4", on</div></div><div><div>157</div><div>see #5/Std. C2/04-HMA, Type 150, 4", on</div></div><div><div>158</div><div>see #5/Std. C2/04-HMA, Type 151, 4", on</div></div><div><div>159</div><div>see #5/Std. C2/04-HMA, Type 152, 4", on</div></div><div><div>160</div><div>see #5/Std. C2/04-HMA, Type 153, 4", on</div></div><div><div>161</div><div>see #5/Std. C2/04-HMA, Type 154, 4", on</div></div><div><div>162</div><div>see #5/Std. C2/04-HMA, Type 155, 4", on</div></div><div><div>163</div><div>see #5/Std. C2/04-HMA, Type 156, 4", on</div></div><div><div>164</div><div>see #5/Std. C2/04-HMA, Type 157, 4", on</div></div><div><div>165</div><div>see #5/Std. C2/04-HMA, Type 158, 4", on</div></div><div><div>166</div><div>see #5/Std. C2/04-HMA, Type 159, 4", on</div></div><div><div>167</div><div>see #5/Std. C2/04-HMA, Type 160, 4", on</div></div><div><div>168</div><div>see #5/Std. C2/04-HMA, Type 161, 4", on</div></div><div><div>169</div><div>see #5/Std. C2/04-HMA, Type 162, 4", on</div></div><div><div>170</div><div>see #5/Std. C2/04-HMA, Type 163, 4", on</div></div><div><div>171</div><div>see #5/Std. C2/04-HMA, Type 164, 4", on</div></div><div><div>172</div><div>see #5/Std. C2/04-HMA, Type 165, 4", on</div></div><div><div>173</div><div>see #5/Std. C2/04-HMA, Type 166, 4", on</div></div><div><div>174</div><div>see #5/Std. C2/04-HMA, Type 167, 4", on</div></div><div><div>175</div><div>see #5/Std. C2/04-HMA, Type 168, 4", on</div></div><div><div>176</div><div>see #5/Std. C2/04-HMA, Type 169, 4", on</div></div><div><div>177</div><div>see #5/Std. C2/04-HMA, Type 170, 4", on</div></div><div><div>178</div><div>see #5/Std. C2/04-HMA, Type 171, 4", on</div></div><div><div>179</div><div>see #5/Std. C2/04-HMA, Type 172, 4", on</div></div><div><div>180</div><div>see #5/Std. C2/04-HMA, Type 173, 4", on</div></div><div><div>181</div><div>see #5/Std. C2/04-HMA, Type 174, 4", on</div></div><div><div>182</div><div>see #5/Std. C2/04-HMA, Type 175, 4", on</div></div><div><div>183</div><div>see #5/Std. C2/04-HMA, Type 176, 4", on</div></div><div><div>184</div><div>see #5/Std. C2/04-HMA, Type 177, 4", on</div></div><div><div>185</div><div>see #5/Std. C2/04-HMA, Type 178, 4", on</div></div><div><div>186</div><div>see #5/Std. C2/04-HMA, Type 179, 4", on</div></div><div><div>187</div><div>see #5/Std. C2/04-HMA, Type 180, 4", on</div></div><div><div>188</div><div>see #5/Std. C2/04-HMA, Type 181, 4", on</div></div><div><div>189</div><div>see #5/Std. C2/04-HMA, Type 182, 4", on</div></div><div><div>190</div><div>see #5/Std. C2/04-HMA, Type 183, 4", on</div></div><div><div>191</div><div>see #5/Std. C2/04-HMA, Type 184, 4", on</div></div><div><div>192</div><div>see #5/Std. C2/04-HMA, Type 185, 4", on</div></div><div><div>193</div><div>see #5/Std. C2/04-HMA, Type 186, 4", on</div></div><div><div>194</div><div>see #5/Std. C2/04-HMA, Type 187, 4", on</div></div><div><div>195</div><div>see #5/Std. C2/04-HMA, Type 188, 4", on</div></div><div><div>196</div><div>see #5/Std. C2/04-HMA, Type 189, 4", on</div></div><div><div>197</div><div>see #5/Std. C2/04-HMA, Type 190, 4", on</div></div><div><div>198</div><div>see #5/Std. C2/04-HMA, Type 191, 4", on</div></div><div><div>199</div><div>see #5/Std. C2/04-HMA, Type 192, 4", on</div></div><div><div>200</div><div>see #5/Std. C2/04-HMA, Type 193, 4", on</div></div><div><div>201</div><div>see #5/Std. C2/04-HMA, Type 194, 4", on</div></div><div><div>202</div><div>see #5/Std. C2/04-HMA, Type 195, 4", on</div></div><div><div>203</div><div>see #5/Std. C2/04-HMA, Type 196, 4", on</div></div><div><div>204</div><div>see #5/Std. C2/04-HMA, Type 197, 4", on</div></div><div><div>205</div><div>see #5/Std. C2/04-HMA, Type 198, 4", on</div></div><div><div>206</div><div>see #5/Std. C2/04-HMA, Type 199, 4", on</div></div><div><div>207</div><div>see #5/Std. C2/04-HMA, Type 200, 4", on</div></div><div><div>208</div><div>see #5/Std. C2/04-HMA, Type 201, 4", on</div></div><div><div>209</div><div>see #5/Std. C2/04-HMA, Type 202, 4", on</div></div><div><div>210</div><div>see #5/Std. C2/04-HMA, Type 203, 4", on</div></div><div><div>211</div><div>see #5/Std. C2/04-HMA, Type 204, 4", on</div></div><div><div>212</div><div>see #5/Std. C2/04-HMA, Type 205, 4", on</div></div><div><div>213</div><div>see #5/Std. C2/04-HMA, Type 206, 4", on</div></div><div><div>214</div><div>see #5/Std. C2/04-HMA, Type 207, 4", on</div></div><div><div>215</div><div>see #5/Std. C2/04-HMA, Type 208, 4", on</div></div><div><div>216</div><div>see #5/Std. C2/04-HMA, Type 209, 4", on</div></div><div><div>217</div><div>see #5/Std. C2/04-HMA, Type 210, 4", on</div></div><div><div>218</div><div>see #5/Std. C2/04-HMA, Type 211, 4", on</div></div><div><div>219</div><div>see #5/Std. C2/04-HMA, Type 212, 4", on</div></div><div><div>220</div><div>see #5/Std. C2/04-HMA, Type 213, 4", on</div></div><div><div>221</div><div>see #5/Std. C2/04-HMA, Type 214, 4", on</div></div><div><div>222</div><div>see #5/Std. C2/04-HMA, Type 215, 4", on</div></div><div><div>223</div><div>see #5/Std. C2/04-HMA, Type 216, 4", on</div></div><div><div>224</div><div>see #5/Std. C2/04-HMA, Type 217, 4", on</div></div><div><div>225</div><div>see #5/Std. C2/04-HMA, Type 218, 4", on</div></div><div><div>226</div><div>see #5/Std. C2/04-HMA, Type 219, 4", on</div></div><div><div>227</div><div>see #5/Std. C2/04-HMA, Type 220, 4", on</div></div><div><div>228</div><div>see #5/Std. C2/04-HMA, Type 221, 4", on</div></div><div><div>229</div><div>see #5/Std. C2/04-HMA, Type 222, 4", on</div></div><div><div>230</div><div>see #5/Std. C2/04-HMA, Type 223, 4", on</div></div><div><div>231</div><div>see #5/Std. C2/04-HMA, Type 224, 4", on</div></div><div><div>232</div><div>see #5/Std. C2/04-HMA, Type 225, 4", on</div></div><div><div>233</div><div>see #5/Std. C2/04-HMA, Type 226, 4", on</div></div><div><div>234</div><div>see #5/Std. C2/04-HMA, Type 227, 4", on</div></div><div><div>235</div><div>see #5/Std. C2/04-HMA, Type 228, 4", on</div></div><div><div>236</div><div>see #5/Std. C2/04-HMA, Type 229, 4", on</div></div><div><div>237</div><div>see #5/Std. C2/04-HMA, Type 230, 4", on</div></div><div><div>238</div><div>see #5/Std. C2/04-HMA, Type 231, 4", on</div></div><div><div>239</div><div>see #5/Std. C2/04-HMA, Type 232, 4", on</div></div><div><div>240</div><div>see #5/Std. C2/04-HMA, Type 233, 4", on</div></div><div><div>241</div><div>see #5/Std. C2/04-HMA, Type 234, 4", on</div></div><div><div>242</div><div>see #5/Std. C2/04-HMA, Type 235, 4", on</div></div><div><div>243</div><div>see #5/Std. C2/04-HMA, Type 236, 4", on</div></div><div><div>244</div><div>see #5/Std. C2/04-HMA, Type 237, 4", on</div></div><div><div>245</div><div>see #5/Std. C2/04-HMA, Type 238, 4", on</div></div><div><div>246</div><div>see #5/Std. C2/04-HMA, Type 239, 4", on</div></div><div><div>247</div><div>see #5/Std. C2/04-HMA, Type 240, 4", on</div></div><div><div>248</div><div>see #5/Std. C2/04-HMA, Type 241, 4", on</div></div><div><div>249</div><div>see #5/Std. C2/04-HMA, Type 242, 4", on</div></div><div><div>250</div><div>see #5/Std. C2/04-HMA, Type 243, 4", on</div></div><div><div>251</div><div>see #5/Std. C2/04-HMA, Type 244, 4", on</div></div><div><div>252</div><div>see #5/Std. C2/04-HMA, Type 245, 4", on</div></div><div><div>253</div><div>see #5/Std. C2/04-HMA, Type 246, 4", on</div></div><div><div>254</div><div>see #5/Std. C2/04-HMA, Type 247, 4", on</div></div><div><div>255</div><div>see #5/Std. C2/04-HMA, Type 248, 4", on</div></div><div><div>256</div><div>see #5/Std. C2/04-HMA, Type 249, 4", on</div></div><div><div>257</div><div>see #5/Std. C2/04-HMA, Type 250, 4", on</div></div><div><div>258</div><div>see #5/Std. C2/04-HMA, Type 251, 4", on</div></div><div><div>259</div><div>see #5/Std. C2/04-HMA, Type 252, 4", on</div></div><div><div>260</div><div>see #5/Std. 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C2/04-HMA, Type 266, 4", on</div></div><div><div>274</div><div>see #5/Std. C2/04-HMA, Type 267, 4", on</div></div><div><div>275</div><div>see #5/Std. C2/04-HMA, Type 268, 4", on</div></div><div><div>276</div><div>see #5/Std. C2/04-HMA, Type 269, 4", on</div></div><div><div>277</div><div>see #5/Std. C2/04-HMA, Type 270, 4", on</div></div><div><div>278</div><div>see #5/Std. C2/04-HMA, Type 271, 4", on</div></div><div><div>279</div><div>see #5/Std. C2/04-HMA, Type 272, 4", on</div></div><div><div>280</div><div>see #5/Std. C2/04-HMA, Type 273, 4", on</div></div><div><div>281</div><div>see #5/Std. C2/04-HMA, Type 274, 4", on</div></div><div><div>282</div><div>see #5/Std. C2/04-HMA, Type 275, 4", on</div></div><div><div>283</div><div>see #5/Std. C2/04-HMA, Type 276, 4", on</div></div><div><div>284</div><div>see #5/Std. C2/04-HMA, Type 277, 4", on</div></div><div><div>285</div><div>see #5/Std. C2/04-HMA, Type 278, 4", on</div></div><div><div>286</div><div>see #5/Std. C2/04-HMA, Type 279, 4", on</div></div><div><div>287</div><div>see #5/Std. C2/04-HMA, Type 280, 4", on</div></div><div><div>288</div><div>see #5/Std. C2/04-HMA, Type 281, 4", on</div></div><div><div>289</div><div>see #5/Std. C2/04-HMA, Type 282, 4", on</div></div><div><div>290</div><div>see #5/Std. C2/04-HMA, Type 283, 4", on</div></div><div><div>291</div><div>see #5/Std. C2/04-HMA, Type 284, 4", on</div></div><div><div>292</div><div>see #5/Std. C2/04-HMA, Type 285, 4", on</div></div><div><div>293</div><div>see #5/Std. C2/04-HMA, Type 286, 4", on</div></div><div><div>294</div><div>see #5/Std. C2/04-HMA, Type 287, 4", on</div></div><div><div>295</div><div>see #5/Std. C2/04-HMA, Type 288, 4", on</div></div><div><div>296</div><div>see #5/Std. C2/04-HMA, Type 289, 4", on</div></div><div><div>297</div><div>see #5/Std. C2/04-HMA, Type 290, 4", on</div></div><div><div>298</div><div>see #5/Std. C2/04-HMA, Type 291, 4", on</div></div><div><div>299</div><div>see #5/Std. C2/04-HMA, Type 292, 4", on</div></div><div><div>300</div><div>see #5/Std. C2/04-HMA, Type 293, 4", on</div></div><div><div>301</div><div>see #5/Std. C2/04-HMA, Type 294, 4", on</div></div><div><div>302</div><div>see #5/Std. C2/04-HMA, Type 295, 4", on</div></div><div><div>303</div><div>see #5/Std. C2/04-HMA, Type 296, 4", on</div></div><div><div>304</div><div>see #5/Std. C2/04-HMA, Type 297, 4", on</div></div><div><div>305</div><div>see #5/Std. C2/04-HMA, Type 298, 4", on</div></div><div><div>306</div><div>see #5/Std. C2/04-HMA, Type 299, 4", on</div></div><div><div>307</div><div>see #5/Std. C2/04-HMA, Type 300, 4", on</div></div><div></div></div>		
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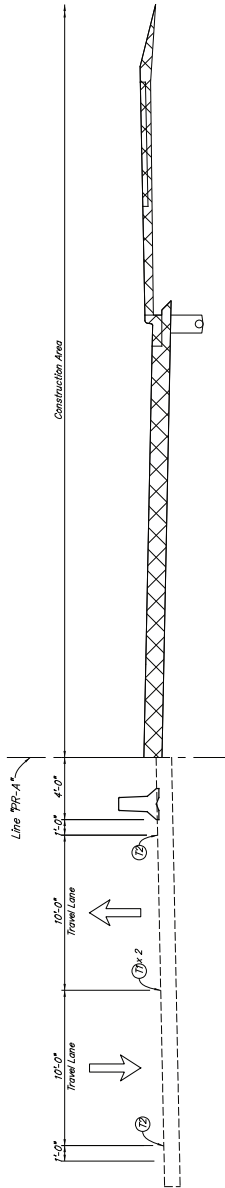


<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGNED: ASU DRAWN: CAK CHECKED: SMC</p>				<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>TYPICAL CROSS SECTIONS</p> <p>LINE "PR-A"</p>				<p>HORIZONTAL SCALE</p>	<p>BRIDGE FILE</p>
<p>DATE</p>				<p>DESIGNATION NO.</p>				<p>VERTICAL SCALE</p>	<p>105151</p>
<p>DESIGNED: SMC CHECKED: SMC</p>				<p>SURVEY BOOK</p>				<p>PROJECT NO.</p>	<p>105151</p>
<p>UNDERDRAW</p>				<p>CONTRACT</p>				<p>SHEETS</p>	<p>10 of 89</p>

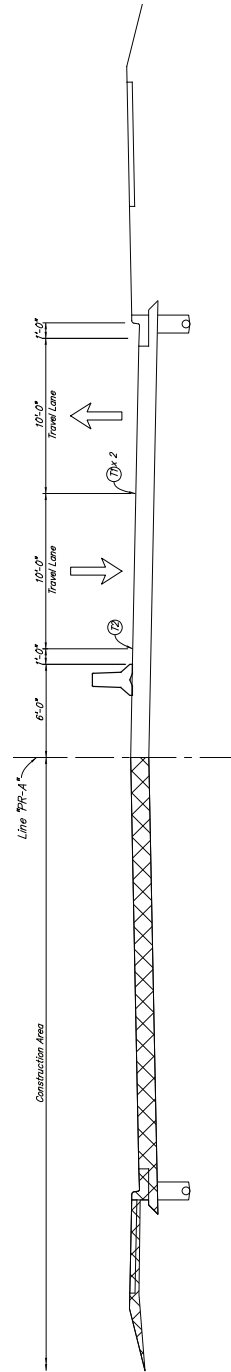
- LEGEND**
- ① 165 #5yd. HMA Surface, Type "1", on 275 #5yd. HMA Intermediate, Type "1", on 6" Compacted Aggregate, No. 53, Base
 - ② Concrete Center Curb, Type "2"
 - ③ Siding, Nursery
 - ④ Concrete Curb
 - ⑤ Matched Seeding, Type "1"
 - ⑥ Combined Concrete Curb and Gutter, Modified
 - ⑦ Concrete Sidewalk, #4
 - ⑧ Concrete Curb and Gutter, Modified
 - ⑨ Retaining Wall
 - ⑩ Underdraw



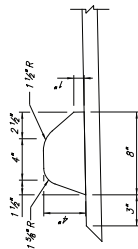
PHASE I



PHASE I



PHASE II



ASPHALT CURB (MOUNTABLE) DETAIL

INDIANA DEPARTMENT OF TRANSPORTATION MAINTENANCE OF TRAFFIC TYPICAL CROSS SECTIONS				RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE		HORIZONTAL SCALE	BRIDGE FILE
										VERTICAL SCALE	DESIGNATION NO.
										SURVEY BOOK	1005151
										CONTRACT	SHEETS
											12 of 89
											PROJECT NO.
											1005151

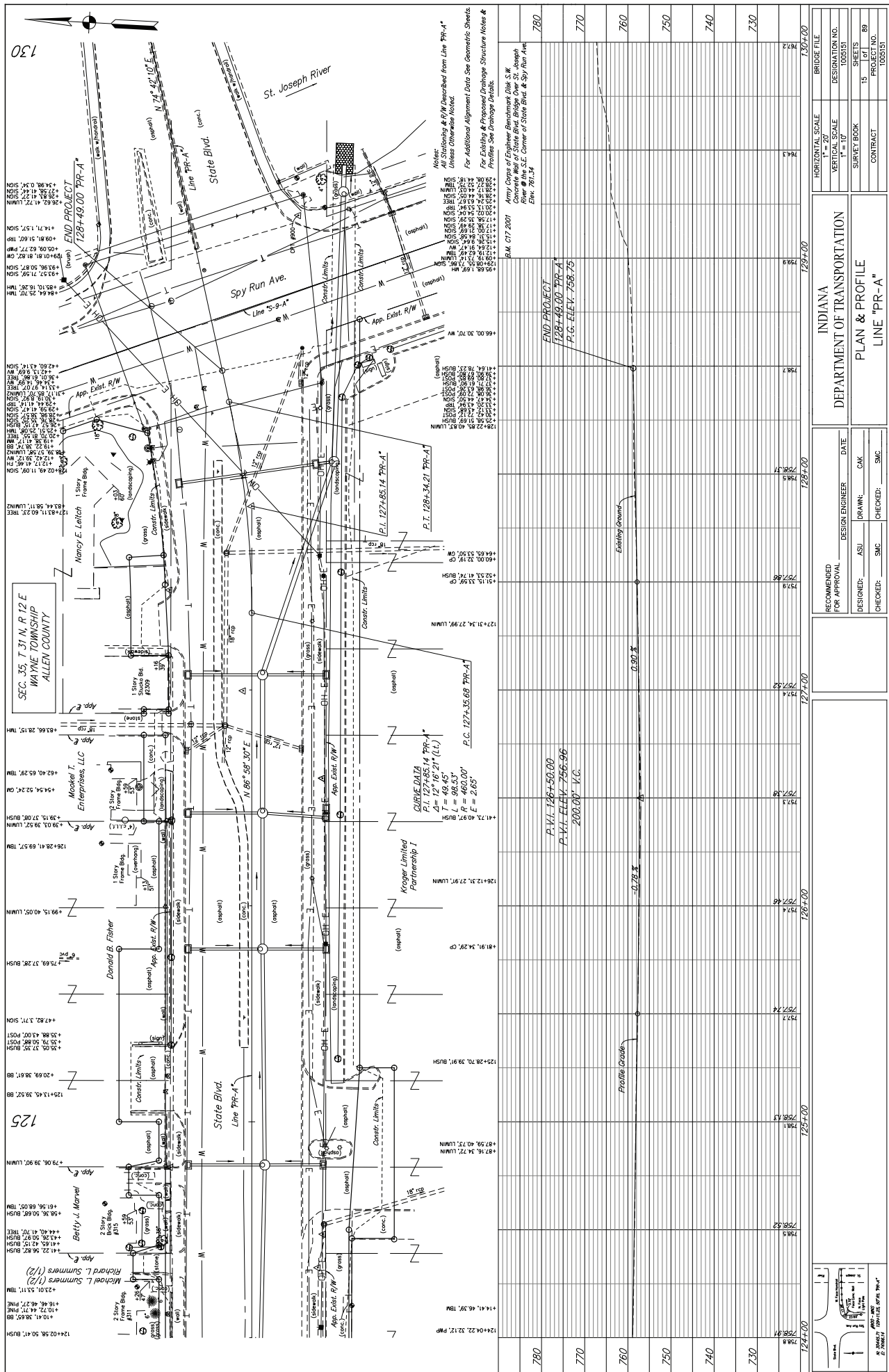
Notes: NMM Surface to be Placed Following Phase II Under Live Traffic

LEGEND		Direction of Traffic
①	Temporary Pavement Marking, Solid, Yellow, 4 in.	Standard Drum
②	Temporary Pavement Marking, Solid, White, 4 in.	Temporary Concrete Barrier
③	Construction Area	

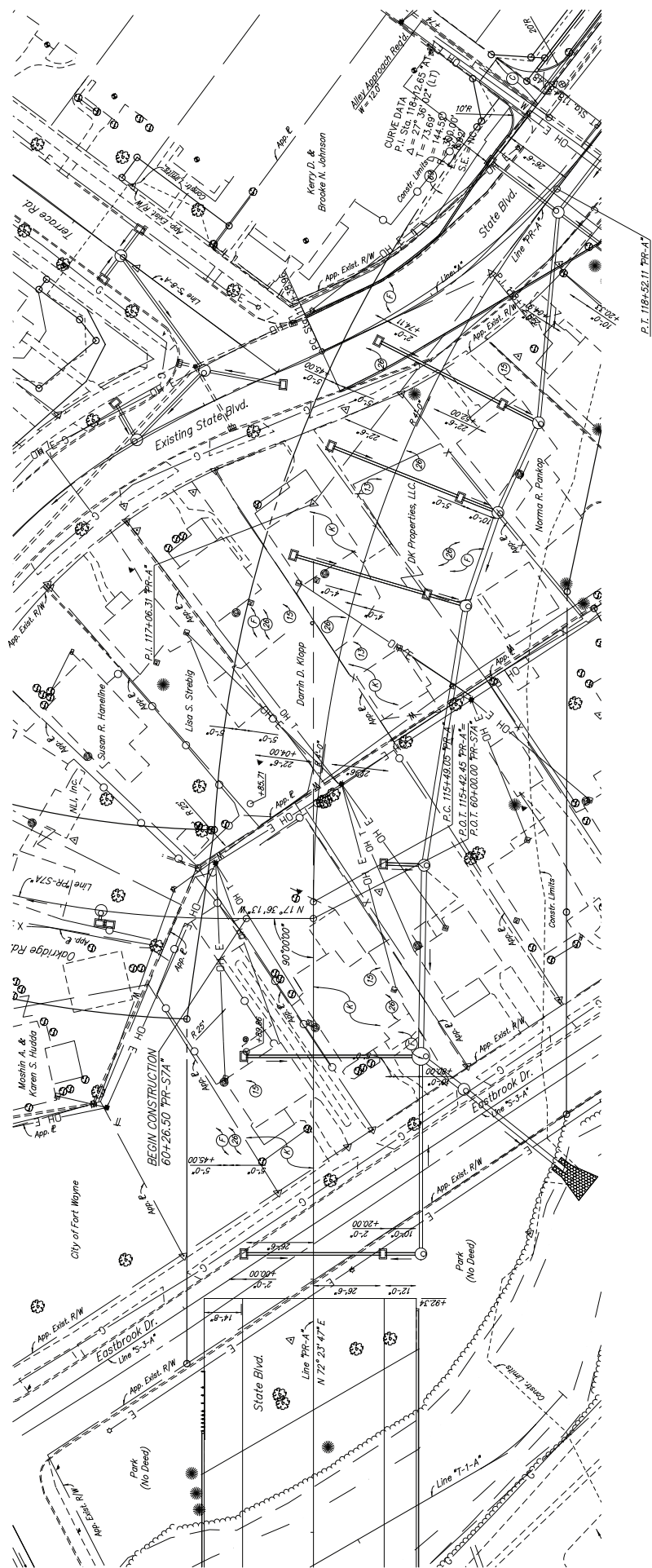
7/12/2011 12:44:48 PM P:\N0507\1004\RD.P1\T1.TXD.dgn







CURVE DATA
P.I. 117+06.31 "PR-A"
 $\Delta = 37^\circ 44' 51"$ (Rt.)
T = 157.26'
L = 303.06'
R = 460.00'
E = 26.14'



Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

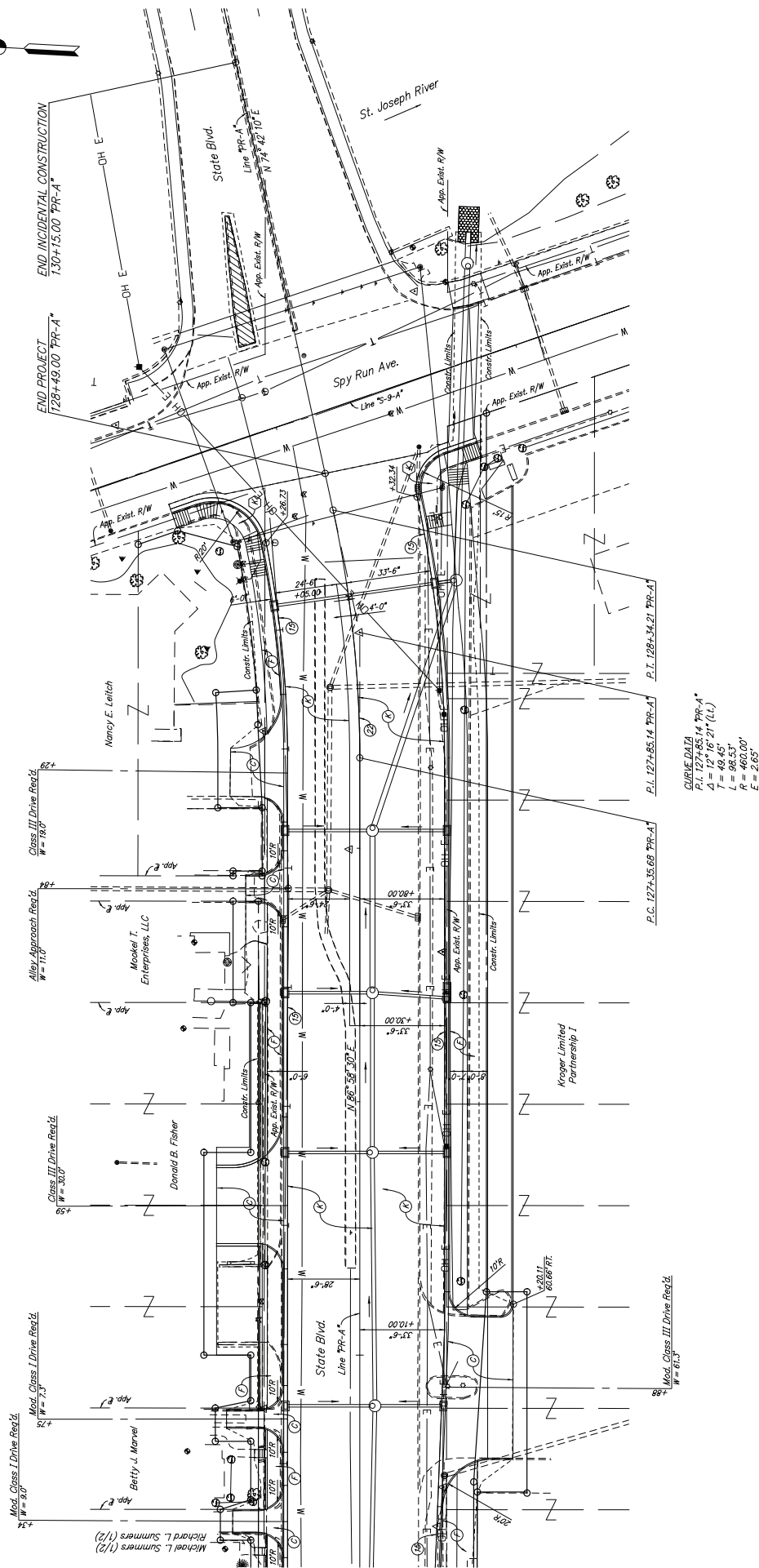
<div>LEGEND</div> <div><div>⑦ 105 #5yd. HMA Surface, Type 1* on 275 #5yd. HMA Intermediate, Type 1*, on 6" Compacted Aggregate for 53, Base</div><div>⑧ xxx #5yd. HMA for Approaches Type 1*, on 6" Compacted Aggregate for 53, Base Type 1*, No. xx</div><div>⑨ Concrete Slabwalk, 1"</div><div>⑩ x" Cement Concrete Pavement for Driveways</div><div>⑪ Full Depth Pavement (See Typical Sections)</div><div>⑫ Concrete Center Curb, Type 2*</div><div>⑬ Concrete Curb</div><div>⑭ Combined Concrete Curb and Outer, Modified</div><div>⑮ Retaining Wall</div><div>⑯ Mulched Seeding, Type 2*</div><div>⑰ Sodding, Nursery</div><div>⑱ Center Curb Removal Limits</div></div>		<div>INDIANA</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>CONSTRUCTION DETAILS</div> <div>LINE "PR-A"</div>		<div>RECOMMENDED FOR APPROVAL</div> <div>DESIGN ENGINEER</div> <div>DATE</div>	<div>HORIZONTAL SCALE</div> <div>1" = 20'</div> <div>VERTICAL SCALE</div> <div>1005151</div>	<div>BRIDGE FILE</div>
<div>DESIGNED: ASU</div> <div>CHECKED: SMC</div>		<div>DRAWN: CAK</div> <div>CHECKED: SMC</div>	<div>SURVEY BOOK</div> <div>18</div> <div>PROJECT NO.</div> <div>105151</div> <div>SHEETS</div> <div>89</div> <div>CONTRACT</div> <div>1005151</div>			

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125

130

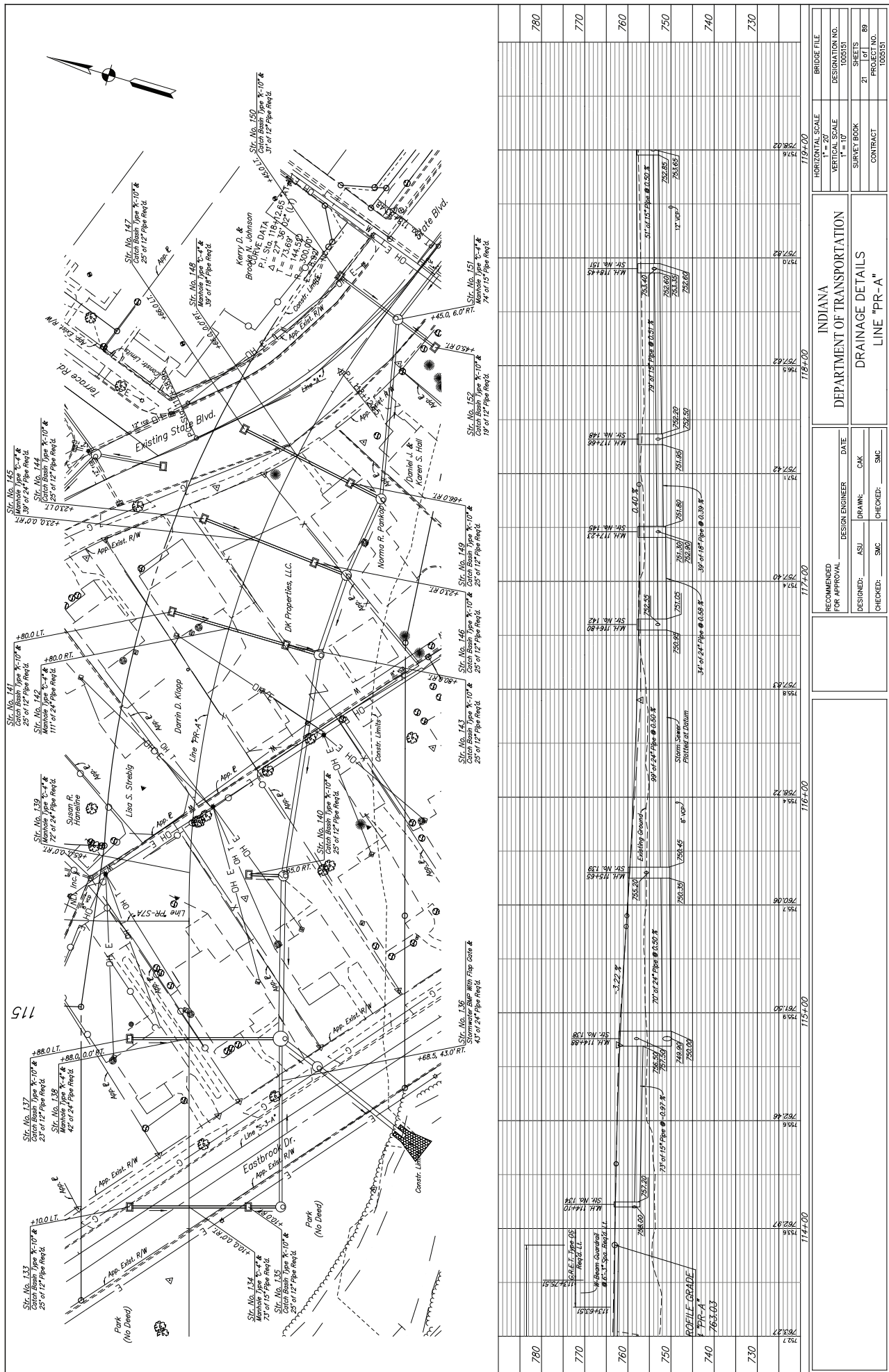
SEC. 35, T. 31 N. R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



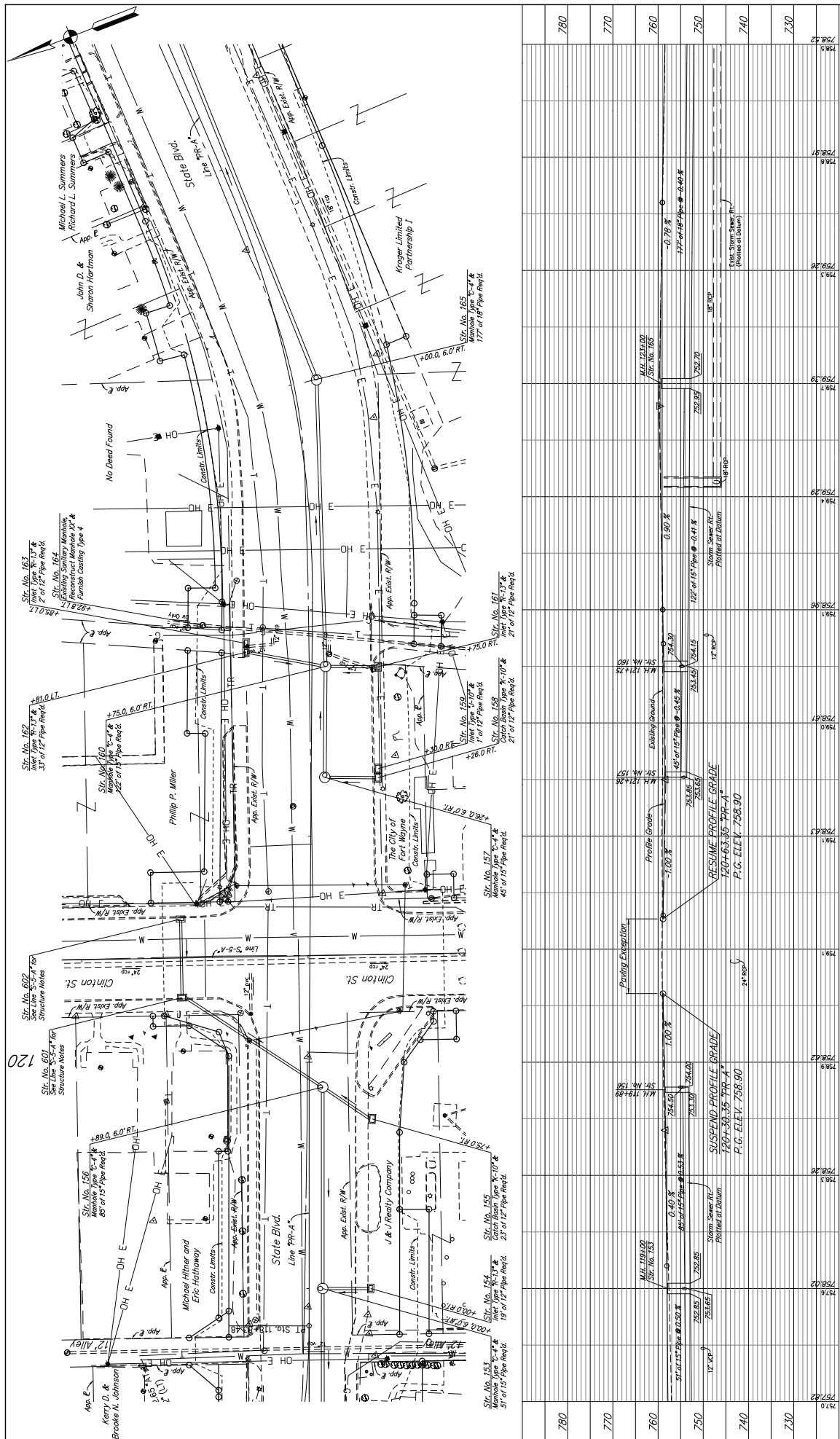
CURVE DATA
P.I. 127+85.14 "PR-A"
 $\Delta = 12^\circ 16' 21''$ (lt.)
 $L = 68.53'$
 $R = 460.00'$
 $E = 2.65'$

Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

		RECOMMENDED FOR APPROVAL		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 20'		BRIDGE FILE	
		DESIGN ENGINEER		DATE		VERTICAL SCALE		DESIGNATION NO.	
		ASU		CAK		SURVEY BOOK		SHEETS	
		SMC		SMC		CONTRACT		PROJECT NO.	
								1005151	

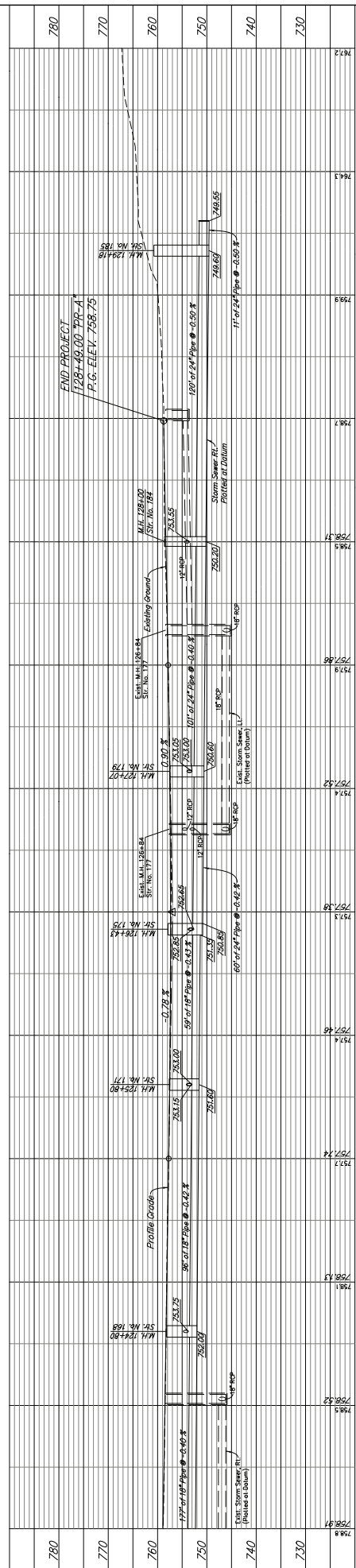
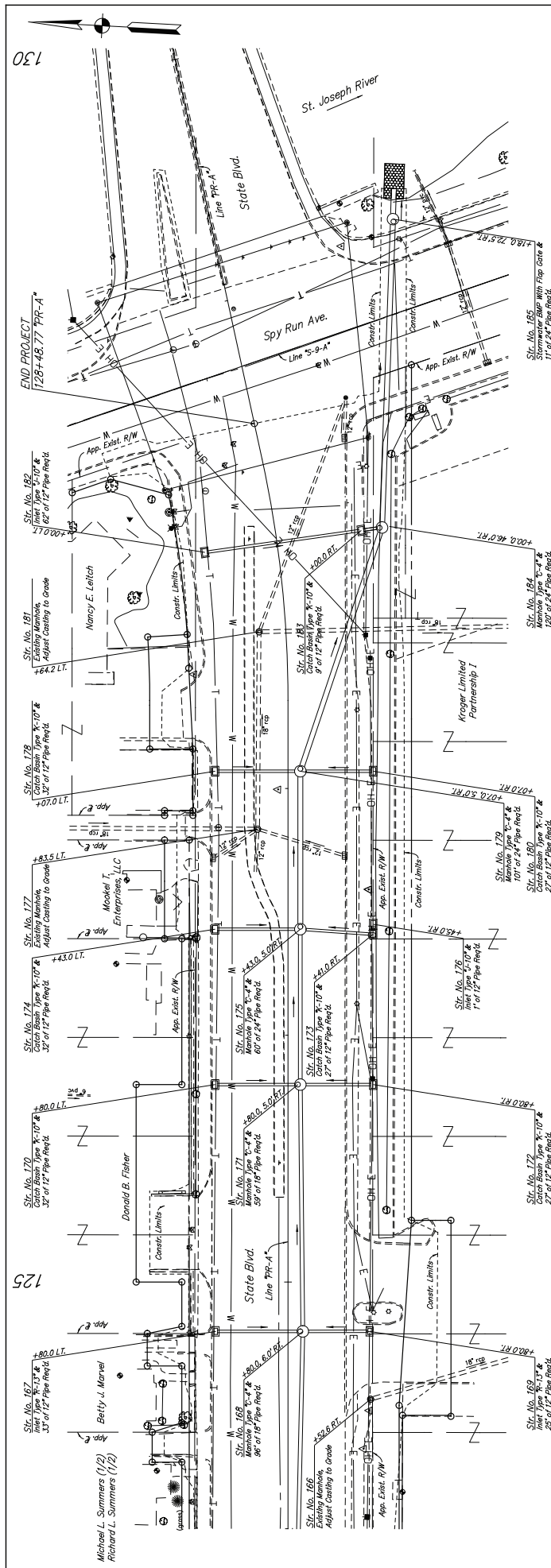


INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
RECOMMENDED FOR APPROVAL				DESIGN ENGINEER				SURVEY BOOK			
DATE				DRAWN				PROJECT NO.			
DESIGNED				CHECKED				SHEETS			
ASU				SMC				21 of 89			
SMC				SMC				CONTRACT			
1005151				1005151				1005151			



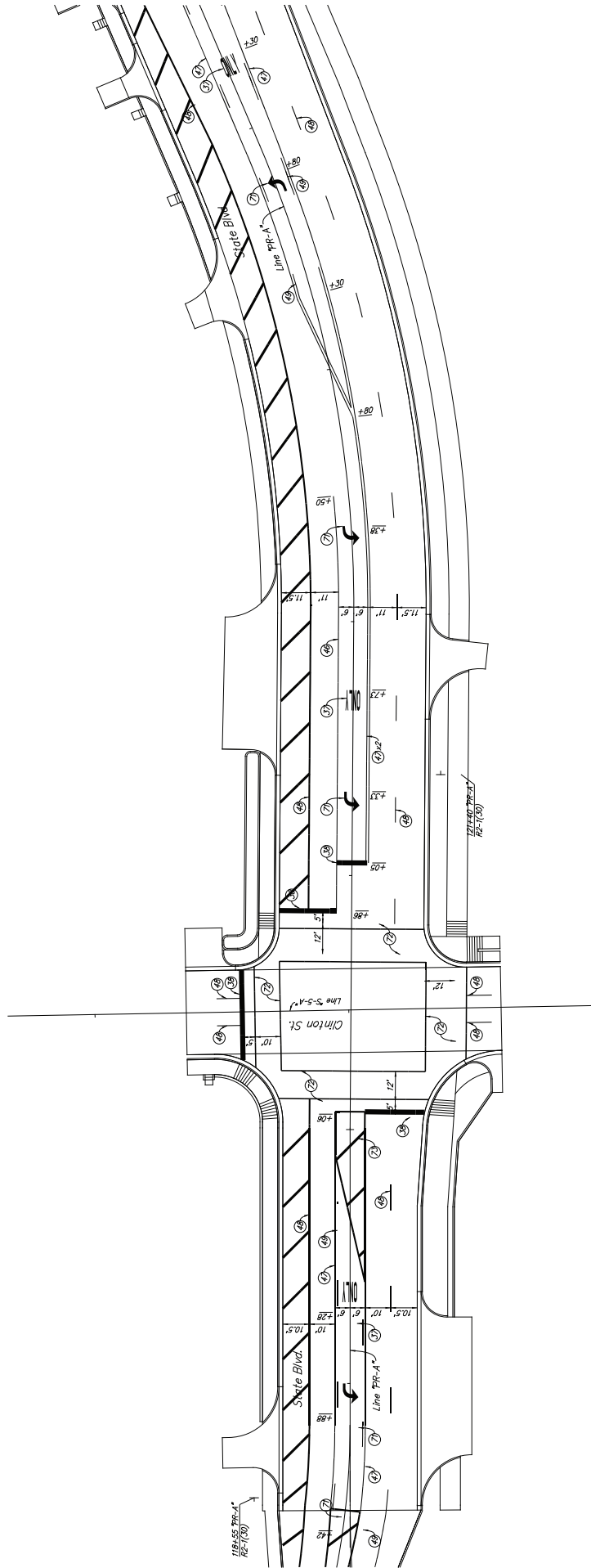
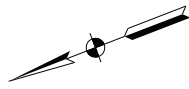
INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
BRIDGE FILE				DESIGN ENGINEER				DRAWN			
HORIZONTAL SCALE				DATE				CAK			
VERTICAL SCALE				ASU				SNC			
DESIGNATION NO.				CHECKED				SNC			
1005151				SNC				SNC			
SHEETS				PROJECT NO.				1005151			
22				CONTRACT				1005151			
of				LINE "PR-A"							

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RECOMMENDED FOR APPROVAL				INDIANA				DEPARTMENT OF TRANSPORTATION				DRAINAGE DETAILS			
DESIGNED:	ASU	CHECKED:	SMC	DESIGN ENGINEER	DATE	DRAWN:	CAK	HORIZONTAL SCALE	VERTICAL SCALE	BRIDGE FILE	DESIGNATION NO.	SURVEY BOOK	SHEETS	PROJECT NO.	CONTRACT
								1" = 10'	1" = 10'		105151		23	89	105151

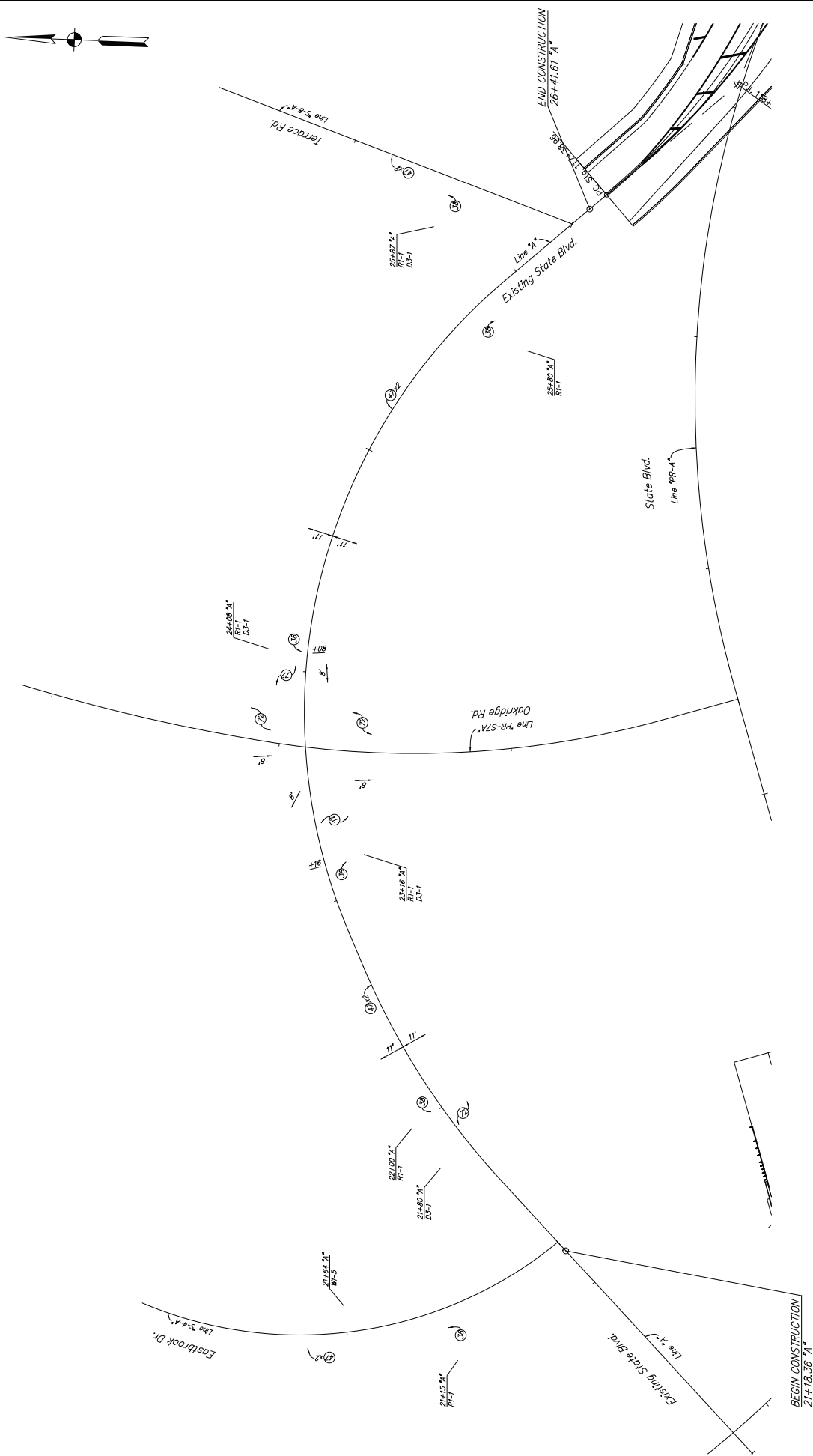
120



INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
PAVEMENT MARKING & SIGNING DETAILS - LINE "PR-A"		VERTICAL SCALE	DESIGNATION NO.
		SURVEY BOOK	SHEETS
		CONTRACT	PROJECT NO.
			10051
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ASU		DRAWN: CAK	
CHECKED: SMC		CHECKED: SMC	
LEGEND		(23) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.	
(17) Pavement Message Markings, Thermoplastic, Word (message)		(7) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)	
(18) Transverse Markings, Thermoplastic, Broken, White, 4 in.		(24) Line, Thermoplastic, Solid, Yellow, 4 in.	
(19) Transverse Markings, Thermoplastic, Stop Line, White, 24 in.		(25) Pavement Message Markings, Thermoplastic, Lane Indication Arrow	
(20) Line, Thermoplastic, Solid, White, 4 in.		(26) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.	
(21) Line, Thermoplastic, Solid, Yellow, 4 in.			
(22) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.			

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INDIANA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING &
SIGNING DETAILS - LINE "A"

RECOMMENDED
FOR APPROVAL

DESIGNED:
CHECKED:

DESIGN ENGINEER
ASU
SMC

DATE
CAK
SMC

HORIZONTAL SCALE
VERTICAL SCALE

SURVEY BOOK
CONTRACT

BRIDGE FILE
DESIGNATION NO.
SHEETS
PROJECT NO.

17 Pavement Message Markings, Thermoplastic, Word (message)

18 Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.

19 Line, Thermoplastic, Solid, White, 4 in.

20 Line, Thermoplastic, Solid, Yellow, 4 in.

21 Transverse Message Markings, Thermoplastic, Lane Indication Arrow

22 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

23 Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.

24 Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)

25 Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)

LEGEND

26 Line, Thermoplastic, Broken, White, 4 in.

27 Line, Thermoplastic, Broken, Yellow, 4 in.

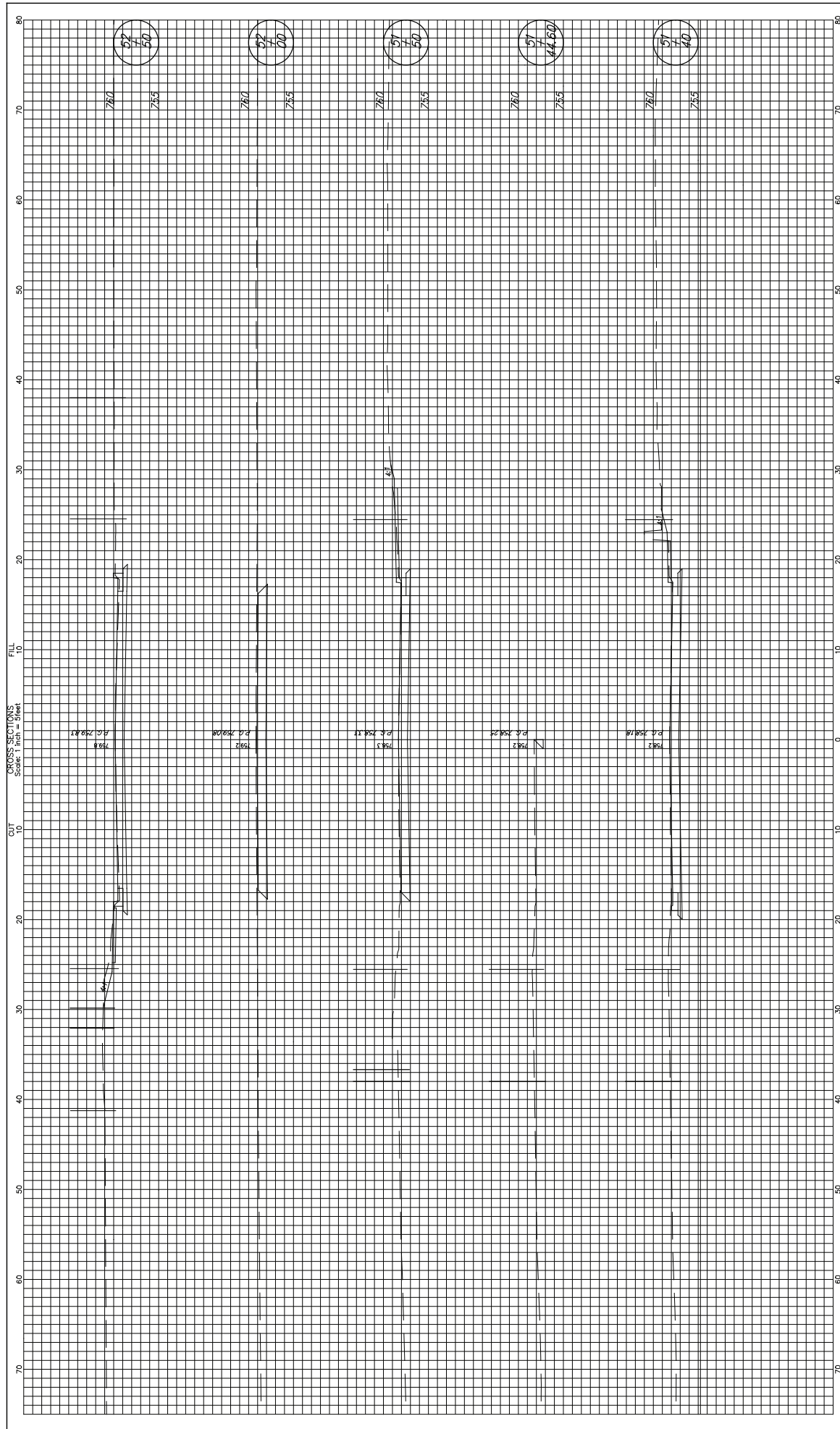
28 Transverse Message Markings, Thermoplastic, Lane Indication Arrow

29 Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

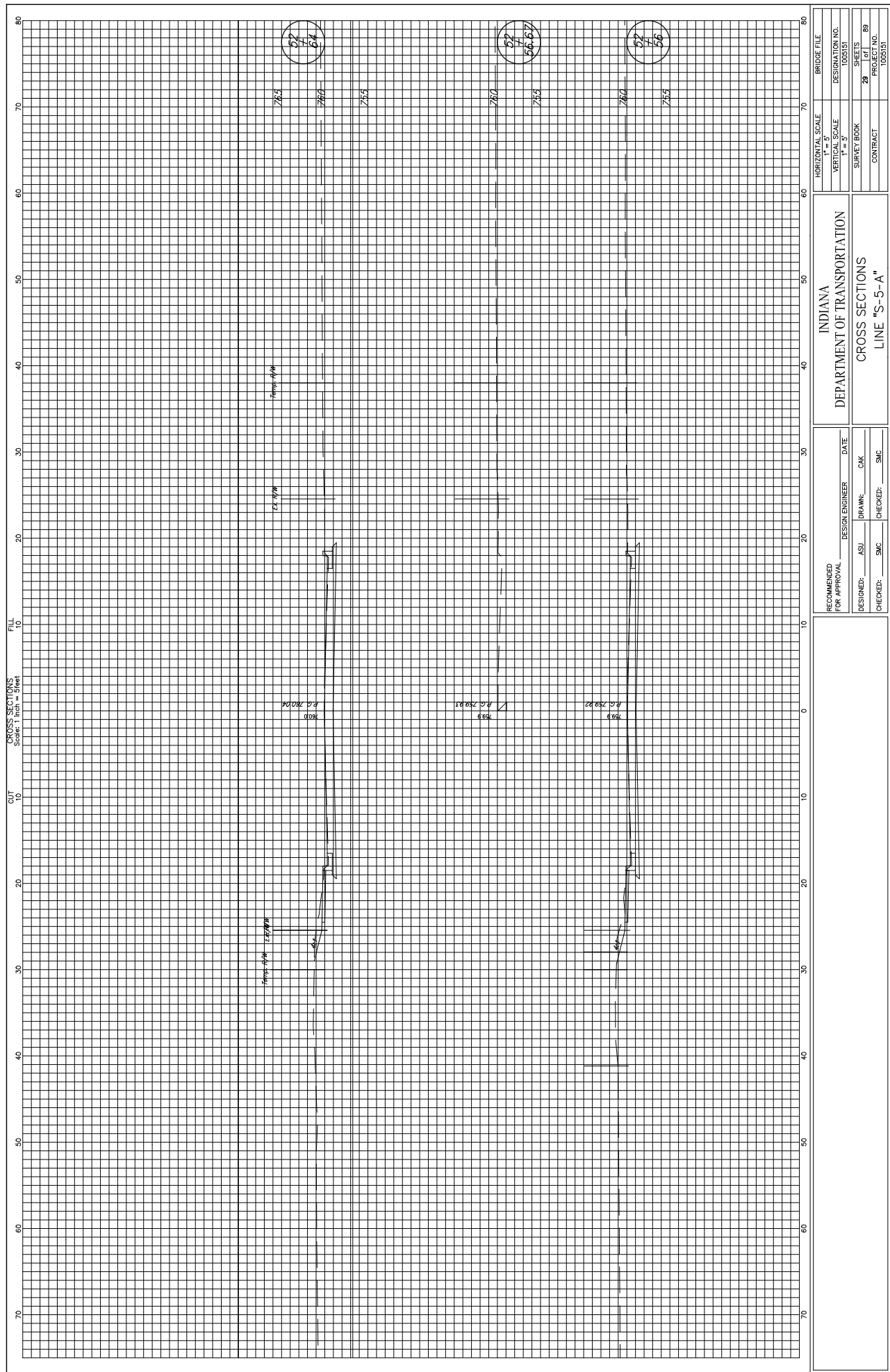
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sortes

Appendix C
Page 62 of 496



INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
CROSS SECTIONS LINE "S-5-A"		DESIGNATION NO. 1005151	
RECOMMENDED FOR APPROVAL		HORIZONTAL SCALE 1" = 5'	
DESIGNED: ASU	DESIGN ENGINEER CAK	VERTICAL SCALE 1" = 5'	
CHECKED: SMC	DRAWN: SMC	SURVEY BOOK CONTRACT	
		SHEETS 28 of 89	
		PROJECT NO. 1005151	



PROJECT	DESIGNATION
1005154	1005154
CONTRACT	BRIDGE FILE

TRAFFIC DATA	WESTBROOK DR. - LINE "PR-S-1-A"
A.A.D.T. (2009)	350
A.A.D.T. (2030)	592
D.I.V.	N/A
DIRECTIONAL DISTRIBUTION	N/A
TRUCKS	N/A
MAINLINE ESAL'S	N/A
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
URBAN/RURAL	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	OAKRIDGE RD. - LINE "PR-S-7-A"
A.A.D.T. (2009)	35
A.A.D.T. (2030)	1545
D.I.V.	N/A
DIRECTIONAL DISTRIBUTION	N/A
TRUCKS	N/A
MAINLINE ESAL'S	N/A
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
URBAN/RURAL	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	EXIST. STATE BLVD. - LINE "A"
A.A.D.T. (2009)	20,650
A.A.D.T. (2030)	Minimal
D.I.V.	N/A
DIRECTIONAL DISTRIBUTION	N/A
TRUCKS	N/A
MAINLINE ESAL'S	N/A
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
URBAN/RURAL	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

TRAFFIC DATA	CASS ST. - LINE "S-6-A"
A.A.D.T. (2009)	Minimal
A.A.D.T. (2030)	Minimal
D.I.V.	N/A
DIRECTIONAL DISTRIBUTION	N/A
TRUCKS	N/A
MAINLINE ESAL'S	N/A
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Local Street
URBAN/RURAL	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

INDIANA DEPARTMENT OF TRANSPORTATION

ROAD PLANS

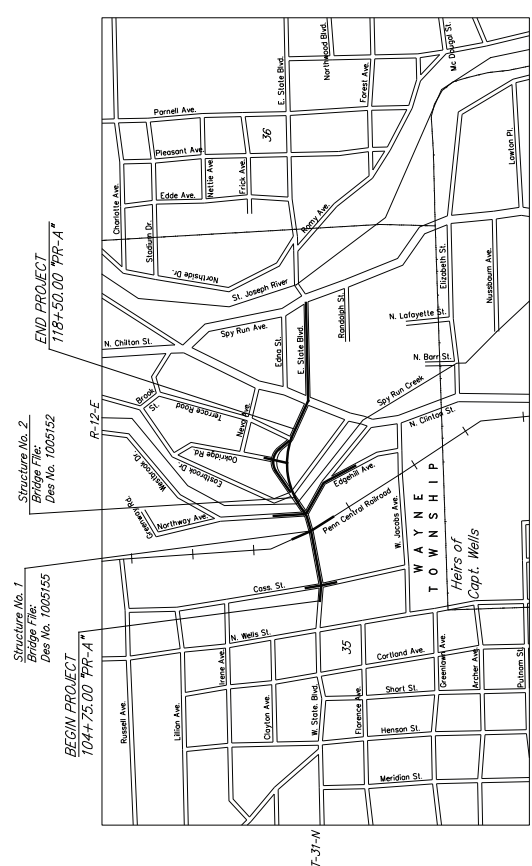
STATE BLVD. RECONSTRUCTION - PART 2

PROJECT NO. 1005154 P.E.

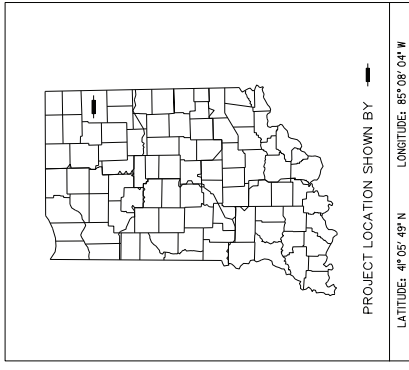
PROJECT NO. R/W

PROJECT NO. CONST.

Project Description: Road Reconstruction on State Boulevard Beginning 202.00 Feet West of the Intersection of State Boulevard and Cass Street, Thence East 1,325.00 Feet Along Line "PR-A" to approximately 1,30.00 Feet East of the Intersection of Terrace Rd. and State Boulevard, All in Section 35, Township 31 North, Range 12 East, Wayne Township, Allen County, Indiana



TRAFFIC DATA	STATE BLVD. - LINE "PR-A"
A.A.D.T. (2009)	20,650
A.A.D.T. (2030)	25,000
D.I.V.	N/A
DIRECTIONAL DISTRIBUTION	N/A
TRUCKS	N/A
MAINLINE ESAL'S	2
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	Reconstruction (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Minor Arterial
URBAN/RURAL	Urban (Built-Up)
TERRAIN	Level
ACCESS CONTROL	None



BRIDGE LENGTH :	0.35	Mile
ROADWAY LENGTH :	0.450	Mile
TOTAL LENGTH :	0.45	Mile
MAX. GRADE :	6.47	%

HYDROLOGIC UNIT CODE: 04100004060060



Scale: 1" = 500'

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2010
TO BE USED WITH THESE PLANS

PLANS PREPARED BY:	American Structurepoint, Inc.	(317) 547-5580	PHONE NUMBER
CERTIFIED BY:			DATE
APPROVED			DATE
FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION		DATE

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	30 of 300
	PROJECT NO.
	1005154

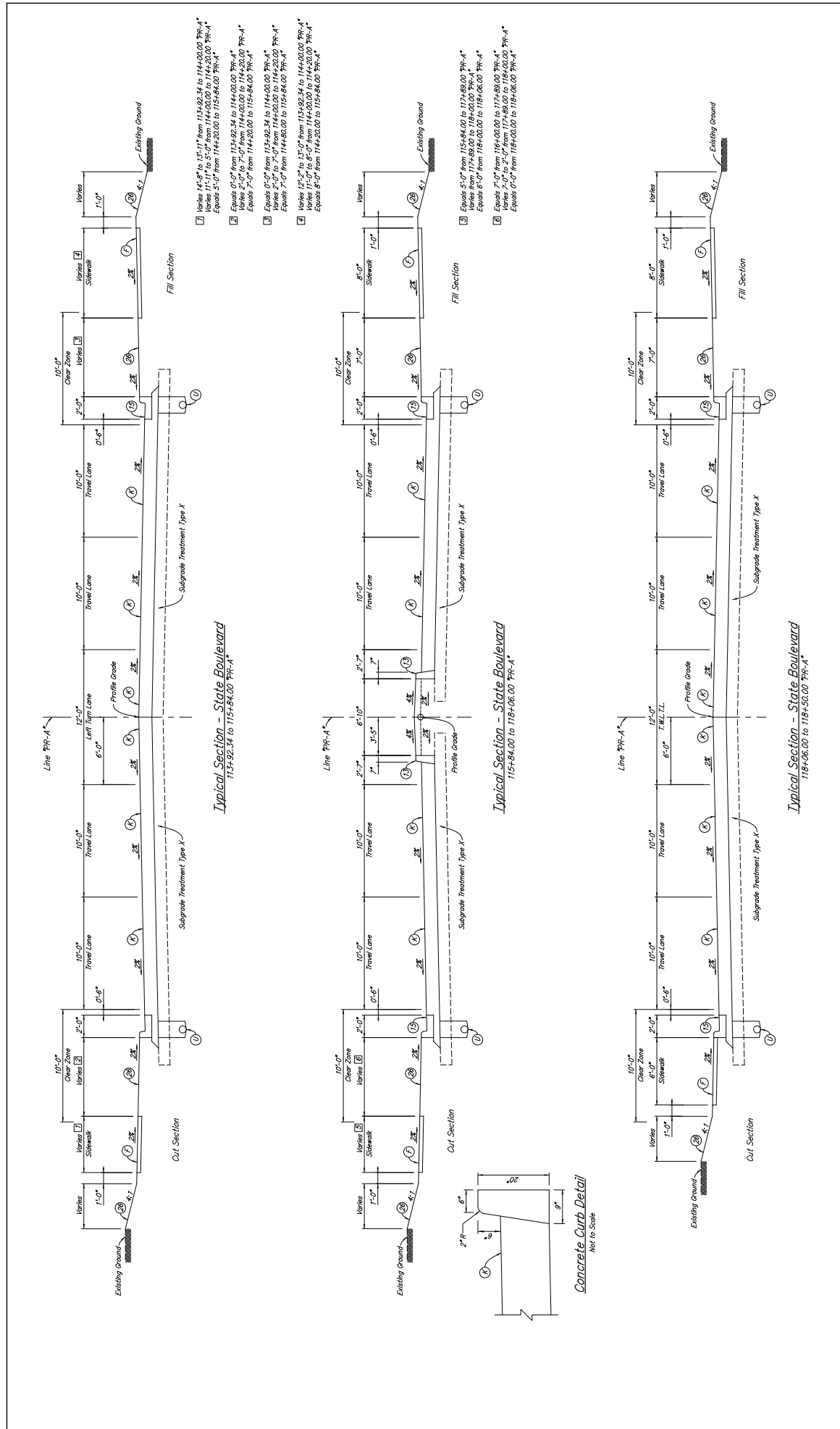


INDEX TO SHEETS

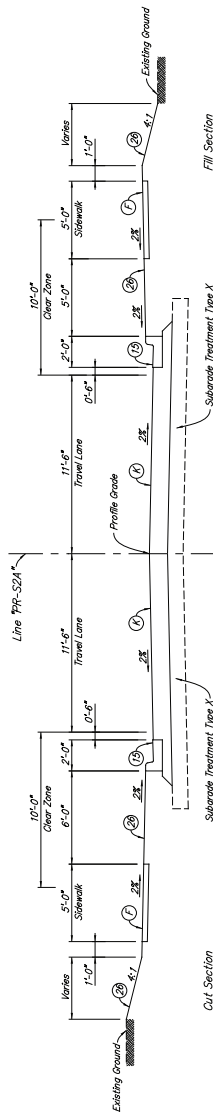
**** REPRESENTS GENERAL NOTES REQUIRED**

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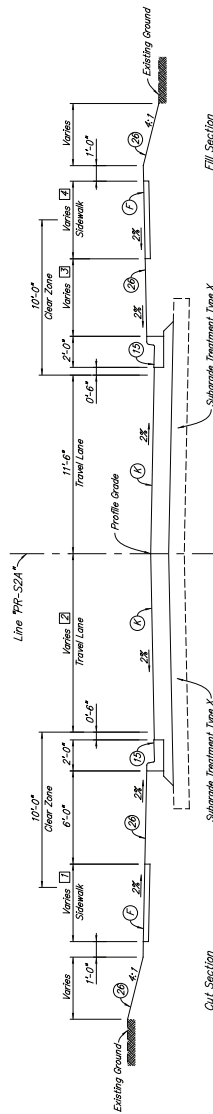
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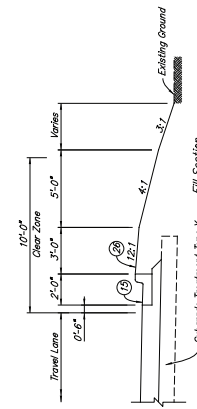
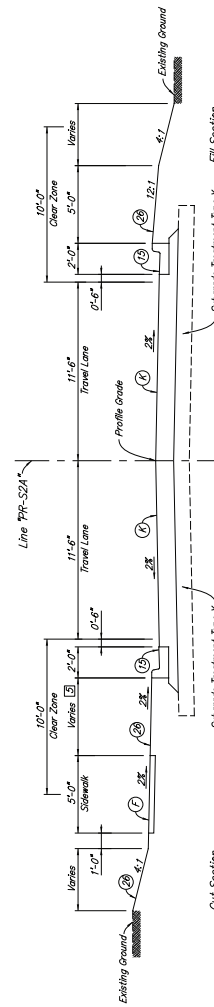
LEGEND		INDIANA DEPARTMENT OF TRANSPORTATION		TYPICAL CROSS SECTIONS LINE "PR-A"	
① 165 #3/4" OC/24"-HMA, x, xx, Surface, xx, on 275 #3/4" OC/24"-HMA, x, xx, Intermediate, xx, on 8" Compacted Aggregate, No. 53, Base		② Concrete Center Curb, Type 2"		RECOMMENDED FOR APPROVAL	
③ Concrete Curb		③ Sodding, Nursery		DESIGN ENGINEER	
④ Combined Concrete Curb and Gutter, Modified		④ Attached Wall		DATE	
⑤ Concrete Sidewalk, 4"		⑤ Concrete Curb		DESIGNED: ASU	
				DRAWN: CMC	
				CHECKED: SMC	
				SURVEY BOOK	
				SHEETS	
				CONTRACT	
				PROJECT NO. 105154	
				BRIDGE FILE	



Incidental Construction
Typical Section - Edgehill Avenue
51+60.00 to 52+12.43 Line "PR-S2A"



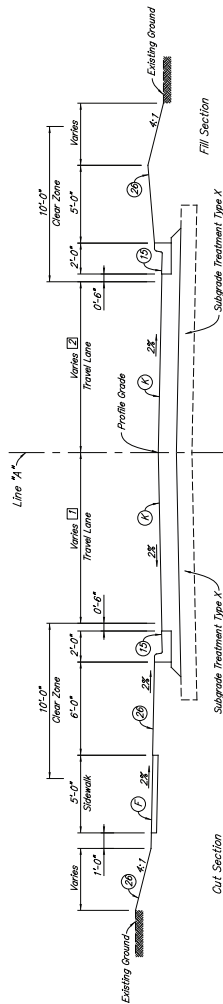
Typical Section - Edgehill Avenue
52+12.43 to 54+66.43 Line "PR-S2A"



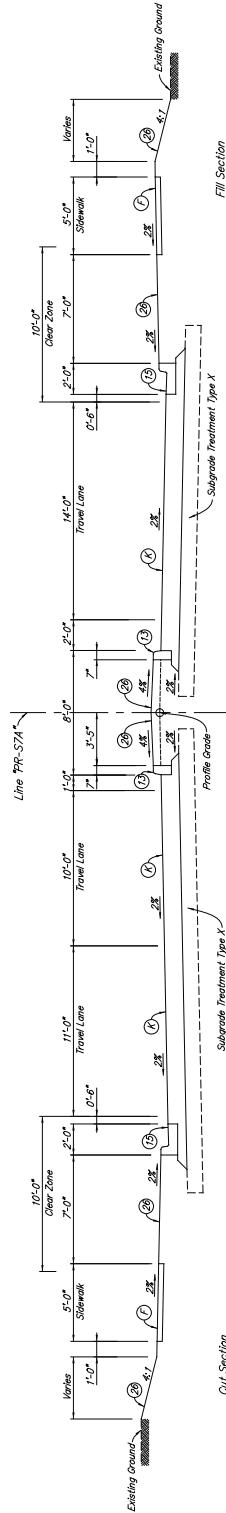
Typical Section - Westbrook Drive
55+27.76 to 58+45.00 Line "PR-S21"

Fill Section Detail
55+27.76 to 56+75.00 Rt. Line "PR-S2A"

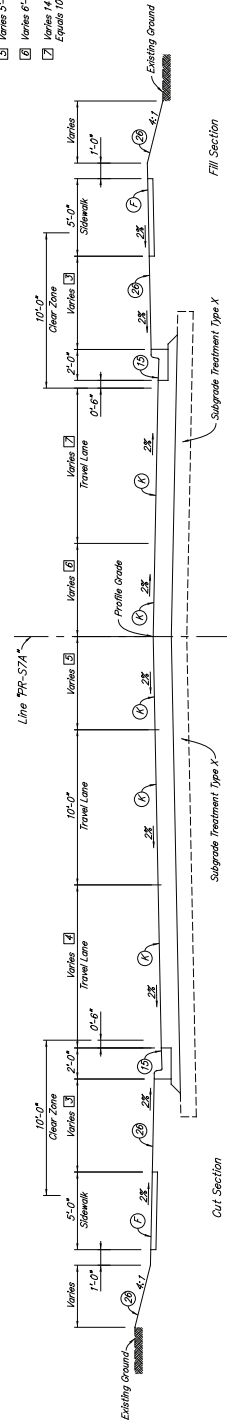
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Typical Section - Existing State Blvd.
21+18.36 to 23+50.05 Line "A"
23+85.48 to 26+41.61 Line "A"

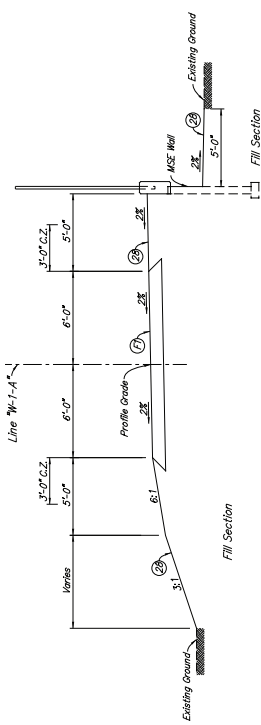


Typical Section - Oakridge Road
60+26.50 to 61+44.00 Line "PR-S7A"

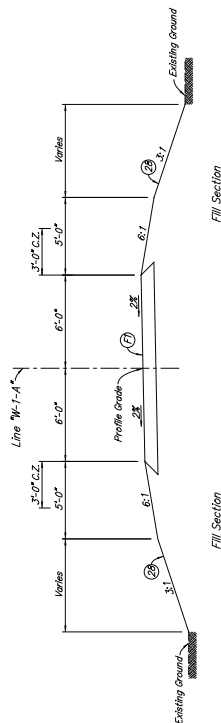


Typical Section - Oakridge Road
61+44.00 to 63+45.00 Line "PR-S7A"

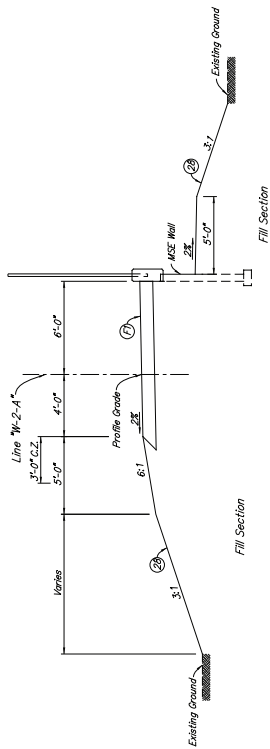
<p> LEGEND (4) xxx #/Syd. 02/04-HMA, x, xx, Surface, xx, on xxx #/Syd. 02/04-HMA, x, xx, Intermediate, xx, on xxx #/Syd. 02/04-HMA, x, xx, Base, xx, on xxx #/Syd. 02/04-HMA, x, xx, Base, xx, on xxx #/Syd. 02/04-HMA, x, xx, Base, xx, on xxx #/Syd. 02/04-HMA, x, xx, Base, xx, on (7) Concrete Slabwork, #* </p>		<p> (24) Concrete Center Curb, Type 2* (25) 16S #/Syd. HMA Surface, Type 1* on (26) 27S #/Syd. HMA Intermediate, Type 1* on (27) 6" Compacted Aggregate, No. 53, Base (28) Concrete Curb (29) Mulched Seeding, Type 2* (30) Retaining Wall </p>		<p> (24) Underdrain (25) Sodding, Nursery (26) Mulched Seeding, Type 2* (27) Retaining Wall </p>		<p> INDIANA DEPARTMENT OF TRANSPORTATION TYPICAL CROSS SECTIONS LINE "A" & "PR-57A" </p>		<p> RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____ DESIGNED: ASU DRAWN: CAC CHECKED: SMC CHECKED: SMC </p>		<p> HORIZONTAL SCALE _____ VERTICAL SCALE 1:100 SURVEY BOOK 36 of 36 CONTRACT _____ PROJECT NO. 100514 BRIDGE FILE _____ </p>	
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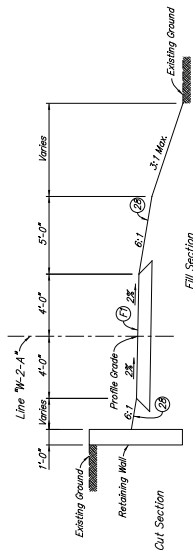
Typical Section Pufferbelly Trail
4+00.00 to 7+02.84 "W-1-A"
7+98.16 to 10+75.00 "W-1-A"



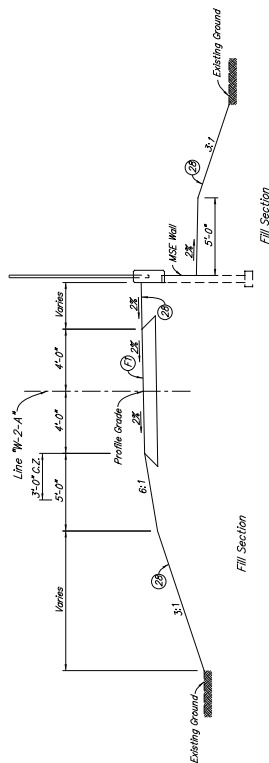
Typical Section Pufferbelly Trail
1+15.00 to 4+00.00 "W-1-A"
10+75.00 to 11+55.00 "W-1-A"



Typical Section Sidewalk Ramp
11+25.00 to 12+15.29 "W-2-A"



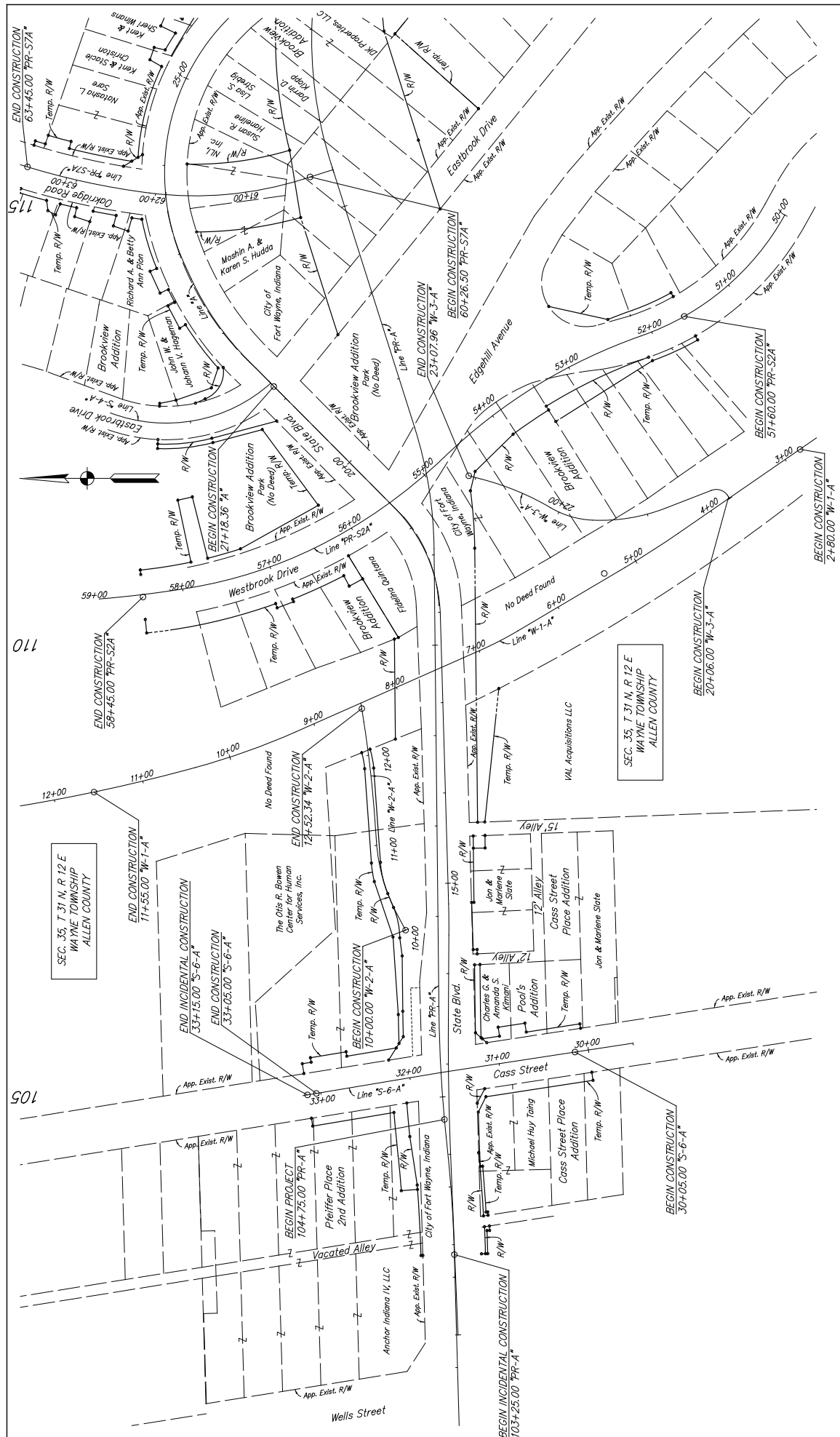
Typical Section Sidewalk Ramp
10+00.00 to 11+25.00 "W-2-A"



Typical Section Sidewalk Ramp
20+00.00 to 23+01.96 "W-3-A"

RECOMMENDED FOR APPROVAL DESIGN ENGINEER: _____ DATE: _____ DESIGNED: ASU DRAWN: CAK CHECKED: SMC		INDIANA DEPARTMENT OF TRANSPORTATION TYPICAL CROSS SECTIONS LINE "W-1-A" & "W-2-A" & "W-3-A"		HORIZONTAL SCALE	BRIDGE FILE
				VERTICAL SCALE	DESIGNATION NO.
				SURVEY BOOK	SHEETS
				CONTRACT	PROJECT NO.
					100514

- LEGEND**
- ① 165 #3/4" HMA Surface, Type "1", on 275 #3/4" HMA Intermediate, Type "1", on 8" Compacted Aggregate, No. 53, Base
 - ② 165 #3/4" HMA Surface, Type "1", on 275 #3/4" HMA Intermediate, Type "1", on 8" Compacted Aggregate, No. 53, Base
 - ③ Concrete Curb
 - ④ Concrete Curb and Gutter, Modified
 - ⑤ Concrete Sidewalk, 4"
 - ⑥ Concrete Center Curb, Type "1"
 - ⑦ Siding, Nursery
 - ⑧ Mulched Seeding, Type "1"
 - ⑨ Retaining Wall

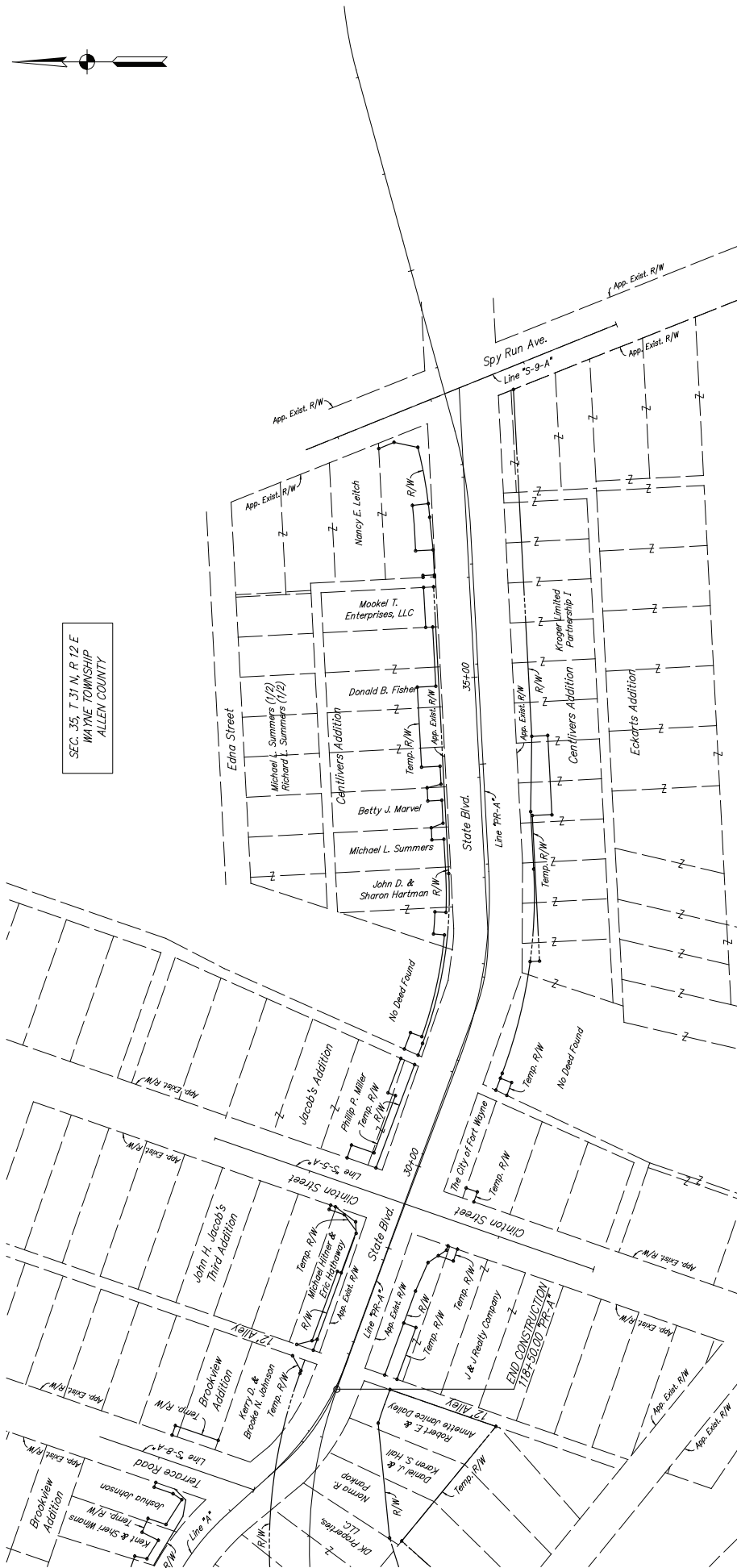


		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
		DESIGNED:	ASU	DRAWN:	PJT
		CHECKED:	SMC	CHECKED:	SMC
		INDIANA DEPARTMENT OF TRANSPORTATION			
		PLAT NO. 1			
		HORIZONTAL SCALE 1" = 50'			
		VERTICAL SCALE 100:1			
		BRIDGE FILE			
		DESIGNATION NO.			
		SURVEY BOOK			
SHEETS					
36 OF 89					
PROJECT NO.					
105151					
CONTRACT					

130

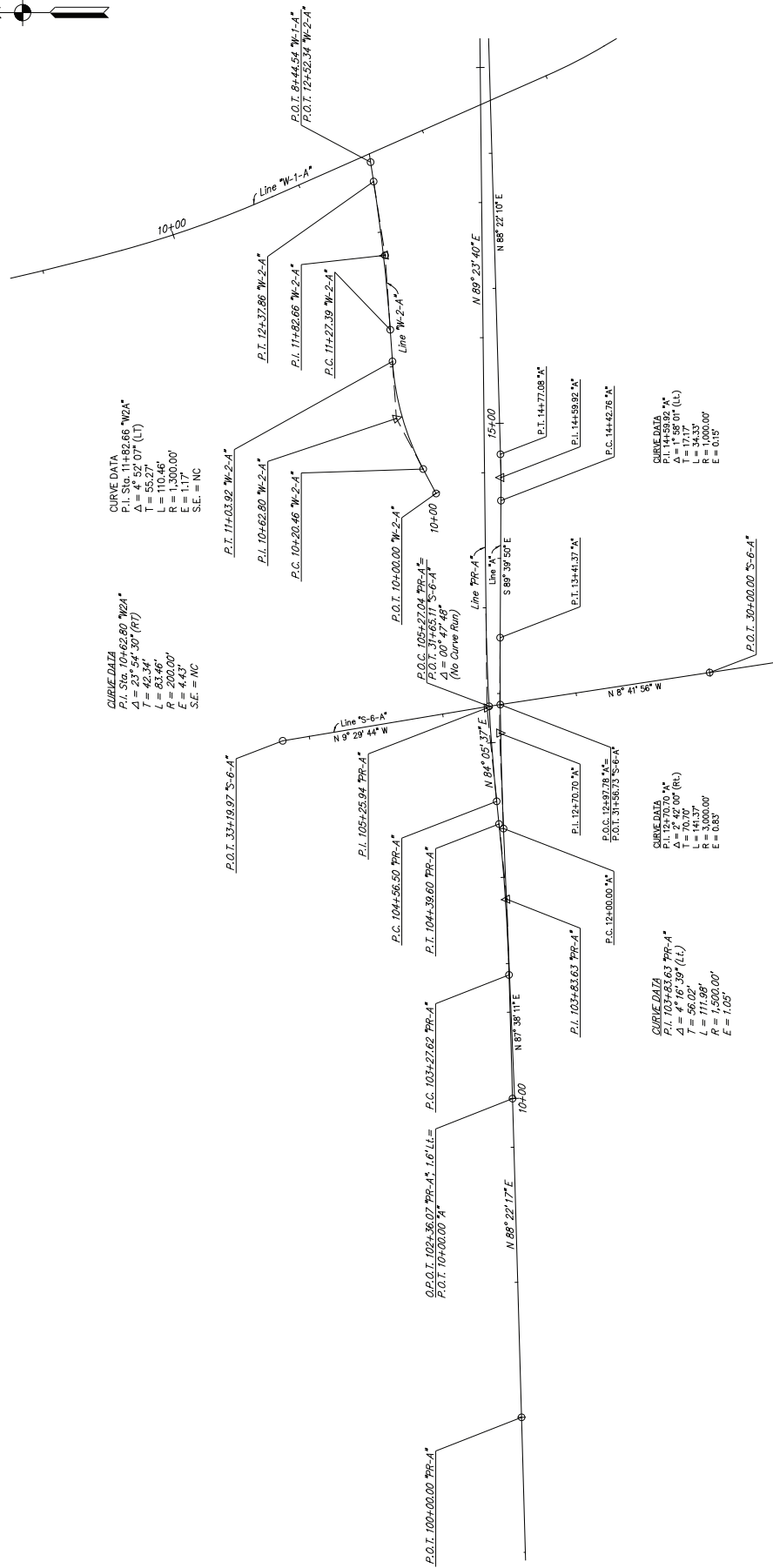
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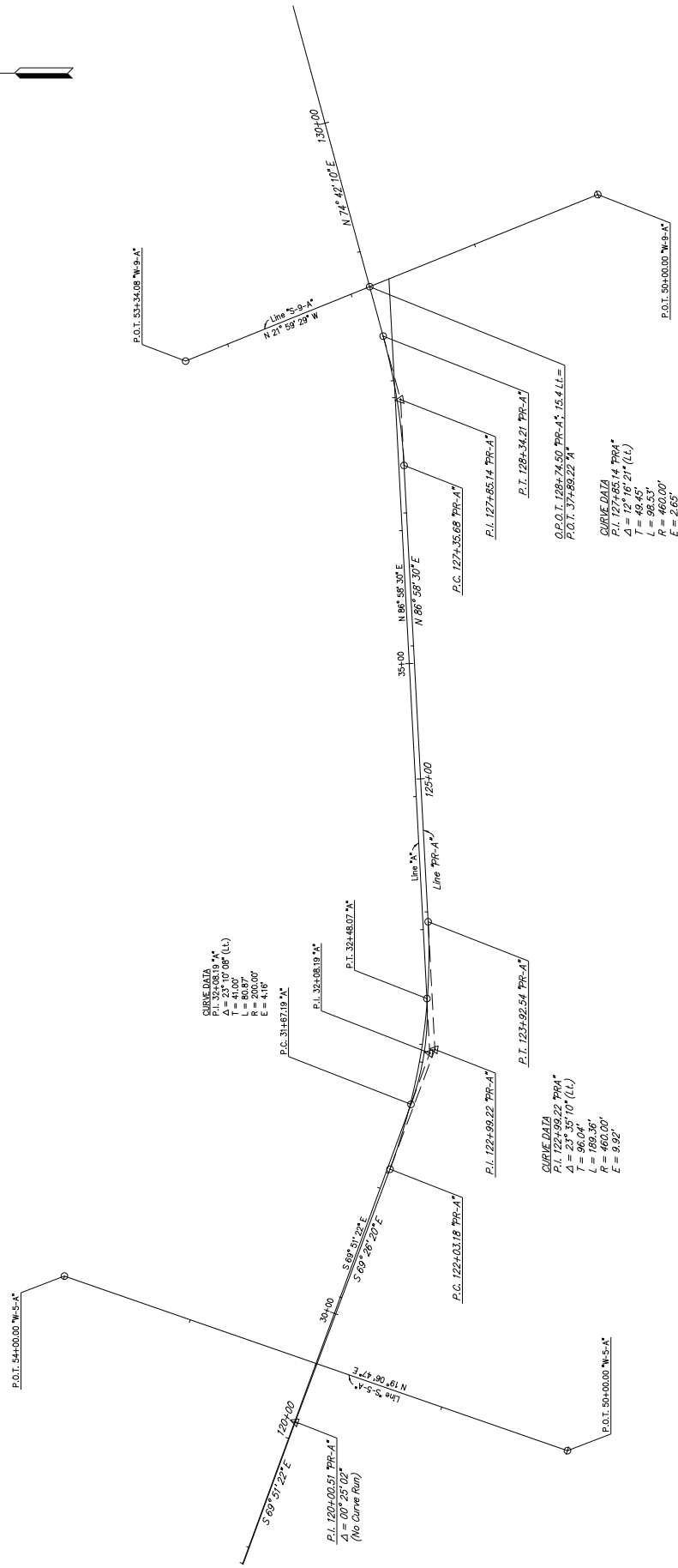
SEC. 35, T. 31 N., R. 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

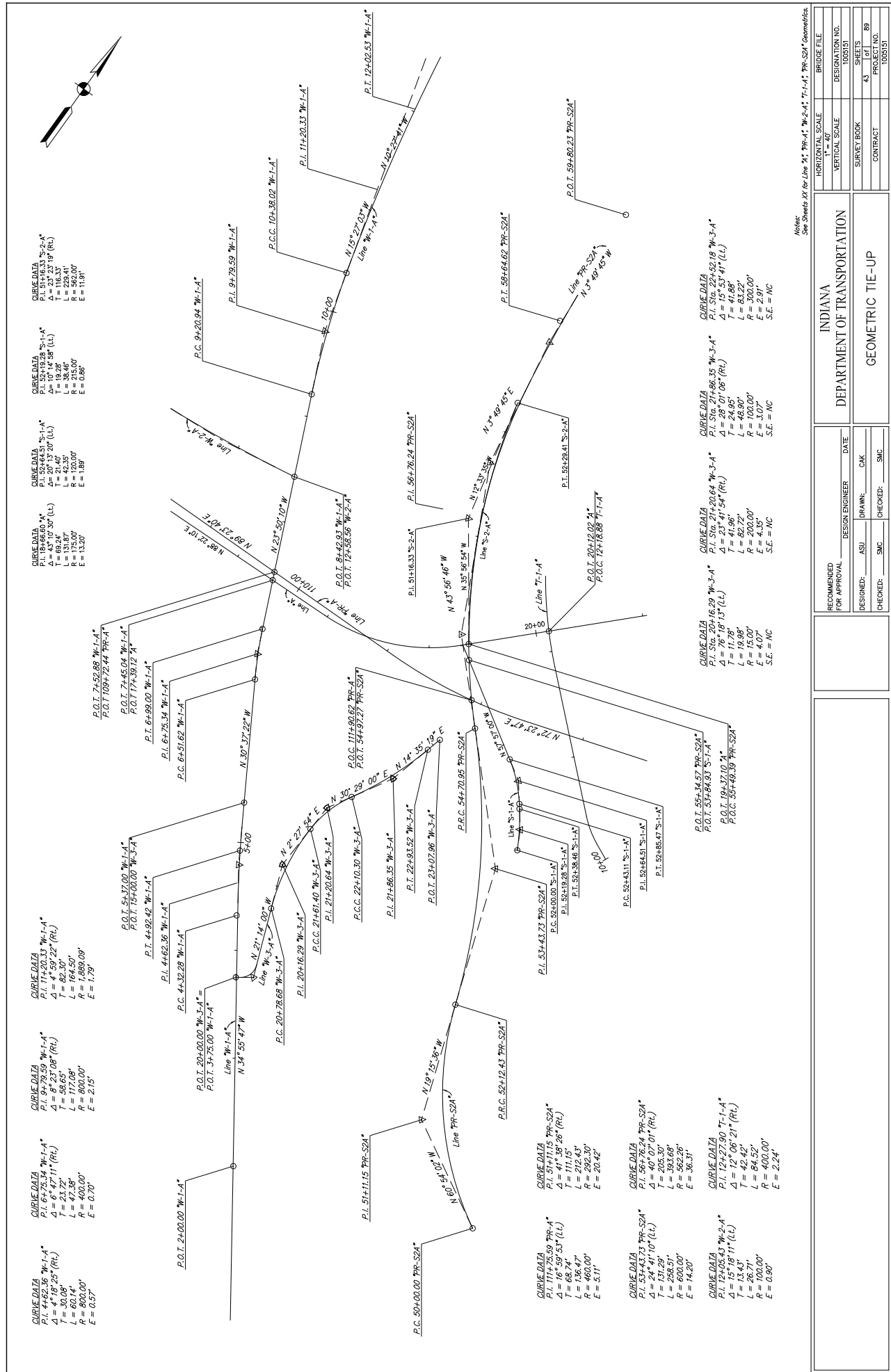
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				DESIGNED: ASU		DRAWN: PJT						SURVEY BOOK		SHEETS	
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										PLAT NO. 1					



Notes:

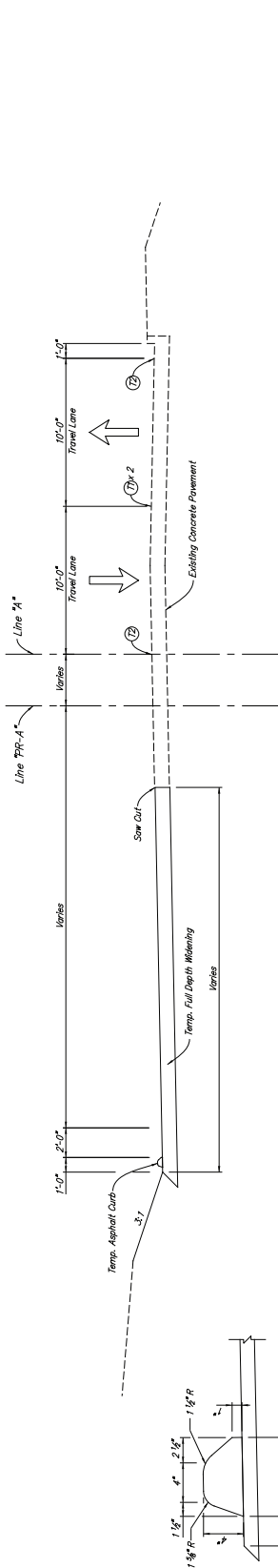
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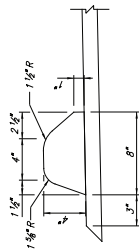


Notes:
See Sheets 20 for Line 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384, 385, 386, 387, 388, 389, 390, 391, 392, 393, 394, 395, 396, 397, 398, 399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

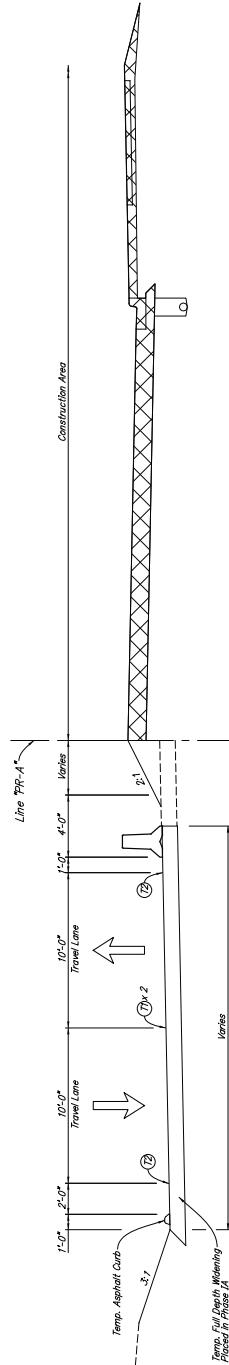
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DESIGNED: ASU		DRAWN: CAK		CHECKED: SMC	
INDIANA		DEPARTMENT OF TRANSPORTATION		GEOMETRIC TIE-UP	
HORIZONTAL SCALE		VERTICAL SCALE		BRIDGE FILE	
1"=100'		1"=10'		DESIGNATION NO.	
1005151		SURVEY BOOK		SHEETS	
43		CONTRACT		PROJECT NO.	
1005151		1005151		1005151	



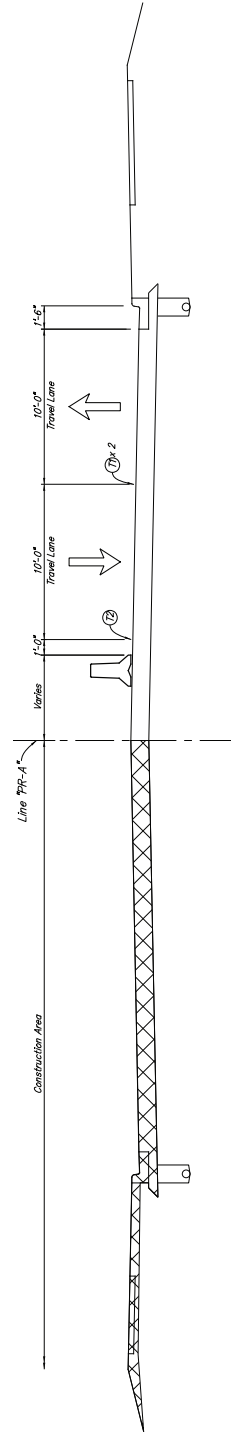
ASPHALT CURB (MOUNTABLE) DETAIL



PHASE I-A



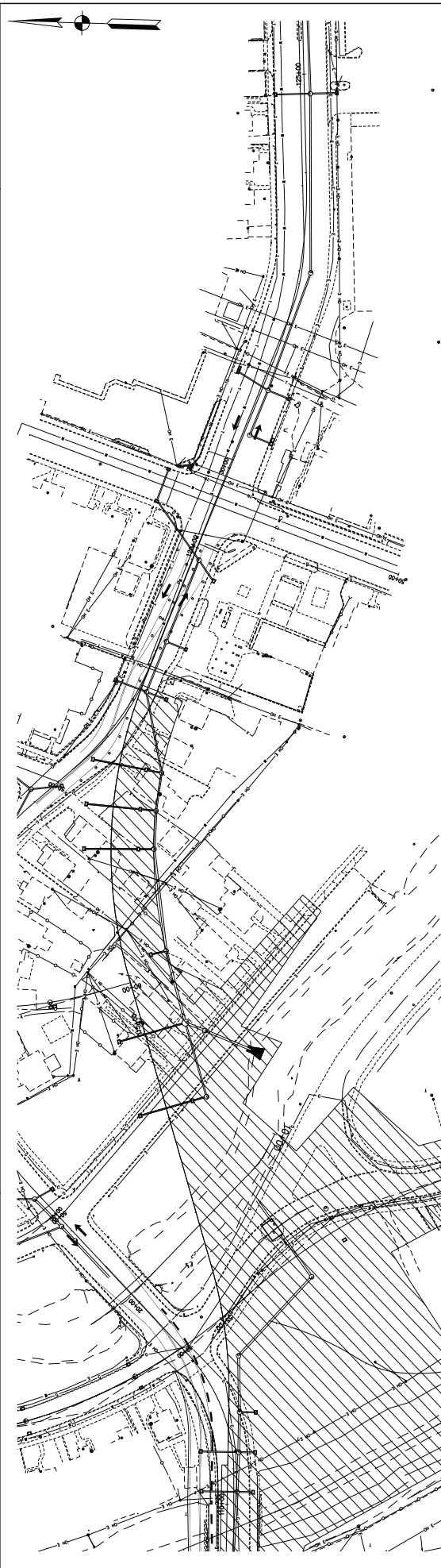
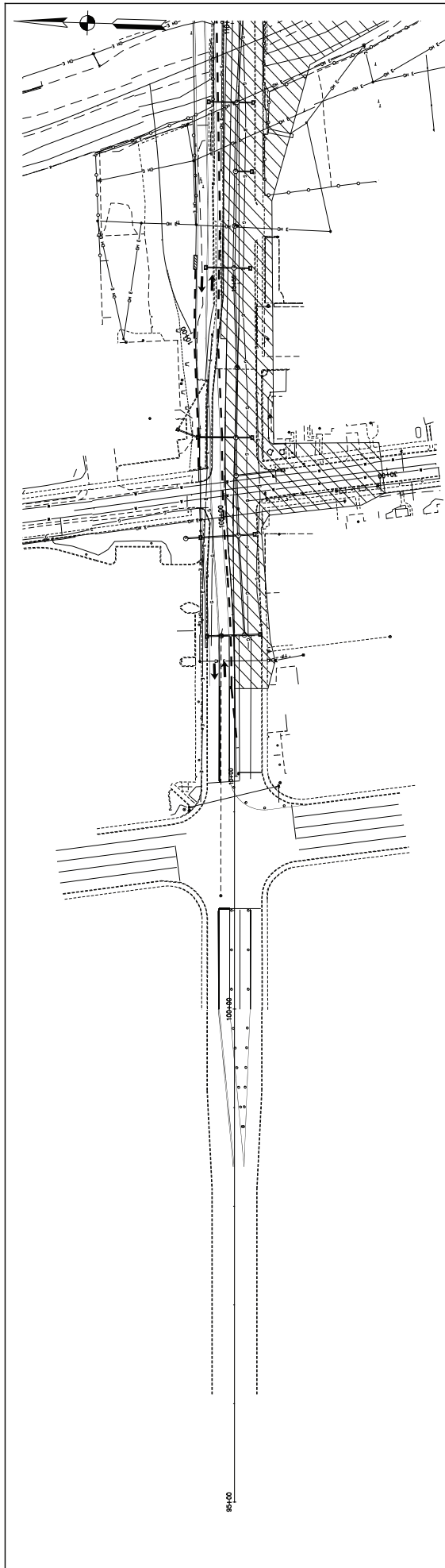
PHASE I



PHASE II

Notes: NMS Surface to be Placed Following Phase II Under Live Traffic

<div>INDIANA</div> <div>DEPARTMENT OF TRANSPORTATION</div> <div>MAINTENANCE OF TRAFFIC</div> <div>TYPICAL CROSS SECTIONS</div>		RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE	
		<div>LEGEND</div> <div>⑦ Temporary Pavement Marking, Solid, Yellow, 4 in.</div> <div>⑧ Temporary Pavement Marking, Solid, White, 4 in.</div> <div>Construction Area</div> <div>Direction of Traffic</div> <div>Standard Drum</div> <div>Temporary Concrete Barrier</div>		DESIGNED: ASU		DRAWN: CAK
				CHECKED: SMC		CHECKED: SMC
				SHEETS		44
PROJECT NO.		100514		CONTRACT		
SURVEY BOOK						
VERTICAL SCALE						
DESIGNATION NO.						
BRIDGE FILE						



LEGEND

-----	Temporary Barrier	↓	Direction of Traffic	--- A A	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid Yellow, 4 in.
---	Detour Route	▨	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid White, 4 in.
		▩	Standard Drum	④	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid White, 24 in.

INDIANA	
DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC	
PHASE I	

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	45 of 100
	PROJECT NO.
	1005154

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<p>LEGEND</p> <p>a. Direction of Traffic:</p> <p>----- Temporary Barrier</p> <p>--- Detour Route</p> <p> Construction Area</p> <p> Construction Sign w/ Light Warning</p> <p> Standard Drum</p>		<p>LEGEND</p> <p>b. Type of Construction Warning Light</p> <p>① Temporary Pavement Marking, Solid, Yellow, 4 in.</p> <p>② Temporary Pavement Marking, Solid, White, 4 in.</p> <p>③ Temporary Pavement Marking, Solid, White, 4 in.</p> <p>c. Construction Sign w/ Light Warning</p>		<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER _____ DATE _____</p> <p>DESIGNED: ASJ DRAWN: PJT</p> <p>CHECKED: SMC CHECKED: SMC</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>MAINTENANCE OF TRAFFIC</p> <p>PHASE I</p>		<p>HORIZONTAL SCALE 1" = 50'</p> <p>VERTICAL SCALE 100:1</p> <p>SURVEY BOOK 46</p> <p>SHEETS 61</p> <p>PROJECT NO. XXX</p> <p>CONTRACT 100S154</p>		<p>BRIDGE FILE</p> <p>DESIGNATION NO. 100S154</p>	
---	--	--	--	---	--	---	--	--	--	---	--



LEGEND

-----	Temporary Barrier	↓	Direction of Traffic	---A---	Type III Construction Warning Light	①	Temporary Pavement Marking, Solid, Yellow, 4 in.
---	Detour Route	⊠	Construction Area	---	Type III Barricade (See Callout for Type)	②	Temporary Pavement Marking, Solid, White, 4 in.
		⊠	Standard Drum	⊠	Construction Sign w/ Light Warning	③	Temporary Pavement Marking, Solid, White, 24 in.

RECOMMENDED FOR APPROVAL

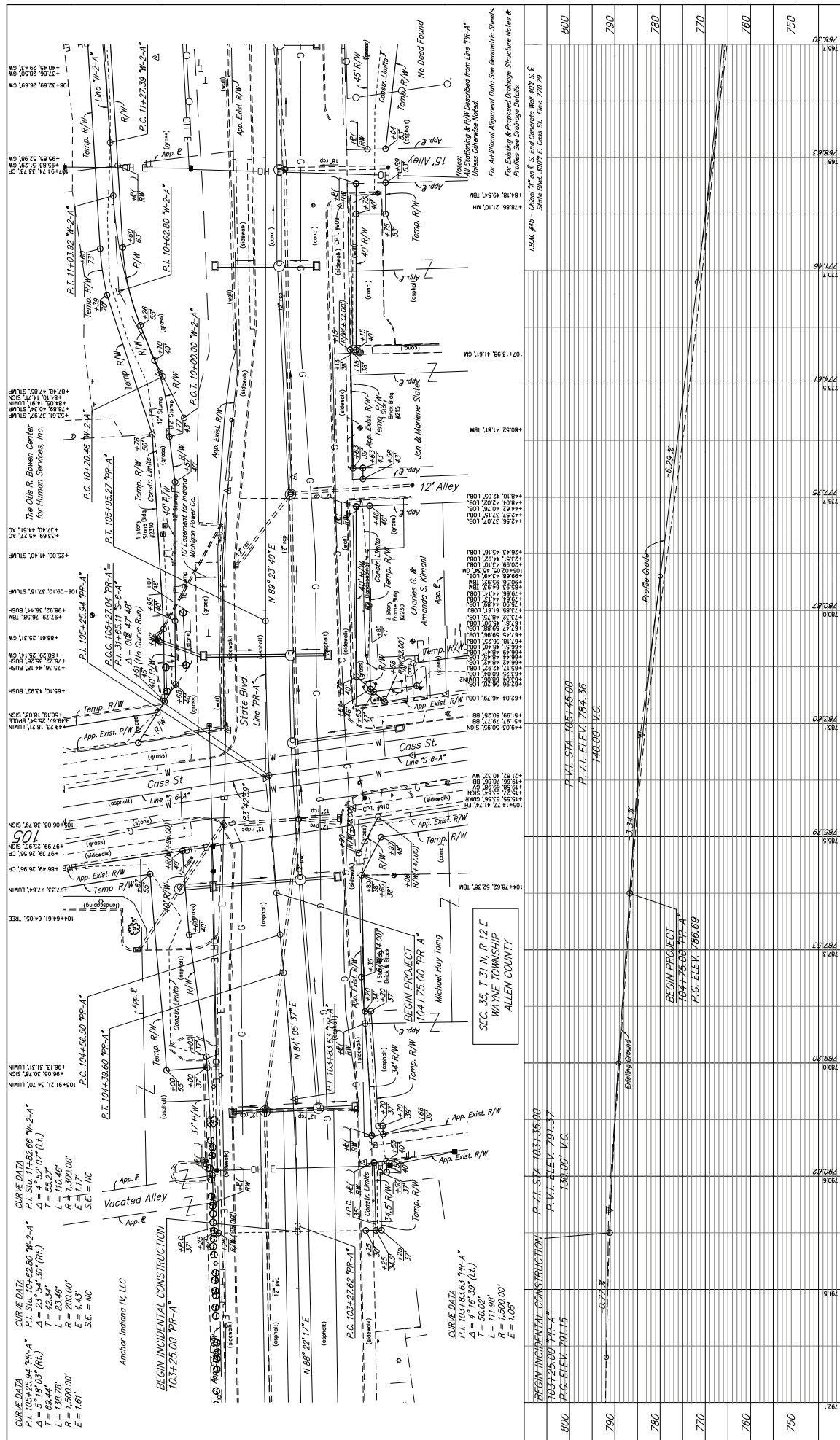
DESIGNED:	ASU	DRAWN:	PJT
CHECKED:	SMC	CHECKED:	SMC

DATE

INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE II

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	SHEETS
CONTRACT	PROJECT NO.
	100514

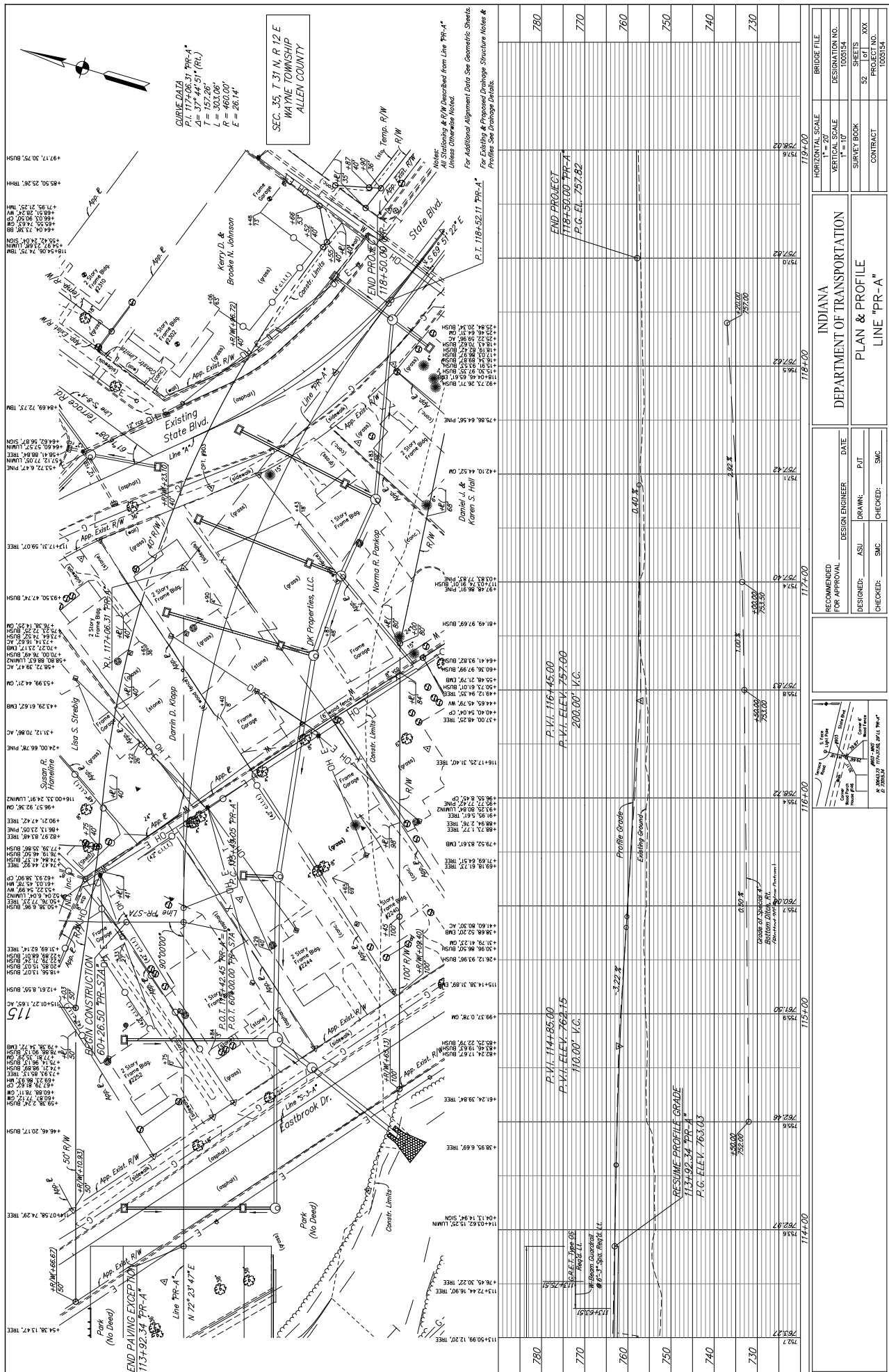
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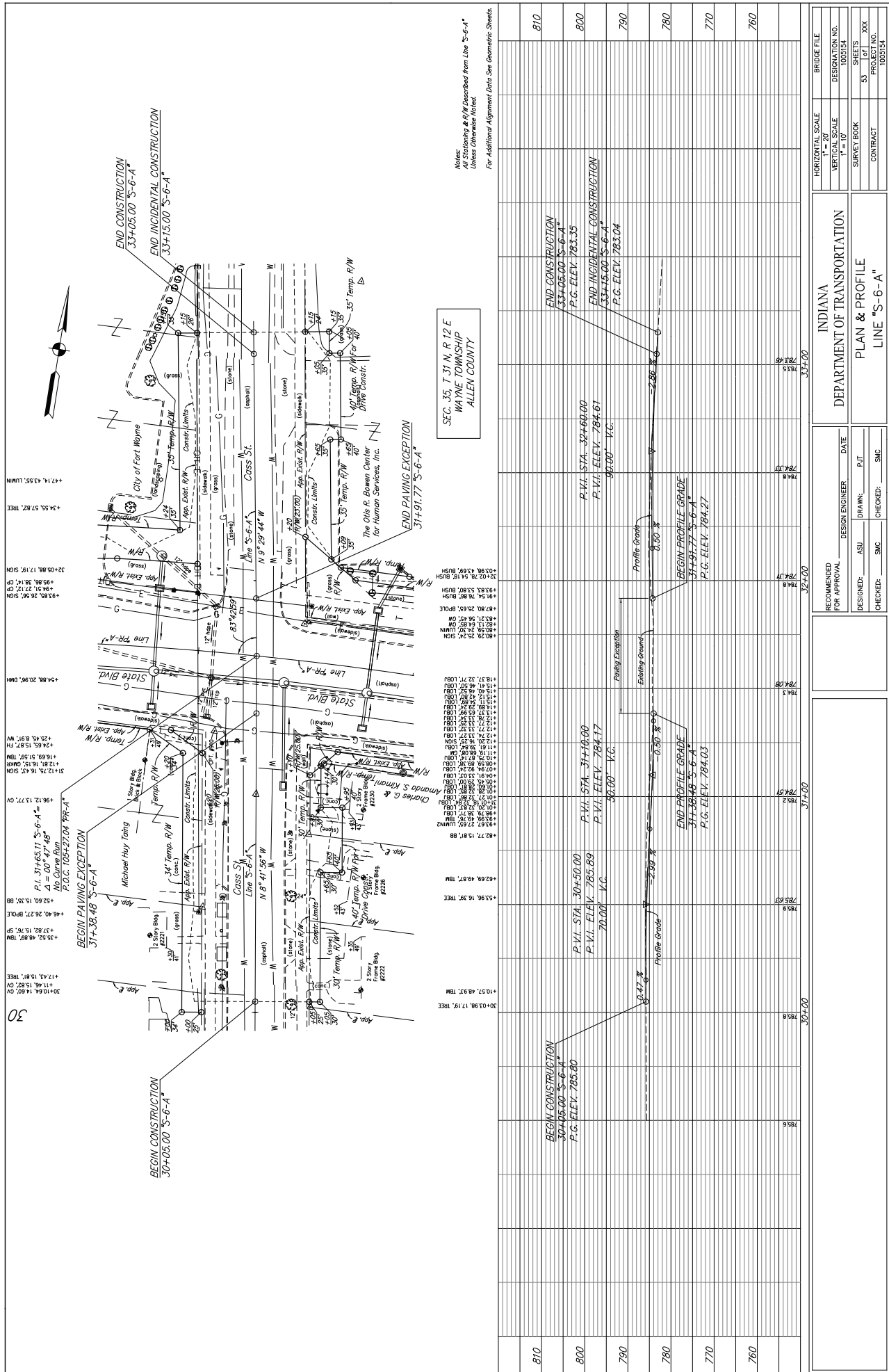
INDIANA DEPARTMENT OF TRANSPORTATION			
PLAN & PROFILE			
LINE "PR-A"			
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	
DESIGNED: ASU	DRAWN: SMC	CHECKED: SMC	
CHECKED: SMC			
BRIDGE FILE	DESIGNATION NO.	SURVEY BOOK	SHEETS
	100514	50	of 100
		CONTRACT	PROJECT NO.
			100514

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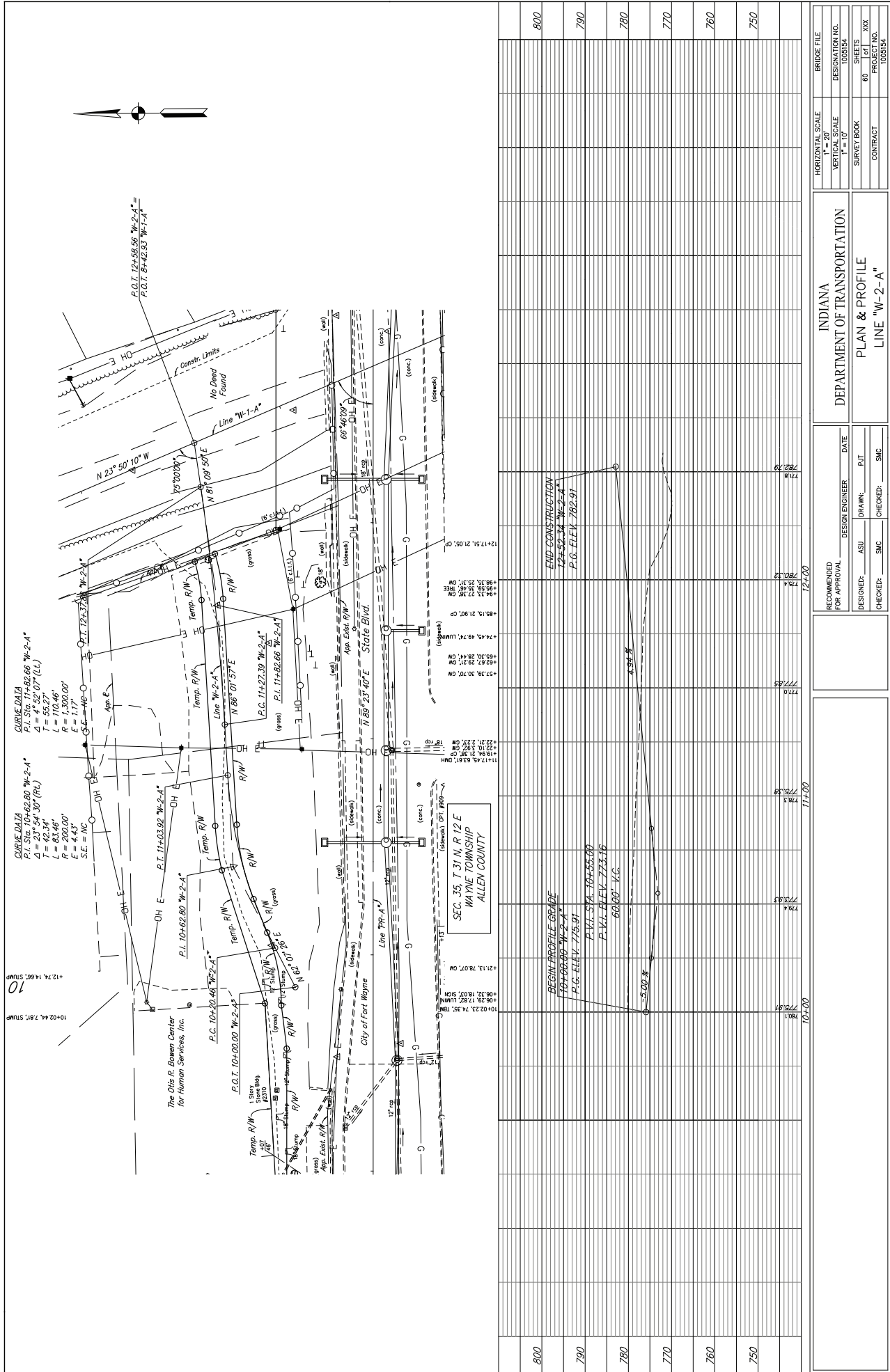


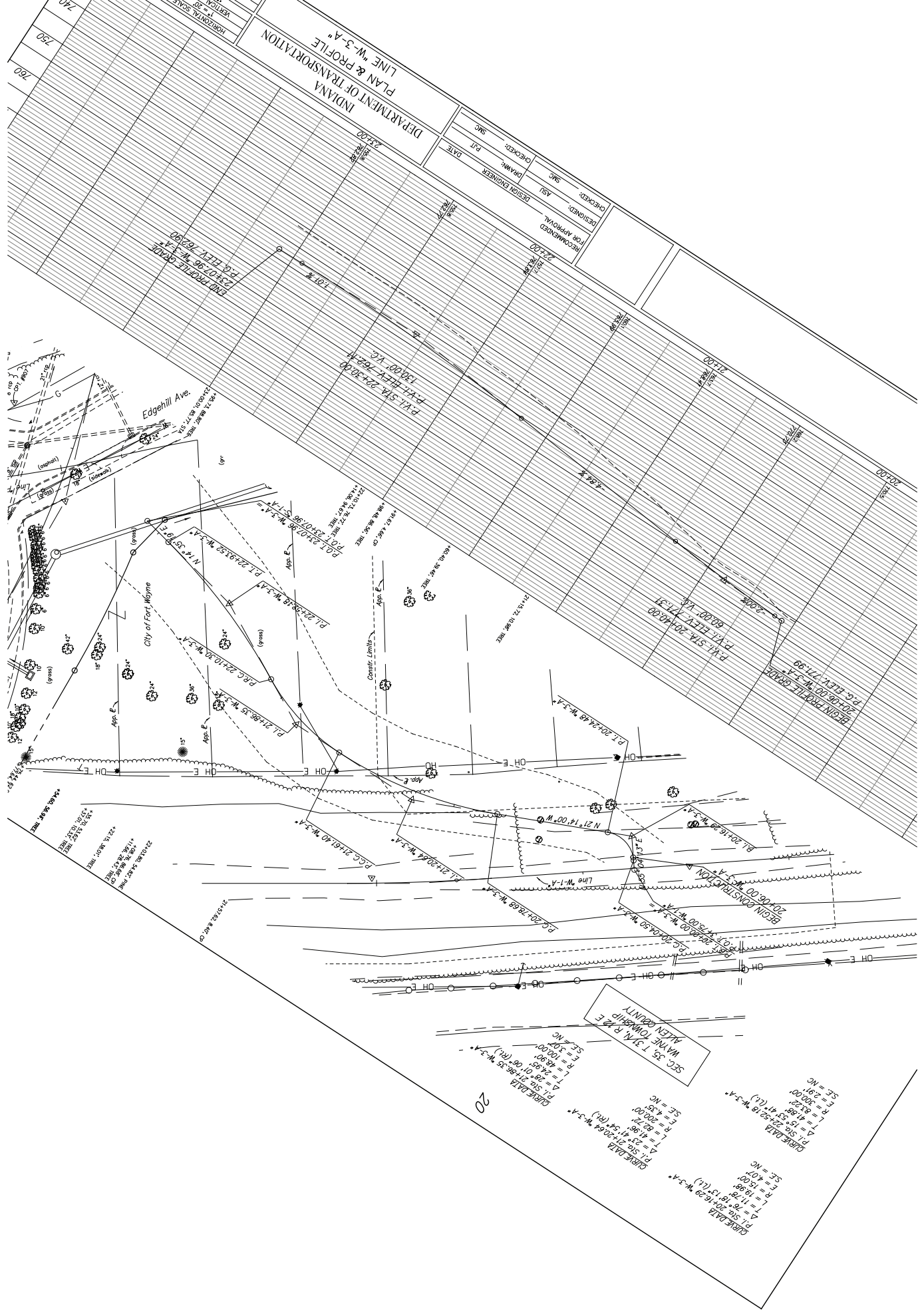


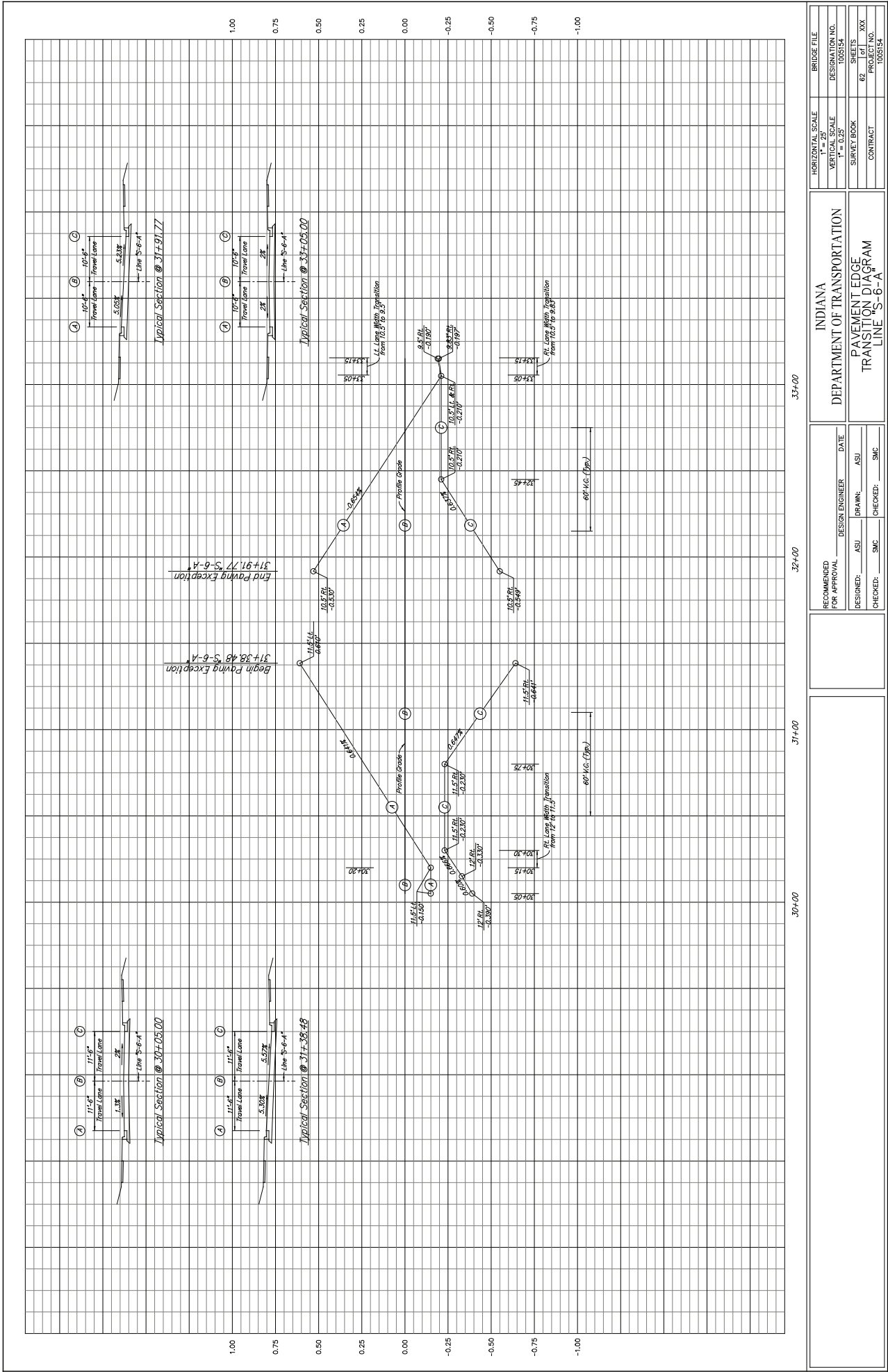
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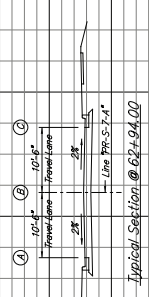
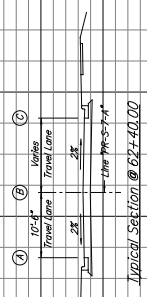
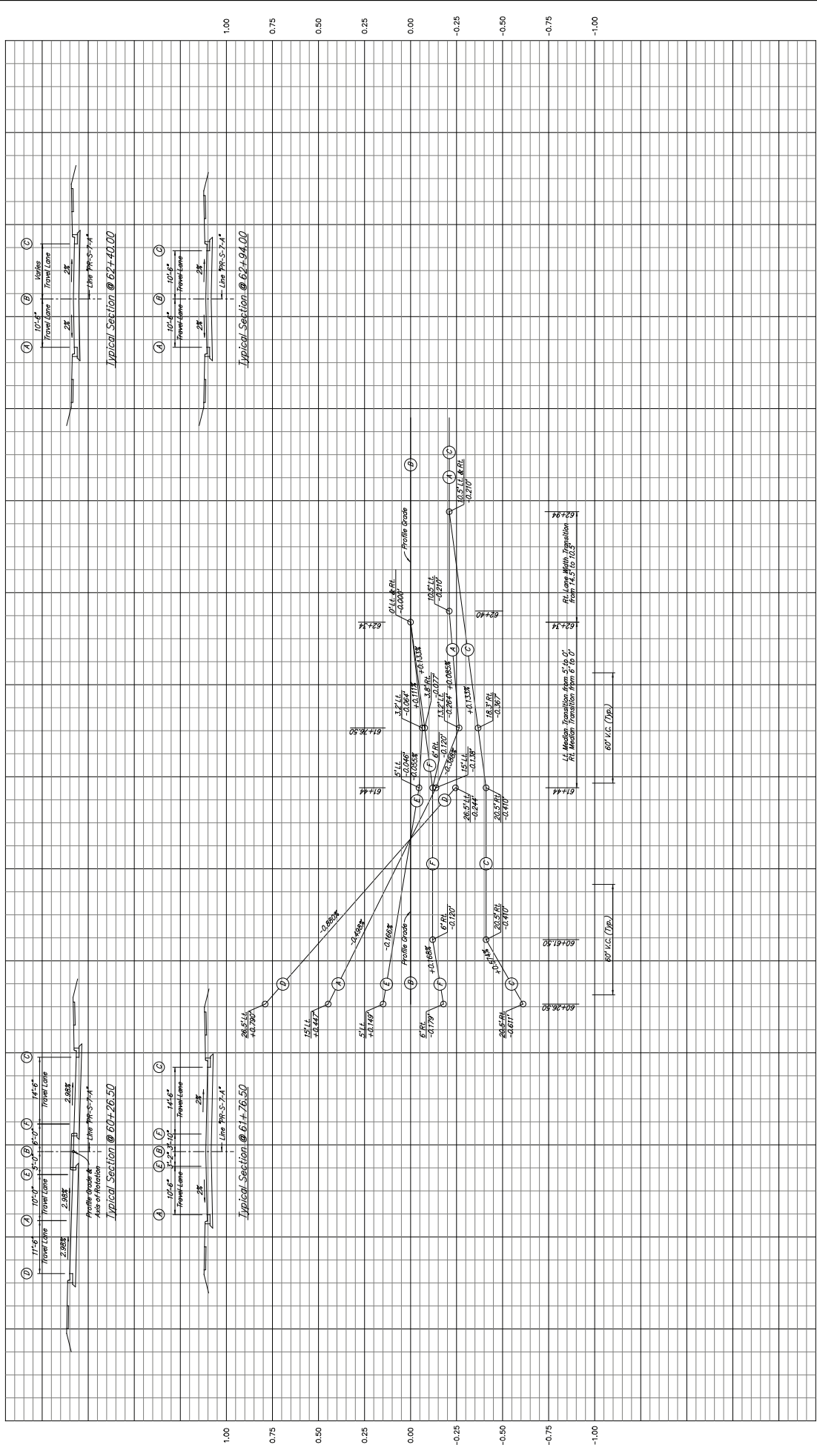




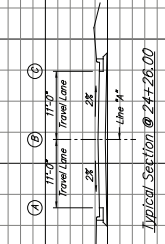
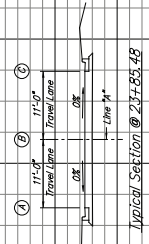
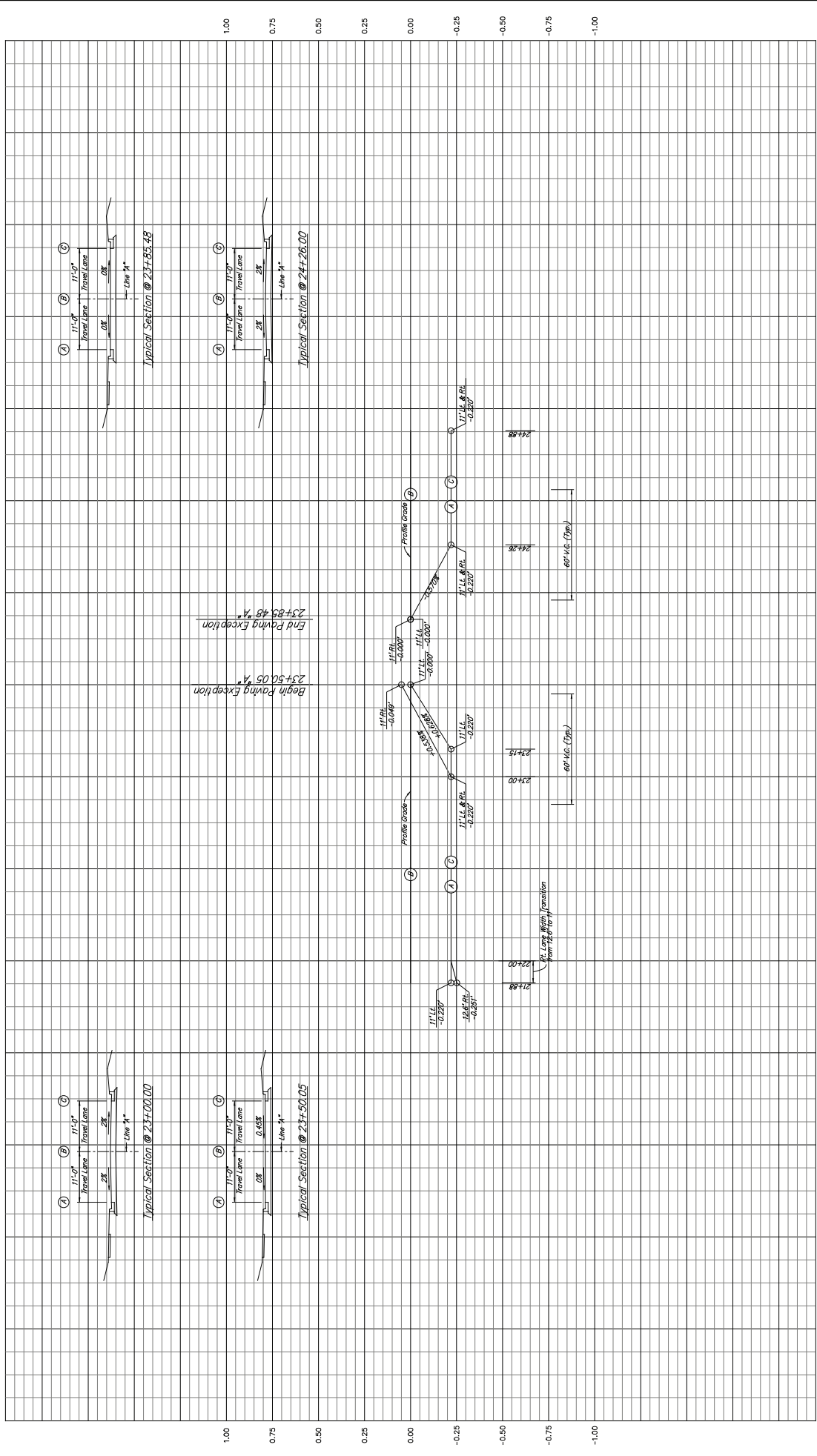




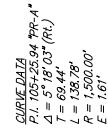




INDIANA		DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		VERTICAL SCALE	
DATE		ASU		1" = 0.25'	
DESIGNED: ASU		DRAWN: ASU		SURVEY BOOK	
CHECKED: SMC		CHECKED: SMC		CONTRACT	
Pavement Edge Transition Diagram		LINE PR S-7-A		SHEETS	
				64 of 64	
				PROJECT NO.	
				100514	



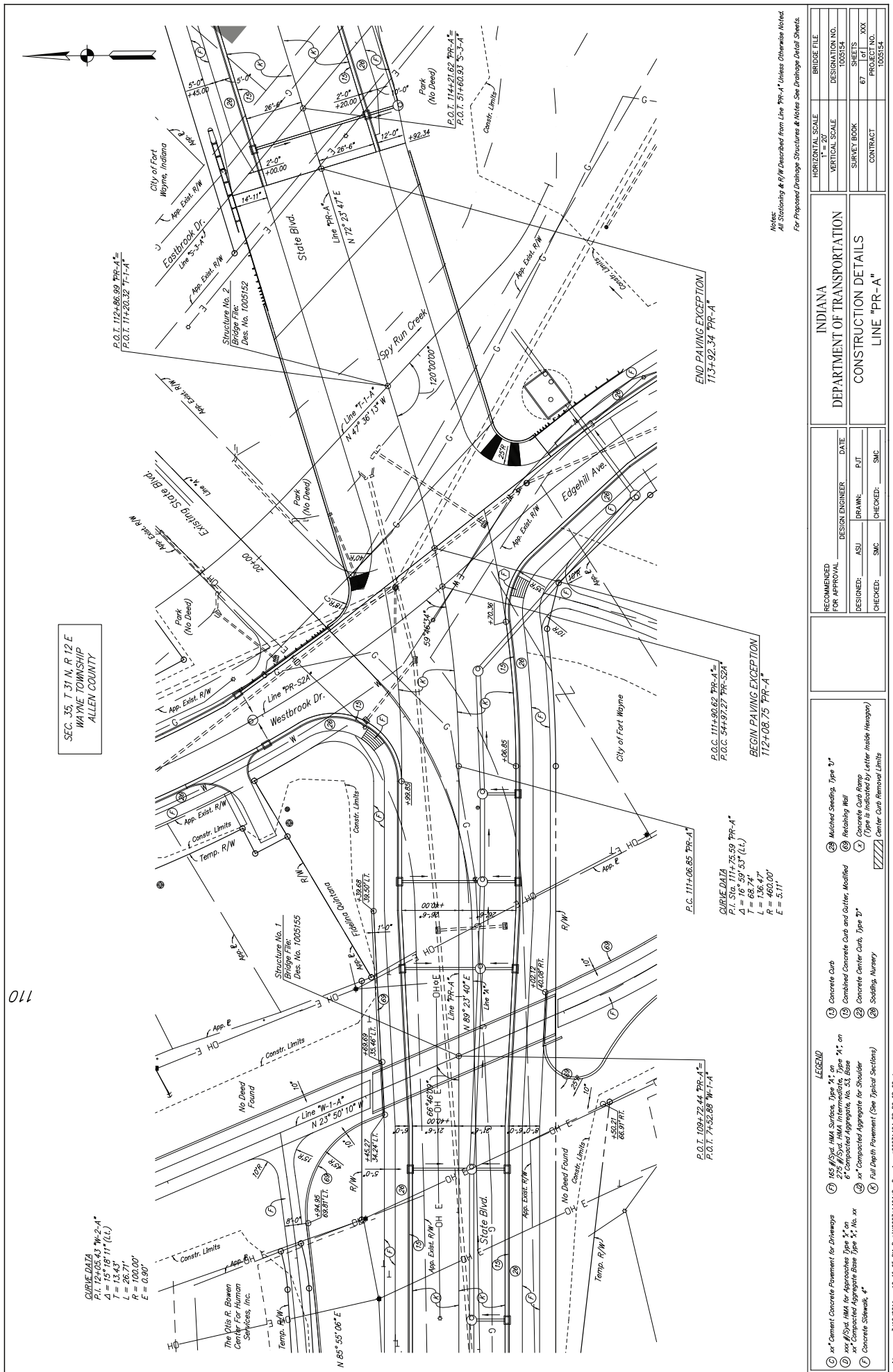
INDIANA		BRIDGE FILE	
DEPARTMENT OF TRANSPORTATION		DESIGNATION NO.	
PAVEMENT EDGE		100514	
TRANSITION DIAGRAM		SHEETS	
LINE "A"		65 of 100	
		PROJECT NO.	
		100514	
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	
		DATE	
DESIGNED: ASU		DRAWN: ASU	
CHECKED: SMC		CHECKED: SMC	



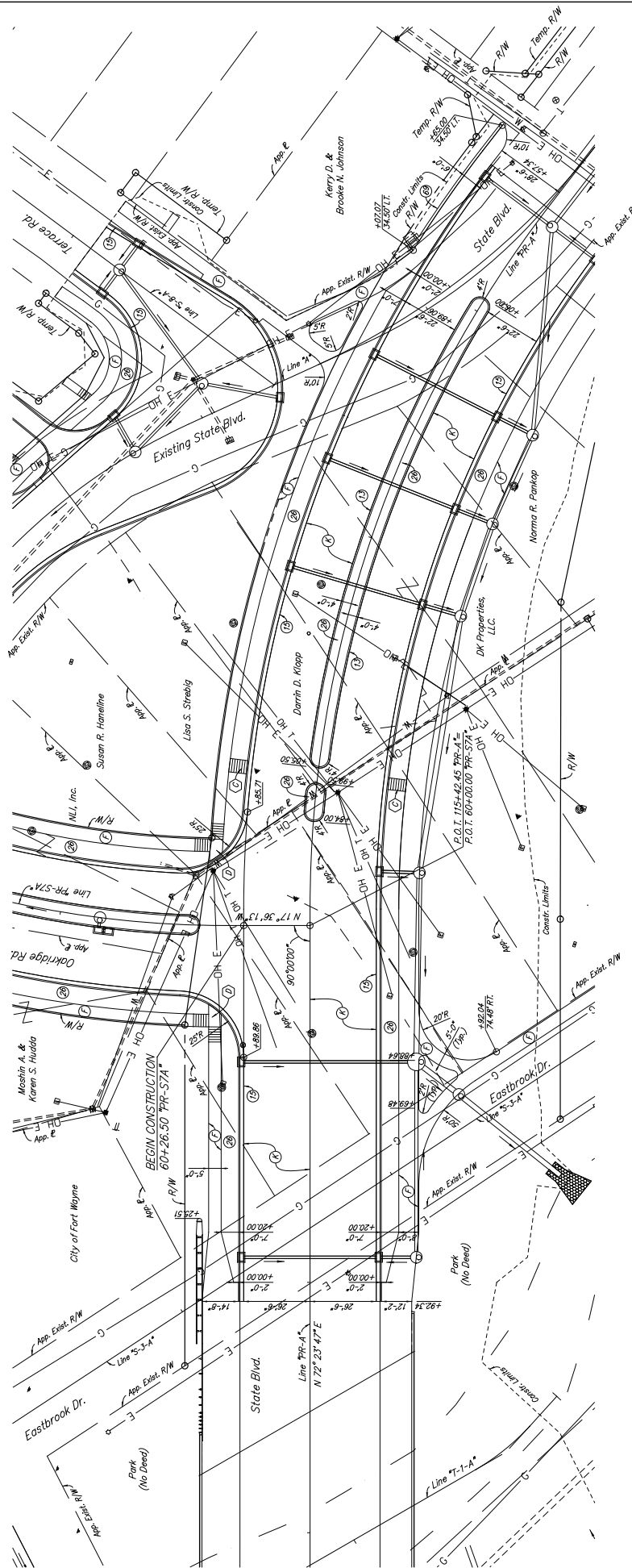
Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

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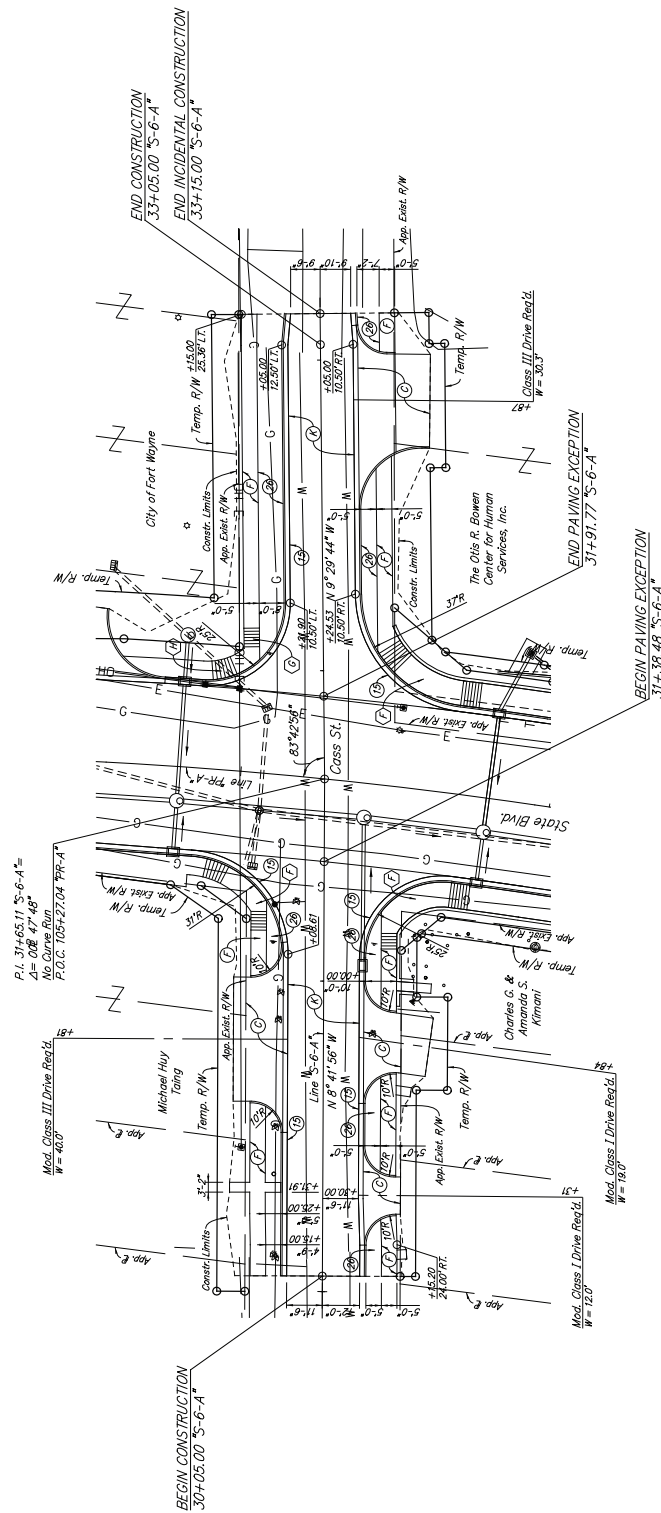
SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PP-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>LEGEND</p> <p>(7) 165 #/yd. MMA Surface, Type "A" on 275 #/yd. MMA Intermediate, Type "B" on 6" Compacted Aggregate, No. 53. Base & Compact Aggregate for Shoulder</p> <p>(8) Concrete Curb</p> <p>(9) Combined Concrete Curb and Culvert, Modified</p> <p>(10) Concrete Curb Ramp (Type is indicated by Letter Inside Heavily)</p> <p>(11) Sodding, Nursery</p> <p>(12) Sodding, Nursery (See Typical Sections)</p> <p>(13) Concrete Slabwork, 4"</p> <p>(14) Cement Concrete Pavement for Driveways</p> <p>(15) 165 #/yd. MMA for Approaches Type "A" on 275 #/yd. MMA Intermediate Type "B" on 6" Compacted Aggregate Type "C" on Base Type "D" on</p> <p>(16) Concrete Slabwork, 4"</p> <p>(17) 165 #/yd. MMA Surface, Type "A" on 275 #/yd. MMA Intermediate, Type "B" on 6" Compacted Aggregate, No. 53. Base & Compact Aggregate for Shoulder</p> <p>(18) Concrete Curb</p> <p>(19) Combined Concrete Curb and Culvert, Modified</p> <p>(20) Concrete Curb Ramp (Type is indicated by Letter Inside Heavily)</p> <p>(21) Sodding, Nursery</p> <p>(22) Sodding, Nursery (See Typical Sections)</p> <p>(23) Concrete Slabwork, 4"</p> <p>(24) Cement Concrete Pavement for Driveways</p> <p>(25) 165 #/yd. MMA for Approaches Type "A" on 275 #/yd. MMA Intermediate Type "B" on 6" Compacted Aggregate Type "C" on Base Type "D" on</p> <p>(26) Concrete Slabwork, 4"</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS</p> <p>LINE "PR-A"</p>		<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p>		<p>HORIZONTAL SCALE</p> <p>VERTICAL SCALE</p>		<p>BRIDGE FILE</p>	
<p>DESIGNED: ASU</p> <p>CHECKED: SMC</p>		<p>DRAWN: PJT</p> <p>CHECKED: SMC</p>		<p>SURVEY BOOK</p> <p>CONTRACT</p>		<p>SHEETS</p> <p>68</p>		<p>DESIGNATION NO.</p> <p>1005154</p>	
<p>PROJECT NO.</p> <p>1005154</p>		<p>PROJECT NO.</p> <p>1005154</p>		<p>PROJECT NO.</p> <p>1005154</p>		<p>PROJECT NO.</p> <p>1005154</p>		<p>PROJECT NO.</p> <p>1005154</p>	

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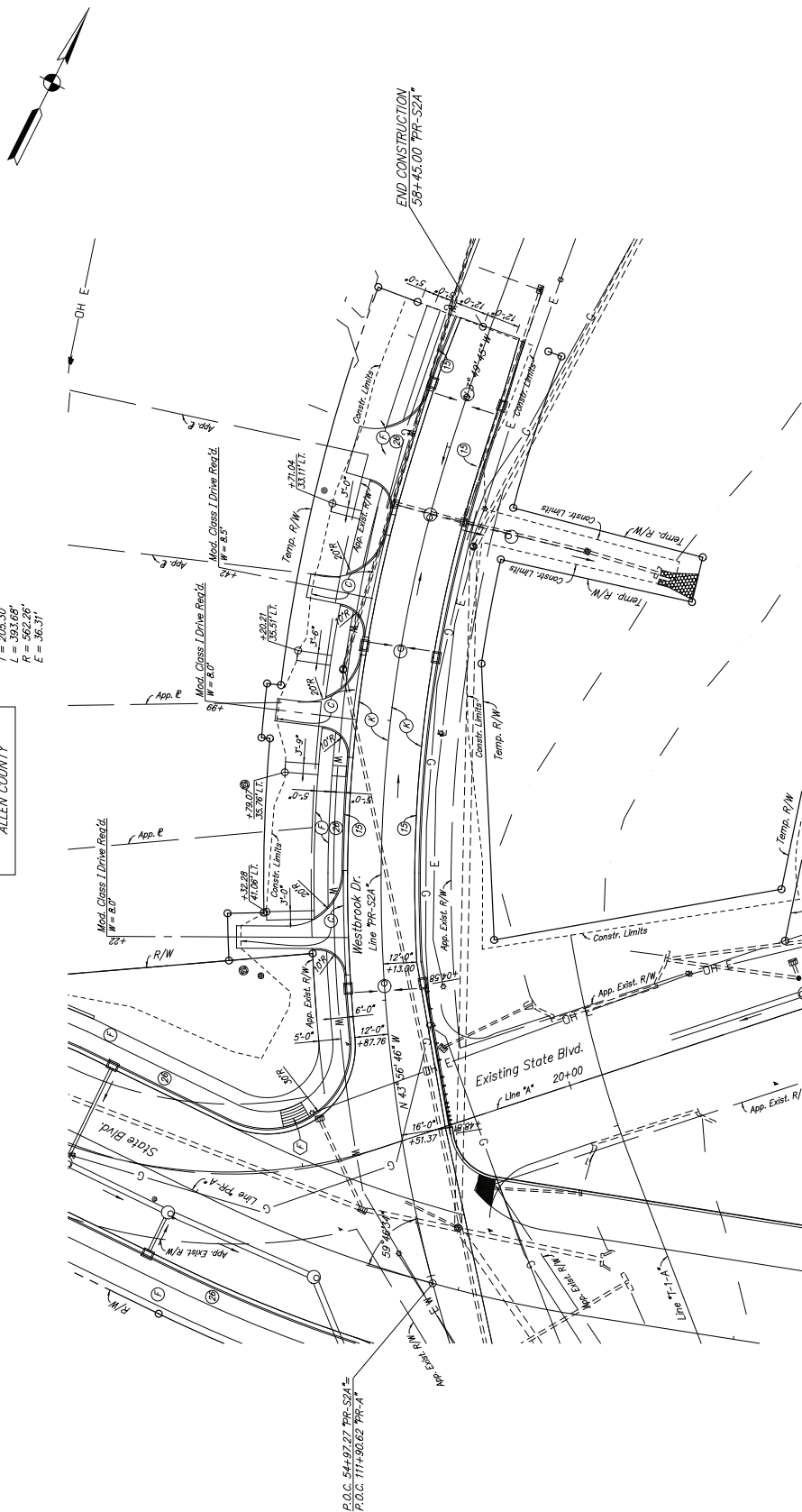


Notes:
All Stationing & R/W Described from Line "S-6-A" Unless Otherwise Noted.

<p>LEGEND</p> <p>⑦ 165 #/yd. HMA Surface, Type "A" on 275 #/yd. HMA Intermediate, Type "A" on 6" Compacted Aggregate, No. 53 Base ⑧ 6" Compacted Aggregate for Shoulder ⑨ 4" Concrete Slabwork, 4" ⑩ Full Depth Pavement (See Typical Sections) ⑪ Concrete Curb ⑫ Combined Concrete Curb and Culvert, Modified ⑬ Retaining Wall ⑭ Matched Seding, Type "B" ⑮ Concrete Curb Ramp ⑯ Concrete Center Cuts, Type "B" ⑰ Sodding, Nursery ⑱ Center Curb Removal Limits</p>		<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS</p> <p>LINE "5-6-A"</p>	
<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p>		<p>HORIZONTAL SCALE</p> <p>VERTICAL SCALE</p>	
<p>DESIGNED: ASU</p> <p>CHECKED: SMC</p>		<p>SURVEY BOOK</p> <p>CONTRACT</p>	
<p>DRAWN: PJT</p> <p>CHECKED: SMC</p>		<p>DESIGNATION NO.</p> <p>PROJECT NO.</p>	
<p>BRIDGE FILE</p>		<p>1005154</p>	

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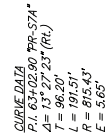
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Δ = 40° 07' 01" (Rt.)
T = 205.30'
L = 393.68'
R = 562.26'
E = 36.31'



Notes:
All Stationing & R/W Described from Line "P-R-2A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

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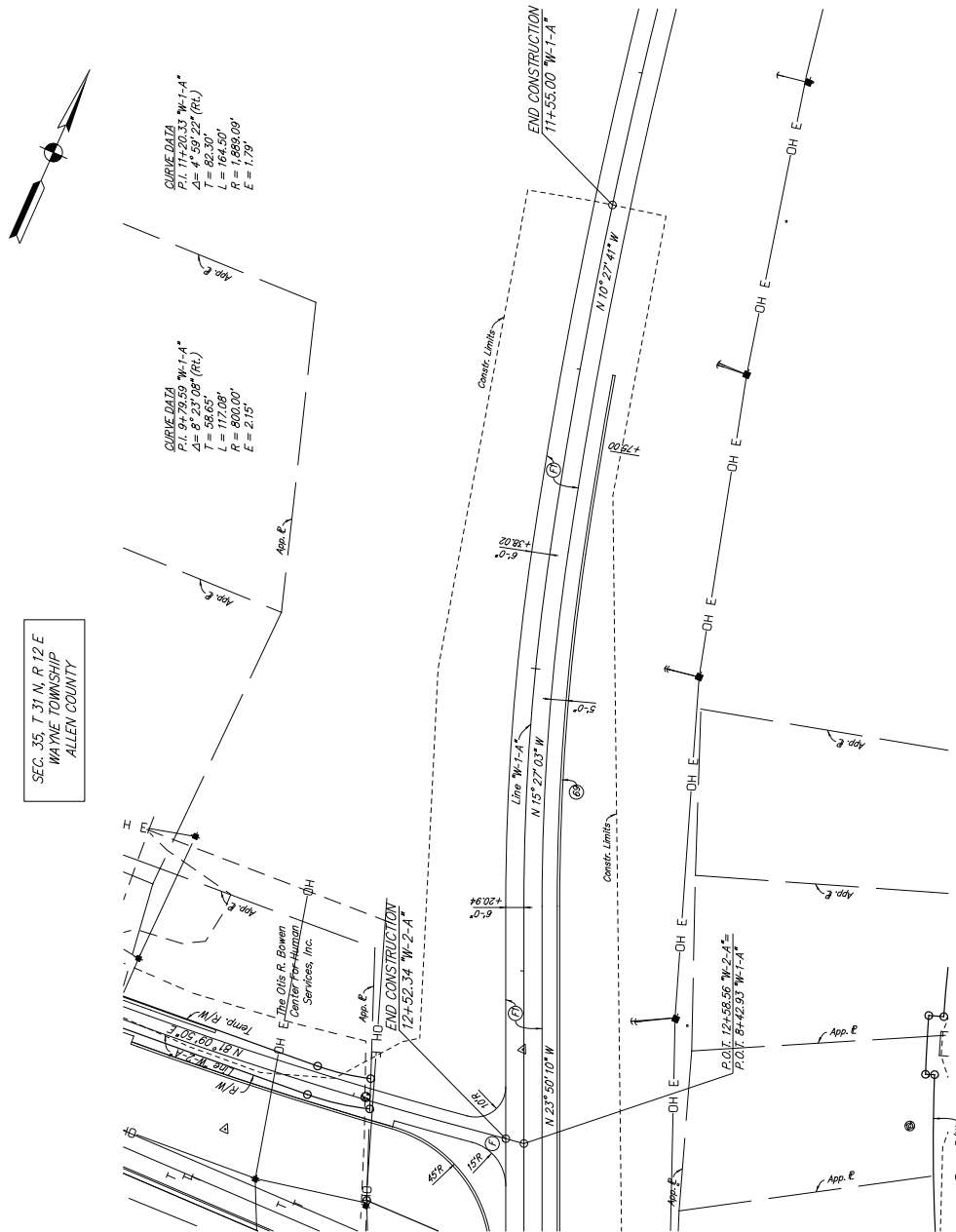
Notes:
All Stationing & R/W Described from Line "PR-S7A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>LEGEND</p> <p>(7) 105 #5yd. HMA Surface, Type 1* on 275 #5yd. HMA Intermediate, Type 1*, on 6" Compacted Aggregate, No. 53, Base</p> <p>(8) xxx #5yd. HMA for Approaches Type 1*, on 275 #5yd. HMA Intermediate, Type 1*, on 6" Compacted Aggregate, No. 53, Base Type 1*, No. xx</p> <p>(9) Concrete Slabwall, 1*</p> <p>(10) x* Cement Concrete Pavement for Driveways</p> <p>(11) Full Depth Pavement (See Typical Sections)</p> <p>(12) x* Compacted Aggregate for Shoulder</p> <p>(13) Concrete Curb</p> <p>(14) Combined Concrete Curb and Culvert, Modified</p> <p>(15) Matched Seeding, Type 2*</p> <p>(16) Precasting Wall</p> <p>(17) Concrete Center Curb, Type 2*</p> <p>(18) Sodding, Nursery</p> <p>(19) Center Curb Removal Limits</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS LINE "PR-S7A"</p>		<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER</p> <p>DATE</p>	<p>DESIGNED: ASJ</p> <p>CHECKED: SMC</p> <p>DRAWN: CAK</p> <p>CHECKED: SMC</p>	<p>HORIZONTAL SCALE = 20'</p> <p>VERTICAL SCALE</p> <p>SURVEY BOOK</p> <p>CONTRACT</p> <p>SHEETS</p> <p>PROJECT NO. 1005154</p>	<p>BRIDGE FILE</p>
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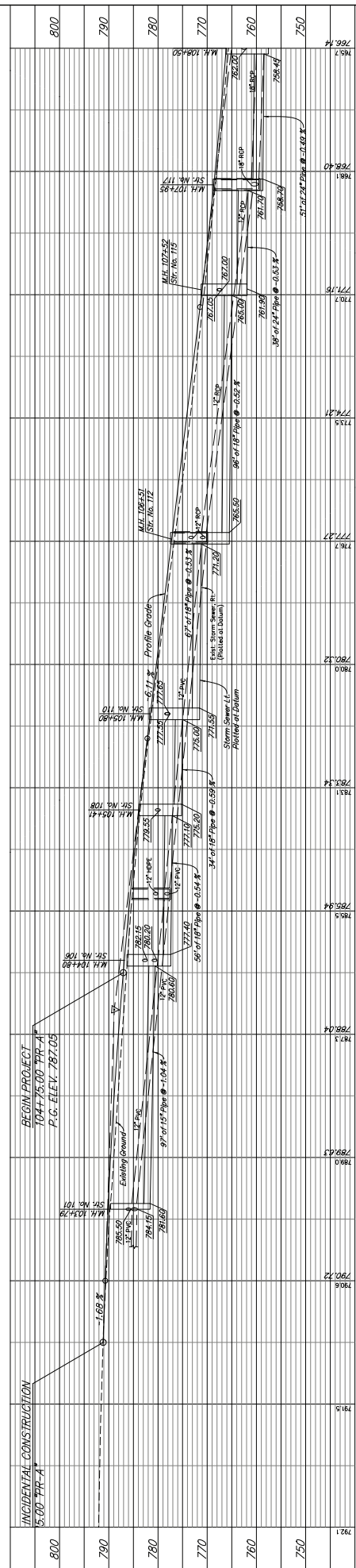
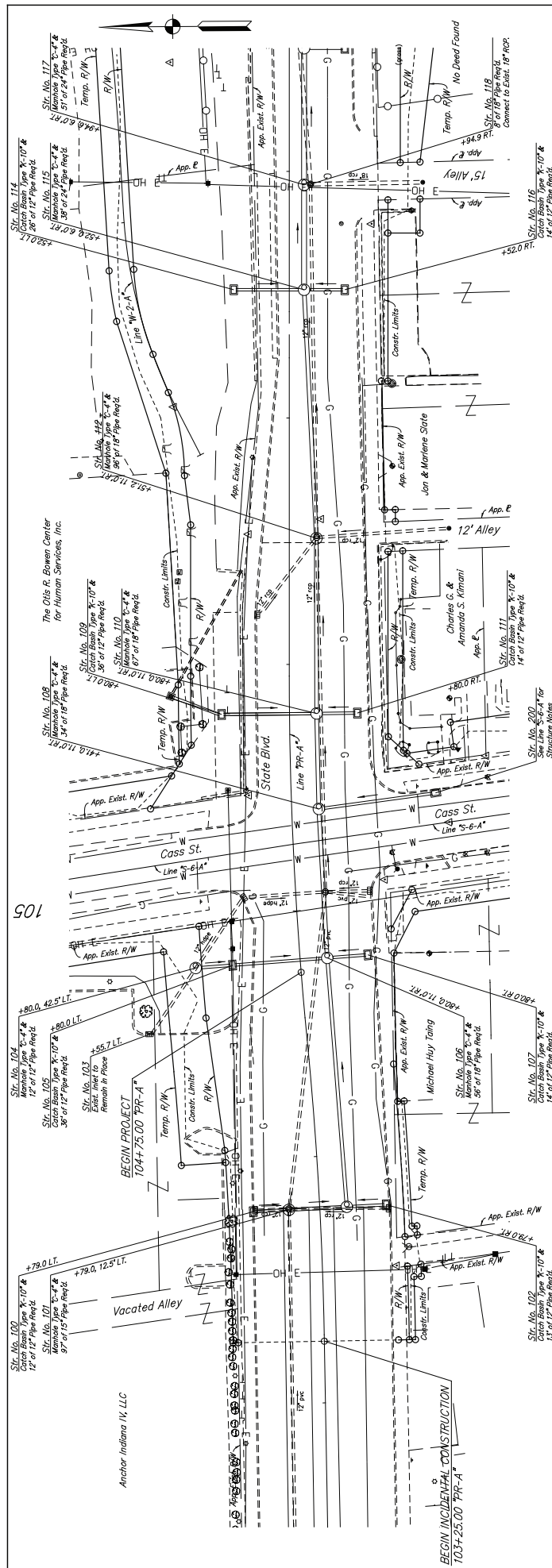
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SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

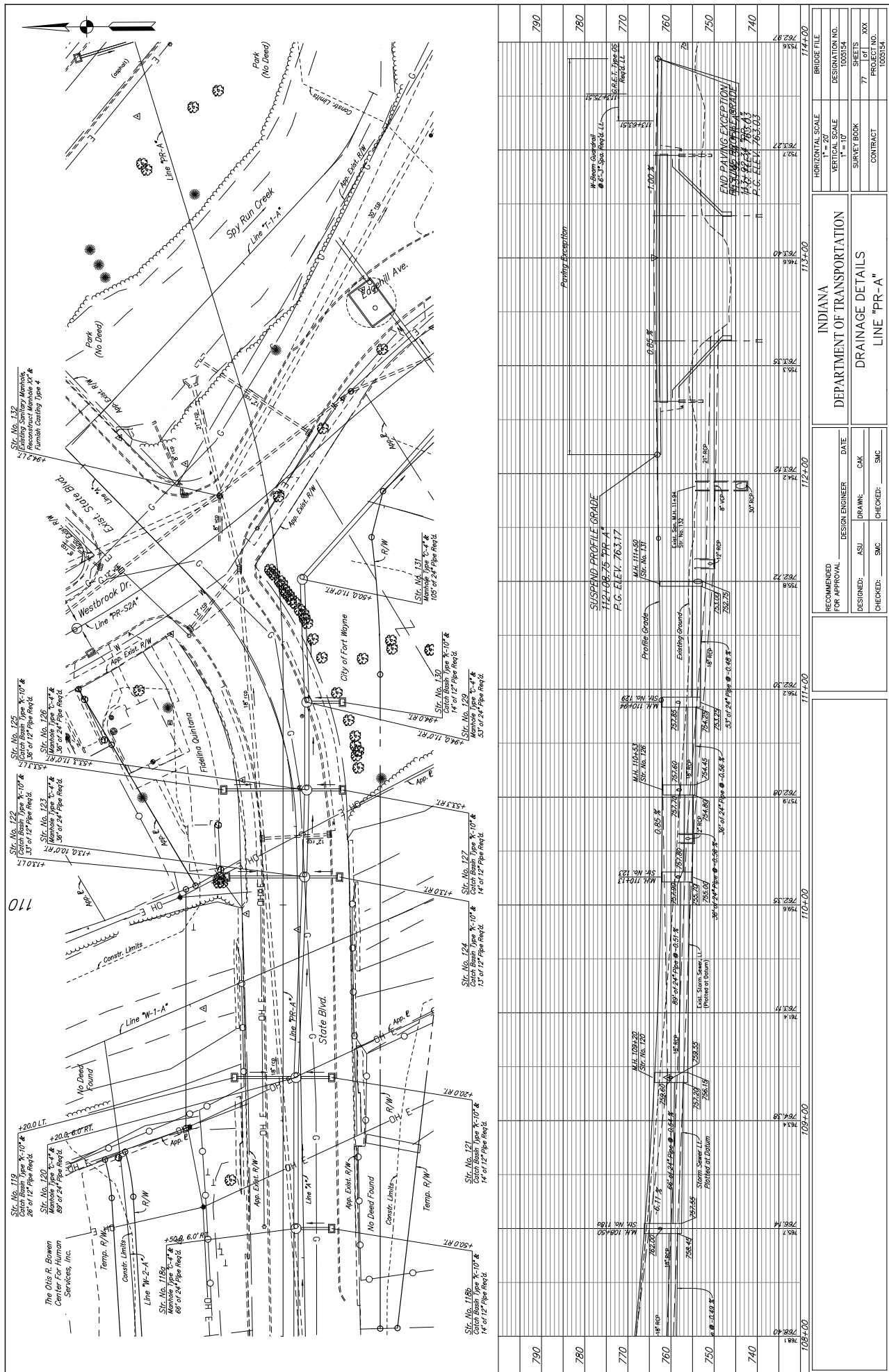


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DEPARTMENT OF TRANSPORTATION					
CONSTRUCTION DETAILS					
LINE "W-1-A"					
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VERTICAL SCALE		SURVEY BOOK		SHEETS	
CONTRACT		PROJECT NO.		100514	

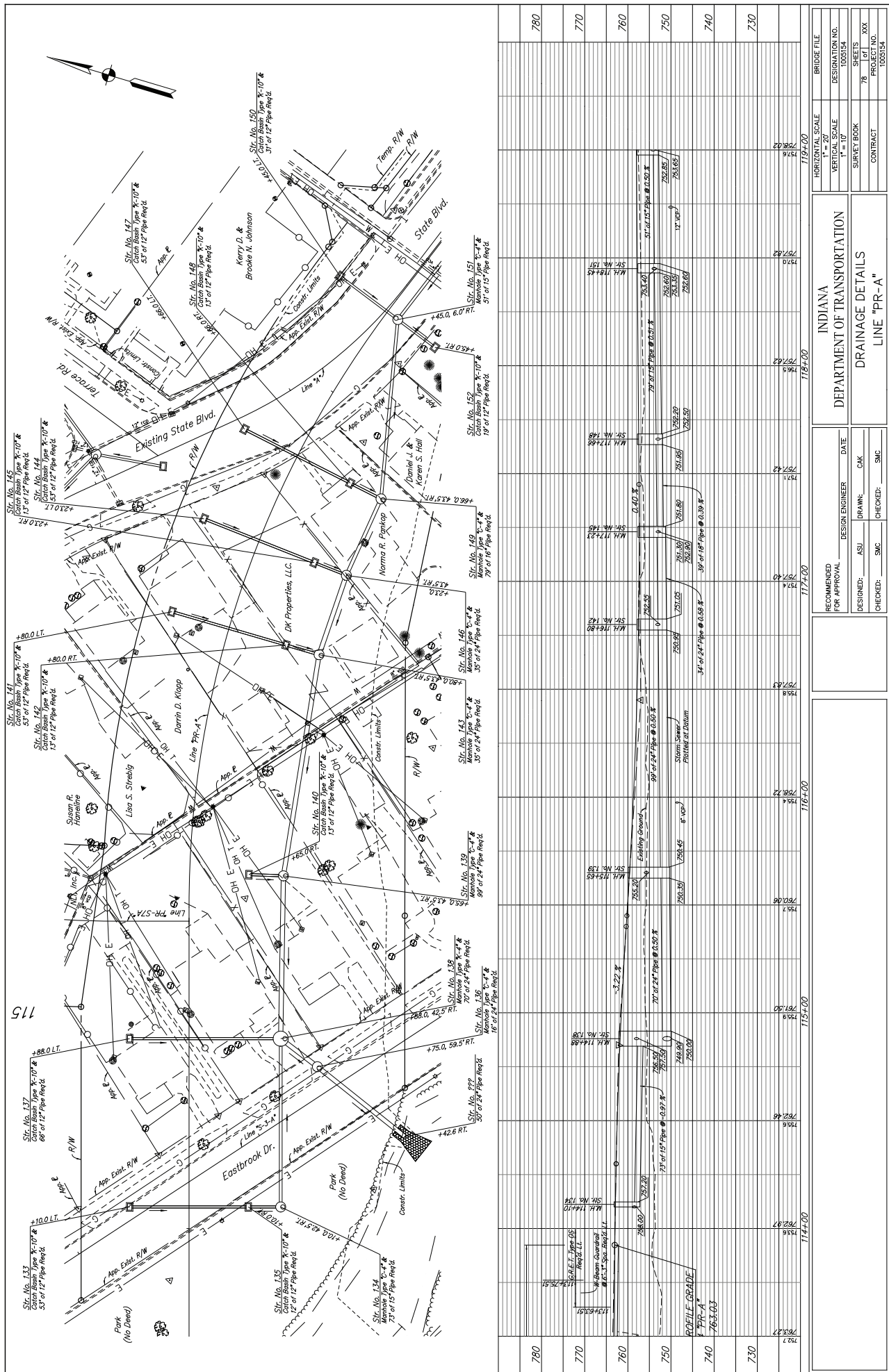
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 - xx' Compacted Aggregate Base, Type "A", No. xx
 - Concrete Sidewalk, 4"
 - Full Depth Pavement (See Typical Sections)
 - Concrete Curb
 - Combined Concrete Curb and Gutter, Modified
 - Concrete Center Curb, Type "B"
 - Sodding, Nursery
 - Mulched Sanding, Type "V"
 - Retaining Wall
 - Concrete Curb Ramp
 - Center Curb Removal Limits



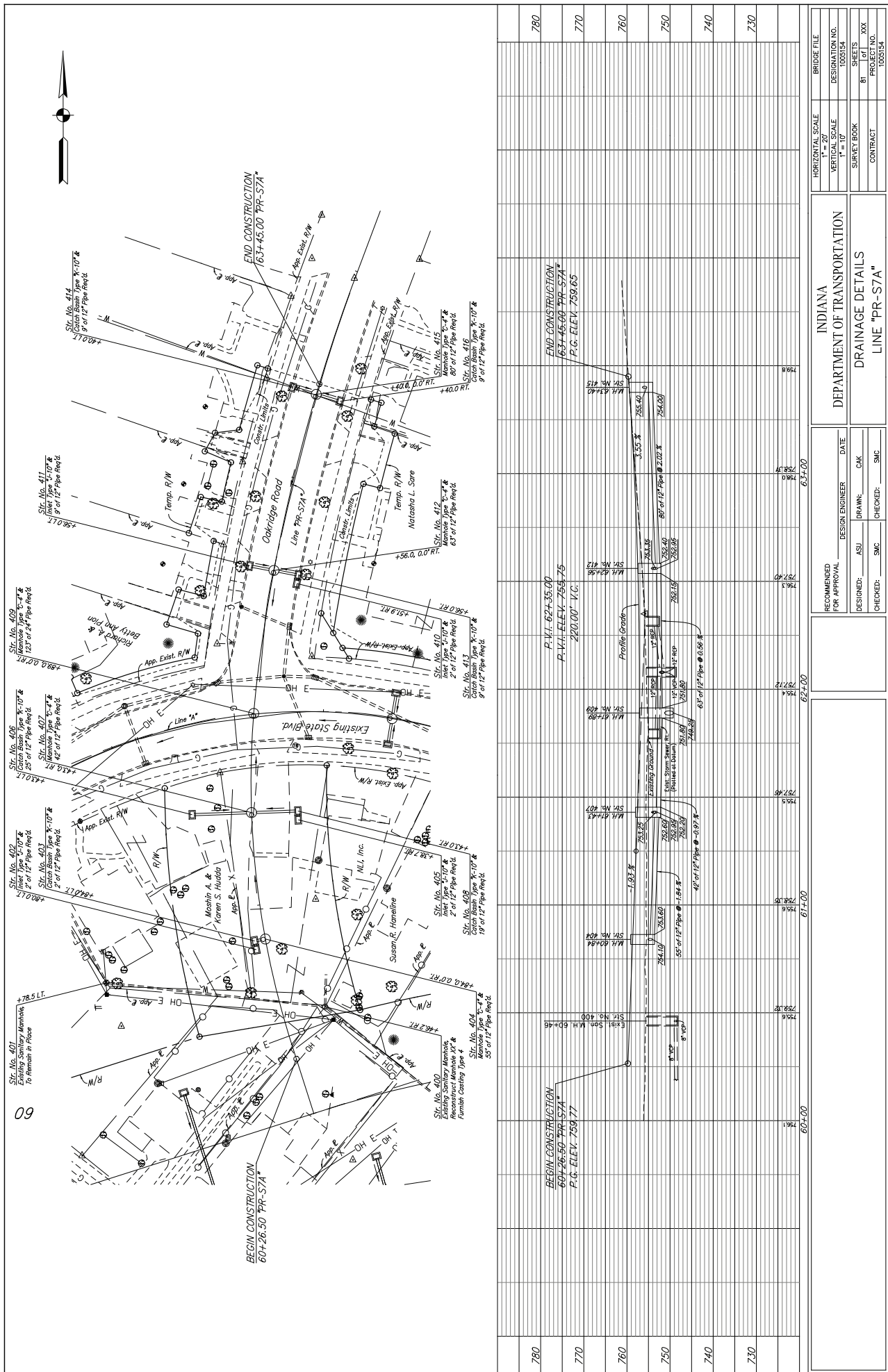
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BRIDGE FILE				DESIGNATION NO.				SHEETS			
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1" = 10'				1" = 10'				76			
PROJECT NO.				CONTRACT				100514			
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FOR APPROVAL				DATE				SIGNED			
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ASU				CAK				SIGNED			
SIGNED				SIGNED				SIGNED			
SIGNED				SIGNED				SIGNED			

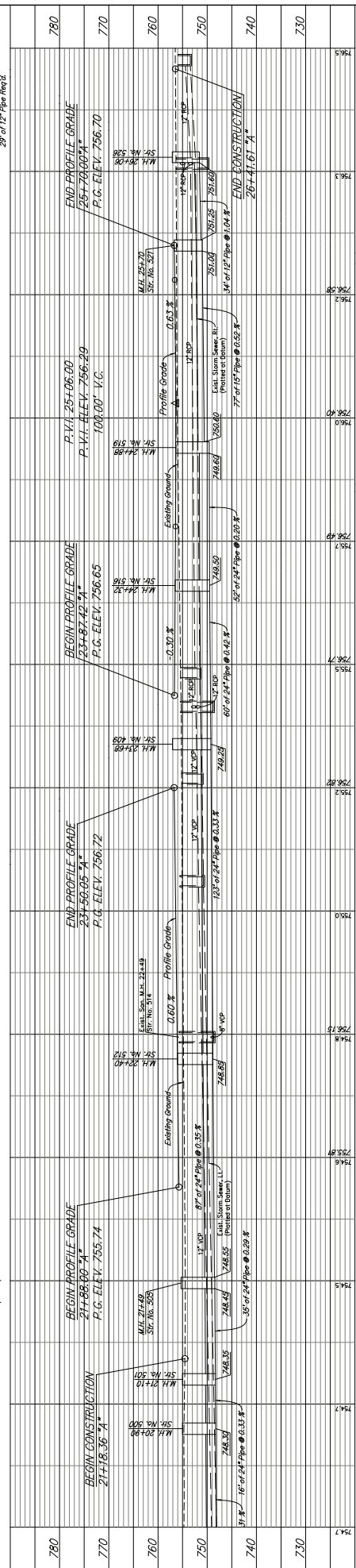
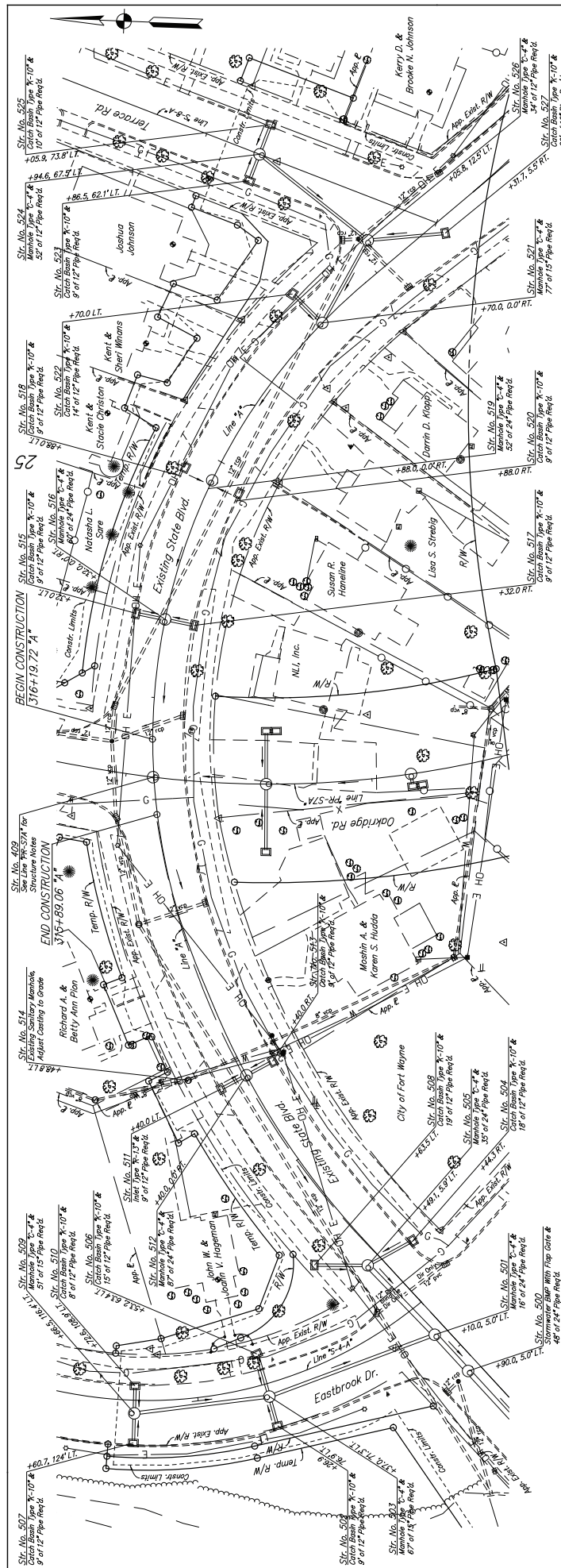


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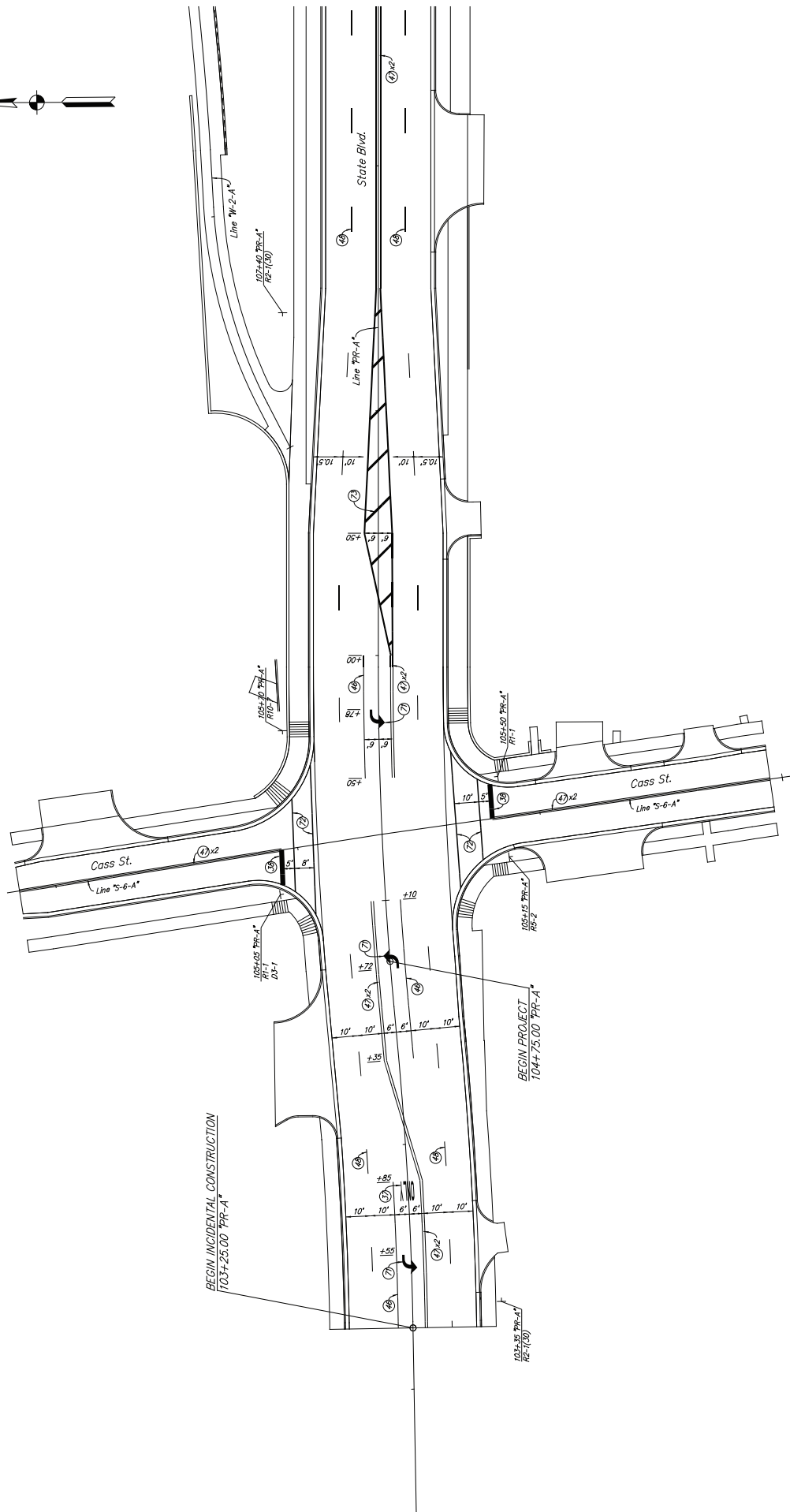
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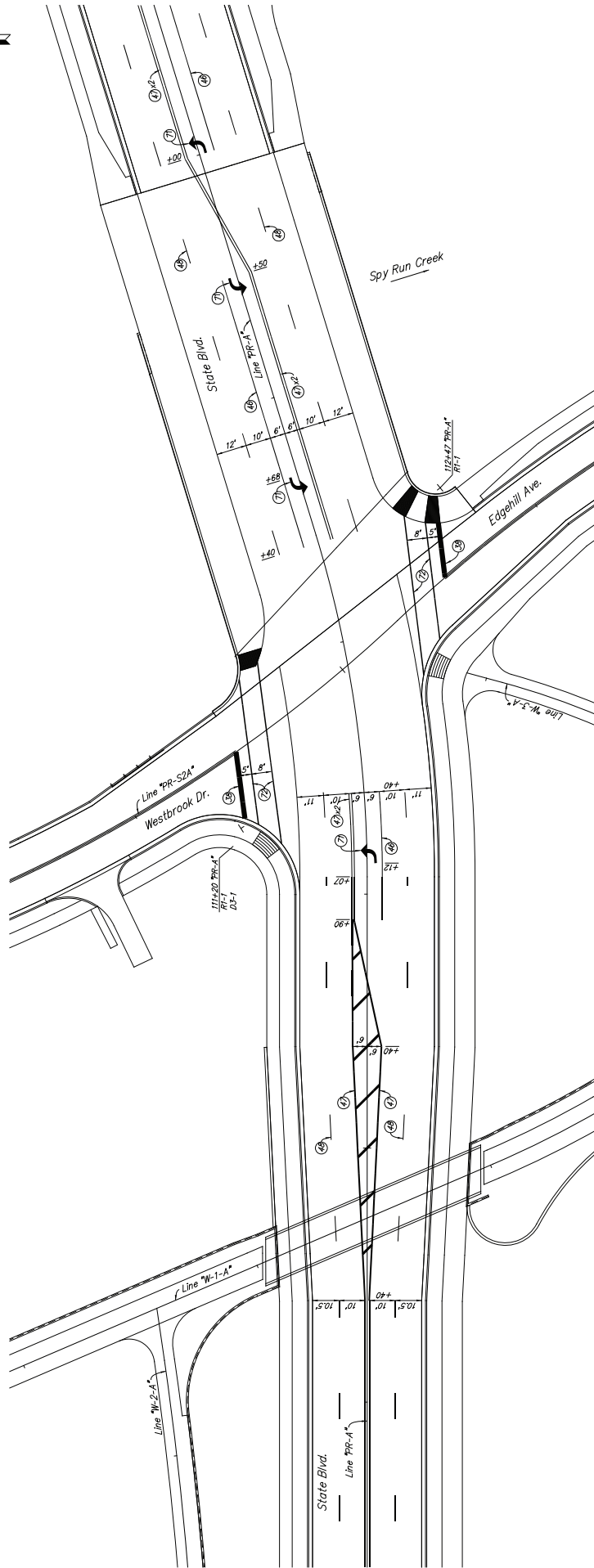


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DATE				ASU				SNC			
DESIGNED				SNC				SNC			
CHECKED				SNC				SNC			
BRIDGE FILE				DESIGNATION NO.				SHEETS			
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VERTICAL SCALE				1" = 10'				CONTRACT			
SURVEY BOOK				82				100514			
PROJECT NO.				100514				LINE "A"			

7/12/2011 12:46:28 PM P:\100514\100514.dwg



<p>① Pavement Message Markings, Thermoplastic, Word (message)</p> <p>② Transverse Markings, Thermoplastic, Broken, White, 4 in.</p> <p>③ Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.</p> <p>④ Line, Thermoplastic, Broken, White, 4 in. (2" Line, 4" Gap)</p> <p>⑤ Line, Thermoplastic, Broken, Yellow, 4 in.</p> <p>⑥ Pavement Message Markings, Thermoplastic, Stop Line, White, 24 in.</p> <p>⑦ Pavement Message Markings, Solid, White, 4 in.</p> <p>⑧ Line, Thermoplastic, Solid, Yellow, 4 in.</p> <p>⑨ Line, Thermoplastic, Thermoplastic, Crosswalk Line, White, 6 in.</p>	<p>⑩ Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.</p> <p>⑪ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)</p> <p>⑫ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)</p>	<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGN ENGINEER _____ DATE _____</p> <p>DESIGNED: _____ ASU DRAWN: _____ PJT</p> <p>CHECKED: _____ SMC CHECKED: _____ SMC</p>	<p>INDIANA</p> <p>DEPARTMENT OF TRANSPORTATION</p> <p>PAVEMENT MARKING & SIGNING DETAILS – LINE "PR-A"</p>	<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE 1" = 100'S</p> <p>SURVEY BOOK _____</p> <p>SHEETS 83 of 100</p> <p>DESIGNATION NO. 100554</p> <p>PROJECT NO. 100554</p> <p>CONTRACT _____</p> <p>BRIDGE FILE _____</p>
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	SMC	CHECKED	SMC	SURVEY BOOK	SHEETS
				CONTRACT	PROJECT NO.
					100514

LEGEND

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(18) Transverse Markings, Thermoplastic, Broken, White, 4 in.

(19) Transverse Markings, Thermoplastic, Stop Line, White, 24 in.

(20) Line, Thermoplastic, Solid, White, 4 in.

(21) Line, Thermoplastic, Solid, Yellow, 4 in.

(22) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

(23) Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.

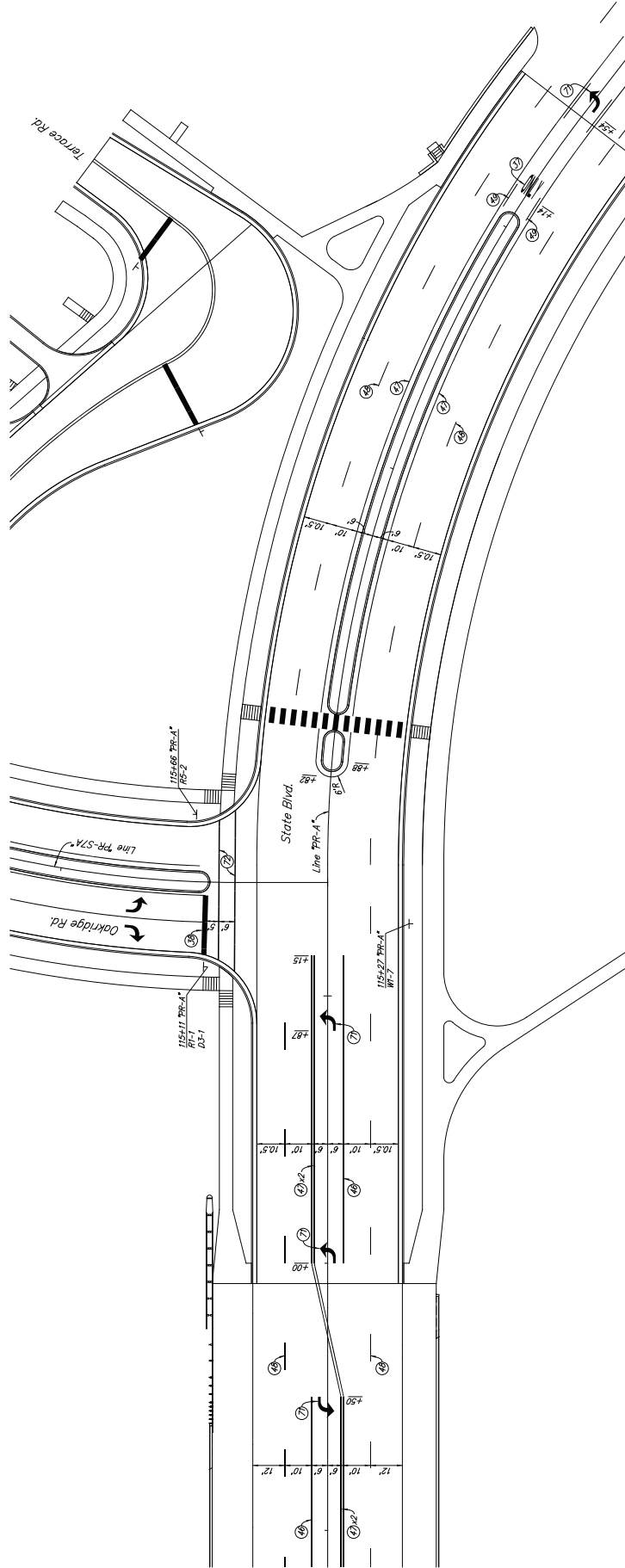
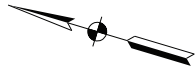
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(25) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)

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scf:tes

115

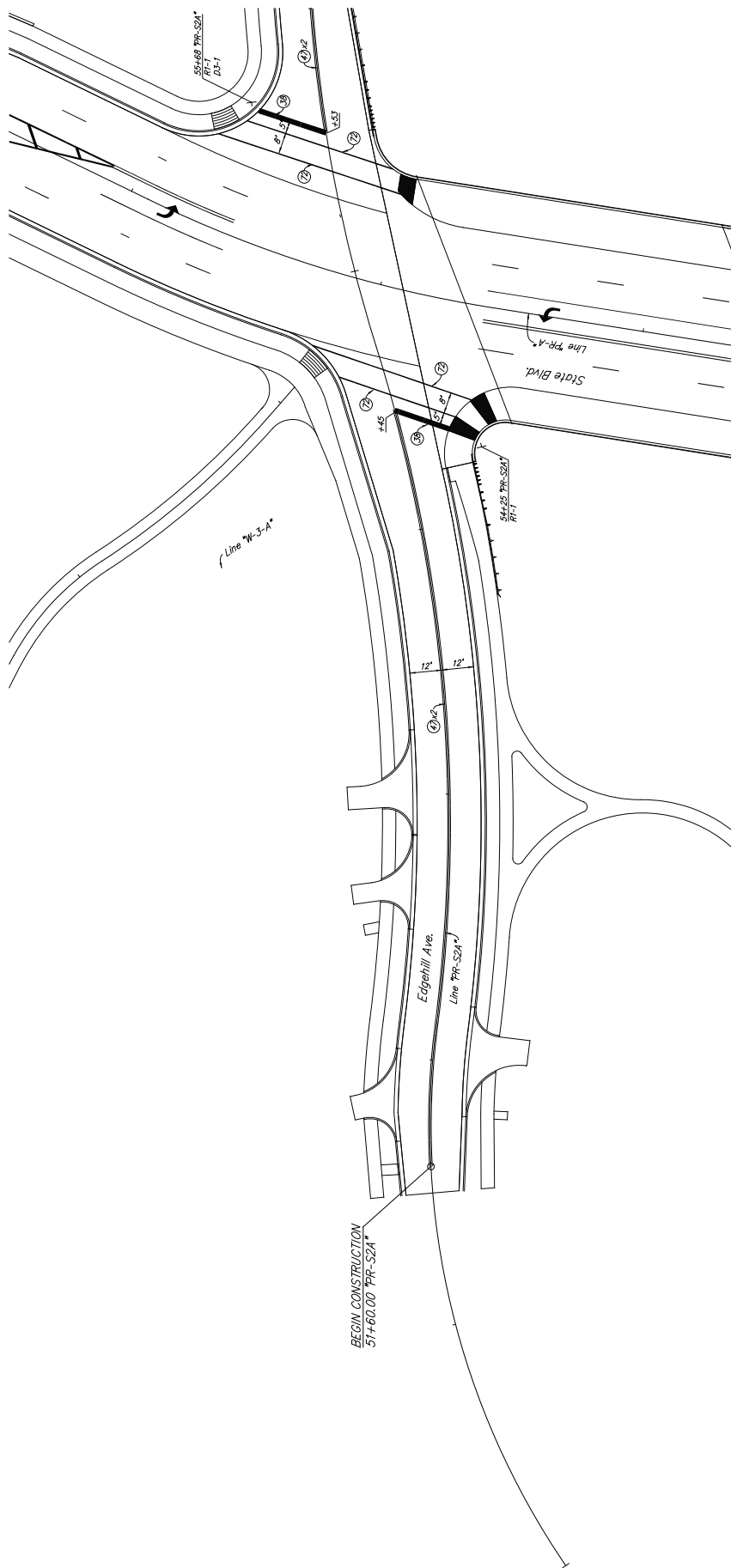
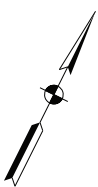


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CHECKED:	SMC	CHECKED:	SMC		
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SIGNING DETAILS - LINE "PR-A"					
HORIZONTAL SCALE		VERTICAL SCALE		BRIDGE FILE	
1" = 40'		1" = 10'		DESIGNATION NO.	
1" = 40'		1" = 10'		1005154	
SURVEY BOOK		CONTRACT		SHEETS	
85		CONTRACT		85	
PROJECT NO.		PROJECT NO.		PROJECT NO.	
1005154		1005154		1005154	

- LEGEND**
- (1) Pavement Markings, Thermoplastic, Word (message), White, 4 in.
 - (2) Line, Thermoplastic, Broken, White, 4 in.
 - (3) Transverse Markings, Thermoplastic, Broken, Yellow, 4 in.
 - (4) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)
 - (5) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)
 - (6) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - (7) Line, Thermoplastic, Solid, White, 4 in.
 - (8) Line, Thermoplastic, Solid, Yellow, 4 in.
 - (9) Pavement Markings, Thermoplastic, Lane Indication Arrow
 - (10) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.

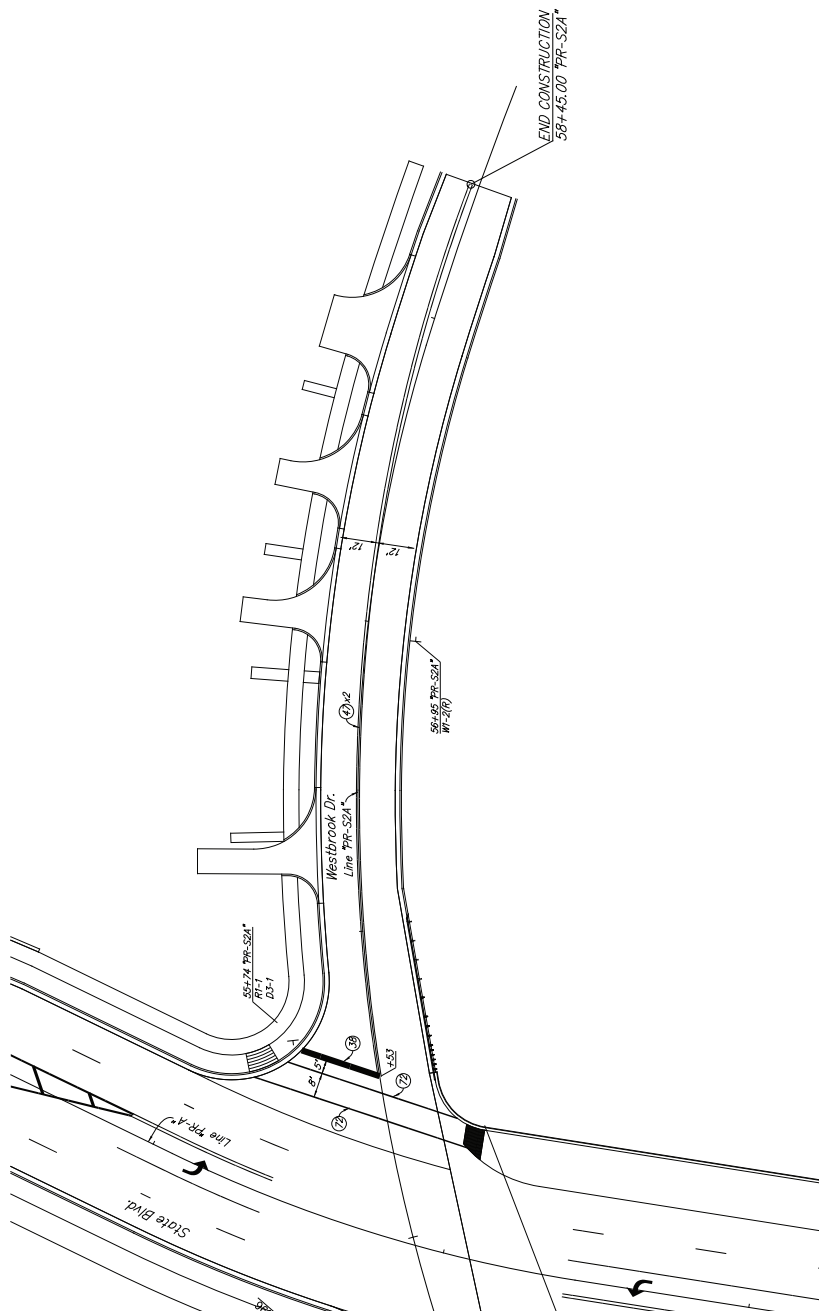
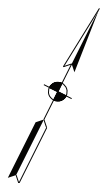
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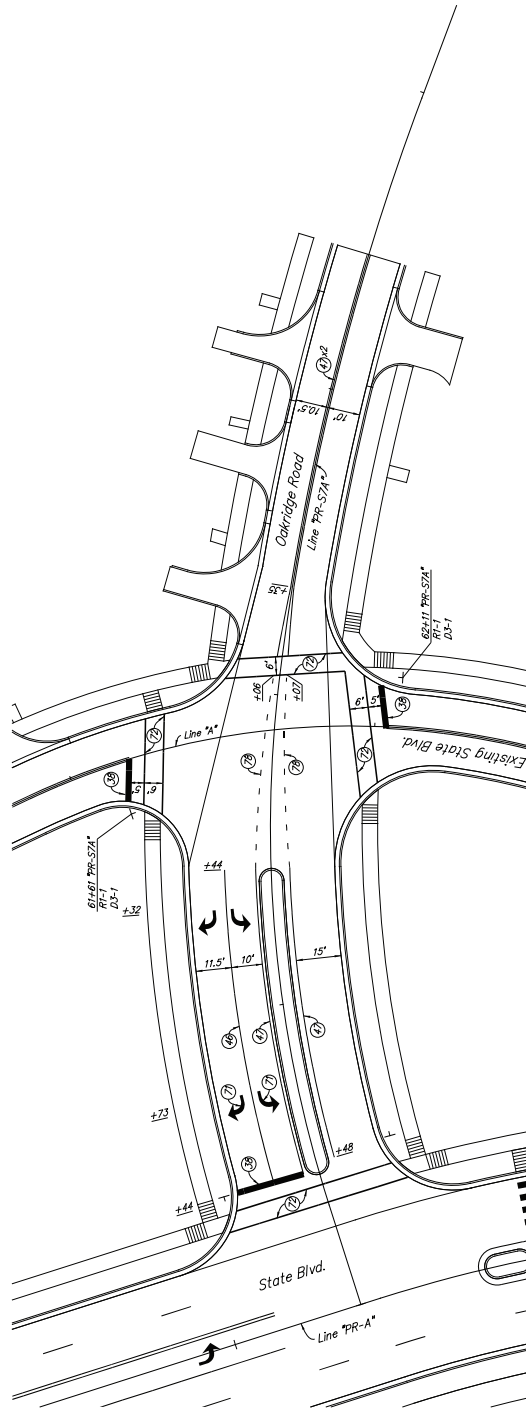
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				86	
				of	
				XXX	
				PROJECT NO.	
				100514	

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 - (3) Pavement Message Markings, Thermoplastic, Solid, White, 4 in.
 - (4) Line, Thermoplastic, Solid, Yellow, 4 in.
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 - (6) Line, Thermoplastic, Solid, Yellow, 4 in.
 - (7) Pavement Message Markings, Thermoplastic, Lane Indication Arrow, White, 6 in.
 - (8) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - (9) Transverse Markings, Thermoplastic, Broken, White, 4 in.
 - (10) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Cap)
 - (11) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
 - (12) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.

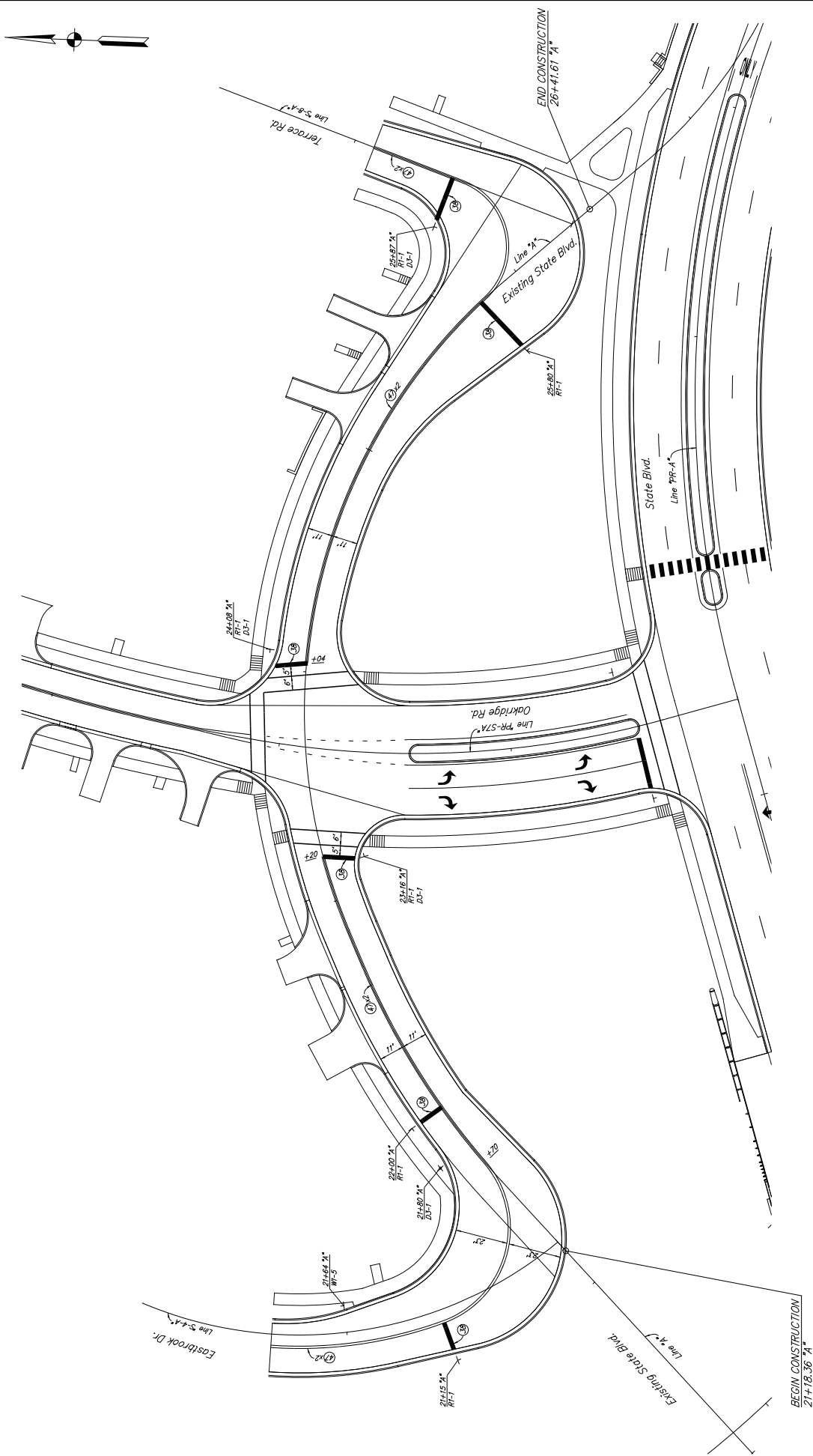


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RECOMMENDED FOR APPROVAL		CONTRACT	
DESIGNED: ASU		PROJECT NO. 100514	
CHECKED: SMC		SHEETS	
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 - (5) Line, Thermoplastic, Solid, Yellow, 4 in.
 - (6) Line, Thermoplastic, Solid, Yellow, 4 in.
 - (7) Pavement Message Markings, Thermoplastic, Lane Indication Arrow
 - (8) Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
 - (9) Transverse Markings, Thermoplastic, Crosshatch Line, (Yellow), 12 in.
 - (10) Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)
 - (11) Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)



INDIANA DEPARTMENT OF TRANSPORTATION		BRIDGE FILE	
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② Transverse Markings, Thermoplastic, Stop Line, White, 24 in.	⑤ Line, Thermoplastic, Broken, Yellow, 4 in.	④ Line, Thermoplastic, Dotted, White, 4 in. (2" Line, 4" Gap)	
③ Line, Thermoplastic, Solid, White, 4 in.	⑥ Pavement Message Markings, Thermoplastic, Lane Indication Arrow	⑤ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Gap)	
④ Line, Thermoplastic, Solid, Yellow, 4 in.	⑦ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.		



INDIANA
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING &
SIGNING DETAILS - LINE "A"

RECOMMENDED FOR APPROVAL

DESIGNED:	ASU	DRAWN:	PJT
CHECKED:	SMC	CHECKED:	SMC

DATE

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	1005154
CONTRACT	SHEETS
	89 of 100
	PROJECT NO.
	1005154

LEGEND

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- ② Line, Thermoplastic, Broken, White, 4 in.
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- ④ Line, Thermoplastic, Broken, Yellow, 4 in.
- ⑤ Line, Thermoplastic, Solid, White, 4 in.
- ⑥ Pavement Message Markings, Thermoplastic, Lane Indication Arrow, White, 6 in.
- ⑦ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
- ⑧ Line, Thermoplastic, Dotted, Yellow, 4 in. (2" Line, 4" Cap)
- ⑨ Transverse Markings, Thermoplastic, Crosswalk Line, White, 6 in.
- ⑩ Transverse Markings, Thermoplastic, Crosswalk Line, (Yellow), 12 in.

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STATE BLVD., EAST VIEW TOWARD SPY RUN AVE.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., WEST VIEW TOWARD PROPOSED STATE AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., SOUTHWEST VIEW OF THE NEW EXTENSION AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., NORTH VIEW TOWARD HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., EAST VIEW TOWARD NEW BRIDGE OVER SPY RUN CREEK



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., WEST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE

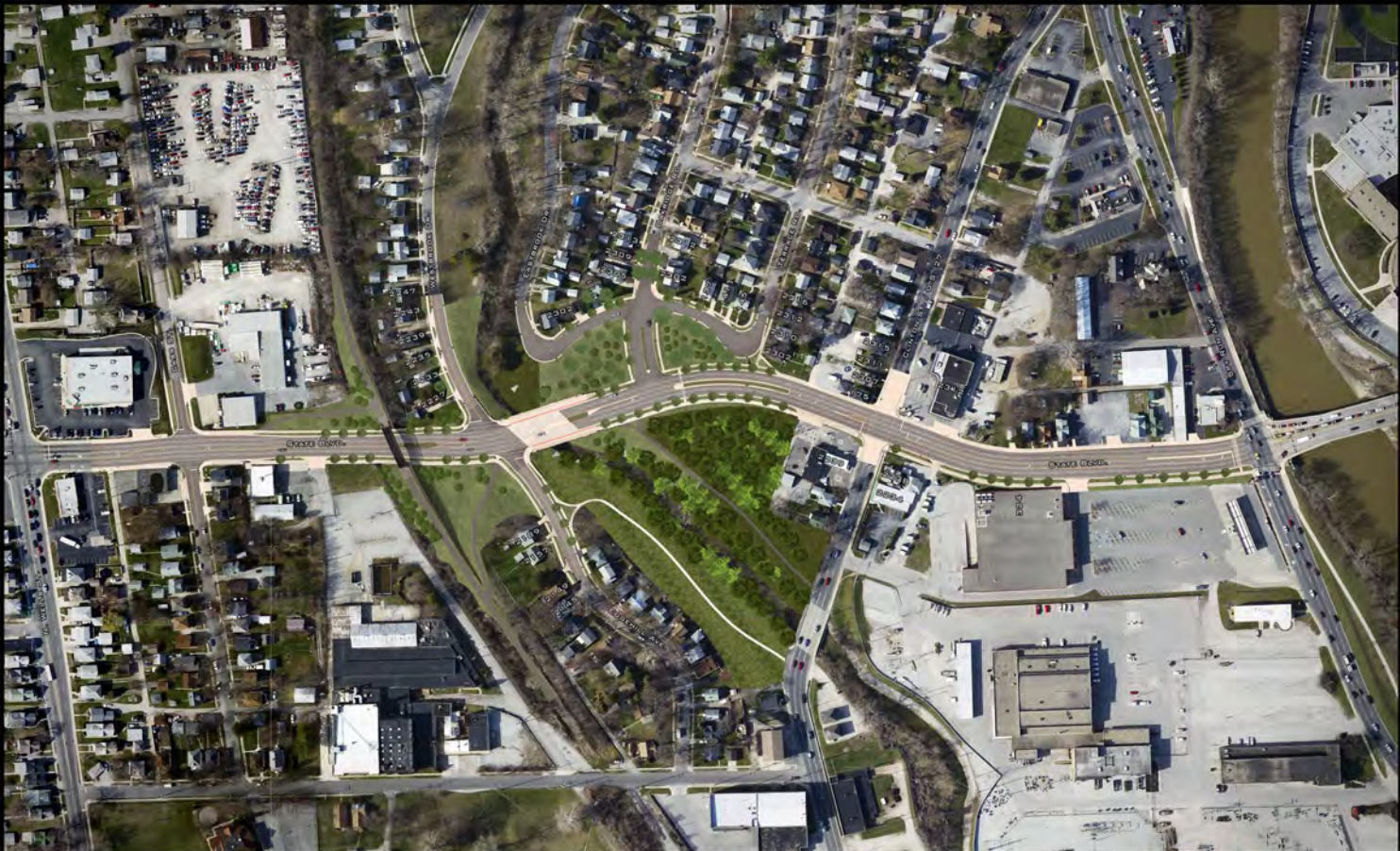


AMERICAN
STRUCTUREPOINT
INC.

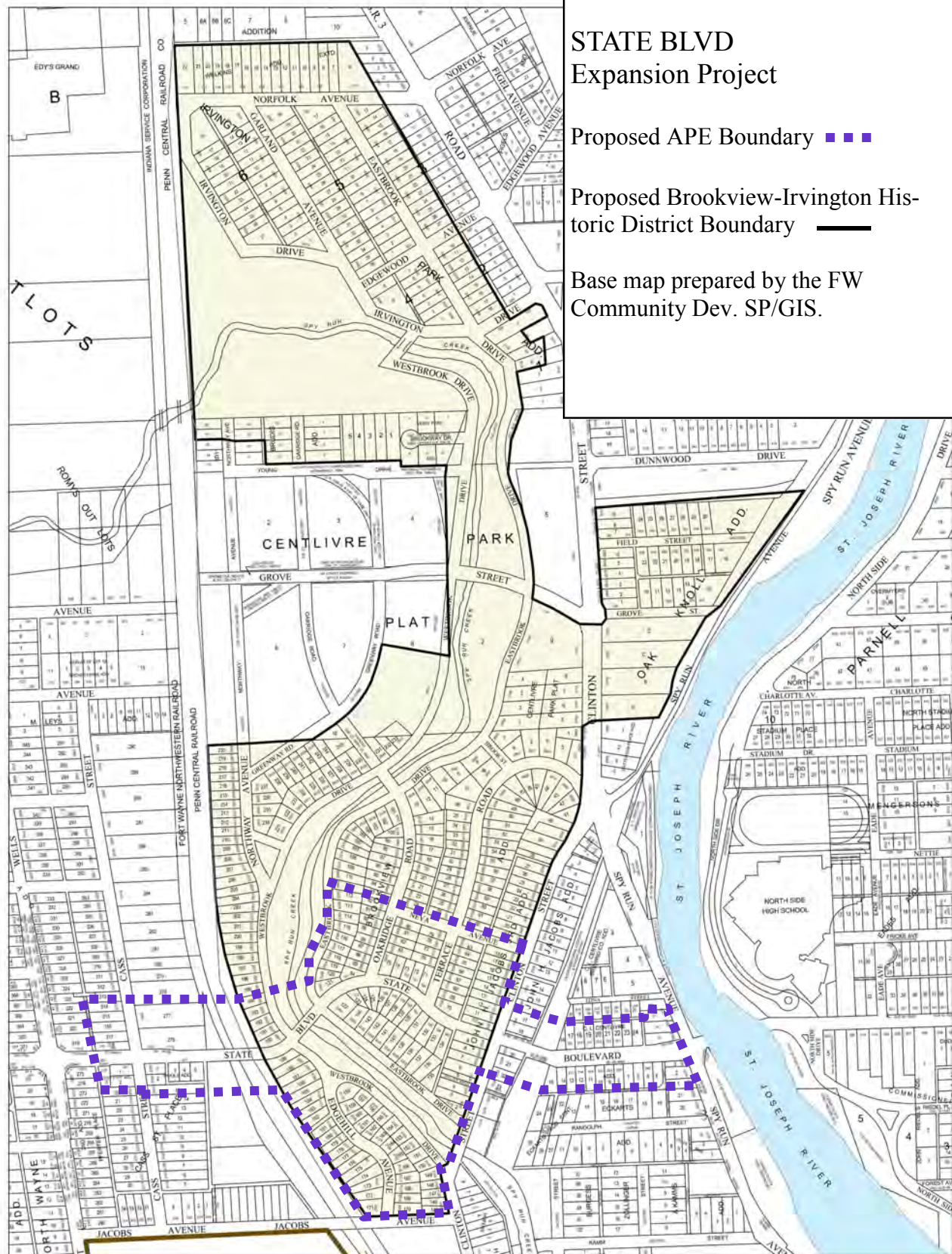


STATE BLVD., EAST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE

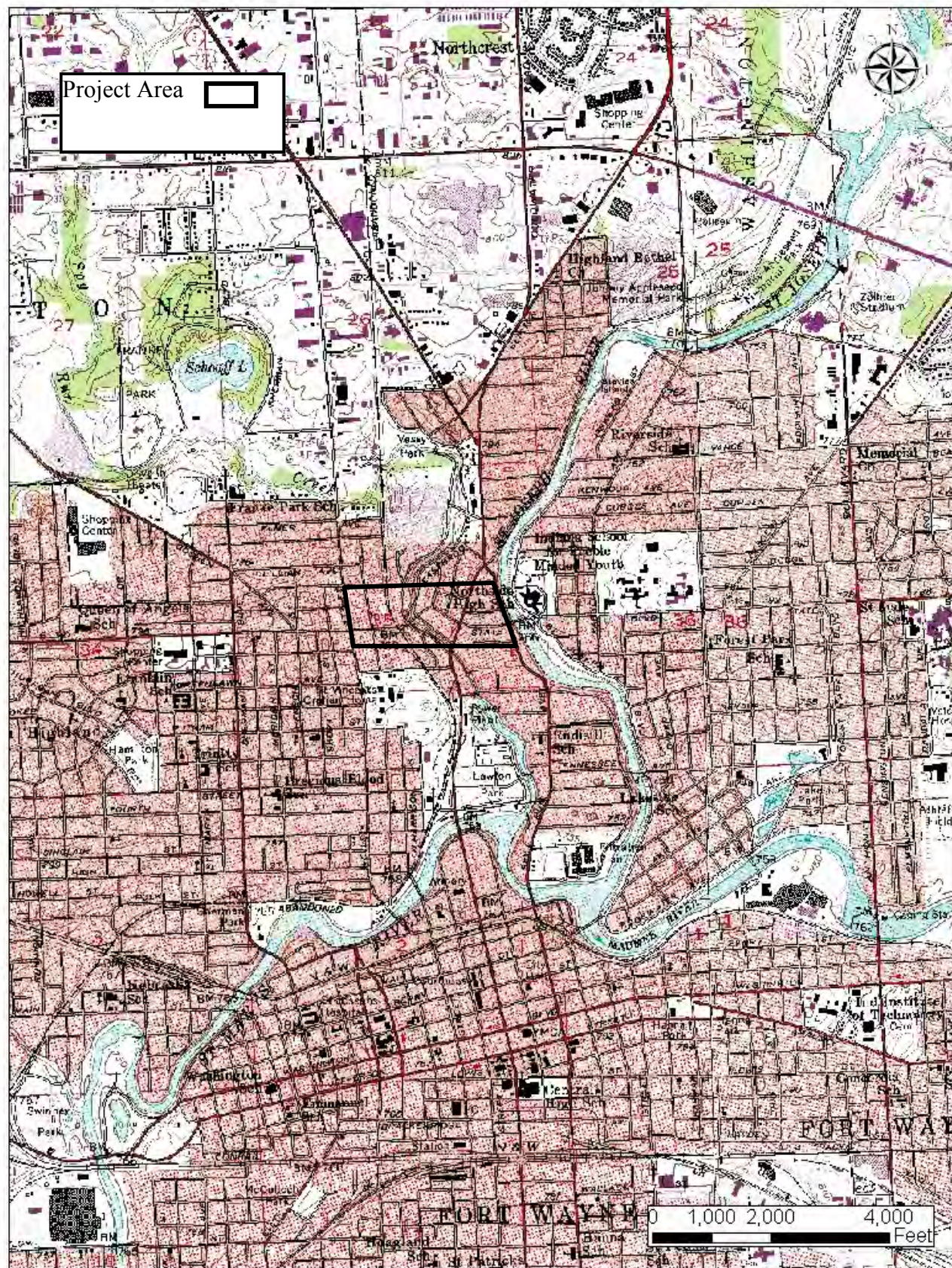


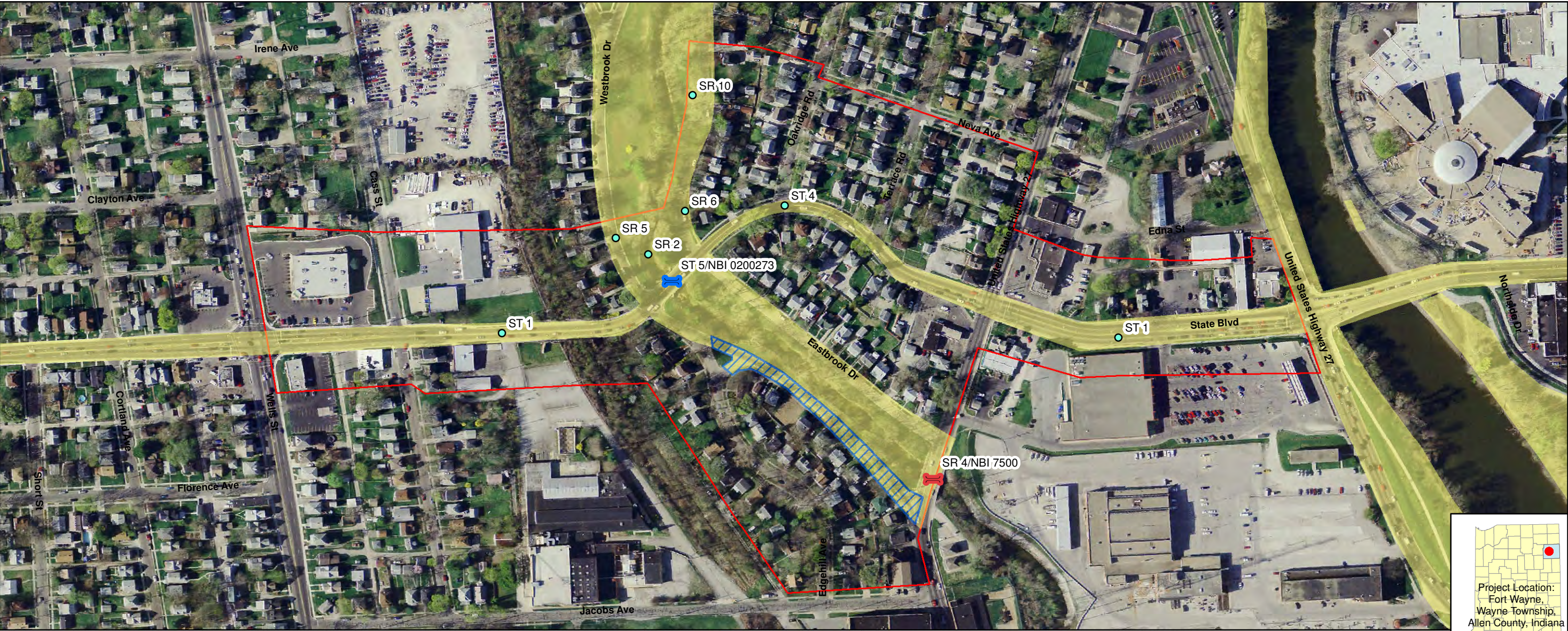


APPENDIX B. Maps



USGS Map





0

250

500

1,000 Feet

Legend

APE

Contributing to FWPB

Non-Contributing Bridge (FWPB)

Determined NR-eligible Bridge

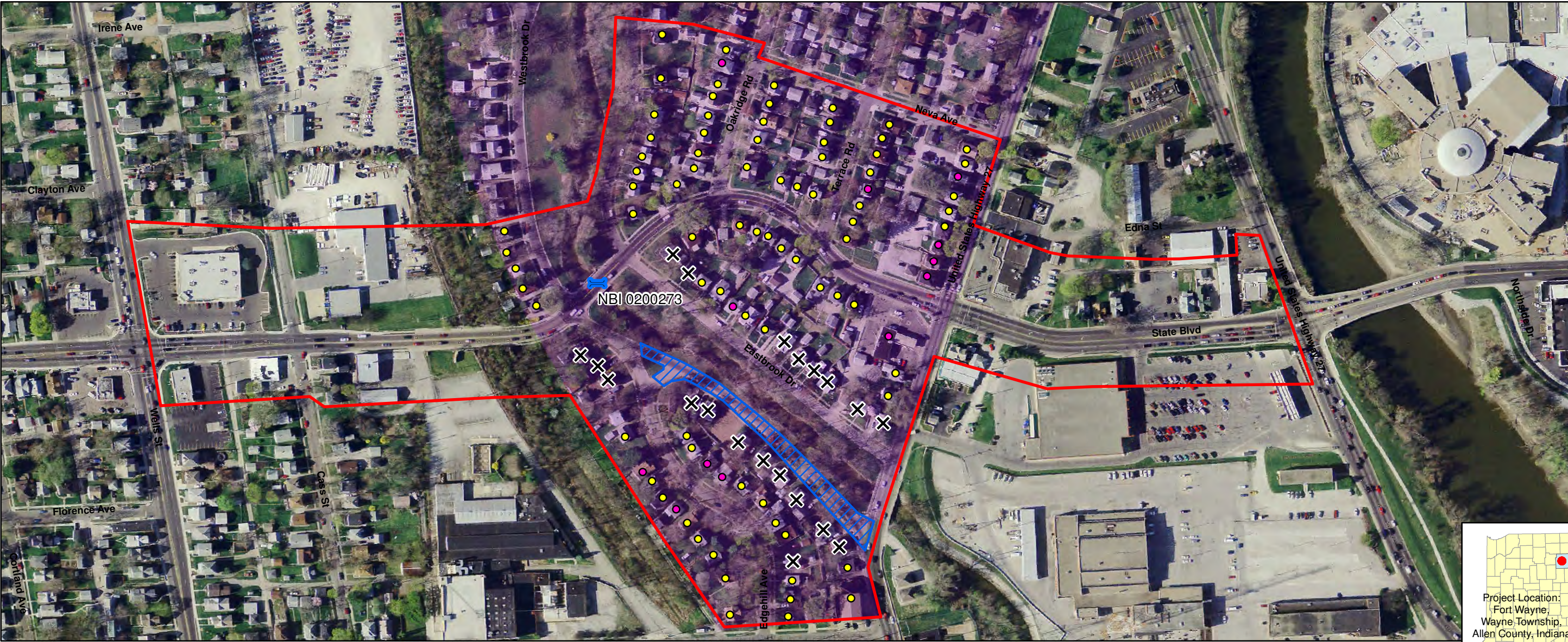
Road Pavement Removed

FWPB Boundary

Project Location:
Fort Wayne,
Wayne Township,
Allen County, Indiana

Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Appendix C
Page 130 of 490



Legend

APE

BIHD

Road Pavement Removed

Determined NR-eligible Bridge

Contributing to BIHD

Non-Contributing to BIHD

No Longer Extant

Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Appendix C
Page 137 of 499

APPENDIX C. Consulting Parties

Individuals or Groups Invited to Join Section 106 Consultation

Indiana State Historic Preservation Officer (SHPO)	ARCH, Inc.
Indiana Department of Transportation (INDOT)	Fort Wayne Historic Preservation Review Board
INDOT—Fort Wayne District,	John Shoaff, Fort Wayne city council member
City of Fort Wayne Engineer	Friends of the Parks of Allen County
Historic Landmarks Foundation of Indiana (now Indiana Landmarks)	Brookview Neighborhood Association
Allen County Historian	Dr. James L. Cooper
Allen County—Fort Wayne Historical Society	Paul Brandenburg, Historic Spans Taskforce

Individuals or Groups Accepting the Invitation to Join Section 106 Consultation, Requesting Consulting Party Status, or Commenting on Project

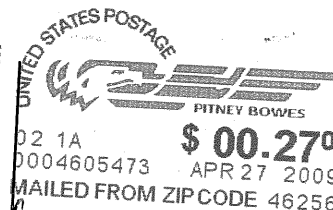
Indiana SHPO	
City of Fort Wayne	Indiana Historic Spans Taskforce
Allen County Historian	Irvington Park Neighborhood Association
Friends of the Parks of Allen County	Charley Shirmeyer, Northside Galleries
Indiana Landmarks—Northern Regional Office	Albert Cohan, Westbrook 5, LLC
Fort Wayne Historic Preservation Commission	Thomas Niezer, Barret & McNagny, LLP
ARCH, Inc.	Ronald Ross, Martin Riley Architects and Engineers
Brookview Neighborhood Association	Dan Ernst, Earth Source, Inc.
	Jan Dailey, State Boulevard Resident

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction
AGENCY CITY OF FORT WAYNE
WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐
CONTACT PERSON SHAN GUNAWARDENA
ADDRESS ONE EAST MAIN ST Rm. 750
FORT WAYNE IN 46802
PHONE (260) 427-6169 FAX (260) 427-1269
E-MAIL shan.gunawardena@ci.ftwayne.in.us DATE 06/01/09

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction
AGENCY Friends of the Parks of Allen Co
WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐
CONTACT PERSON Julie Donnell
ADDRESS 3604 South Washington
Fort Wayne IN
46802
PHONE 260-932-7178 FAX _____
E-MAIL juliemarie57e DATE 5/22/09
earthlink.net



Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

REC'D MAY 28 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY _____

WILL PARTICIPATE ☐

WILL NOT PARTICIPATE ☒

CONTACT PERSON Tom Castaldi

ADDRESS 13707 Black Hollow Ct
FT Wayne
IND 46814

PHONE _____

FAX _____

E-MAIL _____

DATE 3-27-9

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Historic Landmarks Foundation of Indiana

WILL PARTICIPATE ☒

WILL NOT PARTICIPATE ☐

CONTACT PERSON Bob Zeiger

ADDRESS 402 W. Washington
South Bend, IN
46601

PHONE 574-232-4534

FAX 574-232-5549

E-MAIL tzeiger@

DATE 4-13-09

historiclandmarks.org

Northern office

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Ft. Wayne Historic Preservation Commission

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON DON ORBAN

ADDRESS One E. Main St. Rm 800
FORT WAYNE, IN 46802

PHONE 260-427-2160 FAX 260-427-1132

E-MAIL don.orban@cityoffortwayne.org DATE 4-2-09

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY ARCH, Inc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Angie Quinn + Michael Galbraith

ADDRESS ARCH, Inc.
437 E. Berry St. Suite 204
Fort Wayne, IN 46802

PHONE 260-426-5117 FAX Same

E-MAIL Aquinn@archfw.org DATE 3/26/09
mgalbraith@archfw.org

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Brookview Neighborhood Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Michelle Briggs Wedaman

ADDRESS 2326 Eastbrook Drive

FT. WAYNE IN

260-480-0120 46805

PHONE 260-710-4413 FAX

E-MAIL DATE 5/1/09

mbwedaman@verizon.net

UNITED STATES POSTAGE
PITNEY BOWES
02 1A \$ 00.27⁰
0004605473 APR 27 2009
MAILED FROM ZIP CODE 46256

Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 462

REC'D MAY 4 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY INDIANA HISTORIC SPANNS TASCORP

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON PAUL BRANDENBURG

ADDRESS 5868 CROWN CIRCLE
INDIANAPOLIS IN 46254

PHONE 317-347-1204 FAX 317-347-1206

E-MAIL DATE 1-11-2009

INDIANABREDCRS @ 5136601341, 1121

UNITED STATES POSTAGE
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Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

REC'D MAY 4 2009

Section 106 Consultation

PROJECT ID State Boulevard Reconstruction

AGENCY Irvington Park Nbrhd Assoc.

WILL PARTICIPATE ☒ WILL NOT PARTICIPATE ☐

CONTACT PERSON Suzanne Slick

ADDRESS 3318 Garland Ave

Fr. Wayne

Ind

46805

PHONE 260-482-3991 FAX

E-MAIL sjslick@mac.com

DATE 7/9/09

Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

APPENDIX D. Photographs

STATE BOULEVARD RECONSTRUCTION
PHOTOGRAPHS - Page One Map A
DHPA - INDOT Des. No. 0400587



Photo 001



Photo 002



Photo 003



Photo 004

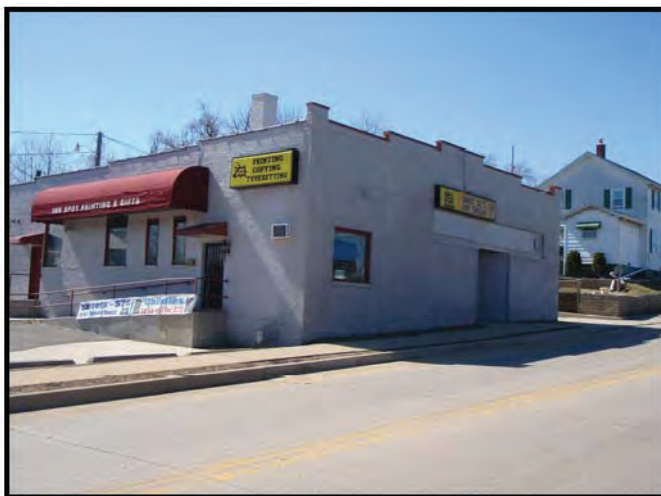


Photo 005

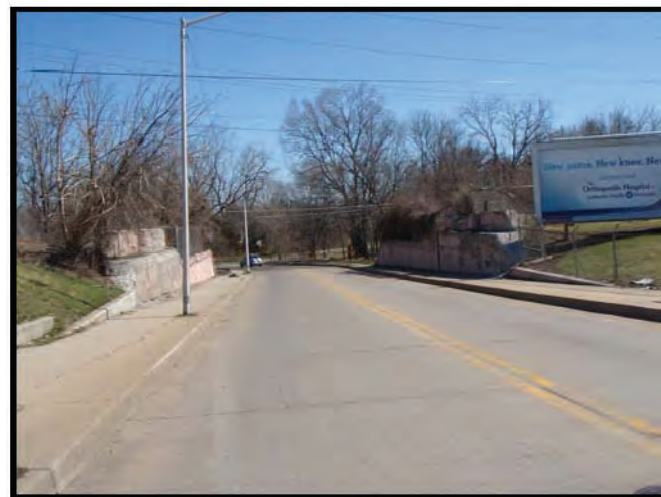


Photo 006



Photo 001A



Photo 001B



Photo 002A



Photo 002B



Photo 007



Photo 008



Photo 009



Photo 010



Photo 011



Photo 012



Photo 008A



Photo 008B



Photo 008C

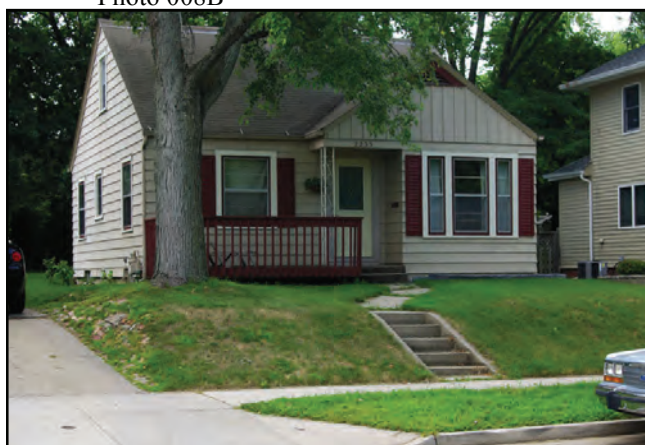


Photo 008D



Photo 008E



Photo 010A



Photo 010B



Photo 010C



Photo 010D



Photo 011A



Photo 011B



Photo 012A



Photo 012B

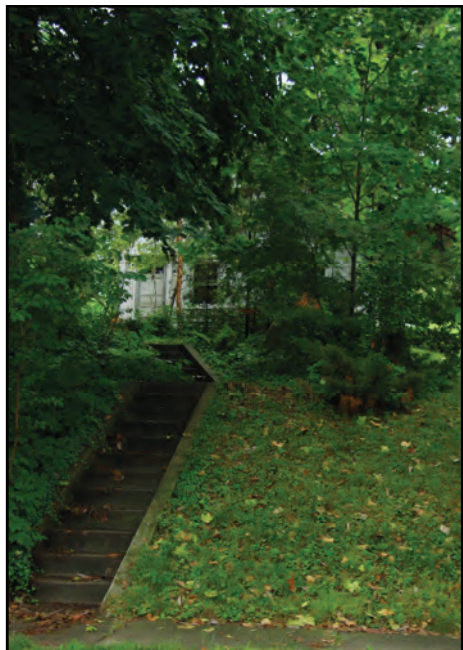


Photo 012C



Photo 012D



Photo 012E



Photo 013



Photo 13A



Photo 13B



Photo 014



Photo 015



Photo 016



Photo 017



Photo 018



Photo 019



Photo 020



Photo 021



Photo 021A

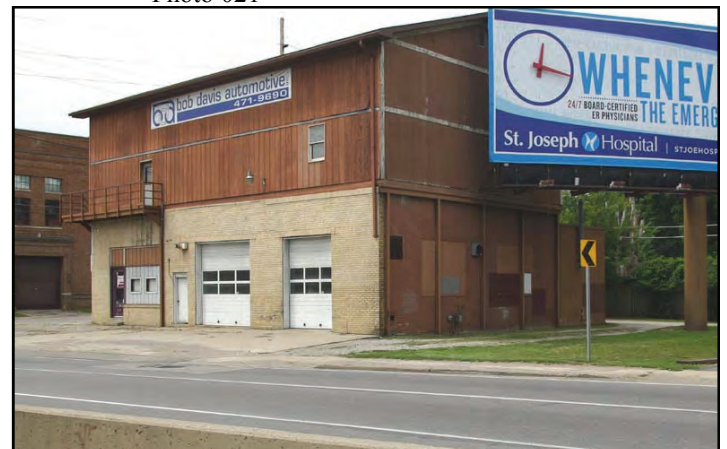


Photo 021B



Photo 022



Photo 023



Photo 024



Photo 025



Photo 026



Photo 027



Photo 025A



Photo 025B



Photo 027A



Photo 027B



Photo 027C



Photo 028



Photo 029



Photo 030



Photo 031



Photo 032



Photo 033



Photo 034



Photo 035



Photo 036



Photo 037



Photo 038



Photo 039



Photo 040



Photo 041



Photo 042



Photo 043



Photo 044



Photo 045



Photo 040A



Photo 040B



Photo 040C



Photo 040D



Photo 040E



Photo041A



Photo 042A



Photo 042B



Photo 042C



Photo 042D



Photo 042E



Photo 044A



Photo 044B



Photo 044C



Photo 044D



Photo 044E



Photo 045A



Photo 046



Photo 047



Photo 048



Photo 049



Photo 050



Photo 051



Photo 052



Photo 053



Photo 054



Photo 055



Photo 056



Photo 057



Photo 058



Photo 059



Photo 060



Photo 061



Photo 062



Photo 063



Photo 064



Photo 065



Photo 066



Photo 067



Photo 068

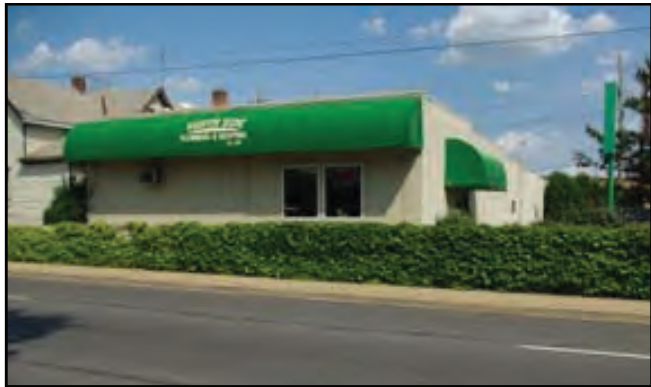


Photo 066A



Photo 067A



Photo 067B



Photo 069



Photo 070



Photo 071



Photo 071A



Photo 071B



Photo 071C



Photo 072



Photo 073



Photo 074



Photo 075



Photo 076



Photo 077



Photo 078



Photo 079



Photo 080



Photo 081



Photo 082



Photo 083



Photo 084



Photo 085



Photo 086



Photo 087



Photo 088



Photo 089



Photo 090



Photo 091



Photo 092



Photo 093



Photo 094



Photo 095



Photo 096



Westbrook looking SE to Edgehill Ave



Westbrook looking NW to State Blvd showing street light



Westbrook looking SE showing filled portion of street and curb



Westbrook looking SE between Edgehill Ave and Spy Run



Looking NE from 2157 Edgehill Ave



Looking SE from 2157 Edgehill Ave



Looking SE from 2232 Eastbrook Dr



SR 4 NC Clinton St Bridge



Looking S to State Blvd from 2320 Oakridge Rd



Looking N to Neva from 2320 Oakridge Rd



Looking SE to State Blvd from Oakridge Rd



Looking SW to State Blvd from Oakridge Rd



Looking SW to Spy Run Bridge from State Blvd



Looking NE to State Blvd from Eastbrook Dr



Looking NE to Eastbrook Dr from State Blvd



Looking SE from 2342 Eastbrook Dr



Terrace Rd looking S to State Blvd



Looking NW from Neva to Oakridge Rd



Looking SW from Neva Ave to Clinton St



Looking W along State Blvd



Looking E across State Blvd



Spy Run Bridge_ST 5_N elevation



Spy Run Bridge_ST 5_Bridge deck



Looking E to Interurban abutments along State Blvd



Looking W along State Blvd from Cass Street



Looking W along State Blvd from Clinton



Looking NW at State Blvd and Spy Run Avenue



09014_Looking NW across State Blvd to 315 E State Blvd



Looking E from 315 W State Blv



Looking W along State Blvd from 315 W State State Blvd



State at Wells looking E into the APE



Spy Run Bridge_ST 5_rail N elevation



09014_315 E State Blvd S and E elevation 6

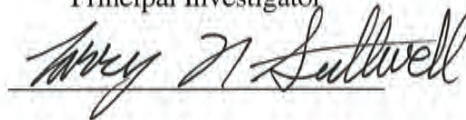
APPENDIX E. Report Summaries

**An Archaeological Field Reconnaissance of the Proposed State Boulevard
Improvements (Des No. 0400587) in Ft. Wayne, Allen County, Indiana**

By

Larry N. Stillwell

Principal Investigator

A handwritten signature in black ink, reading "Larry N. Stillwell", is written over a horizontal line.

Submitted by:

Archaeological Consultants of Ossian

P.O. Box 2374

Muncie, IN 47307

Submitted to:

American Structurepoint, Inc.

Indianapolis, IN

April 2, 2009

Archaeological Consultants of Ossian *Cultural Resource Management Report 09FR55*

Abstract

On March 18, 2009, personnel from Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey of the proposed State Boulevard improvements (Des. No. 0400587) in Ft. Wayne, Allen County, Indiana. The project area totaled approximately 6.43 acres, and was located in an urban setting by the St. Joseph River. Soils within the project area varied from well drained to very poorly drained. Shovel testing and pedestrian walkover survey combined with auger testing was unable to locate any cultural resources within the project limits, and the field reconnaissance determined that the project area was disturbed. It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places. Thus, no further archaeological work is warranted. Project clearance is recommended.

Historic Property Report

State Boulevard Reconstruction

From

Spy Run to Cass Street

Fort Wayne, Allen County



INDOT Des. No. 0400587
Federal Project No.
Prepared for:
StructurePoint
Project No. IN20071404

Prepared by
The Westerly Group, Inc.
Camille B. Fife, Principal Investigator
225 E. Main St., Madison, IN 47250
(812) 273-8826 (F) (812) 265-9606;
wgimadison@aol.com
September, 2009

Executive Summary

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County. Since the project is receiving funding from the Federal Highway Administration (FHWA), it is therefore subject to Section 106 review. More specifically the project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. A new alignment in the section between Westbrook Drive and Terrace Road will provide enhanced roadway safety. The project also includes replacing a bridge over Spy Run Creek and adding a new trail bridge. This report was prepared using information from local and state government repositories, historical societies, local organizations, library, newspapers, and site inspection.

Above-ground resources were identified and evaluated in accordance with Section 106, national Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (Revised January 2001) and Final Rule of Revision of Current Regulations, dated December 12, 2000, and incorporating amendments effective August 4, 2004. As a result of the NHPA, as amended, and CFR Part 800 (Revised January 2001), federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic Properties include buildings, structures, sites, objects, and/or districts. The report follows the Indiana Department of Transportation's (INDOT) Cultural Resource Manual (CRM) guidelines for preparing Historic Property Reports (HPR.)

The project passes through the proposed Brookview-Irvington Park National Register (NR) Historic District. Thirty-eight properties within the proposed historic district were previously surveyed in the Fort Wayne Interim Report(See Table I.) The district boundary encompasses neighborhoods which are significant examples of community planning. The district is eligible for the National Register of Historic Places (NRHP) under criterion C, and criterion A.¹ Taking into account that any effects within part of the district can be considered to effect the entire district, the proposed Area of Potential Effect (APE) for the project has been defined as an area of concentration in the southern portion of the district. This concentrated APE represents the part of the district within which potential alternative alignments for the relocation of State Boulevard might occur. Ninety-four resources were surveyed within this area including two structures. A number of buildings within the APE in the district and one bridge may be impacted by the project, depending upon the preferred alignment. The bridge over Spy Run Creek which is located within the district has been previously determined eligible for listing in the NRHP and contributes to the district.²

Twenty buildings and two structures outside the district, but within the APE for this project, were surveyed. Only one of these buildings was recommended eligible for the NRHP. The section of State Boulevard, outside the historic district was reviewed and recommended not eligible for the NRHP.

This photo was probably taken soon after the State Boulevard overpass was completed in 1927. Both the railroad and the interurban ran above the depressed State Boulevard. Source: Berndt, C. *The Toledo & Chicago Interurban Railway Company, Its Predecessor and Successors.*) **Figure 15**





Historic Property Report
State Boulevard Reconstruction from Spy Run to Cass Street
Additional Information Report
DES No.: 0400587

Prepared for
**Indiana Department of Transportation/
Federal Highway Administration**

Prepared by
WEINTRAUT & ASSOCIATES, INC.

Principal Investigator: Dr. Linda Weintraut

Author: Bethany Natali, M.A.

P.O. Box 5034

Zionsville, Indiana 46077

317.733.9770

Linda@weintrautinc.com

April 2012

Executive Summary:

State Boulevard Reconstruction from Spy Run to Cass Street

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East. The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

This report provides additional information to the Historic Property Report (HPR) for the State Boulevard Reconstruction from Spy Run to Cass Street (Westerly Group, Inc., 2009). In 2009, the Westerly Group, Inc. (WGI) identified and evaluated properties more than fifty years of age at that time within the Area of Potential Effect (APE).

In February 2012, American Structurepoint, Inc. contracted with Weintraut & Associates, Inc. (W&A) to prepare an Additional Information Report (AI) to append the HPR. The purpose of the AI is to supplement the

HPR following the inclusion of two new NR-listed resources within the APE. Project historians who meet the Secretary of the Interior's Professional Standards identified and evaluated historic properties within the APE for this project in accordance with Section 106 of the National Historic Preservation Act (1966), as amended and 36 C.F.R. Part 800.

As part of AI investigations for this project, historians identified two districts that were listed in the National Register of Historic Places (NR) after the HPR (2009) was prepared; portions of both districts are contained within the project APE:

- Fort Wayne Park and Boulevard System Historic District (NR, 2010)
- Brookview-Irvington Park Historic District (NR, 2011)

The Bridge over Spy Run (ST-5/NBI No. 0200273) was previously determined eligible for listing in the NR.

Historians believe proposed project activities will adversely affect the Bridge over Spy Run and the Fort Wayne Park and Boulevard System and Brookview-Irvington Park historic districts. Therefore, the recommended finding of effect for this project is: Historic Properties Affected—Adverse Effect.



INDIANA ARCHAEOLOGICAL SHORT REPORT

State Form 54566 (1-11)

INDIANA DEPARTMENT OF NATURAL RESOURCES
DIVISION OF HISTORIC PRESERVATION
AND ARCHAEOLOGY
402 West Washington Street, Room W274
Indianapolis, Indiana 46204-2739
Telephone Number: (317) 232-1646
Fax Number: (317) 232-0693
E-mail: dhpa@dnr.IN.gov

Where applicable, the use of this form is recommended but not required by the Division of Historic Preservation and Archaeology.

Author: Larry Stillwell

Date (month, day, year): 7/11/12

Project Title: An Archaeological Field Reconnaissance of the Proposed Additional Area Required for the State Boulevard Improvements (Des. No. 0400587) in Fort Wayne, Allen County, Indiana

PROJECT OVERVIEW

Project Description:

The City of Fort Wayne, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map, and in the east ½ of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing two-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the sub-standard horizontal curve. The five lane section will include two new travel lanes in each direction and a center two-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

INDOT Designation Number/ Contract Number: 0400587

Project Number: 12FR66

DHPA Number:

Approved DHPA Plan Number:

Prepared For: American Structurepoint, Inc.

Contact Person: Ms. Briana Hope

Address: 7260 Shadeland Station

City: Indianapolis

State: IN

ZIP Code: 46256-3917

Telephone Number: 317 547 5580

E-mail Address: bhope@structurepoint.com

Principal Investigator: Larry N. Stillwell

Signature:

Company/Institution: Archaeological Consultants of Ossian (ACO)

Address: PO Box 2374

testing methodologies.

Attach photographs documenting disturbances below

Describe Disturbances:

Non-agricultural soil disturbance was noted throughout the project area. In the extreme eastern end of the project commercial development had impacted the few areas of exposed soil that had not been paved (Figure 4). Soils in this area had been graded and filled.

The portion of the project situated between North Clinton Street and the abandoned railroad grade was predominantly residential. Soils had been disturbed in these areas by grading and filling activity associated with home construction, construction of lots and driveways for parking cars, road removal, contouring of the ground for drainage of the neighborhoods, and the removal tracks from a rail line (Figures 4 and 5).

In the western end of the project, soils had been disturbed by residential and commercial development. Most of the project universe was comprised of made land created through commercial development.

No intact naturally occurring soil stratigraphy was encountered during shovel testing of the project.

Comments:

The entire State Boulevard project was 12.8 acres in area. A total of 3.5 acres was originally surveyed by ACO in 2009 (Stillwell 2009). Of the remaining 9.3 acres of additional area examined during the current field reconnaissance, 3.5 acres was covered in existing asphalt pavement. Thus, 5.8 acres of the project was subjected to shovel testing.

RESULTS

☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.

☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.

☒ Phase Ia reconnaissance has located no archaeological resources in the project area.

☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: acres:

Comments:

RECOMMENDATION

☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.

☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.

☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

- ☒ Figure showing project location within Indiana.
- ☒ USGS topographic map showing the project area (*1:24,000scale*).
- ☒ Aerial photograph showing the project area, land use and survey methods.
- ☒ Photographs of the project area.
- ☐ Project plans (*if available*)

Other Attachments:

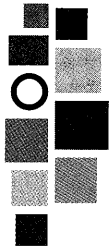
References Cited:

Comments:

Curation

Curation Facility for Project Documentation:

APPENDIX F. Correspondence



AMERICAN
STRUCTUREPOINT
INC.

March 23, 2009

James A. Glass, PhD
State Historic Preservation Officer
Indiana Department of Natural Resources
402 West Washington Street, W247
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle map and in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet. Please see the attached maps and aerial photographs for specific project location.

The existing section from Clinton Street to Spy Run will be widened along the existing alignment. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

The need for the project originates from the substandard horizontal curve along State Boulevard. A new alignment in this section will provide enhanced roadway safety. Curb and gutter, sidewalks, and a new storm sewer system along with "livable" street amenities, such as lighting and landscaping, will also be conducted. The purpose of the project is to improve traffic flow and roadway and pedestrian safety on State Boulevard.

Land use in the vicinity of the project is primarily residential. The project area will be investigated for archaeological and historic resources for compliance with Section 106 of the National Historic Preservation Act. The results of these investigations will be forwarded to the Indiana Department of Transportation (INDOT) and the Indiana State Historic Preservation Officer for their review and concurrence.

IN20071404



James A. Glass, PhD
March 23, 2009
Page 2

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you are hereby requested to be a consulting party to participate in the Section 106 process. This process involves efforts to identify historic properties potentially affected by the undertaking, assess the effect of the undertaking on historic properties, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties.

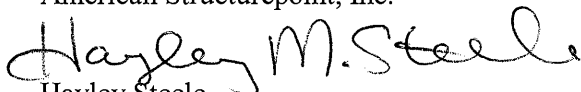
We realize you and/or your organization may want to perform a formal review of properties that may be affected as part of this project and, therefore, may require more detailed information regarding the scope of the project. Please note, preparation of a historical property report is underway and will be sent at a later date to consulting parties.

The following agencies have been invited to be consulting parties:

- State Historic Preservation Officer (SHPO)
- INDOT
- INDOT Fort Wayne District
- City of Fort Wayne Engineer
- Historic Landmarks Foundation of Indiana
- Allen County Historian
- Allen County – Fort Wayne Historical Society
- ARCH, Inc.
- Fort Wayne Historic Preservation Review Board
- John Shoaff, Fort Wayne City Council Member

Per 36 CFR 800.3(f), we hereby request the SHPO notify this office **within 30 days of the receipt of this letter** if the SHPO is aware of any other parties that may be entitled to be consulting parties for this project area.

Very truly yours,
American Structurepoint, Inc.

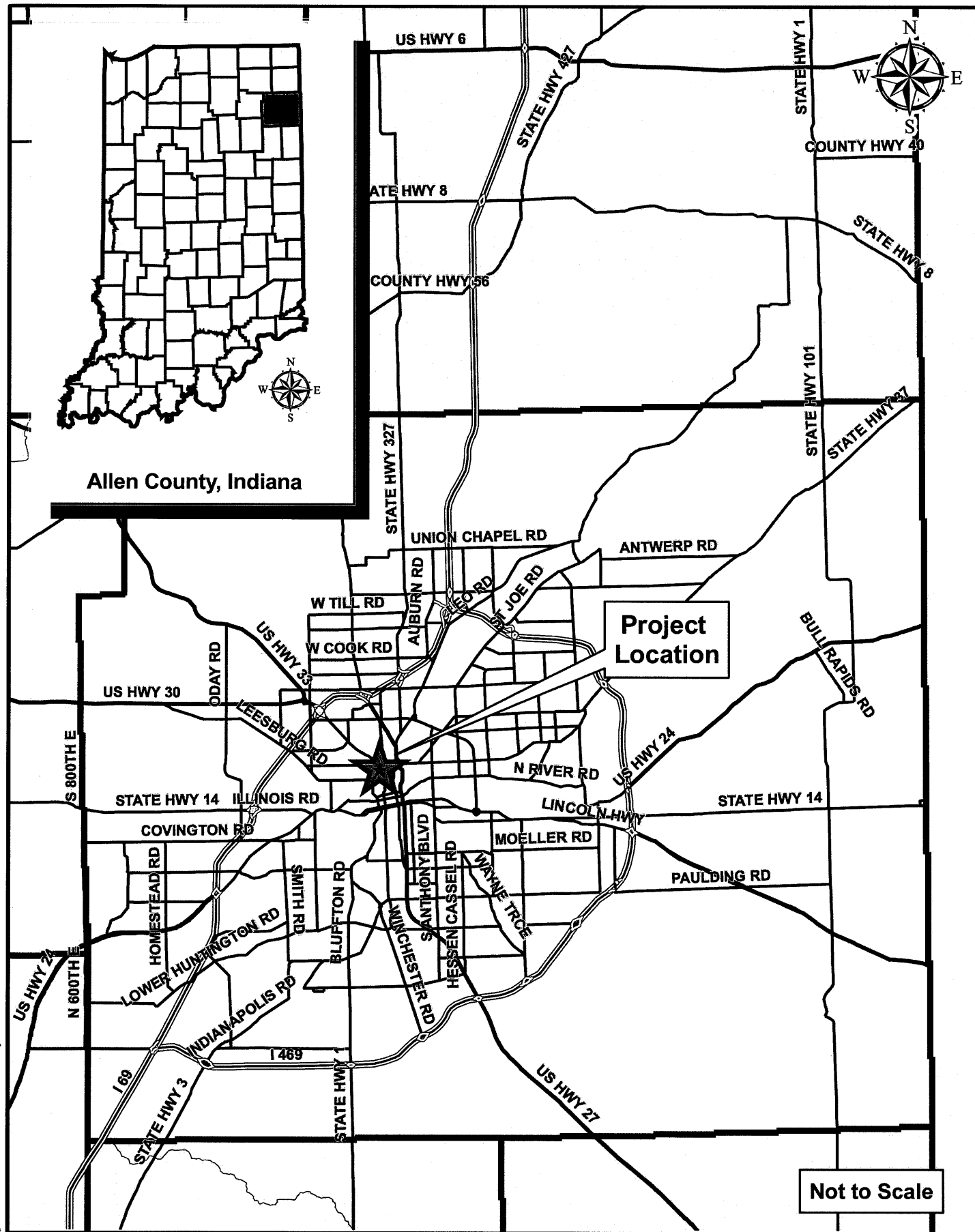

Hayley Steele
Environmental Scientist

HMS:ejp

Enclosures



Allen County, Indiana



Not to Scale

**AMERICAN
STRUCTUREPOINT
INC.**
www.structurepoint.com
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Project Location Map

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

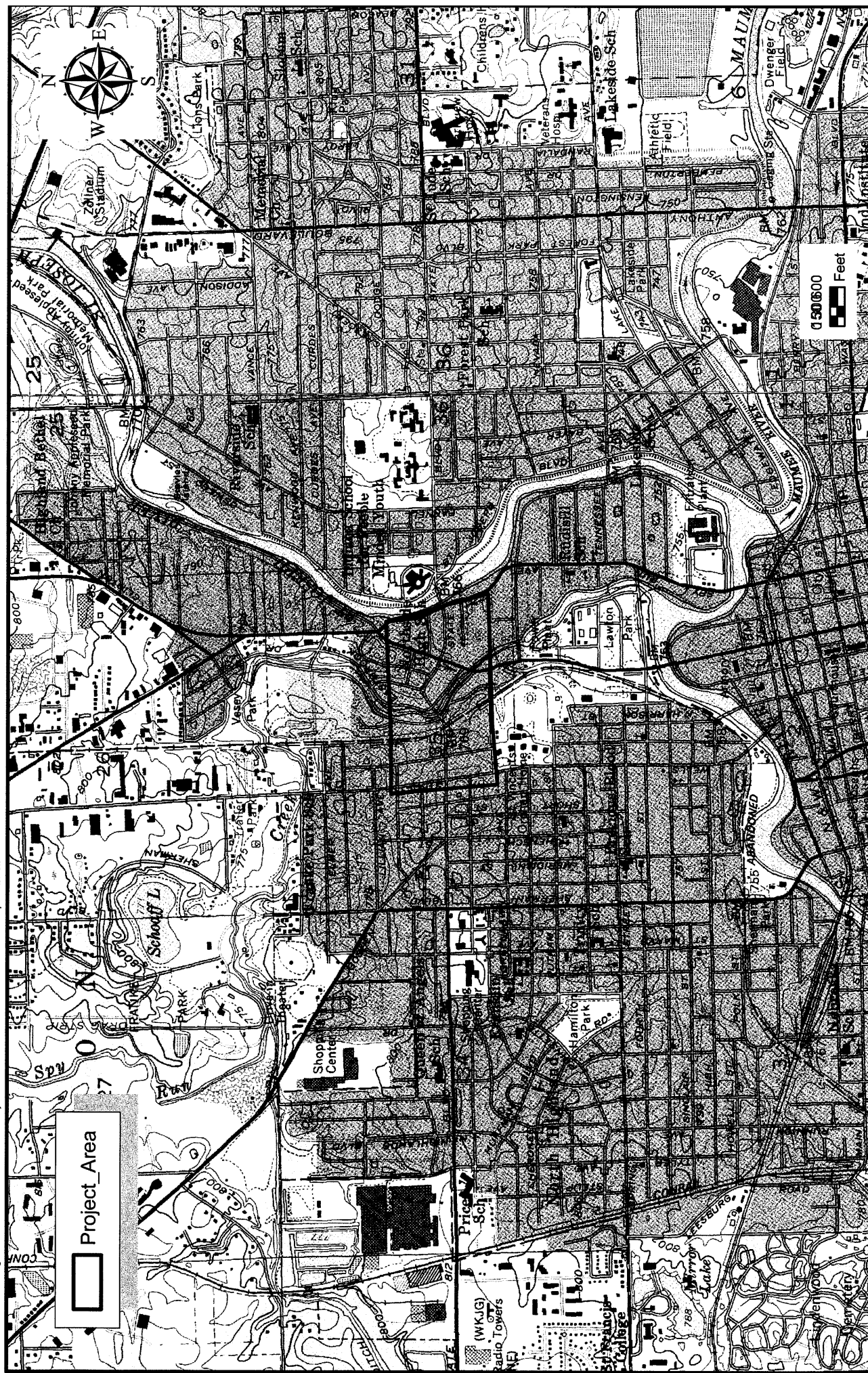
State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 03/12/2009

Sheet 1 of 3

P:\IN2007\1404\ID Drawings\Environmental\Arcview\Exhibits\Early Coordination\IN2007\1404 EV\MAP2009-3-12 State Location.hms.mxd



<p>State Boulevard Reconstruction</p> <p>Location: Fort Wayne Township: Wayne County: Allen State: Indiana</p> <p>Date: 03/12/2009</p> <p>Sheet 2 of 3</p>	<p>USGS Topographic Mapping</p> <p>Fort Wayne West and Fort Wayne East Quadrangles</p> <p>Applicant: City of Fort Wayne, Board of Public Works 420 City-Council Building One Main Street Fort Wayne, Indiana 46802</p>	<p>AMERICAN STRUCTUREPOINT INC.</p> <p>www.structurepoint.com ©2007-2009, American Structurepoint, Inc.</p>
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<p>State Boulevard Reconstruction</p> <p>Location: Fort Wayne Township: Wayne County: Allen State: Indiana</p> <p>Date: 03/12/2009</p> <p>Sheet 3 of 3</p>	<p>Project Location - 2005 Aerial Photography</p> <p>Applicant: City of Fort Wayne, Board of Public Works 420 City-Council Building One Main Street Fort Wayne, Indiana 46802</p>	<p>AMERICAN STRUCTUREPOINT INC.</p> <p>www.structurepoint.com ©2007-2009 American Structurepoint, Inc.</p>
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The mission of ARCH is
to advocate for the protection
and preservation of historically
and culturally significant assets
and historic places in
Allen County and
northeast Indiana.

Hayley Steele
Environmental Scientist
American Structure Point, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957
4/16/09

Dear Ms. Steele,

Thank you for your invitation to become a Consulting Party in the State Boulevard Reconstruction Project located in Fort Wayne, Allen County, Indiana. (Des. No. 0400587, Project No. IN20071404) We accept your invitation and have returned the enclosed postcard indicating the same. If you have not already done so, I suggest that the following two groups that have a demonstrated interest in the project be added as Consulting Parties.

1. Friends of the Parks, a not-for-profit organization that has a concern for the parks of Allen County has a particular concern for the effects on historic parks and designed landscapes such as those likely to be encountered in this project. Contact person is Julie Donnell, 3604 South Washington Road, Fort Wayne, IN 46802.
2. Brookview Neighborhood Association represents the residents in the historic Brookview neighborhood through which the project will pass. They have a vested legal and economic interest in the project and should be added as Consulting Parties. Contact person is Michelle Briggs-Wedaman, 2326 Eastbrook Drive, Fort Wayne, IN 46805

Sincerely yours,

Michael Galbraith
Historic Preservation Specialist, ARCH, Inc.
mgalbraith@archfw.org

ecc: Michelle Briggs-Wedaman, Brookview Neighborhood Association
John Carr, DHPA
Carrie Chapman, Friends of the Parks
Julie Donnell, Friends of the Parks
Todd Zeiger, HLF





Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



April 23, 2009

Hayley Steele
Environmental Scientist
American Structurepoint, Inc.
7620 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Notification concerning the Reconstruction of State Boulevard (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA # 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed your letter dated March 23, 2009 and received on March 25, 2009, for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana.

Thank you for notifying our office of the proposed project. As you know, a complete analysis of the project is not possible at this time. Please provide the following information to facilitate the identification and analysis of buildings and structures in the area of potential effects:

- ❖ Literature Review
- ❖ Historic Context
- ❖ Research Methodology
- ❖ Property Descriptions
- ❖ National Register of Historic Places eligibility evaluations and recommendations

For further guidance on the indicated information, please refer to Appendix AA of INDOT's Indiana Cultural Resources Manual (<http://www.in.gov/indot/7287.htm>). Please keep in mind that additional information may be requested in the future.

It is our understanding from the information contained in your letter that the project area will be investigated for archaeological resources. Therefore, we will await the archaeological information which will be provided.

Similarly, it appears that you will be providing us with a historic property report regarding aboveground properties within an area of potential effects ("APE") that is yet to be defined. If so, then it appears likely that the kinds of information outlined above will be provided to us in that format. We do have a few comments to offer at this time.

Regarding consulting parties, we agree with the recommendation by Michael Galbraith of ARCH, Inc., in his April 16, 2009 letter to you, that Friends of the Parks and Brookview Neighborhood Association be invited to participate as consulting parties in this Section 106 review. It appears that the State Boulevard Bridge over Spy Run Creek (also known as Allen County Bridge #546 or the Evard Bridge), may be eligible for inclusion in the National Register. Because it appears that this bridge

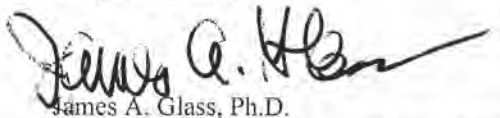
will be affected by the project, we ask that you also invite Paul Brandenburg of the Indiana Historic Spans Task Force (5868 Croton Circle, Indianapolis, Indiana 46254; e-mail: indianabridges@sbcglobal.net) and bridge historian Dr. James L. Cooper (629 East Seminary Street, Greencastle, Indiana 46135; e-mail: jlcooper@ccrtc.com) to participate as consulting parties.

Because this project will physically affect an area at least four city blocks in length, we ask that you carefully delineate the APE to include not only areas where physical effects and obvious visual effects are likely but also areas where foreseeable indirect effects could occur. It appears to us that realigning State Boulevard throughout much of the project area could cause not only a variety of atmospheric effects but also effects such as limiting access to historic properties, either temporarily or permanently, which could alter their uses (*see* 36 C.F.R. § 800.16[d] and [i] and § 800.5[a] and especially [a][2][iv] and [v]).

In a separate undertaking involving INDOT's proposed replacement of the US 27 (Clinton Street) bridge over Spy Run Creek (Des. No. 0200914), we have expressed the opinion that the Brookview-Irvington Park Historic District is eligible for inclusion in the National Register of Historic Places under Criterion C as an example of community planning. ARCH, Inc., has proposed to have an application for nomination of the district to the National Register prepared. Although a formal finding has not yet been made in that project, it is our understanding that FHWA and INDOT preliminarily have agreed with our opinion on the district's eligibility. Presumably, FHWA, INDOT, and we would arrive at a similar conclusion with regard to the undertaking proposed here along State Boulevard, which would cross the district. We have enclosed a map with the preliminary boundary of the district shown in dark lines.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. Questions about buildings or structures should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA # 5903.

Very truly yours,



James A. Glass, Ph.D.

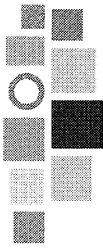
Deputy State Historic Preservation Officer

JAG:ALJ:JLC:jlc

Enclosure

cc: Robert F. Tally, Jr., P.E., Federal Highway Administration, Indiana Division
Michelle Allen, Indiana Department of Transportation

enc: Shaun Miller, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Michael Galbraith, ARCH, Inc.
Julie Donnell, Friends of the Parks
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.
Paul Brandenburg, Indiana Historic Spans Task Force
James L. Cooper, Ph.D., historian



AMERICAN
STRUCTUREPOINT
INC.

7260 Shadeland Station
Indianapolis, Indiana 46256
TEL 317.547.5580
FAX 317.543.0270

www.structurepoint.com


LETTER OF TRANSMITTAL

To: Dr. James Glass State Historic Preservation Officer DNR - Historic Preservation & Archaeology 402 W. Washington Room W274 Indianapolis, Indiana 46204	Date: July 2, 2009	Billing Group:
	Project Number: IN20071404	Billing Phase:
	Project Description: City of Fort Wayne - State Blvd. between Spy Run and Cass Street Fort Wayne, Indiana (IN) Project Des. 0400587 Purchase Order No. 90640023-000	

Deliver by:	We are sending you:		
Time:	<input checked="" type="checkbox"/> Enclosed	via	<input checked="" type="checkbox"/> Mail
Date:	<input type="checkbox"/> Separately		<input type="checkbox"/> Messenger
			<input type="checkbox"/> Overnight
			<input type="checkbox"/> As Requested
			<input type="checkbox"/> For Your Information
			<input checked="" type="checkbox"/> For Your Review & Comment
			<input type="checkbox"/>

Quantity:	Type:	Description:
1	RPT	An Archaeological Field Reconnaissance of the Proposed State Boulevard Improvements (Des. No. 0400587)

Remarks:
Please review the enclosed report and contact me with any questions or comments you may have. Feel free to contact me by phone, (317)547-5580, or by email at hsteele@structurepoint.com. Thank you.

Copies:	Trans:	Enclosure:	To:	Very truly yours, American Structurepoint, Inc.  Hayley Steele
Please return copies to this office.				



AMERICAN
STRUCTUREPOINT
INC.

November 9, 2009

Dr. James A. Glass
Deputy State Historic Preservation Officer
Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 West Washington Street, Room W274
Indianapolis, Indiana 46204

Re: Des. No. 0400587, DHPA #5903
State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Project No. IN20071404

Dear Dr. Glass:

The City of Fort Wayne is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Allen County, Indiana. The purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard. The need for the project originates from the substandard horizontal curve along State Boulevard.

The total project length is approximately 2,300 feet. The existing section of State Boulevard from North Clinton Street to Spy Run will be widened to five lanes along the existing alignment. The existing 2-lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. Project historians from The Westerly Group (WG), who meet or exceed the Secretary of Interior's standards for Section 106 work, identified and evaluated historic properties within the Area of Potential Effect (APE) for this project. Historic properties were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (revised January 2001), Final Rule on Revision of Current Regulations, December 12, 2000, and incorporating amendments effective August 5, 2004. The Historic Properties Report and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on November 6, 2009.

IN20071404



Dr. James A. Glass
November 9, 2009
Page 2

The APE of this undertaking is within the Brookview-Irvington Historic District (District). The District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne, especially the planned suburban developments of the Wildwood Companies. In addition, it is eligible as a designed landscape, the work of Arthur A. Shurcliff. The District contains an estimated 315 individual resources. There are 92 individual buildings within the District that were reviewed to determine their contribution to the District. These were located within the APE and could be directly or indirectly affected by the undertaking. All but 12 of these buildings were deemed to contribute to the District. The bridge over Spy Run Creek, which also contributes to the District, and the Brookview-Irvington Historic District are recommended as eligible for the NRHP. State Boulevard itself, both within the District and to the east and west of it, was analyzed. State Boulevard within the District is recommended individually eligible for the NRHP because of its contribution to the District. State Boulevard outside of the District and within the APE is recommended as not individually eligible for the NRHP. Twenty individual buildings within the APE but outside of the District were analyzed. Nineteen of the 20 buildings are not individually eligible for the NRHP, and one is recommended individually eligible for the NRHP. The former railroad and interurban overpass was evaluated and determined not individually eligible for the NRHP.

Due to the proposed realignment of State Boulevard and the replacement of the bridge over Spy Run, impacts to the Brookview-Irvington Historic District are expected. Because of this, a *preliminary* effect finding of Adverse Affect to Historic Properties is anticipated. Additional effects analyses are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting your review and comment on the Historic Properties Report (HPR) and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the HPR by December 11, 2009.

We are also requesting your dates of availability for a consulting parties meeting. The meeting will occur after SHPO and all consulting parties have had time to review the HPR. We would like to have the meeting during either the week of December 7, 2009, or the week of December 14, 2009. I have attached a chart of the possible days for the meeting. Please return this chart indicating either morning or afternoon on any day that you are available for a consulting parties meeting. After receiving availability from SHPO, the Federal Highway Administration, INDOT, and consulting parties, a meeting will be scheduled and an agenda will be set. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at hsteele@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Hayley M. Steele
Environmental Scientist

HMS:mgn

Enclosures

See distribution list on the next page.

Dr. James A. Glass
November 9, 2009
Page 3

cc: Patrick Carpenter, Historian, Cultural Resources Section – INDOT (via email)
Joyce Newland – Federal Highway Administration
Shan Gunawardena – City of Fort Wayne
Angie Quinn and Michael Galbraith – ARCH, Inc.
Don Orban – Fort Wayne Historic Preservation Commission
Todd Zeiger – Historic Landmarks Foundation of Indiana – Northern Office
Julie Donnell – Friends of the Parks of Allen County
Michelle Briggs-Wedaman – Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg – Indiana Historic Spans Task Force
Charley Shirmeyer – Northside Galleries
Karl Dietsch – Brookview Neighborhood Association
Susan Haneline – Brookview Neighborhood Association
Annette Daily – Brookview Neighborhood Association
Dan Avery – Northeastern Indiana Regional Coordinating Council
Suzanne Slick – Irvington Park Neighborhood Association
Camille Fife – Westerly Group (via email)



M E M O R A N D U M

DATE: December 1, 2009

TO: Ms. Joyce Newland, Federal Highway Administration
Mr. Patrick Carpenter, INDOT Cultural Resources
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Camille Fife, Westerly Group
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association
Mr. John H. Shoaff, Fort Wayne City Council
Dr. James L. Cooper
Mr. Paul Brandenburg, Indiana Historic Spans Task Force
Ms. Susan Haneline, Brookview Neighborhood
Mr. Charley Shirmeyer, Northside Galleries
Mr. Karl Dietsch, Brookview Neighborhood
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council
Ms. Suzanne Slick, Irvington Neighborhood
Ms. Jan Daily, Brookview Neighborhood

FROM: Hayley Steele, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for December 15, 2009 at 9:30 am. The meeting will be held in the City County Building, Room 128. Because several of the consulting parties for the State Boulevard Project are also on a list of consulting parties for a nearby project (US 27 over Spy Run) it was requested the meetings for both projects be held on the same day, and the meeting was therefore scheduled for the morning of the 15th.

The City County Building is located at 1 East Main Street in downtown Fort Wayne. This is between South Calhoun and South Clinton Street along Main Street, approximately 1 mile south of the State Boulevard project area. Parking is available in a parking garage attached to the City County Building.

Please see the attached agenda for the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at hsteele@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

December 15, 2009
9:30 am
City County Building- Room 128
1 East Main Street
Fort Wayne, Indiana

1. Overview of Proposed Project (American Structurepoint)
 - a. Purpose and Need
 - b. Proposed Improvements
 - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Discussion of Potential Mitigation Measures (Westerly Group/Structurepoint)
4. Next Steps (Westerly Group/Structurepoint)
 - a. Development of Memorandum of Agreement
 - b. Follow-up items

162 E State Blvd

Fort Wayne, In. 46805

12/7/2009

Hayley Steele, Environmental Scientist

American Structurepoint, Inc.

7260 Shadeland Station

Indianapolis, In. 46256

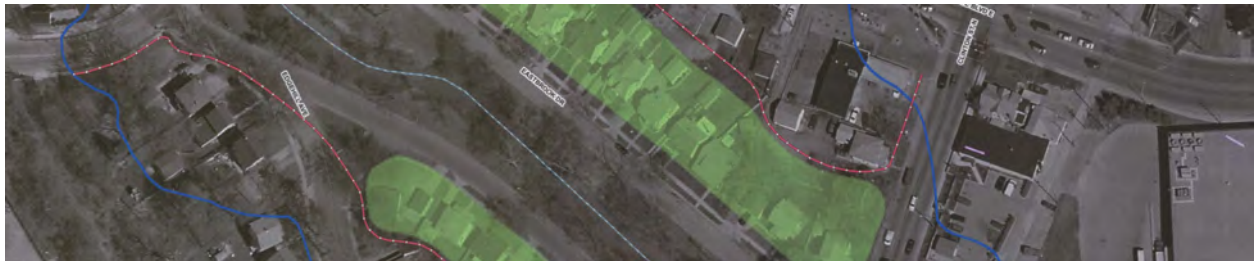
Dear Ms. Steele,

I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effect (APE). The Westerly Group presents the report in a user-friendly format that informs the reader of the project and its anticipated outcome. I appreciate the cautionary notes regarding the preliminary status of the design and the knowledge gained from the report.

One of the recurring themes throughout the report is the mixed-use category that the APE has always had and many residents are acting to preserve its historic nature. As the report notes on page 5, State Boulevard is an urban minor arterial road, (*"Urban minor arterial road" means a route that generally interconnects with and augments an urban principal arterial road and provides service to trips of shorter length and a lower level of travel mobility*) that was designed to slow down traffic by a master landscaper whose examples are limited. The fact that there was a larger design that was never completed makes it likely that the area will become a part of the National Register of Historic Properties under several criterion.

The history of the designers, builders, and the neighborhoods is consistent with the current atmosphere of the APE. The individual descriptions of the properties and the area characteristics allow the reader to understand the primary affected properties and their contribution to the APE. While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling that residents of this neighborhood share.

Furthermore, the recent closing of the Kroger Fuel Center (photo 57) may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek (below).



If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State Boulevard and Spy Run Avenue (as seen in the photo section below) the road would pass behind or around most of the homes in the APE . The designers of the road project could not have foreseen the closing



of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy, it seems only fair that a separate study be conducted factoring in possible land use of the former Kroger Fuel Center.

Sincerely,

Annette "Jan" Dailey



Northern Regional Office
402 W. Washington
South Bend, Indiana 46601
574-232-4534
574-232-5549 (fax)

December 8, 2009

Ms. Hayley Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Dear Ms. Steele,

I am writing today to submit comments concerning the Draft Historic Properties report dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). Thank you for your response to my earlier inquiry concerning the scope of our comments at this time. I understand that we are only commenting on the HPR at this time and that we will receive project specific design information for analysis in the future. In partnership with our organizational partner in Fort Wayne, ARCH, we have a number of specific questions and concerns which I will outline below.

1. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP) under Criterion A for its association with community planning and development in Fort Wayne. We believe that the HPR should note that not only is the district eligible but that a National Register nomination is currently being written by ARCH and the Fort Wayne Historic Preservation, with funding from the Indiana Division of Historic Preservation and Archaeology, the boundaries of which will correspond with the proposed boundaries outlined in the draft HPR. The nomination is being drafted to include eligibility not only under Criteria A but also Criterion C as a designated landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. Project Specific Design information: While it would be unusual to combine an HPR with information normally associated with later parts of the Section 106 or 4f process, we wish to note for the record that no maps delineating proposed road changes were included with the narrative description of the project. We note this due to the inclusion at this early stage of the review process language addressing outlining a "preliminary finding of adverse effect" contained in the cover letter as well as the HPR. With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.
3. Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State

Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.

Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include “the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties”. While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.

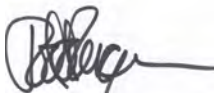
4. Purpose and Need. We note in the cover letter as well as within the HPR the stated Purpose of the project is “*to improve traffic flow, roadway, and pedestrian safety along State Boulevard.*” The Need for the project is stated as “*substandard horizontal curve along State Boulevard*”. We have a number of significant concerns and questions about how the proposed project will advance that purpose and meet the need. Again, it is unusual to get in to the merits of purpose and need and those metrics within the context of a discussion about the Historic Properties Report. Without specific plans it is difficult to analyze what alternatives under review would not only meet the purpose and need of the project but also avoid adverse impacts to the District and the State Boulevard bridge (noted above as an individually eligible and contributing resource to the district.) We anticipate additional questions once we receive more detailed project documents. In an effort to help keep the review moving we offer the following questions based on the narrative supplied in the HPR. We are offering the following based on the stated purpose as outlined in the cover letter and in the Executive Summary on page 3 of the HPR.
 - a For the purposes of our participation in the Section 106 and anticipated 4f review we will utilize the Purpose and Need as outlined in the Cover Letter and on page 3 of the HPR. Alternatively, if this is not correct please forward the project’s Purpose and Need along with the alternatives so that we can evaluate same in a proper manner.
 - b We have a number of questions and concerns about how the project as described in written form in the HPR will meet the stated purpose and need. Again, it is not possible to appropriately review and comment without project maps and designs of alternatives but absent those we can offer the following comments.
 - 1) How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.
 - 2) Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus.
 - 3) Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.
 - 4) Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.
 - 5) Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.
 - 6) Please provide detailed traffic accident studies or report data pertaining to accidents at the “substandard” curve.

- 7) Please provide detailed pedestrian accident studies or report data pertaining to the “substandard curve.”
 - 8) How is the proposal to add a new “trail bridge” over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed “trail bridge”.
5. “Trail Bridge”: We continue to be concerned about an on-going appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future “trail bridge” should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.
 6. Concerning the “substandard horizontal curve”: Page 4 of the HPR states that “the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve.” We believe that this curve, a designed element of Arthur Shurcliff’s plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, “Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive.” We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.
 7. Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7, We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity “early in the undertaking’s planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).
 8. Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that “Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne.” Page 55 of the HPR notes that “With regard

to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.

The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.

Sincerely,



Todd A. Zeiger

Director, Northern Region

cc. Indiana Division of Historic Preservation and Archaeology
Consulting Parties for State Boulevard Project



M E M O R A N D U M

DATE: December 8, 2009

TO: Ms. Joyce Newland, Federal Highway Administration
Mr. Patrick Carpenter, INDOT Cultural Resources
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Camille Fife, Westerly Group
Ms. Karie Brudis, DNR- Division of Historic Preservation and Archaeology
Ms. Angie Quinn & Michael Galbraith, ARCH, Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Historic Landmarks Foundation of Indiana
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Neighborhood Association
Mr. John H. Shoaff, Fort Wayne City Council
Dr. James L. Cooper
Mr. Paul Brandenburg, Indiana Historic Spans Task Force
Ms. Susan Haneline, Brookview Neighborhood
Mr. Charley Shirmeyer, Northside Galleries
Mr. Karl Dietsch, Brookview Neighborhood
Mr. Dan Avery, Northeastern Indiana Regional Coordinating Council
Ms. Suzanne Slick, Irvington Neighborhood
Ms. Jan Daily, Graduate Student, IPFW, Sociological Practice
Mr. Creager Smith, Historic Preservation Planner
Mr. Albert Cohan, Westbrook 5, LLC
Mr. Thomas M. Niezer, Barrett & McNagny, LLP
Mr. Ronald Ross, AIA, Martin Riley Architects and Engineers

FROM: Hayley Steele, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint Project No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

The agenda for the Consulting Parties Meeting scheduled for December 15, 2009 has been revised. Please feel free to contact me at hsteele@structurepoint.com or by phone at (317) 547-5580 with any questions or concerns.

We look forward to meeting with all of you on December 15, 2009 at 9:30 am.

Thank you

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

December 15, 2009
9:30 am
City County Building- Room 128
1 East Main Street
Fort Wayne, Indiana
(Revised 12/07/09)

1. Overview of Proposed Project (American Structurepoint)
 - a. Purpose and Need
 - b. Proposed Improvements
 - c. Project Schedule
2. Review of Historic Properties (Westerly Group/Structurepoint)
3. Future Steps in the Process (Westerly Group/Structurepoint)
 - a. Potential Mitigation Measures
 - b. Development of Memorandum of Agreement
4. Follow-up items

The mission of ARCH is
to advocate for the protection
and preservation of historically
and culturally significant assets
and historic places in
Allen County and
northeast Indiana.

Hayley Steele
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

12/9/09

Dear Ms. Steele,

I am writing in regard to the Historic Property Report (HPR) dated November 9, 2009 for the proposed State Boulevard reconstruction in Allen County, Fort Wayne, Indiana (Des. No. 0400587, DHPA# 5903). ARCH would like to thank you for the opportunity to comment and to commend Ms. Fife for the thorough, well-written Report. I have a few specific questions and concerns regarding the report.

1. 1. Regarding the cover letter. We agree that the Brookview - Irvington Park Historic District is eligible for the National Register of Historic Places (NRHP). We agree that it is likely eligible under Criterion A for its association with community planning and development in Fort Wayne. We also believe that the greater depth provided in the under progress NRHP Nomination will demonstrate eligibility under Criterion A for Architecture and Landscape Architecture. We also agree with your recommendation that the District be considered eligible under Criterion C as a designed landscape, the work of a master, Arthur Shurcliff. We also believe that it may represent the work of a master in its association with Wildwood Builders principal Lee J. Ninde. The greater depth provided in the under progress NRHP Nomination will hopefully demonstrate that eligibility. We also agree with your recommendations that the bridge carrying State Boulevard over Spy Run Creek and State Boulevard within the Brookview - Irvington Park Historic District be considered eligible for the NRHP, both individually and as contributing elements to the Brookview - Irvington Park Historic District.
2. 2. Regarding maps. The HPR contained no maps, plans or drawings of the proposed project. We have found it difficult to study, review, and analyze this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review? Particularly helpful will be all maps, plans and drawings that detail possible alternatives that may have been studied.
3. Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary (page 3) of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states

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that the "purpose of the project is to improve traffic flow, roadway and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "adding a new trail bridge" as stated on page 3, addresses "traffic flow, roadway and pedestrian safety" along State Boulevard? Need is stated in the cover letter to "originate from the substandard horizontal curve along State Boulevard." I have addressed this in point 5 below.

4. Regarding the "Trail Bridge." On page 3 and page 4 of the HPR a new "trail bridge" is described as being part of the project. Page 4 states that the project includes a "pre-fabricated trail bridge over State Boulevard at the abandoned New York central railroad right-of-way between Cass Street and Westbrook Drive." Besides the safety issues that installation of this bridge raises, there is the issue of project aggregation/co-mingling with which we have been struggling in the US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.
5. Regarding the "substandard horizontal curve." Page 4 of the HPR states that "the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We question the analysis of "substandard" and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower

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environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive."

6. Regarding the Area of Potential Effect (APE). Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State Boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR. Also regarding the APE, were potential changes in traffic use in the Brookview - Irvington Park Historic District studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. 36 CFR 800.16 indicates that the APE shall be established to include "the geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties" yet page 6 of the HPR indicates that the APE for the State Boulevard reconstruction project was drawn to consider "potential direct impacts on historic districts or properties". We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this District. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.
7. Regarding the Project Description. Plans for this project seem to be quite complete according to the project description on page 7 of the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking."
8. Regarding the homes south of State Boulevard. Page 7 of the HPR states that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgehill Avenue." We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. As in point 4 regarding the trail bridge, we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 cfr 470) would be appropriate. The

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City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview - Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.

9. Regarding the seasonal flooding. Page 22 of the HPR states that "At the southern end of the district, between East State Boulevard and North Clinton Street, the creek's seasonal flooding has caused damage to homes." We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview - Irvington Park Historic District has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.

Sincerely yours,



Michael Galbraith
Historic Preservation Specialist, ARCH, Inc.
mgalbraith@archfw.org

emc: Dan Avery, NIRCC
Paul Brandenburg, SPANS
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Karie Brudis, DHPA
Patrick Carpenter, INDOT-CRS
John Carr, DHPA
Dr. James L. Cooper, SPANS
Julie Donnell, Friends of the Parks
Camille Fife, Westerly Group
Joyce Newland, FHWA
Don Orban
John Shoaff, Fort Wayne City Council
Creager Smith
Todd Zeiger, HLF

**Friends of the Parks of Allen County, Inc.
3604 South Washington Road
Fort Wayne, Indiana 46802**

Hayley Steele
Environmental Scientist
American StrucutrePoint
7260 Shadeland Station
Indianapolis, IN 46256
12/10/09

Dear Ms. Steele,

Regarding the proposed State Boulevard reconstruction in Allen County (Des. No. 0400587, DHPA# 5903) :

Since, as I told you several weeks ago, I am unavailable to attend the CP meeting on the 15th, I am following your advice and putting some of the concerns of the Friends of the Parks in writing. This letter is a response to your agenda, as amended in the e-mail exchange you have had with Mr. Galbraith, and a response to the HPR.

While The Friends of the Parks do not doubt the sincerity of your intentions, we continue to fear that in this case that this procedure is an afterthought rather than a respectful and necessary procedure.

Our concerns begin as a result of a meeting that we had with the staff of the City of Fort Wayne and other subsequent public meetings in the summer of 2008.

At the time, and only because we requested it, the “concept” of widening State Street at the bridge of Spy Run Creek, was presented to our board. The city staff presented the project, and though it was called a “concept” at the time, it bore no relation to what one would typically call a *conceptual* drawing except insofar as it was not a *detailed* drawing. Otherwise, the location, widening and straightening of State Boulevard were not conceptual, they were the FUTURE, and the staff felt very justified about taking the houses down, for this and for the flood control project on East Brook, referring to the “transient” nature of the people who live there, and suggesting that ultimately high rise apartments would be more suitable in the area. In point of fact, this thinking is supported by the traffic ideas included in the future North River Now Plan Final Report (the City does not yet own the property to be developed) prepared in 2007, which assumes the widening and straightening of State Boulevard, and other traffic connections to it. I do not include this information by way of criticism, but to underline that at the time it was already clear that a course of action had been decided upon and justified by more than one line of reasoning.

Thankfully we now have established that this place is important, and nationally so.

Despite the above being a bit of a historical digression, that which is pertinent to the current process and how it is carried out is that, **at that time, the concept was already in place and was unalterable, having over time been linked to future and current plans for the downtown, despite protests by the public, the recognition of the district's importance, and the requests of historic preservationists to reconsider. It also appeared tied to other disturbing projects that were underway.** City staff insisted that they had reviewed all suitable alternatives, and that this was the one that made the most sense. And, they noted, it was in keeping with other with other projects for the good of the community including the removal of homes for a flood control project on Eastbrook, and the changes for the bridge on US 27 which is now also involved in a Section 106 process.

Several months later, a contract to American StructurePoint was approved by City Council for an "Engineering Study". It was a contract for 1 million dollars. An engineering study, as one understands it, is a study about "how" to carry something out structurally, in this case, widen and straighten a road and build a new bridge, rather than a study to present alternatives to solve the identified problems. In other words, our concern is that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and, that, after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date.

It is particularly appropriate to call this to your attention because of the nature of the historic property in question. There is a difference between "landscape" and "landscaping", which historic preservationists, planners, and traffic engineers in America are beginning to grapple with.

Brookview Neighborhood is a designed landscape, eligible for the National Register of Historic Places as the work of a Master, as the HPR notes, and therefore changes to the interconnecting landscape components should be thoroughly reviewed before any "concept" is put forward at all. In addition to noting that the bridge and the circulation patterns will be affected, as my colleagues have done, we would ask that the project developers respond to any potential changes to the following aspects of the landscape which can be found in the Federal Guidelines for the Treatment of Historic Landscapes:

1. The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?
2. The vegetation. What are the potential impacts on the vegetation of the designed landscape?

3. The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?
4. Vistas and Views
5. What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.

These have been mentioned, but it would be useful to have them further discussed as part of the HPR, before any sort of amelioration is discussed.

This particular historic resource is more than a series of houses of a certain age, which can be easily moved or walled off. It impossible to assume that one can add “landscaping” in the form of extra vegetation or a grass median to successfully ameliorate the transformation of a 2 lane curving road into a 5 lane straight road which is situated in the middle of a historic district known to be a designed cultural landscape, impossible to respond to the totality of the impact that this enormous change will have. “Landscaping” cannot compensate adequately the potential loss of the topography, vegetation, circulation, and visual character of this place, which is a “landscape.”

Further, in this situation, it is not suitable to offer as alternatives that which simply addresses how many yards the bridge might be moved in a certain direction, or what the ingress and egress options should be. Those are details of an established plan rather than real alternatives that address the question of landscape preservation.

Any plan to alter State Boulevard to solve the problems that this concept is intended to solve, problems which so far have not been set forth clearly, should be planned by a qualified expert in preserving historic landscapes.

We also respectfully request that there be consideration of the involvement of The Cultural Landscape Foundation in the proceedings.

We respectfully submit these comments.

Sincerely,

Julie Donnell
President
Friends of the Parks of Allen County, Inc.



Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
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Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



December 14, 2009

Hayley M. Steele
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration

Re: Draft historic property report (The Westerly Group, September 2009) for the proposed State Boulevard Improvements (Des. No. 0400587; Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has reviewed the materials with your cover letter dated November 9, 2009 and received on November 12 for the above indicated project in Fort Wayne, Wayne Township, Allen County, Indiana. You had requested comments by December 11, but that would have allowed us and the other consulting parties fewer than 30 days in which to respond.

The federal agency official is to determine the area of potential effects ("APE") in consultation with the State Historic Preservation Officer (36 C.F.R. 800.4[a][a]). Your cover letter and page 4 of the historic property report ("HPR") indicate that a new alignment for State Boulevard to the west of Clinton Street will be proposed but that no final alignment has yet been identified. Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, especially in graphic form, to indicate where the alignments that are under consideration would run.

Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in an avoidance or minimization of the adverse effect.

Our initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.

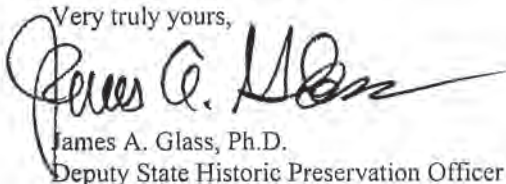
In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places with the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we note that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R.

44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 that took effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues, please contact Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. Questions about buildings or structures please should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:ALJ-JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation
Camille Fife, The Westerly Group, Inc.
Larry Stillwell, Archaeological Consultants of Ossian
Michael Galbraith, ARCH, Inc.
Todd Zeiger, Northern Regional Office, Historic Landmarks Foundation of Indiana, Inc.



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MEETING MINUTES

Location: City of Fort Wayne, City-County Building, Room 128

Date: December 15, 2009 (Minutes revised on February 3, 2010)

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Hayley Steele, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Ken Nicolet (City of Fort Wayne)
Camille Fife (The Westerly Group)
John Carr, Amy Johnson (IDNR, Division of Historic Preservation and Archaeology)
Patrick Carpenter (INDOT, Cultural Resources)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Don Orban (Fort Wayne Historic Preservation Commission)
Annette "Jan" Dailey (Graduate Student, IPFW, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Albert Cohan (Westbrook 5, LLC)
Angie Quinn, Michael Galbraith (ARCH, Inc.)
Karl Dietsch (Brookview Neighborhood Resident)
Michelle Briggs-Wedaman, Scott Simmons (Brookview Neighborhood Association)
Dan Ernst (Earth Source, Inc.)

1. The meeting was held at 9:30 a.m., December 15, 2009, to discuss the State Boulevard Reconstruction Project (Project), the Historic Properties Report written by the Westerly Group, and future steps in the Section 106 process.
2. Rich Zielinski introduced himself and began the meeting with introductions around the room.
3. Scott Crites gave an overview of the Project, including the purpose and need, proposed improvements, four alternatives, and the project schedule (presentation is attached).
4. Camille Fife gave a presentation (attached), including information regarding:
 - a. Section 106 process to date
 - b. Definition of an Area of Potential Effect (APE)
 - c. State Boulevard project area
 - d. The Brookview-Irvington Historic District
 - e. A preliminary APE
5. John Shoaff (Fort Wayne City Council) expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.

6. Michelle Briggs-Wedaman (Brookview Neighborhood Association) also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.
7. Jan Daily (Brookview Neighborhood Resident) also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecks both down Huguenard and along State Boulevard.
8. Angie Quinn (ARCH) discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.
9. Michelle Briggs-Wedaman agreed with Angie about Shurcliff's park-like design. The park setting is one of the primary reasons people chose to live in the neighborhood and how it attracts residents.
10. ARCH questioned Dan Avery (NIRCC) about the alternatives considered and when they were considered for the State Boulevard corridor. Dan explained during the late 70s or early 80s there were a number of studies done that looked at how to improve east-west and north-south travel throughout the urban area. Based on different alternatives, discussions with the public, and a combination of impacts (neighborhoods, parks, homes, etc), State Boulevard was chosen as a prime corridor for improvements to east-west traffic.
11. ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.
12. John Carr asked about the extent of the alignment of State Boulevard that is within the APE as a resulting product of Kessler's plan and of Shurcliff's design. Discussion took place regarding Kessler's plan lacking specific design details and Shurcliff's final product that created State Boulevard. Patrick Carpenter (INDOT) clarified the Spy Run bridge has been determined eligible and is now going through the select/non-select determination. Patrick also gave a brief definition of select and non-select. Although the list has not been finalized, the State Boulevard over Spy Run bridge is deemed non-select right now. The finalized list will be available in March 2010.
13. Camille Fife and ARCH discussed AW Grosvenor being the bridge designer for the State Boulevard over Spy Run bridge. Camille explained AW Grosvenor was not discussed in depth in the Historic Properties Report (HPR) and ARCH expressed the importance of including this information in the HPR and in the documentation of the bridge.
14. Jan Daily asked whether the select/non-select criteria were based on vehicular use of the bridge. Patrick explained this was all part of the analysis of the bridge, and these issues are difficult with a bridge such as the Spy Run bridge because it is concrete. The bridge can not be disassembled or used in part for other projects.
15. Scott explained the bridge is between five and six feet below the 100-year floodplain elevation, and it is completely submerged during flood events. The flood elevation is actually based on the St. Mary's River, so the new bridge will be constructed at an elevation higher than the flood elevation, but will not affect the current flood elevation.
16. Camille discussed the District's eligibility under criteria A and C. ARCH discussed the US 27 project and its eligibility due to community planning, landscape architecture, and architecture. They feel the same issues will arise with the State Boulevard project regarding eligibility criteria and should be agreed upon before mitigation measures are set forth for the project.
17. John Carr clarified the differences between eligibility requirements for properties within a Multiple Property Listing and for individual properties within a district.

18. Camille discussed we anticipate an adverse effect to the Historic District, to State Boulevard, and to the bridge over Spy Run as a result of this project as it has been designed thus far; within the APE and outside of the district, there may be one property that could be adversely affected. She also discussed an eligible property and a property listed on the NR are evaluated on the same level and are treated as equally important resources.
19. American Structurepoint reiterated the fact State Boulevard is remaining in place and the majority of this Project is along existing alignment. The proposed State Boulevard will be realigned to the south of the existing between Terrace Road and Westbrook, allowing the existing State Boulevard to remain in place instead of being completely eliminated. The southern-most alignment was chosen based on anticipated impacts to the neighborhood. As you go north, the substandard curve and the site distance makes the curve not practical to use. The goal in choosing the current alignment was to limit the impacts to the existing State Boulevard and to impact as few houses as possible.
20. American Structurepoint gave an overview of future steps (more meetings, mitigation, memorandum of agreement) in the Section 106 process and how they will be addressed with consulting parties.

GENERAL CONCERNS

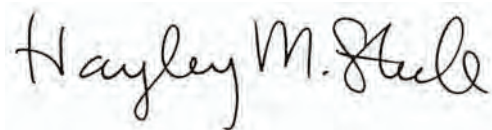
- The purpose and need presented at the meeting are different than those given in the HPR and at previous meetings.
- How will the purpose and need of the project be met with the widening and straightening of State Boulevard? ARCH and the Brookview Neighborhood Association do not feel the purpose and need are strong enough to warrant the project.
- Property values along State Boulevard could possibly decrease as a result of this project.
- Safety of the neighborhood should be a concern in the design of the project.
- The consulting parties would like a time to comment on information being presented as a result of this meeting.

ACTION ITEMS

- American Structurepoint will compile a packet of information including, but not limited to:
 - Purpose and Need of the Project
 - Traffic data
 - Flood studies
 - Alternatives analysis
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Future meetings will be scheduled as design is finalized and mitigation measures are to be discussed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or hsteele@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.



Hayley M. Steele

HMS:mgn

Enclosures



Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



January 27, 2010

Hayley M. Steele
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Steele:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the meeting minutes that we received on December 28, 2009, for the aforementioned project in Fort Wayne, Allen County, Indiana.

The request we had made in our December 14, 2009 letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to the consulting parties.

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard. It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.

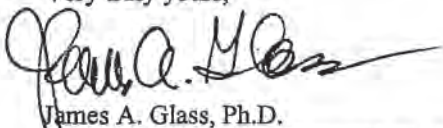
We believe it is important for FHWA to evaluate this project's purpose and need carefully before the Section 106 consultation proceeds much further. Although analysis of purpose and need has not expressly been made a part of the Section 106 review process, it seems to us that it would come into play once an adverse effect has been found formally (see 36 C.F.R. § 800.5), and the consultation turns to the consideration of alternatives that might avoid, minimize, or mitigate an adverse effect (see 36 C.F.R. § 800.6). If purpose and need are not clarified as much as possible now, and the Section 106 process proceeds through the assessment of adverse effects for only the four, very similar alternatives presented to the consulting parties, then we can foresee that by the time the process reaches the point at which it is appropriate to discuss alternatives that might avoid or minimize adverse effects, the applicant and the project planners might be reluctant to invest the time and expense that revisiting purpose and need might entail at that point. Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.

In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc



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M E M O R A N D U M

DATE: February 4, 2010
TO: Consulting Parties and Consulting Parties Meeting Attendees
FROM: Hayley Steele *HS*
RE: Follow-up Information Packet to Consulting Parties Meeting
CC: Scott Crites, Structurepoint

Per your request, enclosed please find a CD containing the City of Fort Wayne 2005 Flood Control Study as well as the traffic data collected for the State Boulevard project corridor.

Moreover, the request to re-evaluate the Purpose and Need is currently being coordinated with the Indiana Department of Transportation (INDOT), the Federal Highway Administration (FHWA), and the City of Fort Wayne. After coordination is complete the alternatives will once again be evaluated, and a determination will be made as to which alternative best meets the Purpose and Need. Once complete, this information will be sent to all consulting parties and attendees of the consulting parties meeting.

Also enclosed, please find the updated meeting minutes for the Consulting Parties meeting that took place on December 15, 2009. Please keep these final meeting minutes for your records.

Please feel free to contact me by phone at (317) 547-5580 or by email at hsteele@structurepoint.com if you have any questions or concerns regarding the attached information. We appreciate your cooperation in the development of this project.



Indiana Department of Natural Resources

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



March 10, 2010

Scott Crites
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration ("FHWA")

Re: Final minutes of the December 15, 2009 consulting parties meeting regarding the State Boulevard Reconstruction (Des. No. 0400587; DHPA No. 5903), the notice of the possible availability of the State Boulevard bridge over Spy Run Creek, the March 2005 "Spy Run Creek Flood Protection Study," and crash and traffic volume data, which was collected at various times between 2004 and 2008, inclusive, for various locations within and near the project area.

Dear Mr. Crites:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the aforementioned documents that accompanied your February 4, 2010 cover memorandum, all of which we received on February 8, 2010, for the aforementioned project in Fort Wayne, Allen County, Indiana.

Thank you for providing us with a copy of the final minutes for the December 15 meeting, which your firm modified in response to comments from Section 106 consulting parties.

As you may know, since the notice of the possible availability of the State Boulevard bridge over Spy Run Creek was prepared, the Indiana Department of Transportation has sent notification of the final lists of Select Bridges and Non-Select Bridges, pursuant to the Indiana Historic Bridges Programmatic Agreement. As your firm had anticipated, the State Boulevard bridge is on the list of Non-Select Bridges.

We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the area of potential effects ("APE") of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished by the buyout program and that some of the houses along Eastbrook Drive between State Boulevard and Clinton Street also have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard project, have they been flood-proofed, and what did that flood-proofing entail?

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled

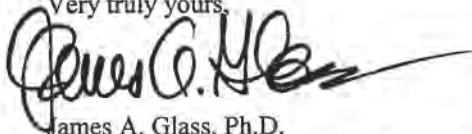
between those points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first blush like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak, weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion we are not sure. Moreover, it appears that a higher volume of traffic—more than 1,700 vehicles during that same hour—traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

The crash data also might yield a useful comparison. In 2008 the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be *realigned* appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned.. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have applied the data correctly. We would appreciate some assistance in that regard.

Another issue about which we are uncertain is whether it might be feasible to *reduce* the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program) that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

If you have questions about our comments on buildings and structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Amy Johnson at (317) 232-6982 or ajohnson@dnr.IN.gov. In all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Robert F. Tally, Jr., P.E., Indiana Division, Federal Highway Administration
Staffan Peterson, Cultural Resources Section, Indiana Department of Transportation

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Patrick Carpenter, Cultural Resources Section, Indiana Department of Transportation
Scott Crites, American Structurepoint, Inc.
Hayley Steele, American Structurepoint, Inc.
Camille Fife, The Westerly Group, Inc.
Amy Johnson, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources
John Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources



AMERICAN
STRUCTUREPOINT
INC.

May 19, 2011

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project and included in two separate letters received by our office. The DHPA letters (dated January 27, 2010, and March 10, 2010) were provided following a December 15, 2009, consulting parties meeting. Copies of the DHPA correspondence are included with this letter for your reference. The Section 106 process has been delayed until recently while we have been further developing the project Purpose and Need Statement in cooperation with INDOT and FHWA. On April 11, 2011, FHWA approved the revised Purpose and Need Statement (attached), allowing the NEPA and Section 106 process to proceed.

DHPA Letter to American Structurepoint, Inc. dated January 27, 2010

1) DHPA Comment - Page 1, Paragraph 2:

The request we had made in our December 14, 2009, letter for more information about this project's purpose and need was addressed, to an extent, during the December 15 consulting parties meeting. Even so, we would appreciate additional clarification and are glad to see that a packet regarding purpose and need will be provided to consulting parties.

Response:

The project Purpose and Need Statement has been significantly revised and was approved by FHWA and INDOT on April 11, 2011. The Purpose and Need Statement is included with this letter and will be provided to all consulting parties.

2) DHPA Comment – Page 1, Paragraph 3:

One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at

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Dr. James A. Glass
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Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.

Response:

Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.

3) DHPA Comment – Page 1, Paragraph 4:

Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.

Response:

The existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the curve radius for a 30 mph roadway is required to be a minimum of 300 ft. Several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.

The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

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For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing reverse curve found on State Boulevard between Clinton Street and Cass Street. Specifically, the intersections of Eastbrook Drive and Terrace Road will be replaced and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.

4) **DHPA Comment – Page 1, Paragraph 4:**

It is our understanding that the section of State Boulevard in question is posted for a 20 mph speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mph limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.

Response:

The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph, which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

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As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

5) DHPA Comment – Page 2, Paragraph 2:

Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass Streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g. Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.

Response:

Pursuant to 36 CFR Section 800.4(a)(1), the APE was drawn to encompass properties on both side of the undertaking and/or with a view shed of it. Generally, properties directly adjacent to the undertaking mask the view from the properties not adjacent. The APE was expanded or contracted based on visibility and the possibility of impact of the undertaking on properties within its view shed. Any indirect or cumulative impacts outside of the APE and Section 106 will be evaluated during the NEPA process.

6) DHPA Comment – Page 2, Paragraph 3:

Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred—and others that may yet occur—as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near the new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given

Dr. James A. Glass
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to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.

Response:

Consideration will be given to this request at the appropriate time.

DHPA Letter to American Structurepoint, Inc., dated March 10, 2010

1) DHPA Comment – Page 1, Paragraph 4:

We appreciate having the “Spy Run Flood Protection Study,” but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgemoor Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended—flood-proofing of some homes—has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne’s flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?

Response:

The flood control study prepared by Christopher B. Burke Engineering, Ltd., for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary’s River.

The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.

The study identifies necessary additional information regarding the flooding issues of the St. Mary’s river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, “This flooding is caused primarily by backwater from the St. Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

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The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.

American Structurepoint is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.

2) **DHPA Comment – Page 1, Paragraph 5:**

We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation; however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006 to 2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 p.m. to 6:00 p.m. That sounds at first like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not, we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.

Response:

Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.

Dr. James A. Glass
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3) **DHPA Comment – Page 2, Paragraph 2:**

The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clint Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which state Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.

Response:

Your interpretation of the traffic and crash data information mentioned in your letter is correct. It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.

4) **DHPA Comment – Page 2, Paragraph 3:**

Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?

Response:


Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact

Dr. James A. Glass
May 19, 2011
Page 8

residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.

It is our intent that the information provided in this letter, in combination with the attached FHWA-approved Purpose and Need Statement shall effectively address the comments and questions brought forth following the December 15, 2009, consulting parties meeting. We realize that a significant amount of time has passed since these comments were brought forth and we appreciate the patience of the DHPA and all consulting parties while we worked with INDOT and FHWA to establish a stronger Purpose and Need Statement. Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining to the project's Purpose and Need Statement, we respectfully request that you provide such comments within 30-days from receipt of this letter. Following this 30-day comment period, we will begin coordination to schedule a Consulting Parties meeting as the next step in continuing the Section 106 process. If I can be of assistance in the interim please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.


Brett W. Lackey
Environmental Scientist

BWL:alo

Enclosures

cc: Karie Brudis
Angie Quinn
Michael Galbraith
Don Orban
Todd Zeiger
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Shan Gunawardena
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Dan Avery
Suzanne Slick
Annette Dailey
Joyce Newland
John H. Shoaff
Patrick Carpenter
Camille Fife
Creager Smith
Albert Cohen
Thomas M. Niezer

 AMERICAN STRUCTUREPOINT, INC.

Dr. James A. Glass

May 19, 2011

Page 9

Ronald Ross

Dan Ernst

John Carr

Amy Johnson

**Friends of the Parks of Allen County, Inc.
PO Box 10152
Fort Wayne, Indiana 46850-0152**

June 14, 2011

Brett Lackey
Environmental Scientist
American Structure Point
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Mr. Lackey:

The Friends of the Parks appreciate being copied on your recent communication to Dr. James Glass dated May 19, 2011.

However, we are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.

Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communication with Doctor Glass, in a formal way under the auspices of whatever form the Section 106 Review continues.

Sincerely Yours,

Julie Donnell
President
Friends of the Parks of Allen County, Inc.

June 14, 2011

Cc: Dr. James Glass
Michael Galbraith

Todd Zeiger

Jill D. Downs
1202 Elmwood Ave.
Fort Wayne, IN 46805

June 15, 2011

RECEIVED
HIST. PRES. & ARCH.

JUN 17 2011

IDNR

Dr. James A. Glass
Deputy State Historic Preservation Officer
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, IN 46204

Dear Dr. Glass:

As chairperson of the Preservation Committee of ARCH, Inc., Allen County's historic preservation organization, I am writing in response to the letter dated May 19, 2011 from American Structurepoint to you regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

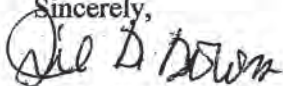
As a nearly lifelong resident of near northeast Fort Wayne, I travel this State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's recent poor attempt to justify its need.

In reading through the May 19 letter, American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than on moving traffic through the area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.

I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.

Thank you for your review of this issue.

Sincerely,



Jill D. Downs

cc: John L. Carr
Team Leader, Historic Structures Review
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
Telephone: 260-459-0231

June 16, 2011

Brett Lackey
American Structurepoint, Inc.
7260 Shadeland Station,
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I'm writing in response to your letter of May 19 to Dr. James Glass of the DHPA addressing questions and concerns posed by Dr. Glass.

First, let me note you said that a Purpose and Need Statement was attached, but I did not find one included with my copy.

In his letter Dr. Glass raises several important concerns, beginning with "it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion." Nor is it clear to me, even after reading your response.

For a three year period, when my mother, who lived on Westbrook Drive, was failing, I made the drive along that stretch almost every day, frequently at the rush hour. On the curve of State Street (as we have always called it), traffic always moved smoothly, but there was often (and always, at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.

Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.

Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the 'Arterial plus Bypass' concept" you refer to on Page 4. This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor", as you are already calling it, as if past errors justify future ones; you argue (bottom of Page 3) that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south - but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelthorn Road - which aligns with and neatly continues the east-west stretch of Coliseum.

Most seriously: while you repeatedly make the case for "continuity" for motorists who wish "to traverse the urban area" (bottom, Page 3), you totally ignore the welfare of the people who live within that area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values of the homes your corridor borders or traverses?

It is in the interest of the whole city to maintain the viability of all these neighborhoods, not only for the sake of the thousands of citizens who live there, but for all our citizens. By driving property values down, as your corridor inevitably will, total city property tax revenues will decline, and this decline will need to be made up elsewhere, or the city will be less able to properly maintain its infrastructure.

Furthermore, one of the prime advantages of living in Fort Wayne, the ability to live in serene, attractive, but affordable neighborhoods close to Downtown, will be diminished by the sum of all the neighborhoods compromised by your efforts to increase cross town traffic. You refer to "The redevelopment of the urban core area" (Page 3, 3rd paragraph from the bottom), something we all want; but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor. Housing stock will be lost or diminished in desirability; your project will end up merely making it a little easier drive across the city at the heavy price of making it less desirable to live in.

Your failure to consider any of these issues no doubt stems from the narrowness of the assignment you were given; but your own profession by now should understand the need to be sensitive to the context in which you design. ISTEA was passed in 1991, the Federal Highway Administration's handbook on *Flexibility in Highway Design* was published in 1995, and the AASHTO handbook on design flexibility (*A Guide for Achieving Flexibility in Highway Design*), which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert (Page 2, 3rd paragraph from the bottom) that standards appropriate for "highway design elements" apply to a road that abuts and serves a modest residential neighborhood!

I am aware that a majority of my colleagues on city council voted to fund your engineering work; but I think many of them thought they heard you, or the city representatives, say that several alternatives would be explored, after meaningful public input. What we finally saw was quite different: one plan, about which you were totally inflexible, with three trivial alternatives as to which neighborhood street would provide the access point to the newly straightened, raised, and lane-doubled thoroughfare. The choice of alternatives was equivalent to asking a condemned man which tree he wanted to be hanged by, and many of the citizens quite rightly felt patronized and insulted.

We on council now face an unpleasant two-fold task of fighting for a properly democratic, participatory process that was denied those citizens, and of fighting to preserve the quality of life and property values now threatened because of they were denied a proper process.

Times have changed, and the damage your plan in its present form will do is even greater than it would have been when it was conceived, back in the 1980s. We now have property tax caps: this means any deterioration of property values means a deterioration of tax revenue we cannot afford. Furthermore, we must enhance the quality of life in our neighborhoods if we are to successfully sell our community as a good place to live and work. The casualness, indifference, and shortsightedness we have shown in the past must be corrected now, if we are to compete

successfully with the progressive cities that have already changed their ways and avoided or even removed corridors that have undermined urban cores and neighborhoods.

I am aware that I address issues that go beyond those directly relevant to your field, Environmental Science (which should be raising issues I haven't even touched on), but it must say something about the attitudes of your principals that they did not see these issues and address them themselves.

Sincerely yours,
John H. Shoaff,
Fort Wayne City Councilman At-large

cc: Dr. James Glass
Consulting parties to the State Boulevard Reconstruction
Members of the Fort Wayne City Council

Lackey, Brett

From: Carr, John [JCarr@dnr.IN.gov]
Sent: Thursday, June 16, 2011 12:12 PM
To: Lackey, Brett
Cc: Hope, Briana; Tharp, Wade
Subject: RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Brett,

Yes, I think 15 days from the consulting parties' receipt of the P&N document is a sufficient extension of time. When do you anticipate they'll receive it?

John L. Carr
IDNR-DHPA
Phone: (317) 233-1949
Fax: (317) 232-0693
E-mail: JCARR@DNR.IN.GOV

Take our survey at www.IN.gov/dnr/historic to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]
Sent: Thursday, June 16, 2011 11:31 AM
To: Carr, John
Cc: Hope, Briana
Subject: RE: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

John,

Thank you for the information. To clarify on your comment below regarding extending the comment period: do you feel that an extension of 15-days (from receipt of the P&N) would be appropriate or should be give another full 30 days? Any thoughts on the matter would be appreciated.

Thanks

Brett Lackey
American Structurepoint, Inc.

-----Original Message-----

From: Carr, John [mailto:JCarr@dnr.IN.gov]
Sent: Thursday, June 16, 2011 10:55 AM
To: Lackey, Brett
Cc: Slider, Chad; Glass, James
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

Brett,

Page 8 of the 9-page, May 19 letter we received from you refers to the "the attached FHWA-approved Purpose and Need Statement," but the only document in the submission we have is the 9-page letter, itself. So, no, we had not received the P&N document until now.

BTW, I haven't yet had a chance to study that submission closely, but we had intended to comment on it within 30 days of having received it (i.e., no later than June 22). Since we just now received the P&N statement, and since the consulting parties apparently hadn't seen it yet, either, it might be appropriate to extend the comment period somewhat.

John L. Carr
Team Leader for Historic Structures Review Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 W. Washington St., Room W274
Indianapolis, IN 46204
Ph. No.: 317-233-1949 Fax No.: 317-232-0693

Take our survey at www.IN.gov/dnr/historic to help the DHPA revise Indiana's Cultural Resources Management Plan!

-----Original Message-----

From: Lackey, Brett [mailto:BLackey@structurepoint.com]
Sent: Thursday, June 16, 2011 9:36 AM
To: Carr, John
Subject: FW: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

John,

Could you let me know whether or not the DHPA received the revised Purpose and Need Statement (attached) which was to be included with the May 19, 2011 letter? We intended for this to be sent to all consulting parties...

Thanks

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
317.850.0257 | cell
www.structurepoint.com

-----Original Message-----

From: Todd Zeiger [mailto:TZeiger@indianalandmarks.org]
Sent: Thursday, June 16, 2011 9:02 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; John Carr
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903
Importance: High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision. Checking with other consulting parties they did not either. If it is the intention that the consulting parties were supposed to take the receipt of this carbon copy letter as our opportunity to re-engage in the 106 review process on this project (and I would add an unusual way to conduct a 106 review) then receipt of the revised purpose and need statement would be of utmost importance.

I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period. If it was in fact the intention of American Structurepoint that receipt of the copy of the May 19, 2011, letter to the Indiana Division of Historic Preservation and Archaeology serves as our opportunity to provide substantive comments, we request a 30 day extension - following the receipt of the revised purpose and need statement and replies to our other concerns and comments. If it was the intention that receipt of a copy of correspondence between the consulting engineer and the Indiana Division of Historic Preservation and Archaeology serves as consulting parties opportunity to comment we respectfully disagree with that process and request direct and professional engagement with all the parties involved. This is a significant project with the potential for profound change to historic neighborhoods and the utmost care needs to be taken to follow the process appropriately.

Thank you for your attention to this matter.

Todd Zeiger
Director
Northern Regional Office
Indiana Landmarks

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<http://www.emaildisclaimers.com/>

Lackey, Brett

From: Todd Zeiger [TZeiger@indianalandmarks.org]
Sent: Thursday, June 16, 2011 9:02 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; John Carr
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Importance: High

Gentlemen, I am writing today concerning the recent May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587 DHPA No 5903). I have a number of questions that I am seeking clarification and guidance and depending on your responses requesting a 30 day extension on which to comment.

I appreciate receipt of a copy of the letter - especially after such a lengthy time where no information or action was taking place on the 106 review process. It is certainly helpful to get an update. After reviewing the substance of the letter, and particularly the last paragraph, I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.

Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision. Checking with other consulting parties they did not either. If it is the intention that the consulting parties were supposed to take the receipt of this carbon copy letter as our opportunity to re-engage in the 106 review process on this project (and I would add an unusual way to conduct a 106 review) then receipt of the revised purpose and need statement would be of utmost importance.

I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period. If it was in fact the intention of American Structurepoint that receipt of the copy of the May 19, 2011, letter to the Indiana Division of Historic Preservation and Archaeology serves as our opportunity to provide substantive comments, we request a 30 day extension - following the receipt of the revised purpose and need statement and replies to our other concerns and comments. If it was the intention that receipt of a copy of correspondence between the consulting engineer and the Indiana Division of Historic Preservation and Archaeology serves as consulting parties opportunity to comment we respectfully disagree with that process and request direct and professional engagement with all the parties involved. This is a significant project with the potential for profound change to historic neighborhoods and the utmost care needs to be taken to follow the process appropriately.

Thank you for your attention to this matter.

Todd Zeiger
Director
Northern Regional Office
Indiana Landmarks



AMERICAN
STRUCTUREPOINT
INC.

June 17, 2011

James A. Glass, PhD
Deputy State Historic Preservation Officer
Division of Historic Preservation and Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204


Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903

Dear Dr. Glass:

On May 19, 2011, American Structurepoint, Inc., sent a letter to the Division of Historic Preservation and Archaeology regarding the State Boulevard project. It was recently brought to our attention that the revised project Purpose and Need Statement referenced in the letter was inadvertently omitted from that mailing. Please find the Purpose and Need Statement enclosed with this letter. We apologize for this oversight, and at this time we would like to extend the comment period for an additional 15 days from the receipt of this letter.

Your continued cooperation will be critical to the development of this project as we continue the Section 106 process. As such, if upon review of this information you have additional questions or comments pertaining, we respectfully request that you provide such comments within 15 days from receipt of this letter. If there are any questions or comments, please contact me at (317) 547-5580 or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Brett W. Lackey
Environmental Scientist

BWL:cgh

Enclosures

IN20071404

Dr. James A. Glass

June 17, 2011

Page 2

cc: Karie Brudis
Angie Quinn
Michael Galbraith
Don Orban
Todd Zeiger
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Shan Gunawardena
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Dan Avery
Suzanne Slick
Annette Dailey
Joyce Newland
John H. Shoaff
Patrick Carpenter
Camille Fife
Creager Smith
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst
John Carr
Amy Johnson

State Boulevard Project DES#0400587

The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E

Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E

Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element. According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.

Curve Radius Table:

Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)
18+66.60	175 ft	300 ft
24+64.47	243 ft	300 ft
27+23.73	210 ft	300 ft

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway's safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a critical Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 ft along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 ft along State Boulevard to safely make the right turn maneuver. As noted in the "Intersection Sight Distance Table" below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard. and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11
State Boulevard. and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38
State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

Lackey, Brett

From: Michael Galbraith [mgalbraith@archfw.org]
Sent: Friday, June 17, 2011 10:46 AM
To: Jim Glass; Lackey, Brett
Cc: Michael Galbraith; Todd Zeiger; jcarr@dnr.in.gov; Jill Downs; John Shoaff; Julie Donnell; Michelle Briggs-Wedaman
Subject: State Boulevard Reconstruction Project Des No. 0400587 DHPA 5903

Dear Dr. Glass and Mr. Lackey-

I am writing today concerning the May 19, 2011, letter from American Structurepoint to the Indiana Division of Historic Preservation regarding the on-going State Boulevard Reconstruction Project (Des. 0400587; DHPA No. 5903) in Fort Wayne, Allen County, Indiana. I have a number of concerns and questions.

1. I am unsure how this letter fits into the Section 106 process and the ability of Consulting Parties and the public to provide meaningful comment and criticism. Although I contacted Mr. Lackey by phone asking whether comment would be accepted from the Consulting Parties in regard to this letter, my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as mere footnote. Our Dec. 8, 2009, comments regarding the HPR and our comments from the initial Consulting Parties (Dec. 15, 2009) remain unaddressed. I request that our concerns and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed, and that we be given time in which to respond to those answers.
2. It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project Purpose and Need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in Purpose and Need.

Michael Galbraith
Preservation Specialist, ARCH, Inc.
818 Lafayette Street, Fort Wayne, IN 46802
mgalbraith@archfw.org
260.4265117

Lackey, Brett

From: Julie Donnell [juliemarie57@earthlink.net]
Sent: Friday, June 17, 2011 11:15 AM
To: Lackey, Brett; James Glass
Cc: Mike Galbraith; Todd Zeiger; Jill Downs; Michelle Briggs Wedaman
Subject: State Boulevard Reconstruction Project (Des. No. 0400587; Project No: IN20071404
Attachments: Structurepoint response 6.13.11.doc

Dear Dr. Glass and Mr. Lackey;

Attached is a letter which outlines the Friends of the Parks' concerns about the the recent communication between DHPA and Structurepoint, which I recieved a copy of earlier in the month.

In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.

We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.

My formal letter is attached.

Sincerely,
Julie Donnell
President, Friends of the Parks of Allen County, Inc.

Julie Donnell
juliemarie57@earthlink.net
EarthLink Revolves Around You.

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

July 1, 2011

Brett Lackey
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

Dear Mr. Lackey,

I am now in receipt of the Purpose and Need (P and N) statement which you intended but failed to include in your letter of May 19, 2011 to Dr. James Glass. It presents a problem: there are there material discrepancies between the accident counts in your new P and N statement and those sent by Hayley Steele of your firm to the Historic Preservation Specialist (now executive director) of ARCH on February 4, 2010 and also sent by the City of Fort Wayne to the Brookview Neighborhood Association president. Discrepancies occur in every one of the accident totals for the intersections of State Boulevard at Spy Run, Clinton Street, Eastbrook Dr., and Westbrook Dr. in the years 2007 and 2008.

In all but 2 of the 8 instances, your figures, relative to the Feb. 4, 2010 and City accident reports, raise the RMV (Rate per Million entering Vehicles) from 2.0 or below (typically well below) - the threshold above which there is a safety problem - to a number above 2.0. The tables you sent in February 2010 and the table that is part of the new P and N statement are included. A summary of the discrepancies:

	<u>Feb. 2010/City Reports</u>	<u>New P & N Figures</u>
State and Westbrook: 2007	7 accidents;	16 accidents
2008	11	17
State and Eastbrook; 2007	3	17
2008	6	17
State and Clinton: 2007	28	41
2008	33	49
State and Spy Run; 2007	36	34
2008	33	35

Feb. 4, 2010/

Your discrepancies are remarkably consistent in providing support for the argument that you are clearly so eager to make but that the 'city figures will not support, namely that, based on RMVs, there are serious safety problems at the four intersections. Only at the Spy Run intersection do the RMVs remain similar. Calculating from your data, the RMVs, when corrected for the Feb.4, 2010/City figures, work out to (in the order listed above, 2007 listed before 2008): State and Westbrook, 1.3, 1.5.; State and Eastbrook, .43, .92; State and Clinton, 1.9, 2.2; State at Spy Run, 2.16, 2.0.

Two points are noteworthy: the RMVs generated by your figures are much higher than the city's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning of and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.

With respect to the Spy Run and Clinton intersections, it strains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part; namely, to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.

The discrepancies in the accident figures raise questions that are both obvious and serious. I await your response.

Sincerely yours,


John H. Shoaff,
Fort Wayne City Councilman At-large

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (ft)	Req'd Sight Distance (ft)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists.

Crash Location	2007				2008				2009			
	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV	Total Crash	Total Injury Crash	Total Fatal Crash	RMV
State Boulevard. and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11
State Boulevard. and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38
State Boulevard. And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48
State Boulevard. and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16

Intersection of State Blvd. & Cass St.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1	1		3				1	
Rear End	2			1			2			2		
Left-Turn	1									1		
Right-Turn							2					
Side-Swipe										1	1	
Head On												
Off Road				1								
Pedestrian/Bicycle												
Total	4	0	0	3	1	0	7	0	0	4	2	0
Total per Year	4			4			7			6		

Crash Rate (Crashes per MEV)	0.74
Number of years	4
Entering Vehicles	19,500
Total Accidents	21

Intersection of State Blvd. & Westbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	1			1			3			1		
Rear End	1			2	1		2	1		1	1	
Left-Turn												
Right-Turn												
Side-Swipe	2						1			2		
Head On		1								4	2	
Off Road	3			1			2					
Pedestrian/Bicycle												
Total	7	1	0	4	1	0	8	1	0	8	3	0
Total per Year	8			5			9			11		

Crash Rate (Crashes per MEV)	1.25
Number of years	4
Entering Vehicles	18,070
Total Accidents	33

Intersection of State Blvd. & Eastbrook Dr.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	2			2						1	1	
Rear End							1					
Left-Turn										1		
Right-Turn												
Side-Swipe												
Head On	1							1				
Off Road	1						1			3		
Pedestrian/Bicycle												
Total	4	0	0	2	0	0	2	1	0	5	1	0
Total per Year	4			2			3			6		

Crash Rate (Crashes per MEV) 0.36
Number of years 4
Entering Vehicles 17,350
Total Accidents 9

Intersection of State Blvd. & Oakridge Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End	2			3			2					
Left-Turn												
Right-Turn												
Side-Swipe				1								
Head On												
Off Road												
Pedestrian/Bicycle												
Total	2	0	0	3	1	0	2	0	0	0	0	0
Total per Year	2			4			2			0		

Crash Rate (Crashes per MEV) 0.32
Number of years 4
Entering Vehicles 17,250
Total Accidents 8

Intersection of State Blvd. & Terrace Rd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle												
Rear End										1		
Left-Turn												
Right-Turn												
Side-Swipe												
Head On	1											
Off Road												
Pedestrian/Bicycle												
Total	1	0	0	0	0	0	0	0	0	1	0	0
Total per Year	1			0			0			1		

Crash Rate (Crashes per MEV)	0.08
Number of years	4
Entering Vehicles	17,250
Total Accidents	2

Intersection of State Blvd. & Clinton St. (SB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	4	3		8	1		7	1		3	3	
Rear End	15	2		10	1		10	1		13	2	
Left-Turn	3			2	1		2			2	1	
Right-Turn	1									1		
Side-Swipe	5			2			3	2		6		
Head On	1							1				
Off Road							1			2		
Pedestrian/Bicycle												
Total	29	5	0	22	3	0	23	5	0	27	6	0
Total per Year	34			25			28			33		

Crash Rate (Crashes per MEV)	2.00
Number of years	4
Entering Vehicles	41,000
Total Accidents	120

Intersection of State Blvd. & Spy Run (NB)												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PD	PI	F
Right Angle	3	2		3			3			1	4	
Rear End	13			13	1		17	2		15	3	
Left-Turn	3	2		2	2		6	1		2		
Right-Turn				2								
Side-Swipe	10	1		6			6			6		
Head On										1		
Off Road	1						0	1		1		
Pedestrian/Bicycle		1		1	2							
Total	30	6	0	27	5	0	32	4	0	25	8	0
Total per Year	36			32			36			33		

Crash Rate (Crashes per MEV)	1.91
Number of years	4
Entering Vehicles	49,250
Total Accidents	137

West 200 block of State Blvd.												
Type of Accident	2005			2006			2007			2008		
	PD	PI	F	PD	PI	F	PD	PI	F	PI	F	F
Right Angle												
Rear End	1						1					
Left-Turn												
Right-Turn												
Side-Swipe							2					
Head On										1		
Off Road	2	1					1					
Pedestrian/Bicycle												
Total	3	1	0	0	0	0	4	0	0	0	1	0
Total per Year	4			0			4			1		

Crash Rate (Crashes per 100 Million VM)	3.14.33
Length of Segment	0.12
Number of years	4
ADT	16,500
Total Accidents	9

Division of Historic Preservation & Archaeology 402 W. Washington Street, W274 Indianapolis, IN 46204-2739
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



July 5, 2011

Brett W. Lackey
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Response to SHPO's January 27, 2010, and March 10, 2010, letters and revised purpose and need statement concerning the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated May 19, 2011 and received on May 23 and the revised purpose and need statement submitted with your cover letter dated June 17, 2011 and received on June 20, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Thank you for responding the questions we had raised in our earlier letters. We continue to think, however, that it might be appropriate to expand the Section 106 area of potential effects ("APE"), if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. You indicated on page 4 of your May 19 letter that the proposed APE was based on view sheds of State Boulevard from nearby properties and that indirect impacts outside the APE will be evaluated during the NEPA process. We believe that any reasonably foreseeable direct or indirect effect should be taken into consideration in determining the APE (36 C.F.R. § 800.16[d]). Certainly, view sheds of areas where demolition or construction will take place are appropriate considerations (see 36 C.F.R. § 800.5[a][2][v]), but we think that areas where the character or use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well (see 36 C.F.R. § 800.5[a][2][iv]).

A case in point may be found in one of the rationales given in the revised purpose and need statement in support of the need for this project: "As the Transportation Plan [for the Fort Wayne Urbanized Area] has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor" (page 4). Improvements listed there include widening the St. Joseph River bridge east of Spy Run Avenue, and widening State Boulevard between Spy Run and Cass Street, between Goshen Avenue and Cass, and between Coliseum Boulevard and Maplecrest Road. In retrospect, it seems to us that, given those improvements to the west and east of the project area under review here—especially those closest to this project area—it might have been foreseeable that there eventually would be a perceived need to improve the two-lane stretch of State Boulevard between Cass Street and Spy Run. In a similar way, it seems to us that it now may be foreseeable that traffic patterns on side streets to the north and south of State Boulevard would change and that those changes could affect historic properties. Consequently, we again request that consideration be given to expanding the Section 106 APE, even if such changes in traffic patterns also would be evaluated in the NEPA process.

We have found no basis to challenge the conclusions of the purpose and need statement. We assume that the facts stated there are accurate, and we have no knowledge of any other information that would contradict the conclusions drawn on them. Because you have indicated that the purpose and need statement has been developed with FHWA and Indiana

Department of Transportation involvement and has received FHWA's approval, we assume, therefore, that those agencies believe that the revised purpose and need statement is persuasive. We have been copied on at least some of the consulting parties' responses to your May 19 letter, and they have identified a number of historic preservation, economic, pedestrian safety, and neighborhood livability values that they believe should be promoted. The revised purpose and need statement identifies the transportation mobility, connectivity, and accessibility and traffic safety values that the City of Fort Wayne wishes to promote. The Section 106 process is not designed to choose among those values; rather, it is intended to ensure that a reasonable opportunity is provided for consulting parties and the public to express their views on effects on historic properties and that the federal agency responsible for the project takes those effects into consideration and gives the Advisory Council on Historic Preservation an reasonable opportunity to comment.

The revised purpose and need statement indicates that two other, possible east-west corridors also were considered, but the State Boulevard Corridor was considered the most practical. It goes on to indicate that these three alternatives will be discussed in the NEPA process. We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevant to the Section 106 process, because 36 C.F.R. § 800.6(a) requires that there be consultation about alternatives that might avoid, minimize, or mitigate adverse effects on historic properties. The alternatives presented to the consulting parties so far appear to be minor variations on the State Boulevard Corridor alternative. It is unclear to us how the alternatives analysis for NEPA and Section 106 purposes for this project as a whole would dovetail with Stipulation III.B. of the Historic Bridges PA, which would seem to apply specifically, and perhaps exclusively, to the Non-Select State Boulevard Bridge over Spy Run. Perhaps FHWA can clarify how the alternatives analysis should be handled.

Some of the consulting parties have expressed concerns that although you have responded to the Indiana SHPO's previous questions, not all of theirs have been addressed. In light of these comments, you may want to review previous correspondence from the consulting parties and the minutes of the December 15, 2009 consulting parties meeting and make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 19 letter. Sometimes in Section 106 consultations, there is a tendency to give greater weight to the Indiana SHPO's concerns than to those submitted by other consulting parties. While the 36 C.F.R. Part 800 regulations assign a somewhat different role to the SHPO than to the other consulting parties, the regulations also provide for the federal agency to consider issues and concerns raised by other consulting parties (see 36 C.F.R. §§ 800.5[c][2][i] and 800.6[a]).

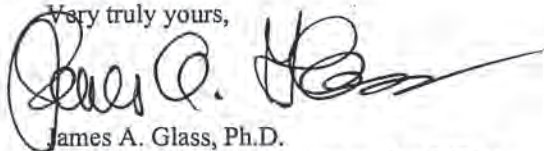
It occurs to us that not all of the consulting parties may be aware of comments being offered by other consulting parties. We are aware that the usual practice is to provide copies of consulting party comments in the documentation that is provided with the finding. However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in response to your May 19 and June 17 letters.

As previously stated in our December 14, 2009, letter to Hayley M. Steele, Environmental Specialist at American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Patrick Carpenter, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Brett W. Lackey, American Structurepoint, Inc.

Lackey, Brett

From: Lackey, Brett
Sent: Wednesday, July 06, 2011 3:17 PM
To: 'aquinn@archfw.org'; 'mgalbraith@archfw.org'; 'tmn@barrettllaw.com'; 'mbwedaman@verizon.net'; 'shan.gunawardena@ci.ft-wayne.in.us'; 'Creager Smith'; 'joyce.newland@dot.gov'; 'jshoaff@proparkwest.com'; 'don.orban@cityoffortwayne.org'; 'juliemarie57@earthlink.net'; 'jandailey59@msn.com'; 'tzeiger@indianalandmarks.org'; 'indianabridges@sbcglobal.net'; 'Carpenter, Patrick A'; Kennedy, Mary; 'sjslick@mac.com'; 'ross@martin-riley.com'; 'danavery@co.allen.in.us'; 'albertcohan@aol.com'; 'jcooper@ccrtc.com'; 'dan@earthsouceinc.net'; 'jcarr@dnr.in.gov'; 'wtharpe1@dnr.in.gov'
Cc: Hope, Briana
Subject: State Boulevard - Section 106 Comments
Attachments: IN20071404.EV.2011-04-21.LTR.Response to SHPO Letters.bwl.pdf; State Blvd P&N Statement to CPs.pdf

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who still intend to comment, please provide comments by next Tuesday July 12th. Please let me know if there are any questions.

Thanks!

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
317.850.0257 | cell
www.structurepoint.com

From: Suzanne [sjslick@me.com]
Sent: Wednesday, July 06, 2011 4:18 PM
To: Lackey, Brett; mayor@ci.ft-wayne.in.us
Cc: aquinn@archfw.org; mgalbraith@archfw.org; tmn@barrettlaw.com; mbwedaman@verizon.net; shan.gunawardena@ci.ft-wayne.in.us; Creager Smith; joyce.newland@dot.gov; jshoaff@proparkwest.com; don.orban@cityoffortwayne.org; juliemarie57@earthlink.net; jandailey59@msn.com; tzeiger@indianalandmarks.org; indianabridges@sbcglobal.net; Carpenter, Patrick A; Kennedy, Mary; rross@martin-riley.com; danavery@co.allen.in.us; albertcohan@aol.com; jcooper@ccrtc.com; dan@earthsouceinc.net; jcarr@dnr.in.gov; wtharpe1@dnr.in.gov; Hope, Briana
Subject: Re: State Boulevard - Section 106 Comments

Dear Brett,

I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State Boulevard correspondence. It seems we are missing a wonderful opportunity to create a beautiful and useful roadway system in this project.

What we will have is a massive concrete thoroughfare that will be unfriendly to pedestrians and bicyclists and probably to drivers, as well. There is no attempt at traffic calming, but a great emphasis on traffic rushing. There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability. Other cities are making great strides in building roadways that are user friendly, and safe for everyone -- not just car and truck drivers -- and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.

Very sincerely,
 Suzanne Slick
 Irvington Park Consulting Party
 Fort Wayne

On Jul 6, 2011, at 3:16 PM, Lackey, Brett wrote:

State Boulevard Project Consulting Parties,

You should have received a hardcopy of the attached letters. Although the letters were addressed specifically to the DHPA, all consulting parties were copied and all consulting parties are/were welcome to submit comments within the 30-day time period and additional 15-day time period specified in the letters. It was recently brought to our attention that not all consulting parties were aware that they could also provide comments on the letters.

For future reference, if you have elected to be a consulting party for this project, you are encouraged to provide us with comments on any correspondence that you receive either directly or as a "cc" during this Section 106 process.

We have already received comments from several of you regarding the attached letters. For those of you who

still intend to comment, please provide comments by next Tuesday July 12th. Please let me know if there are any questions.

Thanks!

Brett W. Lackey
Environmental Scientist
BLackey@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
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<http://www.emaildisclaimers.com/><IN20071404.EV.2011-04-21.LTR.Response to SHPO Letters.bwl.pdf><State Blvd P&N Statement to CPs.pdf>

Lackey, Brett

From: Michelle Briggs Wedaman [mbwedaman@frontier.com]
Sent: Thursday, July 07, 2011 9:05 AM
To: Lackey, Brett
Subject: State Blvd Project Ft Wayne contact info update Brookview Neighborhood

Importance: High

7/7/2011

Hi Brett,
Your 7/6/11 email was forwarded to me by another consulting party member.
Could you update my email from the Verizon to:
mbwedaman@frontier.com

I plan on submitting comment on behalf of the neighborhood.

Thanks,
Michelle Briggs Wedaman
Brookview Neighborhood president, Ft Wayne
260.710.4413



State Boulevard Section 106 Agency Coordination Meeting MEETING MINUTES

Location: INDOT Central Office Room N642
Date: 7/13/2011
Project Name: State Boulevard Reconstruction
Project No.: IN20071404
Owner: City of Fort Wayne
Attendees: John Carr – IDNR DHPA
Scott Crites – Structurepoint
Jim Glass – IDNR DHPA
Briana Hope – Structurepoint
Jason Kaiser – INDOT Fort Wayne District
Mary Kennedy – INDOT Cultural Resources Office
Brett Lackey – Structurepoint
Ben Lawrence – INDOT Office of Environmental Services
Joyce Newland – FHWA
Amanda Ricketts – IDNR DHPA
Greg Smith – INDOT Fort Wayne District
Wade Tharp – IDNR DHPA
Minutes By: Brett Lackey

The following notes reflect our understanding of the discussions and decisions made at this meeting.
If you have any questions, additions, or comments, please contact the issuer of these minutes.

ITEMS DISCUSSED:

- ❖ The meeting opened with introductions and an overall project update by American Structurepoint. Structurepoint also explained that the purpose of the agency coordination meeting was to discuss the following items:
 - SHPO's comments on recent Purpose and Need submission
 - How to address Consulting Parties comments
 - Section 4(f)
 - Inviting ACHP involvement
- ❖ As requested in the July 5, 2011 DHPA letter the re-evaluation of extending the APE to the north to accommodate the potential for added traffic through that neighborhood was discussed.
 - American Structurepoint explained that the project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. American Structurepoint also stated that it is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project.
 - DHPA requested that this be stated in a formal response to the July 5, 2011 letter and copied to all consulting parties.

- ❖ DHPA also asked about consulting parties request to extend the APE east and west.
 - American Structurepoint explained that while some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE.
 - INDOT Fort Wayne District added that the travel patterns in the Fort Wayne area are well established and that it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.
- ❖ DHPA asked about the other alternatives discussed in the revised Purpose and Need.
 - American Structurepoint explained that two additional corridors (Butler Rd-Vance Rd and Spring St-Tennessee Ave) were considered and discarded due to the need for new roadway alignments, relocations, historical impacts, park impacts, and potential hazardous waste impacts.
 - All agencies agreed that more details were needed for the other two east-west corridors studied.
 - Strong discussion of alternatives will be included in both the NEPA document and 4(f) document. American Structurepoint pointed out that consulting parties will have the opportunity to review the alternatives analysis as part of the 4(f) process. American Structurepoint will discuss the alternative analysis in more detail as part of the next consulting parties meeting.
- ❖ DHPA suggested that American Structurepoint coordinate with their National Register experts to determine if the project would result in a need to change the district boundaries.
- ❖ DHPA asked if all consulting party comments had been addressed.
 - American Structurepoint commented that the significant comments related to the Section 106 Process were addressed in the May 19, 2011 DHPA response letter.
 - DHPA suggested that American Structurepoint more specifically address the consulting parties issues and comments in coordination specifically addressed to the consulting parties.
 - Structurepoint suggested creating a spreadsheet identifying each consulting party and their specific comment with a response to the comment. Structurepoint committed to sending this document to consulting parties with the invitation to the next consulting parties meeting.
- ❖ American Structurepoint suggested FHWA invite the Advisory Council on Historic Preservation (ACHP) to participate at this time since the project seems to be controversial.
 - All agencies agreed ACHP should be invited to participate in the State Boulevard project now, rather than later in the Section 106 process.

ACTION ITEMS:

- ❖ Structurepoint will respond to the idea of extending the APE in writing via letter to DHPA.
- ❖ Structurepoint will formally request, via letter, FHWA coordinate with ACHP and request their involvement on the project at this time.
- ❖ Structurepoint will produce a chart with questions/answers that addresses all consulting party and agency comments received to-date – this will be provided to all consulting parties prior to next consulting party meeting.

NEXT MEETING:

Consulting Party Meeting (Date TBD)

cc: Attendees
Consulting Parties

Very truly yours,
American Structurepoint, Inc.

A handwritten signature in black ink, appearing to read 'B. Lackey', with a stylized flourish extending to the right.

Brett Lackey
Environmental Scientist



M E M O R A N D U M

DATE: August 15, 2011

TO: Ms. Angie Quinn, ARCH Inc.
Ms. Jill Downs, ARCH Inc.
Mr. Michael Galbraith, ARCH Inc.
Mr. Don Orban, Fort Wayne Historic Preservation Commission
Mr. Todd Zeiger, Indiana Landmarks
Ms. Julie Donnell, Friends of the Parks of Allen County
Ms. Michelle Briggs-Wedaman, Brookview Civic Neighborhood Association
Dr. James Cooper
Mr. Paul Bandenburg, Indiana Historic Spans Task Force
Mr. Shan Gunawardena, City of Fort Wayne
Ms. Susan Haneline, Brookview Civic Neighborhood Association
Mr. Dan Avery, Northeastern Indiana Regional Coordination Council
Ms. Suzanne Slick, Irvington Park Neighborhood Association
Ms. Jan Dailey, Brookview Civic Neighborhood Association
Ms. Joyce Newland, Federal Highway Association
Mr. John Shoaff, Fort Wayne City Council
Mr. Jason Kaiser, INDOT Fort Wayne District
Mr. Patrick Carpenter, INDOT Cultural Resources
Ms. Mary Kennedy, INDOT Cultural Resources
Ms. Camille Fife, Westerly Group
Mr. Creager Smith, Fort Wayne Office of Planning and Policy
Mr. Albert Cohan, Westbrook 5, LLC
Mr. Thomas Niezer, Barrett & McNagney, LLP
Mr. Ronald Ross, Martin Riley Architects and Engineers
Mr. Dan Ernst, Earth Source Inc.
Dr. James Glass, IDNR DHPA
Mr. John Carr, IDNR DHPA
Ms. Amy Johnson, IDNR DHPA
Ms. Amanda Rickets, IDNR DHPA
Mr. Wade Tharp, IDNR DHPA
Mr. Tom Cain, Fort Wayne Redevelopment

FROM: Brett W. Lackey, American Structurepoint, Inc.

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, American Structurepoint, Inc.

This memo is to notify you that a Section 106 Consulting Parties Meeting regarding the above mentioned project has been scheduled for Thursday, September 1 at 9:30 am. The meeting will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030 located in the Garden Level of Citizens Square.

Please review the enclosed materials prior to the meeting. I can be reached by phone at (317) 547-5580 or by e-mail at blackey@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Agenda
Agency Coordination Meeting Minutes (7/13/2011)
Letter to IDNR DHPA
Letter to FHWA
Historic Bridge Programmatic Agreement Alternatives Analysis
Individual Section 4(f) Alternatives Analysis
Corridor Alternatives Map
Consulting Party Questions/Comments and Responses

AGENDA
Consulting Parties Meeting
State Boulevard Reconstruction (Des. No. 0400587)
City of Fort Wayne, Allen County, Indiana

Thursday, September 1, 2011
9:30 AM
Room 030 (Garden Level)
Citizens Square
200 East Berry Street
Fort Wayne, IN 46802

1. Project Update
 - a. Purpose and Need
 - b. Consulting Party Comments/Responses
2. Project Alternatives Review
 - a. Minimization Measures
3. Future Steps in the Process
 - a. Potential Mitigation Measures
 - b. Development of Memorandum of Agreement
4. Follow-up items



AMERICAN
STRUCTUREPOINT
INC.

August 15, 2011

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN200701404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated July 5, 2011.

The letter states that the DHPA believes that it may be appropriate to expand the APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project. The proposed project will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access, but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut-through traffic. The project would likely draw current cut-through traffic out of the neighborhoods, because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established, and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor. The traffic pattern alteration is not anticipated to result in any adverse impacts.

Your letter also indicates that a discussion of the other two possible east-west corridor alternatives mentioned in the project's purpose and need statement would be relevant to the Section 106 process. Alternatives have continued to be developed throughout the Section 106 process and an alternatives analysis will be provided to all consulting parties prior to the next (second) consulting parties meeting. Please see the following descriptions of the two possible east-west corridor alternatives:

Alternative 1: Butler Road – Vance Road Corridor

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run/Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard. This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land. This alternative avoids impacts to historic properties identified within the APE of the project; however, the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District, as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue Bridge over the St. Joseph River, a select historic bridge which has been determined eligible for the National Register of Historic Places.



Dr. James A. Glass
August 15, 2011
Page 3

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park. This alternative avoids impacts to historic properties identified within the APE of this project; however, the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Your July 5, 2011, letter also expresses concern that not all consulting parties feel that their comments have been adequately addressed. The letter also indicates that not all consulting parties are aware of comments being provided by other consulting parties. In an effort to both address individual comments and distribute comments amongst consulting parties, we have created a table with all comments received thus far, as well as individual responses to all comments. This document shall be distributed to all consulting parties for review prior to the next consulting parties meeting.

Concern regarding how the project may affect the proposed boundaries of the Brookview – Irvington Park National Register District has also been expressed. As such, at this time we respectfully request the DHPA's assistance in soliciting the opinion of the appropriate National Register authority regarding the potential for the project to impact the currently proposed boundaries of the Brookview – Irvington Park National Register District.

We hope that the information in this letter adequately addresses the concerns expressed in the July 5, 2011, DHPA letter. The information herein will be provided to all consulting parties prior to the next consulting parties meeting and will also be discussed during that meeting. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Brett W. Lackey
Environmental Scientist

BWL:alo

Enclosures

cc: John Carr
Wade Tharp
Amanda Ricketts
Mary Kennedy
Patrick Carpenter

Dr. James A. Glass

August 15, 2011

Page 4

Staffan Peterson, Ph.D.
Jason Kaiser
Greg Smith
Joyce Newland
Dan Avery
Creager Smith
Shan Gunawardena
Camille Fife
Angie Quinn
Michael Galbraith
Jill Downs
Todd Zeiger
Don Orban
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Suzanne Slick
Annette Dailey
John Shoaff
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst



AMERICAN
STRUCTUREPOINT
INC.

August 15, 2011

Ms. Joyce Newland
Planning/Environmental Specialist
Indiana Division - Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Re: Request for ACHP Involvement
State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Ms. Newland:

The purpose of this letter is to initiate the coordination necessary for involvement of the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Reconstruction Project in Fort Wayne, Allen County, Indiana. As you know, ACHP involvement in the Section 106 process of an individual project must be solicited by the project's lead Federal agency. Therefore, at this time we respectfully ask that the Federal Highway Administration (FHWA) requests the involvement of the ACHP in this project.

We believe that ACHP involvement is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. Specifically, the State Boulevard Project meets the criteria of **Part (c)(1)** for “...adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district” and **Part (c)(3)** for “...cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...”

If any additional information or supplemental documentation is needed in order for FHWA to request ACHP involvement, please do not hesitate to contact me at (317) 547-5580, or at blackey@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.

Brett W. Lackey
Environmental Scientist

BWL:alo

Ms. Joyce Newland

August 15, 2011

Page 2

cc: James Glass, Ph.D
John Carr
Wade Tharp
Amanda Ricketts
Mary Kennedy
Patrick Carpenter
Staffan Peterson, Ph.D.
Jason Kaiser
Greg Smith
Joyce Newland
Dan Avery
Creager Smith
Shan Gunawardena
Camille Fife
Angie Quinn
Michael Galbraith
Jill Downs
Todd Zeiger
Don Orban
Julie Donnell
Michelle Briggs-Wedaman
James L. Cooper
Paul Brandeburg
Susan Haneline
Charlie Shirmeyer
Karl Dietsch
Suzanne Slick
Annette Dailey
John Shoaff
Albert Cohen
Thomas M. Niezer
Ronald Ross
Dan Ernst

Alternatives Analysis – Historic Bridge PA
State Boulevard Bridge No. 546 over Spy Run Creek
State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)

1. Rehabilitation for Continued Vehicular Use (two-way option)

This alternative involves rehabilitating the existing bridge and leaving it in its current location. The existing two-way traffic configuration of the bridge would be maintained. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The

State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

2. Rehabilitation for Continued Vehicular Use (one-way option)

This alternative involves rehabilitating the existing bridge in its current location and constructing a new parallel bridge. The existing structure would be rehabilitated to replace any damaged or deteriorated structural components and reconfigured for one-way traffic. The new, parallel bridge would be constructed to carry one-way traffic in the opposite direction of the existing rehabilitated structure. This alternative would maintain the existing bridge and would require that the overall project alignment be modified in order to maintain the existing State Boulevard alignment and utilize the existing structure.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary’s River which causes the bridge to be overtopped with backwater from the Saint Mary’s River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) “This flooding is caused primarily by backwater from the St. Mary’s River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

3. Bypass (Non-vehicular use)

This alternative involves maintaining the bridge in-place by a third party for non-vehicular use. A new bridge structure would be designed as part of the State Boulevard project which would meet the current safety and capacity needs.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative does not meet the project purpose and need, specifically the purpose of alleviating flooding along the roadway corridor. By rehabilitating the existing structure in-place, the project would not be able to elevate the State Boulevard Roadway by the proposed 7 feet, which would alleviate flooding in the location of the existing bridge No. 546. Furthermore, State Boulevard must be re-aligned and widened in the area of the bridge in order to meet current INDOT design and safety standards.

This alternative is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure.

This alternative is not prudent because the existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. Our office has reviewed the 2006 Structural Inventory and Appraisal Report (SAI) for Allen County Bridge 546. The structure is a cast-in-place reinforced concrete girder bridge built in 1927. The concrete girders were in serious condition with large spalls and exposed rusted rebar. According to the SAI, the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. Sufficiency ratings of 50 to 80 are considered for rehabilitation, while those under 50 are usually replaced or closed. The SIA report recommended replacement and due to the extremely poor condition of the R/C girders the estimated remaining life of the bridge superstructure is 5 years from the date of the inspection report (2006). The SAI report indicated the structure has the potential to be historic. If the structure were to be rehabilitated it would likely require a complete superstructure replacement eliminating the elements that would contribute to its need for preservation.

The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The

State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street.”

4. Replacement

This alternative involves removal and replacement of the existing bridge. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

This alternative is feasible because it meets the current design standards. This alternative is prudent as it is cost effective and meets the project purpose and need.

5. Relocate and Replacement

This alternative is similar to Alternative 4 but would include relocation of the existing bridge by a third party. The bridge would be relocated off-site and re-used for pedestrian use or rehabilitated for vehicular use by a third party. A new structure would be designed to meet the current safety and capacity needs. There are several alternative structures which are under consideration with regards to the replacement bridge specifications. As part of the State Blvd project, the roadway would be elevated approximately 7ft to alleviate roadway flooding in the location of the existing bridge No. 546 over Spy Run Creek. The existing State Blvd would also be re-aligned and widened from 2 lanes to 4 lanes in the location of existing bridge No. 546 to meet INDOT design and safety standards. A new 4-lane bridge structure would be constructed to carry the re-aligned and widened State Boulevard over Spy Run Creek.

The existing structure would be marketed for re-use per the Historic Bridge Programmatic Agreement. The bridge would be posted as available for re-use on the INDOT Website and in the Fort Wayne Journal-Gazette and advertisements would be posted on the bridge offering it for re-use. Proposals would be accepted for the immediate rehabilitation and reuse or for its storage for future reuse. Proposals would also be accepted for the salvage of elements that may be stored for future repairs of similar historic bridges. To date no responsible party has come forward to fund the preservation or maintenance of the existing bridge.

This alternative is feasible only if the existing bridge is relocated for non-vehicular use. Relocation for continued vehicular use is not feasible because the minimum design standards in the Indiana Design Manual cannot be addressed by rehabilitating the existing structure for vehicular use. While this alternative does meet the project’s purpose and need, it is prudent only if a third party comes forward to fund the rehabilitation, preservation, and maintenance of the existing bridge for non-vehicular use.

Alternatives Analysis – Individual Section 4(f)
State Boulevard Reconstruction Project – Fort Wayne, Indiana (Des# 0400587)

Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at the Riverside School, Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly

Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 16 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 14 residential relocation from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

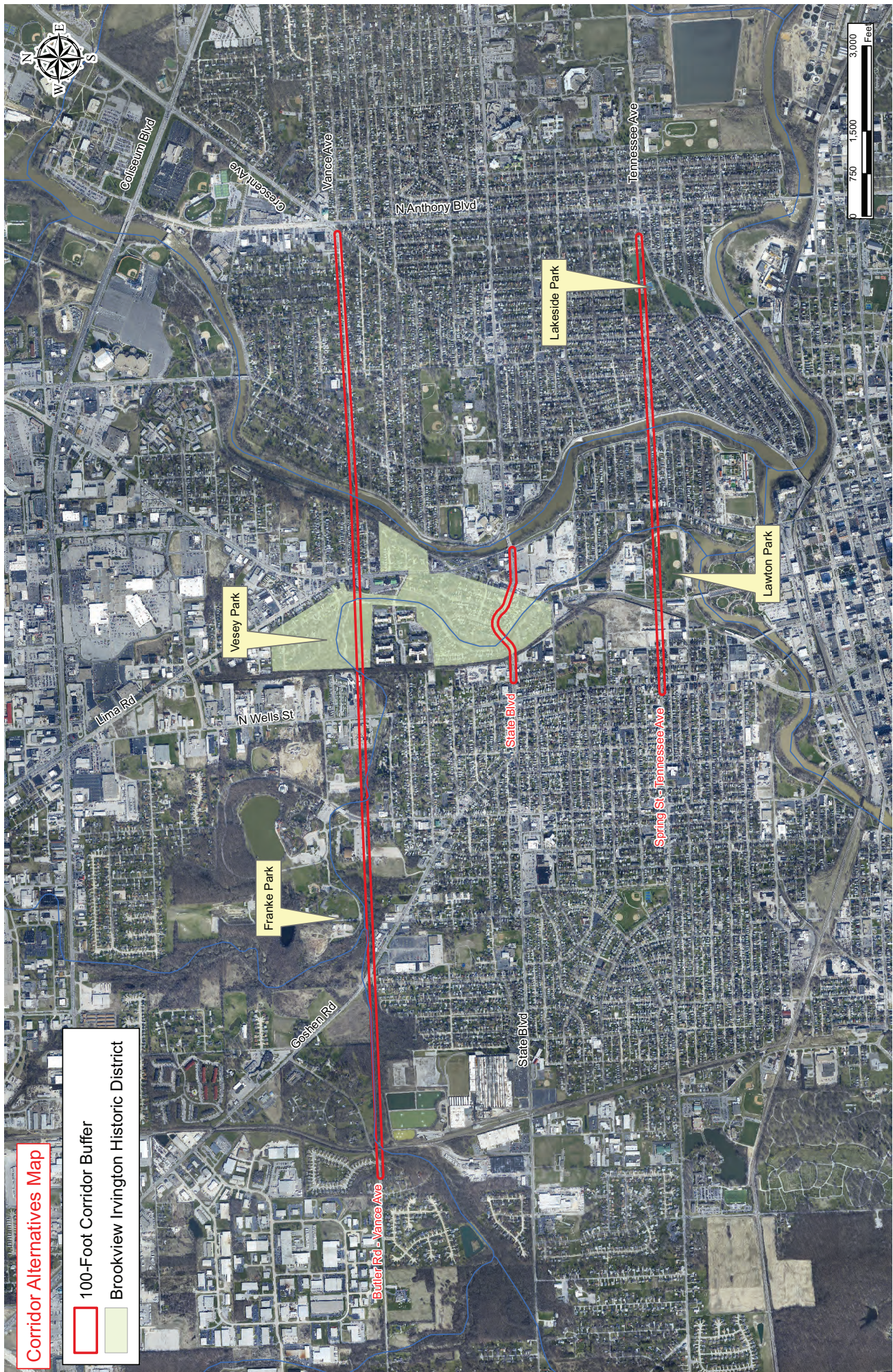
The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage.

This alternative would avoid impacts to historic properties.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.



Comment No.	Name / Organization / Comment Date	Comment	Response
1	Jan Dailey – Brookview Neighborhood Resident 12/7/2009 Letter	The recent closing of the Kroger Fuel Center may affect the plan for the roadway overall. The realignment of the road could now be accomplished by following the original design and running the bulk of the new road along the banks of Spy Run Creek. If the road were to follow the original curvilinear design along the creek originating at the base of the bridge at State boulevard and Spy Run Avenue the road would pass behind or around most of the homes in the APE. The designers of the road project could not have foreseen the closing of a major commercial venture in the middle of the project and have not proposed that other options be explored at this time. Since the HPR has been careful to take into account what this project would destroy it seems only fair that separate study be conducted factoring in possible land use of the former Kroger Fuel Center.	<p>The realignment of State Blvd to follow the bank of the Spy Run Creek would require extensive work in the channel of Spy Run Creek and the skew of the crossing would create the need for a longer bridge which would significantly increase the cost of the bridge. This alignment would also increase cost due to significant increase in the amount of required right of way and the demolition of a major commercial structure.</p> <p>A new intersection with Clinton Street would need to be constructed in close proximity to the existing State Blvd/Clinton Street intersection which would create safety concerns. This alignment would also likely cause economic impacts to the existing businesses located along existing State Blvd due to decreased visibility and accessibility</p>
2	Michael Galbraith – ARCH 12/9/2009 Letter	The HPR contained no maps, plans, or drawings of the proposed project. We have found it difficult to study, review, and analyses this project in the absence of any plans maps or drawings that show the proposed project in a contextual relationship to the proposed Brookview-Irvington Park Historic District. Could such maps, plans and drawings be provided for our review?	Maps, plans, and drawings have been developed since this comment was received. These maps, plans, and drawings were made available for review at the first CP meeting and additional drawings will be provided at the next CP meeting.
3		Regarding the Purpose and Need for the proposed project. There is a reference to the Purpose and Need in the Executive Summary of the HPR and the cover letter, but no corresponding section in the body of the document. Page 3 of the HPR states that the "Purpose of the project is to improve traffic flow, roadway, and pedestrian safety along State Boulevard." Could detailed studies, including traffic numbers and accident data be produced that justify this purpose and need? In addition it appears, according to this, that there is no purpose or need to address the hydrology of Spy Run Creek. Given this purpose and need, could it also be explained how "Adding a new trail bridge" as stated on Page 3, addresses "Traffic Flow, Roadway, and Pedestrian Safety" along State boulevard need is stated in the cover letter to "originate from the substandard horizontal curve along State boulevard."	The Purpose and Need for the project has been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.

4		<p>It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid Project, possibly with the intention of avoiding future section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. We request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project.</p>	<p>Pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
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5	<p>Regarding the substandard horizontal curve: we question the analysis of “substandard” and would like to see this curve analyzed at more than one speed parameter. We believe that this curve, a designed element of Arthur Shurcliff’s plan for Brookview, is a critical character defining feature of the historic property. We also contend that this curve is not substandard under all speed limits. As is the stated desire in planning documents for the City of Fort Wayne, we believe that traffic calming measures, like this curve in residential neighborhoods are appropriate. The HPR on page 23 actually describes our contention quite well; “Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects o the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive.”</p>	<p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway’s safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table><tr><th>Station Line “A”</th><th>Existing Curve Radius</th><th>Req’d Radius (30 mph)</th></tr><tr><td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr><tr><td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr><tr><td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr></table>	Station Line “A”	Existing Curve Radius	Req’d Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
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18+66.60	175 ft	300 ft												
24+64.47	243 ft	300 ft												
27+23.73	210 ft	300 ft												
6	<p>Was the previously determined eligible Indiana Service Corporation Historic District immediately south of State boulevard between Spy Run and Clinton studied for potential effect from this project? I can find no reference in the HPR.</p>	<p>The Indiana Service Corporation Historic District is not included in the Project’s APE and therefore was not studied for potential effect from this project. No impacts to the Indiana Service Corporation Historic District are anticipated to result from this project.</p>												

7		<p>Also regarding the APE, were potential changes in traffic use in the Brookview – Irvington Park Historic District Studied? It appears reasonable to anticipate that if traffic is limited in its access to State Boulevard from one or more roads, that a corresponding increase will be seen on those roads that still retain access to State. We believe that a proposed project as large as this will have wide-ranging effect on historic properties, particularly in traffic and circulation patterns and that the potential effects of those changes must be examined, whether that is indirect or direct. Furthermore we believe that truncating and altering the designed residential suburb's circulation pattern is a direct adverse effect on the character defining features of this district. We believe that the APE must be expanded to include at a minimum all of the Shurcliff-designed Brookview plats.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood due to cut through traffic and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
8		<p>Plans for this project seem to be quite complete according to the project description in the HPR. I hope that we are, as required by 800.1(c), "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for this undertaking."</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>

	<p>We believe that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been in order to avoid Section 106 review for this project. We believe that these projects must be aggregated for Section 106 review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 review that investigation into possible violation of Section 110(k) of the NHPA would be appropriate. The City with one hand, "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur: through its own purchases and demolitions of historic properties even as the other hand applied for and received federal funds to nominate the Brookview – Irvington Park Historic District to the NRHP as well as to complete this State Boulevard Reconstruction project.</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially</p> <p>(continued on next page)</p>
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			<p>reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
9		<p>We agree that in the very recent past that seasonal flooding has caused damage to homes, but contend that this is an issue which is recent. The Brookview – Irvington Park Historic district has historically not been subject to seasonal flooding, and that development and water management both upstream and downstream has caused this relatively new effect.</p>	<p>The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Six out of the 17 years (1978, 1982, 2003, 2004, 2005, and 2008), State Boulevard was actually closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.</p>

10	Todd Zeiger – Indiana Landmarks 12/8/2009 Letter	<p>With no maps or project specific details, including an approved APE or concurrence about the purpose and need and potential 4f impacts it is improper and premature for us to comment on that preliminary finding. We have not been provided any alternatives to review. We will look forward to provision of maps, plans and drawings for the consulting parties' review and comment. Of particular interest to us will be maps, plans and information pertaining to various alternatives that are being studied to avoid an impact to the District. The inclusion of the proposed finding indicates that design of the project is fairly advanced.</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis will be provided to all CPs prior to the next (second) CPs Meeting.</p>
11		<p>Regarding the Area of Potential Effect (APE). We disagree that the APE should be "concentrated" as proposed in the HPR. The proposed realignment of State Boulevard route itself will dramatically alter State Boulevard as a contributing element to the district. Additionally, if constructed as proposed in the narration, the overall traffic flow will be altered in the entire district – that circulation itself a character defining feature of the district.</p> <p>Therefore the possible direct impact to the character defining features of the entire District needs to be evaluated and the APE should reflect the boundaries of the proposed (and currently being nominated) National Register District. The proposed project will also effect potential redistribution of neighborhood traffic, an indirect impact of the project. The APE should be expanded to include the boundaries of the proposed district in order to meet the requirements of 36 CFR 800.16 that the APE shall be established to include "the geographic area or areas within which an undertaking may <u>directly or indirectly</u> cause alterations in the character or use of historic properties". While we have not seen the alternatives under consideration, the one alternative described in the HPR will have a direct and in-direct effect on historic properties including the circulation system.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

12		How does replacing the State Boulevard Bridge meet the purpose? The bridge is before the supposed substandard curve of State Street.	<p>The purpose and need for this project has been revised since this comment was provided. The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is 5 years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p>
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13		<p>Please provide detailed studies or accident report data for pedestrian/car interaction accidents along State Boulevard between the bridge and the project terminus</p>	<p>Detailed accident data compiled by the City was included in the revised purpose and need and provided to all consulting parties.</p> <p>Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As provided in the purpose and need statement, the RMV exceeds 2.0 which indicates that a safety problem exists.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>
14		<p>Please provide detailed studies or accident report data for pedestrian/car accidents specifically related to the existing State Boulevard Bridge.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
15		<p>Please provide detailed studies or accident report data for vehicle accidents on the State Boulevard bridge specifically.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>
16		<p>Please provide detailed traffic accident studies or report data concerning vehicle accidents on State Boulevard from the State Boulevard bridge to the project terminus.</p>	<p>Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.</p>

17		Please provide detailed traffic accident studies or report data pertaining to accidents at the "substandard" curve.	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
18		Please provide detailed pedestrian accident studies or report data pertaining to the "substandard curve."	Detailed accident data compiled by NIRCC was included in the revised purpose and need and provided to all consulting parties.
19		How is the proposal to add a new "trail bridge" over State Boulevard related to the purpose and need of this project? There currently does not exist a walking trail in the area connecting to the location where the proposed bridge is to be located. Without a trail, how does the placement of a pedestrian bridge facilitate the purpose and need for this project? See also number 5 below with regard to this proposed "trail bridge".	<p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p>

20	<div data-bbox="185 787 1023 1438" data-label="Text"> <p>"Trail Bridge": We continue to be concerned about an ongoing appearance of project aggregation/co-mingling without the proper 106 review. This concern has been raised with regard to US 27 over Spy Run Creek Section 106 review (Des. No. 0200914 and 0101527). It appears as if the City of Fort Wayne is including elements for the benefit of other projects in this Federal Aid project, possibly with the intention of avoiding future Section 106 review. Nowhere else in the HPR is any connection or tie-in mentioned in regard to this trail bridge. Nowhere in the HPR is there any review of historic resources along a linear trail that may use this bridge. Nowhere else in the HPR is mentioned any reference to a proposed trail. Nor is it explained how this trail bridge will fulfill the purpose and need for this project. If this bridge is to be reviewed as a part of this 106 process then we request that any trail that is now or in the future a federal aid project using this bridge be aggregated for the purpose of 106 review into this State Boulevard reconstruction project. If the Sponsor intends to include review of the future trail and this bridge then the APE needs to be expanded to include the necessary and appropriate review of possible historic resources associated with that trail and then potential effects to historic properties along the proposed trail. If this is not the intention of the sponsor, then the discussion of the future "trail bridge" should be handled under the 106/4f for that trail project in the future and the discussion of a proposed trail bridge eliminated from this review process and the draft HPR.</p> </div> <div data-bbox="185 128 1023 787" data-label="Text"> <p>The purpose and need for this project has been revised, including additional information pertaining to the trail bridge over State Boulevard.</p> <p>The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being designed by another consulting firm as separate project. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.</p> </div>
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<p>21</p>	<p>Concerning the "substandard horizontal curve": Page 4 of the HPR states that "the existing two lane section of State Boulevard between North Clinton Street and Cass Street will be widened to five lanes while correcting the substandard curve." We believe that this curve, a designed element of Arthur Shurcliff's plan for Brookview, is a critical character defining feature of the historic property (district). This character defining feature includes the fact that, as noted on page 23 of the HPR, "Shurcliff manipulated the alignment of State Boulevard as it passed the Brookview subdivision. His design complements the curvilinear aspects of the other circulation throughout the plan. In addition, this configuration, helped define the changed landscape, providing a slower environment, more conducive to gracious living. The arc of this part of State Boulevard still serves to slow traffic, a secondary effect of the curvilinear drive." We reserve comment about the proposal to widen the road and adjust the curve until we have the detailed plans and maps for the project. It seems premature to comment about the design until an official APE is adopted and we receive more detailed information in the form of maps and drawings about the various alternatives under consideration.</p>	<p>In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>Curve Radius Table:</p> <table border="1"> <thead> <tr> <th>Station Line "A"</th><th>Existing Curve Radius</th><th>Req'd Radius (30 mph)</th></tr> </thead> <tbody> <tr> <td>18+66.60</td><td>175 ft</td><td>300 ft</td></tr> <tr> <td>24+64.47</td><td>243 ft</td><td>300 ft</td></tr> <tr> <td>27+23.73</td><td>210 ft</td><td>300 ft</td></tr> </tbody> </table>	Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)	18+66.60	175 ft	300 ft	24+64.47	243 ft	300 ft	27+23.73	210 ft	300 ft
Station Line "A"	Existing Curve Radius	Req'd Radius (30 mph)												
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<p>22</p>	<p>Project Description: The narrative in the HPR conveys that plans for this project seem to be quite complete according to the project description on page 7. We hope that this is a matter of conveying the most impactful alternative being considered and that we are, as required by 800.1(c), given an opportunity "early in the undertaking's planning, so that a broad range of alternatives may be considered during the planning process for the undertaking. It would be a significant oversight of the requirements if the consulting parties are presented one take it or leave it alternative with only minor nuances afforded us for discussion. Given the 4f implications of this project presentation of a severely limited number of alternatives will not be considered by us as having meet the requirements of 800.1(c).</p>	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis will be provided to all CPs prior to the next (second) CPs Meeting.</p>												

		<p>Historic Resources and anticipatory demolition by the sponsor or its agents: We noted with much interest the statement on page 7 that "Most of the homes along this side have been removed as part of the flood control project already underway by the City of Fort Wayne." Page 55 of the HPR notes that "With regard to integrity, there is some concern at the present time, for the Brookview portion of the District's integrity since the City has begun a flood control project which has removed nearly all of the homes along Westbrook Drive southeast of State Boulevard, and closed the portion of this drive beyond its intersection with Edgemoor Avenue." How convenient that the now diminished integrity of the district, a diminution undertaken and created by the City of Fort Wayne, is now by coincidence feeding in to the design and evaluation of the District and its integrity for the State Boulevard project. Further coincidence is that "the City has begun a flood control project" with separate funds and without a 106 or 4f review – and that now that "flood control" project and its impacts are being used for the possible beneficial purposes of this project.</p> <p>The Consulting parties raised this very concern with the US 27 project currently undergoing 106 mitigation and were assured that there was no connection between the flood control, US 27 and State Boulevard projects. The HPR again raises this notion that the City has functionally aggregated and co-mingled the projects in this area and that the removal of these houses may have been undertaken in order to avoid Section 106 review for this project or alternatively pave the way for a, yet revealed, preferred design alternative. The inclusion of the Trail Bridge in this 106 review as discussed above does not give us much comfort that our suspicions about improper process are not well founded as they pertain to the US 27, Flood Control and State Boulevard interdependence. We are in the process of investigating options with regard to a possible violation of Section 110(k) of the NHPA (16 cfr 470). The City "having legal power to prevent it" may have knowingly "allowed such significant adverse effect to occur" through its own purchases and demolitions of historic properties as a means to facilitate the State Boulevard and US 27 projects. We are investigating what options we may pursue to further explore this concern. It will be very interesting to learn the dates of applications and awards for the various "independent" projects which are simultaneously underway. We want our objections noted with this regard as part of the official record of the Section 106 process for the State Boulevard project.</p>	<p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>The proposed project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program or associated voluntary buyout program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
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24	Julie Donnell – Friends of the Parks 12/10/2009 Letter	The topography. Will there be any grade changes involved in the new bridge construction as there are with both bridges that are being considered on US 27 Clinton Street?	Yes – there is a proposed grade change of approximately 7 feet in the area of the new bridge.
25		The vegetation. What are the potential impacts on the vegetation of the designed landscape?	Development of final plans will include appropriate measures to mitigate and/or enhance landscape features within the project area.
26		The primary water feature of the landscape. How will the Spy Run Creek be affected, not just in the sense of flood control, but relative to the visual character and use of this natural feature both for pedestrians and drivers?	With the exception of the new bridge over Spy Run Creek and channel grading immediately adjacent to the proposed bridge, no impacts to the waterway are anticipated from this project.
27		What is the impact on the as yet undefined connection to the Kessler Park and Boulevard Plan, now being nominated to the National Register? Changes which impact the traffic counts on State Boulevard will have an impact on the circulation in other historic districts which are located along State Boulevard, such as the Forest Park Historic District.	The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.
28	Dr. James Glass - SHPO 12/14/2009 Letter	Given that there could be multiple alignments under consideration and that the final, or preferred, alignment might not run through the center of the APE as currently proposed, we are not yet prepared to comment on the adequacy of the APE. It would be helpful to have more information, specifically in graphic form, to indicate where the alignments that are under consideration would run.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
29		Similarly, it would be helpful to have more information about the purpose and need of this project, especially because it seems likely to have an adverse effect on the identified Brookview-Irvington Park Historic District. Without an adequate statement of purpose and need, it will be harder later in the Section 106 consultation to evaluate alternatives that might result in avoidance or minimization of the adverse effect.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment.

30		<p>In regard to the archaeological report (Stillwell 4/2/09) which was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area which was surveyed for this project by Archaeological Consultants of Ossian. However, we not that on page 4 of the draft historic properties report (Fife 9/09), it is stated that "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known." Therefore, if the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas would be necessary required to determine the presence or absence of archaeological resources.</p>	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
31	<p>John Shoaff – City Council 12/15/2009 CP Meeting</p>	<p>John Shoaff expressed concerns with the APE. He feels the APE is too small and should be expanded eastward across the river and possibly westward.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

32	<p>Michelle Briggs – Wedaman – Brookview Neighborhood Association 12/15/2009 CP Meeting</p>	<p>Michelle Briggs-Wedaman also expressed concerns with the APE and thinks the APE should include the entire District to the north, as well as the Centlivre Apartment and Park Place Condominium communities. The residents of these complexes travel daily through this area and are going to be directly impacted.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
33	<p>Jan Daily – Brookview Neighborhood Resident 12/15/2009 CP Meeting</p>	<p>Jan Daily also agrees the APE should be extended both east and westward. Huguenard Road has become a major north-south corridor, and the expansion of State Boulevard will cause bottlenecks both down Huguenard and along State Boulevard.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>

34	Angie Quinn – ARCH 12/15/2009 CP Meeting	Angie Quinn discussed Shurcliff's intent to develop a park setting. The area was designed to be a park where people would live, and the value of this park-like setting is extremely important in this project throughout the Section 106 process.	It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.
35	ARCH 12/15/2009 CP Meeting	ARCH expressed concern with the alternatives being considered in the late 70s and early 80s because the neighborhood was just recently declared eligible for the National Register of Historic Places (NR) by the State Historic Preservation Office and this may be a reason to reconsider alternatives.	Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.
36	Dr. James Glass - SHPO 1/27/2010 Letter	One of our concerns about purpose and need pertains to the comments of one or more of the local residents attending the meeting to the effect that the vehicular traffic congestion that this project is intended to alleviate actually occurs at major intersections, such as State Boulevard at Clinton Street, rather than on the two-lane section of State Boulevard that passes through the Brookview-Irving Park Historic District. If that is the case, then it is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion.	Vehicular traffic congestion does occur at major intersections, such as State Boulevard and Clinton Street. In addition, vehicular congestion along State Boulevard is caused by 4-lanes of traffic being reduced to two lanes within our project limits. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume. Realigning and widening State Boulevard, including the 2-lane section that passes through the Brookview-Irving Park Historic District, will significantly reduce congestion and improve traffic flow by matching the 4-lane roadway section on either side of the project limits and improving sight distance.

37	<div data-bbox="191 800 737 1430"> <p>Another purpose and need issue on which we would appreciate clarification is the substandard nature of the roadway curvature on State Boulevard. We can understand that if one were designing a major cross-town roadway today, one would not want to include a reverse curve or curves such as those found on State Boulevard between Clinton Street and Cass Street. However, some of the consulting parties asserted at the meeting that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard. It seems as though when a community wishes to slow or otherwise control traffic on a busy street by introducing a median, curb bumpouts, or a roundabout, such treatments are considered to be appropriate traffic calming devices, but when a community wishes to allow traffic to move more quickly, a narrow or curving street is considered substandard.</p> </div> <div data-bbox="191 128 1484 779"> <p>The existing horizontal alignment along State Boulevard does not currently meet AASHTO design guidelines or Indiana Design Manual guidelines for minimum curve radius requirements for low speed urban streets. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway's safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.</p> <p>According to the AASHTO policy on geometric design of highways and streets Exhibit 3-16 as well as Indiana Design Manual Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph low speed urban street is required to be a minimum of 300 ft. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards.</p> <p>The high crash rates along this section of State Boulevard can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.</p> <p>For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.</p> <p>The project will relocate and straighten the section of State Boulevard between Clinton Street and Cass Street to the south; however, design considerations have been made in order to maintain the existing curve found on State Boulevard between Clinton Street and Cass Street.</p> <p>(continued on next page)</p> </div>
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38			<p>Specifically, the intersections of Eastbrook Drive and Terrace Road will be reconstructed and the existing State Boulevard roadway will be modified to curve north slightly at these two locations. Access to this stretch of existing State Boulevard will then be provided off of the new alignment to the south utilizing the Oakridge Rd extension. Doing so will provide residential access to the north side of State Boulevard while maintaining the unique roadway curvature which contributes to the park-like setting intended in the original design.</p>
		<p>It is our understanding that the section of State Boulevard in question is posted for a 20 mile per hour speed limit, but the City of Fort Wayne wishes to upgrade that section to a 30 mile per hour limit, as State Boulevard already is to the east and west of there. It is also our understanding that there is no other east-west street in that part of Fort Wayne that completely crosses the city. Please clarify why it is important for traffic to be able to move quickly across that part of Fort Wayne. In other words, where is the traffic originating, and where is it going? It is not readily apparent to us that State Boulevard plays a key role in moving traffic to or from the downtown, and, other than Parkview Hospital on the east side, it is not readily apparent which major service, employment, or commercial centers are served by State Boulevard.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum</p> <p>(continued on next page)</p>

		<p>Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
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39		<p>In our December 14 letter, we had requested additional information about the locations of alternative alignments that are under consideration, for the purposes of enabling us to comment on the proposed area of potential effects ("APE"). At the December 15 meeting, we received plan sheets depicting four alternatives, which appear to be identical with regard to the alignment proposed for State Boulevard between Clinton Street and Cass Street. Some of the consulting parties have asserted that the APE should be extended in various directions. Not being as familiar as they are with the neighborhood, we are not entirely clear about the rationale for extending the APE farther to the east or west, although we are unable to dismiss such a recommendation. However, it does seem plausible to us that the proposed changes to State Boulevard between Clinton and Cass streets and to State's intersections with side streets might cause some traffic that now enters or exits State between Clinton and Cass (e.g., at Westbrook, Eastbrook, Oakridge, or Terrace) to find other routes either farther north or farther south. If such diversions of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.</p>	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
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40		<p>Finally, we are becoming concerned about a larger effect on the Brookview-Irvington Park Historic District that this project might have, beyond the taking of at least several houses and re-working of street alignments that currently contribute to the district's significance. Given the demolitions that already have occurred-and others that may yet occur-as a result of the City of Fort Wayne's program of buying flood-prone residential properties along Spy Run Creek and given the undefined number of residential demolitions that would occur as a result of this project, a distinct and fairly broad band of non-contributing land and roadway could eventually cross the historic district along and near a new alignment of State Boulevard. It is probably premature to attempt to assess that kind of an effect on the district, but we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.</p>	<p>Consideration will be given to this request at the appropriate time.</p>
41	<p>Dr. James Glass – SHPO 3/10/2010 Letter</p>	<p>We appreciate having the "Spy Run Flood Protection Study," but it brings to mind a few questions that may be relevant to the State Boulevard Reconstruction. As we read it, the only solution recommended by that study that would have an impact on the APE of the State Boulevard project was a combination of flood-proofing and buyouts of homes within the 100-year floodway. Other alternatives that seemingly might have had fewer impacts on historic properties within the APE of this project (such as construction of levees along Spy Run Creek or construction of a detention basin in Franke Park) were not recommended by the study. We have been advised that the houses along Westbrook Drive between Edgehill Drive and Clinton Street already have been acquired and demolished under the buyout program. Thus, we know that the buyout program has been partly implemented, but we do not recall having heard or read whether the companion program that the study recommended – flood-proofing of some homes- has been implemented. Is the flood-proofing of homes also a part of the City of Fort Wayne's flood prevention program? If so, are any of the eligible homes within the APE of the State Boulevard Project, have they been flood-proofed, and what did that flood-proofing entail?</p>	<p>The flood control study prepared by Christopher B. Burke Engineering, Ltd for the City of Fort Wayne was included within the packet sent out to consulting parties for additional information. This study was prepared in 2005 to analyze the Spy Run Creek watershed for the City of Fort Wayne. This flood control study is not a part of the State Boulevard reconstruction project and does not address the flooding of the State Boulevard roadway. As stated in the executive summary, the purpose of the Spy Run Creek Flood Control Study was to analyze feasible solutions to existing flooding problems associated with Spy Run Creek. The State Boulevard roadway flooding problems are associated with the flood elevation of the St. Mary's River.</p> <p>The City of Ft Wayne had previously implemented a voluntary floodplain acquisition program as discussed in the flood control study. This voluntary acquisition program is part of a separate locally funded project and is not a part of the State Boulevard Reconstruction project. As part of this voluntary acquisition program, houses along Westbrook and Eastbrook Drive have already been acquired and demolished. In coordination with the City of Fort Wayne, INDOT, and FHWA, the voluntary acquisition program has been halted for homes within the footprint of the State</p> <p>(continued on next page)</p>

		<p>Boulevard transportation project. Any further acquisitions necessary for the transportation project will be purchased and demolished following Federal guidelines and not as part of the voluntary acquisition program.</p> <p>The study identifies necessary additional information regarding the flooding issues of the St. Mary's river, which affects the roadway safety and causes flooding issues on State Boulevard. According to page 19 of the Spy Run Creek Flood Control Study, "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."</p> <p>project will raise the existing State Boulevard Bridge above the flood elevation of the St. Mary's river which will enable the roadway to remain open allowing emergency traffic during flood events. The proposed project bridge will also reduce the backwater, essentially reducing the flood impacts along Spy Run Creek to Grove Street as mentioned in the flood control study.</p> <p>American Structurepoint, Inc. is not familiar with any additional details related to the City's flood prevention program. Additional questions regarding the locally-funded flood prevention project should be directed toward the City of Fort Wayne, as American Structurepoint, Inc. has no involvement with this program. All homes within the City of Fort Wayne's flood prevention program and within the APE will be evaluated for eligibility as they currently exist.</p>
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42	<div data-bbox="191 793 1224 1444" data-label="Text"> <p>We have examined preliminarily the crash and traffic volume data that you provided. Without the benefit of some interpretation, however, we are finding it difficult to draw many conclusions. If we are reading the traffic data correctly, it appears that the combined eastbound and westbound traffic on State Boulevard between Cass Street and Clinton Street was about 18,000 vehicles per weekday in 2006-2007 and that slightly fewer than 1,500 vehicles per hour traveled between these points during the peak, weekday hour of 5:00 PM to 6:00 PM. That sounds at first bluish like a significant amount of traffic. However, the average frequency of vehicles moving either eastward or westward during that peak weekday hour would be about 25 per minute or about one every five seconds in each direction. Whether that is a sign of severe congestion or not we are not sure. Moreover, it appears that a higher volume of traffic- more than 1,700 vehicles during that same hour-traveled between Clinton Street and Spy Run Avenue, although that stretch of State Boulevard apparently has four through lanes, in contrast to State Boulevard's two through lanes between Cass Street and Clinton.</p> </div> <div data-bbox="191 126 1224 793" data-label="Text"> <p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the table included in the purpose and need statement..</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p> </div>
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43		<p>The crash data also might yield a useful comparison. In 2008, the State Boulevard-Clinton Street intersection experienced 33 crashes. The combined number of crashes along the stretch of State Boulevard that is proposed to be realigned appears to have been 27 in 2008. We included in that total of 27 the crash figures for the four, intervening intersections and the figures for the east and west 100 and 200 blocks of State Boulevard, assuming (without being certain) that those blocks are within the stretch of State that is proposed to be realigned. We did not include crash figures for the State-Cass (6) or State-Clinton (33) intersections in that total of 27, because those intersections apparently are outside the area in which State Boulevard is proposed to be realigned, and we surmise that the improvements might be made to those intersections without realigning State. However, without guidance on how to interpret the data that you have provided to us, we are unsure whether we have plied the data correctly. We would appreciate some assistance in that regard.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
44		<p>Another issue about which we are uncertain is whether it might be feasible to reduce the volume of traffic that uses State Boulevard between Cass Street and Clinton Street, rather than conceding its inevitability and straightening and widening State Boulevard. As was mentioned at the December 15 consulting parties meeting, it appears that Arthur Shurcliff intended that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses (beyond those taken so far by the flood buyout program that contribute to the Brookview-Irvington Park Historic District are demolished. Given that not every motorist who travels between Spy Run Avenue and Clinton Street also travels between Clinton and Cass Street, is there something that could be done to encourage even more drivers to find an alternative to traveling State between Clinton and Cass?</p>	<p>Regarding the feasibility of reducing traffic on State Boulevard between Clinton Street and Cass Street, it's important to note that very few continuous east/west traffic corridors exist near downtown Fort Wayne. State Boulevard is unique in this way because it is an established corridor linking vital residential and commercial resources allowing traffic to flow across the entire City in an east/west direction. In this regard, State Blvd is joined only by Coliseum Boulevard approximately 1.5 miles to the north and Washington Street/Jefferson Street approximately 1.4 miles to the south of State Boulevard. The addition of a new east/west corridor in the vicinity of State Boulevard will likely cause a much more severe impact to residential, historic, and environmental resources. The feasibility of reducing localized traffic between Cass and Clinton Street on the heavily traveled State Boulevard corridor by using an existing local street such as Jacobs Street to the south would not be a feasible option. Routing existing east/west corridor traffic through an existing local street would impact residents along the local street as well as introduce additional intersection movements, increase traffic crashes, and would increase congestion on both Clinton Street and Wells Street. This type of "permanent local detour" would also introduce severe delay's likely not acceptable to the traveling public.</p>

45	Jill Downs - ARCH 6/15/2011 Letter	With the Exception of a relatively short window of time during peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion.	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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46	<div data-bbox="191 793 1485 1444" data-label="Text"> <p>My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as it was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and “traffic-calming” solutions are being devised as ways of protecting neighborhood integrity and pedestrians safety.</p> </div> <div data-bbox="191 128 1485 793" data-label="Text"> <p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p> </div>
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			<p>Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p>
47		<p>The State Boulevard Reconstruction Project is not warranted, and this is evident by American Structurepoint's poor attempt to justify its need.</p>	<p>Please see the project's purpose and need; approved by INDOT and FHWA.</p>
48		<p>...American Structurepoint seems to have deviated from its original purpose and need for this project, trying to put the focus more on safety rather than moving traffic through the area.</p>	<p>Original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

49		<p>It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds.</p>	<p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
50		<p>Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. These congested conditions lead to increased air pollution in the form of emissions, travel time delays for motorists, and loss of productivity. Intersection congestion also causes motorists to attempt to cut through neighboring streets in order to avoid congestion and delays at the intersections. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p>

51		I question whether this deviation should trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.	American Structurepoint is dedicated to following the Section 106 process in its entirety. No deviations from the Section 106 process which would warrant a "New Section 106 Review" have occurred.
52	Todd Zeiger – Indiana Landmarks 6/16/2011 Email	I am unsure what the expectation of American Structurepoint is with regards to Consulting Parties and our comments on this correspondence. I note that it is the intention of American Structurepoint to call a consulting parties meeting after the conclusion of the DHPA's 30 day comment window. However, the concerns of the consulting parties as submitted after the last round of meetings and correspondence, have not been addressed to us as of this date. While some of those concerns are mirrored in the May 19, 2011, letter, still others are not and remain in question.	Additional clarification regarding the opportunity for Consulting Parties to comment was provided. As was intended and as is always the case, Consulting Parties may comment on any information received during the Section 106 process. Every attempt to address each consulting parties comment is being taken: as is evident by this document.
53		Of particular concern was our comments and questions about the Purpose and Need Statement. It was noted in the letter May 19, 2011, letter that a copy of the "revised" purpose and need" was included. However, I did not receive that revision.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.
54		I have been in contact with several of the other consulting parties including our partners at ARCH and it is the consensus that before a consulting parties meeting is called, or we provide comments, a formal response to our concerns should be received including a copy of the revised purpose and need) along with a corresponding appropriate comment period.	An additional comment period was provided for the Purpose and Need. Requesting and considering consulting parties concerns is an ongoing effort and a central purpose of the Section 106 process. We are continually working to address your concerns, however this should not precede additional consulting parties meetings which provide the opportunity for other comments to be provided.
55	John Shoaff – City Council 6/16/2011 Letter	First, let me note you said that a purpose and need statement was attached, but I did not find one included with my copy.	We apologize for this oversight. The Purpose and Need was subsequently provided to all Consulting Parties.

56	<p>In his letter Dr. Glass raises several important concerns, beginning with “It is not clear to us how realigning and widening that part of State Boulevard will contribute significantly to the reduction in congestion”. Nor is it clear to me, even after reading your response.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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57		<p>On the curve of State Street, traffic always moved smoothly; but there was often (and always at rush hour) significant traffic backup at the major intersections of State Street with Clinton and Spy Run Avenues. Those intersections are already four-laned: any four-laning of State Street before you reach the four-laned approach to the intersection can, at best, only minimally decrease congestion.</p>	<p>Under current traffic conditions, congestion occurs at the intersections of Spy Run Avenue and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>The existing traffic data on State Boulevard as provided by the MPO indicates the need to widen State Boulevard between Clinton and Cass from two lanes to four lanes matching into the existing four lane segments both to the west at Cass Street and East of Clinton Street. An additional center left turn lane will also be added where required for left turning vehicles. The addition of a second through lane at the Spy Run intersection will also alleviate congestion for traffic flowing westbound through the Spy Run intersection. Currently, all westbound traffic on State Boulevard must merge into one lane since there is only one receiving westbound lane on the opposite side of the Spy Run intersection. This situation currently causes congestion and delay at the existing Spy Run intersection. Further traffic studies will be performed as part of the NEPA process.</p>
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58	<p>Isn't the congestion at these intersections actually the result of traffic planning (or lack of planning) that over the years has led to heavily traveled traffic corridors that cross each other at grade? The primary cause of congestion is the large streams of north-south traffic that enter and leave the city on Clinton Street and Spy Run Avenue. By four-laning and raising design speeds on State Street as it goes through the Brookview neighborhood and by the additional four-laning of Goshen Avenue, which is next on your master plan, you are deliberately planning for increased east-west traffic. This means even more automobiles, moving faster, will conflict with that north-south traffic. Contrary to your assertions, your plans will not alleviate congestion, but in net will make it worse.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northern Indiana Regional Coordination Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.</p> <p>Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project will reduce delay and improve overall intersection service to acceptable levels of service ("D" or above).</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept NIRCC evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District.</p> <p>(continued on next page)</p>
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59	<div data-bbox="191 793 602 1430"> <p>Your various answers to other questions consistently lead to the same very disturbing conclusion that your primary purpose is to create an east-west corridor consistent with "the arterial plus bypass concept". This is justified on several questionable grounds: "a number of investments have already been made "on the State Boulevard Corridor," as you are already calling it, as if past errors justify future ones; you argue that Coliseum Boulevard (originally built to provide a circum-urban bypass) is compromised because at its east end it turns southward to become north-south – but to assert that this is relevant, you must ignore the connection of Coliseum with Anthony Boulevard, which connects smoothly to Stelhorn Road – which aligns with and neatly continues the east-west stretch of Coliseum.</p> </div> <div data-bbox="191 128 1484 779"> <p>The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.</p> <p>State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.</p> <p>As part of the development of the Metropolitan Transportation Plan and the "Arterial plus Bypass" concept, the Northeast Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option. These alternatives will be discussed through the NEPA process.</p> <p>As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy</p> <p>(continued on next page)</p> </div>
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		<p>Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.</p> <p>The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.</p> <p>State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stielhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.</p>
60	<p>You totally ignore the welfare of the people who live within the area. Where are the studies that show the impact of your east-west corridor on the livability of the neighborhoods and on the property values to the homes your corridor borders or traverses?</p>	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document. This is not appropriate for Section 106.</p>

61		<p>You refer to "The redevelopment of the urban core area" something we all want, but your proposal will work against that. The grotesquely out-of-scale size of your new roadway will destroy the very special feeling of the Brookview neighborhood, and the hazards and unpleasantness of heavy traffic that drive people to the suburbs will be increased all along the corridor.</p>	<p>The mention of "redevelopment of the urban core area" is taken out of context in this comment. As stated in the purpose and need: "The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes." This project is not being presented in order to redevelop the urban core; it is being presented in order to accommodate that redevelopment and the traffic demands it is currently placing on State Boulevard, which will continue to grow over time.</p>
62		<p>ISTEA was passed in 1991, the FHWA handbook on Flexibility in Highway Design was published in 195, and the AASHTO handbook on design flexibility which outlines in some detail a proper process for including stakeholders in the design process, came out in 2004; yet you continue as if you were designing for an empty field. You even assert that standards appropriate for "highway design elements" apply to a road that abuts and serves a modest residential neighborhood!</p>	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p>
63	Michael Galbraith - ARCH 6/17/2011 Letter	<p>...my primary concern is that Consulting Parties and the public be included as integral parts of the Section 106 process. At this point of the process, I do not feel as if that is the case. At best our input appears to be included as a mere footnote. Our Dec 8 2009 comments regarding the HPR and our comments from the initial Consulting Parties (Dec 15 2009) remain unaddressed. I request that our concern and comments regarding the HPR and those raised in the Consulting Parties meeting be addressed and that we be given time in which to respond to those answers.</p>	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
64		<p>It appears that the Purpose and Need for this project has substantially changed from that proposed in American Structurepoint letters dated March 23, 2009 and November 9, 2009. If, as seems probable from the letter addressed to Dr. Glass dated May 19, 2011, the project purpose and need is indeed radically different from that under which the project was conceived, authorized and initiated, it begs the question whether this is indeed the same project for which the Section 106 Review was started. I request that the Consulting Parties be given an opportunity and timeframe to evaluate and respond to this wholesale change in purpose and need.</p>	<p>The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment. The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.</p>

65	Julie Donnell – Friends of the Parks 6 /14/2011 - Letter	...We are surprised to learn that you have felt it necessary to make elaborate changes in the Statement of Purpose for the project cited above, without formally communicating this to the Consulting Parties. This seems to be a departure from the way Section 106 proceedings normally go forward, and we would like to know what, if any, changes in the procedure are anticipated.	The Purpose and Need for the project have been revised. The revised purpose and need was reviewed/approved by INDOT and FHWA and was provided to all consulting parties for review and comment. The original Purpose and Need have been expanded, developed, and supplemented with supporting information; not deviated from or refocused.
66		Also, we would like to have an additional 30 days to review this Statement of Purpose and to have a chance to respond to it, as well as to your communications with Dr. Glass, in a formal way under the auspices of whatever form the Section 106 review continues.	An additional comment period was provided following distribution of the purpose and need.
67	John Shoaff - 7/1/2011 Letter	There are material discrepancies between the accident counts in your new P and N Statement and those sent by Hayley Steele of your firm to ARCH on 2/4/2010.	The discrepancy lies in the methodology used to identify potentially hazardous locations for system wide planning purposes in comparison to method that compiles crashes based on a conservative distance, usually from an intersection. The information utilized for both methods is based on the same data, which is derived from crash reports filed by state and local law enforcement agencies. The "planning method" is used to assist in the screening and allocation of crashes to specific areas, to identify "hot-spots" where actual crashes exceed expected thresholds. This method compiles crashes within a 250' radius from a single point, generally an intersection. When the distance between intersections is less than 500', this method will compile crashes that are common to both intersection. A micro-analysis, that examines each crash report, is the only way to ascertain the precise type, location and cause of each crash. We have conducted a micro-analysis for the State Boulevard corridor, Cass Street to Spy Run Avenue, that identifies all crashes for the years 2007-2010.
68		The RMVs generated by your figures are much higher than the City's for the Westbrook and Eastbrook intersections, which are, respectively, at the beginning and on the curve you wish to eliminate. The city's figures simply don't come close to supporting the case you are trying to make for this highly destructive change.	Both sets of crash data were provided to American Structurepoint by NIRCC. See above response to question number 67.

69		<p>With respect to the Spy Run and Clinton intersections, it stains credibility to argue that bringing more automobiles into them, at higher speeds, will decrease the number of accidents; yet this is an intended consequence of the current master plan of which this project is a part: namely to increase traffic on State Boulevard by making it and Goshen Road an east-west arterial.</p>	<p>The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels.</p> <p>It is anticipated that crashes will decrease due to the modified alignment of the proposed State Boulevard project. The elimination of driveways directly accessed off of State Boulevard between Westbrook Drive and Terrace Road as well as the addition of a center left turn lane will likely decrease rear end and turning accidents. The locations where vehicles will be slowing down to turn left will be reduced and the center left turn lane will allow turning vehicles to move out of the path of the thru traffic thus decreasing rear end collisions. By introducing appropriate horizontal curvature in the alignment as well as the intersection improvements which will be made with the proposed project, sight distance will be improved at the intersections between Westbrook and Clinton Streets. The increased horizontal geometrics and improved intersection sight distance will likely reduce right angle, head on, and off road accidents.</p>
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70	Dr. James Glass – SHPO 7/5/2011	We continue to think, however, that it may be appropriate to expand the Section 106 APE if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project.	<p>The proposed preferred alternative will maintain access to State Boulevard via Oakridge Road. Eastbrook Drive and Terrace Road will lose direct access but will tie into Oakridge Road. The proposed project is anticipated to reduce traffic volumes through the Brookview Neighborhood and the traffic pattern alteration is not anticipated to result in an adverse impact.</p> <p>The project would likely draw current cut-through traffic out of the neighborhoods because the project would improve traffic flow. It is not reasonably foreseeable that traffic will be forced into the adjacent neighborhood as a secondary impact of the proposed project. While some additional traffic can be expected to utilize the improved State Boulevard corridor it is not reasonably foreseeable that the corridor will draw a significant increase in east/west traffic or have a negative impact on neighborhoods located east and west of the existing APE. In addition, travel patterns in the Fort Wayne area are well established and it is not likely that vehicles utilizing other properly functioning east/west corridors will change to the State Boulevard corridor.</p>
71		We do not question the relevance of that discussion to the NEPA process, but we believe that it is also relevance to the Section 106 process. The alternatives presented to the consulting parties so far appear to be minor variations in the State Boulevard Corridor alternative.	<p>Alternatives have continued to be developed throughout the Section 106 process. An alternatives analysis including mapping will be provided to all CPs prior to the next (second) CPs Meeting.</p>
72		However, given the complexities of this project and the span of time since the last consulting parties meeting, we would suggest that you share with all of the consulting parties the comments that have been or shortly will be received in responses to your May 19 and June 17 letters.	<p>Since the purpose and need have been developed, efforts to better address CP comments have been undertaken, including this document. It is our intent that this document, in combination with the next CP meeting, will effectively address past CP comments.</p>
73		Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.	<p>If the final alignment contains areas which were not surveyed by Archaeological Consultants of Ossian (Stillwell 4/2/09), then an archaeological reconnaissance of those areas will be completed to determine the presence or absence of archaeological resources.</p>
74	Suzanna Slick – Irvington Park 7/6/2011 - Email	I was very disappointed as a resident of Irvington Park, which is adjacent to Brookview, that there is very little emphasis on livability best practices in any of the State boulevard Correspondence.	<p>Evaluations such as Environmental Justice, Indirect and Cumulative Impacts, and Community Cohesion will be detailed in the NEPA Environmental document.</p>

75		There is no attempt at traffic calming, but a great emphasis on traffic rushing.	Alleviation of unnecessary congestion is not intended to translate to "traffic rushing." The intent of the project with regard to improving traffic flow and congestion is not to move vehicles through the area quickly, but rather to move vehicles through the area safely. The proposed design speed will be 35 mph. The posted speed will remain at 30 mph which is consistent with the existing posted speed limit and the posted speed limit on either side of the project area as well. Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. Northeast Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area.
76		There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood and little interest in its usability.	<p>The City and American Structurepoint has been and continues to follow all applicable guidelines for project development. Currently, we are following the Section 106 process and are soliciting comments which apply only to Section 106.</p> <p>It is agreed that the park-like setting intended in Shurcliff's design is of paramount importance to this project. All efforts to maintain that park-like setting are being evaluated.</p>
77		Other cities are making great strides in building roadways that are user friendly, and safe for everyone – not just car and truck drivers – and that are not intrusive. I think we are giving up an opportunity here to make something remarkable and forward-thinking. I fear what will be delivered will forever alter that lovely part of town and not in a positive way.	The proposed project is being designed with the central goals of providing a safe roadway for both pedestrians and motorists, while impacting the historic area as little as possible.

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1st, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne.

We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1st, until then please let me know if there are any questions.

Thanks

Brett W. Lackey

Environmental Specialist, Environmental Sciences Group

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U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard
FHWA Liaison
Advisory Council on Historic Preservation
1100 Pennsylvania Avenue, NW
Washington, DC 20004

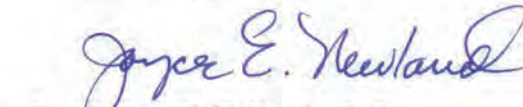
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at joyce.newland@dot.gov.

Sincerely,


Robert F. Tally, Jr., P.E.
Division Administrator

Enclosure



Lackey, Brett

From: Suzanne [sjslick@mac.com]
Sent: Monday, August 29, 2011 5:06 PM
To: Lackey, Brett; mayor@ci.ft-wayne.in.us
Cc: Glass, James; Carr, John; Tharp, Wade; aricketts@dnr.in.gov; Kaiser, Jason; Carpenter, Patrick A; Kennedy, Mary; Newland, Joyce; aquinn@archfw.org; Michael Galbraith; don.orban@cityoffortwayne.org; tzeiger@indianalandmarks.org; juliemarie57@earthlink.net; Michelle Briggs Wedaman; jcooper@ccrtc.com; jandailey59@msn.com; indianabridges@sbcglobal.net; shan.gunawardena@cityoffortwayne.org; danavery@co.allen.in.us; jshoaff@proparkwest.com; creager.smith@cityoffortwayne.org; albertcohan@aol.com; tmn@barrettllaw.com; rross@martin-riley.com; tom.cain@cityoffortwayne.org
Subject: Re: State Boulevard Consulting Party Meeting
Attachments: image001.jpg; ATT00001..htm; image002.jpg; ATT00002..htm; image003.jpg; ATT00003..htm; IN20071404.EV.2011-08-12.Consulting Parties Meeting Memo.pdf; ATT00004..htm

All,

After reading the Consulting Party comments and rebuttals from American Structurepoint I'm not very optimistic about the upcoming meeting -- either Structurepoint is being deliberately obtuse or they refuse to acknowledge our very real concerns about the State Blvd project's impact on our neighborhood and our City. People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are pat, formulaic, vague and evasive. Neighbors who are intimately familiar with the streets and traffic in the area -- much more familiar than anyone else involved in this discussion -- have weighed in in opposition to this massive alteration of our neighborhood, yet the responders continue to insist that this will improve safety and the level of service delivered to the users. The responses repeat the mantra that safety is of utmost importance and the primary goal, yet language regarding traffic calming seems to be deliberately avoided in the answers. While many cities are moving away from the trend to rush traffic quickly through urban areas and toward a complete streets approach to integrated roadways that encourage and expedite usability by non-motorized "traffic", State Blvd's future seems to be the opposite -- an artery of speeding cars and trucks racing in a straight line at high speed bisecting our quiet, quaint neighborhood, in effect cutting neighbors off from anything on the "other" side of State Blvd. In the list of alternatives, one would expect to find some discussion of the use of standard calming devices like reduced speed, raised crosswalks, chicanes, lateral shifts and roundabouts, for example. The "road diet" approach is not mentioned either. There is nothing remotely related to these approaches in any of the responses, just lots of rhetoric about "lengthy delays and congestion". Look, I drive the Cass to Clinton stretch daily -- there are no major delays and no lost productivity for motorists. Accidents in this stretch are primarily caused by speeding motorists which means as speed increases, as it surely will with a multi-lane straightaway, danger of accidents will increase. Certainly, risk to nonmotorized users will increase greatly. And passing off pedestrian needs to the Pufferbelly Trail project seems like an inadequate solution -- more an afterthought than a priority. Are the experts making these decisions and designing this roadway "improvement" that out of step with my neighborhood and with current best practices in street design? Let me point you to some information that will inform the conversation:

Here is a quote from the Kansas City Walkability Plan - <http://ww4.kcmo.org/planning/walkplan/Aappendix.pdf> :

Traffic calming is a way to design streets using engineering principles to encourage people to drive more slowly. It creates physical and visual cues that induce drivers to travel at appropriate speeds. Traffic calming is self-enforcing. The design of the roadway results in the desired effect without reliance on enforcement or voluntary compliance. Traffic control devices such as signals and signs rely on compliance. While elements such as landscaping and lighting do not force a change in driver behavior, they do provide the visual cues that encourage people to drive more slowly.

The reason traffic calming is such a powerful and compelling tool is that it has proven to be so effective. Some goals of traffic calming are clearly measurable such as increasing safety through fewer and less severe crashes. Others, such as supporting community and livability, are less tangible but equally important.

National Complete Streets Coalition -- <http://www.completestreets.org/>

More at these sites:

<http://ww4.kcmo.org/planning.nsf/plnpres/walkability?opendocument>

<http://www.fhwa.dot.gov/environment/sidewalk2/sidewalks209.htm>

<http://www.ite.org/traffic/tcstate.asp>

http://safety.fhwa.dot.gov/ped_bike/

<http://www.virginiadot.org/business/resources/TrafficCalmingGuideOct2002.pdf>

<http://www.pps.org/articles/livememtraffic/>

<http://cityofsparks.us/sites/default/files/assets/documents/traffic/Traffic%20Calming.pdf>

Fort Wayne is a smart, vibrant city that could achieve so much more in improving livability and healthy neighborhoods, the State Blvd project could be an opportunity to do this. I'm afraid what we will get is a noisy, frenetic, dangerous megastructure that citizens will avoid unless they are speeding through in a car or truck on their way to somewhere else. It is not an appealing image to those of us who will have to tolerate its unavoidable presence in our neighborhood. And we have already lost so very much in the last few years to the flood control buy-out, the blighting of Centlivre and even in the loss of ash trees in our green spaces. If we must have this new roadway, can't it be crafted in a forward-thinking, people-friendly, neighborhood-sustaining fashion?

Sincerely,
Suzanne Slick
Consulting Party for Irvington Park

On Aug 16, 2011, at 2:01 PM, Lackey, Brett wrote:

State Boulevard Consulting Parties,

As is indicated on the attached memo, the next consulting parties meeting has been scheduled for Thursday, September 1st, 2011. The meeting will begin at 9:30AM and will be held at Citizens Square at 200 East Berry Street in Fort Wayne. We will be meeting in Room 030, located in the Garden Level of Citizens Square.

The attached memo, as well as several other items for your review were placed in the mail yesterday. You should be receiving this packet of information shortly. We look forward to meeting with you all on September 1st, until then please let me know if there are any questions.

Thanks

Brett W. Lackey
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MEETING MINUTES

Location: City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

Date: September 1, 2011

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross
(City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and
Archaeology)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thomson (Allen County Highway Department)
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
 - 1) Project Update
 - 2) Purpose and Need Update
 - 3) Consulting Party Comments and Responses document
 - 4) Alternatives Review
 - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.

18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.

50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or blackey@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.



Brett W. Lackey

BWL:mgn

Enclosures

Lackey, Brett

From: Lackey, Brett
Sent: Thursday, September 08, 2011 12:18 PM
To: Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich
Cc: 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott
Subject: State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
 - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
 - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
 - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
 - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
 - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
 - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
 - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
 - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
 - Either city website or www.structurepoint.com
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
 - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

Brett W. Lackey

Environmental Specialist, Environmental Sciences Group

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Preserving America's Heritage

September 22, 2011

Robert F. Tally, Jr., P.E.
Division Administrator
FHWA – Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, “Protection of Historic Properties” (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP’s regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking’s effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004
Phone: 202-606-8503 • Fax: 202-606-8647 • achp@achp.gov • www.achp.gov

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at ngabriel@achp.gov.

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson
Historic Preservation Technician
Office of Federal Agency Programs



M E M O R A N D U M

DATE: September 29, 2011

TO: Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross (City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp, Amy Johnson, Amanda Ricketts (IDNR, Division of Historic Preservation and Arch.)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs, Angie Quinn (ARCH, Inc.)
Michelle Briggs-Wedaman, Karl Dietsch (Brookview Neighborhood Association)
Julie Donnell, Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thomson (Allen County Highway Department)
Todd Zeiger (Indiana Landmarks)
Dr. James Cooper, Paul Brandenburg (Indiana Historic Spans Task Force)
Albert Cohan (Westbrook 5, LLC)
Thomas Neizer (Barrett & McNagney, LLP)
Ronald Ross (Martin Riley Architects and Engineers)
Dan Ernst (Earth Source, Inc.)

FROM: Brett W. Lackey (American Structurepoint, Inc.)

RE: State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Structurepoint No. IN20071404

CC: Scott Crites, Briana Hope, Rich Zielinski (American Structurepoint, Inc.)

Enclosed, please find the following items:

- 1) Consulting Party Meeting Minutes (9/01/2011)
Meeting minutes were prepared for the September 1, 2011 Consulting Parties Meeting. The meeting minutes were prepared based on a digital recording of the meeting.
- 2) Agency Coordination Meeting Minutes (9/02/2011)
A meeting was held on Friday, September 2, 2011, at the American Structurepoint office to discuss the State Boulevard Consulting Party Meeting on September 1, 2011. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The meeting minutes summarize the discussion.

3) Individual Section 4(f) Alternative Analysis (Revised)

Following the September 1, 2011 Consulting Parties Meeting, American Structurepoint evaluated an additional State Boulevard Alternative which includes a 3-lane design. This Alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3D*.

American Structurepoint has also re-evaluated an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south. Additional information regarding this alternative has been added to the Alternatives Analysis document and is listed as *Alternative 3C*.

American Structurepoint has added a discussion of three additional configurations for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway. A discussion of these access alternates (*Access Alternates 1-3*) is included as a subset of *Alternate 3A*.

American Structurepoint has added additional information to *Alternate 4 (No-Build)*.

4) Traffic Data from NIRCC

As requested by Consulting Parties during the September 1, 2011 meeting, additional traffic information regarding the intersection Level of Service has been compiled by NIRCC and is enclosed for your information.

Upon further review of the State Boulevard intersection level of service information, the purpose and need statement has been updated by removing the evening peak Spy Run Avenue eastbound through movement from the deficient category of the purpose and need statement. Although the overall level of service for this intersection approach is deficient (LOS E), the LOS associated with the eastbound through movement is LOS D which is considered acceptable. This revision is located on page 2 of 5 of the purpose and need statement. A copy of page 2 with the revision highlighted is included for your review.

5) ACHP Correspondence

As indicated during the September 1, 2011 Consulting Parties Meeting, the FHWA has initiated coordination with the Advisory Council on Historic Preservation (ACHP). The FHWA requested ACHP involvement in a letter dated August 29, 2011. On September 22, 2011, the ACHP provided a response letter to the FHWA which indicated that additional information will need to be evaluated by the ACHP prior to deciding whether or not the ACHP will choose to be involved in the project's Section 106 process. The requested additional information is currently being prepared and will be submitted to the ACHP in the near future. Copies of the two coordination letters are included for your review.

As requested during the September 1, 2011 Consulting Parties Meeting, the City of Fort Wayne has created an online archive for the project's Section 106 correspondence and documents. This data can be accessed online at <http://www.cityoffortwayne.org/publicworks/west-state-blvd-realignment.html>

At this time we are requesting that all consulting parties review the enclosed materials and provide any comments within 30 days of receipt of this mailing. I can be reached by phone at (317) 547-5580 or by e-mail at blackkey@structurepoint.com. If you have any questions or need additional information please feel free to contact me.

Enclosures:

Consulting Party Meeting Minutes (9/01/2011)
Agency Coordination Meeting Minutes (9/02/2011)
Individual Section 4(f) Alternatives Analysis (Revised)
Traffic Data from NIRCC
ACHP Correspondence
Purpose and Need Statement Revision (Page 2 of 5)



MEETING MINUTES

Location: City of Fort Wayne, Citizens Square, 200 East Berry Street, Room 030

Date: September 1, 2011

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Attendees: Brett Lackey, Rich Zielinski, Scott Crites, Briana Hope (American Structurepoint, Inc.)
Shan Gunawardena, Creager Smith, Don Orban, Tom Cain, Alec Johnson, David Ross
(City of Fort Wayne)
Camille Fife (The Westerly Group)
Dr. James Glass, John Carr, Wade Tharp (IDNR, Division of Historic Preservation and
Archaeology)
Patrick Carpenter, Mary Kennedy, Anuradha Kumar (INDOT, Cultural Resources)
Jason Kaiser (INDOT Fort Wayne District)
Joyce Newland (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Annette "Jan" Dailey (IPFW Sociologist, Brookview Neighborhood Resident)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Charlotte Weybright (Friends of the Parks of Allen County)
Susan Haneline (Brookview Neighborhood Resident)
Charley Shirmeyer (Northside Galleries)
Mike Thornson (Allen County Highway Department)
Christian Sheckler (News-Sentinel)

1. The meeting was held at 9:30 a.m., September 1, 2011, to discuss the following agenda items:
 - 1) Project Update
 - 2) Purpose and Need Update
 - 3) Consulting Party Comments and Responses document
 - 4) Alternatives Review
 - 5) Future Steps
2. Briana Hope introduced herself and began the meeting with introductions around the room.
3. Brett Lackey gave an update on project progress since the last consulting party meeting (12/2009), including revisions to the Purpose and Need Statement.
4. Michelle Briggs-Wedaman indicated that the Section 106 process has been unclear with regard to when consulting parties may comment on materials received. The Brookview Neighborhood Association would like to comment on the 8/15/2011 information packet, but has not done so as that mailing was addressed to the IDNR SHPO office. Brett Lackey reiterated that consulting parties are encouraged to comment on anything they receive during the Section 106 process.

5. Michelle Briggs-Wedaman asked if an online archive for Section 106 documents exists. Briana Hope replied that American Structurepoint would coordinate with the City to see if it would be possible to create such an archive.
6. Brett Lackey explained the methodology of the Consulting Party Comments and Responses document, which was provided to consulting parties in the 8/15/2011 mailing.
7. Michael Galbraith expressed concerns with the methodology of the Consulting Party Comments and Responses document and requested that consulting parties be provided with copies of all original correspondence between Consulting Parties.
8. John Shoaff indicated that he believes consulting parties should have the opportunity to go through all comments included in the Consulting Party Comments and Responses document, as he does not understand some of the responses to his comments. Briana Hope reiterated that it is not feasible to go through each of the comments during this meeting, but that if there are additional questions or concerns with the responses to please submit such questions in writing.
9. Joyce Newland indicated that, because there are federal funds involved in the project, FHWA will be issuing the Section 106 effect finding and overseeing the NEPA process. The alternatives review is part of the NEPA and Section 4(f) processes as well. Since this is the second consulting parties meeting, we need to discuss the alternatives and keep the process moving forward.
10. John Shoaff expressed concern that, although there is an environmental review and historic review, they do not address questions about neighborhood planning and protection which goes beyond historic protection and we need the opportunity to address questions about alternate routes.
11. Michael Galbraith indicated that the NEPA and Section 4(f) processes are good and valid processes but they do not invite as much public participation as Section 106 and this is the best opportunity for the public to have their questions answered. Joyce Newland indicated that we may discuss comments from consulting parties but that the process does not allow for consulting parties to veto planning decisions.
12. John Shoaff expressed general concern with the process as it has occurred so far. Mr. Shoaff suggested that the process differs from the current recommended practices established by ASSHTO and FHWA for involving stakeholders at the beginning of the process. Joyce Newland responded that this is the beginning of the process and, as such, we are ready to discuss project alternatives.
13. John Shoaff requested an explanation as to a discrepancy in traffic figures provided to consulting parties. Dan Avery responded that the discrepancy lies in the different methodologies used to analyze crash locations. Numbers that NIRCC provided for the purpose and need statement were based on a hot spot analysis that is based on a 250-foot radius around the intersections. Mr. Avery also indicated that NIRCC has conducted micro analysis which reviews every crash report, and that information is available to be shared with consulting parties.
14. John Shoaff indicated that even during rush hour traffic moves very smoothly through the project area. The congestion occurs at Clinton and Spy Run because those become major north-south corridors.
15. Michelle Briggs-Wedaman indicated that the Brookview Neighborhood Association has requested traffic studies for the area since 2008 and has been told that the data doesn't exist. Ms. Briggs-Wedaman also expressed concern that traffic data has been fabricated in order to create a need and justification for the project and questions whether there really is a need for the project at all.
16. Susan Haneline expressed support for the project and also suggested that we look at how often traffic is affected by the flooding issue on State Boulevard. Since flooding is part of the project's justification, Ms. Haneline suggested we include more flooding data to support that need.
17. Briana Hope reiterated that traffic data has been provided to all consulting parties and that INDOT and FHWA have approved the purpose and need statement and supporting data therein. Therefore, rather than discuss traffic data, meeting should move forward to discuss agenda items.

18. John Shoaff indicated that the flooding issue is caused by flood waters converging at the bridge from north and south and that the little bridge does not hold water back. Mr. Shoaff indicated that the only argument for raising the bridge is to keep it open. Briana Hope reiterated that the purpose of raising and removing the bridge is not solely to alleviate flooding in homes, but to ensure that the roadway can stay open. Homes are likely to still be affected by flooding; however, the roadway will not be closed 4 or 5 times a year.
19. Jan Dailey suggested that a better format structure should be in place which includes archived information. Ms. Dailey suggested that traffic accident data is inherently inaccurate due to discrepancies in reporting. Ms. Dailey also indicated that the roadway has only been closed for a few hours in the last couple of years due to flooding. Ms. Dailey also expressed that traffic counts do not account for reductions in home values.
20. Joyce Newland requested that we continue on with the agenda items.
21. Brett Lackey discussed the idea of expanding the Area of Potential Effects (APE) and the decision that the project is not anticipated to draw traffic away from the adjacent neighborhoods because traffic flow will be improved along State Boulevard. Michelle Briggs-Wedaman asked what traffic studies we have that suggest that conclusion and if they are available to review.
22. Jason Kaiser asked if traffic models suggest that traffic will increase in the general project corridor. Dan Avery responded that there is a projected increase but that it is not a high growth rate.
23. Michelle Briggs-Wedaman again asked if there is a projected increase in traffic, and if so, how much and does it justify the project. Michael Galbraith added that if such data exists he would like to see it. Jan Dailey added that she would also like to see studies on how the project will affect property values.
24. Michael Galbraith expressed concern that the supporting data included in the purpose and need statement has been selectively presented in order to support the project purpose, rather than identifying the project needs based on the data. Joyce Newland responded that this was already addressed when FHWA requested a reevaluation of the Purpose and Need.
25. Michael Galbraith asked if the 250-foot radius used to calculate the figures provided in the Purpose and Need includes an overlap which could potentially result in accidents being counted twice, since Eastbrook and Westbrook are less than 250 feet apart. Dan Avery responded that there may be some overlap and that is an inherent downfall of the 250-foot analysis method. Mr. Avery also indicated that this is the reason why NIRCC conducted a microanalysis and has every crash documented from the Indiana State Police database. That data is mapped and is the most accurate reflection of crash data available. The police reports themselves are confidential, but the figures are available for review if requested.
26. Michael Galbraith asked which set of numbers the Level of Service (LOS) was based on and was the LOS insufficient using the original numbers that the project was drafted upon. Jason Kaiser responded that LOS is not related to crashes and is based on traffic capacity. Dan Avery went on to say that the project is not developed on any one piece of information – safety, LOS, bridge deficiency all play a role in the reasoning and logic for improving the corridor.
27. Michael Galbraith indicated that, in the area of the curve, the numbers end in 2008 and do not reflect large scale changes that have occurred in the area since 2008. Mr. Galbraith asked if there are updated traffic and crash numbers more recent than 2008, as the area has several federally funded projects which have impacted the area. Dan Avery indicated that crash numbers have been compiled through 2010 and are continuously updated.
28. Charlotte Weybright stated that, since INDOT and FHWA have signed off on the purpose and need, it seems like we are ready to move forward with alternatives; however, consulting parties have not signed off on the purpose and need and do not think we can move forward with alternatives yet. Joyce Newland responded that this is the process for evaluating effects on historic properties and that we are trying to present a wide range of alternatives moving forward. John Shoaff added that the effects will be adverse

and disastrous and that we should want to hear and be satisfied that we are not going to destroy a neighborhood and its property values.

29. Michelle Briggs-Wedaman asked how we can look at the historic impact of a project if we have not evaluated the project's effects on property values, and that if we have evaluated the effects on property values, please enlighten us with those results. Briana Hope responded that not everyone is going to be happy with the project but at some point we must move forward. Ms. Hope continued that the purpose of the meeting is to evaluate historic impacts but that we will consider all of the comments provided today.
30. Jan Dailey requested a chart showing the times when most accidents occur. Ms. Dailey suggests that there are only 2 hours of heavy traffic during the day.
31. Patrick Carpenter stated that consulting parties have an opportunity for input on the alternatives analysis. Mr. Carpenter stated that we should be looking at alternatives and ways to mitigate the potential adverse impacts. Mr. Carpenter continued that, while these are valid concerns, the consulting parties' role is to direct the mitigation of the adverse impacts.
32. Mr. Carpenter reiterated that the needs for the project are multi-faceted and one of those needs is the bridge and bridge elevation. Beyond capacity and traffic data, if the bridge were to be replaced and raised there would still be extensive approach work required. Michael Galbraith suggested that that is only necessary assuming the bridge is irreparable. Jason Kaiser responded that FHWA and INDOT would not want to repair the bridge because it is below the flood elevation and would not be able to receive federal funds.
33. John Shoaff stated that just because the bridge needs repaired that is not justification for adding four travel lanes where there are currently two perfectly good lanes.
34. Briana Hope held a meeting break at approximately 11:00 AM
35. Brett Lackey discussed the two east-west corridor alternatives (Butler Road-Vance Road and Spring Street – Tennessee Avenue). Mr. Lackey presented a description of anticipated impacts for both of these alternatives, as described in the documentation provided to consulting parties in the 8/15/2011 mailing. Mr. Lackey indicated that both of these alternative corridors are considered feasible, but not prudent as they do not meet the project's purpose and need. An aerial map depicting the two corridor alignments was displayed on the overhead projector.
36. John Shoaff suggested that, rather than trying to create a new east-west thoroughfare on State Boulevard, we should look at improving Coliseum Boulevard because it is a largely commercial corridor and more appropriate to carry increased traffic volumes.
37. Brett Lackey discussed the three State Boulevard alternatives (widening State Boulevard on existing alignment, reversing the existing alignment/flipping existing alignment to the south, and the preferred alternative of widening on new alignment with bridge replacement). Mr. Lackey presented a description of anticipated impacts for each of the three alternatives, as described in the documentation provided to consulting parties on 8/15/2011. Mr. Lackey indicated that only the preferred alternative is both feasible and prudent. The preferred alternative minimizes impacts by reducing the number of historic property impacts, retaining portions of the existing curb line, and by including design elements, such as landscaping, street lighting, etc., which will be developed later. An aerial map depicting the State Boulevard alternatives was displayed on the overhead projector. Mr. Lackey also described the No-Build or "Do Nothing" alternative.
38. Jan Dailey expressed concern with access to the commercial properties at the southeast corner. Shan Gunawardena indicated that an alley way will connect State Boulevard to the commercial parking lot(s). Briana Hope also indicated that access will be maintained to all properties but that those design details have not been established yet.
39. John Carr asked if we could point out the alternative of reversing the existing alignment/flipping the existing curb to the south. Scott Crites indicated that you would not be able to design the curb to fit between Clinton Street and the St. Joseph River, based on federal standards. Mr. Crites continued that

this would create a new intersection at Clinton. Shan Gunawardena indicated that these two intersections would be too close together.

40. Dr. Glass asked if an alternative further south in the area where homes are already being removed due to flooding has been evaluated. Scott Crites responded that the alignment has been pushed as far south as possible while still designing the curbs to meet standards. Briana Hope added that the bridge approach work would still require a grade change on State Boulevard.
41. Michael Galbraith asked if reducing the design speed to 30 or 25 would allow more options for designing the curb. Scott Crites responded that it has been looked at and is not possible. Jason Kaiser added that additional studies would be necessary in order to alter the design speed in the corridor.
42. Michelle Briggs-Wedaman asked if we could discuss how each of the alternatives would impact such considerations as air quality, light, and sound impacts. Brett Lackey responded that these impacts will be thoroughly evaluated in the NEPA document.
43. Michelle Briggs-Wedaman suggested that the significant amount of non-motorized traffic in the area needs to be taken into account. Briana Hope responded that all of the alternatives will result in an adverse effect, so the goal is to minimize and mitigate the adverse impacts with landscaping, lighting, and interpretive signage, etc.
44. Michelle Briggs-Wedaman indicated that “landscaping” is a broad term and that they are concerned about how the planning process will unfold and when we will be able to participate. Briana Hope indicated that that is an agenda item for discussion today but we first need to finish the alternatives presentation.
45. John Shoaff again stated that there may be special consideration for the bridge replacement but that does not mean we need to change the road to 4 lanes. Mr. Shoaff cited a project in Greenville, South Carolina, which removed an east-west roadway. Mr. Shoaff indicated that this area is special because it was designed by Arthur Shurcliff and the fact that the District is endangered has caught the attention of the National Cultural Landscape Foundation, which has posted about the project on their website. Mr. Shoaff continued that the whole city is going to receive a well deserved black eye nationally if this project goes forward as planned and that Coliseum Boulevard should be developed as a new thoroughfare.
46. Jan Dailey again stated that there is very limited data available on how adding traffic affects home values but that there are numerous studies which indicate that lowering activity in an area will raise property value. Jason Kaiser responded that, if you lower the speed here, resulting in less cars traveling here, that means those cars are now traveling somewhere else – does that then detract from those people’s property values where the cars have now gone? John Shoaff responded that using an existing thoroughfare through commercial areas, such as Coliseum Boulevard, would address that issue. Jason Kaiser responded that Coliseum is currently at capacity. Mr. Shoaff responded that it is still a better corridor to expand and improve as a thoroughfare and that if we allow the grid to do its job, it will accommodate the traffic.
47. Michelle Briggs-Wedaman state that a certain amount of congestion and density is part of what we anticipate and applaud as part of living in the center of the City for those of us who chose to live in the historic neighborhood. Ms. Briggs-Wedaman stated that we are losing connectivity and gaining a massive roadway.
48. Michael Galbraith expressed concern that the goal of the project is not to correct substandard sight curvature but to create a functional east-west corridor to alleviate congestion on Coliseum Boulevard. Jason Kaiser responded that improvements to Coliseum would not alleviate traffic congestion on State Boulevard very much. Patrick Carpenter added that Coliseum Boulevard option would not address the bridge replacement or substandard curve needs.
49. Michael Galbraith stated that the bridge repair options should be fully evaluated. Mr. Galbraith stated that flooding is coming from two ways, north and south, and is caused by factors outside the project area and those problems are addressable outside of this project.

50. Dr. Glass asked if it is feasible to design the project with 2 or 3 lanes rather than 4 lanes. Jan Dailey added that even a third lane would be better, because there is a turning problem on State, not a traffic problem. Michelle Briggs-Wedaman added that a 3-lane option with bridge repair is the preferred alternative of the Brookview Neighborhood Association. Scott Crites responded that there would still be major impacts from this option due to raising the bridge and reconstructing the approaches. Jason Kaiser added that the traffic data would need to support the conclusion that 2- or 3-lane design could accommodate projected traffic volumes. Shan Gunawardena added that the two most congested intersections along this corridor are at Spy Run and Clinton Street and that this is due to 4 lanes funneling into 2 lanes in these areas.
51. Patrick Carpenter suggested the idea of interchangeable, reversible travel lanes similar to the Fall Creek Parkway in Indianapolis. Shan Gunawardena responded that, while this is a good thought, one of the goals is to provide a landscaped median in those areas where a center turn lane is not necessary. Jan Dailey suggested taking the median out of the design. Dan Avery added that removing the median is certainly an option if that is what people want, but that the Fall Creek Parkway has well established directional travel patterns that do not apply to State Boulevard.
52. John Shoaff stated that the project will encourage traffic to come from I-69 and down Goshen Road and increase traffic capacity. Mr. Shoaff stated that he remembers hearing Shan Gunawardena say that he wants to increase the capacity from 18,000 vehicles to 28,000 vehicles. Mr. Gunawardena responded that that was incorrect and out of context. Mr. Gunawardena stated that we do anticipate some increase in traffic volume through this corridor because it is a gateway to downtown, which is experiencing increased redevelopment growth.
53. John Shoaff stated that we should be presenting 3D drawings and renderings of the proposed design and alternatives. Dan Avery responded that we have already been accused of having the project designed. Mr. Shoaff continued that such graphical depictions are not hard and do not take much time for architects to create. Mr. Shoaff continued that such renderings will allow everyone to realize the massive impacts from the project.
54. Michelle Briggs-Wedaman requested the opportunity to consider a 2- or 3-lane alternative. Shan Gunawardena responded that there is still the problem of the elevation change needed to bring the bridge out of the flood zone.
55. Michael Galbraith stated that the Kessler Boulevard Park and Boulevard system is a separate listed National Register Property from the Brookview-Irvington Historic District. This Park and Boulevard system includes this particular curve, so we should not ignore that fact.
56. Dr. Glass suggested that the starting point for continuing the 106 process is for the consultants to look at the implications of reducing the width of the new alignment. Dr. Glass suggested that we evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.
57. Patrick Carpenter suggested that an advisory team be formed similar to the one established for the US 27/Spy Run project. Mr. Carpenter added that the consulting parties for that project found the advisory team helpful and that if the City has enough flexibility in design, many of the issues brought up today could be resolved through the advisory team. Michelle Briggs-Wedaman added that that was a valuable process that they appreciated. Michael Galbraith added that we are not to that point in the process yet.
58. Charlotte Weybright asked if there has been any discussion on how the project might affect traffic east of the project area. Briana Hope responded that it is not reasonably foreseeable that there will be a significant increase in traffic on State Boulevard or that the project would pull traffic from around the area. John Shoaff added that if you build it, they will come, and if you increase capacity people will use the roadway. Mr. Shoaff continued that you will eventually build right back up to the congestion you are trying to avoid and that there is no question that we are going to affect traffic east of the project.

59. John Shoaff asked if it is necessary to meet the 100-year flood elevation or if a 50-year flood elevation would be possible. Jason Kaiser responded that design exceptions do exist but would be unlikely in this case.
60. John Shoaff referenced a study based in Oklahoma City which resulted in the determination that maximum lane widths should be only 11 feet. Mr. Shoaff continued that INDOT has conservative standards that are overly harsh and outdated and that current AASHTO and FHWA standards should be employed.
61. Michael Galbraith asked if the option of using local funds to repair the bridge has been studied. Shan Gunawardena stated that it has not been considered because the recommendation from the FEMA flood study is that the bridge should be raised out of the floodway.
62. Michelle Briggs-Wedaman stated that the project will cause a significant land-use change as homes will be abandoned and rental homes will be less desirable. Ms. Briggs-Wedaman asked if the City is actually attempting to change the land use and stated that the area will become a commercial corridor. Shan Gunawardena responded that there will be no change in land use because there is no land left to develop in the area. Mr. Gunawardena added that the only area left to change is the area between the existing State Boulevard roadway and the proposed roadway, which is being designed specifically to buffer existing homes from the new roadway. Dan Avery added that transportation planning is based on land use development and that there is no projected land use change to the area.
63. Michelle Briggs-Wedaman expressed concern that residential homes between Clinton and Eastbrook will be converted to commercial businesses as a result of the project. Shan Gunawardena responded that the homes in that area would not be attractive locations for commercial properties. Jan Dailey added that she believes there is a clause which states that if you acquire property through eminent domain that you cannot then repurpose the land for commercial property. Ms. Briggs-Wedaman responded that we are talking about voluntary buyout, rather than eminent domain.
64. Michael Galbraith again asked if the City has studied completing the bridge replacement without federal aid. Shan Gunawardena responded that no, the City has not studied that, because any replacement of the bridge that leaves it within the flood zone does not meet the purpose and need. The bridge is owned by the County and they would be responsible for that maintenance. Dan Avery added that that is essentially the do-nothing alternative.
65. John Shoaff stated that we need to hire a professional historical landscape architect that would be American Structurepoint's partner, not subordinate.
66. Briana Hope stated that, in terms of next steps, we know that this is an adverse effect and we are going to evaluate the minimization and alternative suggestions from today's meeting and incorporate those into the Section 4(f) alternative analysis, which will also be incorporated into the Section 800 documentation.
67. Patrick Carpenter pointed out that, when we know there is an adverse effect, FHWA must notify the Advisory Council on Historic Preservation (ACHP) to let them know about the adverse effect in case they want to become involved. FHWA has already invited the ACHP to participate but they have not responded yet.
68. Susan Haneline asked if an environmental impact statement is being prepared and, if so, when will it be available to review. Briana Hope responded that a Categorical Exclusion (CE) Level 4 is being prepared and that the Section 106 process is incorporated into that CE document. Michelle Briggs-Wedaman asked who is overseeing that process. Ms. Hope responded that American Structurepoint is preparing the CE, and it will be reviewed and approved by INDOT and FHWA and then released for public involvement.
69. Michelle Briggs-Wedaman requested that formation of an advisory council or comment process for the overall environmental process be considered. Joyce Newland responded that that is called a Citizens Advisory Council (CAC). Jason Kaiser added that a CAC is not necessarily just for the NEPA process, and that it is really a formal name for a small public information meeting or meetings.

70. John Shoaff again stated that American Structurepoint is primarily a road engineering firm and that the City needs to hire a professional landscape architecture firm. Rich Zielinski responded that we will discuss this with the City. Patrick Carpenter added that that is something else that could be considered during the MOA process.
71. Susan Haneline expressed frustration at being stuck in a holding pattern for 3 years and asked if we could discuss where the project proceeds from here and a timeline. Shan Gunawardena responded that the current schedule will include property acquisition in 2012, project letting in 2013, and construction in 2014.
72. Briana Hope stated that an advisory council similar to the US 27 project will be established to contribute to the MOA and mitigation measures therein.
73. Patrick Carpenter stated that another consulting parties meeting is anticipated and during that meeting we will discuss mitigation and forming the advisory team.
74. Camille Fife stated that State Boulevard is a contributing resource in the newly named Park and Boulevard nomination. Ms. Fife added that this may not make a significant difference in the project since there are already major impacts anticipated for the historic district.
75. Michelle Briggs-Wedaman asked if there are any examples of road corridors in the City that consulting parties can visit for ideas on the landscaping design. Shan Gunawardena responded that the Ardmore corridor is probably the best example. Mr. Gunawardena added that the City remains open to suggestions and comments and is willing to attend neighborhood association meetings to discuss the project.
76. Michelle Briggs-Wedaman thanked everyone for the time and effort with regard to the meeting.
77. Michael Galbraith asked that American Structurepoint outline what we think was accomplished today in terms of the established meeting agenda. Briana Hope responded that the project alternatives were presented and comments were provided which will now be incorporated into the alternatives analysis. Mr. Galbraith added that he does not feel the two east-west corridors were thoroughly evaluated and that additional mapping should be provided to consulting parties. Jason Kaiser responded that the analysis has been done and the impacts have been predicted and elaborate drawings are not necessary to determine the impacts of an alternative which does not even meet the purpose and need.
78. Jan Dailey requested a reevaluation of the option of flipping the existing alignment to the south.
79. Michael Galbraith again requested that a more detailed discussion of the two east-west corridors takes place at some time.
80. Patrick Carpenter requested that we evaluate the option of reducing the width of the preferred alternative to 3 lanes.
81. Dr. Glass requested that we evaluate any additional alternatives for providing the neighborhood access to State Boulevard which may reduce the number of homes that would need to be taken.
82. Dan Avery stated that the current preferred alternative was presented to the neighborhood association at a planning charrette and that there was a large amount of concurrence at that meeting with the proposed design.
83. Briana Hope thanked everyone for their participation and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will coordinate with the City regarding creating an online archive for the project's Section 106 correspondence and documents.
- American Structurepoint will evaluate an additional State Boulevard alternative which includes a 3-lane design.
- American Structurepoint will evaluate an additional State Boulevard alternative which will generally flip or mirror the existing State Boulevard alignment to the south.
- American Structurepoint will coordinate with NIRCC to obtain the most recent traffic volume and crash data (2010).
- The consulting parties will be sent this information and asked to comment and express their concerns with the presented information.
- Once SHPO provides written concurrence with the findings of the Historic Property Report (HPR), the Section 800 documentation will be prepared and the FHWA will issue the "Adverse Effect" finding.
- An additional consulting parties meeting will be scheduled once the "Adverse Effect" finding has been issued by FHWA. The purpose of that meeting will be to discuss the formation of an advisory group and the development of mitigation measures to be included in the Memorandum of Agreement (MOA).

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or blackey@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.



Brett W. Lackey

BWL:mgn

Enclosures

Lackey, Brett

From: Lackey, Brett
Sent: Thursday, September 08, 2011 12:18 PM
To: Newland, Joyce; Carpenter, Patrick A; 'blawrence@indot.in.gov'; Hope, Briana; Johnson, Paul; Zielinski, Rich
Cc: 'Shan.Gunawardena@ci.ft-wayne.in.us'; 'dan.avery@co.allen.in.us'; 'Kaiser, Jason'; Crites, Scott
Subject: State Blvd Mtg 9/2

A meeting was held on Friday, 9/2 at the American Structurepoint office to discuss the 9/1 State Boulevard CP meeting. In attendance were Joyce Newland of the FHWA and Briana Hope, Paul Johnson, Brett Lackey, and Rich Zielinski of American Structurepoint. Patrick Carpenter and Ben Lawrence of INDOT Environmental Services participated in the meeting via conference call. The overall purpose of the meeting was to recap the main points of the CP Meeting and discuss FHWA's concerns with the overall public controversy of the project and potentially elevating the environmental document to an Environmental Assessment (EA). The following meeting minutes summarize the discussion:

- While the purpose and need has been accepted by FHWA and INDOT the CPs still question the purpose and need and alternatives analysis.
 - FHWA has stated that the P& N is acceptable and they are prepared to move forward in the Section 106 Process.
 - FHWA requested the additional traffic data the MPO said was available at the CP Meeting be made available to the CPs for review. American Structurepoint and the MPO will make an effort to highlight and interpret the data for the CPs.
- Joyce Newland (FHWA) brought up the potential of elevating the project to an Environmental Assessment (EA) and forming a Community Advisory Committee (CAC) as requested during the CP Meeting.
 - Ben Lawrence (INDOT) believes the CAC should only be pursued if it is expected to produce a different result. All in attendance agreed that the CAC would likely contain the same people that are participating CPs and the comments and concerns would be the same and that the CAC would not produce different results. In addition, as part of the Memorandum of Agreement (MOA) an advisory team would be formed to act in a similar manner as a CAC being involved in the more detailed context sensitive design elements of the project and able to provide feedback and recommendations.
 - Patrick Carpenter (INDOT) suggested that continuing to have Public Information Meetings would likely be more beneficial than forming a CAC.
 - Ben Lawrence suggests that the project should be left as a CE-4 with the understanding that Section 106 will continue to be a contentious issue. FHWA agreed that the CE-4 remains an appropriate level of environmental documentation.
- Patrick Carpenter suggested that Joyce call the ACHP (Follow-up) and perhaps further encourage their involvement in the project.
 - All parties agreed that the involvement of the ACHP would be very beneficial and help keep the process on track and moving forward.
 - We should hear back from ACHP within 30 days, before the CP Meeting Minutes are sent to CPs.
- All parties agreed American Structurepoint should further elaborate of the alternatives analysis provided to the CPs and that the other alternatives suggested be summarized and explained as to why they are or are not feasible and prudent.
 - Patrick Carpenter suggests we specifically list what alternatives Structurepoint is adding and also re-evaluating as a result of the CP meeting. This should happen either before 800/finding or included in the cover letter with the 800/finding.

- Joyce also suggested that appearance of bisecting the neighborhood also be explained in more detail in the alternatives analysis.
- Structurepoint needs to talk to the City about the possibility of a website to post Section 106 correspondence.
 - Either city website or www.structurepoint.com
- Historic Property Report (HPR) will need to be updated with State Boulevard Roadway listing.
 - Patrick Carpenter indicated that we do not need to produce a new HPR, only provide an addendum to the original and a new cover page.
- Joyce Newland would like to request two hardcopies of the Draft CE, when they are ready for review.

If anyone has any questions or comments on the above meeting minutes, please let me know. A copy of the minutes will be included in the next correspondence sent to consulting parties.

Thanks

Brett W. Lackey

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Alternative 1: Butler Road – Vance Road Corridor (Avoidance of Historic Properties)

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment, in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining approximately 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 125 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations at Franke Parke Elementary School, and Fort Wayne Children's Zoo. Of the approximately 2.25 miles of new roadway alignment required by this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to the north end of the Brookview-Irvington Historic District. Approximately 0.25 mile of this alignment would bisect the Brookview-Irvington Historic District as well as Vesey Park.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential and commercial relocations required for construction.

Alternative 2: Spring Street – Tennessee Avenue (Avoidance of Historic Properties)

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

This alternative would require approximately 0.60 mile of new roadway alignment, in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required, in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining approximately 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of approximately 75 residential relocations and 15 commercial relocations would be required. The alternative would also result in impacts or relocations of the Science Central, Lakeside Park, and Lawton Park.

This alternative avoids impacts to historic properties identified within the APE of this project, however the alternative still results in impacts to other historic properties not included in the project APE, including the Science Central facility.

This alternative avoids impacts to the identified Section 4(f) resources, but transfers those impacts to additional Section 4(f) resources located outside this project's APE. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address corridor connectivity, safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential, commercial, and recreational property impacts/relocations required for construction.

Alternative 3A: State Boulevard Preferred Alternative (Minimization of Impacts to Historic Properties)

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes while correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard will have four 10'-0" travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes will be separated by an 8'-0" wide raised median. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190'-0" south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

Several alternates for providing access to the residential neighborhood located immediately north of the existing State Boulevard roadway were evaluated. A discussion of those access alternates is below.

Access Alternate 1

Access Alternate 1 involved reconstructing the intersection of Terrace Road and State Boulevard. This alternate would maintain the existing State Boulevard alignment to provide access to Oakridge Road and Eastbrook Drive. This alternate was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Access Alternate 2 (Preferred Access Alternative)

Access Alternate 2 involves creating a new access road which will extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections State Boulevard intersections with Eastbrook Drive and Terrace Drive will be eliminated and turned into cul-de-sacs. This is the preferred access alternate.

Access Alternate 3

Access Alternate 3 essentially combines the previous two access alternates. This access alternate would create a new Oakridge Road intersection with the new State Boulevard alignment. The Eastbrook Drive and State Boulevard intersection would be eliminated; however the Terrace Road intersection would be reconstructed to provide direct access to Terrace Road off of the new State Boulevard Alignment. Access Alternate 3 was discarded due to safety and traffic concerns. This access alternate would create the additional intersection of existing State Blvd. and Terrace Rd. approximately 45ft north of the proposed intersection of Terrace Rd. and Proposed State Blvd. This close intersection proximity causes inadequate intersection sight distance and the possibility of increased traffic accidents.

Alternative 3A would require approximately 15 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the new alignment.

Combined concrete curb and gutters will be constructed throughout the corridor. A raised median containing landscape elements will be constructed where left turn lanes are not required between Oakridge Road and Clinton Street.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 20 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard on the existing alignment.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.

Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and widening the new alignment to 4-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street, and “mirror” or “flip” the alignment to the south. This alternative would require a new bridge with additional travel lanes over Spy Run Creek.

This alternative would require approximately 5 residential relocations from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to construct the new roadway and bridge structure. Three commercial relocations near the intersection of Clinton Street and proposed State Boulevard would also be required by this alternative.

While this alternative would reduce impacts to the historic properties on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Blvd would cross the Spy Run Creek, impacts to Spy Run Creek would be increased. The new bridge length would need to be approximately 4 to 5-times longer than the bridge design included in Alternative 3A (Preferred Alternative). This alternative would also require construction of a second intersection of State Boulevard with Clinton Street. The intersection would be built in close proximity to the existing intersection which would cause traffic delays and increase the possibility of additional traffic accidents. The additional intersection would be configured at a skew which would also result in sight distance safety and possible additional traffic accidents. The increased length of the proposed bridge combined with relocating the roadway south would also likely cause the intersection of State Blvd and Clinton Street to be raised thus causing additional reconstruction along Clinton Street and increasing project costs. This alternative would also result in additional impacts to commercial businesses, including the gas station at the corner of Clinton Street and

State Boulevard, as well as the plumbing business on the opposite corner, and the Kroger property. The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the safety and traffic concerns included in the project's purpose and need. Furthermore, the alternative is not prudent due to the increased project costs, impacts to commercial businesses, and significant safety and engineering concerns inherent in the design.

Alternative 3D: Preferred Alignment with 3-Lane Typical Section

This alternative is similar to Alternative 3A (Preferred Alternative) but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes while correcting the substandard horizontal curve.

By reducing the typical section from 4-lanes (Alternative 3A/Preferred Alternative) to 3-lanes, construction limits are reduced by approximately 10-feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from 4-lanes to 3-lanes is only 10-feet, this Alternative would result in impacts to 15 residential properties within the Brookview-Irvington Historic District; the same number of relocations as the preferred alternative.

Beginning at Cass Street and extending to Clinton Street, State Boulevard will have two 10'-0" travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes will be separated by a 12'-0" wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes will be separated by a 12'-0" two way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard geometrics as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment will shift a maximum of approximately 190' south of existing State Boulevard. The vertical alignment will be raised approximately 7'-0" at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue will consist of four 11'-0" travel lanes, two in each direction, separated by a 12'-0" two way left turn lane. As appropriate, left turn lanes will be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue will closely follow the existing roadway.

New sidewalks, varying in width from 5'-0" to 10'-0" will be constructed on both sides of the roadway. The sidewalk will be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically 5'-0" wide, will be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting will be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue will be modified as necessary.

New curb inlets and storm sewer will be constructed throughout the project limits.

A new bridge structure will replace the existing bridge over Spy Run Creek. The proposed bridge will be elevated approximately 7'-0" to eliminate roadway flooding along State Boulevard.

As a part of this project, a new pedestrian bridge will be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps will be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps will be utilized by the proposed Pufferbelly Trail which will be constructed by others.

The alternative is considered feasible. However, the alternative is not considered prudent as it does not address the project's entire purpose and need. This alternative does not address safety concerns, corridor connectivity, and traffic concerns along State Boulevard. This alternative would not address the congestion concerns at the intersections of State Boulevard with Cass Street and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain.

Alternative 4: No Build

This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate, resulting in additional pavement failures, traffic accidents, and flood damage. The existing bridge over Spy Run Creek is rated structurally deficient and would require replacement even under the no-build option. Due to the type of bridge (reinforced concrete girder) and level of deterioration, the bridge would require full replacement. Continued flooding of Spy Run Creek would require the bridge to be replaced at the elevation concurrent with the preferred alternative.


















The No-Build alternative would result in historic impacts, as the existing bridge over Spy Run Creek is considered a non-select, historic bridge.

This alternative is feasible, but is not prudent as it does not meet the purpose and need for the proposed project.

HCM Signalized Intersection Capacity Analysis

1: State Blvd & Clinton St



















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	543	46	200	437	0	0	0	0	126	1707	47
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.99		1.00	1.00						1.00	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3427		1687	1792						4999	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3427		1687	1792						4999	
Peak-hour factor, PHF	0.92	0.84	0.72	0.88	0.85	0.92	0.92	0.92	0.92	0.73	0.84	0.78
Adj. Flow (vph)	0	646	64	227	514	0	0	0	0	173	2032	60
RTOR Reduction (vph)	0	9	0	0	0	0	0	0	0	0	3	0
Lane Group Flow (vph)	0	701	0	227	514	0	0	0	0	0	2262	0
Heavy Vehicles (%)	0%	3%	13%	7%	6%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3						1	
Permitted Phases										1		
Actuated Green, G (s)		18.5		13.5	36.5						34.4	
Effective Green, g (s)		18.5		13.5	36.5						34.4	
Actuated g/C Ratio		0.23		0.17	0.46						0.43	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		792		285	818						2150	
v/s Ratio Prot		c0.20		c0.13	0.29							
v/s Ratio Perm											0.45	
v/c Ratio		0.88		0.80	0.63						1.05	
Uniform Delay, d1		29.7		31.9	16.6						22.8	
Progression Factor		1.00		0.58	1.15						1.00	
Incremental Delay, d2		13.8		11.9	2.0						34.8	
Delay (s)		43.5		30.4	21.0						57.6	
Level of Service		D		C	C						E	
Approach Delay (s)		43.5			23.9			0.0			57.6	
Approach LOS		D			C			A			E	
Intersection Summary												
HCM Average Control Delay			48.2			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.95									
Actuated Cycle Length (s)			80.0			Sum of lost time (s)				13.6		
Intersection Capacity Utilization			75.5%			ICU Level of Service				D		
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

2: State Blvd & Spy Run Ave


















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	93	526	0	0	470	211	131	1166	230	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.97				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1612	3505			1827	1568	1612	4672				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1612	3505			1827	1568	1612	4672				
Peak-hour factor, PHF	0.75	0.90	0.92	0.92	0.85	0.70	0.91	0.93	0.77	0.92	0.92	0.92
Adj. Flow (vph)	124	584	0	0	553	301	144	1254	299	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	29	0	50	0	0	0	0
Lane Group Flow (vph)	124	584	0	0	553	272	144	1503	0	0	0	0
Heavy Vehicles (%)	12%	3%	0%	0%	4%	3%	12%	8%	7%	0%	0%	0%
Turn Type	Prot				Perm				Perm			
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Effective Green, g (s)	6.5	35.2			23.2	23.2	34.5	34.5				
Actuated g/C Ratio	0.08	0.44			0.29	0.29	0.43	0.43				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	131	1542			530	455	695	2015				
v/s Ratio Prot	c0.08	0.17			c0.30			c0.32				
v/s Ratio Perm						0.17	0.09					
v/c Ratio	0.95	0.38			1.04	0.60	0.21	0.75				
Uniform Delay, d1	36.6	15.1			28.4	24.4	14.2	19.1				
Progression Factor	1.56	0.41			1.00	1.00	1.00	1.00				
Incremental Delay, d2	36.4	0.3			50.9	5.7	0.7	2.6				
Delay (s)	93.4	6.5			79.3	30.1	14.9	21.6				
Level of Service	F	A			E	C	B	C				
Approach Delay (s)		21.7			61.9			21.1			0.0	
Approach LOS		C			E			C			A	
Intersection Summary												
HCM Average Control Delay			31.9		HCM Level of Service				C			
HCM Volume to Capacity ratio			0.87									
Actuated Cycle Length (s)			80.0		Sum of lost time (s)				15.8			
Intersection Capacity Utilization			75.5%		ICU Level of Service				D			
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

1: State Blvd & Clinton St




















9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	0	629	67	192	539	0	0	0	0	178	1593	117
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)		4.5		4.5	4.5						4.6	
Lane Util. Factor		0.95		1.00	1.00						0.91	
Frt		0.98		1.00	1.00						0.99	
Flt Protected		1.00		0.95	1.00						1.00	
Satd. Flow (prot)		3445		1687	1810						4955	
Flt Permitted		1.00		0.95	1.00						1.00	
Satd. Flow (perm)		3445		1687	1810						4955	
Peak-hour factor, PHF	0.92	0.95	0.83	0.89	0.89	0.92	0.92	0.92	0.92	0.93	0.95	0.75
Adj. Flow (vph)	0	662	81	216	606	0	0	0	0	191	1677	156
RTOR Reduction (vph)	0	8	0	0	0	0	0	0	0	0	8	0
Lane Group Flow (vph)	0	735	0	216	606	0	0	0	0	0	2016	0
Heavy Vehicles (%)	0%	2%	12%	7%	5%	0%	0%	0%	0%	2%	3%	4%
Turn Type				Prot							Perm	
Protected Phases		3		2	2 3							1
Permitted Phases										1		
Actuated Green, G (s)		29.5		17.5	51.5						59.4	
Effective Green, g (s)		29.5		17.5	51.5						59.4	
Actuated g/C Ratio		0.25		0.15	0.43						0.49	
Clearance Time (s)		4.5		4.5							4.6	
Lane Grp Cap (vph)		847		246	777						2453	
v/s Ratio Prot		c0.21		c0.13	0.33							
v/s Ratio Perm											0.41	
v/c Ratio		0.87		0.88	0.78						0.82	
Uniform Delay, d1		43.4		50.2	29.4						25.8	
Progression Factor		1.00		1.51	0.46						1.00	
Incremental Delay, d2		11.7		26.0	5.7						3.3	
Delay (s)		55.1		101.6	19.1						29.0	
Level of Service		E		F	B						C	
Approach Delay (s)		55.1			40.8			0.0			29.0	
Approach LOS		E			D			A			C	
Intersection Summary												
HCM Average Control Delay			37.1			HCM Level of Service				D		
HCM Volume to Capacity ratio			0.84									
Actuated Cycle Length (s)			120.0			Sum of lost time (s)				13.6		
Intersection Capacity Utilization			109.3%			ICU Level of Service				H		
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis

2: State Blvd & Spy Run Ave

9/29/2011

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (vph)	134	606	0	0	508	275	130	1862	243	0	0	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	5.5	5.5			4.8	4.8	5.5	5.5				
Lane Util. Factor	1.00	0.95			1.00	1.00	1.00	0.91				
Frt	1.00	1.00			1.00	0.85	1.00	0.98				
Flt Protected	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (prot)	1770	3539			1863	1615	1719	5036				
Flt Permitted	0.95	1.00			1.00	1.00	0.95	1.00				
Satd. Flow (perm)	1770	3539			1863	1615	1719	5036				
Peak-hour factor, PHF	0.76	0.92	0.92	0.92	0.92	0.82	0.86	0.97	0.92	0.92	0.92	0.92
Adj. Flow (vph)	176	659	0	0	552	335	151	1920	264	0	0	0
RTOR Reduction (vph)	0	0	0	0	0	10	0	15	0	0	0	0
Lane Group Flow (vph)	176	659	0	0	552	325	151	2169	0	0	0	0
Heavy Vehicles (%)	2%	2%	0%	0%	2%	0%	5%	1%	2%	0%	0%	0%
Turn Type	Prot				Perm			Perm				
Protected Phases	2	2 3			3			1				
Permitted Phases						3	1					
Actuated Green, G (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Effective Green, g (s)	10.5	53.2			37.2	37.2	56.5	56.5				
Actuated g/C Ratio	0.09	0.44			0.31	0.31	0.47	0.47				
Clearance Time (s)	5.5				4.8	4.8	5.5	5.5				
Lane Grp Cap (vph)	155	1569			578	501	809	2371				
v/s Ratio Prot	c0.10	0.19			c0.30			c0.43				
v/s Ratio Perm						0.20	0.09					
v/c Ratio	1.14	0.42			0.96	0.65	0.19	0.91				
Uniform Delay, d1	54.8	22.8			40.6	35.7	18.4	29.5				
Progression Factor	0.89	2.00			1.00	1.00	1.00	1.00				
Incremental Delay, d2	92.1	0.4			27.8	6.4	0.5	6.9				
Delay (s)	140.6	46.0			68.4	42.1	18.9	36.4				
Level of Service	F	D			E	D	B	D				
Approach Delay (s)		65.9			58.5			35.3			0.0	
Approach LOS		E			E			D			A	
Intersection Summary												
HCM Average Control Delay		46.7			HCM Level of Service			D				
HCM Volume to Capacity ratio		0.95										
Actuated Cycle Length (s)		120.0			Sum of lost time (s)			15.8				
Intersection Capacity Utilization		109.3%			ICU Level of Service			H				
Analysis Period (min)		15										
c Critical Lane Group												



U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

August 29, 2011

HDA-IN

Ms. Carol Legard
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Advisory Council on Historic Preservation
1100 Pennsylvania Avenue, NW
Washington, DC 20004

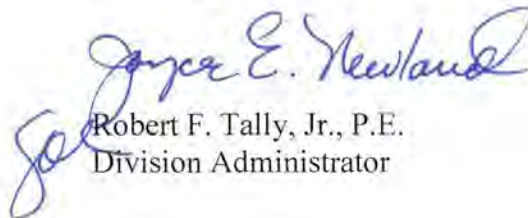
Dear Ms. Legard:

The purpose of this letter is to initiate the coordination necessary for involvement by the Advisory Council on Historic Preservation (ACHP) in the Section 106 process of the State Boulevard Road Reconstruction Project in Fort Wayne, Allen County, Indiana.

FHWA believes that ACHP is warranted based on the criteria set forth in 36 CFR Part 800 Appendix A – Criteria for Council Involvement in Reviewing Individual Section 106 Cases. The State Boulevard Project meets the criteria set forth in Appendix A (c)(1), “...*adverse effects to large numbers of historic properties, such as impacts to multiple properties within a historic district*” and (c)(3) for “...*cases with substantial public controversy that is related to historic preservation issues; with disputes among or about consulting parties which the Council’s involvement could help resolve...*”

If any additional information or supplemental documentation is needed in order for ACHP to decide its involvement, please do not hesitate to contact me at (317)226-5353 or at joyce.newland@dot.gov.

Sincerely,


Robert F. Tally, Jr., P.E.
Division Administrator

Enclosure





Preserving America's Heritage

September 22, 2011

Robert F. Tally, Jr., P.E.
Division Administrator
FHWA – Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Road Reconstruction Project
Fort Wayne, Allen County, Indiana*

Dear Mr. Tally:

On August 30, 2011, the Advisory Council on Historic Preservation (ACHP) received your notification of adverse effect for the referenced undertaking that was submitted in accordance with Section 800.6(a)(1) of our regulations, "Protection of Historic Properties" (36 CFR Part 800). The background documentation included with your submission does not meet the specifications in Section 800.11(e) of the ACHP's regulations. We, therefore, are unable to determine whether Appendix A of the regulations, *Criteria for Council Involvement in Reviewing Individual Section 106 Cases*, applies to this undertaking. Accordingly, we request that you submit the following additional information so that we can determine whether our participation in the consultation to resolve adverse effects is warranted.

- A description of the undertaking, specifying the Federal involvement, and its area of potential effects, including photographs, maps, drawings, as necessary;
- A description of the steps taken to identify historic properties;
- A description of the affected historic properties, including information on the characteristics that qualify them for the National Register;
- A description of the undertaking's effects on historic properties;
- An explanation of why the criteria of adverse effect were found applicable or inapplicable, including any conditions or future actions to avoid, minimize or mitigate adverse effects;
- Copies or summaries of any views or comments provided by the Indiana State Historic Preservation Officer;
- Copies or summaries of any views or comments provided by any affected Indian tribe.

Upon receipt of the additional information, we will notify you within 15 days of our decision.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004
Phone: 202-606-8503 • Fax: 202-606-8647 • achp@achp.gov • www.achp.gov

If you have any questions, please contact Ms. Najah Duvall-Gabriel at 202-606-8585 or via e-mail at ngabriel@achp.gov.

Sincerely,

A handwritten signature in black ink that reads "LaShavio Johnson". The signature is written in a cursive, flowing style.

LaShavio Johnson
Historic Preservation Technician
Office of Federal Agency Programs

Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project will reduce existing congestion and improve traffic flow. State Boulevard is a four-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one west bound lane. East of Clinton Street, State Boulevard is a two lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east-west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E

Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

Removed

Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



November 7, 2011

Brett W. Lackey
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Effects of the State Boulevard Reconstruction and consideration of alternatives (Des. No. 0400587; DHPA No. 5903)

Dear Mr. Lackey:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the materials submitted with your cover letter dated August 15, 2011 and your memorandum dated September 29, 2011 and received which were received on August 16 and September 30, for the above-indicated project in Fort Wayne, Allen County, Indiana.

Your August 15, 2011 letter asked us to offer an opinion on how the National Register boundaries of the listed Brookview-Irvington Park Historic District might be affected by the preferred alternative (now called Alternative 3A in the revised Section 4[f] alternatives analysis). Having considered the marked aerial photograph shown at the last consulting parties meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative. We also think that if the preferred alternative were to be implemented as shown on the marked aerial photograph, one might still be justified in considering the part of the historic district that lies to the southwest of Spy Run (i.e., along Edgehill Avenue, Jacobs Avenue, and Clinton Street south of Westbrook Drive) and south of State Boulevard) to be sufficiently connected to the rest of the district at the Westbrook Avenue-State Boulevard intersection that the buildings and landscapes in that southern end of the district would still contribute to the district's significance. However, that is not an ideal situation from a National Register boundary delineation standpoint. We would anticipate that, even if the right-of-way along the preferred alignment were appropriately landscaped, the eye would perceive be a rather broad and obvious expanse of non-contributing land and mostly straight, new roadway between the current alignment of State Boulevard and the outline that has been left Westbrook, south of Spy Run (as a result of the flood buy-outs and pavement removal). We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished.

The minutes of the September 1, 2011 consulting parties meeting reflect some, but not all, of the comments I offered regarding a couple of other, potential alternatives. One was the widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B). We recall, however, that this alternative was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.

I recall also having asked that an alternative on roughly the same alignment as the preferred alternative (i.e., running south of the existing State Boulevard between Westbrook Drive and a point to the east of Terrace Road) be considered that would have three lanes, instead of four lanes with a left turn lane or landscaped median (captured in part in points 50, 56, and 81 in the minutes). This second alternative would have one through lane in each direction and a center left turn lane. I had asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of

fewer residences than the preferred alternative. It appears that this alternative is essentially what is now called Alternative 3D in the revised Section 4(f) alternatives analysis.

We would like to elaborate on that second alternative, 3D, in light of our observations during a site visit to the project area and the Brookview-Irvington Park Historic District that Wade Tharp, John Carr, and I made following the September 1 meeting. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State. Even if a T-intersection would be practicable only at Eastbrook, perhaps a single, exit-only lane from new State to existing State could be left in place near where the bypass turns southwestward, which would retain westbound access to the bypassed part of State. Narrowing the roadway to three lanes could eliminate the need to take the contributing house at the northwest corner of State Boulevard and Westbrook Drive and leave somewhat more of the back yards of the houses along the south side of State, possibly saving as many as three more houses on State. Narrowing the roadway to three lanes also could reduce the cost of the new bridge over Spy Run and pull the new bridge away from the historic bridge. Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work, and with the installation of bollards both the Eastbrook and Westbrook ends. As a pedestrian bridge, its closure would not be a major impediment to transportation when floodwaters overtop it. We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.

The revised alternatives analysis says that Alternative 3D "is not considered prudent as it does not address the project's entire purpose and need." We wonder, however, whether a project is worthwhile and fundable by FHWA only if it fully meets all the needs and purposes that are identified. We wonder, also, whether there is not room for compromise between purpose and need and other values, such as historic preservation and neighborhood livability and preservation.

In the meeting minutes, points 39 and 40 refer to questions raised about the possibility of a new alignment (now called Alternative 3C) farther to the south than the preferred alternative (Alternative 3A). We do not recall that part of the discussion *verbatim*, but we think that the reference in the point 39 to "reversing the existing alignment/flipping the existing curb to the south" probably refers to a suggestion that the *curve* of the new alignment west of Clinton Street be reversed or flipped, so that most of the new alignment would be farther to the south than the preferred alternative.

The revised alternatives analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection? That curve now is proposed to begin west of the alley between Clinton and Terrace Road. However, if the beginning of the curve could be moved perhaps 100 feet farther east, without changing the radius significantly and without changing significantly the angle of the State-Clinton intersection, most of the new State Boulevard would shift to the east and away from several of the houses on the south side of State between Terrace and Eastbrook Drive. If that shift were coupled with the elimination of the proposed, southward extension of Oakridge that we suggested above for Alternative 3D, then several contributing houses on the south side of State could be avoided by the project. More of the character of the curve of State Boulevard that Arthur Shurcliff designed in 1917 could be preserved if houses were left on both sides of State. Furthermore, we noticed, in the historic properties report and in the National Register nomination for the Fort Wayne Parks and Boulevard System Historic District, that George Kessler's 1912 plan for State Boulevard (or Pfeiffer Boulevard, as he called it) roughly would have aligned State, running westward from its intersection with Spy Run Avenue, with that part of State lying to the west of Spy Run (the stream). It appears to us that shifting the curve of the new State Boulevard toward the east would result in an alignment somewhat closer to that which Kessler had intended (but which was not implemented) between Clinton and Spy Run (the stream), while retaining much of the existing State Boulevard's Shurcliff-designed curve through the Brookview-Irvington Park Historic District.

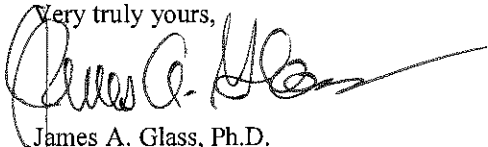
The eastward shifting of the alignment of either Alternative 3A or Alternative 3D probably would result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along existing State Boulevard. Even if the project cost were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff's design of the Brookview-Irvington Park Historic District, while largely meeting the city's purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler's plan.

As we earlier had stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we noted that the draft historic properties report (Fife 9/09), states, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation and Archaeology for review before we can comment further (see list of qualified professional archaeologists at the DHPA website link http://www.in.gov/dnr/historic/bin/qp/qp_archo.pdf).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

A copy of the revised 36 C.F.R. Part 800 regulations that took effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Staffan Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Patrick Carpenter, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Brett W. Lackey, American Structurepoint, Inc.



AMERICAN
STRUCTUREPOINT
INC.

May 17, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

As discussed at the September 1, 2011, Consulting Parties Meeting, resources previously identified in the Historic Properties Report (HPR) (Westerly Group, Inc, 2009) as eligible for listing in the National Register (NR) have since been listed on the NR, requiring an update to the HPR. Weintraut & Associates, Inc., has prepared an Additional Information (AI) Report to append the HPR. The AI is a supplement to the HPR following the inclusion of two new NR listed resources within the Area of Potential Effect (APE). Historians from Weintraut & Associates identified portions of both the Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011) as contained within the project APE. As previously stated, it is anticipated that the proposed project activities will have an adverse affect on these properties; therefore, an "Adverse Effect" finding continues to be anticipated.

The AI and eligibility recommendations therein were approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. Additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. At this time we are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, you are asked to reply with comments on the AI within 30 days of receipt of this letter.

Please also beware that Weintraut & Associates will act as the Qualified Professional associated with the Section 106 process on this project as we continue to move forward. We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. The information herein will be provided to all consulting parties.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

IN20071404



AMERICAN STRUCTUREPOINT, INC.

Linda Weintraut, Ph.D.

May 17, 2012

Page 2

Distribution List

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Haneline, Brookview Civic Neighborhood Association
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amy Johnson, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT



AMERICAN
STRUCTUREPOINT
INC.

May 22, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

You should have recently received a letter dated May 17, 2012, discussing the Additional Information (AI) Report, prepared by Weintraut & Associates, appending the Historic Properties Report (HPR) prepared for the State Boulevard Project. The report included with the May 17 letter was the original HPR and did not include the AI. The AI was accidentally omitted from the coordination.

Please find enclosed the AI and eligibility recommendations approved for distribution to consulting parties by the INDOT Cultural Resources Section on May 10, 2012. As previously stated, additional information in response to consulting party comments, including the November 7, 2011, SHPO letter, are forthcoming and will be provided to the SHPO and all consulting parties. We are requesting SHPO and all consulting parties review and comment on the AI and eligibility determinations therein. To facilitate the development of this project, we have extended the comment period and ask that you reply with comments on the AI within 30 days of receipt of this letter.

We appreciate your cooperation in the development of this project and apologize for any confusion our omission may have caused. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. The information herein will be provided to all consulting parties.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:ead

Enclosures

IN20071404



Dr. Weintraut
May 22, 2012
Page 2

Distribution List

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
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Amanda Ricketts, IDNR
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Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT

Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



June 22, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: "Historic Property Report: State Boulevard Reconstruction from Spy Run to Cass Street, Additional Information Report" ("AI Report"; Natali, 4/2012) to append the historic properties report (Fife, 9/2009) for the State Boulevard Reconstruction Project (Des. No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials submitted with your cover letters dated May 17 and 22, 2012, and received on May 21 and 24, respectively for the above-indicated project in the City of Fort Wayne, Allen County, Indiana.

We agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the area of potential effects, for inclusion in the National Register of Historic Places

We were asked to address specifically the eligibility of two, particular properties. First, we agree with the AI Report that the ca. 1895, two-story gabled-ell house at 315 East State Boulevard does not appear to possess sufficient historical or architectural significance or integrity to be eligible for inclusion in the National Register. Second, the AI Report notes that the Fort Wayne Park and Boulevard System Historic District National Register nomination characterized the portion of State Boulevard that lies within the Brookview-Irvington Park Historic District as being "individually eligible for the NR" (page 75 of the nomination). We do not consider that comment, which was included in an explanatory note about features of the district under "Section 7. Description" within the National Register nomination form, to confer individual eligibility on State Boulevard or any part of it. Furthermore, based on the information provided in the AI Report, we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the area of potential effects of this project is individually eligible for the National Register. We do not disagree, however, with the Fort Wayne Park and Boulevard System nomination's identification of the portion of State Boulevard in question as a contributing resource to that historic district.

As we stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultants of Ossian. However, we note that the historic property report (Fife 9/09) stated, "The final alignment has not yet been determined, thus the actual number of relocations and/or right-of-way to be acquired is not yet known" (page 4). Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources. The survey must be done in accordance with the "Secretary of the Interior's Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716). A description of the survey methods and results must be submitted to the Division of Historic Preservation

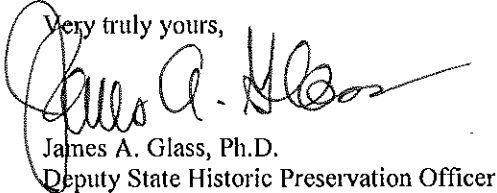
and Archaeology for review before we can comment further (see the list of qualified professional archaeologists at the Division of Historic Preservation and Archaeology website, http://www.in.gov/dnr/historic/bin/qp/qp_archeo.pdf).

An example of an area that might still need to be surveyed would be the portion of a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report (Stillwell 4/2/09). If an entire residential lot, rather than just the portion of it that was surveyed, will now have to be acquired for the State Boulevard Reconstruction, then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction, please continue to refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick A. Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.



6/20/2012 Agency Meeting minutes included in body of letter.

AMERICAN
STRUCTUREPOINT
INC.

July 2, 2012

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to provide additional information in response to comments provided by the DHPA on the State Boulevard Project included in a letter dated November 7, 2011, and items further discussed in an agency meeting held June 20, 2012. In attendance at the June 20, 2012, agency meeting were Dr. James Glass and John Carr - IDNR DHPA, Shan Gunawardena - City of Fort Wayne, Larry Heil - FHWA, Patrick Carpenter and Mary Kennedy - INDOT CRO, Jason Kaiser - INDOT Fort Wayne District (conference line), Dan Avery - NIRCC (conference line), Briana Hope and Scott Crites - American Structurepoint, and Linda Weintraut - Weintraut and Associates.

The letter states that the DHPA believes the minutes of the September 1, 2011, consulting parties meeting reflect some, but not all, of the comments SHPO offered regarding a couple of other, potential alternatives. We are providing responses to these additional comments.

- ❖ **The widening of State Boulevard from two lanes to three on its existing alignment (now called Alternative 3B) ... was rejected because it would not meet purpose and need, due to sight-distance and perhaps other issues.**

Alternative 3B includes widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to four lanes. By reducing the typical section from four lanes to three lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this Alternative would result in impacts to 20 residential properties within the Brookview-Irvington Historical District, the same number of relocations as the 4-lane typical section on existing alignment. This alternative would also still require a new bridge with additional travel lanes over Spy Run Creek. The widening of two lanes to three lanes as suggested by your office would ultimately result in a feasible alternative. However, the alternative is not considered prudent as it does not address the project purpose and need. This alternative does not address safety concerns, design deficiencies, site distance, or roadway flooding concerns along State Boulevard. Furthermore, this alternative is not prudent due to the extensive number of residential historic property impacts/relocations required for construction.



- ❖ [DHPA had] asked that an alternative on roughly the same alignment as the preferred alternative be considered that would have three lanes, instead of four lanes, with a left-turn lane or landscaped median. It appears that this alternative is essentially what is called Alternative 3D in the revised Section 4(f) alternative analysis. [DHPA] asked that consideration also be given to whether the second alternative could be coupled with the elimination of the southward extension of Oakridge Road to the new alignment of State Boulevard, which might require the taking of fewer residences than the preferred alternative. Constructing new intersections of either Eastbrook Drive or Terrace Road, or both, with State Boulevard could eliminate the need to extend Oakridge southward to the new alignment of State Boulevard, which, in turn, could eliminate the need to take three houses on the south side of State.

As part of the alternatives analysis, Alternative 3D was developed to explore the impacts of reducing the proposed State Boulevard to a 3-lane section with one through lane in each direction and a center median or left-turn lane where required. This option would reduce project costs by reducing pavement costs and allowing a narrower bridge to be constructed over Spy Run Creek. In addition to the reasons Alternative 3D was determined to be unfeasible in the 4(f) alternative analysis, the reduction in lanes would also result in traffic congestion and level of service at the intersections remaining the same as the existing conditions. Northeastern Indiana Regional Coordinating Council (NIRCC) has established a Level of Service "D" as the acceptable peak hour service level for intersections and corridors within the urban area. Currently and in the 3-lane section scenario, both intersections exhibit intersection movements having service levels of E or F. The following table (as included and discussed in the purpose and need statement) summarizes the levels of service at the Clinton Street intersection and Spy Run Avenue intersection, with the lane configurations remaining the same as the existing condition due to constructing State Boulevard as a 3-lane section:

**State Boulevard and Spy Run Avenue
 Intersection – Levels of Service**

Morning Peak	LOS Existing
Eastbound Left	F
Westbound Through	E

Evening Peak	LOS Existing
Eastbound Left	F
Eastbound Through	E
Westbound Through	E

**State Street and Clinton Street
 Intersection – Levels of Service**

Morning Peak	LOS Existing
Southbound Through	E

Evening Peak	LOS Existing
Eastbound Through	E
Westbound Left	F

It was also found in our analysis that by reducing the typical section from four lanes to three lanes, the limits of construction are reduced by approximately ten feet in width along both the north and south sides of the proposed State Boulevard. This reduction in construction limit width is not sufficient to significantly reduce impacts along the roadway. This configuration would result in impacts to approximately 15 residential properties within the Brookview-Irvington Historic District, the same as the preferred alternative.

As requested in your November 7, 2011, letter and per subsequent discussions in our June 20, 2012, meeting, alternate configurations for the connection into the Brookview neighborhood were explored.

One possible alternate connection was to extend Eastbrook Avenue to the proposed State Boulevard alignment. The skew of Spy Run Creek in conjunction with the length of the proposed bridge required to span the creek obstruct Eastbrook Avenue from connecting to the proposed State Boulevard roadway. The elevation of proposed State Boulevard near Eastbrook Drive would require the connection between existing State Boulevard and proposed State Boulevard to be constructed with a grade steeper than the 10 percent maximum grade allowed for a low speed urban street per the Indiana Design Manual (IDM). This design would be considered substandard and would contribute to safety concerns by not providing adequate sight distance to safely enter the proposed State Boulevard intersection.

Another possible configuration would be to extend Terrace Road to intersect the proposed State Boulevard alignment. The extension of this intersection would require construction near the midpoint of the State Boulevard mainline curve. In order to minimize impacts, the State Boulevard mainline curve at this location utilizes the maximum curvature allowed per IDM for low speed urban street criteria (460 feet). Constructing an intersection on the curve would increase potential safety concerns due to the geometry of State Boulevard at this location and inadequate sight distance that would not allow a motorist to safely negotiate the proposed intersection with State Boulevard. This scenario would also locate the Terrace Road intersection approximately 250 feet west of the Clinton Street intersection. The minimum intersection spacing per the IDM is 400 feet in order to facilitate traffic and avoid impeding traffic operations and efficiency at both intersections. Locating the intersection at Terrace Road would violate the IDM spacing requirement from the Clinton Street intersection and should be avoided.

After further analyzing and discussing alternate configurations to maintain a connection for the Brookview neighborhood to proposed State Boulevard, it is recommended that the proposed Oakridge Road extension remain as the preferred connection to State Boulevard.

During further discussion at our June 20, 2012, meeting, the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard was also explored. It was agreed that the designer would evaluate potential adjustments where possible to minimize impacts to these houses. The results of this effort will likely minimize impacts, allowing three houses once assumed to be total acquisitions to remain in place.

The following design modifications will be evaluated through the design process to minimize impacts to houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard by reducing the Oakridge Road typical section width:

- Eliminate boulevard roadway section for Oakridge Road
- Eliminate center curbed median and landscaping
- Shorten right-turn lane for vehicles exiting Oakridge Road west on State Boulevard

As agreed at the June 20, 2012, agency meeting, if after the design process is complete it is determined that the houses in question can remain in place, a firm commitment will be made to avoid complete acquisition of these parcels.

- ❖ **Although we realize that the historic bridge is structurally deficient for vehicular use, we wonder whether it might still be suitable for pedestrian use with little or no rehabilitation work.**

The existing State Boulevard bridge is hydraulically inadequate to accommodate a 100-year flood scenario and is an obstruction to water flowing through Spy Run Creek. Allowing this bridge to stay in place would also impede the hydraulic adequacy of the proposed State Boulevard bridge located immediately downstream. The existing concrete girder bridge is in poor condition and would continue to rapidly deteriorate due to continued Spy Run Creek flood events created from the backwater of the St. Mary's River. The close proximity of the western abutment of the existing bridge and the western abutment of the proposed bridge

would also make construction difficult. Due to the poor condition and required bridge hydraulics, it is recommended that the existing State Boulevard bridge be removed.

- ❖ **We also recommend that, where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Schurcliff's landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.**

Existing State Boulevard and Eastbrook Drive curbs and sidewalk will be left place where practicable as a mitigation effort for historic preservation. In addition, other potential mitigation measures were also discussed as part of the June 20, 2012, meeting, including:

- Photo documentation of the neighborhood and structures
 - Research and restore existing State Boulevard streetscape to replicate its look during the early 20th Century
 - Historical markers
 - Distribution locally of literature regarding the historical significance of the area
- ❖ **The revised alternative analysis concludes that Alternative 3C would not be prudent in that it does not address the safety and traffic concerns identified in the purpose and need statement and because of increased cost, commercial business impacts, and safety and engineering concerns. At least some of those disadvantages appear to be based on the assumption that a second intersection of State Boulevard and Clinton Street would have to be constructed and that the existing intersection and the second intersection would coexist, a short distance apart. However, has consideration been given to moving, somewhat farther to the east, the southwestward curve of the preferred alternative (Alternative 3A) or of Alternative 3D for new State Boulevard, as one travels westward from the Clinton Street intersection?**

This suggestion was also discussed in detail at the June 20, 2012, meeting. If the mainline curve is shifted toward the Clinton Street intersection, the mainline State Boulevard roadway would shift slightly to the southeast from the proposed location. Currently, this proposed curve utilizes the maximum curvature allowed per the IDM for low speed urban street criteria and is located approximately 150 feet from the Clinton Street intersection. The proposed curve was designed at its current location in order to provide 150 feet of tangent roadway prior to the Clinton Street intersection, allowing for adequate sight distance to see both the traffic signals and also vehicles that may be stopped at the traffic signal. Relocating or shifting the roadway curve immediately adjacent to the intersection will likely result in increased safety concerns and increased accidents associated with the Clinton Street signal. Shifting the curve to the east would also require the proposed bridge over Spy Run Creek to be constructed at a greater skew, thus lengthening the bridge span and increasing project cost and stream impacts. Due to the associated safety and cost concerns, it is recommended that the mainline State Boulevard curve remain located per the current preferred alternative.

- ❖ **As we earlier stated in our December 14, 2009, letter to Hayley M. Steele of American Structurepoint, Inc., in regard to the archaeological report (Stillwell 4/2/09) that was submitted previously, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the area that was surveyed for this project by Archaeological Consultant of Ossian. Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, than an archaeological reconnaissance of those areas will be required, in order to determine the presence or absence of archaeological resources.**

In reviewing the area previously surveyed by Archaeological Consultants of Ossian, it has been determined that there are in fact areas within the limits of the preferred alternative that have not been surveyed.

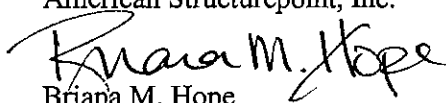
Archaeological Consultants of Ossian is currently performing an archaeological reconnaissance on the additional area and an archaeological report will be provided to your office as soon as it is available.

In addition to the items discussed at the agency meeting regarding the November 7, 2011, DHPA letter and the above responses, it was also determined that the next consulting parties meeting would likely be held in early September once FHWA felt it appropriate to issue a signed finding of adverse effect. The main purpose of the meeting will be to present potential mitigation measures to the consulting parties, as well as solicit additional potential mitigation concepts. The consulting parties will be notified of the date of the next consulting parties meeting concurrently with the distribution and request for comment on FHWA signed finding of adverse effect.

On behalf of Joyce Newland, Larry Heil (FHWA) also reported that the Advisory Council of Historic Preservation had not notified FHWA with a final decision in response to FHWA request for participation in the Section 106 process associated with the State Boulevard Project.

We hope that the information in this letter adequately addresses the concerns expressed in the November 7, 2011, DHPA letter. A copy of this response letter and a copy of the November 7, 2011, DHPA letter will be sent to all consulting parties concurrently with this letter. If I can be of assistance in the interim, please do not hesitate to contact me at (317) 547-5580, or at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

Distribution List

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Haneline, adjacent property owner
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amy Johnson, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT



AMERICAN
STRUCTUREPOINT
INC.

July 16, 2012

James A. Glass, Ph.D.
State Historic Preservation Officer
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: Indiana Archaeological Short Report
State Boulevard Improvements Project – Additional Area
Fort Wayne, Allen County, Indiana
DHPA No. 5903
Des. No. 0400587
Project No. 20071404

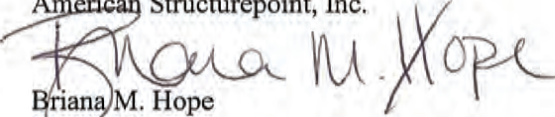
Dear Dr. Glass:

In reviewing the area previously surveyed by Archaeological Consultants of Ossian and brought to our attention in your November 7, 2011 letter, it has been determined that there were in fact areas within the limits of the preferred alternative for the proposed State Boulevard Improvements Project that had not been surveyed. Please find included with this correspondence the Indiana Archaeological Short Report, prepared by Archaeological Consultants of Ossian, for the additional area required for the State Boulevard Improvements project. The short report was reviewed and approved by the Indiana Department of Transportation, Cultural Resources on July 16, 2012.

The Phase I survey detected no properties that are eligible for nomination to the National Register of Historic Places. Since no historically or archaeologically significant sites will be impacted by the proposed undertaking, the Archaeological Consultants of Ossian recommended that project clearance be granted.

To facilitate the development of this project, you are asked to provide any comments you may have regarding this report within 30 days of the receipt of this letter. Please contact me at (317) 547-5580 or by email at bhope@structurepoint.com if you have any questions or require further information.

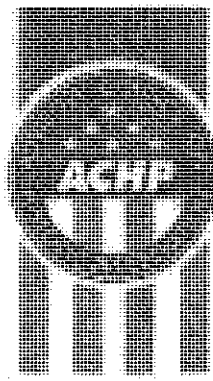
Very truly yours,
American Structurepoint, Inc.


Briana M. Hope
Project Manager

AMB:alo

Enclosures

cc: Shaun Miller, INDOT Cultural Resources



Preserving America's Heritage

July 31, 2012

Mr. Robert F. Tally, Jr.
Administrator
Federal Highway Administration
Indiana Division
575 N. Pennsylvania Street, Rm 254
Indianapolis, IN 46204

Ref: *Proposed State Boulevard Transportation Project
Indiana*

Dear Mr. Tally:

On August 29, 2011, the Advisory Council on Historic Preservation (ACHP) received your letter requesting our participation in the Section 106 consultation for the reference undertaking. On September 22, 2011, we responded to your letter indicating that additional background information was needed in order to complete our review of the undertaking and determine our participation status. On May 10, 2012, the Federal Highway Administration (FHWA) provided the requested information to us and requested, again.

Since receiving the latest request from Indiana FHWA, we have spoken with FHWA staff and the State Historic Preservation Office (SHPO) regarding the timing and basis of your request that we participate in the Section 106 consultation. Based upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time. However, we are available to assist consulting parties regarding compliance with the Section 106 review process in accordance with Section 800.2(b)(2) of our regulations, "Protection of Historic Properties" (36 CFR Part 800).

Once the Indiana FHWA makes its effect determination for this undertaking, please notify the ACHP if you conclude that there is a finding of adverse effect. We will re-evaluate the undertaking once we receive your notification and advise you whether or not we have changed our decision regarding participation in consultation.

ADVISORY COUNCIL ON HISTORIC PRESERVATION

1100 Pennsylvania Avenue NW, Suite 803 • Washington, DC 20004

Phone: 202-606-8503 • Fax: 202-606-8647 • achp@achp.gov • www.achp.gov

Thank you for contacting the ACHP regarding the consultation. Please don't hesitate to contact Najah Duvall-Gabriel at (202) 606-8585 or via email at ngabriel@achp.gov if you have any questions or concerns.

Sincerely,

A handwritten signature in cursive script, appearing to read "Charlene Dwin Vaughn".

Charlene Dwin Vaughn, AICP

Assistant Director

Office of Federal Agency Programs

Federal Permitting, Licensing and Assistance Section



Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



August 1, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Your July 2, 2012 letter responding to our November 7, 2011 letter and to issues discussed at the June 20, 2012 agency meeting in Indianapolis regarding the State Boulevard Reconstruction Project (Des. No. 0400587; Project No. IN20071404; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed your July 2 letter, which we received on July 3, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

Thank you for responding formally to the issues we had raised previously. We especially appreciate the FHWA's encouragement to the City of Fort Wayne and American Structurepoint to investigate the feasibility of preserving some of the houses on the south side of State Boulevard between Terrace Road and Eastbrook Drive and your firm's resulting proposal, on behalf of the City, to attempt to leave in place the houses at 112, 134, and 138 East State Boulevard. We think that if those houses can be avoided by this project, then the remaining segment of the original State Boulevard, as it passes through the Brookview-Irvington Park Historic District, will retain somewhat more of its historic character than it would if all houses on the south side of State Boulevard in that area were to be removed. We have similar hopes for the proposal to leave bypassed curbs and sidewalks in place.

We have no further comments or recommendations at this time for avoidance, minimization, or mitigation of this project's likely effects.

We have received your July 16, 2012 cover letter transmitting the archaeological short report (Stillwell, 7/11/12), which you advised us in your July 2 letter would be forthcoming. We will reply to your July 16 submission in a separate letter.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.
Larry Stillwell, Archaeological Consultants of Ossian, Inc.



Division of Historic Preservation & Archaeology 402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



August 13, 2012

Briana M. Hope
Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana archaeological short report (Stillwell, 7/11/12) of additional areas for the State Boulevard Reconstruction Project (Designation No. 0400587; DHPA No. 5903)

Dear Ms. Hope:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("SHPO") has conducted an analysis of the materials dated July 16, 2012, and received on July 16, 2012, for the above indicated project in the City of Fort Wayne, Allen County, Indiana.

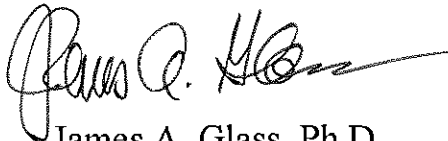
Based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of

Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. If you have questions about buildings or structures please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA No. 5903.

Very truly yours,

A handwritten signature in black ink, appearing to read "James A. Glass".

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:wt

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E., Indiana District, Federal Highway Administration
Patrick A. Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.
Larry Stillwell, Archaeological Consultants of Ossian, Inc.
John L. Carr, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources
Wade T. Tharp, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources



AMERICAN
STRUCTUREPOINT
INC.

August 29, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc
4649 Northwestern Drive
Zionsville, Indiana 46077

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Weintraut:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street in Fort Wayne, Allen County, Indiana. The project is intended to improve corridor connectivity along State Boulevard for both motorists and pedestrians. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c), you were invited to be a consulting party to participate in the Section 106 process via mail with a letter dated March 23, 2009. In response to this invitation or as requested, we have recorded your wish to participate as a consulting party to assess the potential impact of this project on historic properties within or adjacent to the project area.

A Historic Properties Report (HPR) was completed in September 2009 by The Westerly Group, Inc. An Additional Information Report was completed in April 2012 by Weintraut & Associates, Inc. Two historic districts listed in the National Register of Historic Places (NR) were identified within the Area of Potential Effect: Fort Wayne Park and Boulevard System Historic District (NR, 2010) and Brookview-Irvington Park Historic District (NR, 2011). In addition, one historic property was also previously determined eligible for the NR within the Area of Potential Effect: Bridge over Spy Run (NBI No. 0200273).

Enclosed, please find a copy of the August 27, 2012, Federal Highway Administration (FHWA) Findings and Determinations required under procedures established for Section 106 consultation. The approval indicates FHWA concurs with the Area of Potential Effect, the Determination of Properties listed or Eligible for listing in the National Register of Historic Places, and the Determination of Adverse Effect for the proposed State Boulevard Reconstruction Project (DES No. 0400587). Included with this documentation is all correspondence regarding the proposed project and the Section 106 process, including all correspondence since the September 2011 Consulting Parties Meeting.

IN20071404



Linda Weintraut, Ph.D.

August 29, 2012

Page 2

Under the provisions of 36 CFR 800.5(c), the state historic preservation officer and all consulting parties have 30 days from the receipt of an adequately documented FHWA determination to take exception to that determination. You are asked as a consulting party to provide any comments or concerns you have regarding the FHWA Finding of Adverse Effect.

Please provide all comments to:

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Comments must be submitted no later than October 4, 2012. If you do not comment on the Adverse Affect finding by October 4, 2012, your concurrence will be assumed.

In order to initiate the Resolution of Adverse Effects process, we are inviting you to participate in a Consulting Parties Meeting scheduled for:

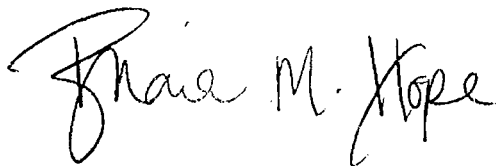
Wednesday, September 19, 2012, at 10 a.m.
Allen County Public Library, Meeting Room A
900 Library Plaza, Fort Wayne, Indiana

The purpose of this meeting will be to discuss Section 106 and potential mitigation measures to offset the Section 106 finding of Adverse Effect. An agenda for the Consulting Parties Meeting will be provided prior to the meeting date. Also enclosed is a copy of the draft Memorandum of Agreement (MOA) to be discussed at the Consulting Parties Meeting. Once the Consulting Parties Meeting is complete and all comments and suggestions have been compiled and taken into consideration, a revised MOA will be distributed to the MOA signatories, as well as provided to the consulting parties for informational purposes.

As you may recall from previous coordination and discussions, FHWA was considering elevating the required National Environmental Policy Act (NEPA) documentation from a Categorical Exclusion Level 4 (CE4) to and Environmental Assessment (EA). Due to the level of public interest associated with this project, FHWA has requested that the environmental document be elevated to an EA. By elevating the proposed project to an EA, a Finding of No Significant Impact (FONSI) will be required to conclude the NEPA process.

We appreciate your cooperation in the development of this project. Please feel free to contact me with any questions or comments you may have. I may be reached by phone at (317) 547-5580 or by email at bhope@structurepoint.com. We look forward to meeting with you on Wednesday, September 19, 2012, at 10 a.m.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandeburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan HanGine, Brookview Civic Neighborhood Association
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, Brookview Civic Neighborhood Association
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette "Jan" Dailey, adjacent property owner
Joyce Newland, FHWA
Larry Heil, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amy Johnson, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT

September 14, 2012

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

To: Ms. Briana M. Hope

Fm: Karl Dietsch, Resident of Brookview Neighborhood

Cc: Shan Gunawardena, City of Fort Wayne

Subject: Safety issue regarding lack of westbound turnoff lane on State Boulevard for Oakridge Road

Thank you for a copy of the memorandum of agreement received recently for the Spy Run to Cass Street, State Boulevard Reconstruction.

Based on the detailed street blueprints enclosed, I have an issue as follows:

1. Westbound traffic on State Boulevard is lacking a separate turn lane for turning right (north) onto Oakridge Road. This is of concern because westbound traffic on State Boulevard is accelerating from the Clinton Street intersection at the same time some traffic is slowing to make a right turn into Oakridge Road.
2. This safety issue can easily be resolved by having a short turn lane as per my attached print.

Regards,

Karl Dietsch, Resident @ 2313 Oakridge Road
Phone: (260) 484-1399

115

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
INDIANA	DEPARTMENT OF TRANSPORTATION	PAYMENT MARKING & SIGNING DETAILS - LINE "PR-A"	

September 17, 2012

To Whom It May Concern,

We as the below listed residents of the Brookview Neighborhood currently reside within the proposed footprint of the E. State Blvd. widening project. We want to take this opportunity to make our voices heard. It seems the only people truly getting an opportunity to voice their opinions are those opposed to the project. We would like to be sure our voices in SUPPORT of the project are not being lost among the more vocal opposition by special interest groups, politicians and Brookview residents whose homes are NOT slated for removal.

Many of us listed on this letter have lived in our residences for many years. We have a very personal interest in this proposed project. Over the years we have seen a substantial increase in traffic along State Blvd. Multiple times throughout the day traffic comes to a standstill from the 300 block of West State Blvd to the 300 block of East State Blvd. This bottleneck makes it nearly impossible to exit our driveways or to enter onto State Blvd from intersecting streets within that area. As the city continues to grow and progress, we feel this issue will only continue to get worse. Those of us on the south side of the 100 block of E. State Blvd also have to contend with large trucks running up off of the curb and driving along our sidewalk / park strips. Because the road is so narrow and the curve is so sharp they do not stay on the paved surface. Listening to the tractor trailers bounce back down onto the pavement has become a constant sound at all hours of the day and night. Add to this the sound of squealing tires as cars try to take the curve at to high a rate of speed and it makes it difficult to enjoy ones time at home.

As residents of this area of State Blvd. and Eastbrook Dr, our quality of life has been greatly affected by the multiple flooding instances that occur yearly in this area. We have to depend on the City of Fort Wayne to place 18"- 36" clay dikes in front of our houses to prevent flood waters from overtaking our residences. These dikes, although necessary... prevent us access to our driveways and property. This leaves us trying to find an alternate location for our vehicles along side streets. Often times our vehicles are stranded in our garages and driveways because we were unlucky enough not get them moved before the thousands of pounds of clay were trucked in. Once the flooding has subsided, we must contend with the mess left behind while the dikes sit and await removal. The issues however are not resolved, even when the clay is taken away. After weeks of the clay sitting in flood water, once removed we are left with massive amounts of mud and debris in the street, our yards and driveways. This flooding halts traffic along one of the cities major east / west streets for days and weeks at a time. This causes an inability of people who live in the area to get to work and take their children to school, not to mention public transportation, school busses and emergency vehicles must also find ways to function when this major artery ceases to be passable. These issues with flooding have, we believe, caused a decrease in our property values and undue financial hardship. Frankly, no one should have to live like this year after year. The fact that the city is trying to address this issue with the State Blvd. widening project gave us hope that a solution was forthcoming. Now... years later it seems this project has become more about personal agendas and political attention then it has about solving these problems. You cannot imagine the stress these delays continue to cause those of us who are most intimately affected. We believe allowing our houses to remain under the guise of historic preservation and community support only makes our problem worse.

The roadway has already been raised for new bridge on Clinton St. south of State Blvd. so it seems counterproductive not to raise the bridge at Spy Run Creek. By delaying or even stopping the State Blvd. project, the lower roadway/bridge on State Blvd. between Westbrook and Eastbrook will only cause flooding in the area to worsen.

It simply does not make sense to not move the road to the south and install a new bridge with the same surface height as the new bridge on Clinton Street. Again, special interest groups, residents outside the footprint for property buyouts and local politicians seems to have gotten so caught up in their own agendas that many of us feel our concerns and support are being drowned out.

We STRONGLY support the buyout of our homes thereby allowing for State Blvd to be relocated and widened to the south of its current location. We also support the removal of the old bridge at Spy Run Creek and the constructing of a new bridge on the relocated State Blvd. We believe that the city / engineers have taken great care with the proposed widening project plans and have the best interest of not only the City of Fort Wayne, but we as the residents who live within the actual footprint of the project. Please do not let those of us desperate for a resolution to these problems continue to be unheard, ignored or worse yet, used as pawns in some sort of political grandstanding.

Please note that of the existing 13 homeowners on Eastbrook and along the south side of E. State Blvd. (between Eastbrook and Clinton), the vast majority want to be listed on this letter as supporting the project and speedy property buyouts. We are NOT in favor of finding ways to retain our homes within the footprint of the project, we feel this will lessen our property values, continue to cause issues with access to our homes and leave the constant flooding issues unresolved.

Again, those of us in favor of the project and the buy out of our homes do not believe that our interests and the interests of the City Of Fort Wayne are being served by delaying or trying to stop this project. We respectfully ask that this project proceed in a timely manner.

Sincerely,

Susan R. Haneline 134 E. State Blvd.

Moshin & Karen Hudda 112 E. State Blvd.

Rex Well, NLI Inc. 128 E. State Blvd.

Lisa Strebig 138 E. State Blvd.

Darrin Klopp 142 and 146 E. State Blvd.

Michael McKinley 2252 Eastbrook Dr.

Peggy S. Roy 2248 Eastbrook Dr.

Joe and Amy Sale 2244 Eastbrook Dr.

Stephanie Mays 2240 Eastbrook Dr.

***Attachments: 3**

- 1. - A photo map with numbers designating homes of persons mentioned in or signing this letter.**
- 2. - 3 pages with signatures or verbal requests to be listed in favor of property buyouts.**
- 3. - Summary and 2 typed pages with property owners information - this corresponds with the above signatures and photo map**



Google earth

© 2012 Google

In summary, 9 out of the 13 property owners are in favor of being bought out. (There are 14 properties shown in this document, but 1 owner has two properties)

Of the 4 remaining properties:

1 property is vacant - no owner information available

1 property is a rental, no landlord contact info. available

1 homeowner does not wish to be bought out

1 home owner was unreachable therefore we are unable to formally list whether they are for or against buyouts.

EASTBROOK DRIVE

2236 Eastbrook Drive

Owner: **KRISTIN A CREAGER & LOIS JEAN LOCKHART**

House 1 on the Map

* Spoke with a male at the residence, he stated he does not wish to sign paper in favor of buyout

2240 Eastbrook Drive

Owner: **ERIKA E MAYS MERRICK & STEPHANIE G MAYS**

260-471-1367 or 260-341-3988

House 2 on the Map

* Signed letter in favor of buyout.

2244 Eastbrook Drive

Owner: **Resource Network Inc Indiana - Joe Sale**

House 3 on the Map

* Signed letter in favor of buyout.

2248 Eastbrook Drive

Owner: **PEGGY S ROY**

260-493-1495

House 4 on the Map

* Signed letter in favor of buyout.

2252 Eastbrook Drive

Owner: **MICHAEL GARY MCKINLEY**

House 5 on the Map

* Spoke with Michael McKinley via telephone on 9/18/12, he made a verbal request to be included in these documents of owners supporting buyouts.

East State Blvd

112 E State Boulevard

Owner: **MOSHIN A HUDDA & KAREN S HUDDA**

260-484-5244

House 6 on Map

* Signed letter in favor of buyout.

128 E State Boulevard

Owner: **NLI INC**

Rex Wells - Verbal signature given on 9/18/2012 via phone

NLI INC / Alliance Property Management

1616 N Harrison St

Fort Wayne, IN 46808 (260) 420-1428

House 7 on Map

** Rental Property,

* Spoke with rep. of NLI INC, owner Rex Wells made a verbal request (via telephone) to be included in these documents of owners supporting buyouts.

134 E State Boulevard

Owner: **SUSAN R HANELINE**

260-482-2200 or 580-2447

House 8 on Map

* Signed letter in favor of buyout.

138 E State Boulevard

Owner: **LISA S STREBIG**

260-760-1956

House 9 on Map

* Signed letter in favor of buyout.

142 E State Boulevard

Owner: **DARRIN D KLOPP**

House 10 on Map

* Verbal request on letter in favor of buyout, contact phone number listed.

146 E State Boulevard

Owner: **DK PROPERTIES LLC**

House 11 on Map

* Verbal request on letter in favor of buyout, contact phone number listed.

154 E State Boulevard

Owner: **NORMA R L/EST PANKOP & PANKOP TRS**

House 12 on Map

This house is a vacant property - no information available on owner

158 E State Boulevard

Owner: **DANIEL J HALL & KAREN S HALL**

**** House 13 on Map**

This is a rental, no landlord contact information available.

162 E State Boulevard

Owner: **ROBERT E DAILEY & JANICE ANNETTE DAILEY**

(260) 483-1303 or 483-6316

**** HOUSE 14 on Map**

This homeowner is a member of the Consulting Parties. Attempts to contact were unsuccessful, so no opinion for or against the buyout is given.

Name of Resident: Moshin & Karen Hudda
Address of Residence: 112 E. State Blvd
Number of people residing in the home: 2 Number on Attached Map 6
Signature: Karen J. Hudda, Moshin A Hudda
Contact Phone Number: 260-484-5244
Additional Comments: _____

Name of Resident: Joe & Amy Solo
Address of Residence: 2244 East Brook Drive
Number of people residing in the home: 4 Number on Attached Map 3
Signature: Joe Solo
Contact Phone Number: _____
Additional Comments: _____

Name of Resident: Stephanie Mays
Address of Residence: 2240 Eastbrook Drive
Number of people residing in the home: 2 Number on Attached Map 2
Signature: Stephanie Mays
Contact Phone Number: 260 471-1367 341-3988
Additional Comments: _____

Name of Resident: Peggy S. Roy
Address of Residence: 2248 Eastbrook Dr.
Number of people residing in the home: 0 Number on Attached Map 4
Signature: Peggy S. Roy
Additional Comments: _____
Contact Phone Number: 493-1495

* Moved due to city's promise of property acquisition.
Have been renting (at a loss) for 6 years. House
has issues at much expense and finding tenants.
is nearly impossible. I consider this to be a hardship.

Name of Resident: Lisa Strebig
Address of Residence: 138 E. State
Number of people residing in the home: _____ Number on Attached Map 9
Signature: Lisa Strebig
Contact Phone Number: 260-760-1956
Additional Comments: _____

Name of Resident: Susan R. Haneline
Address of Residence: 134 E. State Blvd
Number of people residing in the home: 2 Number on Attached Map 8
Signature: Susan R. Haneline
Contact Phone Number: _____
Additional Comments: Author of Attached Letter

Name of Resident: NLI INC.
Address of Residence: 128 E. STATE BLVD
Number of people residing in the home: _____ Number on Attached Map _____
Signature: Verbal Request By Rex Wells of NLI INC to be added
Additional Comments: _____
Contact Phone Number: 420-1428
Contacted NLI 10/16 N. Limerick FFWAYE INC.
Rep Rex Wells wants to be added to this document.
Request via telephone @ 9/18/2012 - may call him to verify if needed

Name of Resident: Michael McKinley
Address of Residence: 2252 Eastbrook
Number of people residing in the home: _____ Number on Attached Map 5
Signature: Verbal request by phone to show supporting buyout
Contact Phone Number: 443-3219
Additional Comments: I spoke with Mr McKinley he wished
to be listed on the residents in favor of a buyout
concentration on 9/18/12 @ 9pm

Name of Resident: DARIN KLOPP
Address of Residence: 142 E - STATE BLVD.
Number of people residing in the home: _____ Number on Attached Map 11 + 10
Signature: Verbal Request by phone to show supporting Buy out
Contact Phone Number: 249-7606
Additional Comments: _____



bethany w <bethany@weintrautinc.com>

Fwd: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

1 message

Linda Weintraut <linda@weintrautinc.com>

Fri, Sep 21, 2012 at 8:42 AM

To: bethany w <bethany@weintrautinc.com>

----- Forwarded message -----

From: **Hope, Briana** <bhope@structurepoint.com>

Date: Tue, Sep 18, 2012 at 4:12 PM

Subject: State Blvd. Reconstruction Project - SHPO requested minimization evaluation

To: "Hope, Briana" <bhope@structurepoint.com>, "mgalbraith@archfw.org" <mgalbraith@archfw.org>, "aquinn@archfw.org" <aquinn@archfw.org>, "don.orban@cityoffortwayne.org" <don.orban@cityoffortwayne.org>, "tzeiger@indianalandmarks.org" <tzeiger@indianalandmarks.org>, "mbwedaman@frontier.com" <mbwedaman@frontier.com>, "juliemarie57@earthlink.net" <juliemarie57@earthlink.net>, "jlcooper@ccrtc.com" <jlcooper@ccrtc.com>, "indianabridges@sbcglobal.net" <indianabridges@sbcglobal.net>, "shan.gunawardena@cityoffortwayne.org" <shan.gunawardena@cityoffortwayne.org>, "dan.avery@co.allen.in.us" <dan.avery@co.allen.in.us>, "sjslick@mac.com" <sjslick@mac.com>, "jandailey59@msn.com" <jandailey59@msn.com>, "joyce.newland@fhwa.dot.gov" <joyce.newland@fhwa.dot.gov>, "larry.heil@fhwa.dot.gov" <larry.heil@fhwa.dot.gov>, "jshoaff@proparkwest.com" <jshoaff@proparkwest.com>, "jasonkaiser@indot.in.gov" <jasonkaiser@indot.in.gov>, "pacarpenter@indot.in.gov" <pacarpenter@indot.in.gov>, "mkennedy@indot.in.gov" <mkennedy@indot.in.gov>, "linda@weintrautinc.com" <linda@weintrautinc.com>, "creager.smith@cityoffortwayne.org" <creager.smith@cityoffortwayne.org>, "albertcohan@aol.com" <albertcohan@aol.com>, "tmn@barrettllaw.com" <tmn@barrettllaw.com>, "rross@martin-riley.com" <rross@martin-riley.com>, "dan@earthsourceinc.net" <dan@earthsourceinc.net>, "jglass@dnr.in.gov" <jglass@dnr.in.gov>, "jcarr@dnr.in.gov" <jcarr@dnr.in.gov>, "kdietsch@comcast.net" <kdietsch@comcast.net>, "alec.johnson@ci.ft-wayne.in.us" <alec.johnson@ci.ft-wayne.in.us>, "Crites, Scott" <SCrites@structurepoint.com>, "Zielinski, Rich" <RZielinski@structurepoint.com>, "gsmith2@indot.in.gov" <gsmith2@indot.in.gov>, "aricketts@dnr.in.gov" <aricketts@dnr.in.gov>, "wtharp1@dnr.in.gov" <wtharp1@dnr.in.gov>, "tom.cain@cityoffortwayne.org" <tom.cain@cityoffortwayne.org>

Consulting Party Members –

Attached to this e-mail is a copy of the letter sent to the SHPO's Office documenting the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

A hardcopy of the this letter has been placed in the mail. In addition, the information presented in the letter will be discussed as part of tomorrows (Sept. 19th) Consulting Parties Meeting.

Thank you,

Briana

Briana M. Hope

Project Manager, Environmental Sciences Group

7260 Shadeland Station, Indianapolis, Indiana 46256

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F 317.543.0270 W www.structurepoint.com



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IN20071404.EV.2012-09-18.LTR.ROW Minimization.bmh - Copy.pdf

400K



AMERICAN
STRUCTUREPOINT
INC.

September 18, 2012

James A. Glass, PhD
Deputy State Historic Preservation Officer
Division of Historic Preservation & Archaeology
Indiana Department of Natural Resources
402 West Washington Street, W274
Indianapolis, Indiana 46204

Re: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Dr. Glass:

The purpose of this letter is to document the efforts made to evaluate potential options to minimize impacts to the houses located at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. American Structurepoint was asked to evaluate the possibility of modifying the proposed Oakridge Road extension to minimize the number of total parcel acquisitions between existing State Boulevard and proposed State Boulevard.

After completion of the proposed project, the three existing structures would be located between the existing and proposed State Boulevard roadways. Due to the location of the structures, investigations were needed to assess the impacts to the properties and feasibility of maintaining the existing structures between the existing and proposed roadways.

Minimization of Property Acquisition

In order to minimize acquisition of property associated with these homes, American Structurepoint, Inc., investigated options that evaluated modifications to the Oakridge Road extension to proposed State Boulevard. The first included shortening the right-turn lane and eliminating the landscaped median. This modification provided a reduction in anticipated right-of-way of approximately six feet. Constructing sidewalks adjacent to the curb with retaining wall placed at the back of sidewalks also reduced the anticipated right-of-way by an additional 16 feet.

The use of guardrail was also evaluated in an effort to minimize potential right-of-way acquisition. Currently, the proposed design utilizes the required 4:1 side slope from the proposed State Boulevard roadway, encroaching on the back yards of the homes in question. The roadway through

IN20071404

this area is elevated due to the need of the proposed roadway to tie into a larger bridge required to span the floodplain of the Saint Mary's River. To utilize guardrail would allow for the construction of a 3:1 side slope, resulting in a minor reduction of required right-of-way, but would ultimately be offset by the requirement of a flat shelf needed for installation of the guardrail at the top of the slope immediately adjacent to the roadway. In addition, utilization of guardrail would pose a safety concern for vehicles making a left-hand movement from Oakridge Drive to proposed State Boulevard by introducing a visual obstruction to the west.

Stormwater Drainage

In order to facilitate stormwater drainage adjacent to the homes, a drainage swale will be necessary in all options. The drainage swale will be constructed at the bottom of the proposed roadway fill slope just south of the houses in question. The drainage swale will collect stormwater runoff from both the proposed roadway and the adjacent properties located to the north. The flow will be conveyed west outletting into Spy Run. Currently, the stormwater drainage system is proposed as a vegetated drainage swale.

The option of an enclosed drainage system utilizing inlets was evaluated in this area, but would still require a vegetated swale above the enclosed system to direct water to the inlets. The enclosed drainage system did not provide a reduction of anticipated right-of-way.

Please see attached exhibits showing the anticipated impacts resulting from the proposed construction/right-of-way limits. The distance between each residence and the proposed construction limits/right-of-way is marked, as well as the anticipated elevation differences. The most significant elevation difference of seven feet exists at 112 East State Boulevard and decreases to three feet near 138 East State Boulevard.

Recommendation

The evaluation found that the properties would be significantly impacted by construction of the proposed roadway and drainage swale.

- 112 East State Boulevard – The property would be located approximately 7.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately eight feet from the existing residence. Approximately 62 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.
- 134 East State Boulevard – The property would be located approximately 3.5 feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately ten feet from the existing residence. Approximately 55 percent of the existing backyard/greenspace between the house and the southern existing property line would still need to be acquired for construction of the roadway, sidewalks, and drainage swale. The storage building and significant portion of the existing yard/greenspace would be included in the area to be acquired.

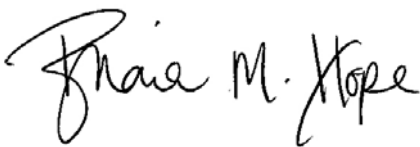
- 138 East State Boulevard – The property would be located approximately three feet below the elevation of the proposed State Boulevard roadway and sidewalk. The proposed roadway construction limits and right-of-way would be located approximately three feet from the existing residence. Approximately 77 percent of the existing backyard/greenspace between the residence and the southern existing property line would still need to be acquired for construction of the roadway, sidewalk, and drainage swale. The existing garage, a portion of the driveway, and significant portion of the existing yard/greenspace would be included in the area to be acquired.

It is the opinion of the designer that the minimization efforts evaluated do not result in a significant reduction of property impact. Therefore, the parcels in question should remain as complete parcel acquisitions. The significant reduction in greenspace between the existing residence and proposed roadway, impacts to existing drives, and removal of non-residential structures located on the properties is appropriate justification for the complete acquisition of the parcels in question.

In additional coordination with the Indiana Department of Transportation, the Right-of-Way and Land Acquisition Section advised American Structurepoint representatives that as part of state and federal law, land cannot be purchased from one property owner and given to another to offset the amount of land being acquired. Therefore, the impacts to one parcel cannot be offset by acquiring an adjacent property and giving or selling that acquired property to an adjacent owner. The land acquired from the parcels in question would result in a significant permanent reduction of property and green space.

We hope that the information in this letter adequately resolves the requested investigation of options to minimize impacts at 112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard. This letter is intended for informational purposes only. A copy will be sent to all consulting parties concurrently with this correspondence. If you have any additional questions, please do not hesitate to contact me at (317) 547-5580, or at bhope@structurepoint.com.

Very truly yours,
American Structurepoint, Inc.



Briana M. Hope
Environmental Project Manager

BMH:mgn

Enclosures

Distribution List

Dr. James A. Glass, IDNR
Angie Quinn, ARCH
Jill Downs, ARCH
Michael Galbraith, ARCH
Don Orban, Fort Wayne Historic Preservation Commission
Todd Zeiger, Indiana Landmarks
Julie Donnell, Friends of the Parks of Allen County
Michelle Briggs-Wedaman, Brookview Neighborhood Association
Dr. James L. Cooper
Paul Brandenburg, Indiana Historic Spans Task Force
Shan Gunawardena, City of Fort Wayne
Susan Haneline, adjacent property owner
Charlie Shirmeyer, Northside Galleries
Karl Dietsch, adjacent property owner
Dan Avery, NIRCC
Suzanne Slick, Irvington park Neighborhood Association
Annette “Jan” Dailey, adjacent property owner
Joyce Newland, FHWA
John Shoaff, Fort Wayne City Council
Jason Kaiser, INDOT
Patrick Carpenter, INDOT
Mary Kennedy, INDOT
Creager Smith, City of Fort Wayne
Albert Cohen, Westbrook 5, LLC
Thomas M. Niezer, Barrett & McNagny, LLP
Ronald Ross, Marin Riley Architects and Engineers
Dan Ernst, Earth Source, Inc.
John Carr, IDNR
Amanda Ricketts, IDNR
Wade Tharp, IDNR
Tom Cain, City of Fort Wayne
Linda Weintraut, Weintraut & Associates
Najah Duvall-Gabriel, ACHP
Greg Smith, INDOT





MEETING MINUTES

Location: Allen County Public Library, 900 Library Plaza, Meeting Room A, Fort Wayne, Indiana

Date: September 19, 2012

Project Name: State Boulevard Reconstruction (Des. No. 0400587)

Project No.: IN20071404

Consulting Party Rich Zielinski, Scott Crites, Briana Hope, Chris Meador (American Structurepoint, Inc.)

Attendees: Shan Gunawardena, Creager Smith, Tom Cain (City of Fort Wayne)
Dr. Linda Weintraut (Weintraut & Associates)
Dr. James Glass, John Carr (IDNR, Division of Historic Preservation and Archaeology)
Patrick Carpenter (INDOT, Cultural Resources)
Jason Kaiser, Greg Smith (INDOT Fort Wayne District)
Joyce Newland, Larry Heil (Federal Highway Administration)
John Shoaff (Fort Wayne City Council)
Suzanne Slick (Irvington Park Neighborhood)
Dan Avery (Northeastern Indiana Regional Coordinating Council)
Michael Galbraith, Jill Downs (ARCH, Inc.)
Michelle Briggs-Wedaman (Brookview Neighborhood Association)
Susan Haneline, Karl Dietsch (Adjacent Property Owner)
Todd Zeiger, Catherine Wright (Indiana Landmarks)
Edward Welling (Friends of the Parks of Allen County)
Dan Ernst (Earth Source, Inc.)

Conference Line: Najah Duvall-Gabriel (Advisory Council on Historic Preservation)

Attendees Frank Saurez (City Public Works), Marty Bender (FWPD/City Council), Shawna
Observing Nicelley, Larraine Weier, Herb Weier, Thomas Roach III, Sarah Krugen Geyman
Meeting:

1. The meeting was held at 10:00 a.m., September 19, 2012, to discuss the following agenda items:
 - 1) Section 106 Update
 - 2) Section 106 Action Items regarding Adverse Effect Finding
 - 3) Additional Mitigation Measures
2. Briana Hope introduced herself and began the meeting with introductions around the room and by stepping through the first item on the agenda, including an update on project progress since the last consulting party meeting (09/2011).
3. Dr. Linda Weintraut presented a PowerPoint presentation briefly recapping the Section 106 process, including identified properties, minimization and avoidance measures, effects, and potential mitigation measures.

4. Susan Haneline asked during the avoidance and minimization portion of the PowerPoint presentation why the owners of the 3 residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes, as they do not want to remain. She thought the property owners should have a say as to whether or not the impact to their property is significant enough to justify leaving the house in place.
 - Larry Heil responded that Section 4(f) requires evaluation of measures to avoid and minimize impacts to the historic district. The evaluation is focused on the historic resources protected by the law. Any time right-of-way is acquired the property owner is reimbursed for the impact to the property, but FHWA is required by law to minimize impacts to the historic district. If there is a way to preserve contributing resources, which the three homes in questions are, FHWA is required to preserve these resources.
5. Following the PowerPoint presentation, Briana Hope opened up the meeting for general discussion regarding the potential mitigation measures proposed and any additional ideas regarding potential mitigation.
6. Michelle Briggs Wedaman stated that she will be providing extensive written comments related to the materials sent with the invitation to the consulting parties meeting, but generally had the following comments:
 - Related to the agenda, feels it is inappropriate to have moved beyond the discussion of purpose and need.
 - Understand timelines and agency requirements but feels the larger issues of real time and real impact on the community and are not guided by the timelines that dictate quick movement on the project but by the guiding principles of the Plan-It Allen report, the comprehensive plan for Allen County. She will be commenting on the relevance of the document in the submitted comments.
 - Context Sensitive Solutions (CSS) – CSS should be applied to the larger issue of the road footprint itself and should occur at the beginning of the project not towards the end of the project or the last stage of a project.
 - The neighborhood supports Sue Haneline and the most directly impacted residents. The neighborhood was not invited to the agency meeting in June and not included in the decisions that were made at the meeting potentially impacting the Oakridge extension and the effort being made to preserve the homes.
 - Encouraged agencies and project sponsors to consider what the final project will look like and to reconsider the outcome of the project and forcing residents to stay.
7. Todd Zeiger had the following comments:
 - Encourages the Advisory Council of Historic Preservation (ACHP) to be involved now and moving forward.
 - Concurs with “adverse effect” but feels it is not complete. Concerned about bifurcation of the district by creating a visual dissection of the neighborhood and district both height-wise and width-wise. The bifurcation needs to be discussed in the effects.
 - Dr. Linda Weintraut stated that the effects document will be updated to include the bifurcation of the historic district.
 - Feels that impacts to individual resources have been minimized by the City of Fort Wayne in what he believes is anticipatory demolition as part of a flood control project. The individual demolitions are directly related to the project because one of the purposes of the project is stated as flood control issues. This is not discussed in official findings and should be.

- Will be responding in writing by October 4th and encourages that before ACHP makes its formal decision that the written comments are included and considered and not just the conversation from the consulting parties meeting.
 - In the letter announcing the consulting parties meeting it states that FHWA is elevating the project to full Environmental Assessment (EA). Questioned why mitigation was being discussed when the full impacts that are going to be discussed in the EA are unknown.
 - Larry Heil responded that when a project has some sensitivity to it FHWA prefers to issue a definitive decision and a Categorical Exclusion (CE) by definition is a type of project excluded from the requirement of NEPA documentation. Because of the sensitivity and public concerns, FHWA wanted to have an official FHWA NEPA decision. The next steps in the EA process include finalizing a Draft 4(f) Analysis, revising the Draft MOA, and finalizing data collection to compile a Draft EA Document to present to the public. Once the Draft EA is released for public involvement, a public hearing will be held and the public will be encouraged to comment and present facts or clarify that facts are inaccurate from a technical standpoint.
8. Karl Dietsch pointed out a safety concern regarding traffic traveling west on new State Boulevard. Traffic will be picking up speed at Oakridge as it is heading west at the same time traffic making a right turn on Oakridge will be slowing down, thus increasing the risk of rear-end accidents. A short right turn lane was suggested for westbound traffic along State at Oakridge Road.
 9. Tom Cain pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in those landscape elements in the documentation need to be discussed. The visual and spatial components of the larger landscape need to be understood so they can be addressed in a mitigation discussion.
 10. Michael Galbraith also stated that he would be providing detailed comments by October 4th and had the following comments:
 - Discussion of mitigation and minimization is a red herring to avoid discussing the issues that have been brought-up and not discussed by the consulting parties.
 - Minimization efforts documented and sent out via e-mail late the evening prior to the consulting parties meeting did not give consulting party members adequate time for review.
 - Larry Heil stated that the letter did not represent FHWA's position.
 - Raised concern about a new Section 106 consultant with brand new information and being able to adjust to Dr. Weintraut as a consultant.
 - Based on the PowerPoint presentation and the May 19th letter from American Structurepoint to Dr. Glass, Mr. Galbraith feels the APE is still too small for the project.
 - Encourages ACHP to consider that this project separates and segregates projects happening in the same geographical location and same time period impacting the same neighborhood and separates them based on agencies. They should be considered amalgamated for review due to their cumulative impacts on the neighborhood. A total of eight federal aid projects are happening in the same area at the same time.
 11. Briana Hope reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed that the consulting parties meeting was an opportunity to verbally express their mitigation comments and ideas.
 12. Michelle Briggs Wedaman expressed the following additional comments:
 - Has not received a traffic study for the area that has been requested since the beginning of the project. How is the need for this project (safety and flooding) documented without a traffic

study? Have issues with the safety and congestion part of the Purpose and Need, specifically related to accidents at intersections, been studied? How will this project calm traffic?

- Larry Heil indicated that FHWA provides funds to help Major Metropolitan Planning Organizations (MPOs) develop reliable traffic models. FHWA reviews the traffic models every four years to evaluate the models. FHWA has a high degree of confidence in the models and utilizes these models around the country to aid in project development.
- 2005 Flood Study is at odds with the project and in no way points to this project as a solution to real life flash flooding.

13. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation. She expressed her concern that the consulting party members were losing the opportunity to have input related to mitigation.

14. John Shoaff commented that he believes the purpose of the proposed project is to make a major arterial out of a street that runs through a number of neighborhoods. Arterials do not have a good safety record related to pedestrians. Mr. Shoaff also made the following comments:

- Presented a letter signed by 15 neighborhoods opposing the project and stated that the letter would be included as part of his comments submitted by October 4th.
- Stated that the majority of the public does not agree with the Purpose and Need Statement.
- The historical aspects of the neighborhood are great and will be destroyed by this project.
- Concerned about the project having a negative impact on property values.

15. Michael Galbraith reiterated his concern regarding the multiple federal projects involved in the neighborhood that are not be looked at and evaluated cumulatively.

16. Edward Welling agrees with the adverse effect but feels that the discussion of mitigation is premature and that the proposed MOA is an attempt to confuse the process, especially since the FHWA elevated the project to an EA. Mr. Welling also had the following comments:

- Asserted that the APE is not appropriate.
- Turning the roadway into a major arterial and the addition of traffic will impact the quality of life along the corridor.
- Suggested that the discussion of mitigation should be postponed until EA is complete.

17. Susan Haneline commented that 14 houses in the initial footprint were under the impression that the project was not just about traffic flow but also related to flooding. She also had the following comments:

- Every house in the footprint of the project has been impacted by flooding.
- Presented a letter documenting the vast majority of owners in the footprint are requesting a buyout due to loss of property, traffic flow issues, inability to access property, flooding, and financial hardships.
- Feels that being listed in a historic district is making it harder for the city and state to address the concerns of the people in the footprint of the proposed project.
- The majority of the home owners in the footprint of the project were grateful for the opportunity presented by this project to vacate their properties, open up a green space, and retain the original footprint of State Boulevard, but dealing with an elevated roadway just for the city to protect contributing properties is not a long-term solution.
- Does not oppose project and looks at it as an opportunity to not continue to lose value in properties.

- Requested agencies and project sponsors to not forget to put a face with the people that are in the footprint and intimately dealing with the flooding issues and not being able to get out of their driveway because of traffic.
18. Dr. Linda Weintraut again reiterated the purpose of the meeting is to discuss potential mitigation measures and asked if anyone had comments related to mitigation.
19. Tom Cain commented that preservation of historic buildings depends of the viability of the properties to remain invested in, in the future. He had the following supporting comments:
- Homes that are in a floodplain have limits on how much can be invested in them, their future preservation may not be ensured.
 - The Section 106 process may preserve buildings but it may also create a condition unattainable for preservation and economic life and existence as a structure.
 - The process of preserving specific structures may impact the need to provide mitigation for the larger landscape and planning characteristics of the neighborhood.
 - The preservation of several specific structures may not ensure long term preservation and limit opportunities to mitigate larger landscape design issues which are the more significant components of the neighborhood based on the Arthur Shurcliff plan for the area.
 - Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. The two designers involved, George Kessler and Shurcliff, had differing approaches to composition of roadways, one was a more formal straightforward boulevard with setbacks, and tree lined streets and the other a more curvilinear pattern.
20. Dr. Linda Weintraut commented that the National Historic Preservation Act was established so that historic properties and modern undertakings could exist in harmony; one was not to be at the expense of the other. We are trying to come up with a compromise that allows both to move forward. Dr. Weintraut again encouraged the consulting parties to think about mitigation and how that might offset the adverse impacts of the undertaking.
21. Michael Galbraith commented that he agrees that the Section 106 process was designed to allow historic properties and transportation projects to live in harmony, but what is happening to the neighborhood is not harmonious. This neighborhood is being destroyed by a combination of federal projects that are being executed piecemeal that have destroyed dozens of houses. If the project proceeds it will destroy dozens more, property values, and the historic resource in the process.
22. Dr. James Glass commented that he recalled at previous consulting party meetings some very strong opinions were voiced similar to the ones heard today and that there still does not seem to be a degree of consensus from the community about the project. He stated that presumably the City of Fort Wayne very strongly supports the project, but also heard John Shoaff (City Council Member) express a very different point of view, as well as differing points of view from the neighborhood. He also had the following comments:
- One issue the community needs to consider is: can a consensus be developed on this project. From a section 106 perspective, the State Historic Preservation Office (SHPO) depends on the project sponsor and the community to develop some kind of consensus on the need of the project and whether all the issues in terms of feasibility have been worked out. SHPO then takes that as a starting point for considering effects on historic properties and ways to mitigate and lessen adverse effects.
 - In June, SHPO had a very preliminary meeting with the agencies to consider what the anticipated project design was based on the safety factors and consideration of alternatives. The meeting

was to provide a basis for a starting point at the consulting parties meeting to try to begin a discussion on mitigation to offset some major adverse effects.

- The meeting today is an invitation for consulting party members to provide additional mitigation ideas or alternative ideas.
- The letter yesterday was sort of a wildcard that no one anticipated. We were waiting for this discussion to bring forth the idea on the terms of the 3 houses and the many significant adverse effects. We would welcome any additional ideas for mitigation.

23. Larry Heil commented that it is critical to point out how federal funds are spent in this area. The decisions are made by the Northeastern Indiana Regional Coordinating Council (NIRCC) Policy Board. There is a reason that MPOs are established and required by federal law. These are regional issues and it is within that context that the regional plan that they identify what the arterials are in the region. State Boulevard is and has historically been an arterial. The plan that is developed and adopted by all the officials from the entire region is what guides all investment in the area. None of the decisions are made purely by City of Fort Wayne staff or one or two elected officials. They are made by the policy board which is made up of a group of elected officials so there is a regional perspective.

24. Dr. Linda Weintraut again asked for mitigation measures that consulting party members may wish to put forward during this opportunity of the Section 106 process. She stated that this is the chance to offer mitigation. Dr. Weintraut explained that the agencies and project sponsors are looking for ideas to offset the impacts such as educational programming, CSS, or any other ideas that the community could put forward. This is the consulting party's opportunity in the process to be heard on this issue.

- Dr. John Carr added the request for any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion.

25. Tom Cain commented that the discussion that has occurred for most the meeting, has taken away from the opportunity to talk about anything based on what is in front of us. He stated that he has a fairly lengthy list of observations and suggestions that he didn't feel he had enough time to discuss.

- Larry Heil requested the Tom Cain provide the comments and suggestions in writing and ensured him that FHWA would review them and take them into consideration in developing the MOA. He also reiterated that the purpose of the meeting was to talk about potential mitigation features and the historical elements that can be preserved.

26. Susan Haneline suggested that the State Boulevard curve be considered. The curve will remain and is not being lost, but as a resident of that particular footprint feels nothing is being done to showcase the feature. By allowing homes to remain in the footprint that are being vacated due to flooding and traffic problems, the curve will not be showcased. The homes that are not retaining value or are specific structures of historic significance should be removed. Ms. Haneline said that there are twenty other houses similar in style to the ones in the footprint found within an equal area [of the historic district]. The significance is simply the footprint of the area. By retaining the existing structures, nothing is being done to showcase the beauty of the Brookview Neighborhood or the feeling that the people that live there would like to have. Removing the negative aspect of leaving property owners that do not wish to remain and finding a way to showcase the features that everyone is feels are so important should be a focus of mitigation.

27. Michael Galbraith commented that the call for the five-minute mitigation measure is inappropriate. He feels the scheduling of the meeting in such a hurried fashion before the consulting parties are allowed a full opportunity to comment on the proposed mitigation measures in writing is inappropriate. People deserve an opportunity to review what has been presented to them and an opportunity to comment in writing if they do not feel comfortable arguing in front of 20 to 30 people.

- Jason Kaiser commented that what was presented were ideas for minimization efforts and that the agencies, project sponsors, and representatives are soliciting additional ideas. An effort was

made to give some ideas for minimization and mitigation to help spur further discussion among the consulting party members.

- Patrick Carpenter stated that the rationale for having the current meeting was to have it in the middle of the comment period. He stated that this does not foreclose comments or discussion but provides an opportunity for consulting parties to hear what other people have to say. This opportunity was provided to avoid comments going into a vacuum and allow consulting party members to get an idea of what everyone is thinking. The meeting was also intended to allow consulting party members to hear comments and ideas and be encouraged to formulate new ideas and put those into your comments that are due October 4th. This is an opportunity for people to provide input and not foreclose anything it helps encourage people to get involved.

28. Dr. Linda Weintraut stated that the consulting party members have until October 4, 2012 to submit written comments and encouraged everyone to comment looking at the minimization efforts, mitigation ideas, and thus consider how to creatively mitigate for the adverse effects. Your role as a consulting party member is to offer up ideas regarding mitigation regarding historical properties.

29. Michelle Briggs Wedaman commented that she is here as a representative of the neighborhood but also carrying a letter representing over 11,000 households and businesses. A lot of the comments that have been heard today speak to the public process. Ms. Wedaman said that she feels the public process has not been followed. Commented that for the last 4 plus years we have been involved in this process and the community has been asking the same questions in looking at this complex project. No roadmap has been provided since August 6, 2008 when we started working with the department of public works. The CSS approach process has been designed for sorting out these types of complex issues. We know that both the State and FHWA join us in wanting to spend the \$11 million dollars in a meaningful way that will benefit the community, but we ask you again to really listen to the questions we have, including why other public offices are not represented.

- Where are our policy officers, economic development officers, and historic preservation officer? Why have these and other officers not been allowed at the central planning table for this project?
- Why is the City going ahead with a project that is clearly out of line with the goals of this community, except just a few residents?

We stand behind the residents of State Boulevard in their plea not to be left next to a highway. To do this in the name of historic preservation makes it even more disturbing to us. Please join us in understanding why we are asking these questions.

30. Karl Dietsch commented in regards to the adverse effects that he sees many more positive aspects to this alternative than adverse effects. He will be able to turn onto State Boulevard safely as compared to now due to visual obstructions. He will also be able to always travel west where before he could not due to road closures as a result of flooding.

31. Patrick Carpenter requested that any comments received be included on the City's website.

- Michael Galbraith requested that the link be re-emailed to all the consulting parties.

32. Briana Hope asked for any closing statement from FHWA and SHPO, thanked everyone for his or her participation, and adjourned the meeting.

ACTION ITEMS

- American Structurepoint will update the online archive for the project's Section 106 correspondence and documents and provide the e-mail address to all consulting parties via e-mail.
- Weintraut and Associates and American Structurepoint will update the effects documentation as needed.

The minutes of this meeting as described above represent the writer's interpretation of the discussions of the meeting. If your interpretation differs substantially, or if there are items that were overlooked, please contact me at (317) 547-5580 or bhope@structurepoint.com to revise the record.

Very truly yours,
American Structurepoint, Inc.

Briana M. Hope
Environmental Project Manager

BMH:

Enclosures

August 21, 2012

Ms. Brianna M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

Sara Kruger Geyman
1827 Forest Park Blvd.
Fort Wayne, IN 46805

Re: Consulting Party/Mitigation Meeting, August 19, 2012

Ms. Hope,

I am so pleased that I was able to attend this meeting; it was quite informative. It was interesting to see first hand how our government works.



Before stating my comments, I would like to first point out that as a resident, I, along with others were uninvited to this meeting; a meeting of *consulting parties*. My first concern is that residents are not and have not been consulted in this matter. And the refused entry of said residents at this meeting only adds to the concern that plans are made behind closed doors – even against the wishes of the taxpaying citizens. Thankfully, due to our councilman, some members of this meeting acquiesced and we did attend.

Secondly, the grossly unprofessional behavior of Dr. Weintraut of Weintraut and Associates was appalling. The sighs and eye rolls that Dr. Weintraut displayed clearly showed her disdain for community feedback, and was very disrespectful.

Comments, as requested, regarding the issues of this plan are as follows:

- Residents were never, and are still not being included, making this faulty from the start.
- The Purpose and Need for this project has been decided and laid out in an **undemocratic way**. The purpose and need is still in question. The city has failed to demonstrate how this project will resolve issues. Residents concerns have had no bearing on Public Works, as they adhere to their plan and plow through historical homes, neighborhoods, and lives.

flooding has yet to be addressed. Failing to understand that and continuing with this plan is equal to giving a patient an aspirin to cure their cancer.

- **Mitigation is premature** in a plan and a process that has been faulty from the beginning. It is a **proverbial lollipop** stuck in the hands of residents to quiet them down and distract them from the truth.
- Albeit premature, the suggested mitigation measures were offensive:
 - “Save” three houses (but add an elevated 5 lane highway.) – Lollipop
 - “Take a picture of the bridge” (but destroy the land and neighborhood in which it once stood) – Lollipop
 - “Educational Measures” – (Is this where we tell future generations of the beautiful thriving neighborhoods of Brookview, Northside and Forest Park Blvd. that once upon a time existed but now is a highway?) – Poison lollipop
- Per the Federal Department Representative attending.. *“Addressing speed is not related to this project.”* It should be, because that is what is being invited. It seems the right hand knows not what the left hand is doing.
- This plan does not solve problems, it creates more of them. Destruction, speeding, urban blight, and the need for more of the same as it invites more of the same.
- This project is not a small isolated proposal, but a disastrous massive plan with immeasurable impact on property (both physical and fiscal), lives, entire neighborhoods, ultimately the city itself. Fort Wayne Public Works seem bent on cutting off its nose to spite its face.
- The stated “safety and congestion” solution that this plan claims, are two entirely different things. ‘Safe’ is slow and minimal. To add an element of safety one should consider **ADDING** curves, reducing the number of vehicles. This plan will add **DANGER** not safety. 
- Congestion should be relieved by sending traffic to the outlying arteries – Coliseum Blvd., Hwy 469 –for which they were created. Inner city streets should be meant for inner city living and working, and playing.
-  Arterials **DO NOT** offer safety (per a city study), but rather an increased rate and higher number of accidents.
- It's been pointed out that many offices have **NOT** been included in the making of plans (i.e., The Housing Dept.) Where is the ;

Traffic study? – This has been requested, but not heeded

Environmental impact study?

Cultural impact study?

Housing study?


Death Study Long term effects of highways *and heart attacks* through neighborhoods study? *Per Harvard*

Design professional?

Urban Planning?

My suggestions:

- This massive and poorly managed project should be **halted immediately** and any plans to modify State Boulevard should begin anew, with a publicly transparent planning process which involves residents and resident neighborhood leaders, elected representatives, urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.
- Keep the well designed traffic calming curves and force commercial and lateral traffic to the arteries north – for which they were created.
- Think progressively! Look to the success of other downtown areas. Invite people back in to the urban areas by **CREATING MORE** desirable areas. Discourage speed and urban sprawl, encourage density in our urban core. (Per Pam Holocher, Ft Wayne deputy director of community development.) Narrow the streets, concentrate use, beautify, slow down. Invite a quality of life, and you invite quality people to populate downtown.
- Finally, I would suggest that Dr. Weintraut not be present in future meetings and be replaced by a respectful professional.

Most Sincerely


Susan R. Haneline
134 E. State Blvd
Fort Wayne, IN 46805
Brookview Neighborhood Home Owner

10/01/2012

To Whom It May Concern,

There has been a lot of talk about the historic significance of the Brookview Neighborhood. Its winding roads, park like settings and flowing creek certainly make it sound like the ideal place to live. I have seen this reflected in pictures of the neighborhood. Taken with such care, on a perfect Spring day... the area seems to shine with fairytale beauty. What those pictures do not reflect is many of the real issues in the neighborhood, the reality of the OTHER part of the neighborhood. I have attached 19 pictures of what MY part of the neighborhood looks like when it floods. This is the part of the neighborhood in the actual footprint of the E. State Blvd. road/bridge project. Please remember, as you look at these pictures, the flooding incidents you are seeing are not the exception any more, they are the norm. Some years we have had multiple instances of flooding lasting weeks at a time. As you can see, 3 - 4 foot clay dikes and dirty, contaminated flood water does not exactly project fairytale beauty.

In my part of the neighborhood, the increase in rental properties and vacant homes tell the story of just how many people are leaving the area because of the issues with traffic and flooding. The cries to maintain the neighborhoods historic significance seem to have lost sight of whether it is actually prudent to save something just for the sake of history. The crumbling bridge between Eastbrook and Westbrook Drives is a case in point. It is too narrow, it sits too low and it is in terrible disrepair. Each time the water in the creek rises to several feet below street level, the bridge acts like a damn, preventing the water from flowing underneath. It forces massive amounts of contaminated water to spill out onto our properties and close our driveways and roads. This is a case where practical is getting lost in the desire to maintain or retain something that is simply broken and outdated.

These problems do nothing to showcase what IS historical about the neighborhood. It is time to honestly address these problems and move forward with the road/bridge project. We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don't have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood. We certainly do not showcase our humanity when we force homeowners to keep properties that are neither safe, desirable or retain their monetary value. This is not a case of big government trying to run rough shod over the

little guy. This is a case of the little guy, begging government to step in and solve a problem, to do the right thing. Please allow us to be gracefully be removed from the burden of properties that have become undesirable places to live. I again ask you to take a hard and honest look at the attached photos. Can you, in good conscious, really retain our homes in the footprint of this project knowing that the issue of flooding and traffic will not be resolved? If you owned property here, would you not be begging for someone to step in and solve the problem? Brookview Neighborhood was designed to have a park life feel, why not allow the homeowners to be bought out and then, as proposed, turn all that land into green space? Give the neighborhood a true entrance, showcase its unique feel through a thoughtful use of lighting, plaques, hardscape and monuments. I am not saying we should not honor our history, but there is a way to honor history without sacrificing homeowners quality of life.

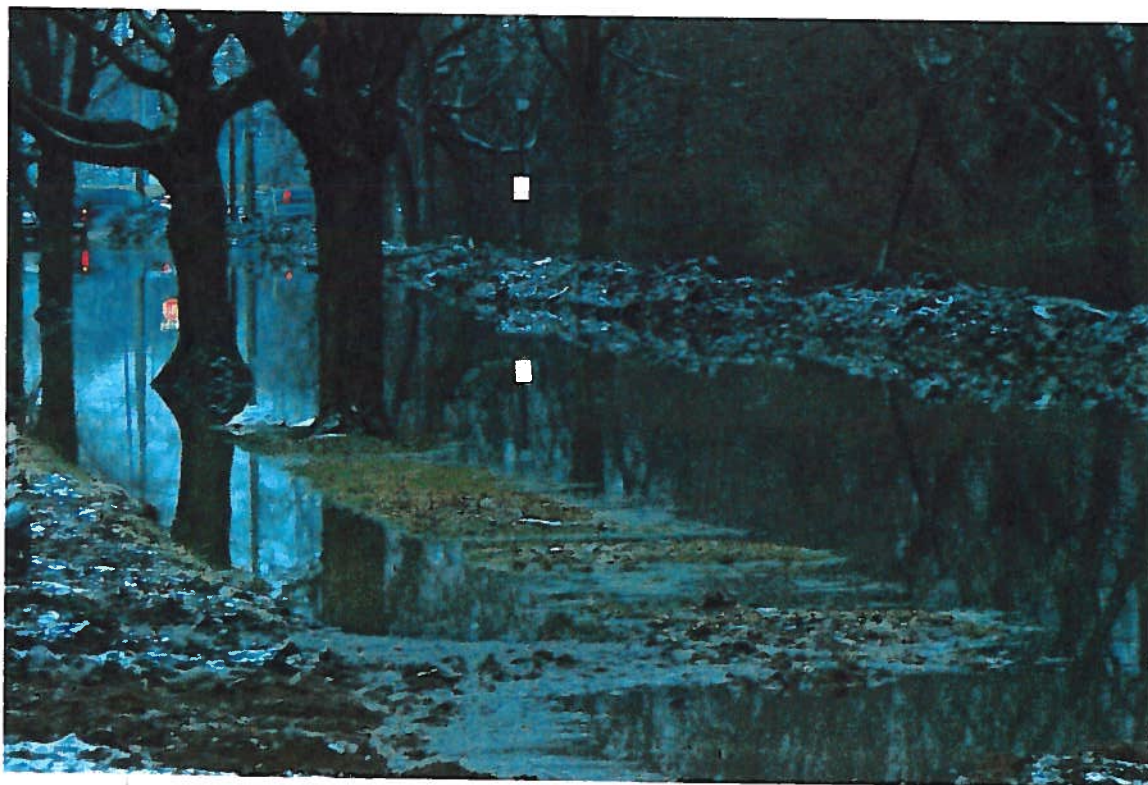
Respectfully,

A handwritten signature in dark ink, appearing to read 'Susan R. Haneline', with a stylized, flowing script.

Susan R. Haneline





















Susan R. Haneline
134 E. State Blvd.
Fort Wayne, IN 46805
Homeowner, Brookview Neighborhood

10/02/2012

To Whom It May Concern,

This letter is in response to questions about how we might best preserve and honor the history of Brookview Neighborhood, while proceeding with the State Boulevard project.

I have done a lot of thinking about the vision of the neighborhood, what it was meant to convey about the city, about progress and the importance of beautiful and aesthetically pleasing urban green spaces.

It is my understanding that the current proposed plan does not remove the historic footprint of the neighborhood. The bridge that is being proposed for removal is, in all honesty, an eyesore and a flooding issue. It has been in a state of disrepair for years. Its removal would improve the overall look of the neighborhood. I do not agree with attempts to fix the bridge in its current location. Even repairing the bridge seems counterproductive. At its current height and size, it would continue to back up the water in the creek which plays a significant part in the flooding of homes in that area. As for the moving of State Blvd, it is my understanding that the old roadway will, for the most part remain intact. It will simply become a part of the residential neighborhood. Perhaps it could simply be renamed something like "Historic State Boulevard". This plan seems to me, to actually enhance historic vision, not cause it to be destroyed. The winding street would still exist, but it would exist as a quiet tree lined boulevard set within a urban neighborhood green space.

As I have stated in previous letters, the homes located in the footprint of this project have been suffering for years due to flooding and traffic flow. The majority of these homeowners would like to be bought out for this project. I honestly believe that these buyouts and the subsequent open green space around the new State Boulevard would improve then neighborhood. To further respect the historic vision, I would suggest the following:

1. Installing historically correct lighting in the area. Perhaps something that reflected lighting styles from the early part of the 1900's. I think reproductions of the old gas style lights would be a wonderful addition to the neighborhood.

2. Plantings and green space that gives the area a park like feel, such as period style benches, groupings of trees and flowers, perhaps even brick style side walks. All these kinds of details and features pay homage to the areas history.

3. Beautiful stone or brick entrance pillars for the neighborhood. Perhaps something similar to the Forrest Park Boulevard area, a feature that would define the neighborhood.

4. I like the idea of a divided new State Blvd, one that is tree lined, perhaps with some perennial cottage or old English type flower beds.

5. I would love to see some small monuments that would tell the story of Arthur Shurcliff and his vision of the neighborhood. Perhaps even pointing out examples in the project that pay homage to his vision.

6. I think the best way to crown the area would be to find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces. Again, I think nod to an old fashion early 1900's park setting would be beautiful. Maybe the addition of an appropriately styled fountain or gazebo could add to the historic feel.

I believe there are many ways to honor our history without halting the progress of a project that I truly believe would benefit the neighborhood, the public and the city.

Respectfully,

A handwritten signature in black ink, appearing to read 'Susan R. Haneline', written in a cursive style.

Susan R. Haneline

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

October 3, 2012

Re: State Boulevard Reconstruction Project
Des. No. 0400587
Project No. IN20071404

Dear Ms. Hope:

As a Councilman elected to represent the welfare and best interests of the citizens of our city, I cannot support the current State Boulevard widening plan in anything like its present form, nor can I support the "concept" as described in the purpose and need statement on which it is based. The attempt to "elevate" (all words in quotation marks are taken from either the P and N statement or the 2030 II Transportation Plan) State Street to a major east-west arterial for regional as well as local traffic is nothing less than a perversion of the proper use of the "By-pass and Arterial concept", a concept which should be used to divert through traffic from residentially populated areas, not to them, as this plan does.

There are two legitimate needs in the Brook View area that must be addressed: the bridge over Spy Run Creek needs serious repair or replacement, and a blind spot at the foot of State Boulevard, near the intersection with Westbrook, needs to be eliminated. Plans responsive to these needs but, unlike the present plan, minimally harmful to the historic district it divides and effectively discouraging of faster traffic and truck traffic should be developed following AASHTO and Federal Highway Administrations Guidelines for public participation in context sensitive traffic project design. I will be pleased to participate in such a process, if properly conducted, and in the development of effective mitigation measures which would integral to it. In any event, discussion of further planning along any lines, mitigation or otherwise, should await the outcome of the Environmental Assessment which I understand is currently under way.

I call your attention to the enclosed letter, signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen. As a councilman at-large I represent them; but not just they will be harmed: the legitimate complaints and distress they convey translate into decreased property values. In turn this translates into a decreased property tax base and decreased tax revenues, which hurts the whole city. In fact this project in its current form will exacerbate the trend of recent decades toward the hollowing out of the city – that is, the migration of homeowners from the city to outlying areas - and it will do so precisely

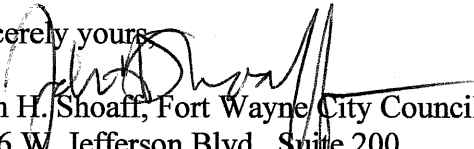
at the time we are trying to revitalize our downtown and at a time when we should be taking advantage of a national countertrend, led by seniors and young people, toward moving back into the city.

Reactions to the last mitigation meeting: Just prior to it Mr. DuMontelle, in a response to an inquiry from Michelle Wedaman, wrote that alternative plans were presented at a public meeting and a preference chosen. He was misinformed. Attendees were presented with just one plan, the only plan the planners have ever shown and about which they have been totally inflexible. The only alternatives offered were three minor ones that had to do with which local street would access the new four and five lane corridor. This fell far short of verbally made promises to explore and offer real alternatives; and, insofar as it was a pretense of doing otherwise, was an insult to the intelligence of my constituents. As I've said elsewhere, it was akin to asking the condemned man which tree he wished to be hanged from.

Mr. DuMontelle might wish to refute my implication that he did not show a proper grasp of the democratic process when he said that he liked the P and N statement because "it was decided by a regional planning body". He could reasonably say that he meant only that NIRCC decides only what recommendation will be sent to the elected body, and that he knows full well that the elected body makes the decision.

Finally, I also suggest that Ms. Weintraut consider excusing herself from any further moderating duties. Her rude and highly unprofessional reactions to speakers she disagreed with made it clear that the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government.

Sincerely yours,



John H. Shoaff, Fort Wayne City Councilman At-large
4646 W. Jefferson Blvd., Suite 200
Fort Wayne, IN 46804
Phone: 260 459 0221

September 1, 2012

To: Fort Wayne Common Council members and Mayor Tom Henry

RE: City's State Boulevard reconstruction project Wells/Cass Streets to Spy Run Avenue
State Blvd Reconstruction - Des. No. 0400587

Dear representative of the people of Fort Wayne:

The City of Fort Wayne wants to expand State Boulevard into a primary east-west arterial corridor like Coliseum Boulevard, and is currently preparing to widen State Boulevard's two lanes between Wells Street and Spy Run Avenue to five lanes, straightening the carefully designed traffic-speed-calming curves over Spy Run Creek and through the celebrated National Register of Historic Places Brookview Neighborhood. The City plans to elevate the roadway 7'-9'; turning this appealing residential district into yet another wide, fast urban highway. State Boulevard is an integral part of Fort Wayne's acclaimed Parks and Boulevard System, recognized in its entirety on the National Register of Historic Places as the Park and Boulevard System Historic District.

This plan displays a failure of the public process. The City claims that this project will address "safety and flooding" but has failed to demonstrate that either will be accomplished, at a cost of more than \$11 million public dollars. The decision-making used to arrive at this unsatisfactory proposal has been unclear, has alienated many of us who have sought input as engaged residents and has failed to address even basic issues about the project's real impacts on our property values (and consequent impact on property tax revenue), land use, safety and flood control, and the character of our neighborhoods. The City's Public Works Department has responded to our concerns only by holding public information meetings where we've been shown again and again the same "preliminary design" engineering drawings with no alternatives offered. The department has not altered its initial plan.

Our concern extends far beyond the additional dozen or so homes which would be razed, adding to the 26 or so recently demolished in the historic Brookview Neighborhood. Roadway design in this area affects the entire length of State Boulevard and the surrounding city. The current proposal will damage all of our property values and overall livability of the area. The decrease in property values will decrease property tax revenue, which will harm the entire city. We predict a migration from the well-tended and well-loved center city historic core, fewer owner-occupied homes, decreased care of properties, blight, and a decreased tax base for the city overall. Any road

projects here must be carefully considered from a comprehensive perspective and understood for what they mean to the people of the community. We are gravely concerned about our property values; quality of life; city attractiveness; safety and transportation accessibility for uses that include local vehicles, biking, walking and public transit; coherence with unique historical character and distinctive natural features; air, water, sound and light quality; and incorporation of meaningful flood control by identification of root causes.

This massive and poorly managed project should be halted immediately, and any plans to modify State Boulevard should begin anew, this time with a publicly transparent planning process which involves us - a working group of resident neighborhood leaders - along with elected representatives, and urban planning, community, economic development, design professionals and others qualified and engaged to evaluate current conditions and the impacts presented by this massive proposed road project.

Thank you,

Michelle Briggs Wedaman, president, Brookview Neighborhood, 275 households and businesses

Paul Gibson, president, Irvington Park, 80 households

Marcia Simmons, president, Park Place Condominium Association, 175 households

John Meinzen, vice president, Spy Run Neighborhood Association, 500 households

Cynthia Keller, president, Northside Neighborhood Association, 4,000 households and businesses

Shawna Nicelley, president, Forest Park Boulevard, 66 households

Steve Morehead, president, North Anthony Area Association, 1,100 households and businesses

Helene Evans, president, Frances Slocum Association, 1,200 households and businesses

Evelyn Bickel, president, Brentwood Park Association, 290 households

Gretchen Johnson, president, Bloomingdale Neighborhood Association, 1,850 households and businesses

Judi Wire, president, The Wells Corridor Business Association, 50 businesses

Mike Reuille, president, North Franke Park Association, 450 households and businesses

Rick Stoeckley, president, Lincoln Park, 680 households

Linda Spurrier, president, Fall Creek Neighborhood, 107 households

Tom Tiernon, president, Historic Oakdale Neighborhood Association, 700 households

CONTACT:

You can reach us all on this matter via Michelle Briggs Wedaman

260-710-4413

mbwedaman@frontier.com

mailing address: 2326 Eastbrook Drive, Fort Wayne, IN 46805

JOHN H. SHOAFF, A.I.A.
ARCHITECT

4646 West Jefferson Boulevard
Fort Wayne, IN 46802

e-mail: jshoaff@proparkwest.com
telephone: 260-459-0221

October 3, 2012

Steve Kennedy
Grants Section
Indiana Division of Historic Preservation and Archeology
402 West Washington Street, Room w274
Indianapolis, IN 46204

Dear Mr. Kennedy,

I am very pleased to join the chorus of those urging that the potential North Anthony Boulevard Historic District be listed on the National Register.

This is an area I have known since childhood, when I began making lifelong friends at the nearby Forest Park School. It has held up well over the decades, and has remained a good place to live, close to our downtown. It represents one stretch of the Kessler Park and Boulevard system, appreciated nationally as one of the most intact of George Kessler's urban plans, and recognition on the National Register would further encourage prideful home ownership.

Thank you for consideration of this excellent candidate for the Register.

Sincerely yours,

John H. Shoaff
Fort Wayne City Councilman At-large

Friends of the Parks of Allen County, Inc.
Po Box 10152
Fort Wayne, Indiana 468050

Ms. Briana Hope
Environmental Project Manager
American Structure Point, Inc.
7620 Shadeland Station
Indianapolis, Indiana 46256

Re: State Boulevard Reconstruction Project
Des. No. 0400587
Project No. IN20071404

The Friends of the Parks of Allen County, Inc. agree that there will be an adverse effect of the Fort Wayne Park and Boulevard System Historic District and the Brookview - Irvington Park Historic District, as well as to the Bridge over Spy Run (NBI No. 0200273).

However we feel that any discussion of mitigation is, at best, premature; at worst, the proposed Memorandum Agreement is a bad faith attempt to confuse an already complicated and unfair process, especially in light of the fact that the Federal Highway Authority has elevated the National Environmental Policy Act (NEPA) Documentation to Environmental Assessment (EA).

We continue to assert that the APE is not comprehensive enough and should include those historic districts along State Boulevard. This project is not just a construction project. It is a project which will continue the ongoing process of turning the historic boulevard into a major (as opposed to "minor") traffic arterial according to the plans that the Northeast Regional Coordinating Council has been following for years. The additional traffic that these plans are intended to generate will degrade the quality of life substantially along the State Boulevard corridor and will negatively impact a number of historic neighborhoods, schools, and other properties. Connectivity (safety), air quality, and visual relationships will be harmed along the entire corridor. We respectfully request that the APE be expanded to include all the properties along State Boulevard.

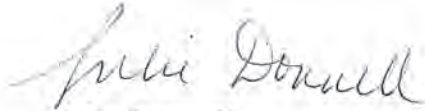
Further, it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete.

In conclusion, although I, myself, did not attend the most recent meeting, Ed Welling, Vice President of the Friends of the Parks was there. The Friends are appalled at the continuing disrespect shown to the citizens of Fort Wayne through this process, in

particular, the very unprofessional behavior of Ms. Weintraut and the assertions of Mr. Dumontelle that alternative plans have been presented to the public.

Under these circumstances, the public is not being served properly at all.

Sincerely,

A handwritten signature in cursive script that reads "Julie Donnell".

Julie Donnell
October 3, 2012

Julie Donnell
President,
Friends of the Parks of Allen County, Inc.
September 19, 2012

260-432-7178
juliemarie57@earthlink.net

From: [Suzanne](#)
To: [Hope, Briana](#)
Subject: Fort Wayne State Blvd project - Mitigation response following meeting of 19 September, 2012
Date: Wednesday, October 03, 2012 11:53:56 PM

All,

On finding ways to mitigate the adverse effects of the massive, intrusive State Blvd project on the impacted area, here's a solution: don't build it.

Fact: It is not needed.

Fact: The proposed plan is flawed.

Fact: The APE is inadequate.

Fact: The process has been unfair and undemocratic, and now we can also say -- abusive.

Regarding the meeting of 19 September, 2012, the disrespectful, contemptuous tone and the conduct of our meeting hosts were appalling. The attempts to deny concerned citizens entrance to the meeting was transparently obstructionist. The scoldings and rude interruptions by our hosts and government agency reps were extremely unprofessional and insulting. The facilitator's dramatic eye-rolling, grimacing, smirking, and toe-tapping while those in opposition were trying to speak were quite a stunning display of dramatics. I hope those of you who attempt to operate with integrity were embarrassed by these ridiculous stunts.

At this so-called mitigation meeting we were admonished to provide only facts and simple ideas for mitigation. Some mitigation ideas offered us by the facilitator included taking a couple of photographs of the existing bridge and coming up with a few educational signs and materials about the historic elements that will be destroyed by this colossal project -- as if such trivial efforts would smooth over the loss of a dozen homes and the eruption of a highway in the midst of our quaint historic district.

Here are more facts that have been offered by the consulting parties and repeatedly rejected by Stucturepoint and our government agency reps:

- The P&N rationale is flawed and has shifted and changed depending on the agenda of the moment. It's the curves, no, it's flooding, no, it's congestion, no, it's . . . fill in the blank with some other fabricated reason, but, never with the NIRCC plan goals to push arterial traffic through the north side of Fort Wayne.
- Safety has been given lip service, but substantive traffic calming is never, *has never been*, included in the design plan in *any way*, even though traffic calming

elements are being designed into other Fort Wayne urban roadways. So *clearly* safety is not a priority here. If it were, accurate data would have been provided to the public. Instead we have vague references to crashes that might have occurred somewhere in the *general* area that might or might not have been caused by congestion, or speeding, or, we don't know what, because that information was never provided!

- It is a fact that the proposed roadway will withstand higher speeds; this does not play nicely with traffic calming.
- The current plan is intrusive, and massive yet no substantive design elements have ever been revealed in renderings that the public can easily evaluate, even though, the bifurcation of the historic district is a **devastating** aspect of the plan. Where are street-level renderings of the proposed design that show the elevated bridge and vacated lots where homes once sat?
- The APE is not adequate. The plan must be rejected until it appropriately addresses the entire impacted area in all of its invasive, destructive aspects. How can a project this devastating and monstrous not impact the entire near north side of the City?
- Here's another fact, I pulled onto State (southbound from Eastbrook, turning west onto State) at 8:05 a.m. on 2 October, 2012, a Tuesday morning, and my car was the SOLE vehicle in either lane of the roadway at that time for the entire stretch of the relevant curved area. In fact, my car was the only vehicle in sight at that time. Your purpose and needs congestion argument is not tenable! And, when I came home about 11:00 a.m., there were widely spaced lines of "traffic" -- about six cars -- moving at a steady speed in both directions. I proceeded to Eastbrook made my left turn safely and without having to wait.
- And one more factoid for you to ponder: on my bicycle today at 5:02 p.m. -- afternoon rush-hour -- I had to wait perhaps 30 seconds to safely ride across State Blvd at Cass from south to north. There was a wide gap, with no traffic in either direction **at 5:00 p.m.** So plug that into your expensive traffic model and explain why we need to squander 11+ million dollars on this disastrous project!

With all *due* respect,

Suzanne Slick
Irvington Park Consulting Party

**Jill D. Downs
1202 Elmwood Ave.
Fort Wayne, IN 46805**

October 4, 2012

Briana Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

Dear Ms. Hope:

I am writing to provide my comments to the most recent findings regarding the State Boulevard Reconstruction Project (Des. No. 0400587, DHPA No. 5903).

In short, I would agree that this project creates an adverse effect in the area. However, it is clear that the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration. Although American Structurepoint and the Federal Highway Administration feel the project is at a point where mitigation discussions can be held, the majority of those opposed to the project remain unconvinced about its perceived necessity. Because of this, it is difficult to move forward. I think this was painfully obvious at the September 19, 2012, meeting of the consulting parties.

Those opposed to the project were not interested in discussing mitigation for a variety of reasons. These include the fact that the Environmental Assessment has not been completed, the resultant bi-furcation of the historic district and consequences of raising of the road should be added to the list of adverse effects, and that because houses in the area had already been removed by the City of Fort Wayne, there appears to be a lesser impact to the district. Also, it was noted by Linda Weintraut in the meeting that there is another project proposed within the area, this being the Pufferbelly Trail, that is creating an impact on the design of the State Boulevard Reconstruction Project. It would seem as those information about the specifics of the Pufferbelly Trail project should be incorporated into the State Boulevard project. It is premature to discuss mitigation.

Overall, I do not believe there is not a need to reconstruct State Boulevard. As a nearly lifelong resident of near northeast Fort Wayne, I travel the State Boulevard corridor on a regular basis. With the exception of a relatively short window of time during two peak travel periods on weekdays, this corridor is easily traveled with minimal delay and congestion. My understanding is that the reconstruction project is not citizen-driven, but is based on a multi-year transportation plan that was originally devised many years ago when the standard was to move as many cars through an area as quickly as was safely possible. Now, numerous community studies have shown that this type of thinking is detrimental to residential neighborhoods, and "traffic-calming" solutions are being devised as ways of protecting neighborhood integrity and pedestrian safety. Such projects are even being undertaken within Fort Wayne, specifically the downtown area. It is not logical to think that creating a five-lane road with a lesser curve will constitute a safer situation as compared to the existing two-lane road with curves requiring slower speeds. Additionally, traffic congestion at peak travel times is a built-in traffic-calming measure resulting in slower traffic when there are more cars in the area. A recent "Travel Time Delay Study" for fiscal year 2012 indicated that during peak travel times, there is only a 5.7 delay in traveling from Sherman Boulevard to Beacon Street. Just from a fiscal standpoint alone, I do not believe spending millions of dollars to save 5.7 minutes is worthwhile. The State Boulevard Reconstruction Project is not warranted, and this is evident by the poor attempts by the City of Fort Wayne, American Structurepoint, and the Federal Highway Administration to justify its need.

Sincerely,

Jill D. Downs



The mission of ARCH is to advocate for the protection and preservation of historically and culturally significant assets and historic places in Allen County and northeast Indiana.

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter dated August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form, fashion, or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintrout.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. Although the material had literally been sent via e-mail at the close of business hours the day before the meeting, it still played an integral part in the American Structurepoint presentation. At the meeting it was concurred by FHWA and the DHPA that furnishing information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other information provided in the 4(F) document.

Given the breadth and scope of the project, and associated substantial adverse impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,

Michael Galbraith
Executive Director, ARCH, Inc.

Ecc: DHPA, FHWA

BROOKVIEW

NEIGHBORHOOD ASSOCIATION

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

October 4, 2012

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to request an additional 30 days to comment on the American Structurepoint letter and packet dated August 29, 2012, about proposed mitigation for State Boulevard Reconstruction in Fort Wayne, IN. We require additional time to incorporate our comments related to the August 29 packet, as additional materials were sent to us from American Structurepoint via email the night before the September 19, 2012 consulting parties meeting. Those materials were then referred to by American Structurepoint during their September 19, 2012 presentation.

Since the last consulting parties meeting notes in 2011, we have received only the May 2012 updates related to historic properties and no indication of or updates on other project alternatives under exploration, and no answers to the very real questions we have about this project. The many questions we asked at both the December 2009 and the September 2011 consulting parties meetings have remained unanswered: questions about the project's Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the real area of impact of this project.

We understand that an environmental assessment is still being completed for this project. How can we discuss mitigation before being enlightened by information contained in that report? How can this group discuss mitigation without being enlightened by information we have all requested about this project?

This project is for us, not a theoretical one. These are our streets and our neighborhood, and we have a right and a responsibility to request and receive not only substantive answers to questions we and others within the consulting party circle have been asking, but ample time to review and comment in writing upon information related to this exploration.

Sincerely,
Michelle Briggs Wedaman
President, Brookview Neighborhood
2326 Eastbrook Drive, Fort Wayne, IN 46805
260.710.4413 mbwedaman@frontier.com

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Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



October 4, 2012

Richard J. Marquis
Acting Division Administrator
Indiana Division
Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft memorandum of agreement for the State Boulevard Reconstruction Project (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No. 5903)

Dear Mr. Marquis:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed American Structurepoint's letters of August 29, 2012 (with enclosures) and September 18, 2012 (with enclosures), and has taken into consideration the discussion at the September 19, 2012 consulting parties meeting, regarding the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

As we had said in our August 13, 2012 letter, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the additional portions of the proposed project area, and we concur with the opinion of the archaeologist, as expressed in the Indiana archaeological short report (Stillwell, 7/11/12), that no further investigations appear necessary at these additional portions of the proposed project area.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

We concur with FHWA's August 27, 2012 Section 106 finding of Adverse Effect for this undertaking.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected:

- Fort Wayne Park and Boulevard System;
- Brookview-Irvington Park Historic District; and
- Bridge on State Boulevard over Spy Run (NBI. No. 0200273).

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

Furthermore, we wish to offer some comments and suggestions about the draft memorandum of agreement, Version 8/24/2012 ("Draft MOA").

SUGGESTIONS FOR FURTHER MINIMIZATION

We think we understand the issues described in your September 18 letter that would make preservation of the houses at 112, 134, and 138 East State Boulevard problematic. We remain concerned about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District. Having reflected further upon your September 18 letter, we wonder whether, as a minimization measure, it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive. It is our impression that most of the existing sidewalk along the south side of State Boulevard could remain in place, and it seems to us that the existing sidewalk could serve pedestrians who would be walking along the north side of the new alignment, even though the northward bow in the existing State Boulevard would make one's walk slightly farther than if a sidewalk immediately paralleled the new alignment along its north side. We are sympathetic to the concerns of property owners at the September 19 meeting who expressed a preference to have their entire properties along the south side of the current alignment of State Boulevard, rather than to sell only large portions of their yards and have the new proposed right-of-way come within only several feet from their houses. However, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect.

It appears to us that if the sidewalk and the grass buffer between the sidewalk and the curb were eliminated from the plans along the north side of the new alignment from Terrace to Eastbrook, and if a railing of some kind were constructed adjacent to the curb, then at least ten feet less right-of way would be needed along that north side of the new alignment. Furthermore, if a retaining wall were constructed near that railing, instead of a sloped embankment and a drainage swale, it appears to us that even less right-of-way would be needed along the north side of the proposed alignment. Also, if there were no sidewalk immediately adjacent to the north side of the new alignment, it appears to us that it might be feasible to eliminate one or both of the new sidewalks that are proposed along the Oakridge Road extension. If there were no sidewalk along the north side of the new alignment, then there would seem to be no need to provide new sidewalks extending southward along the Oakridge extension from the existing State Boulevard to the new alignment of State Boulevard.

We also wonder whether the reconstruction of State Boulevard, which would elevate the roadway above the existing grade as it runs west from Terrace, could be designed to serve to some extent as a levee to prevent most Spy Run floodwaters from reaching the three houses in question on the south side of the existing State Boulevard.

If some or all of the suggestions above prove to be feasible and prudent, then we think they should be incorporated into the memorandum of agreement, in an effort to avoid confusion in the future about the design parameters upon which agreement has been reached.

SUGGESTIONS FOR MITIGATION

Stipulation I. of the Draft MOA appropriately directs that context sensitive solutions be incorporated into the new construction and related landscaping and streetscape design. That stipulation also would establish an advisory team to review and comment on the specifics of that design work, in keeping with the directive contained in Stipulation I.B.ix. of the 2009 "Memorandum of Agreement. . . Regarding the US 27 Southbound Realignment and Bridge Replacement over Spy Run Creek in Fort Wayne, Allen County, Indiana" for future federal projects in the area. The advisory team that was established under that US 27 memorandum of agreement provided useful recommendations for context sensitive solutions for that project. However, we do not believe that there is a need for the Indiana SHPO to be directly involved in all of the meetings and activities of future advisory teams in the area. We believe that the most important input will arrive in the form of the Advisory Team members' recommendations, based on their perceptions of what is best for their community, and of the guidance from FHWA and the Indiana Department of Transportation regarding the feasibility of those recommendations. Consequently, we ask that the Indiana SHPO *not* be given a role in convening advisory team meetings, as is currently proposed in Stipulation I. B. and I.B.vi. of the Draft MOA, and that the Indiana SHPO's participation in meetings of the advisory team be left to the Indiana SHPO's discretion. It would be appropriate, however, for the Indiana SHPO to remain involved in the kind of consultative role that is prescribed in the final sentence of both I.B.vi. and of I.B.viii.

We anticipate that at least one consulting party will be making recommendations for crafting context sensitive solutions in keeping with the natural landscape of the project area and the landscape design philosophy of George Kessler or Arthur Shurcliff. This is an intriguing idea, and we would ask that serious consideration be given to any consulting party recommendations along those lines. It appears that regardless of the particulars of the final design of the reconstructed State Boulevard, a considerable amount of green space will be opened by this project, and how that green space is designed could play an important mitigative role.

We agree that the current State Boulevard bridge over Spy Run (NBI. No. 0200273) should be documented photographically, as provided for generally in the Historic Bridges PA and specifically in Stipulation II. of the Draft MOA. We would ask that such photo-documentation be performed in accordance with the version in effect, at that time, of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards." It recently has been brought to our attention that the State Archives, rather than the State Library's Memory Project, is the legally-authorized repository of all state government records that are required to be preserved.

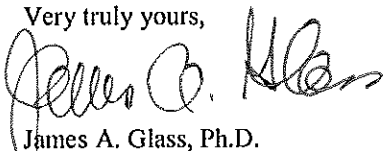
We also request that it be stipulated in the MOA that the portion of the Brookview-Irvington Park Historic District within and immediately adjacent to the proposed project area should be photographically documented. We recommend that streetscape and broad views of the setting of that part of the neighborhood be emphasized, but we think that at least a couple of photographs of each house that is to be demolished also should be included in the documentation. The photographs should be taken from oblique angles so as to document all four elevations of each house.

For both the State Boulevard bridge photographs and the streetscape and district photographs, we request that a set of the photographic images in both print and digital form, saved on a compact disc, and following, as closely as possible the guidance of the "Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards," be provided to our office for ultimate transfer to the State Archives. We also think that a duplicate set of the electronic and print photo-documentation be prepared for and delivered to a local public library or not-for-profit institution that would be capable of and willing to retain the documentation on a permanent basis, so that it would be readily accessible to local researchers.

If you or American Structurepoint, Inc. would find it helpful, we could draft specific MOA stipulation language or modifications to language in Version 8/24/2012 to show how our recommendations might be incorporated into the MOA. If you wish to receive such suggestions of specific language, then, in order to facilitate our drafting efforts, we would appreciate receiving an electronic copy of the MOA in a format that would allow us to show changes and make explanatory comments.

If you have questions about buildings or structures, then please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Questions about archaeological issues should be directed to Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue refer to DHPA No. 5903.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:JLC:jlc

cc: Briana Hope, American Structurepoint, Inc.

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

October 4, 2012

Briana M. Hope
Environmental Project Manager
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

RE: State Boulevard Reconstruction Project

Dear Ms. Hope,

The purpose of this letter is to formally request an additional 30 days to comment on the letter of August 29, 2012 regarding proposed mitigation for the State Boulevard Reconstruction Project. We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(F) compliance document.

This request for additional time should be granted as the consulting parties involved in this project require additional time to evaluate the material which we received the evening before the consulting parties meeting held on September 19, 2012 via email. In fact, I was unaware of the additional documentation and information therein until attending the meeting itself as my travel time required I leave early that morning and did not have an opportunity to see or review that email. At the meeting it was concurred by FHWA and the DHPA that sending pertinent information the evening before the consulting parties meeting did not provide adequate time to review and make informed comments. We would add, we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other informant provided in the 4(F) document.

Given the breadth and scope and associated substantial impacts to historic resources, natural environments and surrounding historic districts, it is not an unreasonable request to grant an additional 30 days for comments on the proposed MOA and mitigation measures.

Sincerely,



Todd A. Zeiger
Director, Northern Regional Office

Ecc: Dr. Jim Glass, Indiana Division of Historic Preservation and Archaeology
Mike Galbraith, ARCH



CITY OF FORT WAYNE

THOMAS C. HENRY, MAYOR

October 4, 2012

Ms. Briana M. Hope
Environmental Project Manager
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256

RE: State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587
DHPA No. 5903
Project No. IN20071404

Dear Ms. Hope:

This letter provides comments on the above-referenced project from both Tom Cain, Fort Wayne Urban Designer, and from Creager Smith, Fort Wayne Historic Preservation Planner. We both agree with the determination that the proposed project would have an adverse effect on three historic properties within the APE.

The bullet points that follow are a list of adverse effects on landscape and planning features, in both the Brookview-Irvington Park Historic District and in the Fort Wayne Park and Boulevard System Historic District, caused by the State Boulevard Reconstruction Project. The list was assembled by Tom Cain, and edited by Creager Smith. It is a suggested starting point for more detailed discussion during any consideration of mitigation of adverse effects. These items are in no particular order; they are the potential basis of mitigation measures, although there may be additional adverse effects that do not appear on the following list.

- Roadway and sidewalk layout changes from historic alignments to new alignments; alteration of the locations, sizes, and shapes of the carefully designed public and private spaces provided by the historic Brookview plat.

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Hope

October 4, 2012

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- Roadway elevation changes and topographic relationship changes between linear public roadway space and the private linear yards.
- Stream corridor vegetation loss from new road and gap in streamside vegetation left by bridge removal.
- New open space fragments that are not part of the historic Brookview plat.
- Disintegration of public space definition on south side of old State Blvd. alignment by demolition of homes and loss of the private space.
- Alteration of historic relationship of road elevation to former rail bridge embankment and bridge abutments.
- Alteration of road width through former rail bridge abutments.
- Substantially increased width of roadway through neighborhood.
- Changes in pedestrian patterns.
- Alteration of neighborhood street connections to State Blvd, concentrating them into a new extension of Oakridge Rd.
- Removal of later street lighting which is of similar scale to historical lighting for the area.
- Removal of historic rail and Spy Run Creek bridge architecture.
- Loss of street trees.
- Loss of historic streetscape patterns of Eastbrook Dr.
- Alterations of Westbrook Dr. and other segments of neighborhood streets, especially at ends of blocks and at driveways.
- Insertion of access ramps from State Blvd. to the new greenway located on rail embankment.
- Increased height of rail embankments to access new greenway bridge over State Blvd.
- New greenway bridge architecture, and new bridge abutments, over State Blvd.
- Loss of historic neighborhood development pattern of the Brookview plat; the plat is of significance to the National Register listing.

Hope

October 4, 2012

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- Loss of houses and garages that are contributing resources in the Brookview-Irvington Park Historic District is an adverse effect; however if the historic context of the relationship between public spaces and private buildings is lost due to alterations and destruction of the Brookview plat, then carefully chosen demolitions should be considered as part of a broader mitigation strategy .
- Loss of spatial definition along linear public ways with removal of houses and the street removal along State Blvd. and Eastbrook.
- Medians placed within the roadway are not a character-defining feature of the Fort Wayne Park and Boulevard System as designed by George Kessler, nor are they a feature of the Brookview plat; if proposed they must be very carefully considered.

As this Section 106 process moves forward we are available to assist in the development of mitigating design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on the team.

Sincerely,



Thomas Cain, RLA, ASLA
Senior Urban Designer



Creager Smith
Historic Preservation Planner

From: "Carpenter, Patrick A" <PA_Carpenter@indot.IN.gov>
Date: October 5, 2012 4:17:47 PM EDT
To: "Hope, Briana" <bhope@structurepoint.com>, <mgalbraith@archfw.org>, <aquinn@archfw.org>, <don.orban@cityoffortwayne.org>, <lzeiger@indianalandmarks.org>, <mbwedaman@frontier.com>, "Donnell, Julie" <juliemarie57@earthlink.net>, <jcooper@ccrtc.com>, <indianabridges@sbcglobal.net>, <shan.gunawardena@cityoffortwayne.org>, <dan.avery@co.allen.in.us>, <jslick@mac.com>, <jandailey59@msn.com>, <joyce.newland@fhwa.dot.gov>, "Heil, Larry" <larry.heil@fhwa.dot.gov>, <jshoaff@proparkwest.com>, "Kaiser, Jason" <JASONKAISER@indot.IN.gov>, "Kennedy, Mary" <MKENNEDY@indot.IN.gov>, <linda@weintrautinc.com>, <creager.smith@cityoffortwayne.org>, <albertcohan@aol.com>, <tmn@barrettllaw.com>, <rross@martin-riley.com>, <dan@earthsourceinc.net>, "Glass, James" <JGlass@dnr.IN.gov>, "Carr, John" <JCarr@dnr.IN.gov>, <kdietsch@comcast.net>, <alec.johnson@ci.ft-wayne.in.us>, "Crites, Scott" <SCrites@structurepoint.com>, "Zielinski, Rich" <RZielinski@structurepoint.com>, "Smith, Gregory" <GSmith2@indot.IN.gov>, "Johnson, Amy \"(DNR)\" <AJohnson@dnr.IN.gov>, "Ricketts, Amanda" <ARicketts@dnr.IN.gov>, "Tharp, Wade" <WTharp1@dnr.IN.gov>, <tom.cain@cityoffortwayne.org>, "Hilden, Laura" <lhilden@indot.IN.gov>, "Mcmullen, Kenneth B." <KMCMULLEN@indot.IN.gov>
Subject: Section 106 Consulting Party Comments-State Blvd. (Des. #0400587)

Dear Consulting Parties,

Thank you for your comments . In light of the comments received during the most recent comment period and at the September 19, 2012 Consulting Parties meeting, the finding and 800.11 documentation will be updated. The updated finding, draft MOA and draft 4(f) Evaluation will be released with the Draft Environmental Assessment for an additional 30-day comment period. As such, we are not extending the current comment period, but providing for an additional 30-day comment period for both consulting parties and the public . This comment period will be initiated once the draft Environmental Assessment (EA) is released for public involvement by FHWA. The draft EA is expected to be released by the end of this year. The City of Fort Wayne and their consultants, Structurepoint, will continue to consult with FHWA and SHPO concerning comments received and measures to minimize adverse effects.

Thank you all for your continued participation.

Patrick Carpenter
Manager, Cultural Resources Office
Environmental Services
Indiana Department of Transportation
100 N Senate Ave., IGCN-Rm. N-642
Indianapolis, IN 46204-2216
[317-233-2061](tel:317-233-2061)

Phone Log

October 15, 2012

Conversation between Tom Cain & Linda Weintraut

At approximately 4:30 pm Tom Cain called to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but want to make them within the context of SHPO's assessment of project impacts and that the City addresses all adverse effects.

Cain also stated that impacts to the Brookview neighborhood should be enumerated.

Linda Weintraut said she would consult with Structurepoint and return Cain's call.

Phone Log
October 16, 2012
Conversation between Tom Cain & Linda Weintraut

I returned Tom's call from yesterday. I told him that Structurepoint was very glad to have his input on this project; at the very minimum, we would consult with him prior to the agency meeting and Briana was checking to see if it would be OK if he attends.

Tom spoke about the landscape changes that would be wrought as a result of the undertaking, particularly the changes from private to public space around the undertaking.

He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s.

He would like for mitigation to deal with changes in scale that will occur; tree planting should occur within 3 feet of the roadway (and not the standard 10 feet required on highways.) Tom believes that this would change the scale of the undertaking for the residents. He has other ideas that he will type up and send to Briana and I.

He said that it is important to achieve the "right feel" for the space.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



November 15, 2012

Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
Post Office Box 5034
Zionsville, Indiana 46077

Federal Agency: Federal Highway Administration ("FHWA")

Re: Indiana State Historic Preservation Officer's recommended changes to the October 17, 2012 draft of the "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Des. No. 0400587; American Structurepoint Project No. IN20071404; DHPA No.)

Dear Dr. Weintraut:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the draft memorandum of agreement submitted with your October 16, 2012 e-mail message, for the aforementioned project in the City of Fort Wayne, Allen County, Indiana.

In response to your acceptance of our offer to draft specific language for the memorandum of agreement ("MOA") to illustrate the recommendations we made in our October 4, 2012 letter, we have enclosed a marked-up copy of the draft MOA.

The most significant changes to the MOA reflect the extensive comments and recommendations we had offered in our October 4 letter, so we have not explained those changes here. We think that the reasoning behind the other changes is self-evident.

One issue that we noticed in the draft MOA but which we have not attempted to change is what appears to us to be the interchangeable use of "shall" and "will." We are not sure that it matters which word is used, but to the extent that they are being used synonymously, we would suggest that one or the other be used consistently.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 230953 or wtharp1@dnr.IN.gov. If you have questions about issues pertaining to buildings or structures, please contact John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please continue to refer to DHPA No. 5903.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:JLC:jlc

Enclosure

emc: Joyce Newland, Indiana Division, Federal Highway Administration
Lawrence Heil, P.E. Indiana Division, Federal Highway Administration
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Briana Hope, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



April 1, 2013

Karen Bobo
Acting Division Administrator
Federal Highway Administration, Indiana Division
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: FHWA's finding of Adverse Effect, with supporting documentation, and draft "Memorandum of Agreement . . . Regarding the State Boulevard Reconstruction from Spy Run to Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana" (Version 2/20/2013) (Des. No. 0400587; Federal Project No. IN20071404; DHPA No. 5903)

Dear Ms. Bobo:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (16 U.S.C. § 470f), 36 C.F.R. Part 800, the "Programmatic Agreement . . . Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana" ("Minor Projects PA") and the "Programmatic Agreement . . . Regarding Management and Preservation of Indiana's Historic Bridges" ("Historic Bridges PA"), the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO") has reviewed the documents submitted with American Structurepoint's March 1, 2013, letter, which we received on March 4, for the aforementioned project in Allen County, Indiana.

We concur with FHWA's February 27, 2013, Section 106 finding of Adverse Effect for the State Boulevard Reconstruction Project.

We also concur, for the purposes of Section 4(f) of the Department of Transportation Act, that the following historic properties will be adversely affected by this project: Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and State Boulevard Bridge over Spy Run (NBI No. 0200273).

Thank you for taking in to consideration our comments on an earlier draft of the draft memorandum of agreement ("MOA"). We have some specific suggestions regarding Version 2/20/2013 of the draft MOA.

In the past, the staff of the Advisory Council on Historic Preservation has recommended that in an MOA for a project where the design is important in determining effect, a reference be included in the MOA to the date of the project plans. Doing so clarifies which elements comprised the project at the time the adverse effect finding was made and serves to reduce the likelihood of later disagreement among the signatories to the MOA about the nature of changes to historic properties that the project may have. Although the supporting documentation provide here includes plan drawings, it occurs to us that it could still be early enough in the design process that it might be necessary for the City of Fort Wayne to make revisions to one or more of those drawings before construction begins. At the very least, plans showing how the project will incorporate context sensitive solutions, such as those recommended in stipulations I.A. and I.D.2., will have to be drawn. Consequently, it does not seem feasible to tie the finding and the MOA commitments for this project to any currently existing set of plans.

We think, however, that aside from new construction design issues, the subject most likely to result in future controversy pertains to the identities of the principal buildings that will be demolished by this project (if they are not demolished for other reasons beforehand). Various alternatives and variations on alternatives have been proposed, either by the City or by other consulting parties, during the Section 106 consultation on this project, and some of those proposals would have left more buildings standing than the preferred alternative now would. Instead of citing in this MOA the date of any

existing construction plans, one measure that could be incorporated into the MOA to avoid a misunderstanding about the extent of demolition that would be allowed in this project would be a listing of the street addresses of all principal buildings that will be demolished, including those that are not considered to be individually eligible for the National Register of Historic Places or to contribute to the significance of an eligible historic district. That way, all consulting parties will be able to visualize, in a general way, the geographic boundaries of the demolition that will be necessary to implement the as-yet-unfinished designs of the new State Boulevard alignment and of the related context sensitive solutions. A "whereas" clause in the preamble to the MOA might be the appropriate place to list those street addresses.

In the last line of Stipulation I.D.2., "the rights-of-way" appears to need a gerund in front of it to describe the kind of context sensitive solution that might be applied to rights-of-way within the project footprint.

The second paragraph of Stipulation III. requires that the content and design of the four interpretive plaques be provided to the Indiana SHPO and the (other) consulting parties for review and comment. However, that paragraph seemingly requires the City to attempt to accommodate only the recommendations of the Indiana SHPO. We suggest that the third sentence of that paragraph be revised to read as follows: "If the Indiana SHPO or any other consulting party responds with recommendations, a good faith effort to accommodate the recommendations will be made." We often have found, not surprisingly, that local consulting parties are better able than we are to evaluate the accuracy of the content and the appropriateness of the design of interpretive markers.

In regard to archaeology, we recommend the following revisions to the Stipulation V. ("Post Review Discovery") section of the Draft MOA. We recommend that the second sentence be revised to state, "Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1, 312 IAC 21, and 312 IAC 22, and the most current Guidebook for Indiana Historic Sites and Structures Inventory—Archaeological Sites."

Following your receipt of these comments and those of other consulting parties, and before you circulate a final version of the MOA for signature, please send the revised draft to all consulting parties for review and comment.

If you have questions about archaeological issues, please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. Questions about issues pertaining to buildings or structures should be directed to John Carr at (317) 233-1949 or jcarr@dnr.IN.gov. In all future correspondence regarding the State Boulevard Reconstruction Project, please continue to refer to DHPA No. 5903 and address it to Chad W. Slider, Assistant Director for Environmental Review, Division of Historic Preservation and Archaeology, Indiana Department of Natural Resources, 402 West Washington Street, Room W274, Indianapolis, Indiana 46204.

Very truly yours,



Ron McAhron
Deputy Director
Indiana Department of Natural Resources

RM:WTT:JLC:jlc

cc: Christine Meador, American Structurepoint, Inc.

emc: Karen Bobo, Federal Highway Administration, Indiana Division
Joyce Newland, Federal Highway Administration, Indiana Division
Lawrence Heil, P.E., Federal Highway Administration, Indiana Division
Patrick Carpenter, Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation
Christine Meador, American Structurepoint, Inc.
Linda Weintraut, Ph.D., Weintraut & Associates, Inc.

MEMORANDUM OF AGREEMENT
BETWEEN THE FEDERAL HIGHWAY ADMINISTRATION AND
THE INDIANA STATE HISTORIC PRESERVATION OFFICER
SUBMITTED TO THE ADVISORY COUNCIL ON HISTORIC PRESERVATION
PURSUANT TO 36 C.F.R. Section 800.6(b)(iv)
REGARDING THE STATE BOULEVARD RECONSTRUCTION FROM SPY RUN TO CASS
STREET
IN FORT WAYNE, WAYNE TOWNSHIP, ALLEN COUNTY, INDIANA

WHEREAS the Federal Highway Administration ("FHWA") proposes to widen and realign a portion of State Boulevard for the State Boulevard Reconstruction from Spy Run to Cass Street (Des No.: 0400587) in, Fort Wayne, Wayne Township, Allen County, Indiana ("Project"); and

WHEREAS the FHWA, in consultation with the Indiana State Historic Preservation Officer ("SHPO"), has defined this State Boulevard Reconstruction from Spy Run to Cass Street's area of potential effects, as the term defined in 36 C.F.R. Section 800.16(d), to be the area extending 250 feet from the alley west of Cass Street to the abandoned New York Central Railroad, encompassing the first properties on the west side of Cass Street, north and south of West State Boulevard. From the abandoned railroad it continues east to the west property line of the property at 2239 Westbrook Drive. Following the north property line of 2239 Westbrook Drive, the APE continues east, crossing Westbrook Drive, Spy Run Creek and Eastbrook Drive, turning north to follow the east side of Eastbrook Drive to the north property line of 2342 Eastbrook Drive and turning east along that property line, including the north line of the property at 2335 Oakridge Road and continuing west along the south side of Neva Avenue to its intersection with North Clinton Street. From North Clinton Street east to Spy Run Avenue, the APE will extend 250 feet from the centerline of the existing roadway; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has found that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and the State Boulevard Bridge over Spy Run are within the area of potential effects; and

WHEREAS the FHWA and the Indiana SHPO both recognize that the Fort Wayne Park and Boulevard System and Brookview-Irvington Park Historic District are listed in the National Register of Historic Places; and

WHEREAS the FHWA and the Indiana SHPO both recognize that the State Boulevard Bridge over Spy Run has been determined eligible for listing in the National Register of Historic Places per the Indiana Statewide Historic Bridge Inventory; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has determined pursuant to 36 C.F.R. Section 800.5(a) that the State Boulevard Reconstruction from Spy Run to Cass Street will have an adverse effect on the Fort Wayne Park and Boulevard System, Brookview-Irvington Historic District, and the Bridge over Spy Run; and

WHEREAS the FHWA, Indiana Department of Transportation ("INDOT"), the Indiana SHPO, and the Advisory Council on Historic Preservation ("Council") have executed in 2006 a Programmatic Agreement on the Management and Preservation of Indiana's Historic Bridges and the FHWA has determined that the State Boulevard Bridge over Spy Run is "Non-Select" (not considered an excellent example of a given type or not suitable candidate for preservation); and

WHEREAS the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Section 800) to resolve the adverse effect on the Fort Wayne Park and Boulevard System and Brookview-Irvington Historic District; and

WHEREAS the FHWA has consulted with the Indiana SHPO and both recognize that 2244 Eastbrook Drive is a non-contributing resource within the Brookview-Irvington Historic District and will be demolished as part of the project; and

WHEREAS the FHWA has consulted with the Indiana SHPO and both recognize that following properties are contributing resources within the Brookview-Irvington Historic District and will be demolished as part of this undertaking: 2221 Westbrook Drive; 112 East State Boulevard; 128 East State Boulevard; 134 East State Boulevard; 138 East State Boulevard; 142 East State Boulevard; 146 East State Boulevard; 154 East State Blvd; 158 East State Boulevard; 162 East State Boulevard; 2252 Eastbrook Drive; 2248 Eastbrook Drive; 2240 Eastbrook Drive; and 2236 Eastbrook Drive; and

WHEREAS the public was given an opportunity to comment on the undertaking's adverse effect in a notice published on **“(give date(s) of publication)”** in the **“(give name of publication)”**; and

WHEREAS the FHWA has notified the Council of the adverse effect and invited the Council's participation in the project, pursuant to 36 CFR Section 800.6(a)(1), in a letter dated August 29, 2011 and in additional material conveyed on May 10, 2012; and

WHEREAS the Council declined to participate in consultation in a letter dated July 31, 2012; and

WHEREAS the FHWA, in consultation with the Indiana SHPO, has invited the INDOT and the City of Fort Wayne to participate in the consultation and to become a signatory/signatories to this memorandum of agreement; and

WHEREAS the FHWA has consulted with the Indiana SHPO in accordance with Section 106 of the National Historic Preservation Act (16 U.S.C. 470f) and its implementing regulations (36 C.F.R. Part 800) concerning the scope of work on March 23, 2009; July 2, 2009; November 9, 2009; December 1, 2009; December 15, 2009; December 28, 2009; February 4, 2010; May 19, 2011; June 17, 2011; July 6, 2011; July 13, 2011; August 15, 2011; August 16, 2011; September 1, 2011; September 2, 2011; September 29, 2011; May 22, 2012; June 20, 2012; July 2, 2012; July 16, 2012, December 18, 2012, and March 1, 2013; and agreed to proceed with the project as proposed April 23, 2009; December 14, 2009; July 5, 2011; November 7, 2011; June 22, 2012; August 13, 2012 and April 1, 2013; and

NOW, THEREFORE, the FHWA and the Indiana SHPO agree that, upon the submission of a copy of this executed memorandum of agreement, as well as the documentation specified in 36 C.F.R. Section 800.11(e) and (f) to the Council pursuant to 36 C.F.R. Section 800.6[b][1][iv]) and upon the FHWA's approval of the State Boulevard Reconstruction from Spy Run to Cass Street, the FHWA shall ensure that the following stipulations are implemented in order to take into account the effect of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

Stipulations

FHWA will ensure that the following measures are implemented:

I. CONTEXT SENSITIVE SOLUTIONS

A. The City of Fort Wayne shall consider and, where feasible, shall implement context sensitive solutions for this undertaking, including but not limited to: the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in parkstrips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.

B. The City of Fort Wayne shall consider and, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences.

C. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:

1. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
2. Context sensitive solutions that may include but not be limited to: protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; and maintaining pedestrian connections along the former Eastbrook and Westbrook drives shall be included among the measures considered.
3. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
4. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.

5. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail or SR 27) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.

6. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

7. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before scheduled meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.

8. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

9. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.

II. PHOTOGRAPHIC DOCUMENTATION

- A. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory

Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges.”

- B. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
 - 1. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - 2. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
- C. This documentation will include black and white prints of digital photographs and a digital video disc (“DVD”) containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the “Indiana DNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards” that are in effect at the time.
 - 1. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;
 - 2. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
 - 3. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
 - 4. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.

III. EDUCATIONAL PROGRAMMING

The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler’s landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps in public projects.

The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within fifteen (15) days, acceptance will be assumed. If the Indiana SHPO or any other consulting party responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

IV. OBJECTION RESOLUTION PROVISION

Disagreement and misunderstanding about how this memorandum of agreement is or is not being implemented shall be resolved in the following manner:

A. If the Indiana SHPO or any invited signatory to this memorandum of agreement should object in writing to the FHWA regarding any action carried out or proposed with respect to the State Boulevard Reconstruction from Spy Run to Cass Street or implementation of this memorandum of agreement, then the FHWA shall consult with the objecting party to resolve this objection. If after such consultation the FHWA determines that the objection cannot be resolved through consultation, then the FHWA shall forward all documentation relevant to the objection to the Council, including the FHWA's proposed response to the objection. Within 45 days after receipt of all pertinent documentation, the Council shall exercise one of the following options:

1. Provide the FHWA with a staff-level recommendation, which the FHWA shall take into account in reaching a final decision regarding its response to the objection; or
2. Notify the FHWA that the objection will be referred for formal comment pursuant to 36 C.F.R. Section 800.7(c), and proceed to refer the objection and comment. The FHWA shall take into account the Council's comments in reaching a final decision regarding its response to the objection.

B. If comments or recommendations from the Council are provided in accordance with this stipulation, then the FHWA shall take into account any Council comment or recommendations provided in accordance with this stipulation with reference only to the subject of the objection. The FHWA's responsibility to carry out all actions under the memorandum of agreement that are not the subjects of the objection shall remain unchanged.

V. POST REVIEW DISCOVERY

In the event that archaeological artifacts (sites), human remains, or one or more historic aboveground properties—other than Fort Wayne Park and Boulevard System Historic District, Brookview-Irvington Park Historic District, and the Bridge over Spy Run—are discovered or that unanticipated effects on historic properties are found during the implementation of this memorandum of agreement, the FHWA shall follow the procedure specified in 36 C.F.R. Section 800.13, as well as and IC 14-21-1-27 and IC 14-21-1-29, by stopping work in the immediate area and informing the Indiana SHPO and the INDOT Cultural Resources Section of such unanticipated discoveries or effects within two (2) business days. Any necessary archaeological investigations will be conducted according to the provisions of IC 14-21-1, 312 IAC 21, and 312 IAC 22, and the most current Guidebook for Indiana Historic Sites and Structures Inventory – Archaeological Sites.

IV. AMENDMENT

Any signatory to this memorandum of agreement may request that it be amended, whereupon the parties shall consult to consider the proposed amendment. 36 C.F.R. 800.6(c)(7) shall govern the execution of any such amendment.

V. TERMINATION

A. If the terms of this memorandum of agreement have not been implemented within five years of the onset of construction, then this memorandum of agreement shall be considered null and void. In such an event, the FHWA shall so notify the parties to this

memorandum of agreement and, if it chooses to continue with the State Boulevard Reconstruction from Spy Run to Cass Street, then it shall reinitiate review of the State Boulevard Reconstruction from Spy Run to Cass Street in accordance with 36 C.F.R. Sections 800.3 through 800.7.

B. Any signatory to the memorandum of agreement may terminate it by providing thirty (30) days notice to the other parties, provided that the parties shall consult during the period prior to termination to seek agreement on amendments or other actions that would avoid termination. In the event of termination, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

C. In the event that the FHWA does not carry out the terms of this memorandum of agreement, the FHWA shall comply with 36 C.F.R. Sections 800.3 through 800.7 with regard to the review of the State Boulevard Reconstruction from Spy Run to Cass Street.

The execution of this memorandum of agreement by the FHWA, the City of Fort Wayne, and the Indiana SHPO, the submission of it to the Council with the appropriate documentation specified in 36 C.F.R. Section 800.11(e) and (f), and the implementation of its terms evidence that the FHWA has afforded the Council an opportunity to comment on the State Boulevard Reconstruction from Spy Run to Cass Street and its effect on historic properties and that the FHWA has taken into account the effects of the State Boulevard Reconstruction from Spy Run to Cass Street on historic properties.

SIGNATORIES (required):

FEDERAL HIGHWAY ADMINISTRATION

Signed by: _____ Date: _____

Name and Title: _____

(Typed or printed)

DRAFT

INDIANA STATE HISTORIC PRESERVATION OFFICER

Signed by: _____ Date: _____

Name and Title: _____

(Typed or printed)

DRAFT

INVITED SIGNATORIES

INDIANA DEPARTMENT OF TRANSPORTATION

Signed by: _____ Date: _____

Name and Title: _____

DRAFT

THE CITY OF FORT WAYNE

Signed by: _____ Date: _____

Name and Title: _____

(Typed or printed)

(If an entity has responsibilities under the MOA, include that entity's name here)

DRAFT

Public Notice

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

The existing reinforced concrete girder, T-beam bridge over Spy Run Creek was constructed in 1927. It is listed as 40.3 feet long, 48.2 feet wide, with a height of 9.5 feet. A.W. Grosvenor and O. Darling are credited as the designers. The July 17, 2006, Structure Inventory and Appraisal Report listed the structure as in poor condition, with a Sufficiency Rating of 27.9 (structurally deficient). The bridge, which is listed as contributing to the proposed Brookview-Irvington Park National Register Historic District, has also been determined eligible for listing in the National Register of Historic Places, according to information in Volume 2 of the Indiana Historic Bridge Inventory. This bridge has been rated as non-select in M&H Architecture, *Indiana Historic Bridge Inventory, Volume 3: Methodology to Identify Select and Non-Select Bridges (draft)*, based on a report by HNTB.

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Before submitting a Letter of Interest (LOI), any interested agency, organization or individuals are encouraged to visit INDOT's Historic Bridges Marketing Program (<http://www.in.gov/indot/2967.htm>) or contact American Structurepoint, Inc. (contact information below), in order to obtain a copy of the Structure Inventory and Appraisal Report. LOIs for this bridge will be accepted prior to and up to 15 days following the public hearing. If no recipient is identified or selected, the bridge will be demolished following the PA among FHWA, INDOT, SHPO, and ACHP for the Management and Preservation of Indiana's Historic Bridges, Attachment B, Standard Treatment Approach for Historic Bridges, Demolition.

For more information, or to submit an LOI, please contact:

Hayley Steele, Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
Telephone: (317) 547-5580
hsteele@structurepoint.com

The Journal Gazette

Allen County, Indiana

Account # 1064183 - 794026
American Structurepoint Inc

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of the type in which the body of the
advertisement is set) -- number of equivalent lines _____

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Total number of lines in notice

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COMPUTATION OF CHARGES

118 lines, 1 column(s) wide equals

118 equivalent lines at \$ 0.371 cents per line

\$ 43.78

Additional charges for notices containing rule or tabular work
(50 per cent of above amount) -

Charge for extra proofs of publication
(\$1.00 for each proof in excess of two) -

TOTAL AMOUNT OF CLAIM

\$ 43.78

DATA FOR COMPUTING COST

Width of single column in picas 9.8 Size of type 7point.

Number of Insertions 1

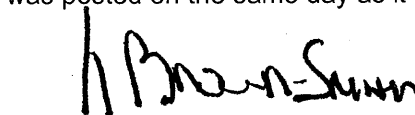
Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 1 times.

The dates of publication being as follows:

2/5/2010

Additionally, Newspaper has a Web site and this public notice was posted on the same day as it was published in The Journal Gazette.



T. Brown-Smith
Legal Clerk

Date: February 5, 2010

PUBLIC NOTICE

The City of Fort Wayne, Indiana, Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. The approximate 0.46-mile project consists of widening the existing two-lane section of State Boulevard between Clinton and Cass Street to five lanes while correcting the substandard horizontal curve. In order to correct the substandard curve, it is proposed the existing bridge over Spy Run Creek be demolished and a new bridge be constructed.

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794026

The News-Sentinel

Allen County, Indiana

Account # 1064183 - 794026
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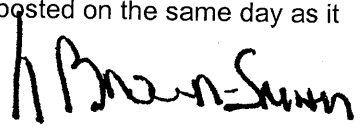
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T. Brown-Smith
Legal Clerk

Date: February 5, 2010

ATTACH COPY OF ADVERTISEMENT HERE

PUBLIC NOTICE

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794026

Appendix D: Red Flag and Hazardous Materials

- Red Flag Investigation – April 26, 2013
- Hazardous Materials Form
- Executive Summary Initial Site Assessment



AMERICAN
STRUCTUREPOINT
INC.

7260 Shadeland Station, Indianapolis, Indiana 46256

TEL 317.547.5580 FAX 317.543.0270

www.structurepoint.com

M E M O R A N D U M

DATE: April 26, 2013
TO: City of Fort Wayne
City County Building
One Main Street
Fort Wayne, Indiana 46802
FROM: Allison M. Barton
RE: Des. No. 0400587
State Boulevard Reconstruction
Fort Wayne, Wayne Township, Allen County, Indiana
Project No. 2007.1404
CC: Briana Hope

Narrative

The purpose of the Red Flag Investigation (RFI) is to provide an overview of environmental conditions and constraints within the proposed project study area and define areas for additional studies or further environmental consideration. The RFI consists of a review of readily available Geographic Information System (GIS) data layers provided by IndianaMap and the Indiana Geological Survey and additional data sources, including the County Interim Reports and the Indiana Natural Heritage Database. Records for infrastructure, environmental sites and hazardous materials, natural resources and hydrology, geology, and historical resources are reviewed within a one-half-mile radius around the proposed project study area.

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

SUMMARY

Infrastructure

Indicate items of concern found within one-half mile, including an explanation why each item within the one-half mile radius will/will not impact the project:

Of the ten records noted within one-half mile of the project, three have the potential to impact the project. The Rivergreenway North to Fernhill Trail runs north-south through the western portion of the project area. State Boulevard to Pemberton Avenue Trail intersects with the eastern terminus of the project. The Rivergreenway Trail also intersects with the eastern terminus of the project. If right-of-way is acquired from these resources, additional coordination will be required.

Other Road Projects	N/A	Airports	N/A
Cemeteries	N/A	Hospitals	N/A
Railroads	1	Recreational Facilities	6
Religious Facility	N/A	Schools	N/A
Trails	3	Pipelines	N/A

Explanation:

Railroads

- One abandoned rail line runs north-south near the western terminus of the project area.

Recreational Facilities

- Bob Arnold North side Park – at East State Street and Parnell Avenue, feature located approximately 1,500 feet east of the project area.
- North side School– at 475 East State Street, feature located approximately 400 feet east of the project area.
- Versey Park– at 3201 Irvington Drive, feature located approximately 2,700 feet north of the project area
- YWCA– at 200 North Wells Street, feature located approximately 1,500 feet south of project area.
- Lawton Park– at 1900 North Clinton Street, feature located approximately 2,200 feet south of project area.
- Zeis Park– at 1700 Spy Run Avenue, feature located approximately 1,900 feet south of project area

Trails

- Rivergreenway North to Fernhill Trail – feature runs north-south through the western portion of the project area.
- State Boulevard to Pemberton Avenue Trail – feature intersects with the eastern project terminus.
- The Rivergreenway Trail – feature intersects with the eastern terminus of the project area.

Water Resources

Indicate items of concern found within one-half mile, including an explanation why each item within the one-half mile radius will/will not impact the project:

Nine water resource records were identified within one-half mile of the proposed project. Spy Creek Aqueduct runs through the project area. Spy Run Creek flows through the project area. A wetland delineation was conducted and did not indicate the presence wetlands.

Canal Routes – Historic	1	Canal Structures – Historic	1
Wetland Line	1	Floodplain-DFIRM	N/A
Lakes/Ponds	1	Wetlands	1
Wetland Points	N/A	Lakes – Impaired*	N/A
Streams – Impaired*	2	Cave Entrance Density	N/A
Sinkhole Areas	N/A	Karst Springs	N/A
Rivers/Streams	2	Sinking-Stream Basins	N/A

* Reason for impairment, if applicable:

Explanation:

Canal Structures (Historic)

- The Spy Creek Aqueduct is located approximately 300 feet south, outside the project area.

Canal Routes- Historic

- Saint Joe Feeder Dam runs through the project area.

Impaired Streams

- Spy Run Creek flows through the project area.
- St. Joseph River flows immediately adjacent to the eastern project terminus.

Lake/Ponds

- One lake is located approximately 1,200 feet south, outside the project area.

Streams

- One stream (Spy Run) flows through the project area.
- St. Joseph River flows immediately adjacent to the eastern project terminus.

Wetland Lines

- One riverine, lower perennial, unconsolidated bottom, permanently flooded (R2UBH) wetland line runs through the project area.

Wetlands

- One riverine, lower perennial, unconsolidated bottom, permanently flooded (R2UBH) wetland is located adjacent to the east of the project area.

Mining/Mineral Exploration

Indicate items of concern found within one-half mile, including an explanation why each item within the one-half mile radius will/will not impact the project:

No mining/mineral exploration records were identified within one-half mile of the proposed project.

Oil Wells	N/A	Gas Wells	N/A
Mines – Surface	N/A	Mines – Underground	N/A
Petroleum Fields	N/A	Other Petroleum Wells	N/A
Sand/Gravel Pits	N/A		

Explanation:

Ecological Information

Coordination with the United States Fish and Wildlife Service (USFWS) in a letter dated April 20, 2009, indicated the project is within the range of the federally endangered Indiana bat (*Myotis catenatus*) and the candidate eastern massasauga rattlesnake (*Sistrurus catenatus*). However, USFS service indicated there is no habitat for either species within the project area and the project is not likely to impact the noted species.

Cultural Resources

A full and comprehensive Historic Properties Review has been prepared for the area surrounding the proposed project.

Hazardous Material Concerns

Indicate items of concern found within one-half mile, including an explanation why each item within the one-half mile radius will/will not impact the project:

A total of 46 hazardous material concerns records were identified within one-half mile of the project. A Phase I Initial Site Assessment (ISA) was recommended for the project study area and has been completed. No recognized environmental conditions (RECs) were identified and no further investigations were deemed necessary.

Confined Feeding Operation	N/A	Construction Demolition Waste	N/A
Industrial Waste Sites	2	Leaking UG Storage Tanks	13
Open Dump Waste Sites	N/A	NPDES Pipe Locations	N/A
NPDES Facilities	N/A	Corrective Active Sites	N/A
Restricted Waste Sites	N/A	Septage Waste Sites	N/A
Solid Waste Landfills	N/A	Superfund Sites	N/A
Tire Waste Sites	N/A	Underground Storage Tanks	28
Voluntary Remediation Program	N/A	Brownfields	N/A
Waste Transfer Stations	N/A	Waste Treatment Storage Disposal	N/A
Manufactured Gas Plant	N/A	State Cleanup Site	2
Etiological Waste Site	N/A	Lagoon	N/A
IDEM 303d Listed Streams*	N/A	IDEM 303d Listed Rivers*	N/A
IDEM 303d Listed Lakes*	N/A	Institutional Control	1

* Reason for impairment, if applicable:

Explanation:

Institutional Control Sites

- Circle K – 2304 Sherman Boulevard, located approximately 1,600 feet west of the project area.

Industrial Waste Sites

- Valspar Coatings – 202 Jacobs Avenue, located adjacent to the project area.
- Kelly Chevrolet, Inc. – 500 East State Street, located approximately 450 feet east of the project area.

Leaking Underground Storage Tanks

- Clark Oil & Refining # 0736 – 3113 North Clinton Avenue, located approximately 2,600 feet north of the project area.
- Speedway #7070 – 2111 Sherman Boulevard, located approximately 2,100 feet west of the project area.
- Clark Oil & Refining # 0499 – 2304 Sherman Street, located approximately 1,800 feet west of the project area.
- Lawton Park Maintenance – 1900 North Clinton Street located approximately 2,000 feet south of the project area.
- Dairy Queen – 2218 Sherman Drive, located approximately 2,000 feet west of the project area.

- Colwell General, Inc. – 1701 North Harrison, located approximately 1,400 feet south of the project area.
- Wood Youth Center – 2929 Wells Street, located approximately 2,000 feet north of the project area.
- Hivley’s VW Service, Inc. – 630 West State Boulevard, located approximately 950 feet west of the project area.
- Omnisource Corporation – 1610 North Calhoun Street, located approximately 2,000 feet south of the project area.
- Kroger – 324 East State Street, located within the project area.
- Lassus Brothers Oil Handy Dandy # 14 – 2218 Sherman Drive, located approximately 2,700 feet south of the project area.
- Rothele Building Materials – 3000 Wells Street, located approximately 2,500 feet north of the project area.
- Marathon Unit 1272 – State and Wells, located adjacent to the project area.

State Cleanup Sites

- North Side Highschool – 475 East State Street, feature located approximately 400 feet east of the project area.
- Fort Wayne YWCA – at 200 North Wells Street, feature located approximately 1,500 feet south of project area.

Underground Storage Tank

- Clark Oil & Refining # 0736 – 3113 North Clinton Avenue, located approximately 2,600 feet north of the project area.
- Speedway #7070 – 2111 Sherman Boulevard, located approximately 2,100 feet west of the project area.
- Clark Oil & Refining # 0499 – 2304 Sherman Street, located approximately 1,800 feet west of the project area.
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- Dairy Queen – 2218 Sherman Drive, located approximately 2,000 feet west of the project area.
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- Kroger – 324 East State Street, located within the project area.
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- Rothele Building Materials – 3000 Wells Street, located approximately 2,500 feet north of the project area.
- Marathon Unit 1272 (feature listed twice) – State and Wells, located adjacent to the project area.
- Christon Nakos – 3126 North Clinton Street, located approximately 2,500 feet north of the project area.

- Park Center, Inc. – 909 East State Boulevard, located approximately 2,200 feet east of the project area.
- Kaman Bearing & Supply – 3025 Wells Street, located approximately 2,600 feet north of the project area.
- Rodenbeck Motor Sales – 1607 Wells Street, located approximately 2,000 feet south of the project area.
- Sunset Motors – 3102 North Clinton Street, located approximately 2,500 feet north of the project area.
- Umber’s Auto Service – 2821 Parnell Avenue, located approximately 2,300 feet east of the project area.
- Paul R. Hosler, Inc. – 215 West State Boulevard, located within the project area.
- Ace Radiator Work – 2119 North Clinton Street, located approximately 300 feet east of the project area.
- Kelly Wrecker Service – 2416 Cass Street, located approximately 2,200 feet east of the project area.
- Valspar Corporation – 202 Jacobs Avenue, located adjacent to the project area.
- Fort Wayne Service Center – 909 East State Boulevard, located approximately 700 feet south of the project area.
- Kelly Chevrolet – 500 East State Boulevard, located approximately 700 feet east of the project area.

Recommendations

Infrastructure

Of the ten records noted within one-half mile of the project, three have the potential to impact the project. The Rivergreenway North to Fernhill Trail runs north-south through the western portion of the project area. State Boulevard to Pemberton Avenue Trail intersects with the eastern terminus of the project. The Rivergreenway Trail also intersects with the eastern terminus of the project. If right-of-way is acquired from these resources, additional coordination will be required.

Water Resources

A total of nine water resource records were identified within one-half mile. Spy Creek Aqueduct runs through the project area. Spy Run Creek also flows through the project area. A wetland delineation was previously conducted and did not indicate the presence of any wetlands on or adjacent to the project area.

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No mining/mineral exploration records were identified within one-half mile of the proposed project.

Ecological Information

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Cultural Resources

A full Historic Properties Review has been completed as part of the overall project development.

Hazardous Material Concerns

A Phase I Initial Site Assessment (ISA) was recommended for the project study area and has been completed. No recognized environmental conditions (RECs) were identified and no further investigations were deemed necessary.

Graphics

A map for each report section with a one-half mile radius buffer around all project area(s) showing all items identified as possible items of concern is attached.

— Project Area
 One Half Mile Buffer

0 500 1,000 2,000 Feet



FRANKE PARK

SHERMAN ST. RIVER GREENWAY

NOTHING FOUND

Natural and Ecological Sites

- | | | |
|--|---|--|
| ● Stream Gauging Stations (USGS) | ● Champion Trees (IDNR) | National Natural Landmarks (USGS) |
| ● Natural Features (USGS) | — Outstanding Rivers (NRC) | Managed Land (IDNR) |

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State Boulevard Reconstruction

Des. No.:0400587



Data displayed on this map was provided by the IndianaMap Project. This information is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

— Project Area
 — One Half Mile Buffer

0 500 1,000 2,000
 Feet



Hazardous Materials Concern

- | | | | |
|---------------------------------|------------------------------|------------------------------------|-------------------------------|
| ● Confined Feeding Operation | ● NPDES Facility | ▲ UG Storage Tanks | ▲ Cleanup Sites (IDEM) |
| ■ Construction Demolition Waste | ■ Restricted Waste Sites | ■ VRP Sites | ■ Corrective Action Sites |
| ▲ Industrial Waste Sites | ▲ Septage Waste Sites | ■ Brownfields | ■ Manufactured Gas Plants |
| ■ Leaking UG Storage Tanks | ● Solid Waste Sites (Active) | ■ Waste Transfer Stations | ■ Old Landfills |
| ● Open Dump Waste Sites | ■ Superfund Sites | ● Waste Treatment Storage Disposal | ■ Institutional Control Sites |
| ▲ NPDES Pipe Locations | ■ Tire Waste Sites | | |

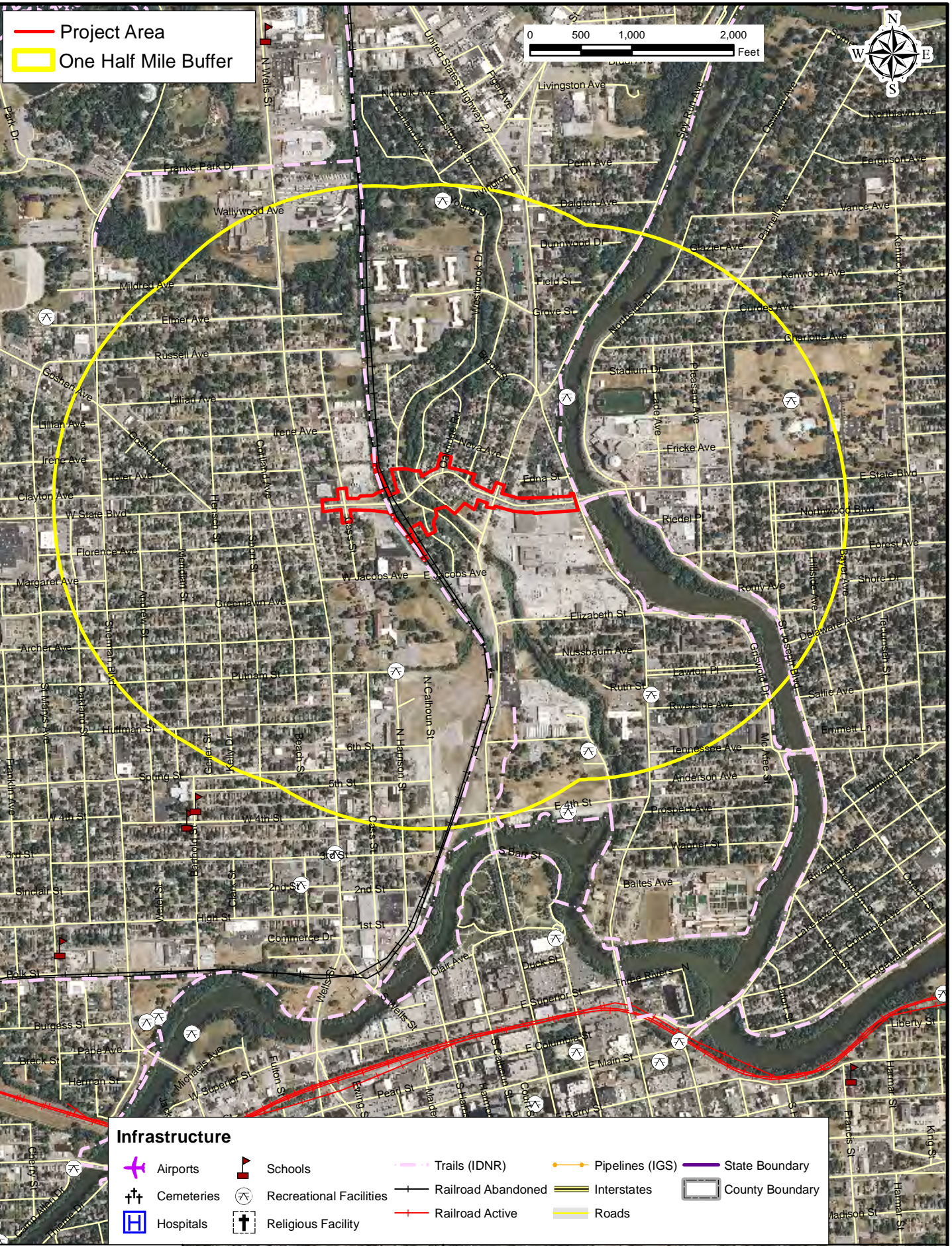


State Boulevard Reconstruction

Des. No.:0400587



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— Project Area
 One Half Mile Buffer

0 500 1,000 2,000 Feet



Infrastructure

- | | | | | |
|------------|-------------------------|--------------------|-----------------|-----------------|
| Airports | Schools | Trails (IDNR) | Pipelines (IGS) | State Boundary |
| Cemeteries | Recreational Facilities | Railroad Abandoned | Interstates | County Boundary |
| Hospitals | Religious Facility | Railroad Active | Roads | |

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State Boulevard Reconstruction

Des. No.:0400587



Data displayed on this map was provided by the IndianaMap Project. This information is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

- Project Area
- One Half Mile Buffer

0 500 1,000 2,000 Feet



NOTHING FOUND

Mining/Mineral Exploration

- Abandoned Gas Storage Wells
- Gas Wells
- Gas Storage Wells
- Oil Wells
- Other Petroleum Test Wells
- Industrial Mineral Site (Active 2001)
- Sand and Gravel Pits - Abandoned
- Quarries - Abandoned
- Petroleum Fields
- Mines - Surface
- Mines - Underground



State Boulevard Reconstruction

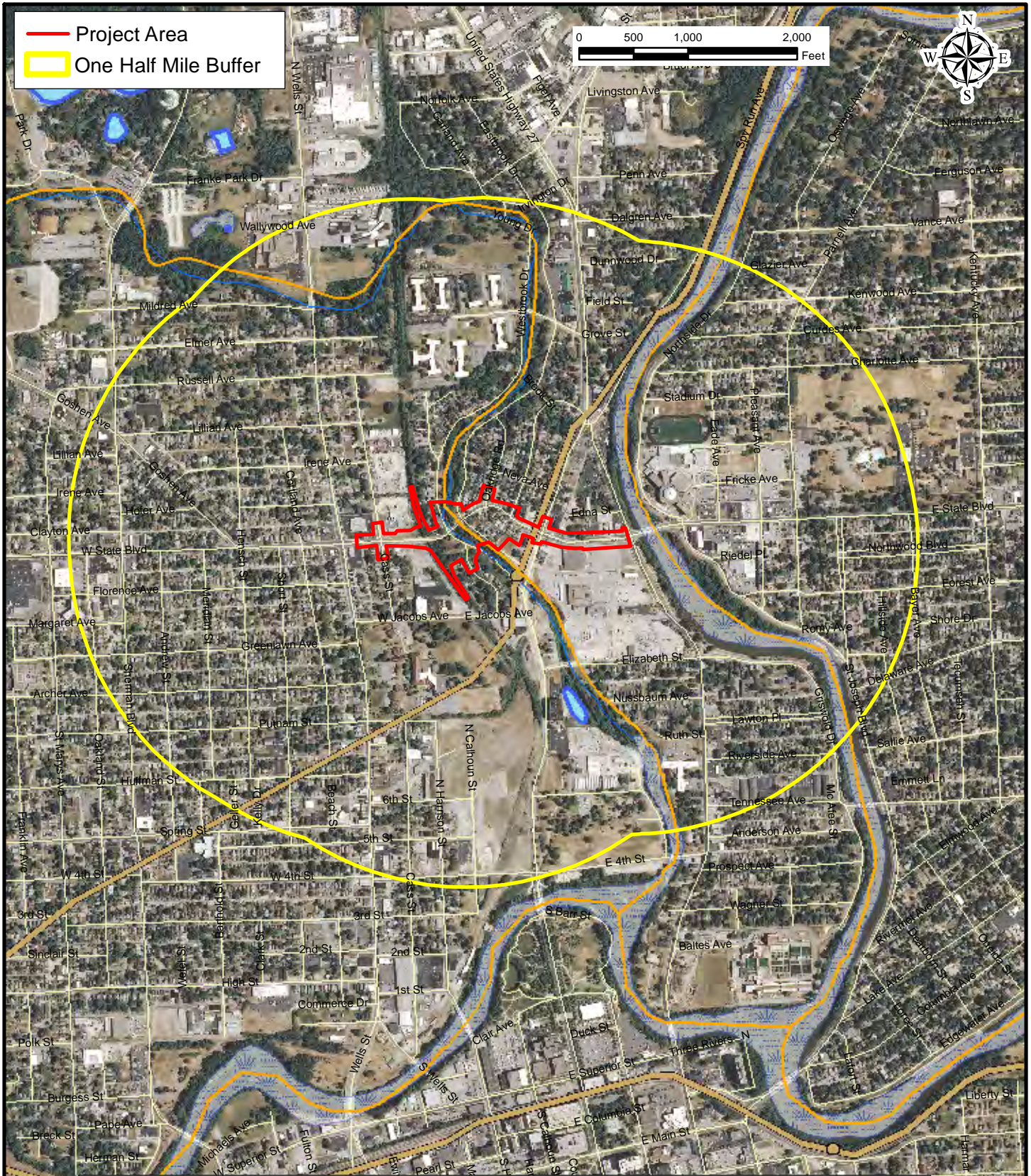
Des. No.:0400587



Data displayed on this map was provided by the IndianaMap Project. This information is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

— Project Area
 — One Half Mile Buffer

0 500 1,000 2,000
 Feet



Water Resources

- | | | | |
|-------------------------------|---------------------------|------------------------------------|-------------------------|
| ● Canal Structures - Historic | — Canal Routes - Historic | ■ Lakes | ■ Sink Hole Area |
| ● Wetland Points (NWI) | — Rivers | ■ Floodplains (DFIRM) | ■ Sinking Stream Basin |
| ▲ Karst Springs | — Wetland Line | ■ National Wetland Inventory* | ■ Cave Entrance Density |
| — Streams - Impaired** | ■ Lakes - Impaired** | # No. of Cave Entrances per Sq.Mi. | |

*Class R2 (Riverine- Lower Perennial) and L1 (Lacustrine- Limnetic) not displayed; Includes 2011 NWI Update ** Based on 2006 IDEM 303(d) Listings



State Boulevard Reconstruction

Des. No.:0400587



Data displayed on this map was provided by the IndianaMap Project. This information is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Appendix D

Page 14 of 17

HAZARDOUS MATERIALS SITE VISIT FORM

Des # 0400587

Road # State Boulevard

Type of Road Project Reconstruction

Description of area (either general location or exact location of parcel) State Boulevard – State Boulevard between Spy Run and Cass Street including the bridge over Spy Run

Person completing this Field Check Briana M. Hope

1. **Has a Red Flag Investigation been completed?** ☐ Yes ☒ No

Notes:

A Red Flag Investigation (RFI) was completed for the proposed project on April 26, 2013. The purpose of the RFI is to provide an overview of environmental conditions and constraints within the proposed project study area and define areas for additional studies or further environmental consideration. The RFI consists of a review of readily available Geographic Information System (GIS) data layers provided by IndianaMap and the Indiana Geological Survey and additional data sources including the Indiana Department of Environmental Management UST and LUST lists, County Interim Reports, and the Indiana Natural Heritage Database. Records for infrastructure, environmental sites and hazardous materials, natural resources and hydrology, geology, and historical resources are reviewed within a half-mile radius around the proposed project study area.

The RFI identified 46 sites for hazardous materials concerns and recommend a Phase I Site Assessment (ISA).

2. Right-of-Way Requirements:

☐ No New ROW ☒ Strip ROW ☐ Minor Take ☒ Whole Parcel Take ☐ Information Not Available

Notes:

Approximately 3.80 acres of permanent ROW and 2.50 acre of temporary ROW will need to be acquired for the proposed project. A total of 15 relocations are expected to be required for the proposed project.

3. **Land Use History and Development:** (Industrial, Light Industry, Commercial, Agricultural, Residential, Other – also, indicate source of data: visual inspection, aerial photos, U.S.G.S. topo maps, etc.)

Setting (rural or urban): Urban

Current Land Uses: Commercial and Residential

Previous Land Uses: Commercial and Residential

Adjacent Land Uses: Commercial and Residential

Describe any structures on the property: The project corridor is lined with both commercial structures and single family residential structures.

4. Visual Inspection:	Property	Adjoining Property		Property	Adjoining Property
Storage Structures:			Evidence of Contamination:		
Underground Tanks	_____	X	Junkyard	_____	_____
Surface Tanks	_____	_____	Auto Graveyard	_____	_____
Transformers	_____	X	Surface Staining	_____	_____
Sumps	_____	_____	Oil Sheen	_____	_____
Ponds/Lagoons	_____	_____	Odors	_____	_____
Drums	_____	_____	Vegetation Damage	_____	_____
Basins	_____	_____	Dumps	_____	_____
Landfills	_____	_____	Fill Dirt Evidence	_____	_____
Other	_____	_____	Vent pipes or fill pipes	_____	_____
			Other	_____	_____

5. Is a Phase I, Initial Site Assessment required? ☒ Yes ☐ No

An ISA was completed for the proposed project on November 11, 2011. The ISA was prepared in general accordance with procedures outlined in the Hazardous Materials Unit Operating Manual published by INDOT – Office of Environmental Services. A total of five sites were assessed as being potential sources of hazardous materials that may affect the proposed project. Based on this review of the environmental database search, IDEM site files, and field investigation, no RECs were identified at any of the five sites. No further investigation of the project area was warranted.

Section 1 – Executive Summary

A Phase I Initial Site Assessment (ISA) is a review of information about past property use to determine whether environmental contamination may be present. The ISA may be initiated if the Red Flag Investigation (RFI) demonstrates possible or likely contamination in the immediate area of the project, or if the project involves excavation or new right-of-way purchase, even though the RFI showed no immediate environmental concerns upon review.

The City of Fort Wayne is developing a federal-aid project to improve State Boulevard from Spy Run (US 27) to Cass Street. The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes while correcting the substandard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center 2-way left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive. The total project length is approximately 2,300 feet.

Pursuant to Section 3.2.74 of ASTM Standard E 1527-05, Recognized Environmental Conditions (RECs) are defined as the “presence or likely presence of hazardous substances or petroleum products on a property under conditions that indicate[d] an existing release, past release, or a material threat of release of any hazardous substances or petroleum products into structures on the property or into the ground, ground water, or surface water of the property.” RECs were generally assigned to properties on, or adjacent to the proposed project with documented or observed contamination, or properties with past land uses suggesting the potential for release. A total of five sites were assessed, with no sites identified as having an REC. Because no RECs were identified, no additional investigations are necessary. Table 1 lists those properties addressed as part of the ISA.

Table 1

Site ID	Address	Site Name	REC	Additional Investigation Recommended
1	215 West State Boulevard	Ink Spot Printing	None	No
2	324 East State Boulevard	Kroger	None	No
3	310 West State Boulevard	Townsend and Pratt Auto Sales	None	No
4	2230 North Clinton Street	Lassus Brothers Oil Handy Dandy	None	No
5	2522 Cass Street	Superior Collision	None	No

Appendix E: Water Resources

- Ecological Evaluation Form
- Hydraulic Summary
- INDOT Hydraulics Approval (May 13, 2010)

ECOLOGICAL EVALUATION FORM

Road: State Boulevard Des. No: 0400587 County: Allen
Project Description: State Boulevard Reconstruction – State Boulevard between Spy Run and Cass Street including the bridge over Spy Run
Project Location: Fort Wayne, Allen County, Indiana
Natural Region and Section: Eastern Corn Belt Plains – Clayey High Lime Plains
8-Digit Watershed: 04100004 and 04100003 USGS Quadrangle: East Fort Wayne and West Fort Wayne
Soil Survey Map Sheet: 50

RIGHT-OF-WAY BY LAND USE TYPE

Permanent Right-of-way

Land Use Type	R/W (ha)	R/W (ac)
Commercial	0.43	1.06
Industrial	0.00	0.00
Residential	0.89	2.19
Agricultural	0.00	0.00
Wooded	0.00	0.00
Other: Park	0.22	0.55
Total Perm R/W	1.54	3.80

Temporary Right-of-way

Land Use Type	R/W (ha)	R/W (ac)
Commercial	0.23	0.57
Industrial	0.00	0.00
Residential	0.73	1.81
Agricultural	0.00	0.00
Wooded	0.00	0.00
Other: Park	0.05	0.12
Total Temp R/W	1.01	2.5

Is the project located in an urban or a rural setting? Urban
Is land use in the project changing? Yes No If yes, explain: _____

QUADRANT DESCRIPTION

Northeast Residential/Commercial
Northwest Residential/Commercial
Southeast Residential/Commercial
Southwest Residential/Commercial

STREAM INFORMATION

	Width	Depth	Maximum Depth
Bank Full Channel	45 feet	8 feet	8 feet
Ordinary High Water Mark	31 feet	1.5 feet	1.5 feet

Substrate Material: (circle one) silt sand gravel loose rock bedrock
Flow Velocity: (circle one) stagnant slow moderate swift rapid
Does the stream contain riffle/pool complexes? Yes No
Does the stream contain meanders within the proposed right-of-way? Yes No
Is channel work proposed as part of this project? Yes No If yes, describe: _____
Is aquatic flora present? Yes No If yes, please list: Reed Canary Grass
Is aquatic fauna present? Yes No If yes, please list: Aquatic insects
Comments: _____

TERRAIN

Immediate Area: Depressed Flat Gently Rolling Rolling Hilly
Extended Area: Depressed Flat Gently Rolling Rolling Hilly

TERRESTRIAL WILDLIFE

Fauna Observed or Indicated

Family ¹	Common Name	Scientific Name	Indication ²

¹Mammal, Bird, Reptile, or Amphibian

²Observed Animal, Tracks, Scat, Homes, and/or Markings

Dominant Flora Observed

Strata ¹	Common Name	Scientific Name	Wetland Indicator ²	Location ³
Herbaceous	Kentucky blue grass	<i>Poa pratensis</i>	FAC	Upland
Overstory	common hackberry	<i>Celtis occidentalis</i>	FAC	Upland/Floodplain
Overstory	black walnut	<i>Juglans nigra</i>	FACU	Upland/Floodplain
Understory	Japanese honeysuckle	<i>Lonicera japonica</i>	FAC	Upland/Floodplain
Understory	sugar maple	<i>Acer saccharum</i>	FACU	Upland/Floodplain
Herbaceous	crabgrass	<i>Digitaria sanguinalis</i>	FACU	Upland/Floodplain
Herbaceous	broadleaf plantain	<i>Plantago major</i>	FAC	Upland/Floodplain
Herbaceous	white clover	<i>Trifolium repens</i>	FACU	Upland/Floodplain
Herbaceous	summer grape	<i>Vitis aestivalis</i>	FACU	Upland/Floodplain

¹Overstory, Understory, Vine, or Herbaceous

²UPL, FACU-, FACU, FACU+, FAC-, FAC, FAC+, FACW-, FACW, FACW+, or OBL

³Floodplain, Depression, or Upland

SOILS INFORMATION

Abbreviation	Soil Name	Soil Texture	Drainage Class ¹	Hydric Soil Status ²	Location ³
MrB	Morley	silt loam	MWD	NH	Upland
Wh	Washtenaw	silt loam	PD	H	Floodplain

¹ED-Excessively Drained, WD-Well Drained, MWD-Moderately Well Drained, SWPD-Somewhat Poorly Drained, PD-Poorly Drained, VPD-Very Poorly Drained

²H-Hydric Soil, HI-Contains Hydric Inclusions, NH-Non-Hydric

³Floodplain, Depression, or Upland

ENDANGERED AND THREATENED SPECIES

Is this project located within the range of any Federally Endangered or Threatened Species? Yes No

If yes, please list below.

Common Name	Scientific Name	Status	Confirmed Occurrences Nearby?	Suitable Habitat Present
Indiana bat	<i>Myotis sodalis</i>	Endangered	No	Yes No
Massasauga rattlesnake	<i>Sistrurus catenatus</i>	Candidate	No	Yes No
				Yes No

Will any of the above listed species be impacted by the planned improvements? Yes No

NATURAL AREAS

Are there any natural areas located within 5 miles of the project area? Yes No

If yes, please list below.

Property Name	Ownership	Proximity to Project

Will any of the above listed properties be impacted by the planned improvements? Yes No

WETLAND INFORMATION

Are wetlands mapped within or adjacent to project limits? Yes No

If yes, please list below.

Wetland Type	Abbreviation	Location within Project	Confirmed in Field?		
			Yes	No	Undetermined
			Yes	No	Undetermined
			Yes	No	Undetermined

Were any of the following wetland indicators observed in or adjacent to project limits?

	Yes	No	Location within Project
Standing Water	___	<u>X</u>	_____
Saturated Soil	___	<u>X</u>	_____
Depressional Areas	___	<u>X</u>	_____
Water Marks on Trees	___	<u>X</u>	_____
Drift Lines	___	<u>X</u>	_____
Fluted Tree Trunks/Roots	___	<u>X</u>	_____
Sediment Deposits	___	<u>X</u>	_____
Water Stained Leaves	___	<u>X</u>	_____
Other _____	___	<u>X</u>	_____

Is there a potential for impacts to jurisdictional wetlands as a result of the planned improvements? Yes No

Comments: _____

GENERAL PROJECT COMMENTS

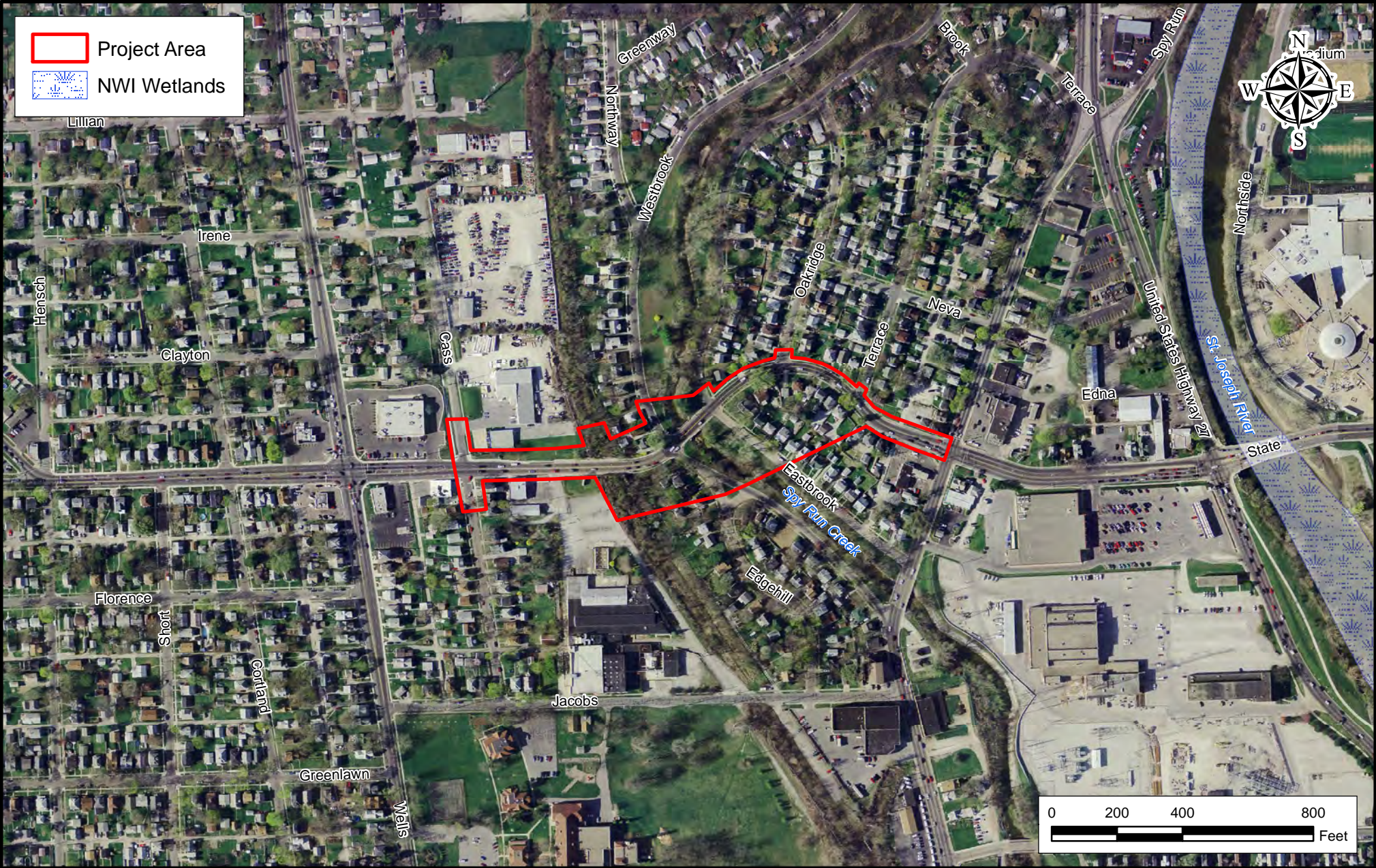
Preliminary investigation of available data by American Structurepoint revealed the presence of Spy Run within the project area. National Wetland Inventory Mapping revealed no mapped wetlands within the project area. The project is within the mapped FEMA 100-year floodplain for Spy Run and the St. Joseph River. The USGS topographic map also noted Spy Run in the project area. Topographic mapping noted the area as heavily developed (red shading) with intermixed commercial and industrial properties (black and purple polygons).

American Structurepoint staff visited the site on August 14, 2009, to evaluate the potential presence of wetlands and other jurisdictional waters and delineate their boundaries. No wetlands were identified in the area. Spy Run was identified in the field. Spy Run will be crossed once within the proposed project area by the newly aligned State Boulevard. The stream flows southerly through the project area before emptying into the Maumee River 0.85 mile southwest of the project. The wetland delineation was conducted in accordance with the *Corps of Engineers Wetland Delineation Manual* (1987) and *Interim Regional Supplement to the US Army Corps of Engineers Wetland Delineation Manual: Midwest Region* (2008).

ATTACHMENTS

NWI Map
Soils Map
FEMA Floodplain Map

Performed by: Briana M. Hope
Date: April 24, 2013

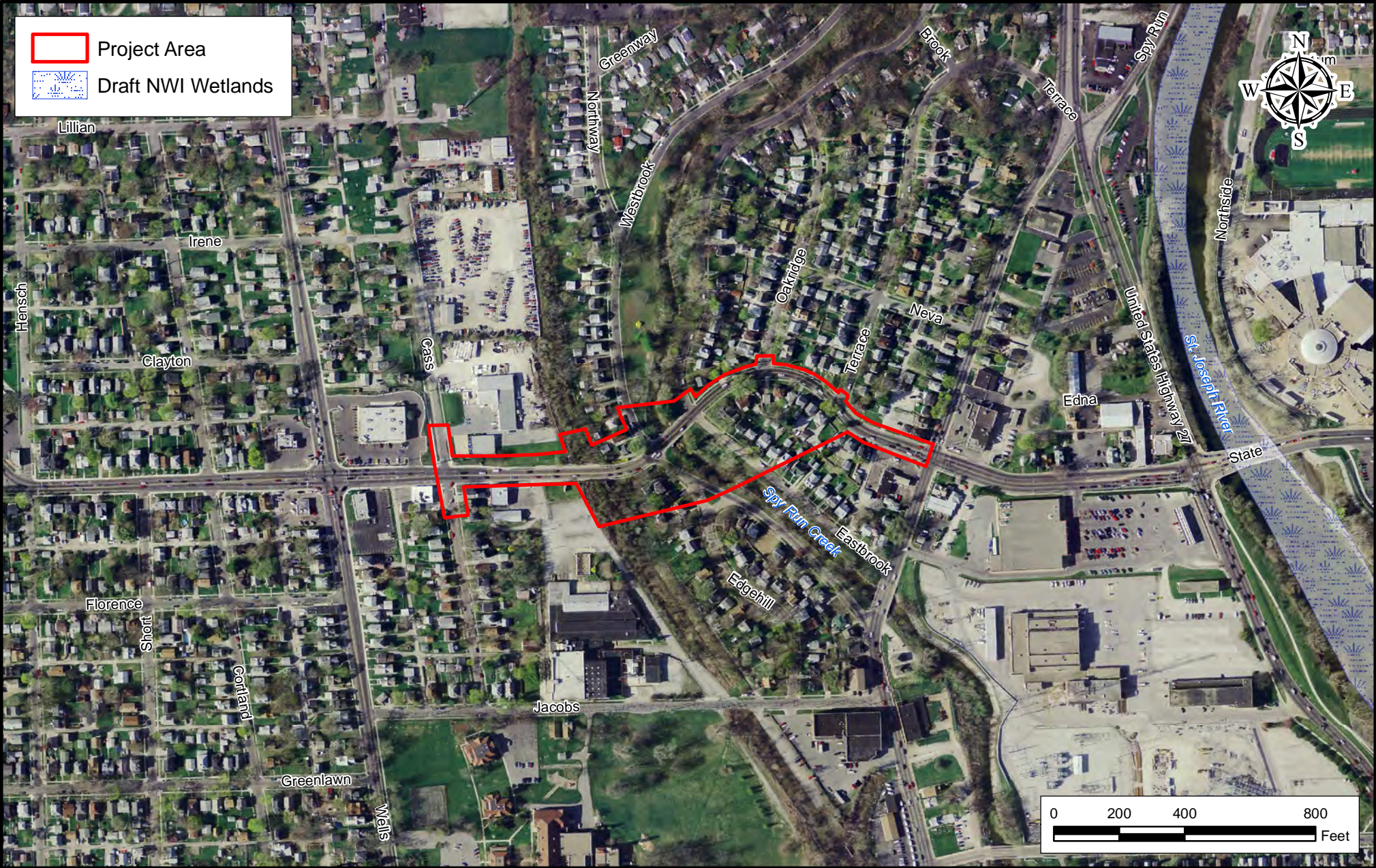


National Wetland Inventory Mapping
US Fish and Wildlife Service Data

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana
Date: 07/21/2010
Appendix E
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Ducks Unlimited Draft NWI Mapping

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana
Date: 07/21/2010
Appendix E
Page 6 of 19



AMERICAN
STRUCTUREPOINT
INC.

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SSURGO Digital Soil Map

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 07/21/2010

Appendix E
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Map Unit Legend

Allen County, Indiana

Map symbol	Map unit name
BeB	Belmore fine sandy loam, 2 to 6 percent slopes
BhA	Belmore loam, 0 to 2 percent slopes
BhB	Belmore loam, 2 to 6 percent slopes
BkA	Berrien loamy fine sand, moderately fine substratum, 0 to 2 percent slopes
BIA	Blount loam, 0 to 2 percent slopes
BmA	Blount silt loam, 0 to 2 percent slopes
BmB	Blount silt loam, 2 to 6 percent slopes
BmB2	Blount silt loam, 2 to 6 percent slopes, moderately eroded
Bn	Bono mucky silty clay
Bo	Bono silty clay
Bp	Borrow pits
Br	Brookston silt loam
Bs	Brookston silty clay loam
Ca	Houghton muck, drained, 0 to 1 percent slopes
ChB	Chelsea fine sand, 2 to 6 percent slopes
ChC	Chelsea fine sand, 6 to 12 percent slopes
ChD	Chelsea fine sand, 12 to 18 percent slopes
CrA	Crosby loam, 0 to 2 percent slopes
CsA	Crosby silt loam, 0 to 2 percent slopes
CsB	Crosby silt loam, 2 to 6 percent slopes
CsB2	Crosby silt loam, 2 to 6 percent slopes, moderately eroded
Dr	Del Rey silt loam
Ee	Eel loam
Es	Eel silt loam
FmA	Fox loam, 0 to 2 percent slopes
FmB	Fox loam, 2 to 6 percent slopes
FmC2	Fox loam, 6 to 12 percent slopes, moderately eroded
Ge	Genesee loam
Gh	Genesee silt loam
Gm	Genesee silty clay loam
Gn	Genesee fine sandy loam, sandy variant
Go	Gilford fine sandy loam
HaA	Haskins loam, 0 to 2 percent slopes
HaB	Haskins loam, 2 to 6 percent slopes
HnA	Whitaker fine sandy loam, 0 to 2 percent slopes
HoA	Whitaker loam, 0 to 2 percent slopes
HoB	Whitaker loam, 2 to 6 percent slopes
HpA	Whitaker silt loam, 0 to 2 percent slopes
Hs	Hoytville silty clay
Le	Lenawee mucky silty clay loam
Ls	Lenawee silty clay loam
Lw	Palms muck, drained, 0 to 1 percent slopes
Ma	Made land
McA	Martinsville loam, 0 to 2 percent slopes
McB	Martinsville loam, 2 to 6 percent slopes
McB2	Martinsville loam, 2 to 6 percent slopes, moderately eroded
McC2	Martinsville loam, 6 to 12 percent slopes, moderately eroded
MeA	Martinsville loam, gravelly substratum, 0 to 2 percent slopes
MeB	Martinsville loam, gravelly substratum, 2 to 6 percent slopes

Map Unit Legend

Allen County, Indiana

Map symbol	Map unit name
MfA	Martinsville silt loam, 0 to 2 percent slopes
MgC3	Martinsville soils, 6 to 12 percent slopes, severely eroded
Mh	Mermill complex
MkB2	Miami loam, 2 to 6 percent slopes, moderately eroded
MIC2	Miami silt loam, 6 to 12 percent slopes, moderately eroded
MmC3	Miami soils, 6 to 12 percent slopes, severely eroded
Mn	Montgomery silty clay
Mo	Montgomery silty clay loam
MrB	Morley silt loam, 2 to 6 percent slopes
MrB2	Morley silt loam, 2 to 6 percent slopes, moderately eroded
MrC	Morley silt loam, 6 to 12 percent slopes
MrC2	Morley silt loam, 6 to 12 percent slopes, moderately eroded
MrD2	Morley silt loam, 12 to 18 percent slopes, moderately eroded
MrE2	Morley silt loam, 18 to 25 percent slopes, moderately eroded
MsB3	Morley soils, 2 to 6 percent slopes, severely eroded
MsC3	Morley soils, 6 to 12 percent slopes, severely eroded
MsD3	Morley soils, 12 to 18 percent slopes, severely eroded
MsE3	Morley soils, 18 to 25 percent slopes, severely eroded
Na	Nappanee silt loam
Np	Nappanee silty clay loam
OfA	Oshtemo fine sandy loam, loamy substratum, 0 to 2 percent slopes
OfB	Oshtemo fine sandy loam, loamy substratum, 2 to 6 percent slopes
OfC2	Oshtemo fine sandy loam, loamy substratum, 6 to 12 percent slopes, moderately eroded
OsA	Oshtemo sandy loam, 0 to 2 percent slopes
OsB	Oshtemo sandy loam, 2 to 6 percent slopes
Pc	Pewamo mucky silty clay loam
Pe	Pewamo silty clay loam
PIB	Plainfield fine sand, moderately fine substratum, 2 to 6 percent slopes
PIC	Plainfield fine sand, moderately fine substratum, 6 to 12 percent slopes
Pmg	Pits, Gravel
Pps	Pits, Quarries, Limestone
RaB	Rawson fine sandy loam, 2 to 6 percent slopes
RIA	Rawson loam, 0 to 2 percent slopes
RIB2	Rawson loam, 2 to 6 percent slopes, moderately eroded
RIC2	Rawson loam, 6 to 12 percent slopes, moderately eroded
Rm	Rensselaer loam
Rn	Rensselaer mucky silty clay loam
Ro	Rensselaer silt loam
Rs	Rensselaer silty clay loam
SaB	St. Clair silt loam, 2 to 6 percent slopes
ScB2	St. Clair silty clay loam, 2 to 6 percent slopes, moderately eroded
ScC2	St. Clair silty clay loam, 6 to 12 percent slopes, moderately eroded
Sh	Shoals silty clay loam
Ta	Adrian muck, drained, 0 to 1 percent slopes
W	Water
Wa	Wallkill silt loam
Wc	Wallkill silty clay loam
Wh	Washtenaw silt loam
Ws	Westland loam

Map Unit Legend

Allen County, Indiana

Map symbol	Map unit name
Wt	Westland silty clay loam
Wu	Willette muck



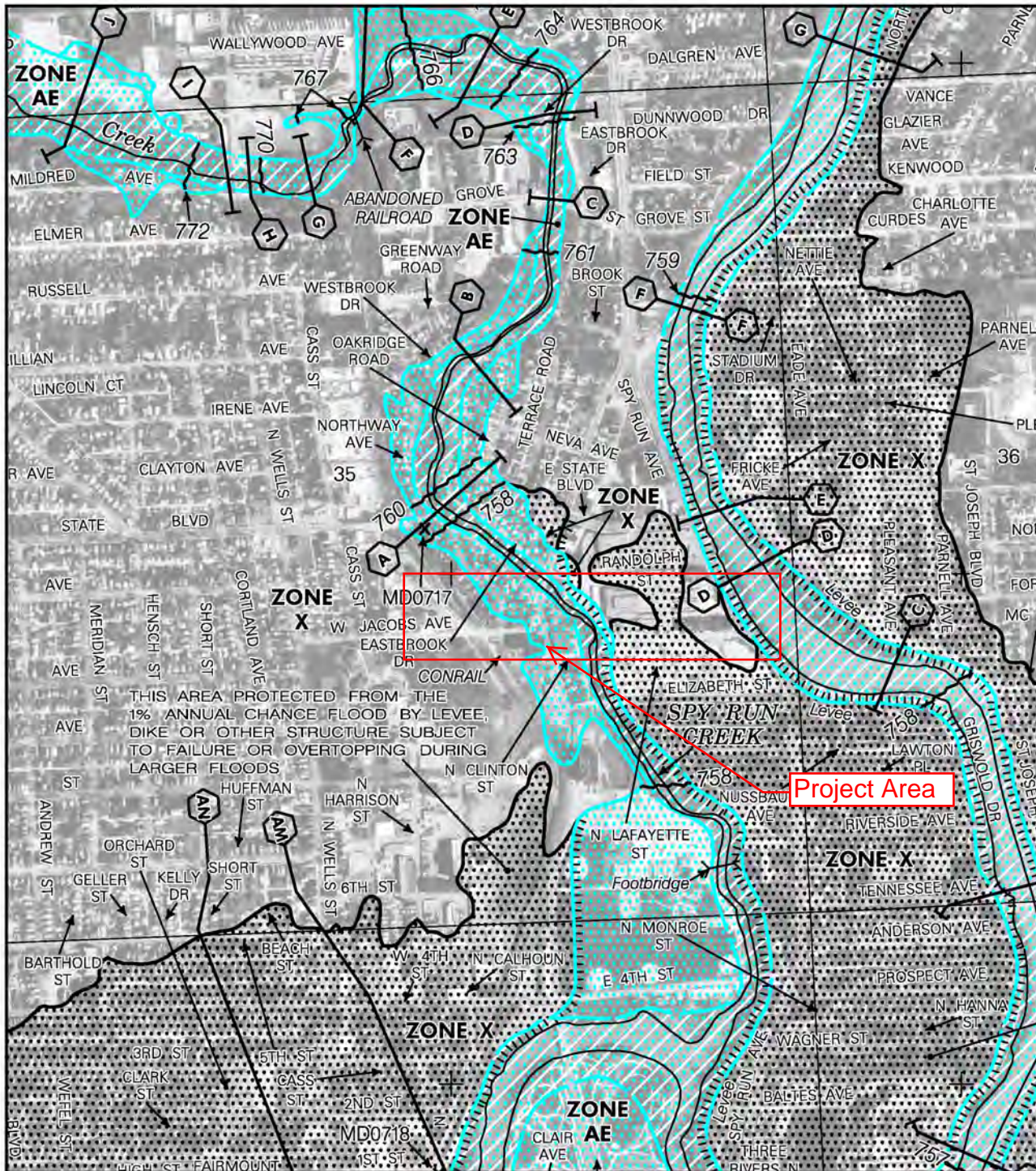
FEMA 100-Year Floodplain Map

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

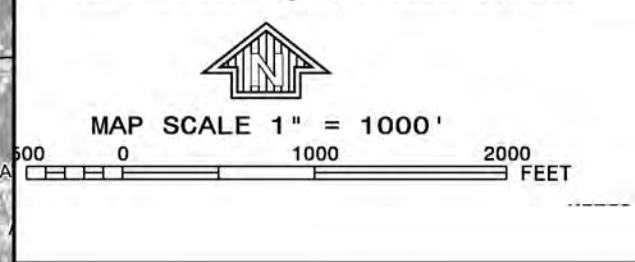
State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 07/21/2010



onal Flood Insurance Program at 1-800-638-6620.



NATIONAL FLOOD INSURANCE PROGRAM

PANEL 0145 F

FIRM

FLOOD INSURANCE RATE MAP

ALLEN COUNTY,
INDIANA
AND INCORPORATED AREAS

PANEL 145 OF 305

(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
ALLEN COUNTY	180302	0145	F
FORT WAYNE, CITY OF	180003	0145	F

Notice to User: The **Map Number** shown below should be used when placing map orders; the **Community Number** shown above should be used on insurance applications for the subject community.

MAP REVISED
NOVEMBER 5, 2003

MAP NUMBER
18003C0145F

Mauvee River Basin Commission
Federal Emergency Management Agency

This is an official copy of a portion of the above referenced flood map. It was extracted using F-MIT On-Line. This map does not reflect changes or amendments which may have been made subsequent to the date on the title block. For the latest product information about National Flood Insurance Program flood maps check the FEMA Flood Map Store at www.msc.fema.gov



AMERICAN
STRUCTUREPOINT
INC.

HYDRAULIC SUMMARY

**State Boulevard over Spy Run Creek
Fort Wayne, IN**

For Submittal to:

Indiana Department of Transportation

Prepared by:

Ryan M. Cummins, EI
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256



April 1, 2010

IN20071404



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Appendix G	HEC – RAS Summary, Standard Table 1 – Corrected Effective
Appendix H	HEC – RAS Summary, Standard Table 1 – Existing Pre-Project Conditions
Appendix I	HEC – RAS Summary, Standard Table 1 – Proposed Post-Project Conditions
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Appendix K	Cross-Section Location Exhibits and Electric Copy of Modeling
Appendix L	Structure Layout
Appendix M	Scour Information

SUMMARY

Project Location

The proposed project is located on the Fort Wayne West quad map at Section 16, Township 31N, Range 12E in Allen County, Indiana. The project involves construction of a new crossing over Spy Run Creek on new alignment for State Boulevard just west of its intersection with Clinton Street. The proposed crossing will be approximately 0.85 river mile upstream from Spy Run's confluence with the St. Mary's River.

Existing Conditions

Currently, the crossing at this location is a single-span concrete girder bridge on poor roadway alignment. The crossing is subject to major overtopping during storm events and is often closed to traffic as a result. The roadway surface is approximately 755.5 whereas the Q100 elevation is near 759, NGVD.

Proposed Conditions

To alleviate the poor roadway alignment and bridge closures due to overtopping, a new crossing is being proposed. The crossing will be relocated slightly downstream on a new roadway alignment and profile with a skew of 35 degrees from the alignment to accommodate Spy Run Creek. The profile will be raised such that a minimum of 1' of freeboard is provided. The proposed crossing will be a 3-span, prestressed concrete box beam structure.

Also of note, the US 27/Clinton Street crossing just downstream of the State Boulevard crossing is being reconstructed prior to this project. The Clinton Street project is being permitted under application FW-25265. In order to accurately model the proposed State Boulevard crossing, the modeling from the Clinton Street permit was provided and much of the information was used in developing the models for this application.

Technical Assessment

Hydraulic computations were completed with the US Army Corps of Engineers computer program "River Analysis System" (HEC-RAS). The 100-year storm event was used for design with a flow value of 2350 CFS coming from the FIS report. Simulations were developed in the following order.

1. Effective Conditions: There is an FIS model for Spy Run Creek, beginning upstream from Graham Drive at cross-section 6.86 and continuing until its confluence with the St. Mary's River. This model was available through the Indiana Department of Natural Resources, Division of Water in HEC 2 format. The State Boulevard crossing is at river mile 0.85 and the study reach was calculated to be approximately 2000'. The model was truncated at cross-section 1.87 (well outside the study reach) due to issues with getting it to run properly.
2. Duplicate Effective: Because the Effective model is in HEC 2 format, it was converted into HEC-RAS using the importing capabilities of the software to create the Duplicate Effective model. In order to get the model to run in HEC-RAS, a small distance was added between the faces of each bridge and their bounding cross-sections. Also, the bridge at Clinton Street did not come into HEC-RAS correctly and had to be added at the proper location using survey information and the FW-25265 Existing Conditions model.

3. **Corrected Effective:** The FIS model was done using the NGVD 29 vertical datum, but the survey information for the project that will be used to develop the plans was done using the NAVD 88 datum. Therefore, the first step in developing the corrected effective model was to make a vertical correction of -0.52 feet to bring the model into the NAVD 88 datum, as shown in Appendix A. Many cross-sections were deleted from the Duplicate Effective model as they had reach lengths of only one foot or were otherwise deemed unnecessary, including cross-sections 0.591, 0.599, 0.841, 0.851, 1.671, and 1.681. Cross-sections were also added to the Duplicate Effective model to provide the proper number of sections at each crossing within the model by copying sections from those nearby, including cross-sections 1.024, 1.026, 1.24, 1.476, 1.484, and 1.52. The bounding cross-sections at Clinton Street were modified per Permit FW-25265. Two pedestrian bridges were added to the model as done in Permit FW-25265 at cross-sections 1.025 and 1.482 as they were existing at the time of the FIS model, but assumed to be omitted due to their small size. The bridge widths for State Boulevard, Clinton Street, and Grove Street were updated to match their actual widths, which have not been modified since the FIS model was completed. The bridges at Clinton Street and State Boulevard were modified to include their bridge rails, as they would impede flow. Cross-sections 4, 3, 2, and 1 for State Boulevard expansion and contraction limits were relocated and re-cut using survey data.
4. **Existing Model:** Levees were added to a few cross-sections downstream of Clinton Street as part of FW-16464. Permit FW-21044 affected one cross-section within the study reach with an obstruction at 1.87 and this was added to the Corrected model. The new bridge at Clinton Street was added as part of FW-25265.
5. **Proposed Model:** The new bridge for State Boulevard was added and cross-sections 3 and 2 (0.85 and 0.84) were moved and re-cut due to the revised alignment.
6. **Natural Model:** Per INDOT requirements, a natural conditions model was developed using the proposed cross sections, but with proposed State Street bridge and ineffective areas removed. This model was used to establish a Q100 elevation at the downstream bridge face location as shown in Appendix A. The maximum backwater created by the proposed structure is 0.11' from these natural conditions, which is less than the 1' maximum required by INDOT.

The remainder of this report contains information to support the modeling and checklist contained in the appendices of this report:

Items to be Submitted with All Models

- Exhibits showing the cross-section locations used in the analysis (Appendix L)
- A diskette containing all electronic data files from the analysis (Appendix L)
- Hard copies of HEC-RAS reports generated for the duplicate effective, corrected effective/existing, and proposed post-project conditions (Appendix E, F, and G, respectively)
- Profile plots (Appendix K)

Cross-Section Exhibits

Cross-sections 0.86 through 0.70 lie within the limits of the site survey and are shown in detail in Appendix L. The remaining cross-sections lie outside the limits of the field survey, but information pertaining to these locations is included in Appendix E.

Adverse Impacts

This project has a maximum surcharge of 0.02' from existing conditions just upstream and downstream of the proposed structure as it reaches the proposed Clinton Street structure, but has dissipated to 0.00' by the end of the study limits.

Starting Water Surface Elevations

The starting water surface elevation is taken at the downstream cross-section 0.27 from the FIS model. This water surface elevation of 753.9 is included in the FIS data, which can be seen in Appendix E. This elevation was converted to NAVD 88 for the Corrected model, forward, giving a starting water surface elevation of 753.38.

Scour Information

A scour analysis was done using HEC-RAS modeling for the 100-year and 500-year storm events. The flow value for the 500-year event is 3,000 CFS from the FIS report. The results show that scour only effects the piers with a total controlling scour value of 4.60'.



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N758
Indianapolis, Indiana 46204-2216 (317) 232-3166 FAX: (317) 232-0238

Mitchell E. Daniels, Jr., Governor
Michael W. Reed, Commissioner

May 13, 2010

MEMORANDUM

TO: Coordinator 8
Project Manager

FROM: Bill P. Schmidt, P.E.
Hydraulics Engineer

SUBJECT: HYDRAULICS REVIEW
Structure: Allen County Bridge #545; State Blvd in Fort Wayne
Des. #: 1005152
Project: bridge replacement
Crossing: Spy Run Creek
Consultant: American StructurePoint

The proposed three-span spill-through bridge is approved.

Drainage Area =	14.9	sq. mi.
Q100 =	2350	cfs
Q100 Elevation =	758.55	ft.
Approximate Skew =	30	degrees
Proposed Gross Waterway Area		
(below Q100 Elev) =	947	sq.ft.
Proposed Road Overflow Area =	0	sq.ft.
Proposed Velocity =	2.56	ft/s
Proposed Backwater =	0.11	ft.
Proposed Low Structure Elev =	759.84	ft.
Required Low Structure Elev =	759.55	ft.
Existing Gross Waterway Area		
(below Q100 Elev) =	170	sq.ft.
Existing Road Overflow Area =	1932	sq.ft.
Existing Velocity =	1.86	ft/s
Existing Backwater =	0.09	ft.
Existing Low Structure Elev =	751.40	ft.

The waterway opening is normal to the direction of flow. The data is based on a flowline elevation of 745.00 ft. The proposed backwater exceeds the existing; however, road overflow has been removed and the proposed backwater is still below 0.14 ft.

Proposed Scour Data

Q100 Contraction Scour =	0.41	ft.
Q100 Total Scour =	3.87	ft.
Q100 Low Scour Elevation =	741.13	ft.
Q100 Max Velocity =	3.56	ft/s.
Q500 =	3000	cfs
Q500 Elevation =	758.90	ft.
Q500 Contraction Scour =	0.81	ft.
Q500 Total Scour =	4.60	ft.
Q500 Low Scour Elevation =	740.40	ft.
Q500 Max Velocity =	4.40	ft/s

The data is based on a flowline elevation of 745.00 ft. The scour data is based on erodible materials. The maximum velocity is the highest velocity value that would occur within the channel.

Note: The low scour elevation had to be corrected to match the flowline elevation. The flowline elevation should be on the downstream side of the bridge.

If you have any questions or comments, please contact me at (317) 232- 5148.

WPS

Appendix F: Public Involvement

- Survey Notice and Mailing List – March 18, 2009
- List of Meetings with Public and/or Neighborhood Representatives
- September 30, 2009 Public Meeting Documents
- Preliminary Renderings and Open House (February 25, March 1, and March 7, 2013) Comments

March 18, 2009

Property Owners Name
Street Address
City, State, Zip Code

Re: State Boulevard Route Survey Project

Dear Property Owner:

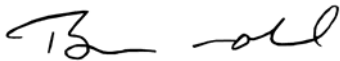
American Structurepoint under the direction of the City of Fort Wayne will be conducting a route survey for the above mentioned project in Fort Wayne, Indiana. Our research information indicates you own property near the project area.

The route survey portion of this project has been authorized and is scheduled to be started in the near future. It may be necessary for our employees to enter upon your property for the purpose of making this survey.

The survey work will consist of mapping the location of features such as buildings, trees, fences, and drives along the proposed corridor. We earnestly solicit your cooperation in this matter.

Please be assured of our sincere desire to cause you no damage or inconvenience.

Very truly yours,
American Structurepoint, Inc.

A handwritten signature in black ink, appearing to read "Bryan J. Moll", is written over the printed name.

Bryan J. Moll, LS
Department Manager

BJM:slr

Adjacent Porperty Owners Mailing List

First Name	Last Name	Address	City	State	Zip code
City of Fort Wayne Board of Public Works		1 East Main Street	Fort Wayne	IN	46802
Kroger Limited Partnership I		5960 Castleway West Drive	Indianapolis	IN	46250
J & J Realty Co.		1800 Magnavox Way	Fort Wayne	IN	46804
Daniel J. & Karen S.	Hall	19 EMS T32 Lane	Leesburg	IN	46538
DK Properties LLC		142 E State Blvd	Fort Wayne	IN	46805
Darrin D.	Klopp	142 E State Blvd	Fort Wayne	IN	46805
Lisa S.	Strebig	138 E State Blvd	Fort Wayne	IN	46805
Susan R.	Haneline	134 E State Blvd	Fort Wayne	IN	46805
NLI Inc.		1616 N Harrison St	Fort Wayne	IN	46808
Moshin A. & Karen S.	Hudda	112 E State Blvd	Fort Wayne	IN	46805
Val Acquisition Co LLC		7108 Covington Rd	Fort Wayne	IN	46804
Charles G. & Amanda S.	Kimani	2230 Cass Street	Fort Wayne	IN	46808
Michael Huy	Taing	301-303 W State Blvd	Fort Wayne	IN	46808
Richard A. & Betty Ann	Pion	111 E State Blvd	Fort Wayne	IN	46805
Nancy E.	Leitch C/O Aquariusl IV LLC	2811 E State Blvd	Fort Wayne	IN	46805
Mookel T Enterprises LLC		335 E State Blvd	Fort Wayne	IN	46805
Donald B.	Fisher	1639 Cape Coral Parkway E Ste 208	Cape Coral	FL	33904
Betty	Marvel	315 E State Blvd	Fort Wayne	IN	46805
Richard L. & Michael L.	Summers	311 E State Blvd	Fort Wayne	IN	46805
John D. & Sharon	Hartman	11821 Linden Grove Dr	Fort Wayne	IN	46845
Michael G Hinter &	Eric L. Hathaway	175 E State Blvd	Fort Wayne	IN	46805
Kerry D. & Brooke N.	Johnson	2302 Terrace Rd	Fort Wayne	IN	46805
Joshua	Johnson	145 E State Blvd	Fort Wayne	IN	46805
Joshua T.	Geary	141 E State Blve	Fort Wayne	IN	46805
Kent & Stacie	Christon	137 E State Blvd	Fort Wayne	IN	46805
Natasha L	Sare	2302 Oakridge Rd	Fort Wayne	IN	46805
John W. & Joann V.	Hageman	327 Dunwood Dr	Fort Wayne	IN	46805
Fidelina	Quintana	2221 Westbrook Dr	Fort Wayne	IN	46805
Fort Wayne City of Department of Redevelopment		1 E Main Street RM 840	Fort Wayne	IN	46802
Philip M.	Miller	3212 N Clinton St	Fort Wayne	IN	46805
Otis R Bown Center for Human Services Inc.		850 N Harrison St	Warsaw	IN	46580
Anchor Indiana IV LLC	Walgreen Co Attn: Tax Dept	300 Wilmot Dr	Deerfield	IL	60015
City of Fort Wayne, Indiana		One Main Street RM 350	Fort Wayne	IN	46802
John & Marlene	Slate	215 W State Blvd	Fort Wayne	IN	46805
Norma	Pankop	154 E State Blvd	Fort Wayne	IN	46805
Robert & Annette Janice	Dailey	162 E State Blvd	Fort Wayne	IN	46805

State Boulevard Realignment Project - Meetings with Public and/or Neighborhood Representatives				
#	Date	Meeting with	Location	Comments
1	7/15/2008	Friends of the Park	First Prebyterian Church	
2	7/29/2008	ARCH	Offices of ARCH	
3	8/6/2008	Neighborhood	Westbrook Dr	on site walk through to discuss Westbrook rain garden and State Blvd projects
4	9/11/2008	Public Meeting	City County Bldg - Omni Room	Discussion of Westbrook rain garden and State Blvd projects
5	10/1/2008	John Shoaff, City Council	City County Bldg	
6	10/27/2008	Public meeting	City County Bldg Room - 350	
7	11/17/2008	Public Meeting	City County Bldg - Omni Room	
8	2/20/2009	Charley Shirmeyer of Northside Galleries	Northside Galleries	with Zenovia Pearson
9	5/28/2009	Public meeting	Northside High School - Cafeteria	
10	6/15/2009	Karen Richards	City County Bldg	with Frank Suarez
11	8/28/2009	Charley Shirmeyer of Northside Galleries	Northside Galleries	with Scott Crites of American Structurepoint
12	9/30/2009	Business owners on State Blvd	City County Bldg	10:00 AM
13	9/30/2009	Public meeting - Charette	Allen County Public Library	5:00 PM
14	10/15/2009	Scott Simmons of Brookview Civic Neighborhood	City County Bldg	
15	11/10/2009	Scott Simmons and Michelle Wedaman of Brookview Civic Neighborhood	on site walk through	with Scott Crites of American Structurepoint
16	12/7/2009	Scott Simmons and Michelle Briggs Wedaman of Brookview Civic Neighborhood	City County Bldg	
17	12/15/2010	First Section 106 consulting parties meeting	City County Bldg	
18	2/1/2010	Scott Simmons of Brookview Civic Neighborhood	City County Bldg	with Scott Crites of American Structurepoint (Michelle Briggs Wedaman was not able to make it due to illness)
19	3/11/2010	Tim Pape & Karen Goldner, City Council	Tim Pape's Office	
20	3/26/2010	Susan Haneline of Brookview Civic Neighborhood	City County Bldg	
21	4/8/2010	Scott Simmons of Brookview Civic Neighborhood	City County Bldg	
22	7/20/2010	Michelle Briggs Wedaman of Brookview Civic Neighborhood	City County Bldg	
23	7/23/2010	Charley Shirmeyer of Northside Galleries	Northside Galleries	
24	7/11/2011	Charley Shirmeyer of Northside Galleries	Northside Galleries	
25	3/7/2012	Friends of the Park	First Presbyterian Church	
26	9/1/2011	Second Section 106 consulting parties meeting	Citizens Square	
27	4/23/2012	CORE Group of Neighborhoods	631 Lawton Place	Meeting was held at residence of John Meinzen, President of Lawton Place Neighborhood Association (7 PM)
28	5/10/2012	Northeast Area Partnership	Good Shephard United Methodist Church	
29	5/16/2012	Southwest Area Partnership	Fort Wayne Sports Club	
30	6/18/2012	Northside Neighborhood Association	Forest Park United Methodist Church	
31	8/3/2012	Mohsin Hudda, resident of East 112 State Blvd	112 West State	with Scott Crites
32	9/13/2012	Northeast Area Partnership	Good Shephard United Methodist Church	
33	9/19/2012	Third Section 106 Consulting Parties Meeting	ACPL Meeting Room A	10:00 AM
34	9/29/2012	John Modezjewski & Kay Smith (North Highlands) and Rick Stoeckley (Lincoln Park)	CSQ Room 220	2:00 PM
35	10/3/2012	Judi and Dan Wire, Wells Street Business Association	CSQ Room 211	1:00 PM
36	10/5/2012	Betty Pion (111 East State) & Lenley Egan (Brookview Residents)	111 East State	10:00 PM
37	10/10/2012	Karl & Kate Dietsch, Brookview residents	CSQ Room 220	4:00 PM
38	10/15/2012	Historic Oakwood NA (President Tom Tiernan)	Redeemer Lutheran Church, 202 West Rudisill Blvd.	7:00 PM
39	10/18/2012	Julie Peeples (2308 Eastbrook - Phone 494-8383)	CSQ 220	8:30 AM
40	10/18/2012	Charlotte Weybright (West Central NA)	CSQ Room 211	4:40 PM (with Bob Kennedy and Frank Suarez)
41	10/18/2012	Northwest Area Partnership (presentation with Dan Avery)	Northridge Baptist Church, 1300 East Cook Road	6:30 PM
42	10/19/2012	Russ Jehl, 2nd District Councilman	CSQ Room 211	9:30:00 AM (with Bob Kennedy and Frank Suarez)
43	10/19/2012	Bud & Jean Mendenhall (Bloomingtondale Neighborhood Assn)	CSQ Room 220	2:00 PM (with Frank Suarez)
44	10/25/2012	Bud & Jean Mendenhall (Bloomingtondale NA), and Shauna Nicelley (Forest Park NA)	CSQ Room 220	6:00 PM (with Frank Suarez and Bob Kennedy)
45	11/1/2012	Kent Christon (137 East State), Josh Johnson (145 East State), Darren Klopp (142 & 146 East State) & Karl Dietsch	137 East State	10:00 AM
46	11/19/2012	Mark Anderson (2227 Westbrook) Resident of Brookview	2227 Westbrook	12:00 Noon (also met him on Friday 11/16 on a site visit)
47	2/25/2013	Open House	Pond Pavilion, Franke Park	5:00 - 7:00 PM
48	3/1/2013	Open House	ACPL Main Branch Meeting Room A	11:00 AM - 1:00 PM
49	3/7/2013	Open House	Psi Ote Barn, Lower Level, Northside Park	5:00 - 7:00 PM
50	3/22/2013	Scott Simmons	CSQ Room 215	3:00 PM
51	4/22/2013	Marsh Davis, President, Indiana Landmarks & Melissa Glaze	Glaze Residence	4:00 PM

PUBLIC MEETING

**State Boulevard Reconstruction from Cass Street to Spy Run
Avenue, Fort Wayne, IN**

INFORMATION PACKET

**Wednesday, September 30, 2009
Presentation: 5:30 p.m.**

**Allen County Public Library
900 Library Plaza
Fort Wayne, Indiana**

Meeting Agenda

STATE BOULEVARD PUBLIC INFORMATION AND INPUT INITIATIVE – 9/30/09

1. Welcome
2. Presentation Agenda
3. Ground rules
4. Area Projects
5. Environmental Process
6. Status of the Design
7. Group Discussions
8. Group Discussion Summary
9. Future Meetings

Project Overview

Project Schedule –

- Anticipated Phase I Construction Beginning Spring 2012 (Clinton Street to Spy Run Ave)
- Anticipated Phase II Construction Beginning Spring 2013 (Cass Street to Clinton Street)

Need For Improvement –

- Safety
- Traffic congestion created by left turning vehicles
- Roadway Drainage
- Traffic congestion at Spy Run Ave and Clinton Street intersections
- Substandard roadway curvature

Project Limits – State Boulevard from Cass Street to Spy Run Avenue

Project Description –

- Two through lanes in each direction
- Curb and gutter
- Two way left turn lane where needed to address left turn movements
- Center median with landscaping where possible
- Variable width sidewalks
- Full depth pavement replacement
- New driveways will be constructed
- New street approaches will be constructed
- Pedestrian trail connections
- New bridge over Spy Run Creek
- Pedestrian Bridge over State Boulevard at the proposed Pufferbelly Trail

Please provide your questions and/or suggestions regarding the three proposed alternatives discussed as well as other portions of the State Blvd Reconstruction project discussed during the public information meeting. Your questions and suggestions are important to us, and we sincerely appreciate your time and participation during the public involvement process. **Comments may be mailed/faxed to the address below, or sent by e-mail to the address below.**

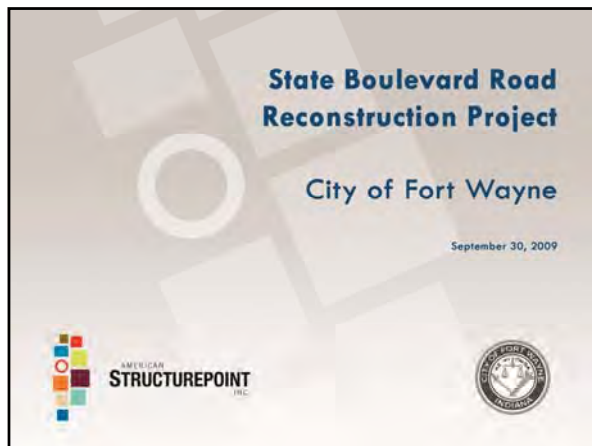
NAME: _____

ADDRESS: _____

COMMENT: _____

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SIGNATURE: _____



National Environmental Policy Act (NEPA)

- o Perform Investigation of Potential Impacts to Natural and Human Environment
 - o Waterways, wetlands, endangered species, etc.
 - o Historical properties
 - o Social and economic factors
- o Environmental Document
 - o Prepared in Accordance with State and Federal Guidelines
 - o Evaluates impacts of proposed project



State Boulevard Reconstruction

STRUCTUREPOINT

National Environmental Policy Act (NEPA) Process

- o Gather Information
 - o Coordination with state, local and federal agencies
 - o Document potential social, economic and environmental factors
 - o Section 106
 - o Identification of potentially historic properties



State Boulevard Reconstruction

STRUCTUREPOINT

National Environmental Policy Act (NEPA) Process

- o Prepare Draft Categorical Exclusion Report
 - o Reviewed by Indiana Department of Transportation and Federal Highway Administration
 - o Released for Public Involvement
- o Conduct Public Hearing
- o Final CE Approved by INDOT and FHWA



State Boulevard Reconstruction

STRUCTUREPOINT

Environmental Status

- o Coordination with state, local, and federal agencies
- o Ecological Resources
 - o Streams, Rivers, Watercourses, Jurisdiction Ditches, Wetlands, Terrestrial habitat, Karst Features, Threatened and Endangered Species
- o Other Resources
 - o Ground Water, Surface Water, Drinking Water, Wellhead Protection Areas, Sole Source Aquifers, Floodplains, Farmland
- o Cultural Resources
 - o Section 4(f)



State Boulevard Reconstruction

STRUCTUREPOINT

Environmental Status

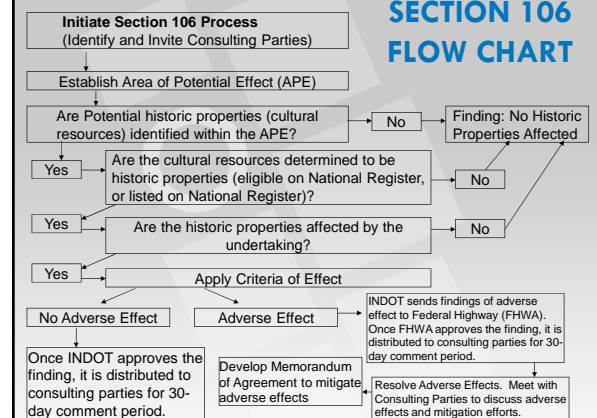
- o Noise Impacts
- o Community Impacts
 - o Neighborhood Factors, Environmental Justice, Relocations
- o Hazardous Materials and Regulated Substances
- o Permits
- o Environmental Commitments



State Boulevard Reconstruction

STRUCTUREPOINT

SECTION 106 FLOW CHART



Section 106 Status

Section 106 Process

- o Coordination with state, local, and federal agencies
 - o Indiana State Historical Preservation Officer
 - o Historic Landmarks Foundation of Indiana
 - o Fort Wayne Historic Preservation Commission
 - o Friends of the Parks of Allen County
 - o ARCH, Inc
 - o City of Fort Wayne
 - o Indiana Historic Spans Task Force
 - o Dr. James L. Cooper
 - o Brookview Neighborhood Association
 - o Northwestern Indiana Regional Coordination Council
 - o Irvington Park Neighborhood Association



State Boulevard Reconstruction

STRUCTUREPOINT

Section 106 Status

- o Phase Ia Archaeological Site Investigation
 - o Recommendation of Project Clearance
- o Historic Properties Report
 - o Draft being finalized
 - o INDOT review
 - o Distribute to consulting parties for review and comment
- o Anticipated Finding of "Adverse Effect"
 - o Memorandum of Agreement required



State Boulevard Reconstruction

STRUCTUREPOINT

Environmental

What's Next

- o Compile field data, early coordination responses, historical findings, and all additional data required
- o Distribute HPR to Consulting Parties, hold Consulting Parties meeting
- o Potential Additional Public Involvement Sessions
- o Section 106 Finding and Mitigation
- o Public Hearing
- o Submit to INDOT and Federal Highway Administration for final review and release of document



State Boulevard Reconstruction

STRUCTUREPOINT

Project Overview

- o State Boulevard - Federal Aid LPA Project
 - o City using Federal Money to Fund Project
 - o 80% Federal Funds; 20% Local Funds
 - o Follow INDOT Standards and Design Guidelines



State Boulevard Reconstruction

STRUCTUREPOINT

Project Schedule

Milestone	Expected Dates
Anticipated Phase I Construction Begins	Spring 2012
Anticipated Phase II Construction Begins	Spring 2013

- o Phase I – Clinton Street to Spy Run Creek
- o Phase 2 – Cass Street to Clinton Street



State Boulevard Reconstruction

STRUCTUREPOINT

Need for Improvement

- o Safety
 - o Substandard sight distance along State Boulevard
 - o Frequent accidents
- o Traffic Congestion created by Left-Turning Vehicles
- o Drainage Issues
 - o Roadway flooding along Little Spy Run Creek
 - o Combined Storm / Sanitary Sewers
- o Traffic Congestion at Spy Run Ave and Clinton Street Intersections
- o Substandard Roadway Curvature



State Boulevard Reconstruction

STRUCTUREPOINT

Curve at Westbrook Dr. - East Side



State Boulevard Reconstruction

STRUCTUREPOINT

Traffic on Westbound State Blvd



State Boulevard Reconstruction

STRUCTUREPOINT

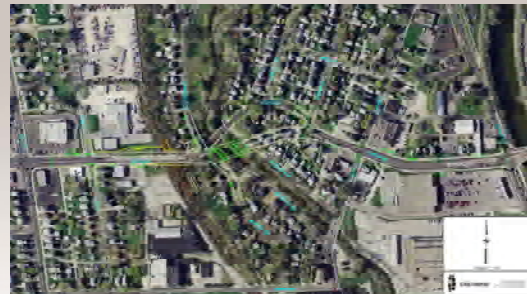
Roadway Flooding



State Boulevard Reconstruction

STRUCTUREPOINT

Project Limits



State Boulevard Reconstruction

STRUCTUREPOINT

Project Description

- o Project Begins at Cass Street
- o Project Ends at Spy Run Ave intersection
- o Two Thru Lanes in Each Direction
- o Curb and Gutter
- o Two-Way Left-Turn Lane where needed to Address Left turn movements
- o Center median with landscaping where possible
- o Variable Width Sidewalks
 - o 6-ft wide on north side and 10-ft wide on south side



State Boulevard Reconstruction

STRUCTUREPOINT

Project Description

- o Full Depth Pavement Replacement
- o New Driveways will be Constructed
- o New Street Approaches will be Constructed
- o Pedestrian trail connections
- o New Bridge over Spy Run Creek
- o New Pedestrian Bridge over State Blvd – Pufferbelley Trail



State Boulevard Reconstruction

STRUCTUREPOINT

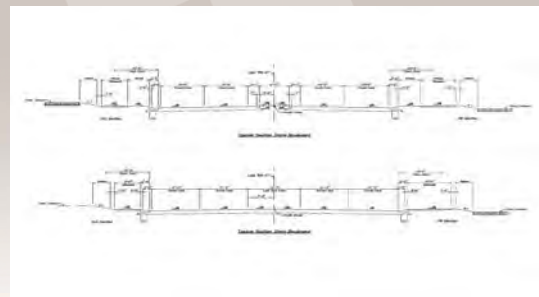
Project Limits



State Boulevard Reconstruction

STRUCTUREPOINT

Typical Section



State Boulevard Reconstruction

STRUCTUREPOINT

Proposed Project Concept – West Limit (Looking East)



State Boulevard Reconstruction

STRUCTUREPOINT

Proposed Project Concept



State Boulevard Reconstruction

STRUCTUREPOINT

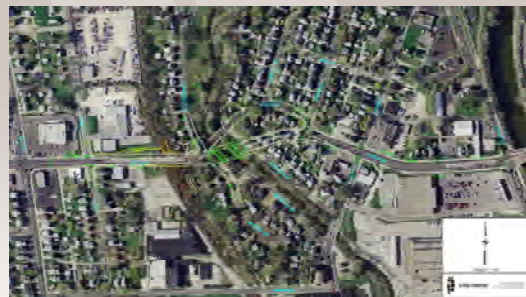
Proposed Project Concept – East Limit (Looking West)



State Boulevard Reconstruction

STRUCTUREPOINT

Project Limits



State Boulevard Reconstruction

STRUCTUREPOINT

Design Alternatives

- o Focus of tonight's meeting
- o Proposals to tie local streets to new State Blvd
- o Three Alternatives for review



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 1 Description

- o Terrace Road will connect to proposed State Blvd
- o Eastbrook Drive will access proposed State Blvd by following the existing State Blvd to the Terrace Rd intersection



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 1 Layout



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 2 Description

- o Extend Oakridge Rd to the proposed State Blvd Alignment
- o Eastbrook Dr. and Terrace Rd. will access proposed State Blvd by following the existing State Blvd to Oakridge Rd.



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 2 Layout



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 3 Description

- o Extend Oakridge Rd to the proposed State Blvd Alignment
- o Eastbrook Dr. will access proposed State Blvd by following the existing State Blvd alignment to Oakridge Rd.
- o Terrace Rd will remain open allowing only right turn movements entering and exiting Terrace Rd from the proposed State Blvd alignment



State Boulevard Reconstruction

STRUCTUREPOINT

Alternate 3 Layout



State Boulevard Reconstruction

STRUCTUREPOINT

Group Discussion

- o 30 minutes to review and comment
- o 3 Alternatives to review and comment on
- o Discuss pros and cons of each alternative
- o Document comments on paper and/or plan sheets
- o Additional Comments can be documented as well
- o Project Team will walk around and try to answer questions as best they can at this stage of process
- o Reconvene at end of 30 minutes for closing comments



State Boulevard Reconstruction

STRUCTUREPOINT

Group Discussion Follow Up

- o Project Team will take comments and review
- o Additional comments/questions submitted in writing to City or Designated Project Team Member
- o Response and follow up will be via e-mail
- o Anticipate additional meetings in future



State Boulevard Reconstruction

STRUCTUREPOINT

Thank You



State Boulevard Reconstruction

STRUCTUREPOINT

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
19	SCOTT SIMMONS	2732 EASTBROOK 46005	
20	SUZANNE SLICK	3318 Garland Ave	
21	Joel Tye	801 Mildred Avenue	
22	KARL & CATHERINE DIETRICH	2313 OAKRIDGE ROAD	
23	W. John Meinzen	631 Lawton	
24	Karen Rebaud	602 S. Calhoun	
25	Chad Williams	2309 Spyken	
26	Jennie Stine	2937 Westbrook Dr	
27			

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
73	John Wallace	1 MAIN ST. Room 840 FORT WAYNE, IN 46802	
74	Bob & Joy Wiers	2227 N. CLINTON ST FT WAYNE, IN 46723	
75	ALB.		
76	Tom Smith	CITY COUNCIL	
77	Jon Slate	215 W. State Fort Wayne 46808	
78	Garrell Henline Classic stereo	2312 N Clinton St 46805	
79	PHILIP C TOWNSEND	2937 WESTBROOK Ft W 46805	
80	JILL DOWNS	1202 GUNBOY AVE.	
81	JULIE PATURK	2510 TERRACE RD	

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
82	Mike Thorason	County Hwy Dept.	
83	Susan Stark E/WE	134 E. State Blvd	
84	Laurie Weyer	46805 2526 West Drive	
85	MICHAEL GALBRATH	ARCH	
86	Creager Smith	CFW- CD	
87	Sharon Neman	2410 EASTBARK DR	
88	Pamela Sully	2507 Terrace Rd	
89			
90			

page
100

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
10	Michelle Briggs Wedaman Mike Wedaman	2326 Eastbrook Dr. 46805	
11	Don Cunningham	2221 Point West Dr. 2D FW 46808	
12	Galen A. Lehman	2708 Eastbrook 46805	
13	Thomas Hilt	2531 W. Clinton St 46806	
14	Kerry Johnson	2302 Terrace Road	
15	Kenley Egan	2309 Oakridge Rd	
16	John D. Hartman	2234 N. Clinton St	
17	Sharon Startman	11821 Linden Grove	
18			

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
1	Peter LeBlanc	2235 Westbrook Dr. Ft. Wayne IN 46805	
2	Gordon Miller	419 Lillian AVE	
3	David Ross	City - Co. Bldg.	
4	Mike Raeger	1219 Tecumseh FW	
5	NIEL KNOX	12323 DOWLING OAKS CT FW, IN 46845	
6	Danielle + Josh Tuck	2315 Terrace Rd FW, IN 46805	
7	John Shoaff	4646 W. JEFFERSON BLVD, 46804	
8	John Leahy	3330 IRVINGTON DR	
9	Jan Dailey	162 E. State Blvd F.W. 46805	

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
91	<i>Paul Wheeler</i>	<i>7526 West Drive H. W. W. W.</i>	
92	<i>Cathy A. Dill</i>	<i>2313 Oak Ridge Rd</i>	
93			
94			
95			
96			
97			
98			
99			

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
28	KIMBERLY MOORE	2227 WESTBROOK DR.	
29			
30			
31			
32			
33			
34			
35			
36			

PUBLIC INFORMATION MEETING

State Boulevard Reconstruction Project

September 30, 2009

SIGN-IN SHEET

	Name	Address	E-Mail
37	Danielle Tuck		
38			
39			
40			
41			
42			
43			
44			
45			



STATE BLVD., EAST VIEW TOWARD SPY RUN AVE.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., WEST VIEW TOWARD PROPOSED STATE AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., SOUTHWEST VIEW OF THE NEW EXTENSION AND HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



OAKRIDGE RD., NORTH VIEW TOWARD HISTORIC STATE BLVD.



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., EAST VIEW TOWARD NEW BRIDGE OVER SPY RUN CREEK



AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., WEST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE

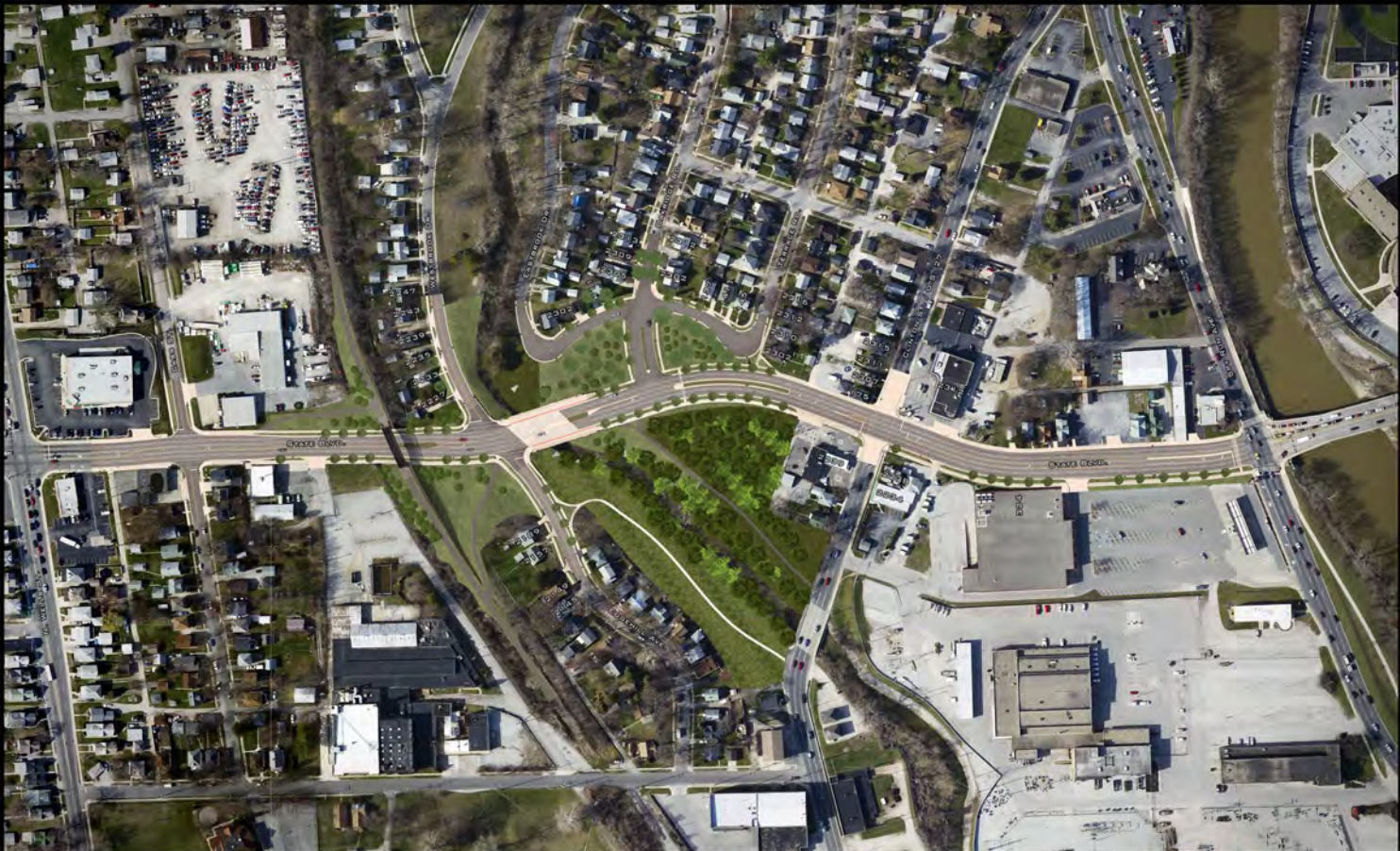


AMERICAN
STRUCTUREPOINT
INC.



STATE BLVD., EAST VIEW TOWARD PUFFERBELLY TRAIL BRIDGE





Appendix G: Air Quality

- Transportation Improvement Program Listing 2014-2017
- State Transportation Improvement Program Amendment
- Air Quality Maps

Transportation Improvement Program

2014-2017

NIRCC
Northeastern Indiana Regional Coordinating Council

RESOLUTION ENDORSING AND APPROVING THE
TRANSPORTATION IMPROVEMENT PROGRAM AND ANNUAL ELEMENT,
AS UPDATED AND AMENDED

WHEREAS, the Northeastern Indiana Regional Coordinating Council is the organization designated by the Governor as the Metropolitan Planning Organization responsible, together with the State, for carrying out the provisions of 23 U.S.C. 134, and capable of meeting the requirements thereof for the Fort Wayne - New Haven - Allen County Transportation Study Area; and

WHEREAS, the Urban Transportation Advisory Board is the policy body of the Fort Wayne - New Haven - Allen County Transportation Study pursuant to Indiana State statutes; and

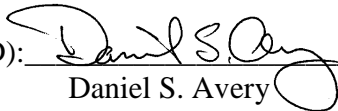
WHEREAS, it is required that the policy body of the Metropolitan Planning Organization endorse the Transportation Improvement Program as a prerequisite to expenditure of Federal Highway Administration and Federal Transit Administration funds; and

WHEREAS, proposed expenditures of Federal-Aid Highway and Federal Transit Administration funds for the Fort Wayne - New Haven - Allen County Transportation Study Area are contained in the Transportation Improvement Program for the Metropolitan Area; and

WHEREAS, such Transportation Improvement Program for the Fort Wayne - New Haven - Allen County Transportation Study is updated and amended:

BE IT, THEREFORE, RESOLVED, that the Urban Transportation Advisory Board on June 4, 2013 and the Northeastern Indiana Regional Coordinating Council on June 6, 2013 endorsed the Fort Wayne - New Haven - Allen County Transportation Study Transportation Improvement Program for the Metropolitan Planning Area.

(SIGNED):


Daniel S. Avery

(DATE): June 6, 2013

RESOLUTION

A RESOLUTION OF THE NORTHEASTERN INDIANA REGIONAL COORDINATING COUNCIL, CERTIFYING THAT THE FY 2014-2017 TRANSPORTATION IMPROVEMENT PROGRAM FOR THE FORT WAYNE-NEW HAVEN-ALLEN COUNTY METROPOLITAN PLANNING AREA, ALLEN COUNTY, INDIANA CONFORMS TO THE REQUIREMENTS SET FORTH IN THE 1990 CLEAN AIR ACT (CAAA)

WHEREAS, The Northeastern Indiana Regional Coordinating Council is the Metropolitan Planning Organization representing the Fort Wayne Urbanized areas, as well as Allen, DeKalb and Wells Counties in Indiana.

WHEREAS, Allen County is currently designated as a maintenance area for ozone by operation of the law under the 1990 Clean Air Act,

WHEREAS, The Northeastern Indiana Regional Coordinating Council is designated as the Lead Agency for air quality planning as it relates to transportation planning and mobile source emissions,

THEREFORE BE IT RESOLVED, that the Northeastern Indiana Regional Coordinating Council herewithin certifies that the FY2014-2017 Transportation Improvement Program conforms to the broad intentions of achieving and maintaining the National Ambient Air Quality Standards (NAAQS).

That the FY2014-2017 Transportation Improvement Program (TIP) is consistent with the 2035 Transportation Plan conformity determination, which is based upon the most recently available estimates of emissions and which have been determined from the most recently available population, employment, travel and congestion estimates as determined by NIRCC using its Travel Demand Forecasting Model and VMT estimation procedures.

That a list of exempt and non-exempt projects in the 2035 Transportation Plan has been circulated to the Interagency Consultation Group and there is concurrence on the project exempt/non-exempt status.

That a review of the 2035 Transportation Plan has been completed and the projects listed in the FY 2014-2017 TIP are consistent with the approved NIRCC 2035 Transportation Plan as Amended.

That no project in the FY2014-2017 Transportation Improvement Program will cause delay in the implementation of any required and identified TCM.

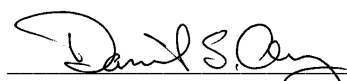
That the FY2014-2017 Transportation Improvement Program for the Fort Wayne-New Haven-Allen County Transportation Management Area contributes to the annual emission reductions consistent with sections 182(b) (1) and 187 (1) and 187 (a) (7) of the 1990 Clean Air Act.

That the MPO is aware of no goal, directive, recommendation, or project identified in the Transportation Improvement Program which contradicts in a negative manner any specific requirements or commitments of the applicable state implementation plan (SIP) for the plan.

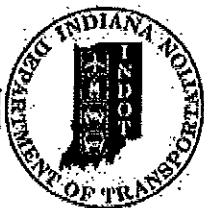
THEREFORE BE IT RESOLVED, that the Urban Transportation Advisory Board on June 4, 2013 and Northeastern Indiana Regional Coordinating Council on June 6, 2013 find the FY 2014-2017 Transportation Improvement Program to conform in all aspects to the requirements of the 1990 Clean Air Act Amendment and 40 CFR Parts 51 and 93.

RESOLVED THIS 6TH DAY OF JUNE, 2013.

THE NORTHEASTERN INDIANA REGIONAL COORDINATING COUNCIL



Daniel S. Avery, Executive Director



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N955
Indianapolis, Indiana 46204

PHONE: (317) 232-5485
FAX: (317) 232-1499

Michael R. Pence, Governor
Michael B. Cline, Commissioner

June 18, 2013

Northeastern Indiana Regional Coordinating Council
Dan Avery, Executive Director
200 E. Berry St, Suite 230
Fort Wayne, IN 46802

Dear Mr. Avery:

The Indiana Department of Transportation (INDOT) has completed its review of the FY 2014-2017 Transportation Improvement Program for The Northeastern Indiana Regional Coordinating Council (NIRCC MPO). State and locally initiated transportation projects were reviewed for accuracy and compliance under the Moving Ahead for Progress in the 21st Century Act (MAP-21).

It is my pleasure to inform you that on behalf of Governor Michael R. Pence, I approve your FY 2014-2017 Transportation Improvement Program. This document will serve as support for the local and INDOT projects in your area that fall within the FY 2014-2017 timeline and will be included by reference in the FY 2014-2017 Indiana Statewide Improvement Program (STIP). However, projects not shown by reference will be amended into the STIP upon request.

If you should have any questions, please feel free to contact Roy Nunnally at 317-234-1692.

Sincerely,

Michael B. Cline
Commissioner
Indiana Department of Transportation

MBC/JAH/jh

cc: Jay Wasson
Roy Nunnally
Jay Dumontelle
Joyce Newland
Jeanette Wilson
Todd Johnson

LOCATION	Project Description	FY 15	LPA	Phase	Total Cost	Federal Share	Funding Type
Anthony Blvd: Tillman Rd to Rudisill Blvd			FW	PE	\$300,000	\$240,000	STP
	<i>Road Reconstruction</i>						
Broadway, Landin Rd and Rose Ave Intersection			NH	PE	\$200,000	\$160,000	CMAQ-Pending
	<i>Intersection Improvement</i>						
Clinton St - Left-Turn Lane Alignment Package			FW	PE	\$111,111	\$100,000	HSIP-Pending
	<i>Intersection Improvement</i>						
Covington Rd Trail: Beal-Taylor Ditch to West Hamilton Rd			FW	CN	\$953,500	\$810,600	TE
	<i>New Trail Construction</i>						
Liberty Mills Rd & County Line Rd			AC	RW	\$206,250	\$165,000	CMAQ
	<i>Intersection Improvement</i>						
*Paulding Rd - Hessen Cassel Rd to Lafayette St			FW	CN	\$1,377,800	\$1,240,000	HSIP
	<i>Road Reconstruction</i>						
Pufferbelly Trail - Fourth St to Fernhill Ave			FW	CN	\$1,782,500	\$1,426,000	TE
	<i>New Trail Construction</i>						
*St Joseph Ctr Rd/Washington Ctr Rd - Clinton St to Campus Ct			FW	RW	\$250,000	\$200,000	CMAQ
	<i>Center-Left Turn Lane and Intersection Improvements</i>						
*Six Mile Creek Trail			FW	CN	\$874,000	\$699,200	TE/TAP
	<i>New Trail Construction</i>						
*State Blvd - Spy Run Ave to Cass			FW	RW	\$2,300,000	\$1,840,000	STP
	<i>Added Travel Lanes</i>						
*Various Signal locations in Fort Wayne, New Haven & Allen County (Black Signal heads with Reflective Back Plates			FW	CN	\$2,596,000	\$2,596,000	HSIP
	<i>Traffic Signal Visibility Improvements</i>						
Total Federal						\$9,476,800	

LOCATION	Project Description	FY 16	LPA	Phase	Total Cost	Federal Share	Funding Type
*Allen County Bridges			AC	PE	\$342,400	\$273,900	BR
	<i>Bridge Inspections</i>						
Bass Rd - Shakespeare Blvd to Clifty Parkway			AC	CN	\$6,144,500	\$4,915,600	STP
	<i>Road Reconstruction</i>						
Bass Rd - Clifty Parkway to Thomas Rd			AC	RW	\$562,500	\$450,000	STP
	<i>Road Reconstruction</i>						
Clinton St - Left-Turn Lane Alignment Package			FW	CN	\$722,200	\$650,000	HSIP-Pending
	<i>Intersection Improvement</i>						
Dupont Rd - Lima Rd (SR 3) to Coldwater Rd			FW	CN	\$10,000,000	\$8,000,000	TAP/STP
	<i>Added Travel Lanes & Pedestrian Underpass</i>						
Maplecrest Rd - State Blvd to Stellhorn Rd			FW	RW	\$500,000	\$400,000	STP
	<i>Road Reconstruction</i>						
Washington Center Rd - Bridge over Spy Run Creek			AC	RW	\$125,000	\$100,000	STP
	<i>Bridge Reconstruction</i>						
Total Federal						\$14,789,500	

LOCATION	Project Description	FY 17	LPA	Phase	Total Cost	Federal Share	Funding Type
Anthony Blvd - Tillman Rd to Paulding Rd	(Phase I) <i>Road Reconstruction</i>		FW	CN	\$1,250,000	\$1,000,000	STP
Broadway, Landin Rd and Rose Ave Intersection	<i>Intersection Improvement</i>		NH	RW	\$187,500	\$150,000	CMAQ-Pending
Liberty Mills Rd & County Line Rd	<i>Intersection Improvement</i>		AC	CN	\$1,062,500	\$850,000	CMAQ
*State Blvd - Spy Run Ave to Clinton St	<i>Added Travel Lanes</i>		FW	CN	\$1,500,000	\$1,200,000	STP
*St Joseph Ctr Rd/Washington Ctr Rd - Clinton St to Campus Ct	<i>Center-Left Turn Lane and Intersection Improvements</i>		FW	CN	\$2,000,000	\$1,600,000	CMAQ
Total Federal						\$4,800,000	

RURAL PROJECTS

LOCATION	Project Description	FY 14	LPA	Phase	Total Cost	Federal Share	Funding Type
Carroll Rd - Preserve Blvd to Bethel Rd	<i>Road Reconstruction</i>		HT	RW	\$84,000	\$67,200	Group IV
Coverdale Rd - from Indianapolis Rd to Airport Exp	<i>Road Reconstruction - includes small structure replacements</i>		AC	CN	\$4,596,700	\$3,976,000	Group IV
Coverdale Rd - Bridge #231 over Robinson-Brindle Ditch	<i>Bridge Replacement</i>		AC	CN	\$862,900	\$690,300	Group IV
*Minnich Rd and Tillman Rd	<i>Intersection Improvement</i>		AC	PE	\$312,500	\$281,250	HSIP
Total Federal						\$5,014,750	

LOCATION	Project Description	FY 15	LPA	Phase	Total Cost	Federal Share	Funding Type
2nd Street - Shoal Ln to Main St	<i>Road Reconstruction</i>		GR	CN	\$1,171,300	\$937,000	Group IV
Carroll Rd - Preserve Blvd to Bethel Rd	<i>Road Reconstruction</i>		HT	CN	\$1,637,500	\$1,310,000	Group IV
Total Federal						\$2,247,000	

LOCATION	Project Description	FY 17	LPA	Phase	Total Cost	Federal Share	Funding Type
*Ryan Rd/Bruick Rd: Dawkins Rd to Harper Rd	<i>Road Reconstruction</i>		AC	CN	\$3,964,800	\$3,171,800	Group IV

Project Location (Description of Project) <i>Fund Type</i>	LRP # DES #	Phase	Est. Cost (\$1000)	Year	Federal (\$1000)	State (\$1000)	Local (\$1000)	Pri- ority	LPA	A/M
*St Joseph Center Rd/Washington Center Rd: from Clinton St to Campus Ct (Center Left-Turn Lane and Intersection Improvements) <i>ITS Component - Signal Interconnection & Online</i> <i>CMAQ</i>	25-055 0710322	PE RW CN	300.0 250.0 2000.0	2014 2015 2017	240.0 200.0 1600.0	0.0 0.0 0.0	60.0 50.0 400.0	1 2 4	FW FW FW	14-7
*Six Mile Creek Trail: Southtown Centre to Lemar Dr (New Trail Construction) <i>TE/TAP</i>	0810457	PE RW CN	221.0 185.0 874.0	2010 2013 2015	165.7 112.5 699.2	0.0 0.0 0.0	55.3 72.5 174.8	 2	FW FW FW	 14-26
*State Blvd: Spy Run Ave to Cass St (Added Travel Lanes) <i>STP</i>	10-021 0400587	RW	2300.0	2015	1840.0	0.0	460.0	1	FW	14-46
*State Blvd: Spy Run Ave to Clinton St (Phase 1) (Added Travel Lanes) <i>ITS Component - Signal Interconnection & Online</i> <i>STP</i>	10-021 1005151	CN	1500.0	2017	1200.0	0.0	300.0	2	FW	14-46
*State Blvd: Clinton St to Cass St (Phase 2) (Added Travel Lanes) - STP (Bridge over Spy Run Creek) - STP (Pedestrian Bridge over State Blvd) - STP (Added Travel Lanes) - CMAQ <i>STP¹ / CMAQ²</i>	10-022 1005154 1005152 1005155 1005154	CN CN CN CN	3022.0 1800.0 500.0 1250.0	2018 2018 2018 2018	2417.6 ¹ 1440.0 ¹ 400.0 ¹ 1000.0 ²	0.0 0.0 0.0 0.0	604.4 360.0 100.0 250.0	3 3 3 3	FW AC FW FW	14-46
*Various Signal locations in Fort Wayne, New Haven & Allen County (Black Signal heads with Reflective Back Plates) (Traffic Signal Visibility Improvements) <i>HSIP</i>	1400453	PE CN	50.0 2596.0	2014 2015	0.0 2596.0	0.0 0.0	50.0 0.0	1 2	FW FW	14-43 14-43
Washington Ctr Rd: Bridge over Spy Run Creek (Bridge Reconstruction) <i>STP</i>	35-106 1382497	PE RW CN	187.5 125.0 1200.0	2014 2016 TBD	150.0 100.0 960.0	0.0 0.0 0.0	37.5 25.0 240.0	1 3 	AC AC AC	

Shaded area indicates previously obligated phase(s)

* Denotes an Amendment or Modification to Project



Statewide Transportation Improvement Program

FY2014-2017



www.in.gov/indot



U.S. Department
of Transportation

Federal Transit Administration Region V 200 West Adams St., Suite 320 Chicago, IL 60606-5253	Federal Highway Administration Indiana Division 575 N. Pennsylvania St., Rm 254 Indianapolis, IN 46204-1576
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July 11, 2013

In Reply Refer To:
HDA-IN

Ms. Brandye Hendrickson
Interim Commissioner
Indiana Department of Transportation
100 North Senate Avenue
Indianapolis, Indiana 46204

Dear Interim Commissioner Hendrickson:

The Federal Highway Administration (FHWA) and Federal Transit Administration (FTA) have received and reviewed the Indiana Department of Transportation's (INDOT) FY 2014-2017 Statewide Transportation Improvement (STIP) Program document for the State of Indiana, and hereby give our conditional approval.

It is recognized that INDOT's first two Public Private Partnership (P3) projects, the Louisville-Southern Indiana Ohio River Bridges Project (LSIORB) and the I-69 Section 5 Project, are included in this STIP. The STIP is consistent with the most recent project finance plans. As the finance plan documents are updated in the coming months, INDOT has committed to updating the STIP to be consistent with the updated finance plans.

FHWA and FTA also note this new STIP includes an Americans with Disabilities Act (ADA) summary. We appreciate INDOT's efforts to develop projects that improve access to public services for everyone.

INDOT has improved the development and quality of the STIP this year. These improvements are discussed in an enclosure to this letter. However, FHWA and FTA are only able to grant conditional approval for the FY 2014-2017 STIP because several elements of the Statewide Planning Process still need improvement. These elements are also identified in the enclosure.

With the recent passage of the *Moving Ahead for Progress in the 21st Century* (MAP-21) Act, FHWA and FTA would like to take this opportunity to ask that INDOT and the Indiana Metropolitan Planning Organizations (MPOs) be aware of new requirements for performance-based plans and programs, as well as the more formal use of performance measures. FHWA and FTA would like to collaborate with INDOT and the MPOs in the coming months to begin this transition. FHWA would also specifically like to engage with INDOT on new ways of advancing safety improvements/programs and improving the obligation rates of Highway Safety Improvement Program (HSIP) funding.

FHWA & FTA's ANALYSIS OF INDIANA's 2014-2017 STIP

INCORPORATION OF TRANSPORTATION IMPROVEMENT PROGRAMS:

At the request of the Indiana Department of Transportation (INDOT), the following Metropolitan Planning Organization's (MPO's) Transportation Improvement Programs (TIPs) are accepted for incorporation into the 2014-2017 Statewide Transportation Improvement Program (STIP):

<u>Metropolitan Planning Organization</u>	<u>TIP Coverage</u>
Madison County Council of Government, covering the Anderson urbanized area	2012-2015
Bloomington/Monroe County Metropolitan Planning Organization, covering the Bloomington urbanized area	2014-2017
Columbus Area Metropolitan Planning Commission, covering the Columbus urbanized area	2012-2016
Evansville Metropolitan Planning Organization, covering the Evansville urbanized area	2013-2016
Northeastern Indiana Regional Coordinating Council, covering the Fort Wayne urbanized area	2014-2017
Indianapolis Metropolitan Planning Organization, covering the Indianapolis urbanized area	2012-2015
Kokomo-Howard County Governmental Coordinating Council, covering the Kokomo urbanized area	2011-2014
Area Plan Commission for Tippecanoe County, covering the Lafayette and West Lafayette urbanized areas	2014-2017
Kentuckiana Regional Planning and Development Agency, covering the Louisville-Southern Indiana urbanized area	2011-2015
Delaware-Muncie Metropolitan Plan Commission, covering the Muncie urbanized area	2012-2015
Northwestern Indiana Regional Planning Commission, covering the Northwestern Indiana urbanized area	2012-2015
Michiana Area Council of Governments, covering the South Bend, Mishawaka, Elkhart and Goshen urbanized areas	2014-2017
West Central Indiana Economic Development District, covering the Terre Haute and Brazil urbanized area	2012-2015
Ohio-Kentucky-Indiana Regional Council of Governments, covering the Cincinnati urbanized area	2012-2015

comment, and comments were provided in June of this year. The agreements still need to be vetted thru and concurred with by the MPOs and transit operators. In order to resolve this element of the State's corrective action, new planning agreements must be executed.

INDOT's "MPO Manual" or handbook that describes how key products are developed and identifies with whom these documents are prepared and when they will be approved by the States: INDOT has prepared a revised "Planning Roles and Responsibilities" document to address this element and submitted it to FHWA. MPOs also need an opportunity to review and comment, on the document before it is finalized. Until this is done, this element of the State's corrective action also remains in place.

Revisions to the Local Public Agency (LPA) Manual to address the role of MPOs in the LPA project development process: Chapter Six of INDOT's LPA Guidance document has been revised to help clarify the roles of the MPOs in the LPA project development process. This element of the corrective action is therefore lifted. FHWA and FTA recommend INDOT review the effectiveness of the manual with the MPOs and its District offices to confirm procedures are working effectively and consistently, and also identify any potential areas for improvement.

Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2014 - 2017

SPONSOR	DES	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	PROGRAM	PHASE	FEDERAL	MATCH	Estimated Cost left to Complete Project*	2014	2015	2016	2017
Allen County																
Indiana Department of Transportation	0800202	I 469	Signing Installation / Repair	From I-69, S jct to SR 37	Fort Wayne	24.158	Interstate	District Other Construction	CN	\$270,000.00	\$30,000.00			\$300,000.00		
Indiana Department of Transportation	0300086	I 69	Bridge Deck Replacement	Bridge over Cedar Creek (NB), 3.62 miles N of SR 1	Fort Wayne	0	Interstate	Bridge Construction	CN	\$1,410,300.00	\$156,700.00			\$1,567,000.00		
Indiana Department of Transportation	0300087	I 69	Bridge Deck Replacement	Bridge over Cedar Creek (SB), 3.62 miles N of SR 1	Fort Wayne	0	Interstate	Bridge Construction	CN	\$1,410,300.00	\$156,700.00			\$1,567,000.00		
Indiana Department of Transportation	0400603	I 469	Pavement Replacement	From (Winchester Rd) 2.63 miles E of SR 1 to .05 mile E of US 27	Fort Wayne	3.603	Interstate	Road Construction	CN	\$19,072,800.00	\$2,119,200.00			\$21,192,000.00		
								Road Consulting	CN	\$0.00	\$0.00			\$0.00		
Indiana Department of Transportation	0400917	I 469	Bridge Deck Overlay	EB bridge over Snyder Ditch, 2.73 miles E of SR 1	Fort Wayne	3.503	Interstate	Road Construction	CN	\$268,200.00	\$29,800.00			\$298,000.00		
Indiana Department of Transportation	0400918	I 469	Bridge Deck Overlay	EB bridge over the St. Mary's River, 0.45 mile W of US 27	Fort Wayne	0	Interstate	Road Construction	CN	\$409,500.00	\$45,500.00			\$0.00	\$0.00	\$455,000.00
Indiana Department of Transportation	0400919	I 469	Bridge Deck Overlay	EB bridge over US 27, 4.96 miles E of SR 1	Fort Wayne	0	Interstate	Road Construction	CN	\$451,800.00	\$50,200.00			\$0.00	\$0.00	\$502,000.00
Indiana Department of Transportation	0400922	I 469	Bridge Deck Overlay	WB bridge over Snyder Ditch, 2.73 miles E of SR 1	Fort Wayne	0	Interstate	Road Construction	CN	\$259,200.00	\$28,800.00			\$0.00	\$0.00	\$288,000.00
Indiana Department of Transportation	0400923	I 469	Bridge Deck Overlay	WB bridge over the St. Mary's River, 0.45 mile W of US 27	Fort Wayne	0	Interstate	Road Construction	CN	\$400,500.00	\$44,500.00			\$0.00	\$0.00	\$445,000.00
Indiana Department of Transportation	0400924	I 469	Bridge Deck Overlay	WB bridge over US 27, 4.96 miles E of SR 1	Fort Wayne	.313	Interstate	Road Construction	CN	\$432,900.00	\$48,100.00			\$0.00	\$0.00	\$481,000.00
Fort Wayne	0710322	IR 1001	Intersect. Improv. W/ Added Turn Lanes	Clinton St & Washington Center Rd	Fort Wayne	.2	On Federal Aid	100% Local Funds	CN	\$0.00	\$400,000.00				\$400,000.00	
								Fort Wayne MPO	CN	\$1,600,000.00	\$0.00				\$1,600,000.00	
								Fort Wayne MPO	RW	\$200,000.00	\$0.00		\$200,000.00			
								100% Local Funds	RW	\$0.00	\$50,000.00		\$50,000.00			
Fort Wayne	1005151	ST 1001	Added Travel Lanes	State Blvd ? Spy Run to Clinton St (Phase One)	Fort Wayne	.19	On Federal Aid	Fort Wayne MPO	CN	\$1,040,000.00	\$0.00			\$1,040,000.00		
								100% Local Funds	CN	\$0.00	\$260,000.00			\$260,000.00		
								Fort Wayne MPO	PE	\$160,000.00	\$0.00			\$160,000.00		

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Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2014 - 2017

SPONSOR	DES	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	PROGRAM	PHASE	FEDERAL	MATCH	Estimated Cost left to Complete Project*	2014	2015	2016	2017
Fort Wayne	1005151	ST 1001	Added Travel Lanes	State Blvd ? Spy Run to Clinton St (Phase One)	Fort Wayne	.19	On Federal Aid	100% Local Funds	PE	\$0.00	\$40,000.00			\$40,000.00		
Indiana Department of Transportation	0301149	US 30	Small Structure Pipe Lining	Br # 3 over Seegar Ditch, 2.16 Miles West of US 33.	Fort Wayne	0	NHS	Bridge Construction	CN	\$103,200.00	\$25,800.00			\$0.00	\$129,000.00	
Indiana Department of Transportation	0800143	US 30	Small Structure Pipe Lining	Small Structure over a Field Run, 11.55 miles E. of SR 205.	Fort Wayne	0	NHS	Bridge Construction	CN	\$103,200.00	\$25,800.00			\$0.00	\$129,000.00	
Allen County	0500892	IR 1001	Road Reconstruction (3R/4R Standards)	Coverdale Rd, from Indianapolis Rd to Airport Exp, 2.25 mi NE of I 469	Fort Wayne	1.5	On Federal Aid	100% Local Funds	CN	\$0.00	\$558,400.00		\$558,400.00			
								100% Local Funds	PE	\$0.00	\$70,840.00		\$70,840.00			
								Group IV Program	PE	\$283,360.00	\$0.00		\$283,360.00			
								Group IV Program	CN	\$2,233,600.00	\$0.00		\$2,233,600.00			
Allen County	0710344	IR 1001	Bridge Deck Replacement	Coverdale Rd: Bridge # 231 over Robinson-Brindle Drain	Fort Wayne	0	On Federal Aid	100% Local Funds	PE	\$0.00	\$11,940.00		\$11,940.00			
								100% Local Funds	CN	\$0.00	\$79,431.00		\$79,431.00			
								Local Bridge Program	CN	\$317,724.00	\$0.00		\$317,724.00			
								Local Bridge Program	PE	\$47,760.00	\$0.00		\$47,760.00			
Allen County	0710345	IR 1001	Bridge Replacement, Other Construction	Coverdale Rd: Bridge #232 over Suter Ditch	Fort Wayne	0	STP	100% Local Funds	CN	\$0.00	\$61,400.00		\$61,400.00			
								Group IV Program	CN	\$245,600.00	\$0.00		\$245,600.00			
Indiana Department of Transportation	0301145	US 27	Small Structure Pipe Lining	Pipeliner for Valentine Ditch, 3.2 miles N of Adams/Allen Co Line.	Fort Wayne	0	NHS	Bridge ROW	RW	\$22,400.00	\$5,600.00			\$2,000.00	\$26,000.00	
								Bridge Construction	CN	\$71,200.00	\$17,800.00					\$89,000.00
								Bridge Consulting	PE	\$16,000.00	\$4,000.00		\$20,000.00			
Indiana Department of Transportation	1296076	SR 205	Small Structure Pipe Lining	Pipeliner for Johnson Ditch, 2.09 Miles North of US 33.	Fort Wayne	0	BR	Bridge Construction	CN	\$67,200.00	\$16,800.00					\$84,000.00
								Bridge ROW	RW	\$1,600.00	\$400.00				\$2,000.00	

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Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2014 - 2017

SPONSOR	DES	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	PROGRAM	PHASE	FEDERAL	MATCH	Estimated Cost left to Complete Project*	2014	2015	2016	2017
Indiana Department of Transportation	0901185	I 469	Bridge Deck Overlay	EB Bridge over Houk Ditch, 2.19 mi E of US 27/US 33 interchange.	Fort Wayne	0	Interstate	Bridge Construction	CN	\$171,000.00	\$19,000.00				\$0.00	\$190,000.00
Indiana Department of Transportation	0901186	I 469	Bridge Deck Overlay	WB Bridge over Houk Ditch, 2.19 mi E of US 27/US 33 interchange.	Fort Wayne	0	Interstate	Bridge Construction	CN	\$171,000.00	\$19,000.00				\$0.00	\$190,000.00
Indiana Department of Transportation	0901298	I 69	Interchange Modification	At the I-69 and SR 1 (Dupont Rd) Interchange	Fort Wayne	.346	Interstate	Mobility Construction	CN	\$1,882,923.30	\$209,213.70		\$2,092,137.00			
Indiana Department of Transportation	1173208	ST 1011	Bike/Pedestrian Facilities	SR1/Dupont Rd Trail crossing of I 69	Fort Wayne	.4	TE	Enhancement - Construction	CN	\$239,200.00	\$59,800.00		\$299,000.00			
Indiana Department of Transportation	1297170	SR 1	Bridge Deck Overlay	Bridge on SR 1 over I-69 at SR 1/(Dupont Rd) & I-69 Interchange.	Fort Wayne	0	Interstate	Mobility Construction	CN	\$578,067.30	\$64,229.70		\$642,297.00			
Indiana Department of Transportation	0400872	US 27	Pavement Replacement	From (Edgewood Ave) 1.01 miles S of SR 930, to SR 930 (Coliseum Blvd).	Fort Wayne	1.079	NHS	Road Construction	CN	\$1,550,400.00	\$387,600.00		\$1,938,000.00			
Fort Wayne	1005152	ST 1001	New Bridge, Other	State Blvd ? Bridge of Spy Run Creek	Fort Wayne	.04	On Federal Aid	Fort Wayne MPO	CN	\$1,440,000.00	\$0.00				\$1,440,000.00	
								100% Local Funds	CN	\$0.00	\$360,000.00				\$360,000.00	
Fort Wayne	1005154	ST 1001	Added Travel Lanes	State Blvd ? Cass St to Clinton St (Phase Two)	Fort Wayne	.46	On Federal Aid	Fort Wayne MPO	PE	\$457,600.00	\$0.00				\$457,600.00	
								100% Local Funds	PE	\$0.00	\$114,400.00				\$114,400.00	
								Fort Wayne MPO	CN	\$2,960,000.00	\$0.00				\$2,960,000.00	
								100% Local Funds	CN	\$0.00	\$740,000.00				\$740,000.00	
Fort Wayne	1005155	ST 1001	Bike/Pedestrian Facilities	State Blvd ? Pufferbelly Pedestrian Brg over State Blvd.	Fort Wayne	.04	Off Federal Aid	Fort Wayne MPO	CN	\$400,000.00	\$0.00				\$400,000.00	
								100% Local Funds	CN	\$0.00	\$100,000.00				\$100,000.00	
Allen County	0400584	VA VARI	Road Reconstruction (3R/4R Standards)	Gump Road; from SR3 (Lima Rd) tp Coldwater Road	Fort Wayne	0	On Federal Aid	Fort Wayne MPO	CN	\$6,246,400.00	\$0.00		\$6,246,400.00			
								100% Local Funds	CN	\$0.00	\$1,561,600.00		\$1,561,600.00			
Fort Wayne	0810513	ST 1011	Bike/Pedestrian Facilities	Covington Rd, Aboite Twnshp Trail System - W Hamilton Rd to Beal Taylor Ditch	Fort Wayne	.9	Off Federal Aid	100% Local Funds	CN	\$0.00	\$90,800.00				\$90,800.00	
								100% Local Funds	PE	\$0.00	\$37,850.00				\$37,850.00	

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Indiana Department of Transportation (INDOT)
State Preservation and Local Initiated Projects FY 2014 - 2017

SPONSOR	DES	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	PROGRAM	PHASE	FEDERAL	MATCH	Estimated Cost left to Complete Project*	2014	2015	2016	2017
Indiana Department of Transportation	1297575	ST 1001	Railroad Protection	Covington Avenue at NS RR in Fort Wayne	Fort Wayne	0	Off Federal Aid	Local Safety Program	CN	\$280,000.00	\$0.00		\$280,000.00			
Allen County	1382497	ST 1027	Bridge Deck Reconstruction	Bridge 95 on Washington Cntr Rd over Spy Run Creek	Fort Wayne	.1	On Federal Aid	100% Local Funds	PE	\$0.00	\$37,500.00		\$37,500.00			
								Fort Wayne MPO	RW	\$100,000.00	\$0.00				\$100,000.00	
								100% Local Funds	RW	\$0.00	\$25,000.00				\$25,000.00	
								Fort Wayne MPO	PE	\$150,000.00	\$0.00		\$150,000.00			
Fort Wayne	1382498	ST 1026	Road Reconstruction (3R/4R Standards)	Anthony Blvd: from Tillman Rd to Rudisill Blvd	Fort Wayne	2.3	On Federal Aid	Fort Wayne MPO	CN	\$1,000,000.00	\$0.00					\$1,000,000.00
								100% Local Funds	PE	\$0.00	\$60,000.00			\$60,000.00		
								100% Local Funds	CN	\$0.00	\$250,000.00					\$250,000.00
								Fort Wayne MPO	PE	\$240,000.00	\$0.00			\$240,000.00		
Allen County	1382493	IR 1025	Road Reconstruction (3R/4R Standards)	Bass Rd: from Shakespeare Blvd to Clifty Parkway	Fort Wayne	0	On Federal Aid	Fort Wayne MPO	CN	\$4,915,600.00	\$0.00				\$4,915,600.00	
								100% Local Funds	CN	\$0.00	\$1,228,900.00				\$1,228,900.00	
								Fort Wayne MPO	RW	\$446,600.00	\$0.00		\$446,600.00			
								100% Local Funds	RW	\$0.00	\$111,650.00		\$111,650.00			
Allen County	1382492	IR 1025	Road Reconstruction (3R/4R Standards)	Bass Rd: from Clifty Parkway to Thomas Rd	Fort Wayne	.7	On Federal Aid	100% Local Funds	RW	\$0.00	\$112,500.00				\$112,500.00	
								Fort Wayne MPO	RW	\$450,000.00	\$0.00				\$450,000.00	
Fort Wayne	0400587	ST 1001	Added Travel Lanes	State Blvd ? Spy Run to Cass Street	Fort Wayne	.16	On Federal Aid	100% Local Funds	RW	\$0.00	\$460,000.00		\$460,000.00			
								Fort Wayne MPO	RW	\$1,840,000.00	\$0.00		\$1,840,000.00			

Allen County Total

Federal: \$148,006,741.00

Match :\$29,318,293.00

2014: \$41,144,522.00

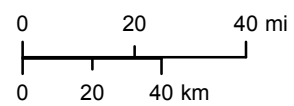
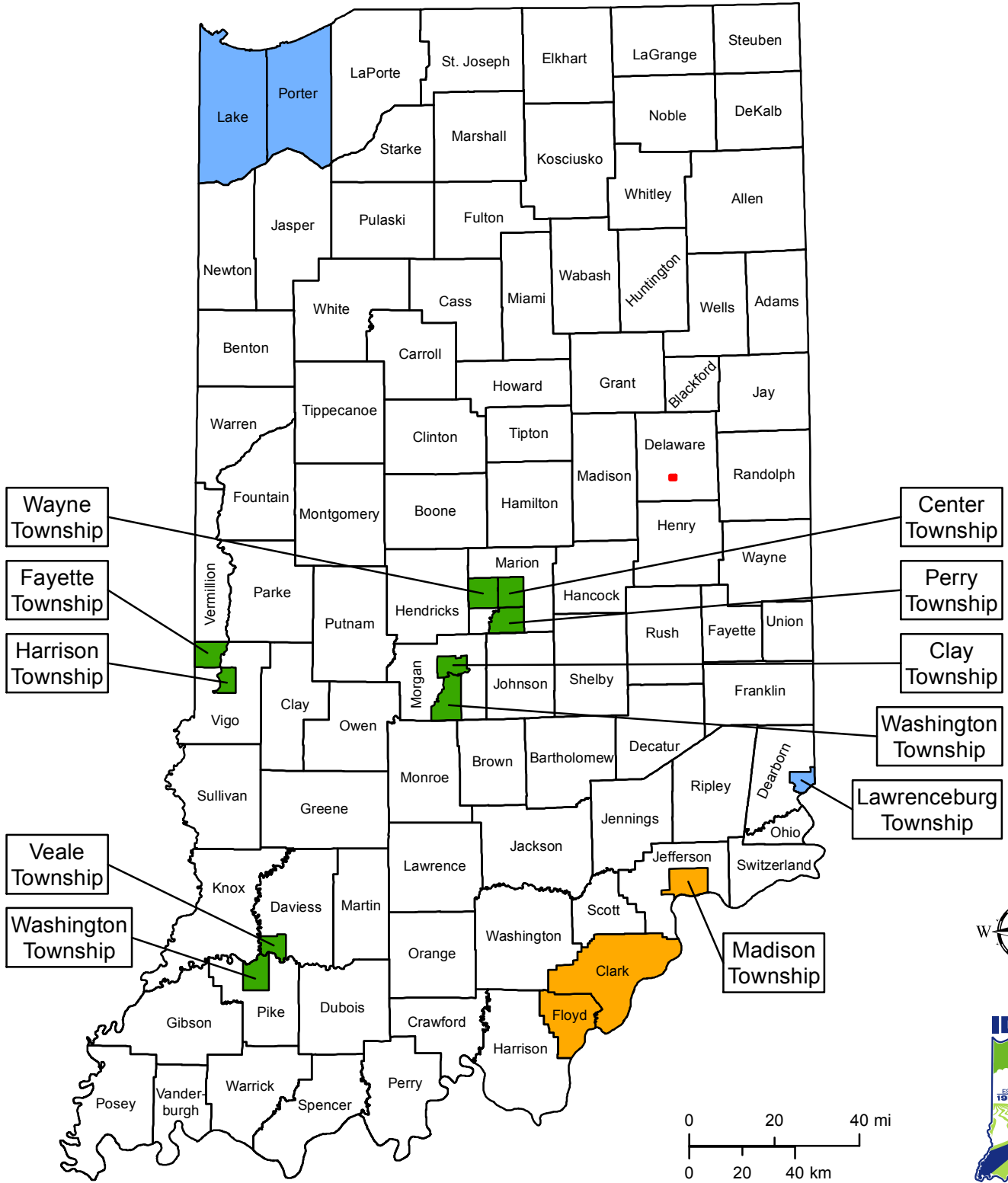
2015: \$49,155,740.00

2016: \$37,093,772.00

2017: \$49,931,000.00

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Nonattainment Areas as of 7/25/2013

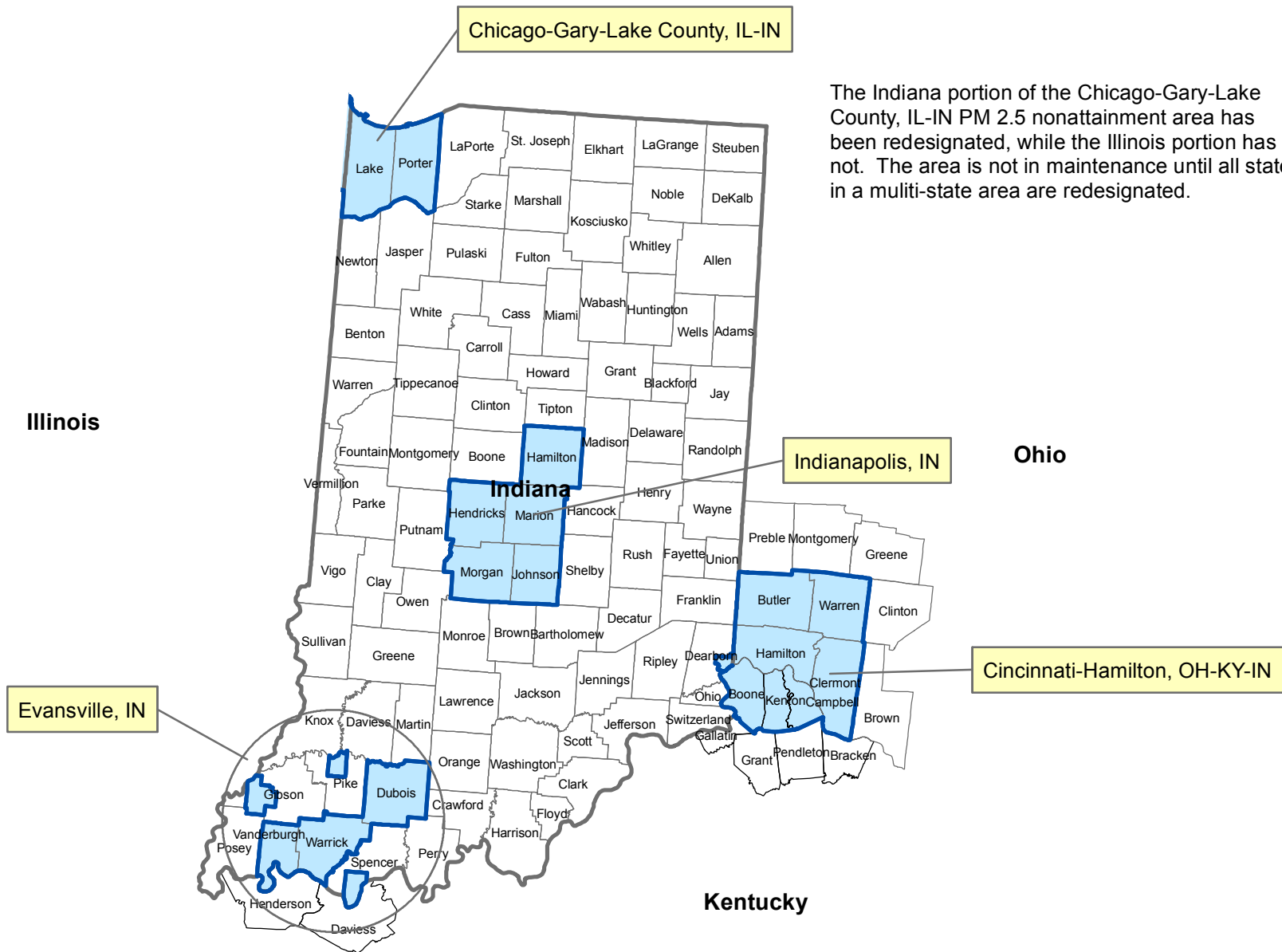


This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

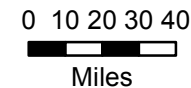
Mapped By: B. Callahan, OAQ **Date:** 7/26/2013
Source: Office of Air Quality
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

- Nonattainment for the 1997 Annual Fine Particle Standard
- Nonattainment for the 2008 8-Hour Ozone Standard
- Nonattainment for the 2010 1-Hour SO₂ Standard
- Nonattainment for the 2008 Lead Standard



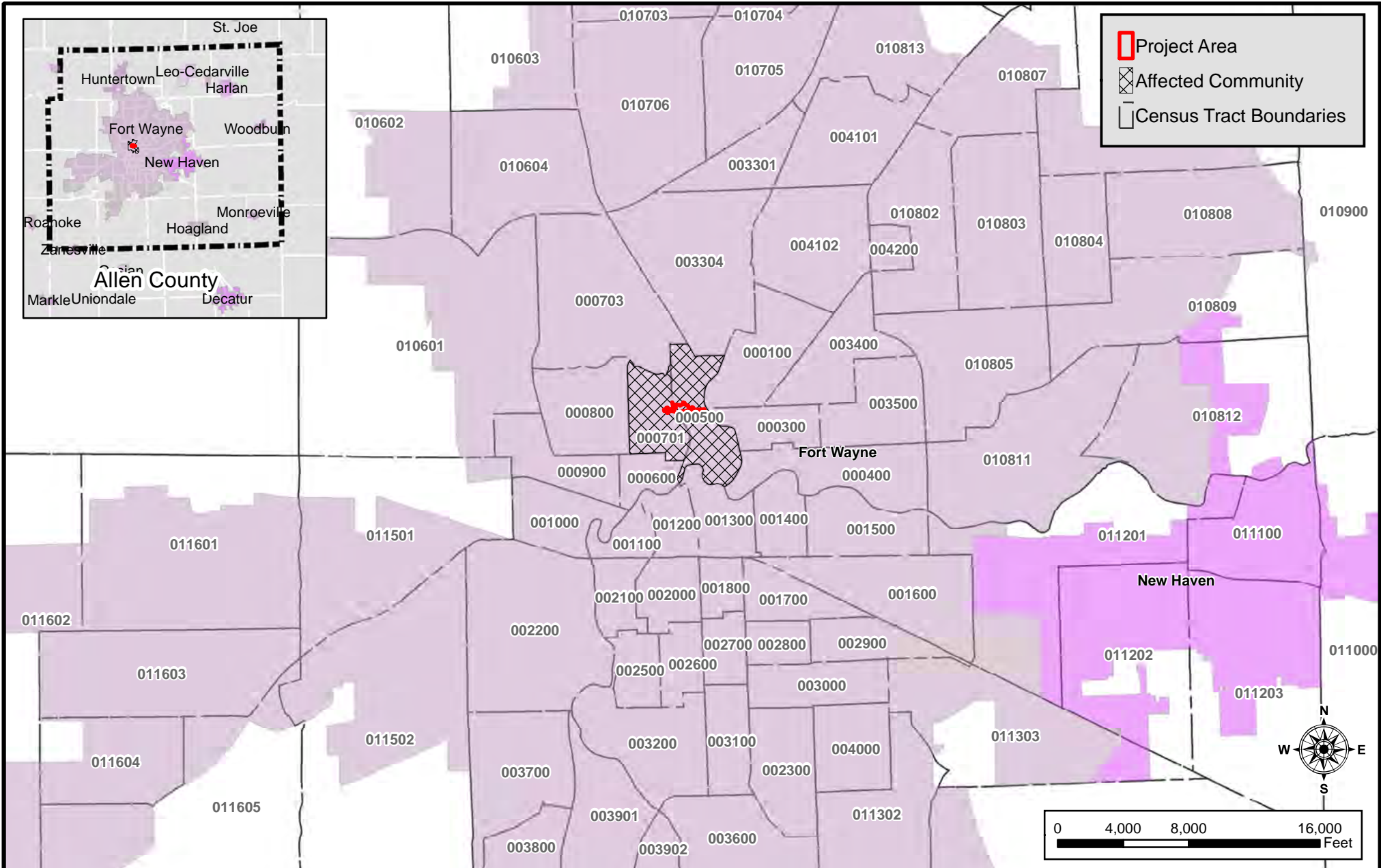


PM-2.5 Maintenance Areas



Appendix H: Environmental Justice

- Environmental Justice Mapping and Data



2010 Census Tracts and EJ Analysis

Applicant: City of Fort Wayne, Board of Public Works
Citizen Square
200 East Berry Street, Suite 240
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Analysis of Allen County and Affected Block Groups

	COC	AC1	AC2
	Fort Wayne City	Census Tract 5, Allen County, Indiana	Census Tract 7.01, Allen County, Indiana
LOW-INCOME			
Total Population for whom poverty status is determined (estimated)	248,772	2,766	3,342
Total Population Below Pverty Level (estimated)	40,534	922	931
Percent Low-income	16.3%	33.3%	27.9%
125 Percent of COC	20.4%	AC>125% COC	AC>125% COC
Potential Low-income EJ Impact?		Yes	Yes
MINORITY			
Total population (all races)	254,228	2,939	3,343
White alone or in combination [1]	181,101	1,915	2,493
Number Non-white/Minority (P007001-P007003)	73,127	1,024	850
Percent Non-white/Minority	28.8%	34.8%	25.4%
125 Percent of COC	36.0%	AC>125% COC	AC>125% COC
Potential Minority EJ Impact?		No	No



B17001

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Universe: Population for whom poverty status is determined

2007-2011 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

	Allen County, Indiana		Census Tract 1, Allen County, Indiana		Census Tract 3, Allen County, Indiana		Census Tract 5, Allen County, Indiana
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total:	347,376	+/-623	2,628	+/-295	3,428	+/-300	2,766
Income in the past 12 months below poverty level:	46,550	+/-2,622	388	+/-187	525	+/-274	922
Male:	20,568	+/-1,382	128	+/-79	223	+/-112	337
Under 5 years	2,365	+/-283	11	+/-18	24	+/-30	34
5 years	467	+/-145	0	+/-89	0	+/-89	22
6 to 11 years	2,850	+/-380	0	+/-89	9	+/-15	135
12 to 14 years	1,480	+/-271	0	+/-89	51	+/-55	0
15 years	508	+/-160	0	+/-89	0	+/-89	10
16 and 17 years	863	+/-180	0	+/-89	0	+/-89	0
18 to 24 years	2,761	+/-431	32	+/-33	43	+/-43	12
25 to 34 years	2,717	+/-361	17	+/-23	0	+/-89	21
35 to 44 years	2,148	+/-333	23	+/-36	31	+/-33	31
45 to 54 years	2,164	+/-320	12	+/-19	37	+/-31	52
55 to 64 years	1,493	+/-242	33	+/-50	17	+/-19	10
65 to 74 years	446	+/-117	0	+/-89	0	+/-89	10
75 years and over	306	+/-120	0	+/-89	11	+/-17	0
Female:	25,982	+/-1,542	260	+/-148	302	+/-183	585
Under 5 years	2,878	+/-385	50	+/-48	27	+/-24	52
5 years	551	+/-160	0	+/-89	0	+/-89	10
6 to 11 years	3,203	+/-465	54	+/-54	19	+/-22	89
12 to 14 years	1,197	+/-227	0	+/-89	0	+/-89	28

	Allen County, Indiana		Census Tract 1, Allen County, Indiana		Census Tract 3, Allen County, Indiana		Census Tract 5, Allen County, Indiana
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
15 years	565	+/-128	0	+/-89	0	+/-89	12
16 and 17 years	821	+/-156	0	+/-89	32	+/-31	0
18 to 24 years	3,704	+/-388	8	+/-14	61	+/-50	102
25 to 34 years	4,291	+/-399	67	+/-48	23	+/-24	156
35 to 44 years	2,942	+/-339	43	+/-56	43	+/-36	49
45 to 54 years	2,313	+/-290	17	+/-20	34	+/-54	29
55 to 64 years	1,761	+/-245	11	+/-17	12	+/-18	29
65 to 74 years	808	+/-235	0	+/-89	28	+/-44	0
75 years and over	948	+/-181	10	+/-17	23	+/-21	29
Income in the past 12 months at or above poverty level:	300,826	+/-2,708	2,240	+/-358	2,903	+/-229	1,844
Male:	148,868	+/-1,458	1,210	+/-225	1,412	+/-163	967
Under 5 years	10,823	+/-307	129	+/-58	119	+/-78	68
5 years	2,172	+/-315	30	+/-29	33	+/-39	9
6 to 11 years	12,111	+/-535	128	+/-87	94	+/-51	14
12 to 14 years	7,002	+/-529	91	+/-74	30	+/-32	10
15 years	2,029	+/-258	0	+/-89	41	+/-39	0
16 and 17 years	4,717	+/-264	21	+/-24	37	+/-31	0
18 to 24 years	12,999	+/-466	68	+/-46	129	+/-79	100
25 to 34 years	20,030	+/-364	118	+/-59	316	+/-102	232
35 to 44 years	20,842	+/-325	249	+/-73	185	+/-70	127
45 to 54 years	22,410	+/-324	207	+/-57	164	+/-53	148
55 to 64 years	17,590	+/-268	97	+/-55	128	+/-45	130
65 to 74 years	9,161	+/-151	40	+/-32	87	+/-40	83
75 years and over	6,982	+/-172	32	+/-28	49	+/-36	46
Female:	151,958	+/-1,579	1,030	+/-165	1,491	+/-169	877
Under 5 years	9,950	+/-385	93	+/-61	68	+/-43	84
5 years	2,172	+/-334	0	+/-89	54	+/-56	21
6 to 11 years	12,366	+/-488	20	+/-24	162	+/-53	61
12 to 14 years	6,204	+/-438	55	+/-62	68	+/-49	29
15 years	1,875	+/-227	0	+/-89	6	+/-10	0
16 and 17 years	4,538	+/-279	34	+/-39	18	+/-23	0
18 to 24 years	12,554	+/-376	73	+/-41	97	+/-53	62
25 to 34 years	19,262	+/-429	125	+/-63	302	+/-69	149
35 to 44 years	20,385	+/-340	146	+/-80	193	+/-58	79
45 to 54 years	23,027	+/-311	144	+/-51	147	+/-48	214
55 to 64 years	18,705	+/-248	82	+/-42	232	+/-69	65
65 to 74 years	10,570	+/-264	131	+/-42	85	+/-51	31
75 years and over	10,350	+/-332	127	+/-37	59	+/-36	82

	Census Tract 5, Allen County, Indiana	Census Tract 7.01, Allen County, Indiana		Fort Wayne city, Indiana	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total:	+/-396	3,342	+/-431	248,772	+/-635
Income in the past 12 months below poverty level:	+/-351	931	+/-368	40,534	+/-2,305
Male:	+/-194	479	+/-214	17,933	+/-1,203
Under 5 years	+/-29	41	+/-42	2,091	+/-281
5 years	+/-27	0	+/-89	441	+/-148
6 to 11 years	+/-122	72	+/-82	2,456	+/-367
12 to 14 years	+/-89	36	+/-44	1,271	+/-233
15 years	+/-16	0	+/-89	435	+/-156
16 and 17 years	+/-89	0	+/-89	695	+/-169
18 to 24 years	+/-20	108	+/-84	2,544	+/-416
25 to 34 years	+/-23	67	+/-52	2,260	+/-321
35 to 44 years	+/-27	70	+/-55	1,915	+/-315
45 to 54 years	+/-56	9	+/-14	1,866	+/-269
55 to 64 years	+/-17	76	+/-76	1,314	+/-242
65 to 74 years	+/-16	0	+/-89	383	+/-111
75 years and over	+/-89	0	+/-89	262	+/-110
Female:	+/-221	452	+/-211	22,601	+/-1,413
Under 5 years	+/-51	131	+/-100	2,607	+/-378
5 years	+/-15	9	+/-14	506	+/-160
6 to 11 years	+/-85	40	+/-42	2,798	+/-416
12 to 14 years	+/-37	32	+/-29	943	+/-201
15 years	+/-18	0	+/-89	496	+/-127
16 and 17 years	+/-89	7	+/-12	722	+/-149
18 to 24 years	+/-89	66	+/-56	3,257	+/-387
25 to 34 years	+/-116	125	+/-80	3,754	+/-381
35 to 44 years	+/-42	13	+/-22	2,542	+/-317
45 to 54 years	+/-23	5	+/-8	1,972	+/-279
55 to 64 years	+/-33	5	+/-9	1,547	+/-232
65 to 74 years	+/-89	10	+/-15	731	+/-227
75 years and over	+/-47	9	+/-15	726	+/-155
Income in the past 12 months at or above poverty level:	+/-322	2,411	+/-349	208,238	+/-2,387
Male:	+/-218	1,171	+/-198	101,819	+/-1,318
Under 5 years	+/-56	101	+/-53	7,637	+/-445
5 years	+/-16	38	+/-41	1,336	+/-265
6 to 11 years	+/-22	110	+/-78	8,027	+/-553
12 to 14 years	+/-15	26	+/-39	4,611	+/-506
15 years	+/-89	27	+/-26	1,355	+/-209
16 and 17 years	+/-89	56	+/-44	3,183	+/-297
18 to 24 years	+/-68	141	+/-71	9,180	+/-479
25 to 34 years	+/-106	242	+/-77	14,673	+/-445
35 to 44 years	+/-67	75	+/-44	13,855	+/-380
45 to 54 years	+/-89	142	+/-63	14,793	+/-423
55 to 64 years	+/-58	88	+/-46	12,168	+/-381

	Census Tract 5, Allen County, Indiana	Census Tract 7.01, Allen County, Indiana		Fort Wayne city, Indiana	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
65 to 74 years	+/-54	91	+/-49	5,890	+/-280
75 years and over	+/-37	34	+/-26	5,111	+/-228
Female:	+/-190	1,240	+/-194	106,419	+/-1,563
Under 5 years	+/-71	200	+/-166	7,098	+/-436
5 years	+/-24	10	+/-17	1,423	+/-279
6 to 11 years	+/-47	86	+/-48	8,228	+/-428
12 to 14 years	+/-26	56	+/-41	3,987	+/-400
15 years	+/-89	22	+/-21	1,155	+/-209
16 and 17 years	+/-89	7	+/-14	2,863	+/-287
18 to 24 years	+/-48	36	+/-29	9,311	+/-466
25 to 34 years	+/-82	256	+/-86	14,326	+/-501
35 to 44 years	+/-44	112	+/-60	13,660	+/-417
45 to 54 years	+/-94	205	+/-48	15,885	+/-395
55 to 64 years	+/-37	73	+/-43	13,081	+/-360
65 to 74 years	+/-31	135	+/-66	7,417	+/-319
75 years and over	+/-59	42	+/-26	7,985	+/-322

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

While the 2007-2011 American Community Survey (ACS) data generally reflect the December 2009 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2000 data. Boundaries for urban areas have not been updated since Census 2000. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2007-2011 American Community Survey

Explanation of Symbols:

1. An '***' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.
4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.



B03002

HISPANIC OR LATINO ORIGIN BY RACE
Universe: Total population
2007-2011 American Community Survey 5-Year Estimates

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Data and Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities and towns and estimates of housing units for states and counties.

	Allen County, Indiana		Census Tract 1, Allen County, Indiana		Census Tract 3, Allen County, Indiana		Census Tract 5, Allen County, Indiana
	Estimate	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error	Estimate
Total:	353,721	*****	2,649	+/-296	3,428	+/-300	2,939
Not Hispanic or Latino:	331,270	*****	2,584	+/-302	3,367	+/-281	2,579
White alone	272,150	+/-196	2,219	+/-214	3,136	+/-286	1,915
Black or African American alone	41,189	+/-674	229	+/-272	113	+/-69	383
American Indian and Alaska Native alone	945	+/-175	16	+/-21	13	+/-19	0
Asian alone	8,882	+/-361	27	+/-43	48	+/-78	163
Native Hawaiian and Other Pacific Islander alone	51	+/-55	0	+/-89	0	+/-89	9
Some other race alone	454	+/-207	23	+/-37	0	+/-89	0
Two or more races:	7,599	+/-812	70	+/-83	57	+/-49	109
Two races including Some other race	188	+/-109	19	+/-30	0	+/-89	0
Two races excluding Some other race, and three or more races	7,411	+/-798	51	+/-75	57	+/-49	109
Hispanic or Latino:	22,451	*****	65	+/-82	61	+/-67	360
White alone	11,301	+/-873	64	+/-82	20	+/-26	312
Black or African American alone	460	+/-233	0	+/-89	0	+/-89	0
American Indian and Alaska Native alone	226	+/-146	0	+/-89	0	+/-89	0
Asian alone	17	+/-22	0	+/-89	0	+/-89	0
Native Hawaiian and Other Pacific Islander alone	0	+/-89	0	+/-89	0	+/-89	0
Some other race alone	8,815	+/-936	1	+/-2	24	+/-37	29
Two or more races:	1,632	+/-362	0	+/-89	17	+/-26	19
Two races including Some other race	1,136	+/-324	0	+/-89	17	+/-26	19
Two races excluding Some other race, and three or more races	496	+/-204	0	+/-89	0	+/-89	0

	Census Tract 5, Allen County, Indiana	Census Tract 7.01, Allen County, Indiana		Fort Wayne city, Indiana	
	Margin of Error	Estimate	Margin of Error	Estimate	Margin of Error
Total:	+/-396	3,343	+/-432	254,228	+/-300
Not Hispanic or Latino:	+/-317	3,046	+/-461	234,952	+/-559
White alone	+/-265	2,493	+/-332	181,101	+/-866
Black or African American alone	+/-161	96	+/-71	39,047	+/-794
American Indian and Alaska Native alone	+/-89	7	+/-11	740	+/-161
Asian alone	+/-137	163	+/-184	6,816	+/-544
Native Hawaiian and Other Pacific Islander alone	+/-16	0	+/-89	51	+/-55
Some other race alone	+/-89	37	+/-49	411	+/-206
Two or more races:	+/-116	250	+/-224	6,786	+/-801
Two races including Some other race	+/-89	0	+/-89	159	+/-94
Two races excluding Some other race, and three or more races	+/-116	250	+/-224	6,627	+/-795
Hispanic or Latino:	+/-241	297	+/-220	19,276	+/-540
White alone	+/-209	96	+/-99	9,784	+/-887
Black or African American alone	+/-89	0	+/-89	442	+/-231
American Indian and Alaska Native alone	+/-89	0	+/-89	182	+/-126
Asian alone	+/-89	0	+/-89	0	+/-89
Native Hawaiian and Other Pacific Islander alone	+/-89	0	+/-89	0	+/-89
Some other race alone	+/-37	169	+/-198	7,668	+/-955
Two or more races:	+/-34	32	+/-31	1,200	+/-308
Two races including Some other race	+/-34	32	+/-31	795	+/-265
Two races excluding Some other race, and three or more races	+/-89	0	+/-89	405	+/-191

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see Accuracy of the Data). The effect of nonsampling error is not represented in these tables.

While the 2007-2011 American Community Survey (ACS) data generally reflect the December 2009 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural population, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2000 data. Boundaries for urban areas have not been updated since Census 2000. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Source: U.S. Census Bureau, 2007-2011 American Community Survey

Explanation of Symbols:

1. An '*' entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
2. An '-' entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution.
3. An '-' following a median estimate means the median falls in the lowest interval of an open-ended distribution.

4. An '+' following a median estimate means the median falls in the upper interval of an open-ended distribution.
5. An '****' entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
6. An '*****' entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
7. An 'N' entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
8. An '(X)' means that the estimate is not applicable or not available.

Appendix I: Noise Analysis

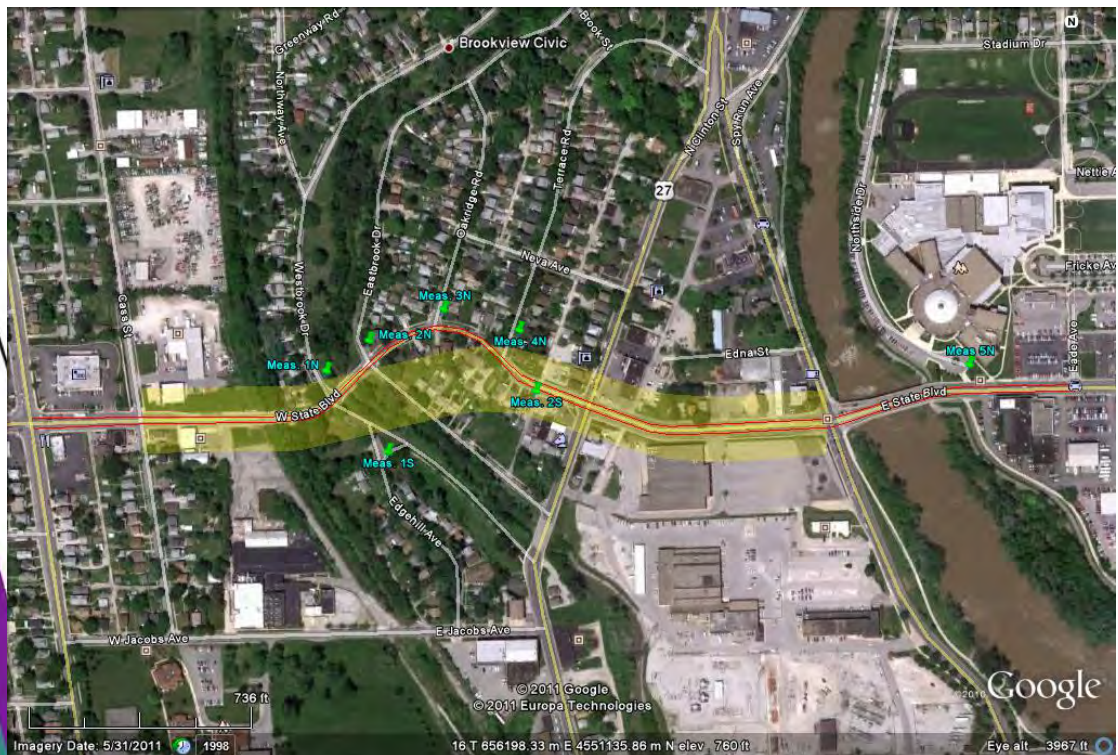
- Noise Study Report – October 10, 2011
- INDOT's Determination of Technical Sufficiency – October 18, 2011

Noise Study Report

State Boulevard Reconstruction, Ft. Wayne

Submitted to:

American Structurepoint, Inc.



Submitted by:

The Corradino Group, Inc.

TED STONE

Ted Stone

October 10, 2011

Note: The Corradino Group has purchased the professional version of Google Earth and hereby credits Google Earth for the aerial photography used herein.

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Executive Summary

This Noise Study Report accompanies a Categorical Exclusion (CE) level 4¹ for the reconstruction of State Boulevard between Cass Street and Spy Run Avenue (US 27) in Ft. Wayne, Wayne Township, Allen County, Indiana (Figure 1). The purposes of this approximately 2500-foot reconstruction are to improve pedestrian and motor vehicle safety along State Boulevard, decrease traffic congestion, alleviate roadway flooding, and correct substandard sight distances and geometrics.

As some proposed road improvements are on new alignment, and federal funds are involved, under 23 CFR, part 772, the project is considered a “Type I” noise project. This means a noise analysis should be performed to determine whether the project will cause noise impacts and, if so, whether there are feasible and reasonable ways to mitigate those impacts.

This noise analysis follows the guidance in the Federal Highway Administration’s (FHWA’s) *Highway Traffic Noise: Analysis and Abatement Guidance* (July 2010), the Indiana Department of Transportation’s (INDOT’s) *Procedural Manual for Preparing Environmental Documents* and its *Traffic Noise Policy* (July 2011).

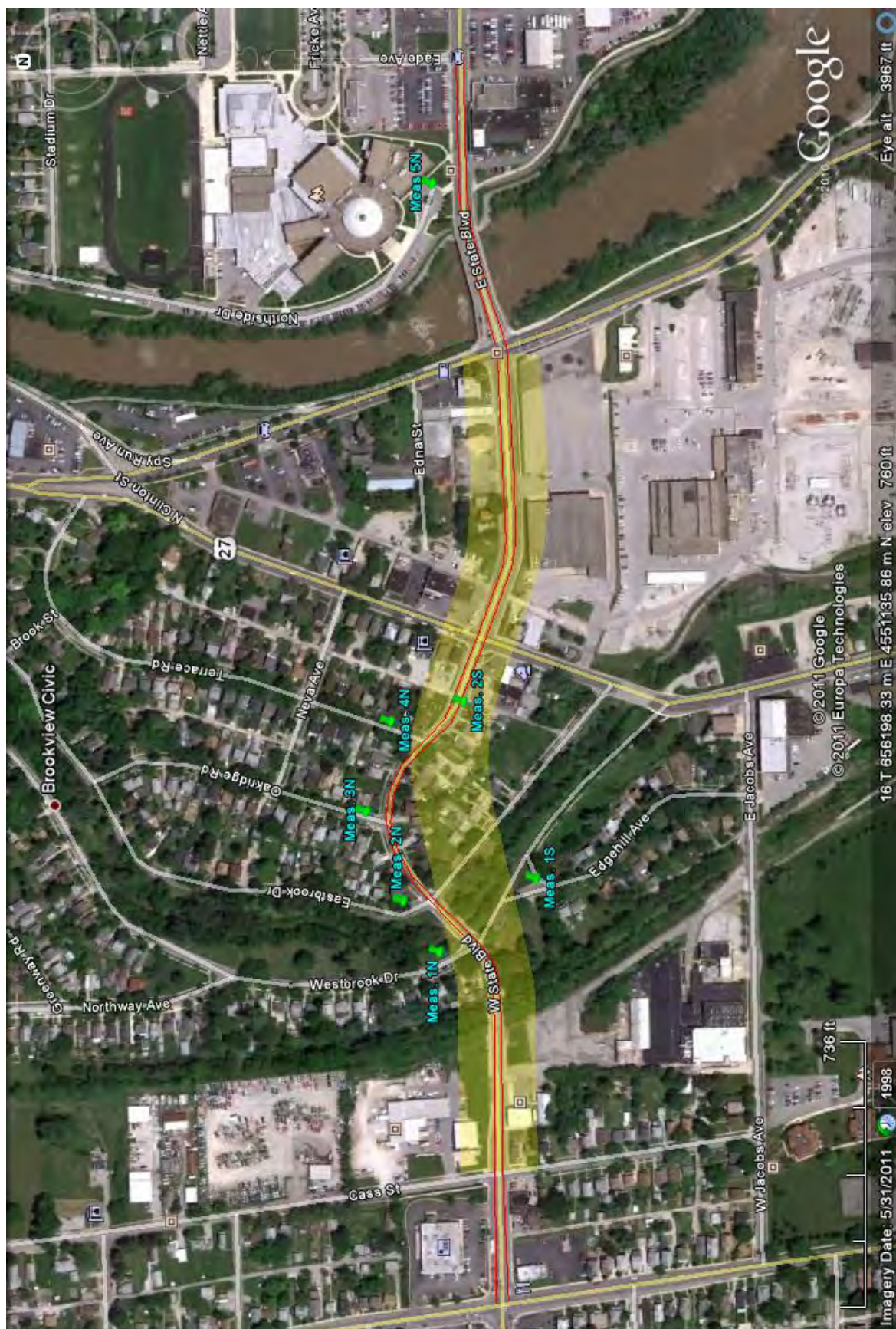
Noise measurements were made in conformance with Federal Highway Administration (FHWA) guidance at six locations that represent 63 residential receivers present within 500 feet of the proposed improvement (the analysis distance criterion set in INDOT’s *Traffic Noise Policy*). The noise measurement locations represent worst case locations for all homes in what are considered noise sensitive areas. An additional measurement was made at another noise sensitive receiver, North Side High School, beyond the east construction limit of the proposed project. Land use at the west project end is commercial, as it is in the east, with the exception of the school. The residential receivers fall into land use category B in terms of FHWA’s Noise Abatement Criteria (NAC) (Table 1). The applicable noise criterion for this land use is 67 dBA in terms of the one-hour equivalent noise level, expressed as L_{eq} (1h). Because Part 772 defines potential impacts in terms of noise levels approaching or exceeding the NAC and INDOT’s *Noise Policy* defines approaching as one decibel, the effective value for impact analysis in Indiana for land use category B is 66 dBA, rather than 67 dBA. The school falls into NAC land use category C, which is subject to the same NAC dBA criterion.

Existing measured noise levels did not approach or exceed the NAC at any receiver, with the exception noted below. Analysis using the Traffic Noise Model (TNM2.5) validated the noise measurements obtained in the field. TNM2.5 modeling also finds no receivers will experience future project noise levels that approach or exceed the NAC, with the same exception. And, no modeled receiver will experience predicted noise levels that substantially exceed existing noise levels (INDOT’s *Noise Policy* defines this as 15 dBA). So, except for measurement site 2S, there are no noise impacts and no mitigation is needed.

Measurement site 2S represents a home on the south side of State Boulevard, where the new alignment joins the existing alignment west of Clinton Street, plus the home across State Boulevard on the east side of Terrace Street. These homes are 22 feet and 16 feet, respectively, from existing State Boulevard. The home on the south side of State Boulevard was a measurement site because early engineering did not call for its acquisition. More detailed design found it was necessary to acquire this home for the project. The house on the north side will remain and will be approximately 50 feet from the future roadway edge. It will experience noise levels exceeding the NAC. However, there is no feasible or reasonable mitigation that could protect this home.

¹ Prepared by American Structurepoint, Inc. for the City of Fort Wayne.

Figure 1
Overall Project Map with Noise Receivers



Note: Yellow band is not to scale; it simply indicates project length. Red links represent roadway links used in the TNM2.5 model.

Table 1
FHWA - Noise Abatement Criteria (NAC)
Hourly A-Weighted Sound Level-decibels (dBA)

Activity Category	Activity Criteria L _{eq} (1h)	Description of Activity Category
A	57 (Exterior)	Lands on which serenity and quiet are of extraordinary significance and serve an important public need, and where the preservation of those qualities is essential, if the area is to continue to serve its intended purpose.
B	67 (Exterior)	Residential.
C	67 (Exterior)	Active sport areas, amphitheaters, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails, and trail crossings.
D	52 (Interior)	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	72 (Exterior)	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A-D or F.
F	--	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.
G	--	Undeveloped lands that are not permitted

Source: Federal Highway Administration – 23 CFR 772.

In terms of overall project impacts, because the project straightens the existing curve in State Boulevard, a number of homes will be acquired for right-of-way. The new alignment will be farther from the remaining homes to the north, lessening noise there due to the increased distance. On the other hand, two factors tend to slightly increase the level of noise from the new alignment section of State Boulevard: higher travel speeds (no curves) and a higher roadway elevation along State Boulevard due to the higher bridge over Spy Run Creek (seven feet). The higher roadway elevation allows noise to propagate more directly to homes. On balance, the increase in distance to the remaining 48 homes within 500 feet of the project on the north side of State Boulevard means noise levels will decrease. Noise levels on the south side of the new alignment (Edgehill Avenue – 13 homes) will increase perceptibly (normally a 3 dBA increase is considered perceptible), but will still be well below the NAC criterion.

In summary, impacts are limited to one single-family dwelling on the north side of the reconstructed State Boulevard on the east side of Terrace Road. No mitigation is feasible or reasonable at that home because Terrace Road will remain open with sidewalk access to the front of the house and an alley at the rear of the house will also remain open. Any noise wall along the remaining old State Boulevard (along the side yard of the house) would be compromised by noise passing around the ends of the wall. For individual, isolated single-family dwellings, the cost of a noise wall will exceed the allowable cost per benefitting dwelling unit, which is incorporated into the determination of reasonability in INDOT's *Noise Policy*.

1. Project History and Project Background

This Noise Study Report accompanies a Categorical Exclusion (CE) level 4² for the reconstruction of State Boulevard between Cass Street and Spy Run Avenue (US 27) in Ft. Wayne, Wayne Township, Allen County, Indiana (Figure 1).

The need for this project derives from the traffic congestion along the corridor between Cass Street (west project end) and Spy Run Avenue (east project end), the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Spy Run Creek and Clinton Street. The section of State Boulevard to be improved provides fewer lanes than the adjacent roadway sections to the east and west. In addition, pedestrian safety is compromised due to the high level of traffic congestion and substandard sight distances.

The purposes of this approximately 2500-foot reconstruction are to improve pedestrian and motor vehicle safety along State Boulevard, decrease traffic congestion, alleviate roadway flooding, and correct substandard sight distances and geometrics.

The project will meet the need by straightening and raising the road, and providing two travel lanes in each direction, with turn lanes at either project end and at Clinton Street. Raising the road about seven feet over Spy Run Creek will alleviate flooding issues.

2. Existing Conditions

Traffic signals are present today at Cass Street, Clinton Street, and Spy Run Avenue. Other cross streets are controlled by stop signs, with traffic on State Boulevard freely flowing. These conditions will not change, except some side streets will not have direct access to the new alignment.

Average Annual Daily Traffic on State Boulevard is expected to increase from 20,650 in 2009 to 26,200 in 2030, with a 2030 Design Hour Volume of 2,620. Two percent of the traffic is trucks. No traffic data are available for the side streets other than Clinton Street and Spy Run Boulevard. These two streets operate as a one-way pair and each carries approximately 15,200 vehicles a day. The 15-minute counts for the side streets made during noise measurements were factored to one hour for use in the TNM2.5 model. The factored one hour traffic volumes for side streets from west to east are:

- Westbrook – 44 cars and no other vehicles
- Edgehill – 40 cars and no other vehicles
- Eastbrook – 80 cars and no other vehicles
- Oakridge – 12 cars and no other vehicles
- Terrace - 28 cars and no other vehicles

State Boulevard is an urban arterial. Operating speeds are up to 35 mph, but observation during the noise measurements found operating speeds to be as low as 25 at the east project end due to congestion. Operating speeds will increase in the project midsection as capacity is increased, lanes are widened, and sight distances are improved.

The land use is commercial from the west project limit to the abandoned railroad track, which is being converted by others to a bikeway. From this point east to near Clinton Street, land use is single-family

² Prepared by American Structurepoint, Inc. for the City of Fort Wayne.

residential, and these homes represent the noise sensitive areas, with the exception of the North Side High School beyond the east end of the project (Figure 1). Land use from just west of Clinton Street to the east project limit is commercial.

3. Existing Noise Environment

Seven measurement sites were identified to represent 61 single family residences, plus the high school. These represent all the sensitive receivers within 500 feet of the project. The homes are subject to NAC B and the school is subject to NAC C. In both cases this means 67 dBA less the approach factor of 1 dBA, for an effective level of 66 dBA.

Table 2 shows the number of homes represented by each measurement site. Generally each measurement site represents the homes equal to, or more distant from, State Boulevard for a given side street. More description is provided below for each individual site. The figures showing the measurement locations follow the site descriptions.

Table 2
Existing PM Peak Noise Levels (dBA)

Measure. Location	# of Receivers Represented	NAC Land Use	NAC Approach ³ L _{eq} (1h)	2011 Measured Level	2011 Estimated Noise Level	Exceedance of Approach Level
1S	13	B	66	54	53	No
2S	1	B	66	68	68	Yes
1N	10	B	66	62	60	No
2N	8	B	66	64	64	No
3N	16	B	66	63	64	No
4N	12	B	66	58	60	No
5N	1	C	66	64	63	No

Source: The Corradino Group

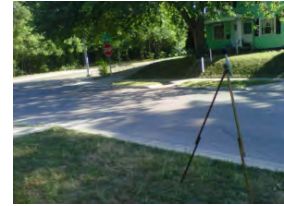
Measurement Site 1S was on the east side of Edgehill Avenue opposite the nearest home to State Boulevard (Figure 2). It represents the 13 homes on Edgehill Avenue within 500 feet of the project. The measured value of 54 dBA was validated by a TNM2.5 model run of 53 dBA.



Measurement Site 2S was on the south side of State Boulevard west of Clinton Street and represents the house across State Boulevard on the east corner of Terrace Street (Figure 3). This home is located so close to State Boulevard (16 feet) that it experiences noise levels above the NAC. The measured value of 68 dBA was validated by a TNM2.5 model run of 68 dBA.

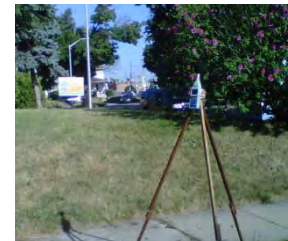
³ The Noise Abatement Criteria is 67 dBA, but is adjusted downward 1 dBA for analysis consistent with 23 CFR 772's "approach" language that is defined by INDOT as 1 dBA.

Measurement Site 1N was on the east side of Westbrook Drive across from the driveway of the second home north of State Boulevard (Figure 2). The home closest to State Boulevard will be acquired for the project. This measurement site represents the ten homes on Westbrook Drive within 500 feet of the project. The measured value of 62 dBA was validated by a TNM2.5 model run of 60 dBA.



Measurement Site 2N was on the east side of Eastbrook Drive at the setback from State Boulevard of the first home north of State Boulevard (Figure 2). This measurement site represents the eight homes on Eastbrook Drive within 500 feet of the project. The measured value of 64 dBA was validated by a TNM2.5 model run of 64 dBA.

Measurement Site 3N was on the east side of Oakridge Road at the setback from State Boulevard of the first home north of State Boulevard (Figure 4). This measurement site represents the 14 homes on Oakridge Road within 500 feet of the project, plus two homes that front directly onto State Boulevard. The measured value of 63 dBA was validated by a TNM2.5 model run of 64 dBA.



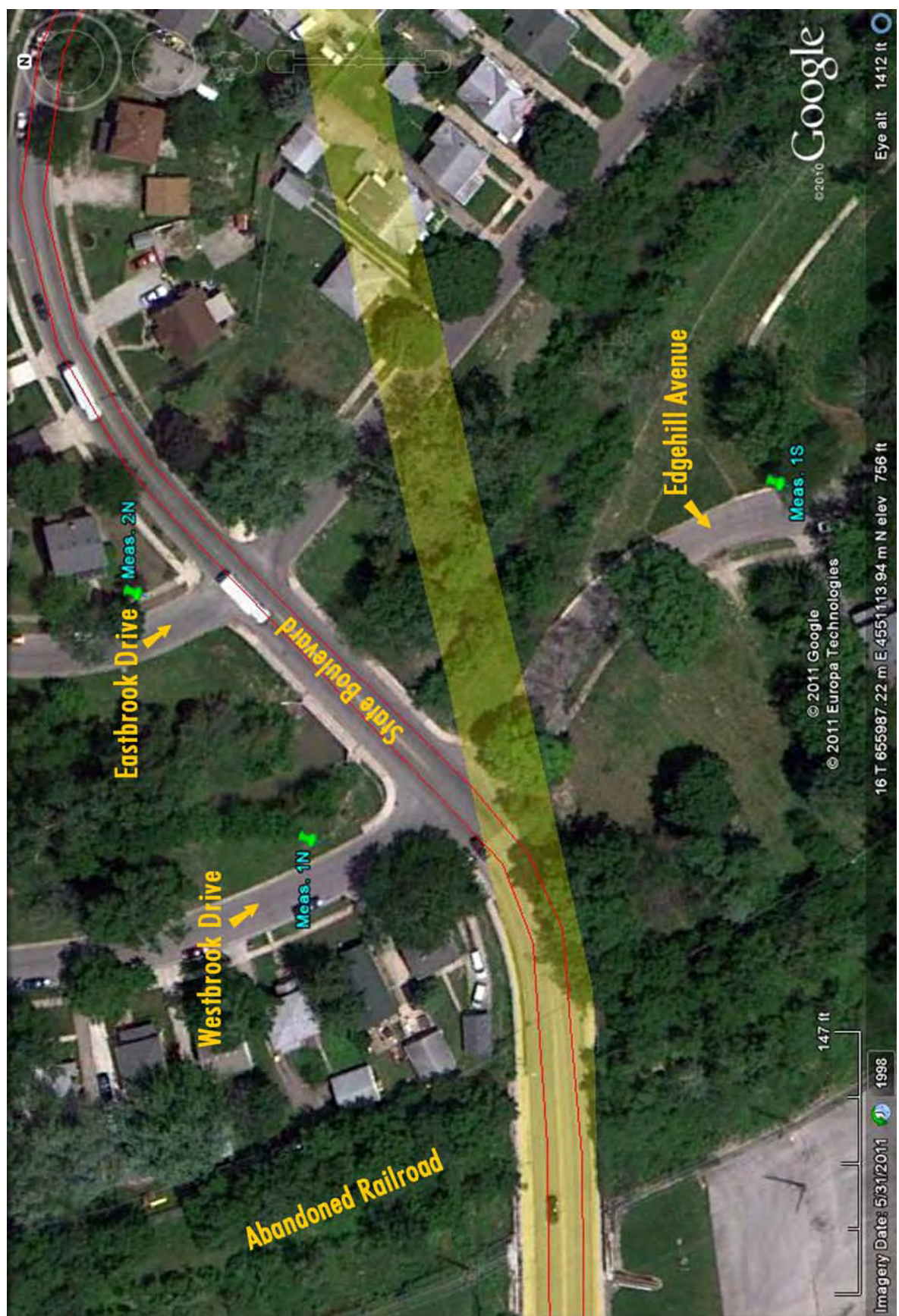
Measurement Site 4N was on the east side of Terrace Road at the setback from State Boulevard of the first home north of State Boulevard on the west side of Terrace Road (Figure 3). This measurement site represents the 12 homes on Terrace Road within 500 feet of the project. The measured value of 58 dBA was validated by a TNM2.5 model run of 60 dBA.

Measurement Site 5N was on the north side of State Boulevard east of St. Joseph's River on the grounds of the North Side High School (Figure 5). This measurement site represents the nearest approach of the high school buildings to State Boulevard. There are no activities in this area. Playing fields are to the rear of the school. There is parking along the floodwall that divides the high school from the river, and there is a student drop-off loop for one building entrance. That entrance is 175 feet from State Boulevard, versus the measurement site that was less than 50 feet from State Boulevard. The measured value of 64 dBA was validated by a TNM2.5 model run of 63 dBA.



As noted on the Noise Data Sheets in Appendix A, a Rion NL-31 Sound Level Meter was used for the measurements, set on slow response, A-weighting, and an exchange rate of 3. This meter allows internal self calibration, which was performed before and after the measurements were taken. Also a Norsonic Sound Calibrator type 1443 emitting 114 dBA was used. The locations of the sites are as noted in the referenced figures. The setup height was five feet on a tripod and the tripod was set away from reflective surfaces. All measurements and traffic counts were 15 minutes in duration.

Figure 2
Measurement Sites 1S, 1N and 2N



Note: Yellow band is not to scale; it simply indicates project length. Red links represent roadway links used in the TNM2.5 model.

Figure 3
Measurement Sites 2S and 4N



Note: Yellow band is not to scale; it simply indicates project length. Red links represent roadway links used in the TNM2.5 model.

Figure 4
Measurement Site 3N



Note: Yellow band is not to scale; it simply indicates project length. Red links represent roadway links used in the TNM2.5 model.

Figure 5
Measurement Site 5N



Note: Yellow band is not to scale; it simply indicates project length. Red links represent roadway links used in the TNM2.5 model.

Existing noise levels were measured July 25, 2011, during the PM peak. The results of the measurements are shown in Table 2. Only measurement site 2S experiences noise levels today above the applicable Noise Abatement Criterion. The TNM2.5 model runs that validated the field measurements are in Appendix B.

4. Analysis Methodology

This noise analysis follows the guidance in the Federal Highway Administration's (FHWA's) *Highway Traffic Noise: Analysis and Abatement Guidance* (July 2010), the Indiana Department of Transportation's (INDOT's) *Procedural Manual for Preparing Environmental Documents* and its *Traffic Noise Policy* (July 2011).

Noise measurements were made in conformance with Federal Highway Administration (FHWA) guidance. These locations represent all the sensitive receivers, which are the noted residences present within 500 feet of the proposed improvement (the analysis distance criterion is set in INDOT's *Traffic Noise Policy*) (Figure 1). The noise measurement locations represent worst case locations for all homes and one high school. There are no other sensitive receivers. These residential receivers fall into land use category B in terms of FHWA's Noise Abatement Criteria (NAC) (Table 1). The applicable noise criterion for this land use is 67 dBA in terms of the one-hour equivalent noise level, expressed as L_{eq} (1h). Because Part 772 defines potential impacts in terms of approach or exceed the NAC, the effective value for impact analysis in Indiana for land use category B is 66 dBA, rather than 67 dBA.

The TNM2.5 was used to model existing noise levels (Appendix B and Table 2). All existing modeled values were within 2 dBA of the measured values, validating the TNM2.5 model. Existing side streets were all shown as traffic links, and terrain lines were used to represent the elevated profile of the abandoned rail line/bikeway. Building lines were used along the east side of Clinton Street and the east side of Terrace Street to reflect how buildings there shield receivers from noise.

The TNM2.5 was used to estimate future noise levels with the project (Appendix C). The *Abbreviated Engineer's Assessment* notes the design year to be 2030. The project design speed will be 35 mph, and it will be posted at 30 mph. For the modeling a speed of 35 mph was used, as that speed was observed during the noise measurements and while operating a car in the corridor during the site visit.

The project average annual daily traffic (AADT) is estimated to be 26,200, compared to a 2009 value of 20,650. The design hour value (DHV) is anticipated to be 2,620 in 2030 with trucks expected to represent 2% of traffic. These data were used as input to the future model runs made with TNM2.5, with a 60/40 peak hour directional split on State Boulevard. This means that 60 percent of traffic was assumed to flow westbound and 40 percent eastbound. The links for the eastbound and westbound movements were modeled separately. This split represents the worst case for the north side of State Boulevard, where the overwhelming majority of homes are located. The *Engineer's Assessment* has no data for local roads, so the values counted in the field during the noise measurements were expanded from 15 minutes to one hour. No traffic growth was assumed on these local roads as the neighborhoods they serve are mature and not subject to growth. Normally as neighborhoods age, their populations do likewise, and trip rates drop. The new alignment was added to base model architecture and "old" State Boulevard was left in place in truncated form, acting as the conduit for Eastbrook, Oakridge and Terrace Roads.

5. Future Noise Environment

Future no build traffic volumes on State Boulevard would increase by a factor of 1.27 (26,200/20,650). Using the basic relationship for a change in traffic volume the future noise level will be the existing noise level plus the contribution from the increase traffic, which = $10 \log (\text{future traffic/existing traffic})$, or $10 \log 1.27$, or one decibel (Table 3).

The future noise levels presented in Table 3 for build conditions demonstrate that noise levels will remain well below the applicable NAC, except at the one house represented by measurement location 2S.

Each of the measurement sites was selected to represent the worst case for the set of homes it represents. This means that, while the noise level at the home represented directly by measurement site 1S would be 57 dBA with the project in the design year 2030, the other 12 homes on Edgehill Avenue within 500 feet of State Boulevard would experience progressively lower noise levels as the distance from State Boulevard increases. The same principle applies to locations 1N, 2N, 3N, and 4N. None of these homes would experience a noise impact. Likewise, the North Side High School would not experience a noise impact.

Table 3
Future PM Peak Noise Levels (dBA)

Measure. Location	# of Receivers Represented	NAC Land Use	NAC Approach ⁴ $L_{eq}(1h)$	2011 Measured Level	2011 Estimated Noise Level	2030 Estimated No Build Noise Level	2030 Estimated Build Noise Level	Noise Impact
1S	13	B	66	54	53	54	57	No
2S	1	B	66	68	68	69	71	Yes
1N	10	B	66	62	60	61	60	No
2N	8	B	66	64	64	65	59	No
3N	16	B	66	63	64	65	57	No
4N	12	B	66	58	60	60	59	No
5N	1	C	66	64	63	64	64	No

Source: The Corradino Group

The home represented by measurement site 2S would continue to experience noise impacts due to its existing proximity to State Boulevard. Noise levels exceed the residential NAC today and will continue to. It faces Terrace Street, but its side yard is along State Boulevard, with an alley at the rear of the property.

Noise walls would not be feasible due to the inability to construct a long, uninterrupted barrier. Driveways, side streets, and openings to get to the house would compromise abatement of noise.

Noise walls would not be reasonable because the cost of providing a wall for an individual home would exceed INDOT cost-effectiveness guidelines. Therefore, there is no feasible or reasonable mitigation that could protect this individual home. For individual, isolated single-family dwellings, the cost of a noise

⁴ The Noise Abatement Criteria is 67 dBA, but is adjusted downward 1 dBA for analysis consistent with 23 CFR 772's "approach" language that is defined by INDOT as 1 dBA.

wall will exceed the allowable cost per benefitting dwelling unit, which is incorporated into the determination of reasonability in INDOT's *Noise Policy*. In summary, impacts are limited to one single-family dwelling. No mitigation is feasible or reasonable.

Based on the studies thus far accomplished, the State of Indiana has not identified any locations where noise abatement is likely. Noise abatement is based upon preliminary design costs and design criteria. Noise abatement has not been found to be feasible or reasonable based on the inability to block noise from the primary source (State Boulevard) due to side streets and an alley, and is not reasonable due to the fact that there is a single sensitive receiver that will experience impacts. A reevaluation of the noise analysis will occur during final design. If during final design, it has been determined that conditions have changed such that noise abatement is feasible and reasonable, the abatement measures might be provided. The final decision on the installation of any abatement measure(s) will be made upon the completion of the project's final design and the public involvement processes.

A TNM2.5 run was made to determine the location of the 66 dBA contour line. It would be approximately 35 feet from the roadway edge on both sides of the road (Appendix D). Apart from the home noted as having noise levels above the residential NAC, no other homes would fall within this 35 foot zone.

The future Pufferbelly Trail crosses the project along the old railroad alignment at the project's west end. The project will construct a pedestrian overpass to serve that future trail. Parkland is being developed on the south side of the project on either side of Edgehill Avenue. The 66 dBA contour line would penetrate into this parkland slightly, depending on the final right-of-way.

The TNM2.5 files are found on a disk inserted in a sleeve on the back cover of this report.

6. Construction Noise

It is difficult to predict levels of construction noise at a particular receiver or group of receivers. Heavy machinery, the major source of noise in construction, is constantly moving in unpredictable patterns. Daily construction normally occurs during daylight hours when people tolerate occasional loud noises. The duration for individual receivers should be short; therefore, there are no anticipated disruptions of normal activities. However, the project plans and specifications include provisions requiring the contractor to make every reasonable effort to minimize construction noise through abatement measure such as work-hour controls and maintenance of muffler systems.

7. Coordination with Local Officials

Consistent with 23 CFR 772.17, this report is being provided to the Dearborn County Board of Commissioners, who initiated the project.

APPENDIX A

NOISE MEASUREMENT DATA SHEETS

NOISE DATA SHEET

Job #: 4093-01					AM/PM		Site # 1S		
Project: State Street widening w replacement bridge, Ft. Wayne					Date: 25 Jul 2011				
Instrumentation					Day of Week <u>M</u> T W T F				
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3									
Norsonic Sound Calibrator type 1443 @ 114 dB					Calibration Confirmed <u>Yes</u> /No				
Location					Temp. 85 F				
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.					Heavy Overcast/ <u>Light Overcast</u> /Sunny/ Clear Night/ Overcast Night				
Receptor Represents					Homes on Edgehill Road				
Major Noise Source					Humidity		70 %		
Secondary Source					Pavement		<u>Dry</u> /Wet		
Land Use Category					Wind		Upwind -1 to -5		
A-57dBA Serene Park		B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)		E-72dBA Motels/Rest./ Offices/Devel.		F-NA Agric./Manuf./ Mainten./Retail		G-NA Undeveloped lands not yet permitted	
						Calm -1 to +1			
						Downwind +1 to +5			

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	30
Secondary Road	2	12	NA	25	20

Test 1 – min.	From	3:18 PM	To	3:33 PM
Decibel Reading	54.3	L Aeq	71.6	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	162	132	2	8
Medium Trucks (3-axle)	2	2	0	0
Heavy Trucks	1	1	0	0
Buses	0	0	0	0
Motorcycles	3	3	0	0

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is on west side of Edgehill Avenue at home closest to existing and future State Street. Speeds on both roads variable due to stop sign control on secondary road and curves and turns on State Street.

NOISE DATA SHEET

Job #: 4093-01		AM/PM		Site # 2S	
Project: State Street widening w replacement bridge, Ft. Wayne				Date: 25 Jul 2011	
Instrumentation				Day of Week <u>M</u> T W T F	
		Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3			
		Norsonic Sound Calibrator type 1443 @ 114 dB		Calibration Confirmed <u>Yes</u> /No	
Location		Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.		Temp. 85 F	
Receptor Represents		Home remaining on State Road		Heavy Overcast/ <u>Light Overcast</u> / Sunny/ Clear Night/ Overcast Night	
Major Noise Source		State Road		Humidity	
Secondary Source		US 27 Clinton		70 %	
Land Use Category		A-57dBA Serene Park		B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)	
		E-72dBA Motels/Rest./ Offices/Devel.		F-NA Agric./Manuf./ Mainten./Retail	
		G-NA Undeveloped lands not yet permitted		Wind	
				Upwind -1 to -5	
				Calm -1 to +1	
				Downwind +1 to +5	

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	25
Secondary Road	2	12	NA	35	35

Test 1 – min.	From	5:15 PM	To	5:30 PM
Decibel Reading	68.2	L Aeq	87.9	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	173	190	0	380
Medium Trucks (3-axle)	2	1	0	2
Heavy Trucks	3	2	0	4
Buses	0	0	0	0
Motorcycles	2	2	0	4

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at setback of sole remaining home on south side of State Street. During green cycle for Clinton, noise predominates from that source. Traffic on eastbound State is queued in front of the measurement site on every State Street red signal cycle.

NOISE DATA SHEET

Job #: 4093-01					AM/PM		Site # 1N		
Project: State Street widening w replacement bridge, Ft. Wayne					Date: 25 Jul 2011				
Instrumentation					Day of Week <u>M</u> T W T F				
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3									
Norsonic Sound Calibrator type 1443 @ 114 dB					Calibration Confirmed <u>Yes</u> /No				
Location					Temp. 85 F				
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.					Heavy Overcast/ <u>Light Overcast</u> / Sunny/ Clear Night/ Overcast Night				
Receptor Represents					Homes on Westbrook Drive				
Major Noise Source					Humidity		70 %		
Secondary Source					Pavement		<u>Dry</u> /Wet		
Land Use Category					Wind		Upwind -1 to -5		
A-57dBA Serene Park		B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)		E-72dBA Motels/Rest./ Offices/Devel.		F-NA Agric./Manuf./ Mainten./Retail		G-NA Undeveloped lands not yet permitted	
					Calm -1 to +1		Downwind +1 to +5		

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	30
Secondary Road	2	12	NA	25	10

Test 1 – min.	From	3:41 PM	To	3:56 PM
Decibel Reading	62.0	L Aeq	87.2	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	167	139	6	5
Medium Trucks (3-axle)	2	2	0	0
Heavy Trucks	0	1	0	0
Buses	0	0	0	0
Motorcycles	2	2	0	0

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at south end of Westbrook Drive at home 2nd closest to existing and future State Street. First home will be acquired for project. Speeds on both roads variable due to stop sign control on secondary road and curves and turns on State Street.

NOISE DATA SHEET

Job #: 4093-01					AM/PM		Site # 2N
Project: State Street widening w replacement bridge, Ft. Wayne					Date: 25 Jul 2011		
Instrumentation					Day of Week <u>M</u> T W T F		
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3							
Norsonic Sound Calibrator type 1443 @ 114 dB					Calibration Confirmed <u>Yes</u> /No		
Location					Temp. 85 F		
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.					Heavy Overcast/ <u>Light Overcast</u> /Sunny/ Clear Night/ Overcast Night		
Receptor Represents					Homes on Eastbrook Drive		
Major Noise Source					Humidity		70 %
Secondary Source					Pavement		<u>Dry</u> /Wet
Land Use Category					Wind		
A-57dBA Serene Park	B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)	E-72dBA Motels/Rest./ Offices/Devel.	F-NA Agric./Manuf./ Mainten./Retail	G-NA Undeveloped lands not yet permitted	Upwind -1 to -5		
					Calm -1 to +1		
					Downwind +1 to +5		

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	25
Secondary Road	2	12	NA	25	10

Test 1 – min.	From	4:03 PM	To	4:18 PM
Decibel Reading	64.0	L Aeq	80.9	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	189	155	8	12
Medium Trucks (3-axle)	3	3	0	0
Heavy Trucks	1	0	0	0
Buses	0	0	0	0
Motorcycles	1	1	0	0

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at south end of Eastbrook Drive on east side at home closest to existing and future State Street. Speeds on both roads variable due to stop sign control on secondary road and curves and turns on State Street.

NOISE DATA SHEET

Job #: 4093-01					AM/PM		Site # 3N		
Project: State Street widening w replacement bridge, Ft. Wayne					Date: 25 Jul 2011				
Instrumentation					Day of Week <u>M</u> T W T F				
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3									
Norsonic Sound Calibrator type 1443 @ 114 dB					Calibration Confirmed <u>Yes</u> /No				
Location					Temp. 85 F				
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.					Heavy Overcast/ <u>Light Overcast</u> / Sunny/ Clear Night/ Overcast Night				
Receptor Represents					Homes on Oakridge Road				
Major Noise Source					Humidity		70 %		
Secondary Source					Pavement		<u>Dry</u> /Wet		
Land Use Category					Wind		Upwind -1 to -5		
A-57dBA Serene Park		B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)		E-72dBA Motels/Rest./ Offices/Devel.		F-NA Agric./Manuf./ Mainten./Retail		G-NA Undeveloped lands not yet permitted	
					Calm -1 to +1		Downwind +1 to +5		

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	30
Secondary Road	2	12	NA	25	10

Test 1 – min.	From	4:24 PM	To	4:39 PM
Decibel Reading	63.3	L Aeq	75.0	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	159	171	1	2
Medium Trucks (3-axle)	0	3	0	0
Heavy Trucks	1	0	0	0
Buses	0	1	0	0
Motorcycles	2	2	0	0

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at south end of Oakridge Road on east side at home closest to existing and future State Street. Speeds on both roads variable due to stop sign control on secondary road and curves and turns on State Street.

NOISE DATA SHEET

Job #: 4093-01		AM/PM		Site # 4N	
Project: State Street widening w replacement bridge, Ft. Wayne				Date: 25 Jul 2011	
Instrumentation				Day of Week <u>M</u> T W T F	
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3					
Norsonic Sound Calibrator type 1443 @ 114 dB				Calibration Confirmed <u>Yes</u> /No	
Location				Temp. 85 F	
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.				Heavy Overcast/ <u>Light Overcast</u> / Sunny/ Clear Night/ Overcast Night	
Receptor Represents				Homes on State Street and Terrace Road	
Major Noise Source				State Road	
Secondary Source				Terrace Road	
Land Use Category				Humidity 70 %	
A-57dBA Serene Park		B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)		E-72dBA Motels/Rest./ Offices/Devel.	
				F-NA Agric./Manuf./ Mainten./Retail	
				G-NA Undevel. lands not yet permitted	
				Pavement <u>Dry</u> /Wet	
				Wind	
				Upwind -1 to -5	
				Calm -1 to +1	
				Downwind +1 to +5	

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	25
Secondary Road	2	12	NA	25	10

Test 1 – min.	From	4:50 PM	To	5:05 PM
Decibel Reading	58.1	L Aeq	72.0	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	154	190	6	1
Medium Trucks (3-axle)	3	2	0	0
Heavy Trucks	0	0	0	0
Buses	0	0	0	0
Motorcycles	1	2	0	0

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at south end of Terrace Road on east side between homes 2nd and 3rd closest to existing and future State Street. Speeds on both roads variable due to stop sign control on secondary road and curves and turns on State Street. Eastbound traffic on State backed up to Terrace on four different cycles of the signal at Clinton.

NOISE DATA SHEET

Job #: 4093-01					AM/PM		Site # 5N
Project: State Street widening w replacement bridge, Ft. Wayne					Date: 25 Jul 2011		
Instrumentation					Day of Week <u>M</u> T W T F		
Rion NL-31 Sound Level Meter, slow response, A-weighting, exchange rate = 3							
Norsonic Sound Calibrator type 1443 @ 114 dB					Calibration Confirmed <u>Yes</u> /No		
Location					Temp. 85 F		
Project is bridge replacement and new approaches on State Road in Ft. Wayne between Cass Street and Spy Run Avenue.					Heavy Overcast/ <u>Light Overcast</u> /Sunny/ Clear Night/ Overcast Night		
Receptor Represents					North Side High School		
Major Noise Source					Humidity 70 %		
Secondary Source					Pavement <u>Dry</u> /Wet		
Land Use Category					Wind		
A-57dBA Serene Park	B&C-67dBA Residential/Active Park/ Hosp/Church/Section 4(f)	E-72dBA Motels/Rest./ Offices/Devel.	F-NA Agric./Manuf./ Mainten./Retail	G-NA Undeveloped lands not yet permitted	Upwind -1 to -5		
					Calm -1 to +1		
					Downwind +1 to +5		

	# Lanes	Lane Width	Median Width	Posted Speed	*Observed Speed
Major Road	2	16	NA	35	35
Secondary Road	2	12	NA	NA	NA

Test 1 – min.	From	5:40 PM	To	5:55 PM
Decibel Reading	63.9	L Aeq	77.9	L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars	220	175	NA	NA
Medium Trucks (3-axle)	3	1	NA	NA
Heavy Trucks	1	0	NA	NA
Buses	0	0	NA	NA
Motorcycles	4	3	NA	NA

Test 2 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Test 3 – min.	From		To	
Decibel Reading		L Aeq		L max
Traffic Volumes	Major Road		Secondary Road	
	NB/EB	SB/WB	NB/EB	SB/WB
Cars				
Medium Trucks (3-axle)				
Heavy Trucks				
Buses				
Motorcycles				

Notes

Measurement site is at setback of closest school building to State Street. There are no activity areas on this side of the school except a parking lot and drop-off. Noise from Spy Run was inaudible across river and due to presence of floodwall/berm along river.

APPENDIX B
TNM2.5 INPUT DATA AND RESULTS
(EXISTING)

RESULTS: SOUND LEVELS

4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5
Calculated with TNM 2.5

RESULTS: SOUND LEVELS

PROJECT/CONTRACT: 4093-01 Ft. Wayne State St

RUN: Existing Validation

BARRIER DESIGN: INPUT HEIGHTS

ATMOSPHERICS: 80 deg F, 70% RH

Average pavement type shall be used unless
a State highway agency substantiates the use
of a different type with approval of FHWA.

Receiver																
Name		No.	#DUs	Existing LAeq1h		No Barrier LAeq1h		Increase over existing		Type	With Barrier		Noise Reduction		Calculated	
				dBA		dBA		Calculated	Crit'n	Impact	Calculated	LAeq1h	Calculated	Goal	Calculated	minus Goal
								dB					dB			dB

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2 August 2011

INPUT: ROADWAYS

The Corradino Group
Ted Stone

4093-01 Ft. Wayne State St

2 August 2011
TNM 2.5

INPUT: ROADWAYS
PROJECT/CONTRACT:
RUN:

4093-01 Ft. Wayne State St
Existing Validation

Average pavement type shall be used unless
a State highway agency substantiates the use
of a different type with the approval of FHWA

Roadway Name		Points									
	Width	Name	No.	Coordinates (pavement)			Flow Control		Percent Vehicles Affected	Segment	
				X	Y	Z	Control Device	Speed Constraint		Pvmt Type	On Struct?
	ft			ft	ft	ft		mph	%		
State EB	12.0	point1	1	18,590.0	3,537.0	792.00				Average	
		point2	2	18,937.0	3,560.0	785.00				Average	
		point3	3	19,362.0	3,574.0	771.00				Average	
		point4	4	19,485.0	3,584.0	759.00				Average	
		point5	5	19,527.0	3,606.0	758.00				Average	
		point6	6	19,565.0	3,638.0	757.00					
State WB	20.0	point25	25	22,035.0	3,756.0	756.00				Average	
		point26	26	21,666.0	3,740.0	756.00				Average	
		point27	27	21,554.0	3,726.0	763.00				Average	
		point28	28	21,381.0	3,677.0	768.00				Average	
		point29	29	21,224.0	3,625.0	760.00					
		point53	53	19,544.0	3,668.0	757.00				Average	
Westbrook	24.0	point54	54	19,487.0	3,769.0	754.00				Average	
		point55	55	19,458.0	3,874.0	754.00				Average	
		point56	56	19,434.0	4,124.0	754.00					
	24.0	point57	57	19,587.0	3,614.0	756.00				Average	
		point58	58	19,649.0	3,581.0	756.00				Average	
		point59	59	19,690.0	3,542.0	756.00				Average	
Edgehill		point60	60	19,721.0	3,492.0	756.00				Average	
		point61	61	19,725.0	3,457.0	756.00				Average	
		point62	62	19,772.0	3,321.0	757.00				Average	
		point63	63	19,834.0	3,245.0	757.00				Average	
		point64	64	19,898.0	3,195.0	758.00				Average	
		point65	65	19,977.0	3,119.0	758.00				Average	
Eastbrook	24.0	point66	66	19,671.0	3,787.0	758.00				Average	

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1

2 August 2011

INPUT: ROADWAYS		4093-01 Ft. Wayne State St										Average	
			point67	67	19,643.0	3,857.0	758.00					Average	
			point68	68	19,639.0	3,903.0	758.00					Average	
			point69	69	19,648.0	3,965.0	758.00					Average	
			point70	70	19,707.8	4,449.3	758.00						
Oakridge	24.0		point71	71	19,895.0	3,915.0	757.00					Average	
			point72	72	19,907.0	3,965.0	758.00					Average	
			point73	73	20,051.0	4,448.0	761.00						
Terrace	20.0		point74	74	20,134.0	3,812.0	757.00					Average	
			point75	75	20,321.0	4,394.0	757.00						
Clinton	36.0		point76	76	20,529.7	4,023.7	766.00					Average	
			point77	77	20,418.9	3,668.7	760.00						
Spy Run	36.0		point80	80	21,395.0	3,208.0	760.00					Average	
			point81	81	21,234.0	3,588.0	760.00					Average	
			point82	82	21,217.0	3,638.0	760.00					Average	
			point83	83	21,051.0	4,061.0	762.00						
State EB-2	12.0		point84	84	19,569.4	3,641.9	757.00					Average	
			point7	7	19,686.5	3,766.0	756.00						
State EB-2-2	12.0		point86	86	19,691.6	3,770.4	756.00					Average	
			point8	8	19,752.0	3,832.0	757.00					Average	
			point9	9	19,822.0	3,873.0	757.00					Average	
			point10	10	19,899.0	3,896.0	756.00						
State EB-2-2-2	12.0		point87	87	19,902.7	3,894.5	756.00					Average	
			point11	11	19,985.0	3,885.0	756.00					Average	
			point12	12	20,050.0	3,857.0	756.00					Average	
			point13	13	20,124.0	3,786.0	757.00						
State EB-2-2-2-2	20.0		point88	88	20,126.2	3,783.8	757.00	Signal	15.00	100		Average	
			point14	14	20,180.0	3,731.0	757.00					Average	
			point15	15	20,247.0	3,702.0	760.00					Average	
			point16	16	20,408.0	3,641.0	760.00					Average	
			point17	17	20,618.0	3,571.0	759.00					Average	
			point18	18	20,697.0	3,561.0	759.00					Average	
			point19	19	20,886.0	3,571.0	759.00					Average	
			point20	20	21,224.2	3,597.1	760.00						
State EB-2-2-2-2-2	20.0		point89	89	21,243.6	3,601.7	760.00					Average	
			point21	21	21,390.0	3,656.0	768.00					Average	
			point22	22	21,567.0	3,706.0	763.00					Average	
			point23	23	21,688.0	3,719.0	757.00					Average	
			point24	24	22,043.0	3,737.0	756.00						
State WB-2	20.0		point90	90	21,214.4	3,623.5	760.00					Average	
I:\Projects\4093-01\TNM\Existing												2	
												2 August 2011	

INPUT: ROADWAYS		4093-01 Ft. Wayne State St									
		point49	49	20,888.5	3,597.6	760.00				Average	
		point30	30	20,690.0	3,588.0	760.00				Average	
		point31	31	20,624.0	3,595.0	760.00				Average	
		point32	32	20,417.0	3,664.0	760.00				Average	
		point50	50	20,258.3	3,720.9	758.50				Average	
		point33	33	20,189.0	3,748.0	757.00				Average	
		point34	34	20,133.0	3,800.0	757.00					
State WB-2-2	12.0	point91	91	20,129.3	3,803.0	757.00				Average	
		point35	35	20,058.0	3,867.0	757.00				Average	
		point36	36	19,987.0	3,899.0	756.00				Average	
		point37	37	19,894.6	3,908.9	756.00					
State WB-2-2-2	12.0	point92	92	19,890.2	3,907.4	756.00				Average	
		point38	38	19,814.0	3,884.0	757.00				Average	
		point39	39	19,741.3	3,843.9	757.00				Average	
		point51	51	19,673.9	3,772.8	757.50					
State WB-2-2-2-2	12.0	point93	93	19,667.2	3,766.1	757.50				Average	
		point52	52	19,555.8	3,650.1	757.75					
State WB-2-2-2-2-2	12.0	point94	94	19,551.4	3,645.7	757.75				Average	
		point40	40	19,523.0	3,622.0	758.00				Average	
		point41	41	19,478.0	3,603.0	759.00				Average	
		point42	42	19,357.6	3,594.0	766.00				Average	
		point43	43	19,084.0	3,585.0	777.00				Average	
		point44	44	18,929.0	3,583.0	785.00				Average	
		point45	45	18,587.0	3,572.0	792.00					
Clinton-2	36.0	point96	96	20,416.7	3,662.3	760.00				Average	
		point78	78	20,408.9	3,643.1	760.00					
Clinton-2-2	36.0	point97	97	20,406.7	3,637.6	760.00				Average	
		point79	79	20,294.0	3,287.0	756.00					

I:\Projects\4093-01\TNM\Existing

INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5

INPUT: TRAFFIC FOR LAeq1h Volumes

PROJECT/CONTRACT: 4093-01 Ft. Wayne State St

RUN: Existing Validation

Roadway Name		Points													
Name		No.	Segment												
			Autos		MTTrucks		HTTrucks		Buses		Motorcycles				
			V	S	V	S	V	S	V	S	V	S			
			veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph			
State EB	point1	1	668	35	8	35	1	35	1	35	1	35	8	35	
	point2	2	668	35	8	35	1	35	1	35	1	35	8	35	
	point3	3	668	35	8	35	1	35	1	35	1	35	8	35	
	point4	4	668	35	8	35	1	35	1	35	1	35	8	35	
	point5	5	668	35	8	35	1	35	1	35	1	35	8	35	
	point6	6													
State WB	point25	25	700	35	12	35	8	35	1	35	1	35	12	35	
	point26	26	700	35	12	35	8	35	1	35	1	35	12	35	
	point27	27	700	35	12	35	8	35	1	35	1	35	12	35	
	point28	28	700	35	12	35	8	35	1	35	1	35	12	35	
	point29	29													
	point53	53	44	15	0	0	0	0	0	0	0	0	0	0	
Westbrook	point54	54	44	15	0	0	0	0	0	0	0	0	0	0	
	point55	55	44	15	0	0	0	0	0	0	0	0	0	0	
	point56	56													
	point57	57	40	20	0	0	0	0	0	0	0	0	0	0	
	point58	58	40	20	0	0	0	0	0	0	0	0	0	0	
	point59	59	40	20	0	0	0	0	0	0	0	0	0	0	
Edgehill	point60	60	40	20	0	0	0	0	0	0	0	0	0	0	
	point61	61	40	20	0	0	0	0	0	0	0	0	0	0	
	point62	62	40	20	0	0	0	0	0	0	0	0	0	0	
	point63	63	40	20	0	0	0	0	0	0	0	0	0	0	

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2 Augu

INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St

	point64	64	40	20	0	0	0	0	0	0	0	0	0	0	0
	point65	65													
Eastbrook	point66	66	80	10	0	0	0	0	0	0	0	0	0	0	0
	point67	67	80	10	0	0	0	0	0	0	0	0	0	0	0
	point68	68	80	10	0	0	0	0	0	0	0	0	0	0	0
	point69	69	80	10	0	0	0	0	0	0	0	0	0	0	0
	point70	70													
Oakridge	point71	71	12	10	0	0	0	0	0	0	0	0	0	0	0
	point72	72	12	10	0	0	0	0	0	0	0	0	0	0	0
	point73	73													
Terrace	point74	74	28	10	0	0	0	0	0	0	0	0	0	0	0
	point75	75													
Clinton	point76	76	1520	35	8	35	16	35	4	35	16	35			35
	point77	77													
Spy Run	point80	80	1520	35	8	35	16	35	4	35	16	35			35
	point81	81	1520	35	8	35	16	35	4	35	16	35			35
	point82	82	1520	35	8	35	16	35	4	35	16	35			35
	point83	83													
State EB-2	point84	84	668	30	8	30	4	30	4	30	8	30			30
	point7	7													
State EB-2-2	point86	86	756	30	12	30	4	30	4	30	4	30			30
	point8	8	756	30	12	30	4	30	4	30	4	30			30
	point9	9	756	30	12	30	4	30	4	30	4	30			30
	point10	10													
State EB-2-2-2	point87	87	636	25	4	25	4	25	4	25	8	25			25
	point11	11	636	25	4	25	4	25	4	25	8	25			25
	point12	12	636	25	4	25	4	25	4	25	8	25			25
	point13	13													
State EB-2-2-2-2	point88	88	692	25	8	25	12	25	4	25	8	25			25
	point14	14	692	25	8	25	12	25	4	25	8	25			25
	point15	15	692	25	8	25	12	25	4	25	8	25			25
	point16	16	692	30	8	30	12	30	4	30	8	30			30
	point17	17	692	30	8	30	12	30	4	30	8	30			30
	point18	18	692	30	8	30	12	30	4	30	8	30			30

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INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St									
	point19	19	692	30	8	30	12	30	4
	point20	20							
State EB-2-2-2-2-2	point89	89	880	35	12	35	4	35	4
	point21	21	880	35	12	35	4	35	4
	point22	22	880	35	12	35	4	35	4
	point23	23	880	35	12	35	4	35	4
	point24	24							
State WB-2	point90	90	760	30	4	30	8	30	4
	point49	49	760	30	4	30	8	30	4
	point30	30	760	30	4	30	8	30	4
	point31	31	760	30	4	30	8	30	4
	point32	32	760	30	4	30	8	30	4
	point50	50	760	25	4	25	8	25	4
	point33	33	760	25	4	25	8	25	4
	point34	34							
State WB-2-2	point91	91	760	25	8	25	4	25	4
	point35	35	760	25	8	25	4	25	4
	point36	36	760	25	8	25	4	25	4
	point37	37							
State WB-2-2-2	point92	92	684	25	12	25	4	25	4
	point38	38	684	30	12	30	4	30	4
	point39	39	684	30	12	30	4	30	4
	point51	51							
State WB-2-2-2-2	point93	93	620	30	12	30	4	30	4
	point52	52							
State WB-2-2-2-2-2	point94	94	556	30	8	30	4	30	0
	point40	40	556	30	8	30	4	30	0
	point41	41	556	30	8	30	4	30	0
	point42	42	556	30	8	30	4	30	0
	point43	43	556	30	8	30	4	30	0
	point44	44	556	30	8	30	4	30	0
	point45	45							
Clinton-2	point96	96	1520	35	8	35	16	35	4
	point78	78							

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INPUT: TRAFFIC FOR LAeq1h Volumes

Clinton-2-2	point97	97	1520	35	8	35	16	35	4	35	16	35
	point79	79										

4093-01 Ft. Wayne State St

INPUT: RECEIVERS												4093-01 Ft. Wayne State St											
The Corradino Group Ted Stone												3 August 2011 TNM 2.5											
INPUT: RECEIVERS																							
PROJECT/CONTRACT:												4093-01 Ft. Wayne State St											
RUN:												Existing Validation											
Receiver																							
Name		No.	#DUs	Coordinates (ground)			Height above Ground	Input Sound Levels and Criteria				Active in Calc.											
				X	Y	Z		Existing LAeq1h	Impact Criteria LAeq1h	Sub'l	NR Goal												
				ft		ft		ft	dBA	dBA	dB	dB											
1S		1	13	19,744.0	3,461.0	756.00	4.92	0.00	66	10.0	8.0	Y											
2S		2	2	20,234.0	3,675.0	757.00	4.92	0.00	66	10.0	8.0	Y											
1N		3	10	19,534.1	3,722.2	755.00	4.92	0.00	66	10.0	8.0	Y											
2N		4	8	19,672.0	3,830.0	756.00	4.92	0.00	66	10.0	8.0	Y											
3N		5	16	19,920.0	3,938.0	757.00	4.92	0.00	66	10.0	8.0	Y											
4N		6	12	20,178.0	3,882.0	760.00	4.92	0.00	66	10.0	8.0	Y											
5N		7	1	21,682.0	3,812.1	755.00	4.92	0.00	66	10.0	8.0	Y											

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3 August 2011

INPUT: BUILDING ROWS 4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5

INPUT: BUILDING ROWS

PROJECT/CONTRACT: 4093-01 Ft. Wayne State St

RUN: Existing Validation

Building Row			Points				
Name	Average Height ft	Building Percent %	No.	Coordinates (ground)			
				X	Y	Z	
				ft	ft	ft	
Building N	20.00	80	1	20,484.0	4,025.0	766.00	
			2	20,398.0	3,733.0	760.00	
Building S	20.00	40	3	20,352.0	3,592.0	759.00	
			4	20,297.0	3,428.0	757.00	
Building Terrace	20.00	70	5	20,181.7	3,777.3	760.00	
			6	20,232.9	3,931.2	760.00	
			7	20,243.2	3,924.3	760.00	

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2 Augu

4093-01 Ft. Wayne State St

INPUT: TERRAIN LINES

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5

INPUT: TERRAIN LINES

PROJECT/CONTRACT: 4093-01 Ft. Wayne State St

RUN: Existing Validation

Terrain Line Name	Points			
	No.	Coordinates (ground)		
		X	Y	Z
		ft	ft	ft
Terrain Line1	1	19,240.0	4,005.0	770.00
	2	19,342.0	3,641.0	770.00
	3	19,408.0	3,533.0	770.00
Terrain Line2	4	19,608.0	3,211.0	770.00
	5	21,651.8	3,760.2	768.00
Terrain Line at Riv	6	21,419.3	4,187.6	768.00

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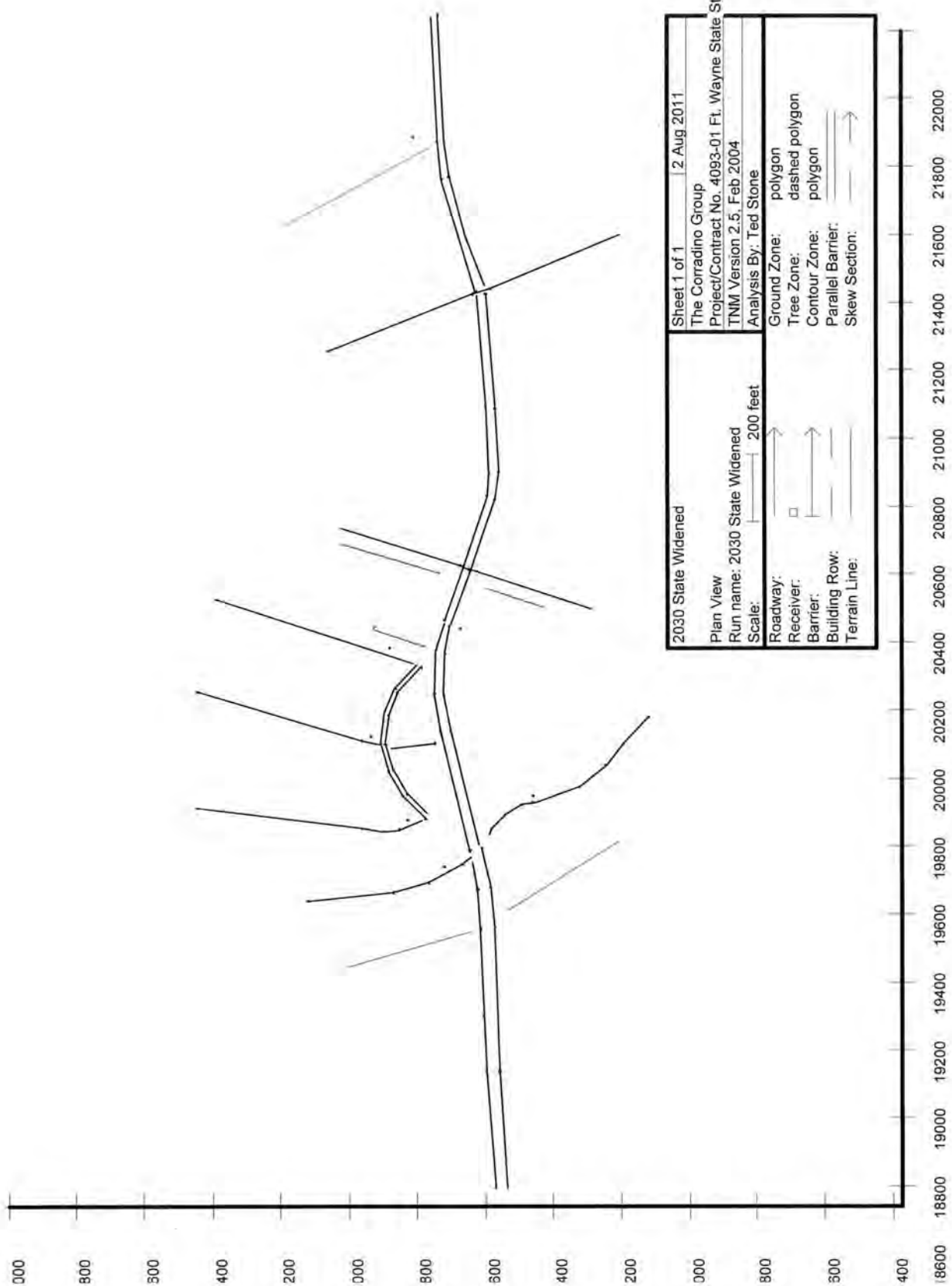
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2 Augu

APPENDIX C

TNM2.5 INPUT DATA AND RESULTS

(FUTURE)



2030 State Widened		Sheet 1 of 1	2 Aug 2011
The Corradino Group		Project/Contract No. 4093-01 Ft. Wayne State St	
Run name: 2030 State Widened		TNM Version 2.5, Feb 2004	
Scale: 200 feet		Analysis By: Ted Stone	
Roadway:	(solid line)	Ground Zone:	(solid line)
Receiver:	(square symbol)	Tree Zone:	(dashed line)
Barrier:	(dashed line)	Contour Zone:	(dotted line)
Building Row:	(long dashed line)	Parallel Barrier:	(long dashed line)
Terrain Line:	(short dashed line)	Skew Section:	(arrow symbol)

RESULTS: SOUND LEVELS

4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5
Calculated with TNM 2.5

RESULTS: SOUND LEVELS

PROJECT/CONTRACT:
2030 State Widened
BARRIER DESIGN:
INPUT HEIGHTS

4093-01 Ft. Wayne State St
2030 State Widened
INPUT HEIGHTS
80 deg F, 70% RH

Average pavement type shall be used unless
a State highway agency substantiates the use
of a different type with approval of FHWA.

ATMOSPHERICS:

80 deg F, 70% RH

Receiver		#DUs		Existing		No Barrier		Increase over existing		Type		With Barrier		Noise Reduction		Calculated minus Goal	
Name		No.		LAeq1h		LAeq1h		Calculated		Impact		Calculated		Calculated		Calculated minus Goal	
				dBA		dBA		dBA		Sub'l Inc		dBA		dBA		dBA	
1S		1	13	0.0	0.0	57.0	66	57.0	66	10	---	57.0	66	0.0	8	-8.0	-8.0
2S		2	2	0.0	0.0	70.8	66	70.8	66	10	Snd Lvl	70.8	66	0.0	8	-8.0	-8.0
1N		3	10	0.0	0.0	59.8	66	59.8	66	10	---	59.8	66	0.0	8	-8.0	-8.0
2N		4	8	0.0	0.0	58.7	66	58.7	66	10	---	58.7	66	0.0	8	-8.0	-8.0
3N		5	14	0.0	0.0	57.2	66	57.2	66	10	---	57.2	66	0.0	8	-8.0	-8.0
4N		6	12	0.0	0.0	59.4	66	59.4	66	10	---	59.4	66	0.0	8	-8.0	-8.0
5N		7	1	0.0	0.0	64.5	66	64.5	66	10	---	64.5	66	0.0	8	-8.0	-8.0
Dwelling Units		# DUs		Noise Reduction													
				Min		Avg		Max									
				dB		dB		dB									
All Selected		60		0.0		0.0		0.0									
All Impacted		2		0.0		0.0		0.0									
All that meet NR Goal		0		0.0		0.0		0.0									

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2 August 2011

INPUT: ROADWAYS

The Corradino Group
Ted Stone

4093-01 Ft. Wayne State St

2 August 2011
TNM 2.5

INPUT: ROADWAYS
PROJECT/CONTRACT:
RUN:

4093-01 Ft. Wayne State St
2030 State Widened

Average pavement type shall be used unless
a State highway agency substantiates the use
of a different type with the approval of FHWA

Roadway Name	Width ft	Points			Coordinates (pavement)			Flow Control			Segment	
		Name	No.	X	Y	Z		Control Device	Speed Constraint mph	Percent Vehicles Affected %	Pvmt Type	On Struct?
State EB	12.0	point1	1	18,590.0		3,537.0	792.00				Average	
		point2	2	18,937.0		3,560.0	785.00				Average	
		point3	3	19,362.0		3,574.0	771.00				Average	
		point 4	4	19,479.8		3,587.5	768.00				Average	
		point5	5	19,525.3		3,595.5	765.00				Average	
		point6	6	19,593.3		3,611.9	763.00				Average	
State WB	20.0	point25	25	22,035.0		3,756.0	756.00				Average	
		point26	26	21,666.0		3,740.0	756.00				Average	
		point27	27	21,554.0		3,726.0	763.00				Average	
		point28	28	21,381.0		3,677.0	768.00				Average	
		point29	29	21,224.0		3,625.0	760.00				Average	
		point53	53	19,584.7		3,640.4	757.00				Average	
Westbrook	24.0	point98	98	19,539.7		3,670.2	755.50				Average	
		point54	54	19,487.0		3,769.0	754.00				Average	
		point55	55	19,458.0		3,874.0	754.00				Average	
		point56	56	19,434.0		4,124.0	754.00				Average	
	24.0	point57	57	19,629.9		3,590.5	756.00				Average	
		point58	58	19,649.0		3,581.0	756.00				Average	
Edgehill		point59	59	19,690.0		3,542.0	756.00				Average	
		point60	60	19,721.0		3,492.0	756.00				Average	
		point61	61	19,725.0		3,457.0	756.00				Average	
		point62	62	19,772.0		3,321.0	757.00				Average	
		point63	63	19,834.0		3,245.0	757.00				Average	
		point64	64	19,898.0		3,195.0	758.00				Average	
		point65	65	19,977.0		3,119.0	758.00				Average	

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2 August 2011

INPUT: ROADWAYS		4093-01 Ft. Wayne State St											
Eastbrook	24.0	point66	66	19,671.0	3,787.0	758.00					Average		
		point67	67	19,643.0	3,857.0	758.00					Average		
		point68	68	19,639.0	3,903.0	758.00					Average		
		point69	69	19,648.0	3,965.0	758.00					Average		
		point70	70	19,707.8	4,449.3	758.00							
Oakridge	24.0	point71	71	19,895.0	3,915.0	757.00					Average		
		point72	72	19,907.0	3,965.0	758.00					Average		
		point73	73	20,051.0	4,448.0	761.00							
Terrace	20.0	point74	74	20,134.0	3,812.0	757.00					Average		
		point75	75	20,321.0	4,394.0	757.00							
Clinton	36.0	point76	76	20,529.7	4,023.7	766.00					Average		
		point77	77	20,418.9	3,668.7	760.00							
Spy Run	36.0	point80	80	21,395.0	3,208.0	760.00					Average		
		point81	81	21,234.0	3,588.0	760.00					Average		
		point82	82	21,217.0	3,638.0	760.00					Average		
		point83	83	21,051.0	4,061.0	762.00							
State EB-2-2	12.0	point86	86	19,691.6	3,770.4	756.00					Average		
		point8	8	19,752.0	3,832.0	757.00					Average		
		point9	9	19,822.0	3,873.0	757.00					Average		
		point10	10	19,899.0	3,896.0	756.00							
State EB-2-2-2	12.0	point87	87	19,902.7	3,894.5	756.00					Average		
		point11	11	19,985.0	3,885.0	756.00					Average		
		point12	12	20,050.0	3,857.0	756.00					Average		
		point13	13	20,124.0	3,786.0	757.00							
State EB-2-2-2-2	20.0	point15	15	20,247.0	3,702.0	760.00	Signal	15.00	100		Average		
		point16	16	20,408.0	3,641.0	760.00					Average		
		point17	17	20,618.0	3,571.0	759.00					Average		
		point18	18	20,697.0	3,561.0	759.00					Average		
		point19	19	20,886.0	3,571.0	759.00					Average		
		point20	20	21,224.2	3,597.1	760.00							
State EB-2-2-2-2-2	20.0	point89	89	21,243.6	3,601.7	760.00					Average		
		point21	21	21,390.0	3,656.0	768.00					Average		
		point22	22	21,567.0	3,706.0	763.00					Average		
		point23	23	21,668.0	3,719.0	757.00					Average		
		point24	24	22,043.0	3,737.0	756.00							
State WB-2	20.0	point90	90	21,214.4	3,623.5	760.00					Average		
		point49	49	20,888.5	3,597.6	760.00					Average		
		point30	30	20,690.0	3,588.0	760.00					Average		
		point31	31	20,624.0	3,595.0	760.00					Average		
I:\Projects\4093-01\TNM\2030 State Widened												2	2 August 2011

INPUT: ROADWAYS

4093-01 Ft. Wayne State St

			point32	32	20,417.0	3,664.0	760.00			Average	
			point50	50	20,258.3	3,720.9	758.50				
State WB-2-2	12.0		point91	91	20,129.3	3,803.0	757.00			Average	
			point35	35	20,058.0	3,867.0	757.00			Average	
			point36	36	19,987.0	3,899.0	756.00			Average	
			point37	37	19,894.6	3,908.9	756.00				
State WB-2-2-2	12.0		point92	92	19,890.2	3,907.4	756.00			Average	
			point38	38	19,814.0	3,884.0	757.00			Average	
			point39	39	19,741.3	3,843.9	757.00			Average	
			point51	51	19,673.9	3,772.8	757.50				
State WB-2-2-2-2	12.0		point94	94	19,549.1	3,637.9	763.00			Average	
			point40	40	19,508.7	3,630.8	765.00			Average	
			point41	41	19,464.2	3,622.0	768.00			Average	
			point42	42	19,349.0	3,614.7	771.00			Average	
			point43	43	19,092.3	3,605.7	777.00			Average	
			point44	44	18,929.0	3,598.5	785.00			Average	
			point45	45	18,587.0	3,572.0	792.00				
Clinton-2	36.0		point96	96	20,416.7	3,662.3	760.00			Average	
			point78	78	20,408.9	3,643.1	760.00				
Clinton-2-2	36.0		point97	97	20,406.7	3,637.6	760.00			Average	
			point79	79	20,294.0	3,287.0	756.00				
State EB New Alignment	12.0		point99	99	19,599.0	3,618.0	763.00			Average	
			point100	100	19,937.0	3,703.0	763.00			Average	
			point101	101	20,043.0	3,723.0	763.00			Average	
			point102	102	20,167.0	3,719.0	760.00			Average	
			point103	103	20,244.0	3,705.2	757.00				
State WB New Alignment	12.0		point104	104	20,253.0	3,721.0	757.00			Average	
			point105	105	20,170.0	3,746.0	760.00			Average	
			point106	106	20,039.0	3,750.0	763.00			Average	
			point107	107	19,933.0	3,732.0	763.00			Average	
			point108	108	19,579.0	3,644.0	763.00				
New Oakridge	24.0		point109	109	19,884.2	3,876.5	757.00			Average	
			point110	110	19,897.9	3,746.5	763.00				

INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5

INPUT: TRAFFIC FOR LAeq1h Volumes

PROJECT/CONTRACT:
4093-01 Ft. Wayne State St
2030 State Widened

RUN:

Roadway Name		Points													
Name	No.	Segment													
		Autos		MTrucks		HTrucks		Buses		Motorcycles					
		V	S	V	S	V	S	V	S	V	S				
		veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph	veh/hr	mph
State EB	point1	1	668	35	8	35	1	35	1	35	1	35	8	35	35
	point2	2	668	35	8	35	1	35	1	35	1	35	8	35	35
	point3	3	668	35	8	35	1	35	1	35	1	35	8	35	35
	point 4	4	668	35	8	35	1	35	1	35	1	35	8	35	35
	point5	5	668	35	8	35	1	35	1	35	1	35	8	35	35
	point6	6													
State WB	point25	25	1516	35	15	35	15	35	15	35	7	35	15	35	35
	point26	26	1516	35	15	35	15	35	15	35	7	35	15	35	35
	point27	27	1516	35	15	35	15	35	15	35	7	35	15	35	35
	point28	28	1516	35	15	35	15	35	15	35	7	35	15	35	35
	point29	29													
	point53	53	44	15	0	0	0	0	0	0	0	0	0	0	0
Westbrook	point98	98	44	15	0	0	0	0	0	0	0	0	0	0	0
	point54	54	44	15	0	0	0	0	0	0	0	0	0	0	0
	point55	55	44	15	0	0	0	0	0	0	0	0	0	0	0
	point56	56													
	point57	57	40	20	0	0	0	0	0	0	0	0	0	0	0
	point58	58	40	20	0	0	0	0	0	0	0	0	0	0	0
Edgehill	point59	59	40	20	0	0	0	0	0	0	0	0	0	0	0
	point60	60	40	20	0	0	0	0	0	0	0	0	0	0	0
	point61	61	40	20	0	0	0	0	0	0	0	0	0	0	0
	point62	62	40	20	0	0	0	0	0	0	0	0	0	0	0

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1

INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St

	point63	63	40	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point64	64	40	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point65	65															
Eastbrook	point66	66	80	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point67	67	80	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point68	68	80	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point69	69	80	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point70	70															
Oakridge	point71	71	12	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point72	72	12	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point73	73															
Terrace	point74	74	28	10	0	0	0	0	0	0	0	0	0	0	0	0	0
	point75	75															
Clinton	point76	76	1520	35	8	35	16	35	4	35	16	35	4	35	16	35	35
	point77	77															
Spy Run	point80	80	1520	35	8	35	16	35	4	35	16	35	4	35	16	35	35
	point81	81	1520	35	8	35	16	35	4	35	16	35	4	35	16	35	35
	point82	82	1520	35	8	35	16	35	4	35	16	35	4	35	16	35	35
	point83	83															
State EB-2-2	point86	86	40	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point8	8	40	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point9	9	40	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point10	10															
State EB-2-2-2	point87	87	14	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point11	11	14	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point12	12	14	20	0	0	0	0	0	0	0	0	0	0	0	0	0
	point13	13															
State EB-2-2-2-2	point15	15	1011	25	10	25	11	25	5	25	10	25	5	25	10	25	25
	point16	16	1011	25	10	25	11	25	5	25	10	25	5	25	10	25	25
	point17	17	1011	25	10	25	11	25	5	25	10	25	5	25	10	25	25
	point18	18	1011	25	10	25	11	25	5	25	10	25	5	25	10	25	25
	point19	19	1011	25	10	25	11	25	5	25	10	25	5	25	10	25	25
	point20	20															
State EB-2-2-2-2-2	point89	89	880	35	12	35	4	35	4	35	16	35	4	35	16	35	35

INPUT: TRAFFIC FOR LAeq1h Volumes

4093-01 Ft. Wayne State St

	point21	21	880	35	12	35	4	35	4	35	16	35
	point22	22	880	35	12	35	4	35	4	35	16	35
	point23	23	880	35	12	35	4	35	4	35	16	35
	point24	24										
State WB-2	point90	90	1516	30	15	30	15	30	7	30	15	30
	point49	49	1516	30	15	30	15	30	7	30	15	30
	point30	30	1516	30	15	30	15	30	7	30	15	30
	point31	31	1516	30	15	30	15	30	7	30	15	30
	point32	32	1516	30	15	30	15	30	7	30	15	30
	point50	50										
State WB-2-2	point91	91	14	20	0	0	0	0	0	0	0	0
	point35	35	14	20	0	0	0	0	0	0	0	0
	point36	36	14	20	0	0	0	0	0	0	0	0
	point37	37										
State WB-2-2-2	point92	92	40	20	0	0	0	0	0	0	0	0
	point38	38	40	20	0	0	0	0	0	0	0	0
	point39	39	40	20	0	0	0	0	0	0	0	0
	point51	51										
State WB-2-2-2-2	point94	94	556	30	8	30	4	30	0	0	8	30
	point40	40	556	30	8	30	4	30	0	0	8	30
	point41	41	556	30	8	30	4	30	0	0	8	30
	point42	42	556	30	8	30	4	30	0	0	8	30
	point43	43	556	30	8	30	4	30	0	0	8	30
	point44	44	556	30	8	30	4	30	0	0	8	30
	point45	45										
Clinton-2	point96	96	1520	35	8	35	16	35	4	35	16	35
	point78	78										
Clinton-2-2	point97	97	1520	35	8	35	16	35	4	35	16	35
	point79	79										
State EB New Alignment	point99	99	1011	35	10	35	11	35	5	35	10	35
	point100	100	1011	35	10	35	11	35	5	35	10	35
	point101	101	1011	35	10	35	11	35	5	35	10	35
	point102	102	1011	35	10	35	11	35	5	35	10	35
	point103	103										

I:\Projects\4093-01\TNM\2030 State Widened

INPUT: TRAFFIC FOR LAeq1h Volumes

State WB New Alignment	point104	104	1516	35	15	35	15	35	7	35	15	11
	point105	105	1516	35	15	35	15	35	7	35	15	35
	point106	106	1516	35	15	35	15	35	7	35	15	35
	point107	107	1516	35	15	35	15	35	7	35	15	35
	point108	108										
New Oakridge	point109	109	120	20	0	0	0	0	0	0	0	0
	point110	110										

4093-01 Ft. Wayne State St

I:\Projects\4093-01\TNM\2030 State Widened

INPUT: RECEIVERS												4093-01 Ft. Wayne State St											
The Corradino Group Ted Stone												3 August 2011 TNM 2.5											
INPUT: RECEIVERS												4093-01 Ft. Wayne State St											
PROJECT/CONTRACT:												2030 State Widened											
RUN:																							
Receiver																							
Receiver Name	No.	#DUs	Coordinates (ground)			Height above Ground	Input Sound Levels and Criteria					Active In Calc.											
			X	Y	Z		Existing LAeq1h	Impact Criteria LAeq1h	Sub'l	NR Goal													
			ft	ft	ft	ft	dBA	dBA	dB	dB													
1S	1	13	19,744.0	3,461.0	756.00	4.92	0.00	66	10.0	8.0	Y												
2S	2	2	20,234.0	3,675.0	757.00	4.92	0.00	66	10.0	8.0	Y												
1N	3	10	19,534.1	3,722.2	755.00	4.92	0.00	66	10.0	8.0	Y												
2N	4	8	19,672.0	3,830.0	756.00	4.92	0.00	66	10.0	8.0	Y												
3N	5	16	19,920.0	3,938.0	757.00	4.92	0.00	66	10.0	8.0	Y												
4N	6	12	20,178.0	3,882.0	760.00	4.92	0.00	66	10.0	8.0	Y												
5N	7	1	21,682.0	3,812.1	755.00	4.92	0.00	66	10.0	8.0	Y												

I:\Projects\4093-01\TNM\2030 State Widened

1

3 A

INPUT: BUILDING ROWS 4093-01 Ft. Wayne State St

The Corradino Group
Ted Stone

2 August 2011
TNM 2.5

INPUT: BUILDING ROWS

PROJECT/CONTRACT: 4093-01 Ft. Wayne State St

RUN: 2030 State Widened

Building Row Name	Average Height ft	Building Percent %	Points			
			No.	Coordinates (ground)		
				X	Y	Z
				ft	ft	ft
Building N	20.00	80	1	20,484.0	4,025.0	766.00
			2	20,398.0	3,733.0	760.00
Building S	20.00	40	3	20,352.0	3,592.0	759.00
			4	20,297.0	3,428.0	757.00
Building Terrace	20.00	70	5	20,181.7	3,777.3	760.00
			6	20,232.9	3,931.2	760.00
			7	20,243.2	3,924.3	760.00

I:\Projects\4093-01\TNM\2030 State Widened

INPUT: TERRAIN LINES		2 August 2011	
The Corradino Group		TNM 2.5	
Ted Stone			
INPUT: TERRAIN LINES		4093-01 Ft. Wayne State St	
PROJECT/CONTRACT:		2030 State Widened	
RUN:			
Terrain Line		Points	
Name	No.	Coordinates (ground)	
		X	Y
		ft	ft
			Z
			ft
Terrain Line1	1	19,240.0	4,005.0
	2	19,342.0	3,641.0
Terrain Line2	3	19,408.0	3,533.0
	4	19,608.0	3,211.0
Terrain Line at Riv	5	21,651.8	3,760.2
	6	21,419.3	4,187.6
			770.00
			770.00
			770.00
			770.00
			768.00
			768.00

APPENDIX D
66 FOOT CONTOUR LINE
WITH PROJECT



Lackey, Brett

From: Lawrence, Ben [BLAWRENCE@indot.IN.gov]
Sent: Tuesday, October 18, 2011 11:00 AM
To: Lackey, Brett
Cc: Bales, Ronald; Kaiser, Jason; Smith, Gregory
Subject: State Boulevard Between Spy Run and Cass Street in Fort Wayne, Des # 0400587

Brett,

We have reviewed the noise study for the above-referenced project and have determined that it is technically sufficient. Please make the following minor corrections prior to distribution:

1. On page 1, in the second paragraph, correct the CFR reference to 772 rather than 772.
2. On page 13, consider removing the public involvement paragraph (third paragraph, second full paragraph). Our understanding is that no public involvement activities were carried out for the noise study.

Note that INDOT no longer approves noise wall decisions on local projects, so we will not be providing any opinion on the appropriateness of recommendations.

Please let us know if you have any questions.

Ben Lawrence, PE
Environmental Policy Manager
Environmental Services
Indiana Department of Transportation
V: 317-233-1164 F: 317-233-4929

Appendix J: Section 4(f)

- Vesey Park – de minimis impact 4(f) documentation
- Draft Section 4(f) Evaluation for Impacts to Historic Properties (text only)
- Department of the Interior comments – July 8, 2013



AMERICAN
STRUCTUREPOINT
INC.

January 16, 2013

Mr. Steve McDaniel
Deputy Director of Parks
705 East State Boulevard
Fort Wayne, Indiana 46805

Re: Section 4(f) Coordination
State Boulevard Reconstruction
Fort Wayne, Allen County, Indiana
Des. No. 0400587
Project No. IN20071404

Dear Mr. McDaniel:

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve State Boulevard from Spy Run to Cass Street. The project area is located in Wayne Township, Fort Wayne, Allen County, Indiana. It is more specifically located on the Fort Wayne West USGS 7.5 Minute Quadrangle Map, in the east half of Section 35, Township 31 North, Range 12 East. The total project length is approximately 2,300 feet.

The existing 2-lane section of State Boulevard between Clinton Street and Cass Street will be widened to five lanes, while correcting the sub-standard horizontal curve. The 5-lane section will include two new travel lanes in each direction and a center, 2-way, left-turn lane. A boulevard-type section with median landscaping will be provided in those areas where a center left-turn lane is not required. The project also includes a new bridge over Spy Run Creek and a prefabricated trail bridge over State Boulevard at the abandoned New York Central railroad right-of-way between Cass Street and Westbrook Drive.

In order to construct the project, right-of-way will need to be acquired from Vesey Park located southeast of the existing State Boulevard Bridge. The existing State Boulevard Bridge will be removed as part of the project and a new bridge constructed on new alignment. Removal of the existing bridge will require the acquisition of 0.12 acres of temporary right-of-way for construction and grading. Approximately 0.55 acre of land is proposed to be acquired in the location of the new bridge over Spy Run Creek. Approximately 0.021 acre is proposed to be acquired along the western edge of Eastbrook Avenue on the north side of existing State Boulevard. An additional 0.033 acre is proposed to be acquired for an underground storm sewer outfall east of Westbrook Avenue. During design, efforts will be made to minimize the amount of right-of-way to be acquired. No impacts are anticipated to park equipment or features.





AMERICAN STRUCTUREPOINT, INC.

Mr. Steve McDaniel

January 16, 2013

Page 2

Because Vesey Park is significant and publically owned, it is considered a Section 4(f) resource. Analysis of potential impacts to Section 4(f) resources is required for any federally funded transportation project. The Federal Highway Administration (FHWA) has developed guidance for projects that do not have significant impacts to Section 4(f) properties. Impacts evaluated under this guidance are termed *de minimis* if they meet certain criteria.

The following criteria must apply for a project to be considered to have *de minimis* impacts to a section 4(f) resource.

1. An official or officials with jurisdiction over the resource must agree in writing that the proposed project will not adversely affect the activities, features, and attributes that qualify the resource for protection under Section 4(f).
2. The transportation use of the Section 4(f) resource, together with any impact avoidance, minimization, and mitigation or enhancement measures incorporated into the project, does not adversely affect the activities, features, and attributes that qualify the resource for protection under Section 4(f).
3. The official(s) with jurisdiction over the property are informed of FHWA or Federal Transit Administration (FTA) intent to make the *de minimis* impact finding based on their written concurrence that the project will not adversely affect the activities, features, and attributes that qualify the property for protection under Section 4(f).
4. The public has been afforded an opportunity to review and comment on the effects of the project on the protected activities, features, and attributes of the Section 4(f) resource.

The project fulfills or will fulfill all criteria listed above and is not expected to adversely affect the Section 4(f) property. Per criteria Item 1 above, if you agree the proposed State Boulevard project will not adversely affect the Section 4(f) resource under your jurisdiction, we respectfully request your written concurrence. Your written statement of approval must be obtained in order to complete the Section 4(f) *de minimis* evaluation and will be included in the environmental documentation for the project.

Project mapping has been included with this letter for your reference. Please feel free to provide your correspondence via standard mail, email, or fax as you would prefer. To facilitate the development of this project, your timely cooperation will be greatly appreciated.

Please contact me at (317) 547-5580 or cmeadeor@structurepoint.com if there are any questions or if additional information is needed.

Very truly yours,
American Structurepoint, Inc.



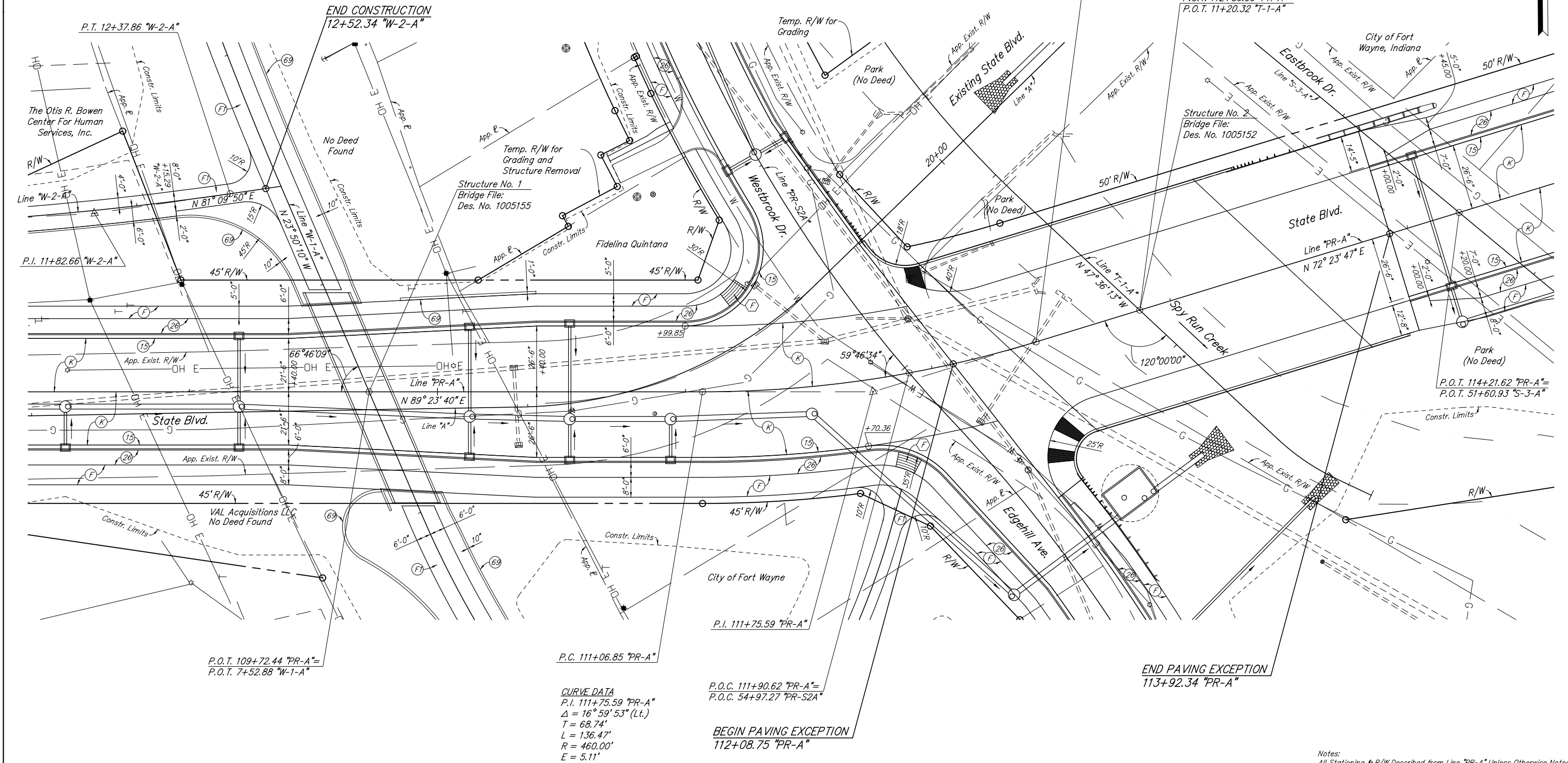
Christine Meador
Environmental Scientist

CAM:alo

Enclosures

CURVE DATA
P.I. 11+82.66 "W2A"
 $\Delta = 4^\circ 52' 07"$ (Lt.)
T = 55.27'
L = 110.46'
R = 1,300.00'
E = 1.17'

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY



Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

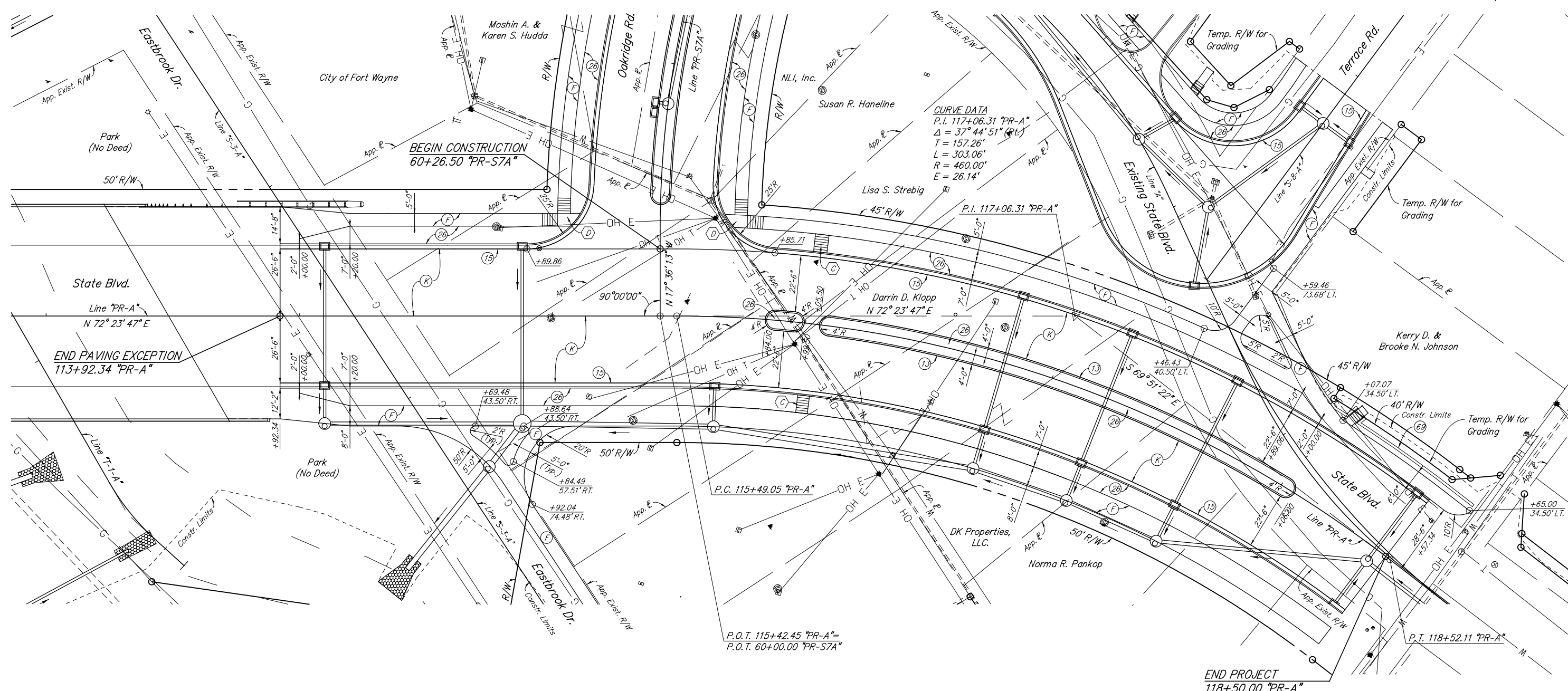
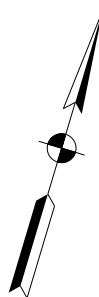
LEGEND			
(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C1) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F1) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall
	(U) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: PJT	
CHECKED: SMC	CHECKED: SMC	

INDIANA DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS LINE "PR-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS
CONTRACT R-33130	38 of 138
	PROJECT NO. 1005154

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

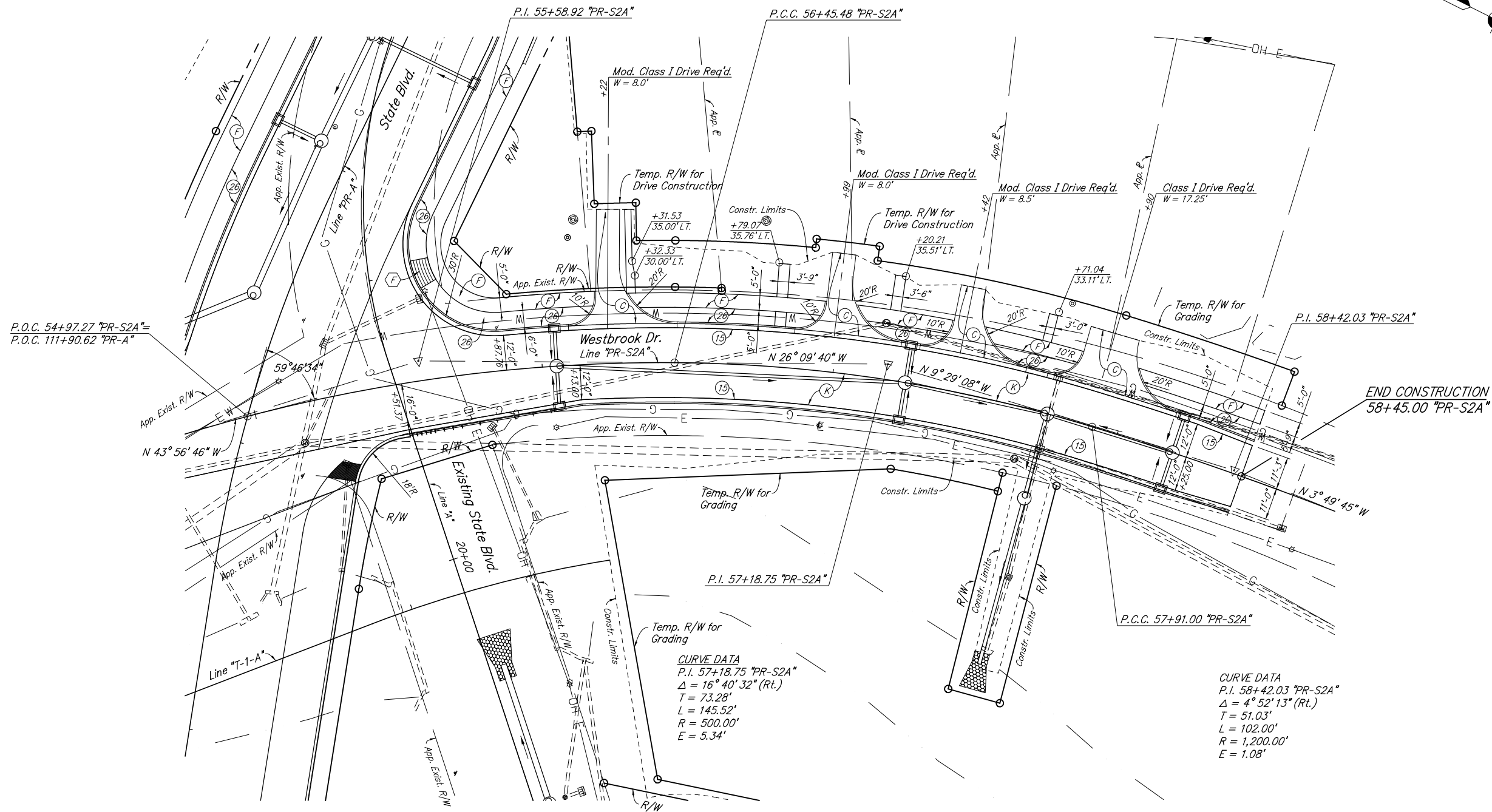


Notes:
All Stationing & R/W Described from Line "PR-A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>(C) PCCP for Approaches, 6"</p> <p>(C) PCCP for Approaches, 9"</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"</p>		<p>LEGEND</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</p> <p>(F) Concrete Sidewalk, 4"</p> <p>(F) Shared Use Path Pavement (See Typical Sections)</p> <p>(V) Compacted Aggregate, No. 53</p>		<p>(K) Full Depth Pavement (See Typical Sections)</p> <p>(13) Concrete Curb</p> <p>(14) Integral Concrete Curb</p> <p>(15) Curb and Gutter, Concrete, Modified</p> <p>(22) Concrete Center Curb, Type "D"</p> <p>Center Curb Removal Limits</p>		<p>(26) Sodding, Nursery</p> <p>(28) Mulched Seeding, Type "U"</p> <p>(69) Retaining Wall</p> <p>(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)</p>		<p>RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE</p> <p>DESIGNED: ASU DRAWN: PJT</p> <p>CHECKED: SMC CHECKED: SMC</p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>CONSTRUCTION DETAILS LINE "PR-A"</p>		<p>HORIZONTAL SCALE 1" = 20'</p> <p>VERTICAL SCALE NONE</p> <p>SURVEY BOOK CONTRACT R-33130</p>		<p>BRIDGE FILE DESIGNATION NO. 1005154</p> <p>SHEETS 39 of 138 PROJECT NO. 1005154</p>	
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CURVE DATA
 P.I. 55+58.92 "PR-S2A"
 $\Delta = 17^\circ 47' 06''$ (Rt.)
 $T = 87.97'$
 $L = 174.53'$
 $R = 562.26'$
 $E = 6.84'$

SEC. 35, T 31 N, R 12 E
 WAYNE TOWNSHIP
 ALLEN COUNTY



Notes:
 All Stationing & R/W Described from Line "PR-S2A" Unless Otherwise Noted.
 For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

<p>(C) PCCP for Approaches, 6"</p> <p>(C) PCCP for Approaches, 9"</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</p> <p>(F) Concrete Sidewalk, 4"</p> <p>(F) Shared Use Path Pavement (See Typical Sections)</p> <p>(V) Compacted Aggregate, No. 53</p>	<p>LEGEND</p> <p>(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"</p> <p>(F) Concrete Sidewalk, 4"</p> <p>(F) Shared Use Path Pavement (See Typical Sections)</p> <p>(V) Compacted Aggregate, No. 53</p>	<p>(K) Full Depth Pavement (See Typical Sections)</p> <p>(13) Concrete Curb</p> <p>(14) Integral Concrete Curb</p> <p>(15) Curb and Gutter, Concrete, Modified</p> <p>(22) Concrete Center Curb, Type "D"</p>	<p>(26) Sodding, Nursery</p> <p>(28) Mulched Seeding, Type "U"</p> <p>(69) Retaining Wall</p> <p>(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)</p> <p>Center Curb Removal Limits</p>
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RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	
DESIGNED: ASU	DRAWN: CAK
CHECKED: SMC	CHECKED: SMC

INDIANA DEPARTMENT OF TRANSPORTATION	
CONSTRUCTION DETAILS LINE "PR-S2A"	

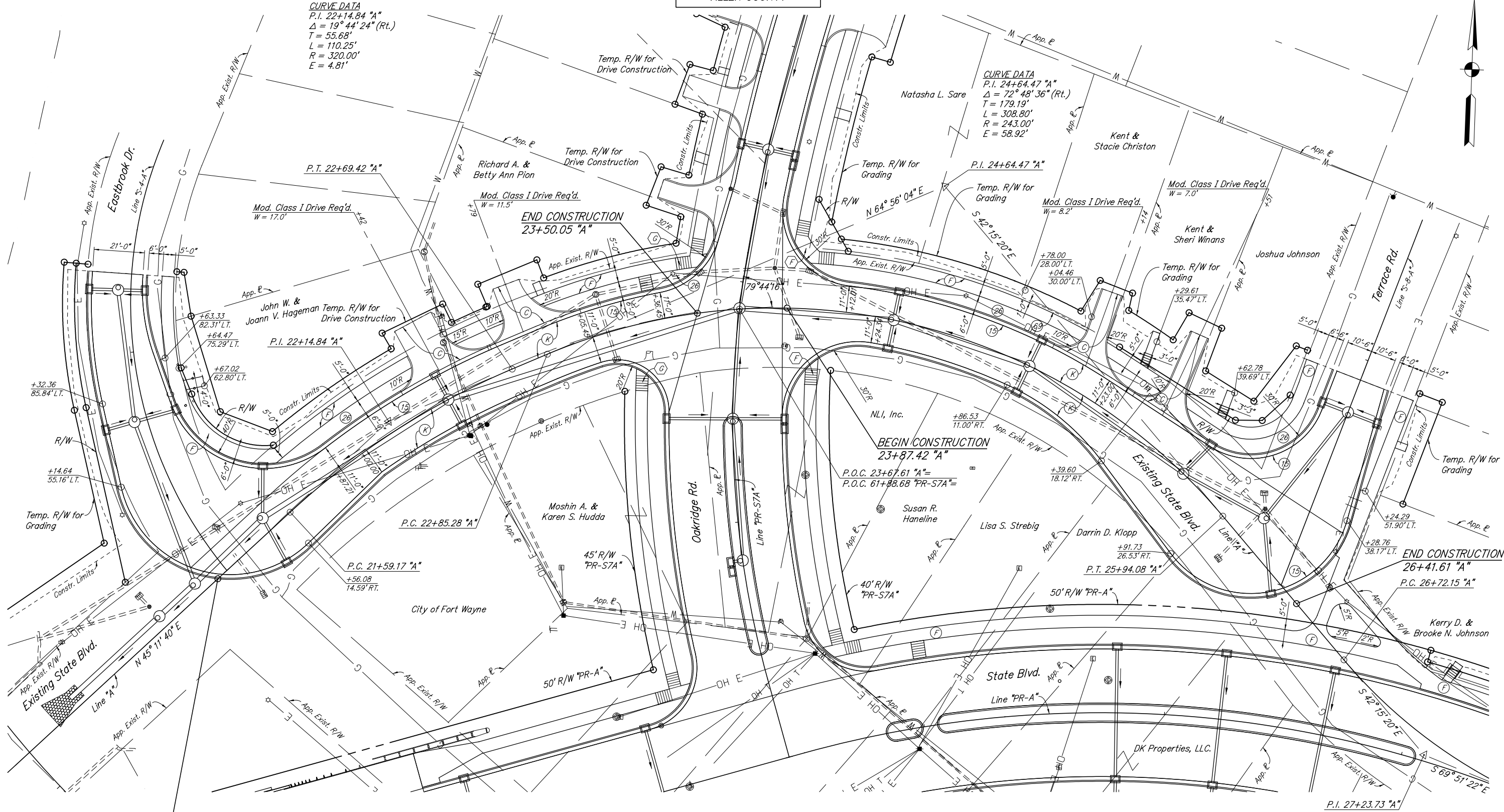
HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS 42 of 138
CONTRACT R-33130	PROJECT NO. 1005154

SEC. 35, T 31 N, R 12 E
WAYNE TOWNSHIP
ALLEN COUNTY

25

CURVE DATA
P.I. 22+14.84 "A"
 $\Delta = 19^\circ 44' 24''$ (Rt.)
T = 55.68'
L = 110.25'
R = 320.00'
E = 4.81'

CURVE DATA
P.I. 24+64.47 "A"
 $\Delta = 72^\circ 48' 36''$ (Rt.)
T = 179.19'
L = 308.80'
R = 243.00'
E = 58.92'



BEGIN CONSTRUCTION
21+18.36 "A"

END CONSTRUCTION
26+41.61 "A"
P.C. 26+72.15 "A"

Notes:
All Stationing & R/W Described from Line "A" Unless Otherwise Noted.
For Proposed Drainage Structures & Notes See Drainage Detail Sheets.

(C) PCCP for Approaches, 6"	(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B"	(K) Full Depth Pavement (See Typical Sections)	(26) Sodding, Nursery
(C1) PCCP for Approaches, 9"	(F) Concrete Sidewalk, 4"	(13) Concrete Curb	(28) Mulched Seeding, Type "U"
(D) HMA for Approaches 165 #/Syd. HMA Surface Type "B" on 275 #/Syd. HMA Intermediate Type "B" 880 #/Syd. HMA Base Type "B"	(F1) Shared Use Path Pavement (See Typical Sections)	(14) Integral Concrete Curb	(69) Retaining Wall
	(V) Compacted Aggregate, No. 53	(15) Curb and Gutter, Concrete, Modified	(X) Concrete Curb Ramp (Type is Indicated by Letter Inside Hexagon)
		(22) Concrete Center Curb, Type "D"	Center Curb Removal Limits

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ASU	DRAWN: CAK	
CHECKED: SMC	CHECKED: SMC	

INDIANA
DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAILS
LINE "A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE
VERTICAL SCALE NONE	DESIGNATION NO. 1005154
SURVEY BOOK	SHEETS
CONTRACT R-33130	44 of 138 PROJECT NO. 1005154



FORT WAYNE
PARKS AND
RECREATION

Play well...Live well



January 23, 2013

Christine Meador
Environmental Scientist
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256-3957

Re: Section 4(f) Coordination
State Blvd Reconstruction
Fort Wayne, IN 46805
Des. No. 0400587
Project Number - IN20071404

Dear Ms Meador:

The staff of the Fort Wayne Parks and Recreation Department has reviewed your packet dated January 16, 2013 with regards to improvements proposed for State Boulevard Reconstruction in Fort Wayne, Indiana. It appears that there will not be any impact to the recreational activities, features and attributes of the land currently being used as recreational space.

If you have any questions of the Fort Wayne Parks and Recreation Department regarding the Park land in the scope of work, please feel free to contact me at your earliest convenience.

Sincerely,

Steve McDaniel
Deputy Director of Park Maintenance
Fort Wayne Parks and Recreation Department
705 E. State Blvd.
Fort Wayne, IN 46805

Cc: Al Moll, Director of the Fort Wayne Parks and Recreation
Shan Gunawardena, City of Fort Wayne Engineer

705 E. State Blvd.
Fort Wayne, IN 46805

(260) 427-6000

www.fortwayneparks.org

**Draft Section 4(f) Evaluation
for
Impacts to Historic Properties**

**State Boulevard Reconstruction Project
Fort Wayne, Allen County, Indiana
Des. No. 0400587**

**City of Fort Wayne
200 E. Berry Street
Fort Wayne, Indiana 46802**

**Indiana Department of Transportation
Office of Environmental Services
642 Government Center North
100 North Senate Avenue
Indianapolis, Indiana 46204**

**Federal Highway Administration
254 Federal Office Building
575 North Pennsylvania Street
Indianapolis, Indiana 46204**

February 14, 2014

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 - Section 800.6(a)(3) Documentation

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 F. Correspondence

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Introduction

Section 4(f) of the US Department of Transportation Act (DOT Act) of 1966 [49 U.S.C. 303 (c)] states the use of any land from a significant publicly owned park or recreation area, wildlife and waterfowl refuge, or private or publically owned historic site on or considered eligible for the National Register of Historic Places (NR) shall not be allowed unless:

- a. There is no feasible and prudent alternative to the use of such land.
- b. The proposed project includes all possible planning to minimize harm to such park, recreational area, wildlife and waterfowl refuge, or historic site resulting from such use.

Pursuant to regulations at 23 CFR Part 774, a full evaluation is required to determine the most feasible federal-aid route that causes the least overall harm considering the following factors:

- a. The ability to mitigate adverse impacts to each Section 4(f) property, including measures that benefit the property
- b. The relative severity of the remaining harm, after mitigation to the protected activities, attributes, or features that qualify each property for Section 4(f) protection
- c. The relative significance of each Section 4(f) property
- d. The views of the official(s) with jurisdiction over each Section 4(f) property
- e. The degree to which each alternative meets the purpose and need of the project
- f. After reasonable mitigation, the magnitude of any adverse impacts to resources not protected by Section 4(f)
- g. Substantial differences in costs among the alternatives

The purpose of this report is to evaluate and summarize the proposed project's purpose and need, reasonable alternatives, the Section 4(f) resources, the 4(f) resources that are used by these alternatives; avoidance alternatives that relate to these 4(f) resources, and all possible planning to minimize harm, if the resources cannot be avoided.

Proposed Action

The City of Fort Wayne Board of Public Works is developing a federal-aid project to improve a section of State Boulevard between Spy Run and Cass Street in Fort Wayne, Wayne Township, Allen County, Indiana. The project area is located in Wayne Township in the east half of Section 35, Township 31 North, Range 12 East.

The project extends from Cass Street to the west and Spy Run Avenue to the east, an overall project length of 2,370 feet. The current proposed alternative involves widening the existing 2-lane section of State Boulevard between Cass Street and Clinton Street to four lanes and correcting the substandard horizontal curve. In this segment, State Boulevard would have four 10-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot wide raised median and a 2-way left turn lane. The horizontal and vertical alignment will be modified between Westbrook Drive and Clinton Street to correct substandard roadway geometrics, as well as alleviate roadway flooding at Spy Run Creek. The

horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four 11-foot travel lanes, two in each direction, separated by a 12-foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway alignments. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs.

Combined concrete curb and gutters, including curb inlets and storm sewer, would be constructed throughout the corridor. A raised median containing landscape elements would be constructed where left turn lanes are not required between Oakridge Road and Clinton Street. New sidewalks, varying in width from five feet to ten feet would be constructed on both sides of the roadway. The sidewalk would be constructed adjacent to the curb throughout the corridor. A sodded, landscaped utility strip, typically five feet wide, would be installed between the back of curb and sidewalk where available space permits between the bridge over Spy Run Creek and Terrace Road.

New decorative lighting would be installed along the project and the existing traffic signals at Clinton Street and Spy Run Avenue would be modified as necessary.

As a part of this project, a new pedestrian bridge would be constructed over State Boulevard at the existing abandoned railroad crossing. Sidewalk ramps would be extended from proposed State Boulevard to the pedestrian bridge approach connecting State Boulevard to the future Pufferbelly Trail. The pedestrian bridge and ramps would be utilized by the proposed Pufferbelly Trail which would be constructed by others.

Purpose and Need for the Proposed Action

The primary purpose of the proposed project is to improve corridor connectivity along State Boulevard for both motorists and pedestrians alike. Currently, the existing corridor does not provide a safe environment for motorists, bicyclists, or pedestrians as the existing roadway is significantly congested and exhibits substandard sight distance and geometrics. In addition, State Boulevard is often impassable due to roadway flooding caused by Spy Run or the Saint Mary's River.

The need for this project derives from the traffic congestion along the corridor between Cass Street and Spy Run Avenue, the substandard sight distances at various intersections along the corridor, roadway flooding, and the substandard horizontal geometrics between Cass Street and Clinton Street. The State Boulevard project corridor also becomes congested at the intersections due to the reduction in lanes through this segment. In addition, pedestrian safety is compromised due to this level of congestion and insufficient sight distance at the substandard horizontal curves. Pedestrian facilities do not currently provide connectivity between the Greenways Trail System.

The selected and approved Transportation Plan for the Fort Wayne Urbanized Area is based on an "Arterial plus Bypass" concept to improve mobility, connectivity, and accessibility within the region. This concept includes improvements to a number of arterial corridors and the completion of I-469 as a "bypass" around the urban area. State Boulevard is one of the arterials identified in the Transportation Plan for improvement.

State Boulevard is one of a few east-west arterials that provide some continuity as motorists and pedestrians traverse the urban area. Continuous adjacent parallel roadways include the Washington Center Road/St. Joe Center Road corridor (approximately 2.5 miles north) and the Washington Road/Jefferson Boulevard corridor (one-way pair approximately 1.3 miles south). Coliseum Boulevard (approximately 1.5 miles north) also helps

to serve east-west travel but also traverses north-south as it passes through the urban area, breaking its east-west continuity. Due to the limited number of continuous east-west corridors, the carrying capacity required of corridors such as State Boulevard to meet travel demands is elevated.

As part of the development of the Metropolitan Transportation Plan and the “Arterial plus Bypass” concept, the Northern Indiana Regional Coordinating Council (NIRCC) evaluated a number of potential roadways for improvement to help improve east-west traffic flow in the area north of the Fort Wayne Central Business District. Three corridors were considered for improvements to facilitate east-west travel by providing additional east-west roadways. The corridors included State Boulevard, Butler Road-Vance Road, and Spring Street-Tennessee Avenue. Through the Transportation Plan development, reviews of these corridors determined that State Boulevard was the most practical option.

As the Transportation Plan has been implemented, a number of investments in transportation improvements have been constructed on the State Boulevard Corridor. These improvements include widening the bridge over the St. Joseph River just east of Spy Run Avenue, a project necessary to support the widening project between Spy Run and Cass Street. A major intersection improvement project was also completed at State Boulevard and Wells Street that included the widening of State Boulevard between Goshen Avenue and Cass Street. State Boulevard has also been widened to four lanes east of the proposed project between Coliseum Boulevard and Maplecrest Road to facilitate traffic flow and reduce congestion.

The State Boulevard project from Spy Run Avenue (US 27 northbound) to Cass Street is a project consistent with the current Transportation Plan and improvement projects implemented in accordance with the transportation planning process. The proposed project would reduce existing congestion and improve traffic flow. State Boulevard is a 4-lane arterial from east of Maplecrest Road to Spy Run Avenue. It reduces to three lanes west of Spy Run Avenue, with two eastbound through lanes and one westbound lane. East of Clinton Street, State Boulevard is a 2-lane road with one travel lane in each direction. East of the project area, Goshen Road, an arterial traversing through the northwest portion of the urban area, merges into State Boulevard, approximately doubling the daily traffic volume.

State Boulevard is also an important east west arterial in the Fort Wayne Central Business District Fringe Area. It connects with a number of important north-south arterials including Hillegas Road, Sherman Street, Wells Street, Clinton Street (US 27 south bound), Spy Run Avenue (US 27 north bound), Parnell Avenue, Crescent Avenue, Anthony Boulevard, Hobson Road, Coliseum Boulevard (State Road 930), Reed Road and Maplecrest Road. State Boulevard merges with Maysville Road and Stelhorn Road as it leaves the Urban Area east of I-469 and becomes State Route 37.

Under current traffic conditions, congestion occurs at the intersections of Spy Run Boulevard and Clinton Street resulting in unacceptable service levels. The redevelopment of the urban core area will continue to place travel demands on the State Boulevard corridor and contribute to modest increases in traffic volumes. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within the urban area. Currently, both intersections exhibit intersection movements having service levels of E or F as described in the following table.

State Street and Spy Run Avenue Intersection

Morning Peak	LOS Existing
East Bound Left	F
West Bound Through	E
Evening Peak	LOS Existing
East Bound Left	F
East Bound Through	E
West Bound Through	E

State Street and Clinton Street Intersection

Morning Peak	LOS Existing
South Bound Through	E
Evening Peak	LOS Existing
East Bound Through	E
West Bound Left	F

Both intersections at Spy Run Avenue and Clinton Street also exhibit lengthy delays demonstrating the congested conditions. Modest increases in traffic volumes will exacerbate these conditions and cause additional delay and service failures. The proposed project would reduce delay and improve overall intersection service to acceptable levels of service (“D” or above).

In addition to the congestion issues, the existing horizontal alignment along State Boulevard does not currently meet Indiana Design Manual guidelines for minimum curve radius. The Level One controlling design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are those highway design elements which are judged to be the most critical indicators of a highway’s safety and its overall serviceability. The horizontal alignment and minimum curve radius of a roadway is considered to be a very important level one controlling design element.

According to IDM Chapter 43, Figure 43-3B, the horizontal alignment for a 30 mph roadway is required to be a minimum of 300 feet. As noted in the curve radius table below, several of the existing horizontal curve radii along the existing alignment currently do not meet proper Level One design standards. For further reference to the IDM see http://www.in.gov/indot/design_manual/design_manual_2013.htm.

Curve Radius Table:

Station Line “A”	Existing Curve Radius	Required Radius (30 mph)
18+66.60	175 feet	300 feet
24+64.47	243 feet	300 feet
27+23.73	210 feet	300 feet

The Level Two design criteria found in Section 40-8.02 of the INDOT Design Manual (IDM) are judged to be important indicators of a highway’s safety and serviceability but are not considered as critical as the Level One Criteria. The intersection sight distance along the roadway is a critical Level Two design element essential for a safe corridor for both vehicular and pedestrian traffic. A motorist entering State Boulevard and turning left must be able to see 420 feet along State Boulevard to safely make the left turn maneuver. Similarly, a motorist entering State Boulevard and turning right must be able to see 375 feet along State Boulevard to safely make the right turn maneuver. As noted in the “Intersection Sight Distance Table” below, many of the intersections along the State Boulevard corridor do not meet the proper Level Two design standards.

Intersection Sight Distance Table:

Intersection	Turning Direction	Approx. Exist. Sight Dist. (feet)	Required Sight Distance (feet)
Cass Street (south)	LT	300	420
Cass Street (south)	RT	160	375
Westbrook Dr. (South)	LT	150	420
Westbrook Dr. (North)	LT	210	420
Eastbrook Dr. (South)	LT	270	420
Eastbrook Dr. (South)	RT	210	375
Eastbrook Dr. (North)	LT	250	420
Terrace Rd. (North)	RT	160	375

Congestion, substandard horizontal alignment, and inadequate sight distance likely contribute to the high crash rate along the State Boulevard project corridor. Four of the major intersections along the project corridor are in the top twenty high crash locations in Allen County for the time period 2007-2009. In order to be placed on this list, the locations must consistently (all three years) display a high crash frequency, high crash rate (RMV-rate per million entering vehicles), and high index of crash costs. As shown in the table below, the RMV exceeds 2.0 which indicates that a safety problem exists for the years 2007 to 2009 and for both 2010 and 2011 at State Boulevard and Clinton Street.

Crash Location	2007				2008				2009				2010				2011			
	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV	Total Crash	Injury Total Crash	Total Fatal Crash	RMV
State Boulevard and Eastbrook Dr.	17	4	0	2.41	17	4	0	2.61	15	1	0	2.11	9	1	0	1.26	12	3	0	1.69
State Boulevard and Clinton St.	41	7	0	2.74	49	10	0	3.28	35	8	0	2.38	30	3	0	2.04	36	8	0	2.45
State Boulevard And Spy Run Ave.	34	4	0	2.04	35	8	0	2.12	41	6	0	2.48	27	7	0	1.63	43	11	0	2.60
State Boulevard and Westbrook Dr.	16	3	0	2.31	17	5	0	2.38	12	1	0	2.16	9	1	0	1.26	12	3	0	1.69

The high crash rates can likely be attributed to traffic congestion, substandard geometrics, intersection sight distances, and the multiple driveways that are directly accessed from State Boulevard between Westbrook Drive and Terrace Road. Currently, State Boulevard does not provide motorists with a center left turn lane to allow turning vehicles to move out of the path of the thru traffic, or provide required sight distance between Westbrook and Clinton Streets to allow for adequate stopping distance.

For many of the same reasons stated above, pedestrian safety is also a concern along the State Boulevard project corridor. The existing pedestrian facilities through this corridor are in poor condition. The existing sidewalks exhibit extensive deterioration such as cracking, settling, and heaving due to age and weathering. The north/south pedestrian connectivity is also very limited due to the traffic congestion and poor sight distance for pedestrians attempting to cross State Boulevard between Cass Street and Clinton Street.

Currently pedestrians and bicyclists have to share deteriorating narrow sidewalks along State Boulevard. The Pufferbelly Trail, a piece of the Greenways Trail System which will run along the west side of Westbrook Drive and will cross State Boulevard with a pedestrian bridge, is currently being constructed. The St. Joseph Pathway, also a piece of the Greenways Trail System, runs along the St. Joseph River and crosses State Boulevard near

the eastern project terminus. The State Boulevard project corridor currently does not provide an adequate and safe link between the two trails.

The existing bridge carrying State Boulevard over Spy Run Creek provides insufficient waterway area and is quickly deteriorating. According to the 2006 Allen County Structure Inventory and Appraisal Report the existing bridge has a sufficiency rating of 27.9 which classifies the bridge as structurally deficient. According to the report, the expected remaining life of the bridge superstructure is five years from the date of the inspection report (2011). The existing bridge is currently below the flood elevation of the St. Mary's River which causes the bridge to be overtopped with backwater from the Saint Mary's River with relative frequency, therefore affecting roadway safety by flooding State Boulevard. According to the Spy Run Flood Control Study (Christopher B. Burke, 2005) "This flooding is caused primarily by backwater from the St. Mary's River which controls the water surface elevation up to about State Boulevard. The State Boulevard crossing causes a significant backwater affecting the upstream water surface elevation to about Grove Street."

According to recent City of Fort Wayne records, Spy Run Creek has experienced flood events causing sandbag or clay berm protection in the following years: 1976, 1978, 1981, 1982, 1985, 1991, 1993, 1999, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, and 2010. Seven out of the 17 years (1978, 1982, 2003, 2004, 2005, 2008, and 2009), State Boulevard was closed due to the flooding events. Road closure due to flooding events appear to be happening more consistently in recent years, restricting emergency traffic more often.

Description of the Section 4(f) Resources

Three historic properties and one park were identified within the limits of the proposed project.

The Westerly Group, Inc. (Westerly) and Weintraut & Associates Historians, Inc. (Weintraut) were contracted by American Structurepoint, Inc. to prepare a Historic Properties and Section 106 Documentation and Findings. Westerly and Weintraut, in conjunction with recommendations and comment from the State Historic Preservation Officer (SHPO) and consulting parties, determined three historic properties listed in or eligible for listing in the National Register of Historic Places (NR) would be affected by the undertaking. The three properties include the Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the Bridge over Spy Run Creek.

The park identified as being affected by the undertaking includes the greenway portion of Vesey Park running parallel to Spy Run Creek, along both the east and west banks.

Fort Wayne Park and Boulevard System Historic District (NR, 2010). The Fort Wayne Park and Boulevard System Historic District is generally bound by the 1912 plan for the City of Fort Wayne. The district encompasses the system of 11 parks, four parkways (including ten "park or park-like areas" associated with the parkways), and ten boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. The district includes nearly 2,000 acres of parks, boulevards, and sites. There are eight resources identified as part of the Fort Wayne Park and Boulevard System historic district located within the APE for this project. Seven of those identified resources contribute to the historic district and include: Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and bridge over Spy Run Creek (NBI No. 0200273). The Fort Wayne Park and Boulevard System Historic District was listed on the NR in 2010 and is significant under Criteria A and C in the areas of Community Planning and Development, Entertainment/Recreation, and Landscape Architecture. The period of significance is from 1909, marking the date of the first park and boulevard master plan, to 1955, marking the date when the park and boulevard plan was "essentially realized."

Brookview-Irvington Park Historic District (NR, 2011). The Brookview-Irvington Park Historic District is roughly bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street, and Jacobs Avenue. The district contains a total of 424 contributing resources including houses, garages, and the combined plats of the district, as well as the previously determined eligible bridge over Spy Run Creek (NBI No. 0200273). Ninety-two resources associated with the historic district are within the project APE. The district is significant under Criteria A and C in the areas of Community Planning and Development, Landscape Architecture, and Architecture. The period of significance is 1906-1965, representing the construction dates of most buildings within the historic district, and also encompasses the utilization of Centlivre Park (no longer extant) as a resort destination.

Bridge over Spy Run Creek (NBI No. 0200273). The bridge over Spy Run Creek (NBI No. 0200273) is a reinforced concrete girder, T-Beam bridge constructed in 1927 by contractor Herman W. Tapp and featuring the design of A.W. Grosvenor and O. Darling. The bridge was previously determined eligible for listing in the NRHP per the Indiana Statewide Historic Bridge Inventory (2010). The bridge over Spy Run Creek is eligible under Criterion C for Engineering/Architecture and is a Non-Select bridge. The period of significance is 1927, the year it was constructed.

Vesey Park: Additionally, Vesey Park was noted in the project limits. This park is operated by the City of Fort Wayne Parks Department and includes the green space along Spy Run between Eastbrook Drive and Westbrook Drive. It connects the larger portion of Vesey Park located at Irvington Drive and Eastbrook Drive to the south to Lawton Park along the St. Mary's River. The park features open space among the trees with areas for picnicking and views of Spy Run Creek.

With the exception of the structures discussed, no other significant features are on the affected properties. No known covenants or other restrictions or conditions would relate to the acquisition of the necessary right-of-way from any of the properties.

Alternatives

Avoidance Alternative

There are no alternatives that can simultaneously meet the project's Purpose and Need while also avoiding all Section 4(f) resources. All the reasonable alternatives use 4(f) resources. Given the extensive north-south boundaries of the Brookview-Irvington Park Historic District, and the east-west nature of the transportation corridor need, no other avoidance alternatives, besides the No Build Alternative, were identified that would not result in a use a Section 4(f) resource.

Alternative 4: No Build

With the No Build Alternative, there would be no use of resources subject to Section 4(f) provisions. This alternative would leave the existing State Boulevard roadway as it currently exists. No reconstruction of the roadway to meet the project's purpose and need would be implemented. The existing roadway and bridge would continue to deteriorate. The existing roadway would continue to flood causing continued problems with accessibility and pavement deterioration. Traffic accidents would most likely continue to increase as the current congestion issues would not be addressed. The existing bridge over Spy Run Creek is currently rated structurally deficient and the estimated remaining life of the superstructure is five years. This structure is in immediate need of replacement due to the condition. East-west connectivity would continue to be a problem for the overall transportation network. The no build alternative would likely result in the complete failure of the structure over Spy Run Creek.

The No Build Alternative would not meet any of the needs of the project; therefore, is not considered a feasible and prudent alternative.

Initial 4(f) Use Alternatives Considered and Screened

Alternative 1: Butler Road – Vance Road Corridor

This alternative includes developing the Butler Road – Vance Road Corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile north of the existing State Boulevard roadway. The alternative would begin at the Butler Road intersection with Cedar Ridge Run / Sprunger Road East and proceed east a distance of approximately 3.25 miles to a terminus at the Vance Road intersection with North Anthony Boulevard.

This alternative would require approximately 2.25 miles of new roadway alignment in order to connect the existing terminus of Butler Road with the existing (western) termini of Vance Road, which is located immediately east of the St. Joseph River. The remaining 1.0 mile of the corridor (east of Spy Run Creek) would be constructed along the existing Vance Road alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would also require the construction of new bridges over Spy Run Creek and the St. Joseph River.

This alternative would require extensive residential and commercial relocations. A minimum of 125 residential relocations and 15 commercial relocations would be required. This alternative would also result in impacts to the Franke Park Elementary School and the Fort Wayne Children's Zoo. Of the approximate 2.25 miles of new roadway alignment required for this corridor, approximately 2.0 miles would be constructed on presently undeveloped, forested land.

Alternative 1 results in the use of the Brookview-Irvington Historic District (northern extents), Vesey Park, and Franke Park, all 4(f) resources.

This alternative is not reasonable as it does not address any of the Project's purpose and need. Alternative 1 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential and commercial relocations for construction and approximately 2.0 miles of new roadway through existing forested land. For these reasons, Alternative 1 has been eliminated from further consideration.

Alternative 2: Spring Street – Tennessee Avenue

This alternative includes developing the Spring Street – Tennessee Avenue corridor to improve east-west travel through Fort Wayne. The corridor would be located approximately 0.50 mile south of the existing State Boulevard roadway. The alternative would begin at the Spring Street terminus at the North Wells Street intersection and proceed east a distance of approximately 1.50 miles to a terminus at the intersection of Lake Avenue and Forest Park Boulevard.

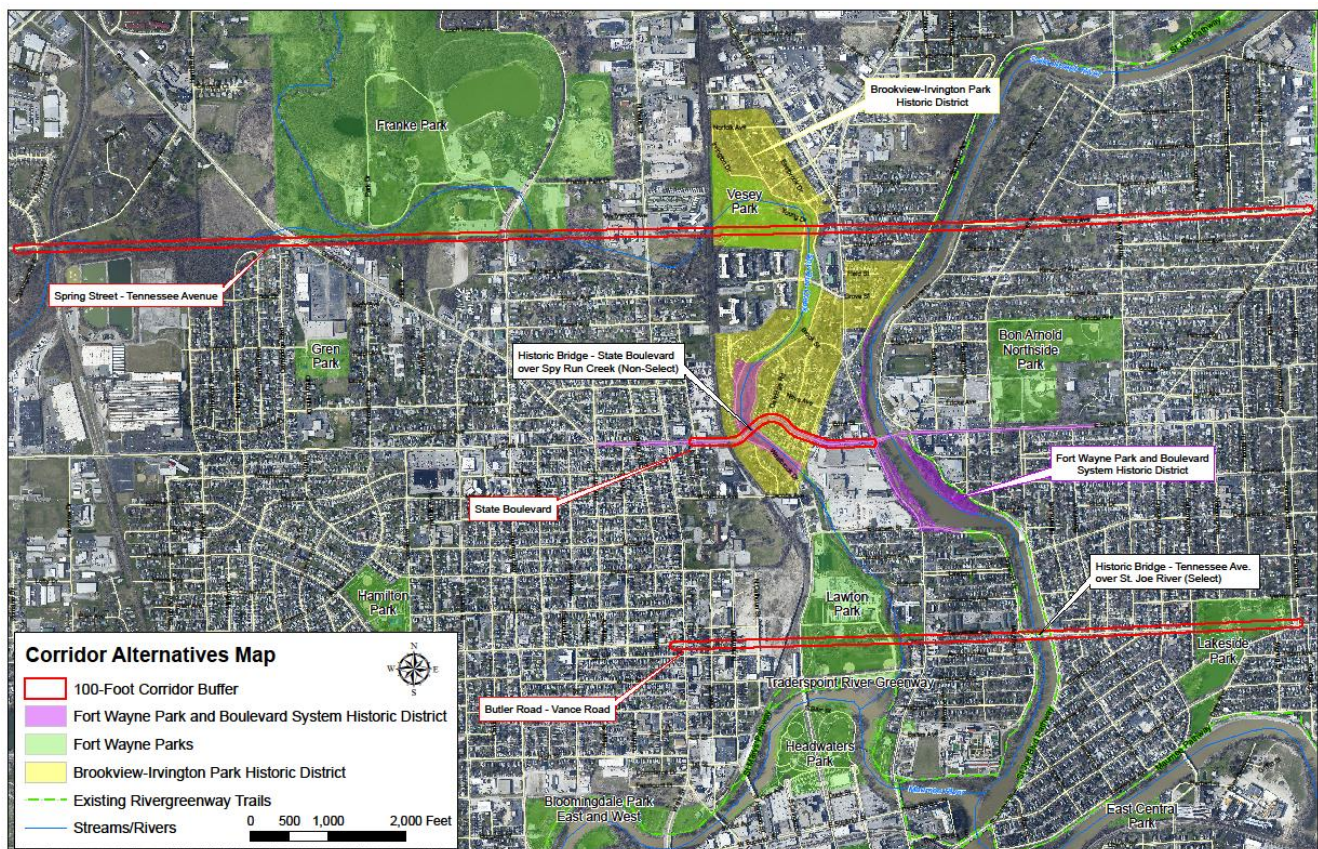
This alternative would require approximately 0.60 mile of new roadway alignment in order to connect the existing (eastern) terminus of Spring Street with the existing (western) terminus of Tennessee Avenue, which is located immediately east of the Spy Run Creek. An additional 0.25 mile of new roadway alignment would be required in order to connect the existing (eastern) terminus of Tennessee Avenue with Lake Avenue. The remaining 0.65 mile of the corridor would be constructed along the existing Tennessee Avenue alignment, expanding the existing roadway travel lanes to accommodate anticipated traffic volumes. This alternative would require the construction of a new bridge over Spy Run Creek. This alternative would also require the expansion of the existing Tennessee Avenue bridge over the St. Joseph River, a select historic bridge determined to be eligible for the National Register of Historic Places.

This alternative would require extensive residential and commercial relocations. A minimum of 75 residential relocations and 15 commercial relocations would be required. This alternative would also result in impacts or relocations to the Science Central Museum, Lakeside Park, and Lawton Park.

This alternative would result in the use of 4(f) resources including Lakeside Park, Lawton Park, and the NRHP eligible bridge over the St. Joseph River.

The alternative is not reasonable as it does not address any part of the Project's purpose and need. Alternative 2 does not address connectivity along the State Boulevard corridor, correct the substandard horizontal curve, or address the roadway flooding concerns along State Boulevard. Furthermore, this alternative would require an extensive number of residential, commercial, and recreational property impacts/relocations for construction. For these reasons, Alternative 2 has been eliminated from further consideration.

Alternatives 1 and 2



4(f) Use Alternatives Retained for Further Consideration

Alternative 3A: Substandard Horizontal Curve Correction with 4-Lane Typical Section

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes and correcting the substandard horizontal curve. State Boulevard would have four 10-foot travel lanes, two in each direction. Between Oakridge Road and Clinton Street, the travel lanes would be separated by an 8-foot wide raised median and a 2-way left turn lane. The horizontal and vertical alignment would be modified between Westbrook Drive and Clinton Street to correct substandard roadway geometrics, as well as alleviate roadway flooding at Spy Run Creek. The horizontal alignment would shift a maximum of approximately 190 feet south of existing State Boulevard. The vertical alignment would be raised approximately

seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four 11-foot travel lanes, two in each direction, separated by a 12-foot 2-way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway alignments. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Oakridge Road and State Boulevard. The existing intersections of State Boulevard with Eastbrook Drive and Terrace Drive would be eliminated and turned into cul-de-sacs. New sidewalks, varying in width from five feet to ten feet would be constructed on both sides of the roadway. As a part of this alternative, a new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

Alternative 3A would result in the use of The Fort Wayne Park and Boulevard System Historic District, the Brookview-Irvington Park Historic District, and the bridge over Spy Run Creek (NBI No. 0200273). The Federal Highway Administration (FHWA) has determined the Section 106 finding of “Adverse Effect” is appropriate for the properties listed. The following summarizes anticipated use of Section 4(f) properties by the proposed project.

Fort Wayne Park and Boulevard System Historic District - The undertaking would affect the Fort Wayne Park and Boulevard System Historic District. In correcting the substandard horizontal curve and widening the roadway, the project would acquire right-of-way from the District and alter the historic location of State Boulevard. In addition, Eastbrook Drive (contributing feature) would be eliminated to the south of State Boulevard as the project would acquire all residential properties located along this portion of the roadway rendering the street unnecessary. Eastbrook Drive would be converted to a cul-de-sac north of State Boulevard, eliminating the existing Eastbrook Drive and State Boulevard intersection. The undertaking also proposes the removal of the existing bridge over Spy Run Creek, a contributing property, as the existing bridge does not provide a sufficient waterway opening and is in poor condition. The realigned State Boulevard profile would have a significant increase in vertical elevation (approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). A prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing a new visual element to the District. FHWA has determined the appropriate Section 106 finding is “Adverse Effect” and there is a Section 4(f) use.

As mitigation for the impacts to the district, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the present State Boulevard bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements.

Brookview-Irvington Park Historic District - The undertaking would require the removal of approximately 15 contributing residential resources (not individually NRHP eligible) from the Brookview-Irvington Park Historic District, which would also result in a change to the orientation of the Brookview neighborhood plat (contributing resource). One residential property was identified as individually eligible along the State Boulevard corridor; however, no portions of this property would be converted to a transportation use. The realignment of State Boulevard and change in elevation would also result in the bifurcation of the district. Most of the contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the bridge over Spy Run

Creek. Through the realignment of State Boulevard, the conversion of both Eastbrook Drive and Terrace Drive (north of State Boulevard) to cul-de-sacs, the replacement of the bridge over Spy Run Creek, and the removal of 15 contributing properties, the landscape of the area would be modified altering the character and setting of the district by creating much larger open public spaces. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard. FHWA has determined the appropriate Section 106 finding is “Adverse Effect” and there is a Section 4(f) use.

As mitigation for the impacts to the district, context sensitive solutions would be implemented, such as utilizing large scale, low-branched vegetation to emulate the street edge along the former path of State Boulevard as a reminder of the former roadway. In addition, fill slopes leading to the higher road elevations would be made gentle and obscured with low branched trees. Medians planted with low shrubs would be utilized to break roadways into smaller components that would be in scale with other neighborhood streets. The design of the present State Boulevard Bridge over Spy Run Creek (NBI No. 0200273) would be recalled in the design of the new bridge, and the utilization of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods along the new roadway alignment would help maintain continuity between the various elements. In addition, the City of Fort Wayne would make an effort to salvage architectural details from homes demolished for use in other District residences, as well as explore funding opportunities to provide low cost grants/loans to improve/rehabilitate historic resources within the Brookview-Irvington Historic District.

The bridge over Spy Run Creek (NBI No. 0200273) – The bridge over Spy Run Creek, located near the center of the project area, would be removed as it does not provide a sufficient waterway opening and is in poor condition. The removal or demolition would be consistent with the “Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana’s Historic Bridges” (Historic Bridge PA). The pending removal or demolition of the bridge is considered an adverse effect.

This alternative addresses the projects purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Alternative 3A also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. Alternative 3A fully satisfies the projects purpose and need.

Table 1 identifies Section 4(f) resources, their location, and use by the proposed construction (Alternative 3A) on each of the resources.

Table 1: Summary of Section 4(f) Resources and Anticipated Use (Alternative 3A)				
Section 4(f) Resource	Location	Right-of-Way to be Acquired	Structures to be Removed	Section 4(f) Use
Fort Wayne Park and Boulevard System Historic District (NR 2010)	Includes Spy Run Creek, Sloping Hills and Natural Features, Clinton Street Bridge, Westbrook Drive, Eastbrook Drive, State Boulevard (Lindenwood to Anthony), State Boulevard through Brookview, and bridge over Spy Run Creek (NBI No. 0200273)	0.60 acre permanent	State Boulevard, Eastbrook Drive, bridge over Spy Run Creek	Permanent right-of-way acquisition and removal of contributing resources from historic location
Brookview-Irvington Park Historic District (NR 2011)	Bound by Northfolk Avenue, Lima Road, Spy Run Avenue, North Clinton Street and Jacobs Avenue	2.6 acre permanent	15 contributing residential structures (not individually NRHP eligible), bridge over Spy Run Creek (non-select)	Permanent right-of-way acquisition and removal of contributing resources from historic location
Bridge over Spy Run Creek (NBI. 0200273)	State Boulevard at Spy Run Creek	None	bridge over Spy Run Creek (non-select)	Programmatic Section 4(f) for Historic Bridges ¹
Vesey Park	Along both east and west banks of Spy Run Creek between Westbrook Drive and Eastbrook Drive	0.55 acre permanent, 0.12 acre temporary	None	<i>De minimis</i> ¹

1. *Programmatic Section 4(f) Evaluation for Historic Bridges and De minimis Section 4(f) evaluation will be completed as part of the Environmental Assessment. Further discussion of this Section 4(f) use will not be included in this document*

Alternative 3A



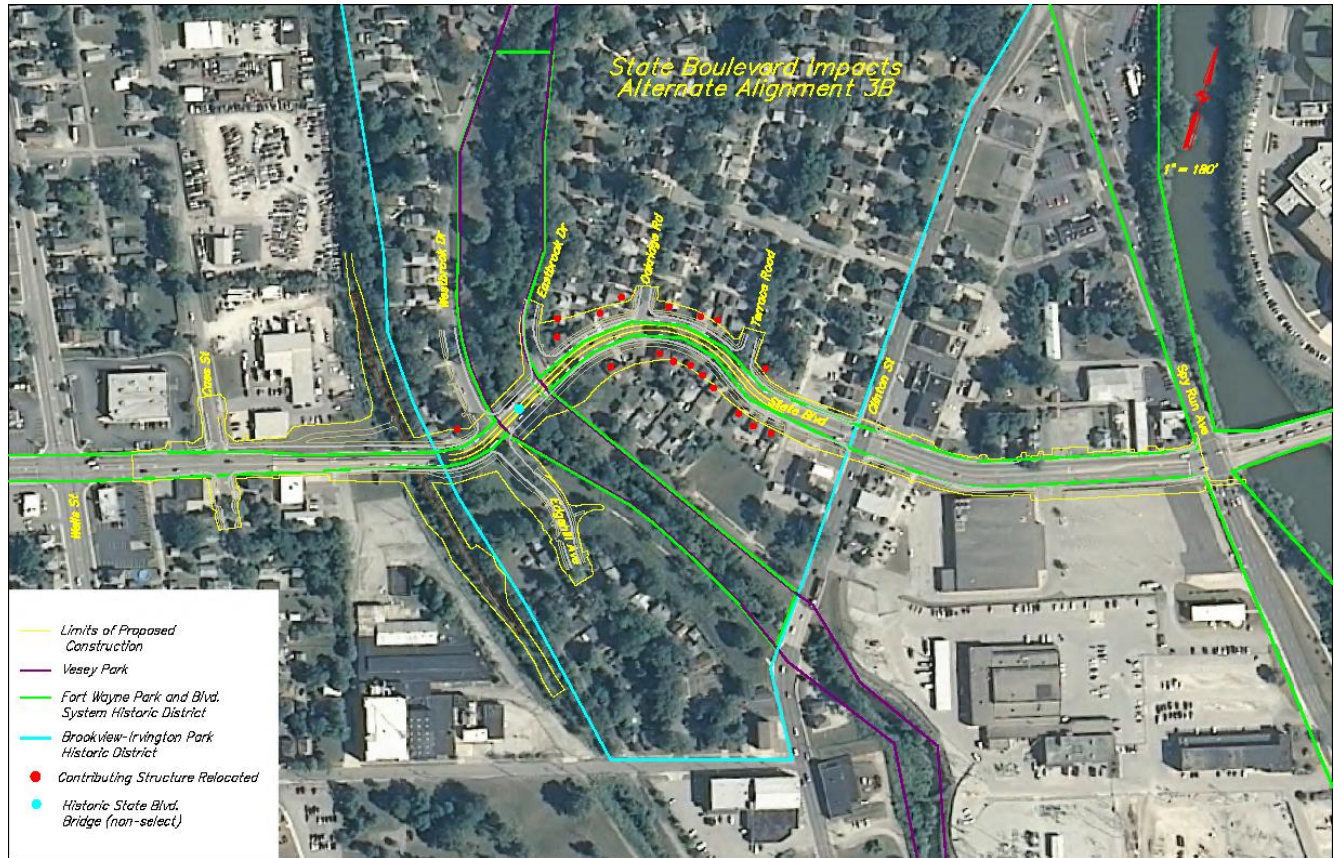
Alternative 3B: Widen State Boulevard on Existing Alignment

This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 4-lanes. This alternative would require a new bridge over Spy Run Creek at an elevation 7 feet above the existing bridge elevation. The overall alternative length is 2,700 feet.

This alternative would require approximately 18 residential relocations (contributing properties) from the Brookview-Irvington Historic District in order to provide the right-of-way necessary to widen State Boulevard along on the existing alignment.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. Furthermore, this alternative requires a higher number of residential and historic property relocations for construction as compared to other alternatives.

Alternative 3B



Alternative 3C: Shift State Boulevard Alignment South

This alternative involves shifting the alignment of State Boulevard south and constructing the new alignment for 4-lanes. This alternative would essentially take the existing State Boulevard alignment between Westbrook Drive and Clinton Street and “mirror” or “flip” the alignment to the south. The roadway would be designed to meet current roadway geometric standards. The existing intersection of State Boulevard with Eastbrook Drive would be eliminated and converted to a cul-de-sac. Access to existing State Boulevard would be via a new access road which would extend from the new State Boulevard alignment north to the existing intersection of Terrace Road and State Boulevard. The Terrace Road extension would be required to provide access to the neighborhood north of existing State Boulevard as a result of access restrictions due to Clinton Street being a one-way south roadway. This alternative would also require a new bridge over Spy Run Creek at an elevation seven feet above the existing bridge elevation.

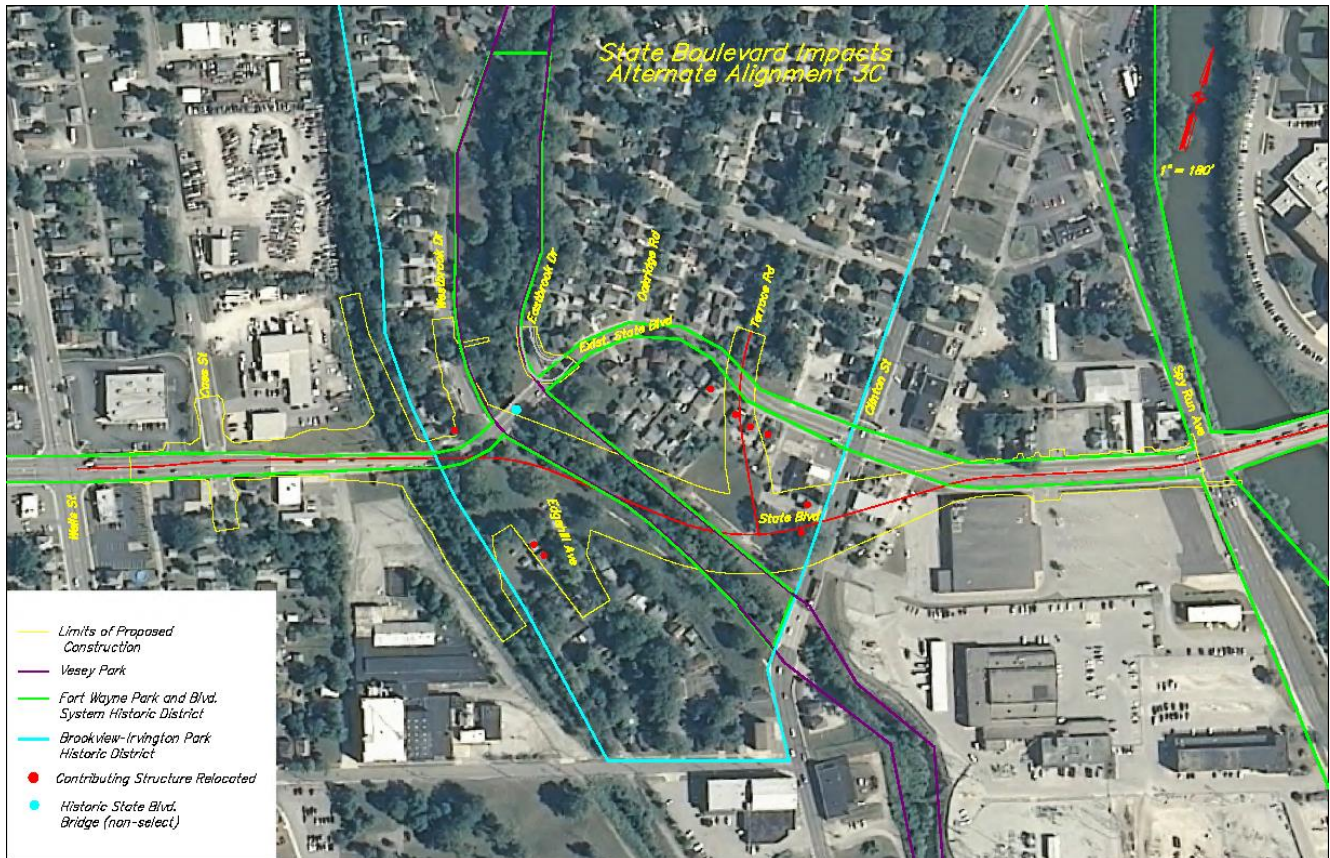
Similar to Alternative 3A, the realignment of State Boulevard and change in elevation would result in the bifurcation of the Brookview-Irvington Park Historic District. Contributing resources located within the project area would be removed from their historical locations: State Boulevard realignment, removal of residential resources, and the removal of the existing bridge over Spy Run Creek. Through the realignment of State Boulevard, the conversion of Eastbrook Drive (north of State Boulevard) to a cul-de-sac, the replacement of the bridge over Spy Run Creek, and the removal of five contributing properties, the landscape of the area would be modified altering the character and setting of the district. The construction of a prefabricated trail bridge over State Boulevard at the abandoned New York Central Railroad would also change the character of the district along State Boulevard. Furthermore, the realignment of State Boulevard would require the acquisition of right-of-way from the Fort Wayne Park and Boulevard System Historic District, again altering the historic location of State Boulevard. The realigned State Boulevard profile would have a significant increase in vertical elevation

(approximately 7-feet) as it passes over Spy Run Creek, introducing a visual barrier through the historic district as well as diminishing the presence of the sloping hills and natural features (contributing feature). The prefabricated trail bridge, access ramps, and retaining walls (associated with the Pufferbelly trail) would be constructed over the contributing State Boulevard at the abandoned New York Central Railroad bridge, introducing new visual element to the Fort Wayne Park and Boulevard System Historic District.

While this alternative would reduce the number of contributing property relocations on the south side of existing State Boulevard, it would require extensive engineering considerations and significantly increased project costs. Due to the skew angle that State Boulevard would cross Spy Run Creek; impacts to the creek would be increased by approximately 330 linear feet for the purposes of re-grading. The new bridge length would be approximately 250 feet longer than the bridge design included in Alternatives 3A or 3D. This alternative would also require construction of a new intersection of State Boulevard with Clinton Street. The new intersection would be built in close proximity to the new Terrace Road intersection which would significantly impede traffic operations and efficiency as well as increase project costs due to additional traffic signal work. The increased length of the proposed bridge combined with relocating the roadway south would also require the intersection of State Boulevard and Clinton Street to be raised two to three feet, thus causing additional reconstruction along Clinton Street (approximately 500 feet) and further increasing project costs. In addition to the nine residential relocations that are also considered contributing resources, this alternative would result in the relocation of four commercial businesses, including the gas station at the southwest corner of Clinton Street and State Boulevard, a plumbing business on the southeast corner, a dog grooming business located just south of the gas station, and a storage unit business located on the southwest corner of Spy Run Avenue and State Boulevard.

Alternative 3C addresses the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street which would create new operational and safety issues due to its close proximity to the new Terrace Road intersection. Project costs associated with Alternative 3C are an estimated five million dollars more than any other alternative due to increased impacts to commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street.

Alternative 3C



Alternative 3D: Substandard Horizontal Curve Correction with a 3-Lane Typical Section

This alternative is similar to Alternative 3A but features a 3-lane typical section rather than a 4-lane typical section. This alternative involves widening the existing 2-lane section of State Boulevard between Clinton Street and Cass Street to 3-lanes and correcting the substandard horizontal curve. Beginning at Cass Street and extending to Clinton Street, State Boulevard would have two ten foot travel lanes, one in each direction. Between Westbrook Drive and Oakridge Road, the travel lanes would be separated by a twelve-foot wide left-turn lane. Between Oakridge Road and Clinton Street, the travel lanes would be separated by a twelve foot two way left turn lane. The vertical alignment would be raised approximately seven feet at the proposed bridge over Spy Run Creek. The roadway from Clinton Street to Spy Run Avenue would consist of four eleven foot travel lanes, two in each direction, separated by a twelve foot two way left turn lane. As appropriate, left turn lanes would be installed at the intersections. The horizontal and vertical alignment between Clinton Street and Spy Run Avenue would closely follow the existing roadway. As a part of this project, the new pedestrian bridge would also be constructed over State Boulevard at the existing abandoned railroad crossing.

By reducing the typical section from 4-lanes (Alternative 3A) to 3-lanes, construction limits are reduced by approximately ten feet on each side of the roadway. Because the reduction in construction limits associated with reducing the typical section from four lanes to three lanes is only ten feet, this alternative would continue to result in the same 4(f) use as Alternative 3A to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run Creek.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain, likely eliminating the need for roadway closures due to flooding. However, Alternative 3D does not

fully address corridor connectivity or traffic congestion concerns along the corridor. This alternative would not address the congestion concerns at the intersection of State Boulevard and Clinton Street. NIRCC has established a Level of Service “D” as the acceptable peak hour service level for intersections and corridors within an urban area. This intersection currently functions at a low Level of Service. Alternative 3D would not address the poor Level of Service (E/F) at State Boulevard and Clinton Street. While the dedicated left-turn lane may help alleviate some traffic congestion along the corridor, the congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would still remain. Furthermore, this alternative would result in the same use of 4(f) resources as compared to Alternative 3A.

Alternative 3D



Measures to Minimize Harm

The proposed State Boulevard Reconstruction Project has been designed to reduce and minimize the use of each of the identified 4(f) resources. In an effort to minimize the overall footprint of the proposed roadway, a 3-lane typical section was considered and evaluated. It was determined that a 3-lane typical section would reduce the construction limits by approximately ten feet on each side of the roadway. Because the reduction would only be ten feet, the alternative would still result in the same use of 4(f) resources as the 4-lane typical section to the Brookview-Irvington Historic District, the Fort Wayne Park and Boulevard System Historic District, and the Bridge over Spy Run Creek. In addition, a 3-lane typical section would address some of the project’s safety concerns and the project’s substandard geometrics; however, a 3-lane typical section would not address corridor connectivity or traffic congestion concerns along State Boulevard. Traffic congestion concerns would not be addressed at the intersection of State Boulevard and Clinton Street and the Level of Service would remain at an unacceptable level. The congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersections would also still remain.

The modification of the proposed Oakridge Road extension was evaluated to minimize the number of total parcel acquisitions of contributing properties (112 East State Boulevard, 134 East State Boulevard, and 138 East State Boulevard) between existing State Boulevard and proposed State Boulevard. Shortening the right-turn lane and eliminating the landscaped median, constructing sidewalks adjacent to the curb with retaining wall placed at the back of sidewalks, the use of guardrail, and enclosed drainage systems utilizing inlets were all options evaluated. The evaluated aspects did not result in a significant reduction of property impact. It was concluded that the significant reduction in greenspace between the existing residence and proposed roadway, impacts to existing drives, and removal of non-residential structures located on the properties would still likely result in a total parcel acquisition outcome.

Alternatives 3A, 3C, and 3D shift State Boulevard from its historical location; however, existing curvature of State Boulevard could be maintained between Eastbrook Drive and Terrace Road. In addition, the relocation of State Boulevard associated with Alternatives 3A and 3C would require the acquisition of the remaining homes along Eastbrook Drive (south of State Boulevard), resulting in the elimination of this portion of Eastbrook Drive (contributing resource). In an effort to further minimize the use of identified Section 4(f) resources, the existing curb lines of Eastbrook Drive would remain in place where possible along this portion of the roadway.

Mitigation

Mitigation measures have been detailed in a draft MOA to be executed by consulting parties. The draft MOA includes the following mitigation measures for historic properties:

FHWA will ensure that the following measures are implemented:

I. CONTEXT SENSITIVE SOLUTIONS

A. The City of Fort Wayne shall consider and, where feasible, shall implement context sensitive solutions for this undertaking, including but not limited to the delineation of the former path of State Boulevard as a reminder of the former roadway; use of new, large scale, low-branched vegetation to emulate the street edge and the exterior walls of homes removed as a result of the undertaking in the Brookview plat; fill slopes leading to higher road elevations such that the slope is made gentle and obscured with low branched trees; medians planted with low shrubs to break roadways into smaller components that will be in scale with other neighborhood streets; use of retaining walls minimized but where used buffered by vegetation; design of present State Boulevard Bridge over Spy Run (NBI No. 0200273) recalled in the design of the new bridge; and use of streetscape elements such as historically scaled lighting, trees in park strips and other elements seen in the District neighborhoods in the new area to maintain continuity between the various elements.

B. The City of Fort Wayne shall consider and, where feasible, salvage architectural details from homes demolished as a result of the undertaking for use in other District residences.

C. The City of Fort Wayne will explore funding opportunities that will, if appropriate, provide low costs grants/loans to people in the neighborhood to improve/rehabilitate historic resources within the Brookview-Irvington Historic District. All improvements will be in compliance with, and with the oversight of, the Fort Wayne Historic Preservation Commission.

D. As soon as practical, FHWA and the City of Fort Wayne will convene an Advisory Team to ensure that the Project is designed in a manner that respects the historic qualities, landscapes, historic buildings, and features in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. Responsibilities of and participation on the Advisory Team include the following:

1. The Advisory Team will function in an advisory capacity to assist FHWA and the City of Fort Wayne in developing Project design details to implement the measures stipulated in this MOA regarding the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District.
2. Context sensitive solutions, such as protecting existing character-defining landscape features, both created and natural; dealing with light, sound, and air quality issues; providing pedestrian access across the bridge; maintaining pedestrian connections along the former Eastbrook and Westbrook drives; the rights-of-way, shall be included among the measures considered.
3. The City of Fort Wayne and FHWA shall have the authority for final approval of actions regarding the implementation of measures to avoid, minimize, or mitigate effects to the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System.
4. Representatives of the following jurisdictions and organizations will be invited by FHWA and the City of Fort Wayne to participate on the Advisory Team, based on their established geographic connection to or specific interest in the Brookview-Irvington Park Historic District, or expertise pertaining to the historic preservation area: City of Fort Wayne Parks & Recreation Department, City of Fort Wayne historic preservation planners, City of Fort Wayne Engineer, City of Fort Wayne Urban Designer (Community Redevelopment Department), the Fort Wayne Greenway Consortium, ARCH, Inc., Brookview Neighborhood Association, Friends of the Parks of Allen County, and Indiana Landmarks. The Indiana SHPO or representatives may participate in Advisory Team meetings at their discretion. The City of Fort Wayne shall provide a licensed landscape architect to attend the Advisory Team meetings.
5. Additional participants having geographic connection to, or specific interest in, the Brookview-Irvington Park Historic District or Fort Wayne Park and Boulevard Historic District or expertise pertaining to the historic preservation of the area may be invited to participate on the Advisory Team at the discretion of the City of Fort Wayne, FHWA, and the Indiana SHPO. In addition, the City of Fort Wayne shall invite the project managers of or representatives from the consultants for the other projects in the vicinity of the historic district (e.g., Pufferbelly Trail Des. No. 0710990 or US 27 Nos. 0101527 and 0200914) to participate in the meetings of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team.
6. As soon as practical, FHWA and the City of Fort Wayne will convene the Advisory Team for an initial organizational meeting to establish processes and procedures for operation of the Advisory Team will need to meet to ensure the timely completion of the project, and the number and dates of future meetings. The Advisory Team will review plans, comment, and make specific recommendations regarding Project design scopes of work and details for consideration by FHWA and the City of Fort Wayne. The Advisory Team will be chaired by a representative of the City of Fort Wayne's engineering and/or environmental consultant. The chair will be responsible for convening meetings of the Advisory Team, preparing and maintaining a summary of meetings, and preparing and submitting Advisory Team recommendations to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

7. The City of Fort Wayne's engineering and/or environmental consultant shall provide any materials needed for review by the Advisory Team at least fifteen (15) days before schedule meetings. In addition to comments voiced in the meetings, the Advisory Team members may provide written comments to the chair within fifteen (15) days following the scheduled meeting.

8. Based on the comments provided by the Advisory Team members, the chair will develop recommendations and submit them to FHWA and the City of Fort Wayne for consideration and action, in consultation with the Indiana SHPO.

9. If other Federal undertakings planned in the vicinity of the Brookview-Irvington Park Historic District and Fort Wayne Park and Boulevard System Historic District are found to result in an adverse effect to the historic district, the City of Fort Wayne shall encourage the creation of Advisory Teams of the same composition of the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to guide the development of context sensitive design as part of the mitigation of such adverse effects. The City of Fort Wayne shall make meeting minutes and other pertinent records and materials from the State Boulevard Reconstruction from Spy Run to Cass Street Advisory Team available to other such Advisory Teams.

II. PHOTOGRAPHIC DOCUMENTATION

- A. Prior to commencement of the demolition of the existing historic State Boulevard Bridge over Spy Run (NBI No. 0200273) for this undertaking, the City of Fort Wayne will ensure that photographic documentation of the State Boulevard Bridge over Spy Run (NBI No. 0200273) will take place, as provided for in the 2006 "Programmatic Agreement Among the Federal Highway Administration, the Indiana Department of Transportation, the Indiana State Historic Preservation Officer, and the Advisory Council on Historic Preservation Regarding Management and Preservation of Indiana's Historic Bridges."
- B. Prior to the commencement of site preparation, demolition, or construction activities for this undertaking within the Brookview-Irvington Park Historic District, the City of Fort Wayne will ensure that photographic documentation of the part of the Historic District that will be altered by this undertaking will take place. The photographs will concentrate on the following subjects:
 - 1. The streetscape and setting, including broad views of the main facades of buildings facing the street, within the parts of the existing State Boulevard and Eastbrook Drive that will be altered; and
 - 2. Those houses that contribute to the significance of the Historic District and that will be demolished. At least two photographs of each of those houses will be taken, and they will be taken from oblique angles in order to document all four elevations of each house.
- C. This documentation will include black and white prints of digital photographs and a digital video disc ("DVD") containing the photographs, recorded as closely as possible in keeping with the relevant standards of the version of the "Indiana IDNR – Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" that are in effect at the time.
 - 1. Separate sets of the photographs of the State Boulevard Bridge over Spy Run and of the photographs of the parts of the Brookview-Irvington Park Historic District will be prepared;

2. The photography will be conducted by a professional photographer or a qualified professional who meets relevant professional qualification standards of the Secretary of the Interior;
3. A draft set of photographs on DVD of the Bridge and a draft set of photographs on DVD of the Historic District will be submitted to the Indiana SHPO for review and approval within 30 days of receipt, and the Indiana SHPO has the discretion to require that photographs be retaken or that additional photographs be taken; and
4. After the Indiana SHPO has approved the sets of photographs of the Bridge and of the Historic District, the City of Fort Wayne will provide duplicates of the photographic prints and digital video discs to the Indiana SHPO, for ultimate transmittal to the Indiana State Archives, and to one or more libraries or other not-for-profit institutions in Fort Wayne that will commit to retaining them permanently and to providing the public with access to them.

III. EDUCATIONAL PROGRAMMING

The City of Fort Wayne will fund the research, design, manufacture, and installation of a series of four interpretative plaques to be placed at accessible locations. The plaques may include, but not be limited to: 1) discussion of Brookview Plat, 2) information about George Kessler's landscape design, 3) history of Vesey Park and Centlivre beer garden grounds, 4) the role of Civilian Conservation Corps or other WPA era programs in public projects.

The development of the proposed content and design of the plaques will be provided to the Indiana SHPO and consulting parties at ninety-five (95) percent completion for review and comment. If the Indiana SHPO does not respond within fifteen (15) days, acceptance will be assumed. If the Indiana SHPO responds with recommendations, a good faith effort to accommodate the recommendations will be made. The City of Fort Wayne will inform the SHPO and the consulting parties of its response to such recommendations and provide any revisions to the Indiana SHPO and consulting parties for their files.

4(f) Least Overall Harm Analysis

This section compares and summarizes the use of Section 4(f) resources associated with each alternative evaluated in the Section 4(f) Evaluation and leads to a determination of the alternative with the least overall harm to Section 4(f) properties.

Each the remaining four alternatives (3A, 3B, 3C, and 3D) result in the use of an identified 4(f) resources. Table 2 presents the comparison of alternatives showing the evaluation and use of the identified section 4(f) properties.

Table 2: Summary of Alternatives Evaluation and Use of Section 4(f) Properties

Project Impacts/Effects	Alternative 3A: Substandard Horizontal Curve Correction	Alternative 3B: State Boulevard on Existing Alignment	Alternative 3C: Shift State Boulevard Alignment South	Alternative 3D: 3 Lane Typical Section
Use of 4(f) Resources				
Brookview-Irvington Park Historic District	15 Contributing Property Relocations (residential)	18 Contributing Property Relocations (residential)	9 Contributing Property Relocations (residential)	15 Contributing Property Relocations (residential)
Fort Wayne Park and Boulevard System Historic District	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced	Clearing/altering landscape, State Blvd widened and elevated, bridge over Spy Run Creek replaced	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced	Clearing/altering landscape, Eastbrook Dr. access to State Blvd altered, State Blvd removed from historic location, bridge over Spy Run Creek replaced
Bridge over Spy Run Creek (Non-Select Historic Bridge)	Replaced	Replaced	Replaced	Replaced
Vesey Park	0.605 ac permanent, 0.122 ac temp ROW	0.313 ac permanent, 0.055 ac temp ROW	1.46 ac permanent, 0.092 ac temp ROW	0.517 ac permanent, 0.143 ac temp ROW
Factors for Consideration (774.3(c)(1)(i-vii))				
Ability to mitigate adverse effects	Moderate	Low	Mod-High	Moderate
Relative severity of remaining harm after mitigation	High	Highest	Moderate	High
Relative significance of each Section 4(f) property	High	Mod-High	Mod-High	High
Views of officials with jurisdiction(SHPO)-Adverse Effect for all alternatives	Severe	Most Severe	Less Severe	Severe
Relative satisfaction of Purpose and Need	High	Low	Mod-High	Mod-Low
Magnitude of any adverse effects to non-4(f) resources				
Neighborhood cohesion	+	=	=	+
Environmental Justice	=	=	=	=
Business Relocations/ Encroachments Outside of Historic Districts	=	=	--	=
CAC/Public Involvement	No Consensus	No Consensus	No Consensus	No Consensus
Additional residential building relocations	=	=	-	=
Natural Resources (streams, wetlands, forest)	-	-	--	-
Project Costs Estimates (millions)**	8	8.5	13.5	7.5

Key: ++ Very Positive Effect; + Positive Effect, = Status Quo; - Negative Effect; -- Very Negative Effect

Alternative 3A – Sub-standard Horizontal Curve Correction

Alternative 3A would require the relocation of 15 contributing properties from the Brookview-Irvington Park Historic District. In addition, the realignment of State Boulevard and change in elevation would cause a bifurcation of the district and the removal of contributing features from their historical location. A similar use of 4(f) resources, resulting from the alteration and removal of contributing features from their historical location would also occur to the Fort Wayne Park and Boulevard System Historic District. Alternative 3A also requires the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park.

Alternative 3A addresses the project's purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Alternative 3A also elevates the roadway above of the 100-year floodplain. Alternative 3A fully satisfies the projects purpose and need.

Alternative 3B – Existing Alignment Improvements

Alternative 3B would result in a similar use of Section 4(f) resources as Alternate 3A to properties from the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. This alternative would also require the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park. However, Alternative 3B would result in the relocation of 18 contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3B would address the flooding and congestion concerns by elevating the roadway and adding two additional travel lanes. However, this alternative would require level one design exceptions with regards to roadway geometrics as it does not correct the substandard horizontal curve. Therefore, Alternative 3B does not address the safety issues resulting from substandard sight distance and substandard geometrics. This alternative would require a higher number of residential and historic property relocations for construction as compared to other alternatives. Alternative 3D would not meet all the needs for the project.

Alternative 3C – Southern Most Alignment

Alternative 3C would result in similar use of Section 4(f) resources as 3A and 3B to properties in the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. This alternative would also require the replacement of the bridge over Spy Run Creek (non-select historical bridge) and minor right-of-way acquisition from Vesey Park. However, Alternative 3C would only result in the relocation of nine contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3C would address the flooding issue by elevating the roadway above of the 100-year floodplain. It would also address some of the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve. However, it introduces a new intersection at State Boulevard and Clinton Street which would create new congestion and traffic operational issues due to its close proximity to the Terrace Road intersection. Due to the introduction of new congestion and traffic operational issues, Alternative 3C would not meet all of the needs for the project.

Alternative 3D – 3-Lane Typical Section

Alternative 3D would result in the exact same use of Section 4(f) resources as Alternative 3A, including the relocation of 15 contributing properties from the Brookview-Irvington Park Historic District.

Alternative 3D addresses some of the project's safety concerns and the project's substandard geometrics through the correction of the substandard horizontal curve. It also elevates the roadway above of the 100-year floodplain. However, Alternative 3D does not fully address corridor connectivity or congestion along State Boulevard. This alternative would not address the congestion at the intersection of State Boulevard Clinton Street. The congestion associated with four lanes of traffic funneling into two lanes at the Cass Street and Clinton Street intersection would still remain. Alternative 3D would not meet all of the needs for the project.

Conclusion

Alternative 3B results in the most overall harm to Section 4(f) resources, requiring the relocation of 18 contributing properties. Alternative 3D and 3A result in the same use of Section 4(f) resources. However, Alternative 3A better satisfies the project's purpose and need. Alternative 3C appears to cause the least amount of overall harm to Section 4(f) resources with the anticipated relocation of only nine contributing properties.

The magnitude of adverse effects to non-4(f) resources associated with Alternative 3C is significant. Alternative 3C would also result in the relocation of four commercial businesses. Project costs associated with Alternative 3C would be an estimated five million dollars more than any other alternative due to the required relocation of the commercial businesses, a much longer bridge, and the reconstruction and elevated grade change along Clinton Street. Alternative 3C addresses the project's congestion and safety issues through the addition of travel lanes and the correction of the substandard horizontal curve and also elevates the roadway above of the 100-year floodplain. However, Alternative 3C introduces a new intersection at State Boulevard and Clinton Street, creating traffic operational issues due to its close proximity to the new Terrace Road intersection with State Boulevard. Therefore, Alternative 3C does not sufficiently satisfy the purpose and need of the project.

Alternative 3A is the only alternative that fully addresses the project's purpose and need. Both congestion and safety are addressed through the addition of travel lanes and the correction of the substandard horizontal curve. Alternative 3A also elevates the roadway above of the 100-year floodplain. While Alternative 3A has a greater number of contributing property relocations than Alternative 3C, the relative significance, value, and use of the 4(f) resource in Alternative 3A does not exceed the magnitude of adverse effects to non-Section 4(f) resources in Alternative 3C. In addition, the contributing properties relocated by Alternative 3A do not possess any unique features, when compared to the remaining properties in the Brookview-Irvington Park Historic District, which would make them individually eligible for the NR. Representative photographs of the relocated structures can be seen in Appendix 2. A significant portion of the contributing properties to be relocated by Alternative 3A are also located in areas that flood multiple times a year and thus continue to deteriorate at a relatively rapid rate.

In summary, the reduction of harm to Section 4(f) resources resulting from Alternative 3C does not outweigh the harm to non-Section 4(f) resources and properties adversely affected by this alternative. Therefore, among the remaining build alternatives which use 4(f) resources, Alternative 3A is considered the alternative which causes the least overall harm in light of the statute's preservation purpose. The proposed action includes all possible planning to minimize harm to each of the four identified 4(f) resources.

Agency Coordination

During the course of consultation, the following organizations have responded affirmatively to the invitation to join consultation: City of Fort Wayne; Friends of the Parks of Allen County; Allen County Historian; Indiana Landmarks—Northern Regional Office; Fort Wayne Historic Preservation Commission; ARCH, Inc.; Brookview Neighborhood Association; Indiana Historic Spans Taskforce; Irvington Park Neighborhood Association. Additionally, the following individuals or organizations participated in or requested to join consultation: Charley Shirmeyer, Northside Galleries; Albert Cohan, Westbrook 5, LLC; Thomas Niezer, Barret & McNaghy, LLP; Ronald Ross, Martin Riley Architects and Engineers; Dan Ernst, Earth Source, Inc.; Jan Dailey, State Boulevard Resident. (See Appendix B: Consulting Parties.)

In a letter dated April 16, 2009, Michael Galbraith writing on behalf of ARCH, Inc., requested that Friends of the Parks of Allen County and Brookview Neighborhood Association be invited to join consultation. (See Appendix F: Correspondence and Meeting Minutes and Appendix C: Consulting Parties.)

On April 23, 2009, SHPO wrote in response to the notification concerning the reconstruction of State Boulevard and requested a literature review, historic context, research methodology, property descriptions, and NR eligibility evaluations and recommendations to aid analysis of the project. SHPO recommended the Friends of the Parks and Boulevard Neighborhood Association, Indiana Historic Spans Task Force, and bridge historian Dr. James L. Cooper be invited to participate as consulting parties. (See Appendix F: Correspondence and Meeting Minutes.)

On December 7, 2009, Jan Dailey, State Boulevard Resident, wrote in response to the HPR: "I have reviewed the Historic Properties Report and find that it accurately describes the nature of the properties and their contributions to the Area of Potential Effects." In regard to the project, she stated, "While some may feel that redesigning the road and forever changing the integrity of the historic nature of State Boulevard is progress and must be accepted, this report more accurately reflects the feeling that residents of this neighborhood share." She also requested that "a separate study be conducted in possible land use of the former Kroger Fuel Center." (See Appendix F: Correspondence and Meeting Minutes.)

On December 8, 2009, Indiana Landmarks—Northern Regional Office wrote in response to the HPR. Landmarks agreed that Brookview-Irvington Park Historic District is eligible for the NR and suggested modifications to the HPR recommendations in light of NR nominations being composed by ARCH, Inc. Indiana Landmarks also requested more information on the proposed design in order to comment on a preliminary effect finding. Indiana Landmarks disagreed with the APE, asked some preliminary questions regarding the purpose and need in relation to historic properties, questioned the appropriateness of including a "trail bridge" in this Section 106 investigation, expressed the opinion that the "substandard horizontal curve" was a "character defining" element of the Brookview-Irvington Park historic district, and expressed the need for a "broad range of alternatives" to be included as part of the project options, and expressed concerns about the impacts of a different project on this Section 106 undertaking. (See Appendix F: Correspondence and Meeting Minutes.)

On December 9, 2009, ARCH, Inc. wrote in response to the HPR. Arch, Inc. agreed with the recommendation of eligibility for the Brookview-Irvington Park Historic District, noting that an NR nomination was being prepared. ARCH, Inc. requested the inclusion of proposed design maps, requested more detailed data regarding the project purpose and need, questioned the inclusion of the "trail bridge" in this Section 106 study, expressed the opinion that the "substandard horizontal curve" was a "character defining" element of the Brookview-Irvington Park historic district, disagreed with the APE, stated the importance of consulting "early in the undertaking's planning," expressed concerns about the impacts of a different project on this Section 106 undertaking and specifically stated "we believe that these projects must be aggregated for Section 106 Review. We also believe that if these houses south of State Boulevard were removed in order to avoid Section 106 Review that investigation into a possible violation of Section 110(k) of the NHPA (16 CFR 470) would be appropriate." Finally, ARCH, Inc. agreed with statements regarding flooding in the area, but stated they "contend that this is an issue which is recent."

In a letter dated December 10, 2009, Julie Donnell, president of the Friends of the Parks of Allen County, Inc. wrote in response to the meeting agenda and HPR. Donnell expressed concern over the project's Section 106 process, including the concern "that an extreme amount of expenditure has gone into solidifying this alternative, even after the concerns about historic preservation were brought to the attention of the City, contrary to what a Section 106 process would seem to demand, and that after that expenditure, the engineering study will be presented as that alternative at the meeting on December 15, or, if not, at some later date." The letter also commented on the Brookview Neighborhood, concurring with other consulting party comments on the resource and posing questions regarding the project's effects on the landscape, and expressed the integral importance of the landscape in the Brookview neighborhood's integrity. The letter requested considering the inclusion of the Cultural Landscape Foundation in the Section 106 process. (See Appendix F: Correspondence and Meeting Minutes.)

On December 14, 2009, SHPO wrote in response to the Draft HPR. Regarding the APE, SHPO wrote that “we are not yet prepared to comment on the adequacy of the APE.” SHPO commented on the HPR in the same letter, stating, “[o]ur initial impression is that the evaluations of above-ground properties contained in the HPR are probably accurate. However, we would like to hear the comments of other consulting parties at the meeting in Fort Wayne tomorrow before commenting in more detail on the HPR.” SHPO also wrote in response to the archaeological report that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [NR] within the area which was surveyed for this project by Archaeological Consultants of Ossian,” but noted that the final alignment was not yet determined and that further archaeological investigations may be necessary. SHPO asked for more information on the project alignment and the purpose and need. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held December 15, 2009, in Fort Wayne, consulting parties expressed concern with the APE used in the HPR, noted the importance of the “park-like setting” to the Brookview neighborhood, and questioned the selection of alternatives. (See Appendix F: Correspondence and Meeting Minutes.)

On January 27, 2010, SHPO responded to minutes of the consulting party meeting held December 15, 2009. SHPO requested more information regarding the purpose and need but stated that perhaps their questions would be answered in the forthcoming information packet for consulting parties. SHPO expressed concern about the Purpose and Need of the project. SHPO also asked for “clarification” on “the substandard nature of the roadway curvature on State Boulevard,” especially in light of statements from consulting parties “that the curves were intended by Arthur Shurcliff to contribute to a park-like setting for the residential area now known as the Brookview-Irvington Park Historic District, even though the curves were connected to relatively straight, east-west streets on either end that were known as, or later became, State Boulevard.” SHPO also stated “[w]e believe it is important for FHWA to evaluate this project’s purpose and need carefully before the Section 106 consultation proceeds much further. . . Clarifying purpose and need might result in a refinement of those key factors, which, in turn, might require consideration of alternatives that have not been presented to date.” Regarding the APE, SHPO asked some questions given the list of the alternatives provided at the December 15, 2009, consulting party meeting as well as in light of statements from consulting parties. “If . . . diversion of traffic onto other neighborhood streets foreseeably could increase traffic on streets that currently are lightly traveled, it seems to us that there might be indirect effects on historic properties outside the boundaries of the APE as currently proposed. Accordingly, we would appreciate it if further consideration were given to the possibility of such indirect effects and to the possible need to extend the APE to include areas that might be affected.” SHPO also stated that “we want to suggest that, at the appropriate time in the consultation, consideration be given to whether the southern boundary of the National Register-eligible district might have to be drawn at the new State Boulevard alignment, if the project is implemented as currently proposed.” (See Appendix F: Correspondence and Meeting Minutes.)

SHPO wrote on March 10, 2010, in response to the revised meeting minutes from the December 15, 2009, meeting. In the letter, SHPO stated that the Spy Run Bridge had been finalized as a Non-Select, NR-eligible bridge per the Indiana Statewide Historic Bridge Inventory. SHPO restated the understanding that Arthur Shurcliff intended “that part of what is now State Boulevard to have a park-like setting, which seems likely to be lost if the curvilinear character of that part of State Boulevard is diminished and if at least several more houses . . . that contribute to the Brookview-Irvington Park Historic District are demolished.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 15, 2011, Jill D. Downs, chairperson of the Preservation Committee of ARCH, Inc., wrote to the Deputy SHPO regarding American Structurepoint’s May 19, 2011, letter. Downs questioned whether the revised Purpose and Need would “trigger a new Section 106 review. It also appears as though American Structurepoint has deviated from proper Section 106 procedures by not copying consulting parties on their May 19 correspondence with you.” (See Appendix F: Correspondence.)

On June 16, 2011, John H. Shoaff wrote that as a member of the city council, they “face an unpleasant two-fold task of fighting for a properly democratic, participatory process...” (See Appendix F: Correspondence and Meeting Minutes.)

On June 16, 2012, Todd Zeiger, Indiana Landmarks, sent an email asking for clarification of whether consulting parties were to comment on the May 19, 2012, letter and requesting a thirty day extension to the review period. (See Appendix F: Correspondence and Meeting Minutes.)

On June 17, 2011, Julie Donnell of the Friends of the Parks of Allen County sent an email to American Structurepoint conveying her letter dated June 14, 2011, in which she requested an additional thirty days of review. She expressed surprise that changes were made to Purpose and Need without “communicating this.” In the text of the email, Donnell wrote: “In short, we believe that the current Section 106 process may have been circumvented by the extensive changes in the Statement of Purpose and would like to have time to respond.” The email also said, “We also continue to be very concerned that this project is being planned in detail before the DHPA has made any findings on the project.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 1, 2011, John H. Shoaff wrote to point out discrepancies in traffic numbers presented. (See Appendix F: Correspondence and Meeting Minutes.)

On July 5, 2011, SHPO responded to American Structurepoint’s letter of May 19, 2011. In their letter, SHPO wrote that it appeared appropriate to expand the APE “if it is foreseeable that traffic will increase significantly on other streets as a result of a limitation of access to or from State Boulevard being cut off or otherwise limited as a result of this project” and stated foreseeable “areas where the character of use of a historic property may be changed by a project could appropriately be included within the Section 106 APE, as well.” SHPO also requested American Structurepoint review previous correspondence and meeting minutes and “make a reasonable effort to respond to questions or issues raised there, if they have not already been dealt with in your May 10 letter.” SHPO also suggested that American Structurepoint share comments “that have been or shortly will be received in response to your May 19 and June 17 letters.” The letter re-stated comments from December 14, 2009, regarding the archaeology report. (See Appendix F: Correspondence and Meeting Minutes.)

Suzanne Slick, of the Irvington Park Neighborhood Association, sent an email on July 6, 2011, expressing disappointment with the project’s evaluation of impacts to neighborhood residents. The letter also stated, “There is little concern for the historic value of the roadway and surrounding neighborhood, little interest in the esthetics of the built structures in our quaint neighborhood, and little interest in its usability.” (See Appendix F: Correspondence and Meeting Minutes.)

On July 7, 2011, Michelle Briggs Wedaman of the Brookview Neighborhood Association emailed American Structurepoint and asked that her email address be updated in the project record and that she would provide comments on behalf of the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

At an Agency Coordination meeting held July 13, 2011, SHPO suggested that American Structurepoint coordinate to evaluate if the project would result in a need to change the NR district boundaries. SHPO also suggested that American Structurepoint more specifically address the consulting party issues and comments in coordination. It was also agreed upon that the ACHP should be invited to participate in the State Boulevard project at this stage in the Section 106 process, rather than later. (See Appendix F: Correspondence and Meeting Minutes.)

On August 29, 2011, Suzanne Slick wrote regarding the consulting party comment and response form. Slick wrote regarding the consultation process, “People who understand streets and cities and neighborhoods and quality of life issues and the impact that large public works projects have on historical, environmental, esthetic and safety elements have weighed in against this project with substantial legitimate objections, yet responses are

pat, formulaic, vague and evasive.” Slick expressed concern with the proposed project and provided links to websites associated with various aspects encountered in this project. (See Appendix F: Correspondence and Meeting Minutes.)

At a consulting party meeting held September 1, 2011, consulting parties questioned the response process and whether all comments had been shared. Consulting parties were encouraged to respond to any Section 106 correspondence, even if the thirty day time period had passed. An effort would be made to post all Section 106 documentation on the City of Fort Wayne’s website. Consulting parties suggested that the project include consultation with a professional landscape architect. It was also noted that the State Boulevard curve is included in the Fort Wayne Park and Boulevard System Historic District which is different from the Brookview-Irvington Historic District. SHPO requested the consultant “look at the implications of reduction the width of a new alignment. . .[and]. . . evaluate if such a design would result in fewer historic property impacts or fewer impacts to the Shurcliff design elements.” (See Appendix F: Correspondence and Meeting Minutes.)

On September 2, 2011, at the Agency Meeting with FHWA and INDOT, FHWA stated it would follow-up on its invitation to the ACHP, noting that the ACHP’s involvement in the process would be beneficial. During the meeting it was agreed that American Structurepoint would provide consulting parties with a more elaborate alternatives analysis, would look into developing a Section 106 page for this project on the City of Fort Wayne’s website, and that an addendum to the HPR would be prepared. (See Appendix F: Correspondence and Meeting Minutes.)

The ACHP responded to FHWA’s invitation to join consultation on September 22, 2011. ACHP requested additional documentation in order to “determine whether our participation in the consultation to resolve adverse effects is warranted.” (See Appendix F: Correspondence and Meeting Minutes.)

On November 7, 2011, SHPO responded to the material conveyed August 15, 2011, and September 29, 2011. Regarding the Brookview-Irvington Park Historic District, SHPO stated, “Having considered the marked aerial photograph shown at the last consulting party meeting, we do not believe that the historic district, as a whole, would be rendered ineligible by the preferred alternative.” However, SHPO added, the proposed realignment of State Boulevard within the district “is not an ideal situation from a [NR] boundary delineation standpoint.” Further, SHPO stated, “We think the Brookview-Irvington Park Historic District would suffer a loss of integrity of setting, feeling, and association from the preferred alternative that would exceed the sum of the contributing buildings that would be demolished.” SHPO also offered additional comments from the September consulting party meeting that had not been recorded in the meeting minutes regarding the alternatives analysis. SHPO also questioned the feasibility of converting the existing Spy Run Bridge into a pedestrian bridge. SHPO stated they would also recommend, “where practicable, the curbs or sidewalks of abandoned sections of Eastbrook and State be left in place to recall, at least faintly, Shurcliff’s landscape design of that part of the neighborhood, as was done when most of Westbrook south of State was abandoned to eliminate the Clinton Street-Westbrook intersection and to establish a rain garden.” SHPO also suggested shifting the proposed alignment somewhat to the east to better reflect Kessler’s original plan for connecting State Boulevard. SHPO noted that this change may “result in a somewhat longer and costlier bridge over Spy Run than would be required for the proposed alignment of 3A, but it appears that there could also be cost savings from the acquisition of fewer residences along State Boulevard. Even if the project costs were somewhat higher, we think there could be intangible benefits from preserving more of Shurcliff’s design of the Brookview-Irvington Park Historic District, while largely meeting the city’s purpose and need with an alignment of the new State Boulevard that would be somewhat closer to Kessler’s plan.” (See Appendix F: Correspondence and Meeting Minutes.)

On June 20, 2012, an Agency meeting was held to discuss the State Boulevard Project. At the meeting, American Structurepoint reviewed the responses to the SHPO letter of November 7, 2011, and agreed to send them in writing. It was decided to hold a meeting with consulting parties in early September to discuss the Additional Information HPR, to present the preferred alternative and to discuss the MOA. Mitigation ideas from

that meeting included: Advisory team similar to US 27; Photographic documentation of bridge over Spy Run; Restore character of State Boulevard within the district; and Educational mitigation.

On June 22, 2012, SHPO provided comment on the AI Report. In the letter, SHPO stated, “we agree with the conclusions of the AI Report regarding the eligibility or ineligibility, of properties within the [APE], for inclusion in the [NR].” SHPO agreed that the house at 315 East State Boulevard “does not appear to possess sufficient historical or architectural significance or integrity to be eligible of inclusion in the [NR].” SHPO also commented on the explanatory note contained in the Fort Wayne Park and Boulevard NR nomination form which stated the portion of State Boulevard within the Brookview-Irvington Historic District was individually eligible for the NR. SHPO stated, “we do not consider that comment . . . to confer individual eligibility on State Boulevard or any part of it.” SHPO further stated, “we do not believe that any part of the State Boulevard roadway, curbs, or sidewalks lying within the [APE] is individually eligible” for the NR, but added “[w]e do not disagree, however, with the Fort Wayne Park and Boulevard system nomination identification of the portion of State Boulevard in question as a contributing resource to that historic district.” (See Appendix F: Correspondence and Meeting Minutes.)

Regarding archaeology, SHPO stated, “Please be reminded that if the final alignment contains areas that were not surveyed by Archaeological Consultants of Ossian, then an archaeological reconnaissance of those areas will be required, in order to determine the presence of absence of archaeological resources.” SHPO noted that one example of areas that may need archaeological survey included “a residential lot that was outside the area surveyed, according to the depiction of the surveyed area in the original archaeological report.” If the entire lot would need to be acquired as part of the project, “then we would recommend that consideration be given to whether further archaeological investigation is needed. This might apply even if the alignment of the new roadway is essentially the same as it had been proposed at the outset of the Section 106 review process.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated July 31, 2012, the ACHP wrote that “[b]ased upon the information we obtained, we believe our involvement in consultation would be premature at this time. As such, we decline to participate in the consultation at this time.” However, the Council did request to be notified in the event of an Adverse Effect finding and at that time the Council would “re-evaluate the undertaking . . . and advise you whether or not we have changed our decision regarding participation in consultation.” (See Appendix F: Correspondence and Meeting Minutes.)

On August 13, 2012, the Indiana SHPO concurred with the archaeology short report (Stilwell, July 11, 2012) that “no further investigations appear necessary at these additional portions of the project area” and that the office had not identified any archaeological resources listed or eligible for listing in the NR. (See Appendix F: Correspondence and Meeting Minutes.)

At the consulting party meeting held on September 19, 2012, consulting parties were asked to provide input into mitigation for the proposed undertaking. Most comments focused on purpose and need for the project; some spoke about traffic issues. Michelle Briggs Wedaman (Brookview Neighborhood Association) asked for context sensitive solutions at the beginning of the project rather than the end. Susan Haneline (property owner) asked why the owners of the three residences being evaluated to remain were not consulted or asked if they wanted to remain in the homes. Todd Zeiger (Indiana Landmarks) encouraged the involvement of the ACHP because he feels that there was anticipatory demolition as part of a flood control project. He asked that it be noted in this documentation that there is a bifurcation of the district. Tom Cain (City of Fort Wayne) pointed out that everyone needs to recognize that the landscape character is important and the layout of human development patterns on that landscape are the significant components that make-up a substantial part of the historic resources of the neighborhood. The change in those landscape elements needs discussion in the documentation. The visual and special components of the larger landscape need to be understood so they can be addressed in a mitigation discussion. Michael Galbraith (ARCH, Inc.) encouraged ACHP involvement, objected to the change

in historic consultant, asserted that the APE is inappropriate, and raised the question of cumulative impacts. Edward Welling (Friends of the Parks of Allen County) said that mitigation is premature since the APE is not appropriate; the MOA should be postponed until Environmental Assessment is complete. Mitigating for the larger landscape design impacts would create a condition that is more in line with the characteristics planned for the area. This should be the bigger issue addressed rather than the small detail of specific structures. Dr. James Glass (Deputy SHPO) expressed reservations that consensus can be developed for this project; he stated that this meeting was the time for consulting parties to put forth mitigation ideas. John Carr (SHPO staff) requested any ideas on ways to conserve more of the character defining features of the two historic districts, emphasizing the tangible physical features as a priority discussion. Mr. Galbraith objected to the timing of the consulting party meeting; Patrick Carpenter, manager of the INDOT-CRO, said that the timing was established so that consulting parties could discuss mitigation and formulate new ideas. Ms. Wedamen said that she did not believe that the public process has been followed. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 14, 2012, Karl Dietsch wrote regarding a safety issue in the proposed project area. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated September 17, 2012, 11 residents of the Brookview Neighborhood jointly submitted a letter regarding the State Boulevard project. The letter expressed support of the project. The residents stated, “We STRONGLY support the buyout of our homes thereby allowing for State Boulevard to be relocated to the south of its current location” and went on to conclude, “We are NOT in favor of finding ways to retain our homes within the footprint of the project; we feel this will lessen our property values, continue to cause issues with access to our homes, and leave the constant flooding issue unresolved.” (See Appendix F: Correspondence and Meeting Minutes.)

Sara Kruger Geyman, a member of the public, wrote in response to the meeting held September 19, 2012. (Note that the letter conveying responses to the consulting party meetings was dated August 21, 2012, and is likely a typo.) Geyman expressed concern “that residents are not and have not been consulted in this matter” and expressed dissatisfaction with meeting’s facilitation. Geyman offered comments to the project in general, objecting to its necessity and, regarding Section 106, stating: “Mitigation is premature in a plan and a process that has been faulty from the beginning. It is a proverbial lollipop stuck in the hands of resident to quiet them down and distract them from the truth.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 1, 2012, Susan R. Haneline, a Brookview neighborhood homeowner, expressed support for the project, noting that the current problems with flooding and bridge deterioration “do nothing to showcase what IS historical about the neighborhood.” Haneline added, “We CAN retain the beauty of the neighborhood, we CAN celebrate its design and vision. What we don’t have to do is force homeowners to retain properties that are simply, in and of themselves, of no historic value, nor necessary to the overall feeling of the neighborhood.” Haneline’s letter also included photographs showing recent flooding in the neighborhood. (See Appendix F: Correspondence and Meeting Minutes.)

Susan Haneline submitted an additional letter dated October 2, 2012. Haneline stated the current proposed design, “seems . . . to actually enhance historic vision, not cause it to be destroyed.” Haneline offered suggestions to “respect the historic vision,” including: 1.) “Installing historically correct lighting in the area”; 2.) “Plantings and green space that gives the area a park like feel, such as period style benches, grouping of trees and flowers, perhaps even brick style sidewalks”; 3.) “stone or brick entrance pillars for the neighborhood”; 4.) adding trees and flower beds to the bifurcated State Boulevard; 5.) “small monuments” conveying the history of the neighborhood and Arthur Shurcliff; 6.) “find ways to encourage people both inside and outside the neighborhood to spend time in the open green spaces.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 3, 2012, John Shoaff wrote regarding the project, consulting party meeting, and 800.11 materials. Shoaff wrote, "I cannot support the current State Boulevard widening plan in anything like its present form. . ." In particular, Shoaff objected to plans to elevate the road as a "perversion of the proper use of the 'Bypass and Arterial concept' . . ." Shoaff identified "two legitimate needs" in the Brookview neighborhood: the repair or replacement of the Bridge over Spy Run Creek and the elimination of a "blind spot at the foot of State Boulevard, near the intersection with Westbrook." Shoaff stated that project plans should address these needs but be "minimally harmful to the historic district." Shoaff added that discussion of project planning and mitigation discussion "should await the outcome of the Environmental Assessment." (See Appendix F: Correspondence and Meeting Minutes.)

Shoaff also included comments on the September 19, 2012, consulting party meeting. Shoaff responded to comments received by Michelle Briggs Wedaman from FHWA's representative. Shoaff objected to the facilitation of the meeting stating "the proceedings were far from impartial, and were guaranteed to further alienate citizens from their government."

Shoaff enclosed letter "signed by 14 neighborhood association presidents and one vice-president, representing over 11,000 households, that was sent to the mayor and all city councilmen." The letter objected to the State Boulevard project. (See Appendix F: Correspondence and Meeting Minutes.)

Also on October 3, 2012, Suzanne Slick wrote regarding the project and the consulting party meeting of September 19, 2012. Slick stated that not building the project is preferable to mitigation and objected to the facilitation of the consulting party meeting. The letter re-stated some comments offered previously by consulting parties regarding the Purpose and Need and design. Slick objected to the traffic data previously supplied by American Structurepoint and offered two examples in which she found low-volume traffic while utilizing the State Boulevard. Slick stated the APE was inappropriate. (See Appendix F: Correspondence and Meeting Minutes.)

Julie Downs, Friends of the Parks of Allen County, submitted comments via a letter dated October 3, 2012. Downs stated the Friends of the Parks of Allen County agreed with the finding of adverse effect for the project but added "any discussion of mitigation is, at best, premature; at worst, the proposed [MOA] is a bad faith attempt to confuse an already complicated and unfair process." Downs also stated the "APE is not comprehensive enough and should include historic districts along State Boulevard" and "it is only prudent to postpone any and all discussion of mitigation until after the Environmental Assessment is complete." Finally, on behalf of members of the Friends of the Parks of Allen County who attended the September 19, 2012, consulting party meeting, Downs objected to the facilitation of the meeting and concluded, "Under these circumstances, the public is not being served properly at all." (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Jill Downs wrote regarding the 800.11(e) and draft MOA. Downs agreed with the project's adverse effect finding but noted "the process that has been undertaken regarding the development and progression of this project has created a rather hostile environment resulting in a breakdown of the needed understanding and collaboration" and pointed to the September 19, 2012, consulting party meeting as proof of this breakdown. She stated it was premature to discuss mitigation because the Environmental Assessment had not been completed; the bifurcation of the district, elevation of State Boulevard, and the Pufferbelly Trail project should be added to the list of adverse effects; the Pufferbelly Trail project should be incorporated into the effects discussion; and the project has not fully accounted for the previous removal of several homes by the City of Fort Wayne which creates the impression of less impact as a result of the project. Downs concluded by stating she did not see the need to reconstruct State Boulevard. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michael Galbraith of ARCH, Inc., wrote formally requesting an extension of the thirty-day comment period for the proposed MOA and mitigation measures. Galbraith stated, "We do not in any form, fashion, or manner concur with the proposed mitigation as present either in the draft supplied with the

FHWA 4(f) compliance document or in the presentation narrated by American Structurepoint and Dr. Weintraut. Galbraith also stated that “we fail to understand how a draft MOA can be developed prior to all of the information being in hand about potential design alternatives to avoid impact.” (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Michelle Briggs Wedaman of the Brookview Neighborhood Association, wrote requesting a thirty-day extension of the consulting party comment period to incorporate the material provided on September 18, 2012, into their comments. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the revised Section 800.11 documentation in the Environmental Assessment.) Wedaman stated that previous questions from the December 2009 and September 2011 consulting party meetings “have remained unanswered,” particularly those dealing “Purpose and Need, exploration, documentation and analysis of current conditions and likely impacts of this project, and about the area of impact of this project.” Wedaman questioned how an appropriate discussion of mitigation could take place prior to the completion of the environmental assessment. (See Appendix F: Correspondence and Meeting Minutes.)

The SHPO wrote in response to the project in a letter dated October 4, 2012. SHPO concurred with the opinion of the archaeological short report, the Section 106 finding of effect and that the Fort Wayne Park and Boulevard System, Brookview-Irvington Park Historic District, and Bridge on State Boulevard over Spy Run would all be adversely affected as part of this undertaking. SHPO expressed concern “about the extent to which the removal of all houses along the south side of existing State Boulevard between Terrace Road and Eastbrook Drive would change the setting of that interior part of the Brookview-Irvington Park Historic District and suggested some minimization measures. In particular, SHPO wondered if “it would be feasible to eliminate the sidewalk along the north side of the proposed new alignment of the reconstructed State Boulevard between Terrace Road and Eastbrook Drive.” SHPO expressed sympathy for the preference of some property owners along the south side of State Boulevard who preferred to have their entire property, rather than a smaller portion, purchased; “however, we think that preserving even three houses (112, 134, and 138 East State Boulevard) along the south side of the existing State Boulevard that contribute to the Brookview-Irvington Park Historic District would help to reduce, but not eliminate, the adverse effect.”

SHPO also offered suggestions for design for minimizing impacts and suggestions for mitigation, including an advisory team, use of context-sensitive designs, photographic documentation of the Bridge over Spy Run. (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Todd Zeiger of Indiana Landmarks—Northern Regional Office wrote formally requesting a thirty-day extension on the comment period in light of the material conveyed September 18, 2012. (Please note that in an email sent October 5, 2012, INDOT declined to extend the comment period for this project, noting consulting parties and the public would have an opportunity to comment on the Environmental Assessment.) Zeiger stated “We do not in any form fashion or manner concur with the proposed mitigation as presented either in the draft MOA supplied with the FHWA 4(f) compliance document.” Zeiger added “we fail to understand how a draft MOA can be developed prior to all of the information being in hand about alternative design alternatives to avoid impact. Additional time is needed to evaluate that information and assess it within the context of the other informant provided in the 4(F) document.” (See Appendix F: Correspondence and Meeting Minutes.)

In a letter dated October 4, 2012, Tom Cain, Fort Wayne urban designer and Creager Smith, Fort Wayne historic preservation planner, wrote regarding the project. Both agreed with the project’s adverse effect finding. The letter listed twenty-one specific adverse effects of the project on the landscape to serve as the “potential basis of mitigation measures.” Cain and Smith also stated “we are available to assist in the development of mitigation

design features that can restore and recollect historic features where possible, and to integrate new features within the historic contexts of the Brookview-Irvington Park Historic District and the Fort Wayne Park and Boulevard System Historic District. We agree with the proposal put forth in the draft Memorandum of Agreement to form an Advisory Team, and we are both available to serve on a team.” (See Appendix F: Correspondence and Meeting Minutes.)

On October 15, 2012, Tom Cain, City of Fort Wayne, called W&A to inquire whether SHPO will change their assessment of project impacts. Cain explained that the City of Fort Wayne is ready to prepare mitigation but wanted to make suggestions within the context of SHPO’s assessment of project impacts so that the City may address all adverse effects. Cain also stated that impacts to the Brookview neighborhood should be enumerated. (See Appendix F: Correspondence and Meeting Minutes.)

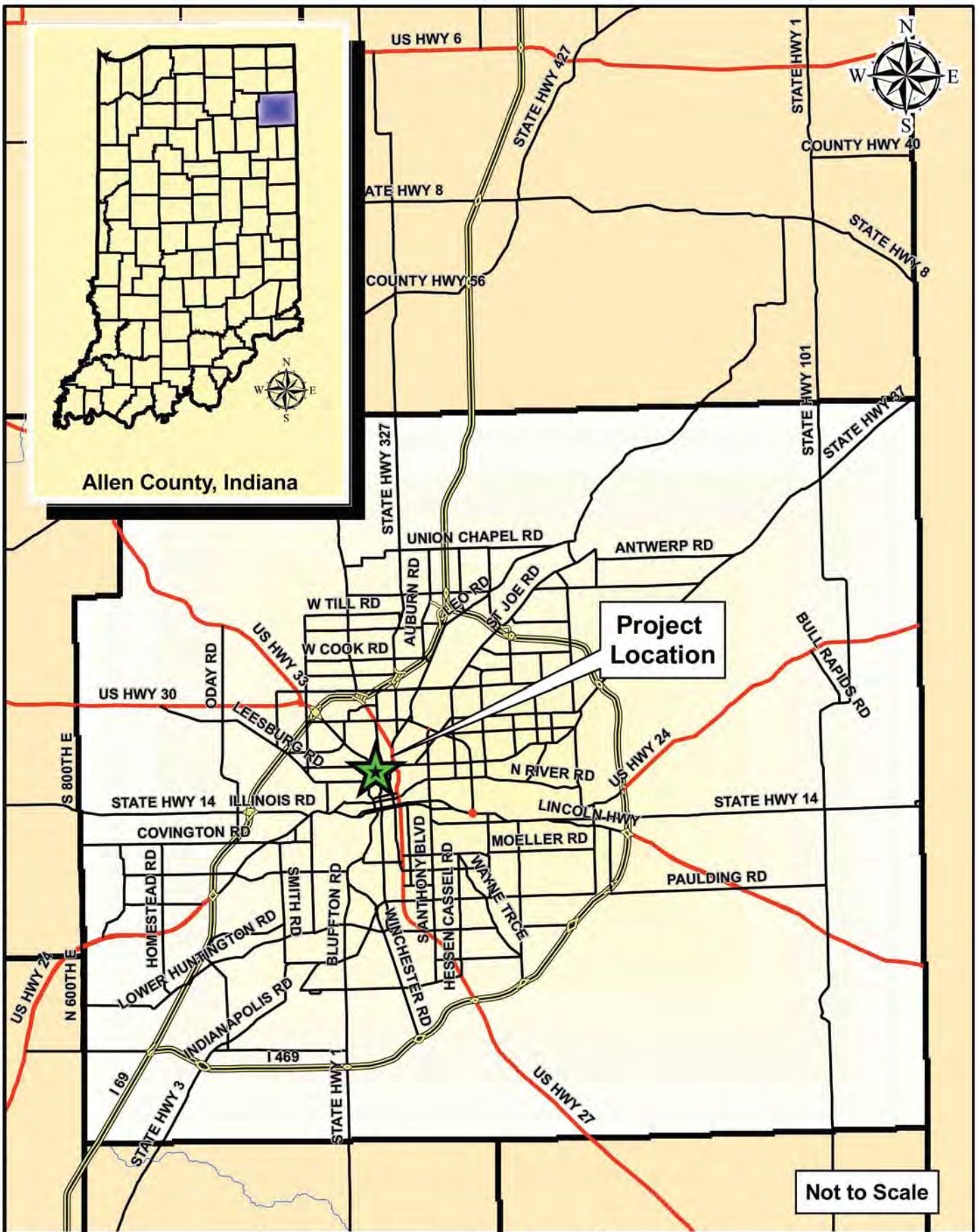
On October 16, 2012, W&A contacted Tom Cain in response to his phone call the previous day. W&A explained that American Structurepoint was very glad to have his input on this project and, at a minimum, would consult with him prior to the agency meeting. Cain spoke about the landscape changes that would take place as a result of the undertaking, particularly the changes from private to public space around the undertaking. He said that originally the areas along Spy Run had been grassy plain with a tree canopy; secondary growth was a result of a lack of maintenance beginning in the 1970s. Cain stated he would like for mitigation to deal with changes in scale that will occur; tree planting should occur within three feet of the roadway (and not the standard ten feet required on highways.) Cain stated this would change the scale of the undertaking for the residents. Cain also stated he would convey additional mitigation suggestions via email and stated the importance of achieving the “right feel” for the space. (See Appendix F: Correspondence and Meeting Minutes.)

On November 15, 2012, SHPO wrote in response to American Structurepoint’s offer to draft specific language for the MOA. (See Appendix F: Correspondence and Meeting Minutes.)

On December 18, 2012, American Structurepoint invited representatives from FHWA, INDOT, SHPO, and the City of Fort Wayne to meet to discuss landscape mitigation that has been developed by the City of Fort Wayne. Thomas Cain (landscape architect/City of Fort Wayne) made the presentation. Cain’s plan looked at larger scale issues of community rather than focusing on the individual resources. He wished to borrow a pastoral model of streets with houses on one side of the road, while retaining visual site lines as a ghost vision of the Shurcliff plan of the plat. He advocated use of native trees and disguising the change in slope by using larger trees at the periphery. Smaller trees would recall the footprint of the houses; he suggested the use of curbs, trees, and historic plaques to educate the public regarding the lost elements of the district. (See Appendix A, Plans.) Dr. James Glass (SHPO) expressed appreciation for the effort Mr. Cain had put forth for a thoughtful landscape plan. Dr. Glass said that his office needed time to digest but that he understood Mr. Cain’s point that in a Section 106 sense, there was a need to mitigate for the houses and for the loss of historic character. He also understood that there are larger issues of flood control and engineering that make this project difficult. There was discussion of other resources that may be preserved as far as compensation for the lost historic resources (houses and landscaping). It was agreed that SHPO would be given time to digest the landscape design presented at the meeting and that the City and its consultants would look for additional ways to mitigate, such as grants to rehabilitate the facades of existing houses (if practical and legally viable to do so), landscaping along the waterways, and rehabilitating an existing bridge for the loss of the Bridge over Spy Run. Mary Ann Naber (FHWA preservation officer) suggested that the attendees look at the mitigation provided in Tampa. (See Appendix F: Correspondence and Meeting Minutes.)

Appendix 1 – Mapping

State of Indiana Map



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Project Location Map

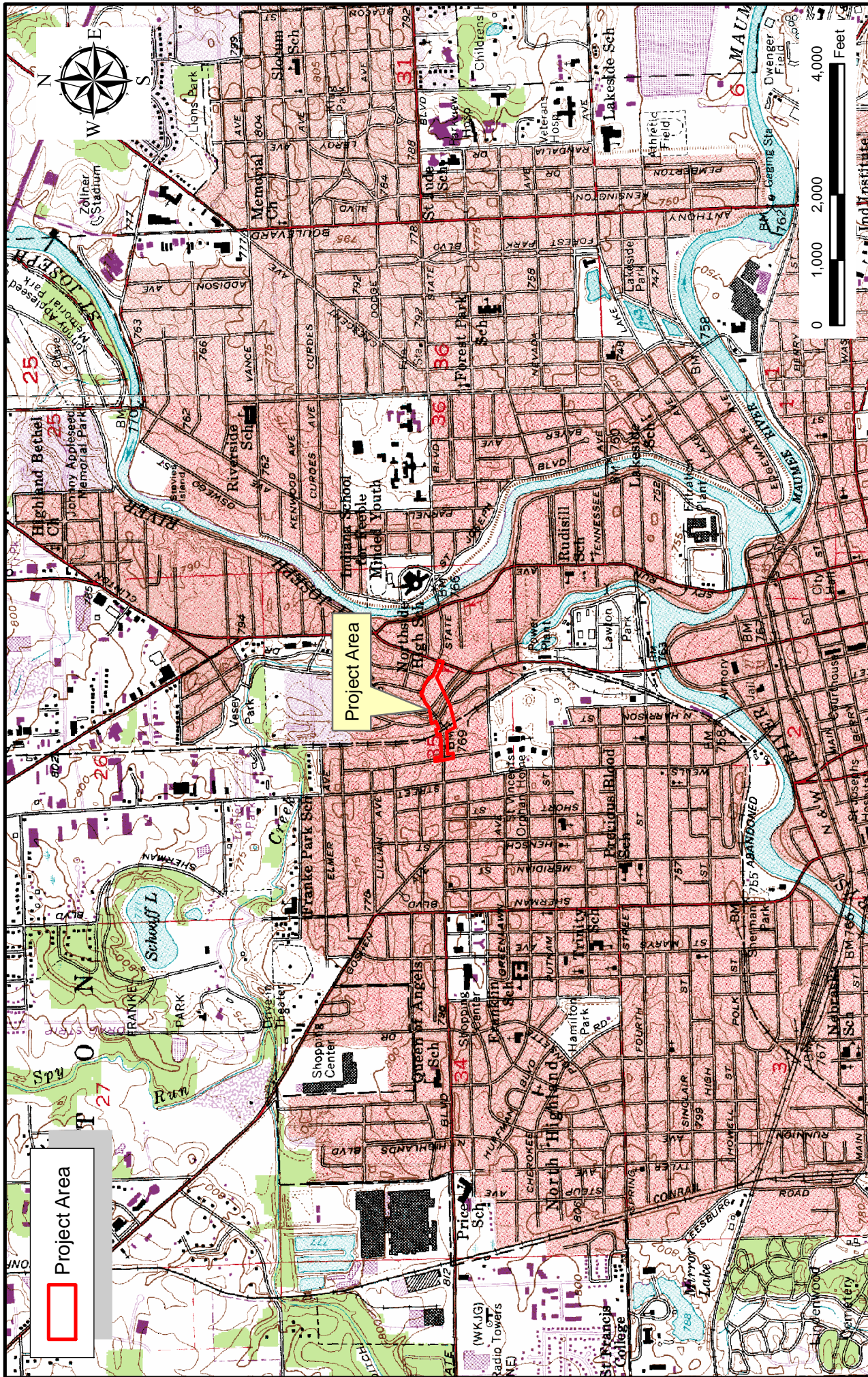
Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 03/12/2009

USGS Topographic Map



USGS Topographic Mapping

Fort Wayne West and Fort Wayne East Quadrangles

Applicant: City of Fort Wayne, Board of Public Works
420 City-Council Building
One Main Street
Fort Wayne, Indiana 46802

State Boulevard Reconstruction

Des. No. 0400587

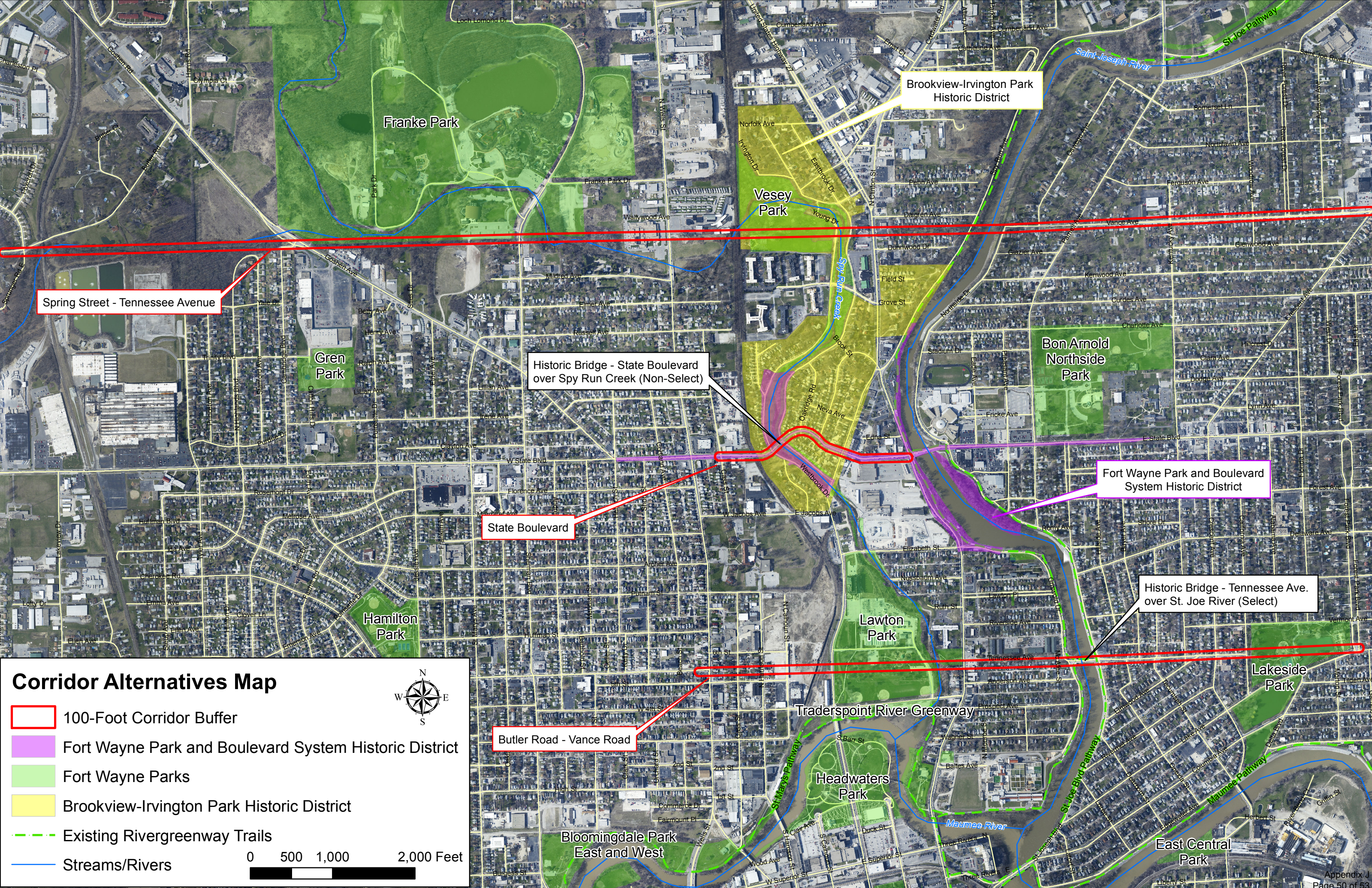
Location: Fort Wayne
Township: Wayne
County: Allen
State: Indiana

Date: 07/22/2010

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Aerial Photography/Area of Potential Effect



Corridor Alternatives Map

100-Foot Corridor Buffer

Fort Wayne Park and Boulevard System Historic District

Fort Wayne Parks

Brookview-Irvington Park Historic DistrictExisting Rivergreenway TrailsStreams/Rivers

05001,0002,000 Feet

N

W

E

S

**See EA Appendix D, Pages D-2 to
D-485**

Appendix 2 – Section 106 800.11(e) Documentation

Section 106 Findings and Determinations

Section 800.6(a)(3) Documentation

- A. Plans**
- B. APE Maps and Site Plans**
- C. Consulting Parties List**
- D. Photos**
- E. Report Summaries**
- F. Correspondence**
- G. Memorandum of Agreement (MOA)**



IN REPLY REFER TO:

United States Department of the Interior

OFFICE OF THE SECRETARY
Office of Environmental Policy and Compliance
Custom House, Room 244
200 Chestnut Street
Philadelphia, Pennsylvania 19106-2904



July 8, 2013

9043.1
ER 13/0365

Ms. Karen A. Bobo
Acting Division Administrator
Federal Highway Administration
575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

Dear Ms. Bobo:

As requested, the Department of the Interior (Department) has reviewed the Draft Section 4(f) Evaluation for the Proposed State Boulevard Added Travel Lanes Project, Fort Wayne, Allen County, Indiana. The Department offers the following comments and recommendations for your consideration:

Section 4(f) Comments

This document considers effects to four identified properties in the project study area eligible to be considered under Section 4(f) of the Department of Transportation Act of 1966 (codified at 49 U.S.C. 303§ 771.135) associated with the State Boulevard project. The State Boulevard project extends from Spy Run Avenue (U.S. 27 northbound) to Cass Street within the city of Fort Wayne, Indiana; it is intended to reduce existing congestion and improve traffic flow. Currently State Boulevard is a 4-lane road from east of Maplecrest Road to Spy Run Avenue, then reduces to 3 lanes west of Spy Run Avenue. East of Clinton Street, State Boulevard is a 2-lane road with 1 travel lane in each direction. To the east of the project area, Goshen Road merges into State Boulevard which has the effect of doubling the daily traffic volume.

This evaluation, prepared by Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA), considered the impacts to three properties eligible for the National Register of Historic Places, including a portion of the Fort Wayne Park and Boulevard System Historic District, which includes the system of 11 parks, 4 parkways, and 10 boulevards envisioned by Charles Mumford Robinson and George Kessler and based on the City Beautiful Movement. Also impacted is a portion of the Brookview-Irvington Park Historic District containing a total of 424 contributing resources including houses, garages, and the combined plats of the district, as well as the Bridge over Spy Run, a reinforced concrete girder, T-Beam bridge constructed in 1927, determined eligible on its own. Finally, the fourth property impacted is Vesey Park operated by the City of Fort Wayne Parks Department; it includes green space

along Spy Run between Eastbrook Drive and Westbrook Drive. The park features open space with areas for picnicking and views to Spy Run Creek.

For the Fort Wayne Park and Boulevard System Historic District, the preferred alternative will avoid all structures but will impact about one-half acre of land within the district. For the Brookview-Irvington Park Historic District, 15 structures will need to be removed and 2.6 acres of the district will be converted to transportation purposes. The Bridge over Spy Run will be replaced; FHWA and INDOT propose to use a programmatic 4(f) historic bridge determination for this property. And finally, FHWA and INDOT propose to make a *de minimis* determination for the impacts to the Vesey Park property, which would result in a little more than one-half acre of the park permanently converted to transportation use, as well as a small amount of park property for temporary construction use.

The Department concurs with the FHWA and the INDOT on a determination of no feasible or prudent alternative to the preferred alternative, if built as proposed, which would result in impacts to eligible properties. Constrained linear features such as State Boulevard offer few good alternatives when 4(f) resources have grown up on either side of the corridor and the functionality of the feature becomes compromised by growing populations. The detail of the consultation process found in the evaluation demonstrates that at least as recently as this last December, there was still considerable disagreement over the project and its mitigation. The Department cannot concur with the INDOT and FHWA because there is no evidence that all parties, including the State Historic Preservation Officer, have agreed to the mitigation measures, nor is there evidence in the evaluation that the Memorandum of Agreement been signed. We will reserve our concurrence with the hope that the final 4(f) will present the necessary agreements.

The Department has a continuing interest in working with the FHWA and the INDOT to ensure impacts to resources of concern to the Department are adequately addressed. For issues concerning section 4(f) resources, please contact Regional Environmental Coordinator Nick Chevance, Midwest Regional Office, National Park Service, 601 Riverfront Drive, Omaha, Nebraska 68102, telephone 402-661-1844.

We appreciate the opportunity to provide these comments.

Sincerely,

A handwritten signature in black ink, appearing to read 'Lindy Nelson', with a stylized flourish extending to the right.

Lindy Nelson
Regional Environmental Officer